

TORONTO STAFF REPORT

January 15, 2002

To: North York Community Council

From: Director, Community Planning, North District

Subject: Final Report
Bessarion-Leslie Context Plan
Ward 24 - Willowdale
File No. UD03-BES

Purpose:

This report recommends a Context Plan for the Bessarion and Leslie Nodes to guide consideration of the Canadian Tire and other future development proposals for lands included within the Context Plan area.

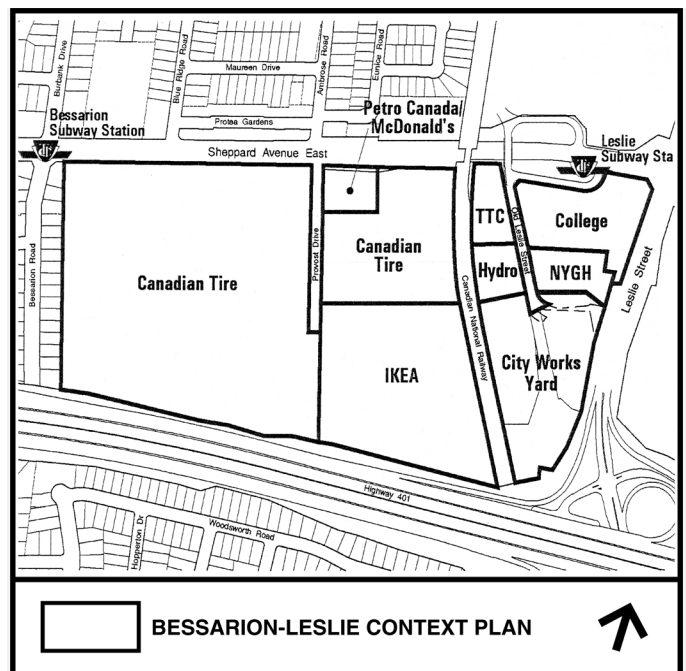
Financial Implications and Impact Statement:

There are no financial implications resulting from the adoption of this report.

Recommendations:

It is recommended that City Council:

- (1) Adopt the Context Plan for use as an implementation tool to guide the review of the Canadian Tire development application as well as other future development applications for lands included within the Context Plan area.
- (2) Direct staff to enter into negotiations with the Toronto District School Board and the Toronto Catholic District School Board on a joint use City park, community centre and school facility, and to report back prior to or at the



same time as the Canadian Tire subdivision application.

Background:

In February 2000, Canadian Tire submitted a redevelopment proposal for their lands located south of Sheppard Avenue, east of Bessarion Road and west of the railway tracks. These lands are located in the Bessarion and Leslie Nodes of the Sheppard East Subway Corridor Secondary Plan (OPA 392). The Plan enables Council to require the formulation of a context plan for key development areas to ensure co-ordinated development. North York Community Council, at its meeting of May 2, 2000, directed staff to undertake a Context Plan study of the Bessarion and Leslie Nodes with regard to the objectives of the Sheppard East Subway Corridor Secondary Plan.

The general objectives of the Sheppard East Subway Corridor Secondary Plan are to:

- Encourage high quality development in support of and to take advantage of the Sheppard Subway;
- Encourage development to occur in key development areas;
- Protect existing stable residential areas;
- Manage development within the capacity of existing and committed transportation and servicing infrastructure;
- Provide sufficient community amenities to serve future residents; and
- Monitor development and introduce measures as required to ensure compatibility with existing communities.

The Secondary Plan identifies a potential road connection to Leslie Street to address capacity issues at the Leslie/Sheppard intersection. The City initiated a Class Environmental Assessment study to address road capacity constraints in the Leslie/Sheppard area and improve access and circulation in this area. The EA study, which is being reported on separately by Works and Emergency Services, has formed an important component in the Context Plan study.

In addition, Planning staff have undertaken a review of community services and facilities along the Sheppard corridor area between Bayview Avenue and Don Mills Road. This review also forms a component of the Context Plan study and the results relating to the Context Plan area are included in this report. The results of the overall community services and facilities review will be reported on separately.

Proposal

Canadian Tire submitted an Official Plan and zoning amendment and plan of subdivision application to the City in February 2000 to permit a mixed-use development including the following:

- Apartments, townhouses, a new Canadian Tire store, additional retail uses, and a public park west of Provost Drive
- Office buildings, apartments and townhouses east of Provost Drive.

The proposal was for a total of 426,852 m² of gross floor area including 3,974 residential units.

The application is the first major redevelopment proposal in the Bessarion and Leslie Nodes under the new Sheppard East Subway Corridor Secondary Plan.

In considering this proposal, it became apparent that the application could not be looked at in isolation from the rest of the lands in the Bessarion and Leslie Nodes and that a context plan addressing a network of roads, density distribution, building heights, parks and open space, and community facilities, should be prepared. The context plan would generally identify municipal expectations for this area's development to the public, landowners and the development community, and serve as a framework for Council to evaluate this and other development applications.

Study Process and Community Consultation

The Context Plan study has involved extensive participation from community members, City staff and outside agencies. The study process has included an interdepartmental and agency staff team, resident steering committee and work groups as well as meetings with landowners. A public information centre was held in February 13, 2001 where information on the Context Plan study, community services and facilities review and environmental assessment study was presented to the community. Interested residents were invited to join a steering committee, which met on April 10, 2001. From this group, residents and landowners signed up to join transportation and urban design work groups. The work groups met 6 times over a 5 month period developing land use, built form, height, transportation and design concepts. There was considerable community involvement in formulating the Context Plan guidelines. The steering committee met again on October 10, 2001 to discuss the Context Plan coming out of work from the work groups and City staff.

A community wide meeting was held on November 28, 2001 to present the Context Plan to the larger community. Approximately 100 area residents and interested landowners attended the meeting.

Site and Surrounding Area

The Context Plan study area is 36 hectares (89 acres) in size. The study area is bounded by Sheppard Avenue to the north, Highway No. 401 to the south, Leslie Street on the east and the rear of the residential lots along Bessarion Road on the west.

The area covered by the Context Plan includes a variety of uses and landowners:

- West of Provost Drive: An existing Canadian Tire store, gas station, and warehouse facility.
- Between Provost Drive and the railway tracks: An existing Ikea store, vacant Canadian Tire lands, gas bar/car wash and McDonald's restaurant.

- Between the railway tracks and Leslie Street: TTC bus/subway station and commuter parking lot, GO train platform, lands for new fire hall, City works yards, Canadian College of Naturopathic Medicine, and North York General Hospital lands.

This area has a high level of transportation services and accessibility:

- The Sheppard subway runs along Sheppard Avenue.
- A new subway station just east of Bessarion Road.
- A new subway station at Leslie Street with entrances at Old Leslie Street and provisions for an entrance on the west side of the railway track.
- A bus station and GO train station.
- Adjacent to the ramps to Highway 401.

The lands around the Context Plan study area include the following:

- Low density residential neighbourhoods to the north and west.
- Commercial uses on the north side of Sheppard Avenue between Burbank Drive and Blue Ridge Road.
- Office and medical office buildings on the north side of Sheppard Avenue adjacent to the railway track.
- Valley lands and a branch of the Don River traversing the Sheppard/Leslie intersection.
- North York General Hospital located on the east side of Leslie Street.

Official Plan

In 1996 North York Council adopted the Sheppard East Subway Corridor Secondary Plan (OPA 392). The Plan introduced a new secondary plan with policies to manage and direct future development in the Sheppard Corridor. The Secondary Plan was recently approved by the OMB.

The lands in the Context Plan area are subject to the Sheppard East Subway Corridor Secondary Plan which identifies them as key development areas and part of the Bessarion and Leslie Nodes. The Plan designates most of the lands in the Context Plan area as Mixed Use, which is intended to accommodate a mix of residential, commercial, institutional and recreational uses. The IKEA site is designated Commercial. The Secondary Plan sets out densities for the Mixed Use lands ranging from 2.0 FSI to 3.0 FSI. The Plan also sets out specific policies for different lands within the Context Plan area (see Attachment 4).

The lands west of Provost Drive are located within the Bessarion Node. The Bessarion subway station is viewed as a walk-in community station serving the immediate area. There is a specific development policy for the Mixed Use lands west of Provost Drive, which encourages both residential and employment uses, with more commercial uses toward the Leslie Node. There are to be transitions in density, height and massing toward the stable residential area to the west. The policy encourages retail uses near the Sheppard Avenue frontage, including the southwest corner of Sheppard and Provost. Buildings along Sheppard Avenue are to be a maximum of 6 storeys with an increase in height towards the Leslie Node.

The lands east of Provost Drive to Leslie Street are in the Leslie Node, which is recognized as a mainly employment development node with superior transit accessibility provided by the Leslie subway station and the Oriole GO station. The lands west of the rail track are intended for residential and employment uses, while the lands east of the rail track are to be mainly for employment and service commercial uses as well as institutional uses and service uses complementary to the hospital and naturopathic college.

The Secondary Plan also identifies potential park lands in the Context Plan area, and seeks the opportunity to include school sites in the lands west of the rail track.

Zoning

Most of the lands are zoned M2 (Industrial Zone Two) permitting industrial and some commercial uses. The IKEA site is zoned C1(6), which is a commercial zoning with site specific regulations. The lands on the east side of Old Leslie Street are zoned C2 (Local Shopping Centre Zone), G (Greenbelt) and R3 which is a detached dwelling residential zone.

Comments:

New Road Network & Traffic Infiltration

The Context Plan includes a network of public roads, walkways and bicycle routes and paths providing access to and through this area. The road network includes a new east-west road to link Leslie Street to Provost Drive (which is the subject of a Class Environmental Assessment study), which is intended to function as a major urban collector. New local roads are proposed as well as improvements to Provost Drive, Leslie Street and Sheppard Avenue. The layout of streets and blocks is intended to capitalize on views and vistas from the site to the Don River valley and the neighbourhood park.

In setting out a new road network to serve the lands south of Sheppard west of the rail track, staff examined a number of options including aligning the new roads with the existing roads to the north, offsetting the new roads so they do not link up with the existing roads north of Sheppard Avenue, having one way streets, and closing of streets. Neighbourhood residents have expressed concerns that traffic from the new development south of Sheppard Avenue would result in traffic infiltration through the existing residential communities north of Sheppard. To address this concern, the Context Plan includes a proposal that the two new streets that are to align with Blue Ridge Road and Ambrose Road have road diverters installed which would enable traffic to either turn left or right but would not permit through traffic. The same road diverters are also proposed for Blue Ridge and Ambrose. The road diverters would be reinforced with a by-law restricting through traffic. This would make it difficult for traffic to infiltrate the existing residential communities using Blue Ridge or Ambrose.

Monitoring of the effectiveness of these tools will be carried out in accordance with the requirements of the Sheppard Corridor Secondary Plan and possibly as conditions of approval for major development applications. Additional local studies may be required to identify the extent and routing of any future transient traffic. Further measures internal to the Bayview

Village community and in the lands covered by the Context Plan may be introduced as necessary based on the information gathered in the monitoring and additional studies.

Class Environmental Assessment

The City is completing an environmental assessment of the Leslie-Sheppard area in order to improve road capacity and enhance local access and circulation around the Leslie/Sheppard intersection. The study is recommending a new collector road under the rail track, which would connect Leslie Street with Provost Drive. The new road would have two lanes in either direction, a centre turning lane, as well as sidewalks and bike lanes on either side. The study is also proposing road improvements to Leslie Street to provide 3 continuous lanes in each direction from Sheppard Avenue to south of Highway 401, as well as improvements to Provost Drive to provide 4 through lanes and a centre left turn lane. The Leslie Street improvements and the new east-west road would be undertaken at the same time. The Class Environmental Assessment report is being tabled at Works Committee in January 2002. This work has been an integral component of the Context Plan study. Once the EA is approved, an Official Plan amendment will be brought forward to incorporate Provost Drive and the new east-west road as a collector road in the Official Plan transportation map (Map C.7.1)

Developing a Mixed Use Community

Redevelopment of the lands in the Context Plan area provides the opportunity to create a new community with a mix of uses and a wide range of built form to take advantage of and support the Sheppard subway.

The area between Bessarion Road and the rail track is envisaged to be a new mixed-use community with non-residential uses increasing in intensity approaching the Leslie node.

- Lands west of Provost are intended for both residential and employment uses as well as park/community facility uses. The Sheppard Avenue frontage is viewed as mixed use with opportunities for residential, institutional and commercial uses. A new Canadian Tire store is planned for the south-west corner of Sheppard and Provost. If at any point in the future a decision is made to not locate a new large retail store at this location, this area of the context plan would need to be re-visited with respect to all elements – in particular, the distribution of height and density.
- Lands south of the Sheppard Avenue frontage (west of Provost) to Highway 401 are seen as being primarily for residential uses provided in a variety of building forms.
- A large park/community facility is planned for the south-west portion of the area. This would include a city park, community centre, and possibly include a joint facility with both Public and Catholic school boards for 2 elementary schools. The joint use centre may also include a new public library.

- The Lands east of Provost Drive north of the new east-west road are envisioned as mixed use with commercial and residential uses. Office uses are encouraged to locate in this area to take advantage of the proximity to the Leslie subway station and GO station.
- On the lands east of Provost and south of the new east-west road is an existing IKEA store with opportunities for expansion and a new parking structure.

Participants in the work groups suggested a number of elements to commemorate the area's local heritage: name the area Oriole Village, include imagery associated with oriole, such as yellow, orange and black colours as well as shapes and features be included in architecture, street names, streetscape elements, public art and park features.

The work groups focussed most of their attention on developing a vision and guidelines for the lands west of the railway track. Not as much attention was given to the lands east of the railway track since there is no development application for these lands.

- The Official Plan indicates that lands on the east side of the railway tracks to Leslie Street are to be mainly for employment and service commercial uses, as well as institutional uses and service uses complimentary to North York General Hospital. The City Works yard is currently operational and will remain at its location for some time until an appropriate location and facility can be found. There are currently no plans to relocate the Works yard. Other uses adjacent to the railway tracks include a TTC bus and subway station, a fire hall as well as a relocation of the GO platform.

Once a major development application is submitted for the lands east of the railway track, further review of the Context Plan may be necessary.

Community Services and Facilities (CS&F) Review

Planning staff are currently undertaking a community services and facilities (CS&F) review in the Sheppard Corridor area in accordance with the policies of the Sheppard East Subway Corridor Secondary Plan, which requires the monitoring of appropriate community facilities as development proceeds, including an assessment of the impact of new development on the existing social infrastructure. Given a large increase in the number of development proposals for the area, it is timely to undertake such a review.

This review has been carried out concurrently with the Bessarion-Leslie Context Plan study and focuses on two key geographic areas: Central (Bayview Avenue to Leslie Street) and East (Leslie Street to Don Mills Road). The Bessarion-Leslie area is located within the Central Area and was evaluated within the context of the findings for the larger review area. As part of this work, focus group meetings have been held with community groups, survey feedback obtained from agencies and residents along with the undertaking of a demographic profile and population projections.

The CS&F review provides a framework to ensure that the new development along the Sheppard Corridor will be planned to allow for the timely provision of the required social infrastructure to

support the area's growth. This will assist in ensuring that the communities (existing and emerging) are healthy and liveable. The establishment of new community facilities will focus on the efficient use of lands in the area, consistent with the Sheppard East Subway Corridor Secondary Plan that encourages the development and coordination of joint-use facilities between the School Boards and the City.

The following facility needs have been identified for the Bessarion-Leslie area:

- One joint-use community facility, to include two elementary schools - Public and Catholic JK-8 of approximately 4,600 m² each (50,000 ft²) and a community recreation centre up to 4,600 m² (50,000 ft²);
- Dedicated and accessible community service space for education, recreation and human services, particularly for young children, seniors, youth and new immigrants;
- Fire station of approximately 900-1000 m² (10,000-11,000 ft²);
- Two accessible, high quality, non-profit child care facilities (licensed capacities of 52 and 72 spaces); and
- Public library of approximately 900 m² (10,000 ft²).

The securing of child care facilities will be considered in the processing of the Canadian Tire development application.

One of the objectives in the Sheppard East Subway Corridor Secondary Plan is to ensure the provision of appropriate school facilities in the Bessarion-Leslie area to support new growth. Resident feedback has identified school accommodation as one of their major concerns. Planning staff have held various meetings with both the Toronto District School Board (TDSB) and the Toronto Catholic District School Board (TCDSB) to understand the school capacity situation in this area as a result of new residential development.

The TDSB has identified four schools in the broader Central Area that are located on the north side of Sheppard Avenue, just outside the Bessarion-Leslie Context Plan area. Two are closed and leased facilities (Page PS and Appian PS) while the other two are currently operating slightly under capacity (Elkhorn PS and Bayview MS). The TDSB project that the new developments along the Sheppard Corridor would generate approximately 2,356 pupil places. The TDSB has identified that they will accommodate approximately 1,103 pupil places in the four schools (eg. opening closed facilities, expansion to existing facilities). The TCDSB have no school facilities within the Central Area.

Both School Boards have confirmed a requirement for school facilities to accommodate approximately 1000 students (500 students for each school). This area is also included in the TCDSB's Educational Development Charges (EDC) By-law background study which has identified the need to designate a school site. The TCDSB is seeking the designation of a school site as part of the Context Plan to meet their future requirements.

The School Boards have identified various options to deal with the school capacity problems. The preferred option is the establishment of a joint-use community facility to include two elementary schools together with a community recreation centre and possibly a public library.

This concept has been agreed upon in principle by the School Boards and City staff and meets the objectives of the Sheppard East Subway Corridor Secondary Plan with respect to the securing of integrated community facilities that are required for the area.

The joint-use facility could be secured through the redevelopment of the Canadian Tire lands as part of the plan of subdivision process (Section 51 of the Planning Act). This process provides the School Boards with the ability to secure new schools and ensures the adequacy of school sites as set out in the Secondary Plan. Discussions are on-going with respect to how such a facility may be developed jointly, including a phasing and monitoring approach together with a detailed design process to be undertaken by the partners (eg. School Boards and City). The undertaking of a shared-use facility under City ownership has major benefits including integrating programs and services, conserving land, saving capital and operating costs and maximizing facility use.

Urban Design – Built Form and Massing

Urban design principles and guidelines have been developed with the objective of achieving a high-quality, pedestrian friendly environment that reflects the site's unique topography, location along the subway line and rail track, the existing natural environment around the lands and the existing stable residential area to the north and west.

A variety of residential building types ranging from townhouses, mid-rise apartments and taller residential buildings are encouraged in the new neighbourhood. Other built forms include large retail stores, as well as office and institutional buildings. The Context Plan includes the following height guidelines:

- On the west side of the Context Plan lands abutting the Bessarion stable residential area, grade related buildings up to 4 storeys for townhouses and 3 storeys for a community centre and school facility.
- Sheppard Avenue mainstreet buildings with a maximum of 6 storeys.
- South of the Sheppard frontage lands, transitional heights to mid-rise buildings of up to 12 storeys.
- Mid-rise buildings up to 12 storeys along the east side of the railway track.
- Transition heights increasing to taller buildings adjacent to the park along Highway 401, and taller buildings in the south portion of the community along Highway 401, and along the railway track east of Provost Drive. Through the community consultation process, the expectation is that the taller buildings could have heights up to 28 storeys.
- In the south-west quadrant of Leslie/Sheppard, mid-rise buildings up to 12 storeys with taller buildings closer to Old Leslie Street.

These guideline objectives are based upon working estimates of the draft applications for zoning and subdivision. The translation of building heights with the various height categories shown on the Context Plan height diagram will be determined in detail through the review of development applications and the enactment of zoning by-laws.

Bessarion-Leslie Context Plan

The attached Context Plan has been prepared for the lands south of Sheppard Avenue and west of Leslie Street in the Bessarion and Leslie Nodes. It provides a range of development options to illustrate how the Secondary Plan policies can be achieved in this area. The purpose of the Context Plan is to provide the residents of the adjacent communities and the current landowners with general development guidelines as to how this area could ultimately be redeveloped. It is to be a framework for Council to evaluate development applications in this area. The Context Plan addresses roads, urban design and built form, parkland and open space and community facilities and services for these lands. The Context Plan is to be used in conjunction with the Official Plan and zoning by-law.

The Context Plan is comprised of structure, height and linkages diagrams, and a set of development, urban design and community facility/service guidelines to guide development of the lands.

- The Structure Diagram outlines a framework for the distribution of uses and the physical form of redevelopment. It identifies a potential road system and public realm for the new neighbourhood using new and existing streets, parks and open spaces. It also addresses traffic infiltration/management strategies both within and outside the new neighbourhood.
- The Linkages Diagram shows potential pedestrian connections, bike routes and paths within the Context Plan area and connections to the surrounding communities.
- The Height Diagram provides a generalized concept of the preferred distribution of building heights in the neighbourhood as outlined in the development guidelines.

Together, the maps and guidelines provide a framework for the coordinated development of these lands, and for Council to review individual development applications. The objectives of the Context Plan and the policies of the Secondary Plan are to be implemented through various mechanisms including environmental assessment studies, zoning by-laws, draft plans of subdivision and site plan approvals, as development proceeds. The Context Plan provides a vision and direction on the potential future form of development while maintaining some flexibility since the lands are expected to redevelop over a long time period. The Context Plan may be revisited in the future for the lands east of the railway track when significant development in that area is proposed.

Conclusions:

The Bessarion-Leslie Context Plan provides for a range of development options to illustrate how the Secondary Plan policies can be achieved for this area. It sets out a vision for land use, roads, urban design and built form, parkland and open space and community facilities and services for the lands south of Sheppard Avenue between Leslie Street and Bessarion Road. The Context Plan provides residents in the surrounding area, landowners and the development community with general development guidelines as to how this area could ultimately be redeveloped. It

generally identifies municipal expectations for this area's development, and serves as a framework for Council to evaluate development applications in this area.

Contact:

Nimrod Salamon, Senior Planner

Telephone: 416-395-7134

Fax: 416-395-7155

Rob Dolan
Director, Community Planning, North District

List of Attachments:

Attachment 1: Context Plan Area

Attachment 2: Official Plan

Attachment 3: Zoning

Attachment 4: Site Specific Official Plan policies

Attachment 5: Bessarion-Leslie Context Plan

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Bessarion-Leslie Context Plan

January 2002

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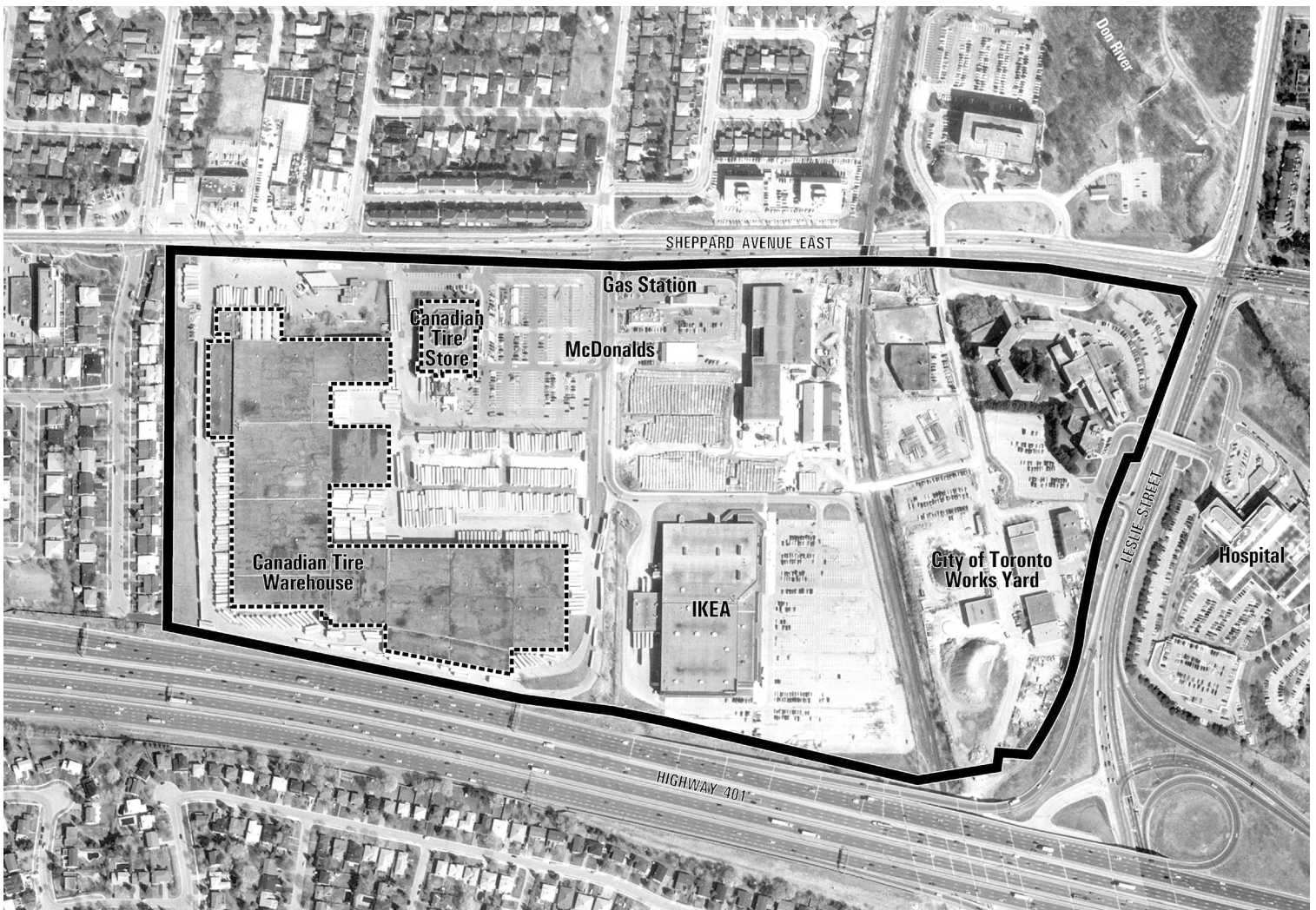
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Introduction

The purpose of the Context Plan is to provide general development guidelines for the Bessarion-Leslie area's potential redevelopment. The Context Plan is comprised of development, urban design and community facility guidelines and diagrams. It is intended to be a framework for Council's evaluation of development applications in this area. The Context Plan provides an additional layer to existing Official Plan policies and zoning regulations to guide the review of individual development applications. It is not intended to replace Official Plan policies or zoning by-laws, but rather be used in conjunction with these documents.



Aerial photo of site and surrounding area showing main land uses and features

Vision for a New Community



View east along Sheppard Avenue East approaching Provost Drive



Looking north along existing rail tracks through the study area



View of North York General Hospital from Oriole Vista Drive

The study area, informally known as the Bessarion-Leslie area, is located on the site of the former Oriole Village. Through the public steering committee process, participating residents suggested that the new community be named Oriole Village to commemorate the area’s local heritage and to suggest connections with this site history and the natural environment. The imagery associated with the “oriole”, a bright orange, and black bird, is encouraged to influence architecture, street and civic names, streetscape elements, public art and park features in the emerging community.

Oriole Village is envisioned to be a new mixed use neighbourhood where people can live, work and play. The new neighbourhood should be designed to have a distinct identity while being integrated with the larger community.

Redevelopment of the lands in the Context Plan area provides the opportunity to create a new community that:

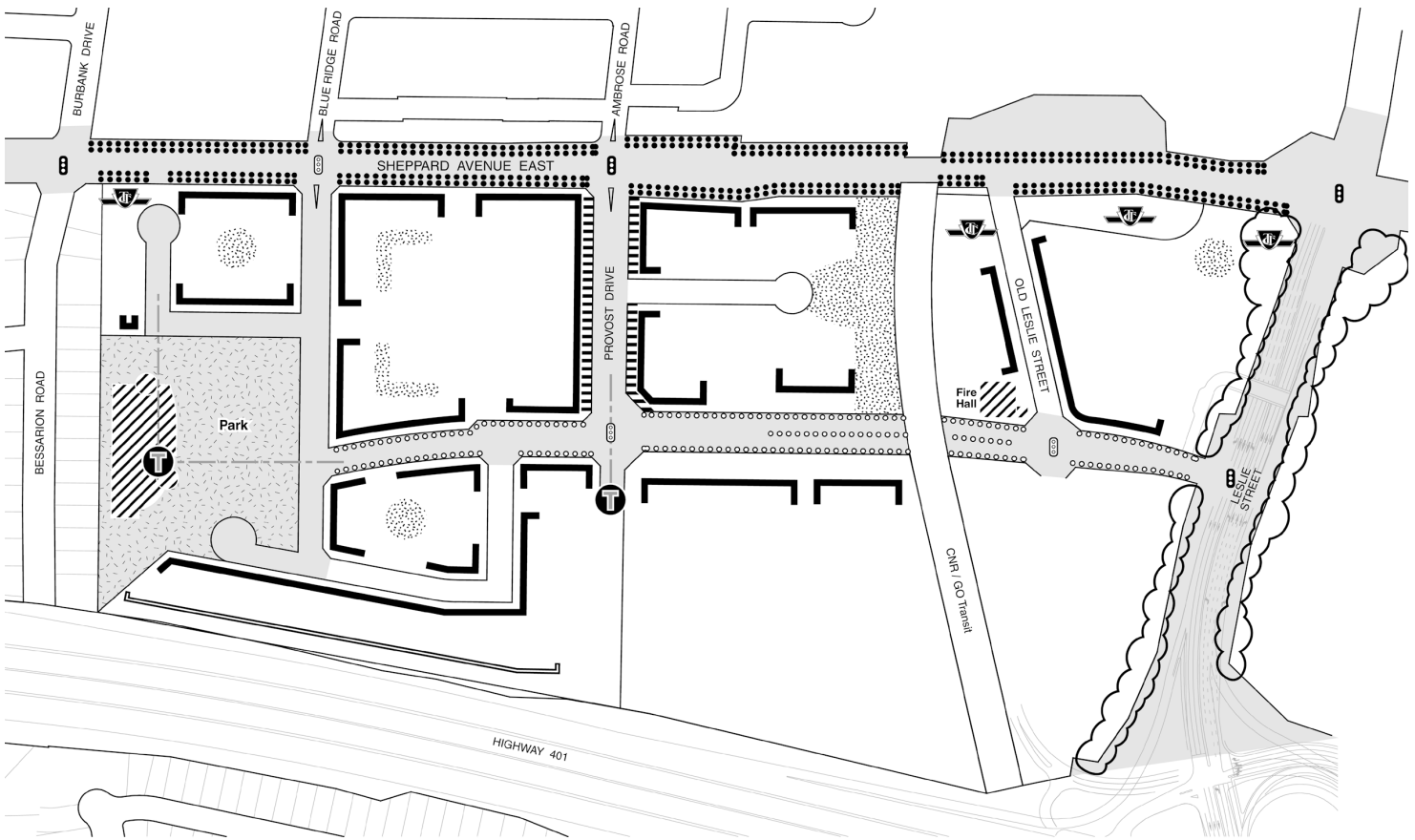
- Includes a mix of uses including residential, retail, office and institutional uses in a range of built forms;
- Has a public road network, and should include walkways and bicycle paths;
- Provides sufficient community, recreational and social services and amenities including parks, open space, schools and community facilities, to serve future residents and employees;
- Incorporates community identity elements that are associated with the local area’s heritage; and
- Increases the number of residents and employees living and working close to the Leslie and Bessarion subway stations in order to facilitate their use.



Oriole bird imagery is encouraged to be used in the design of the new Oriole Village community

Development Framework

The development framework is intended to be used as a guide to the form and layout of new development within the Context Plan lands. A Structure Diagram, a Linkages Diagram and a Height Diagram are part of this framework and graphically illustrate the essential elements and important relationships that should shape the emerging community.



Bessarion-Leslie Context Plan Structure Diagram

TORONTO Urban Development Services
 Prepared by JCS Graphics & Presentation • January 2002

- Possible building edge
- Significant streets and open spaces
- Public open space
- Private open space
- Built form edge for noise attenuation
- Existing TTC station
- Future civic building landmark
- Important view terminus
- Leslie Street scenic drive
- Sheppard Avenue promenade
- Provost Drive shopping street
- Oriole Vista Drive
- Traffic diverter - "No Through Traffic"
- Existing traffic signal
- Proposed traffic signal

Streets

Streets

A network of new and existing streets, walkways, bicycle routes and paths will provide access to and through this area. The network is illustrated in both the Linkages Diagram and the Structure Diagram. The layout of streets and blocks is intended:

- To balance vehicular and pedestrian needs;
- To divide the land into appropriately sized development blocks considerate of its future use;
- To capitalize on views and vistas from the site to the Don River valley and the neighbourhood park; and
- To ensure visibility and access to parks.

The structure of Oriole Village is based on a modified fine-grain grid of connecting streets and walkways to facilitate pedestrian travel throughout the new neighbourhood and to the surrounding existing neighbourhoods. Everyone in Oriole Village will be within a 10-minute walk of a subway station (Leslie or Bessarion).

The layout of new streets must ensure sufficient capacity to accommodate the expected demand for automobile travel without relying on local residential streets north of Sheppard Avenue. Previous transportation assessments and the Leslie Street – Sheppard Avenue Class Environmental Assessment identify the major road improvements that are required to support the new community. The road network consists of the following streets that serve transportation needs and strengthen and provide structure to this new community's emerging character and identity:

- Sheppard Avenue Promenade: the widening of Sheppard Avenue
- Provost Drive Shopping Street: widening to provide two through lanes in each direction plus a centre left turn lane
- Oriole Vista Drive: a new east-west collector road linking Provost Drive to Leslie Street
- Leslie Street: improvements to establish three continuous lanes between Sheppard Ave. and the CN Rail overpass south of Lesmill Road
- Local Streets: a network of public streets

Details of these improvements can be found in the Leslie Street-Sheppard Avenue Class Environmental Assessment Environmental Study Report (December 2001) and the Sheppard Avenue Widening Environmental Study Report (1992).

- Sidewalks in the new neighbourhood are considered the main pedestrian routes in the area. Landscaping and other amenity features may be needed to promote pedestrian use;
- Widened sidewalks and walkway connections may be required at intersections, the railway underpass, important building entrances or routes connecting to transit facilities (GO transit, buses, subway);
- Street trees and other streetscape elements should be used to mediate the scale of development, promote a unified community character and provide environmental benefits. Mid-rise buildings predominate in Oriole Village, so it is important that the scale of streetscape elements be appropriate to the scale of development. To achieve this, the landscaping along streets should build on the emerging streetscape by providing a framework of high-branching deciduous trees evenly spaced along the street frontage, as well as massed shrub planting and coniferous tree planting as another planting layer along the site frontage;
- The site topography changes quite significantly from east to west and from north to south within Oriole Village. Streets should be designed to respond to grade changes in a way that provides pedestrians with a comfortable and attractive streetscape environment.
- Generally, grades should be subtle with gentle transitions;
- Where retaining walls and/or fences are needed, the design, articulation and materials should integrate with the building facades and complement the public streetscape design and features; and
- The massing and height of retaining walls should be designed to be respectful of pedestrian comfort and scale along the street.

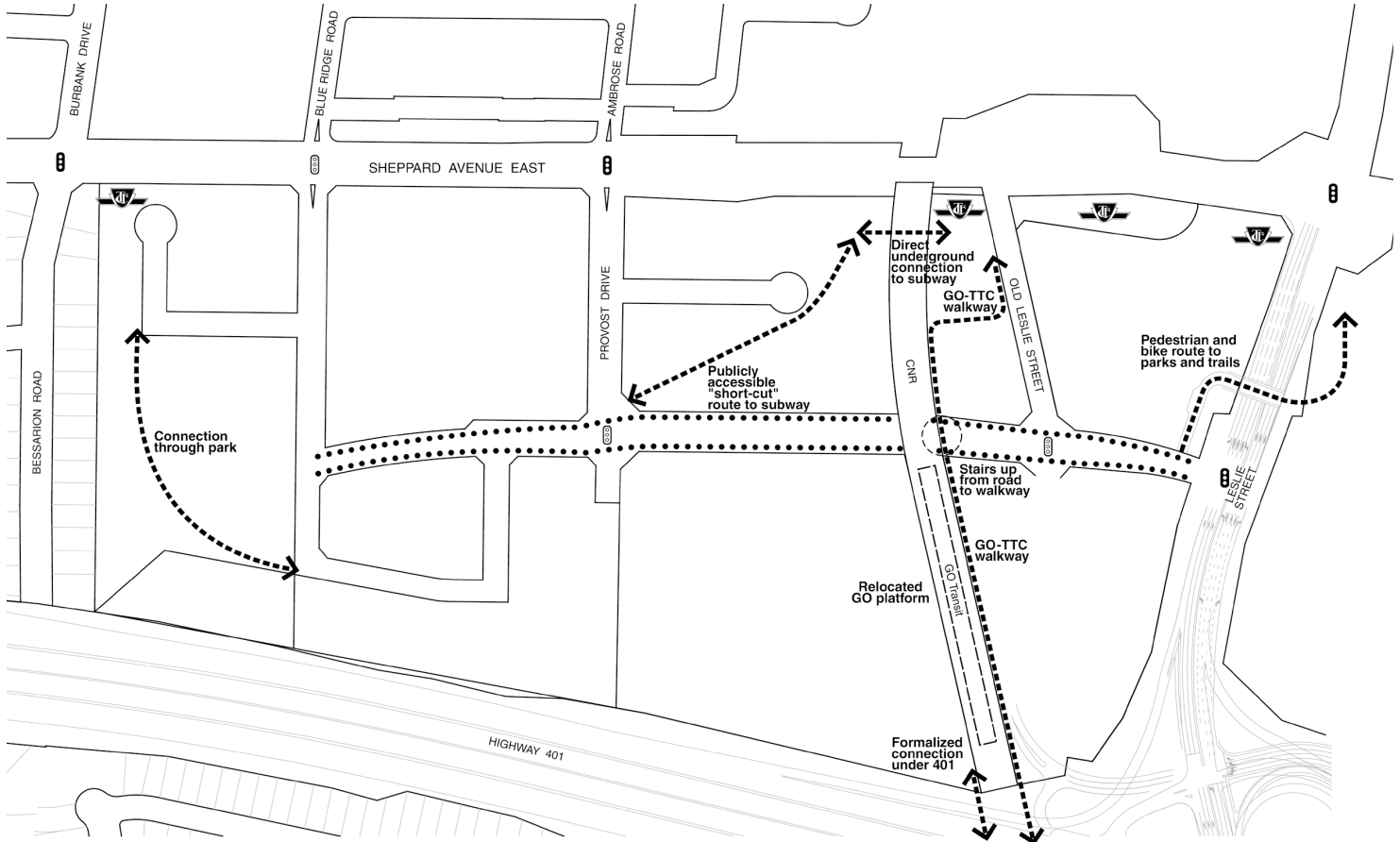


An example of how the streetscape within the new community could be developed



Taller and mid-rise buildings are encouraged to have a 2-3 storey base building along the street frontage with setbacks and gardens

Linkages Diagram



Bessarian-Leslie Context Plan
Linkages Diagram



Toronto Urban Development Services
Prepared by UDS Graphics & Presentations • January, 2002

- Bike lanes
- Pedestrian links
- TTC station
- Traffic diverter - "No Through Traffic"
- Existing traffic signal
- Proposed traffic signal

Sheppard Avenue Promenade

Sheppard Avenue Promenade

With the development of the Sheppard Subway and implementation of the Sheppard Streetscape Master Plan, Sheppard Avenue is being transformed to a social and recreational space for pedestrians – a “people place”. The distinctive urban streetscape includes double row tree plantings, a landscaped median and a generous urban boulevard featuring street furniture, pedestrian lighting and decorative pavement.



Buildings along Sheppard Avenue should be no higher than 6 storeys and have a clearly defined base condition (recent apartment building at Bloor and Royal York)

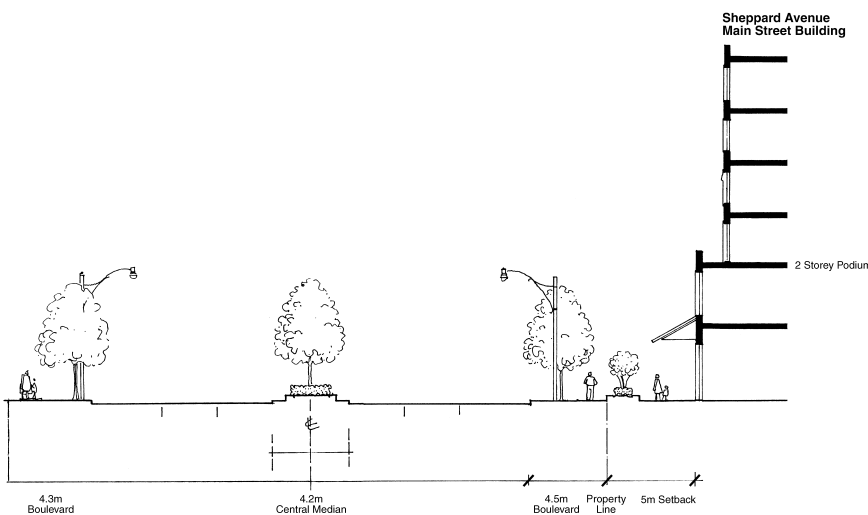


Leslie subway station entrance to TTC along Sheppard Avenue incorporates the elements of the Sheppard Avenue Streetscape Master Plan

Guidelines

- Massing should support the objectives of the Sheppard Avenue Streetscape Master Plan to provide a continuity of built form edge to the street;
- The street wall condition may be interrupted with courtyards, publicly accessible open space and pedestrian connections to the new community, but the building base should provide a continuous built form to define the edges of these pedestrian oriented open spaces;
- Pedestrian amenity features such as benches, weather protection and canopies should be provided along Sheppard Avenue and streets connecting to Sheppard Avenue to encourage pedestrian comfort; and
- Special consideration should be made regarding pedestrian movements across Sheppard Avenue:
- North-south signal timing should respect pedestrian crossing times.

Sheppard Avenue East



Cross section of Sheppard Avenue showing central landscaped median, street trees, and building massing as per OPA 392

Provost Drive Shopping Street

Provost Drive Shopping Street

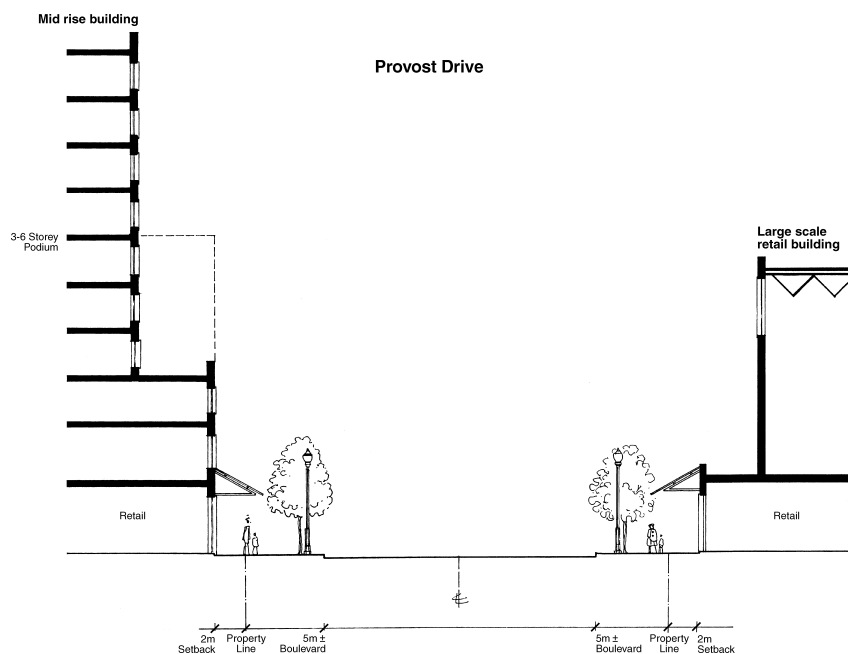
Provost Drive is intended to become a mainstreet shopping precinct. This street should be developed with a continuous streetwall setback from a hard landscaped boulevard with at grade uses that could benefit from interaction with the public sidewalk (retail, commercial uses and entrances to large buildings). The streetscape is anticipated to be attractive and comfortable for pedestrian travel and other outdoor activities.



Existing character of Provost Drive at Sheppard Avenue intersection



Canopies promote pedestrian comfort and amenity



Pedestrian streetscape features and building massing along Provost Drive - continuous pedestrian protection and building setbacks

Guidelines

- A continuous building wall of retail and mixed use buildings should line Provost Drive and include a consistent 2 – 3 storey base condition;
- Buildings along Provost Drive should be designed to promote pedestrian comfort;
- A continuous, covered sidewalk connection should be provided along Provost Drive from Oriole Vista Drive to Sheppard Avenue. This feature (canopies or awnings) should be integrated with the building massing and design and should be consistent along the length of Provost Drive. It may be developed as a thematic element of the streetscape, possibly using an oriole theme; and
- “Post and ring” bicycle parking should be provided in the municipal right-of-way along the retail frontage.

Oriole Vista Drive: East-West Road
Oriole Vista Drive: East-West Road

Oriole Vista Drive is a landscaped road that runs from Leslie Street through the centre of the new community terminating at the large neighbourhood park/community centre located at the site's west boundary. View lines to the park are important to the street character. Street trees, sodded boulevards, sidewalks, landscaped setbacks and landscaped medians, where possible, should be used to promote a pleasant, comfortable pedestrian environment.

Oriole Vista Drive is the new east-west road proposed to link Leslie Street to Provost Drive and is the subject of a Class Environmental Assessment study by the City of Toronto. This portion of the road is intended to function as a major urban collector with particular attention paid to cycle and pedestrian comfort and safety.

The proposed new road is comprised of:

- two through lanes in each direction;
- turning lanes at the intersections and major property access points;
- bicycle lanes in both directions;
- a planted median, where possible; and,
- generous sidewalks and landscaping on both sides.



A landscaped median and street trees on both sides of the street contribute to a high quality streetscape that encourages pedestrian use

West of Provost Drive, it becomes a local residential street terminating at the neighbourhood park/community centre at the site's west boundary. As a local residential street, the road and right-of-way will narrow to provide one lane in each direction, continuation of the bicycle lanes, and turning lanes where required. Similar to the eastern portion of the road, generous sidewalks and landscaping should be provided on both sides of the street.

Guidelines

- A landscaped, accessible and direct route from the intersection of Provost Drive and Oriole Vista Drive should be provided to the Leslie Subway Station and GO Transit Platform;
- A distinctive street tree planting scheme that includes a central landscaped median and landscaped boulevards should be incorporated into the streetscape design;
- The design of the railway underpass should promote pedestrian safety and comfort. The underpass structure should be designed to include stairway(s) to connect the public sidewalk with the GO Transit train platform above the street level; and
- **The Block located north of Oriole Vista Drive and east of Provost Drive:**
The grades for both Sheppard Avenue and Oriole Vista Drive fall off from west to east, leaving the block significantly higher than the surrounding public roads at the east end. Direct vehicular access to underground parking from Oriole Vista Drive should be provided and integrated with the streetscape.

Leslie Street Scenic Drive
Leslie Street Scenic Drive

The Don Valley is a prominent feature of the Leslie Street and Sheppard Avenue intersection. Portions of Leslie Street are visually connected to the natural valley system of the Don River, taking on the character of a scenic drive. To visually reinforce the image of this natural system and scenic drive to Oriole Village, development along Leslie Street should convey the image of a pavilion in a park or landscaped setting.



View of Leslie Street north to Sheppard Avenue from Bridge to North York General Hospital over Leslie Street

Guidelines

- The south west corner of the Leslie-Sheppard intersection should be open and predominately landscaped;
- Landscaped building setbacks should be provided from or maintained on all sides of buildings allowing views through and into the sites to reinforce the openness and “greenness” of the Don Valley corridor;
- To reinforce the connection to the natural valley system, the use of native planting material indigenous to the local environment and reminiscent of the nearby Don River Valley is encouraged. Casual, picturesque, naturalized planting may be promoted on a large scale with groves and clumps of trees rather than linear or other geometrical planting schemes.

Local Streets

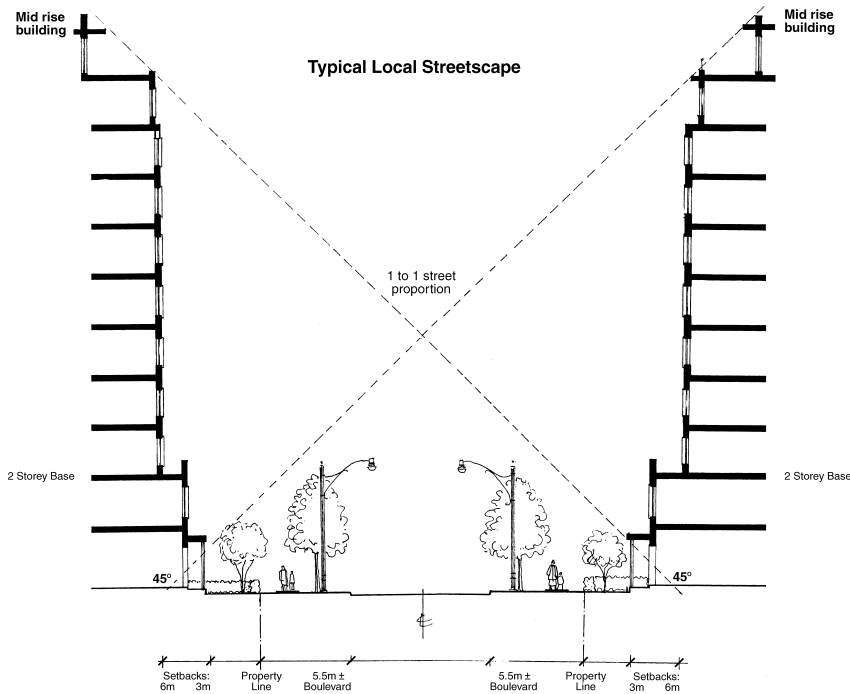
Generally, local streets should be tree-lined with sidewalks on both sides. Local streets will be based on 20m right-of-ways with potential for widenings to accommodate bicycle facilities, turning lanes, or where high levels of vehicular traffic are expected.



Local streets should be tree-lined with setbacks and gardens to promote predominantly “green” character

Guidelines

- Buildings should be set back 3-6 m from a street;
- Building setbacks should be landscaped; and
- The massing, articulation and design of buildings located on corners and view termini should create positive community images and pedestrian relationships. Blank walls and empty unorganized spaces should be avoided in these areas.



Local street cross section showing mid-rise buildings with 1:1 street proportion

A Neighbourhood Park

Parks, Open Space and Community Facilities

A Neighbourhood Park

A park will be developed as the focus for the new community at the west end of the site terminating Oriole Vista Drive. It is to be the largest park in the district and the setting for a civic building that may house community facilities such as a community centre, schools, daycare and/or a library.



The park should include active recreation facilities



Public walkways through the park provide circulation and passive recreation opportunities

Guidelines

- The Community Building should be sited within the park to:
 - be highly visible, in a location surrounded by public streets and walkways;
 - provide a terminal view to Oriole Vista Drive, the east-west connector street; and
 - define and support adjacent parks and open spaces.
- Buildings surrounding the street frontage around the park should have a 2 to 3 storey base condition to create a consistent built form massing and edge;
- Except for the park's west boundary with the Bessarion neighbourhood, the park should have direct frontage on public streets;
- The neighbourhood park should be served by landscaped walkway and / or sidewalk connections to the surrounding community; and
- Direct pedestrian routes to the Bessarion Subway Station should be provided through the park and enhanced with pedestrian-scaled lighting and other safety features. The walkway routing should support the future school location and activities, while not precluding the establishment of sports playing fields or other appropriate site uses.

Private Open Space

Private outdoor open space provides visual amenity and, where appropriate, physically accessible open space that complements and enhances the network of public open spaces within the community.



Private landscaped areas adjacent to the sidewalk can make a positive contribution to the public realm

Guidelines

- Non residential development is encouraged to provide publicly accessible private open space;
- Private open space and amenity areas may take the form of courtyards, plazas, forecourts, walkways, urban gardens, patios or enclaves;
- Private outdoor amenity space is encouraged to be visually, or physically linked to adjacent streets or parks;
- Private open space is encouraged, where appropriate, to provide safe and direct alternative pedestrian routes between the subway stations and the community;

The Railway Interface

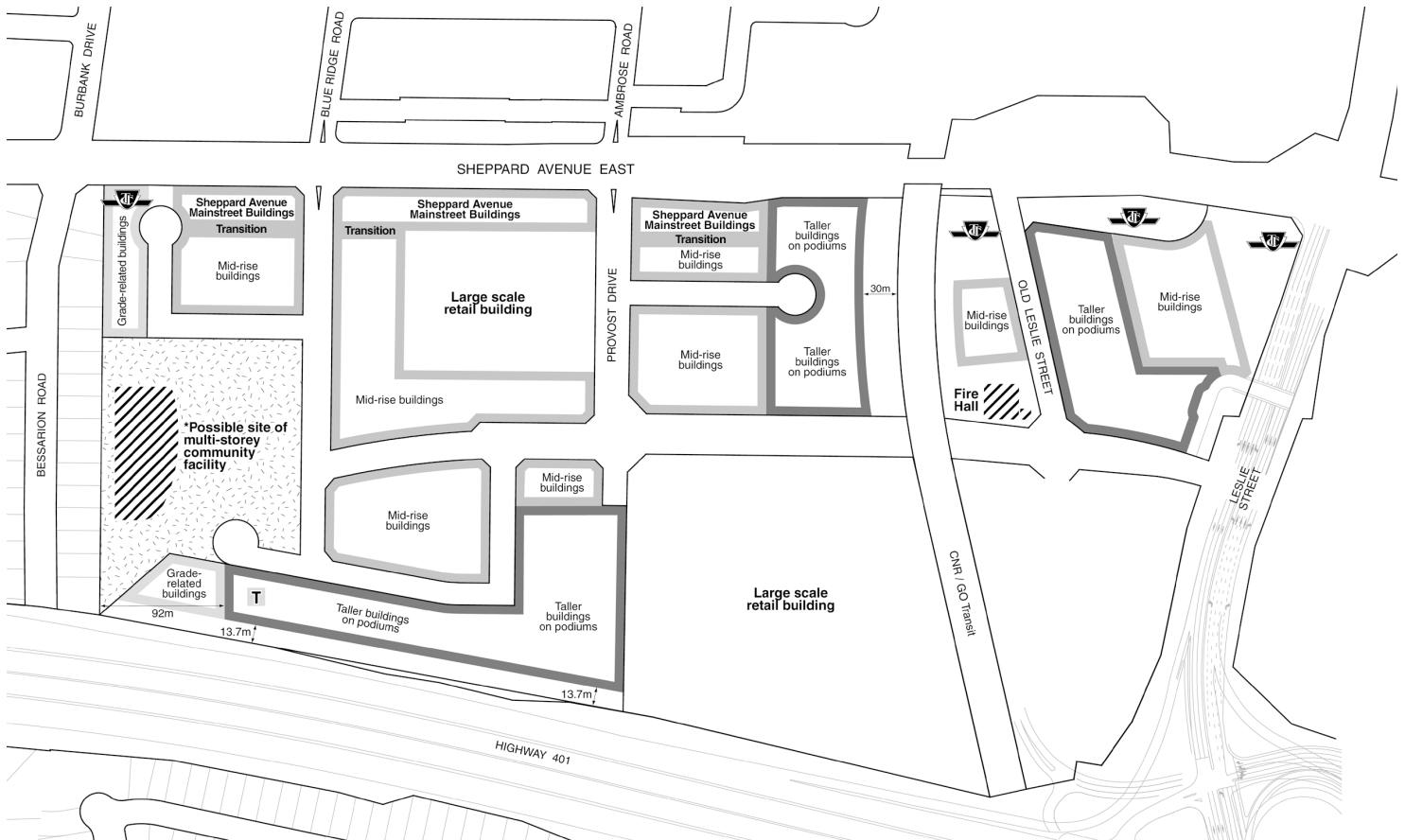
- A significant setback is required between buildings and the railway lands;
- The potential for publicly accessible open space to be located within the setback area should be considered to facilitate pedestrian access to the GO transit and TTC transit facilities; and
- Buildings should be massed and oriented to the railway interface to provide casual surveillance opportunities and to promote comfort, amenity and pedestrian usage of the area.

Building Massing and Height

Building Massing and Height

In accordance with Official Plan policies, the massing of new buildings is intended to:

- frame and support the neighbourhood streets and open spaces at a scale that balances building height and form with street width;
- support the Sheppard Avenue Streetscape initiatives; and
- provide adequate transition to adjacent stable areas.



Bessarion-Leslie Context Plan Height Diagram

TORONTO Urban Development Services
Prepared by UDS Graphics & Presentations • January, 2012

- Grade-related buildings
- Mid-rise buildings
- Taller buildings on podiums
- Future civic buildings
*Location of community facility building within park to be determined
- Existing TTC station building
- Public open space
- Transition in building heights from 15 storeys (west) up to 28 storeys (east)

Grade Related Buildings / Sheppard Avenue Mainstreet Buildings

A variety of building types, ranging from townhouses to towers on podiums, have been organized in the Height Diagram to meet these principles while providing a range of housing and employment opportunities within the new community. The Height Diagram identifies appropriate locations for different types of buildings including the following:

Grade Related Buildings

Grade related buildings are generally 2 to 4 storeys and commonly take the form of townhouses, stacked townhouses and walk-up apartment buildings. This building type has been located along the west boundary of the site near the Bessarion neighbourhood as transition buildings to the low scaled stable area to the west.

The buildings should be developed as follows to respect the existing stable residential neighbourhood.



Buildings can be designed to be integrated with the local topography, hiding service areas and absorbing changes in grade



Townhouses or other low rise building forms are proposed adjacent to the Bessarion neighbourhood

Guidelines

- A generous rear yard setback should be provided; and
- Buildings are encouraged to be built into the existing slope.

Sheppard Avenue Mainstreet Buildings

Buildings along Sheppard Avenue should be massed and sited to create a street wall that frames the street at a pedestrian scale and provides enough space to create a generous landscape promenade in keeping with the Secondary Plan and Council’s streetscape guidelines.



Six storey building with a well-defined base at Bloor and Royal York

Guidelines

- Buildings up to 6 storeys in height should be located along Sheppard Avenue between Bessarion Road and Provost Drive in accordance with the Sheppard East Subway Corridor Secondary Plan.
- A 1 to 2 storey base building is encouraged. The upper parts of the building that rise above the base should step back from the street edge of the base building; and
- A 5m building setback should be provided to accommodate on-site landscaping that is to be integrated with the emerging streetscape.

Mid-Rise Buildings

Mid-Rise Buildings

Mid-rise buildings are buildings whose height normally achieves the one to one street proportion principle in the OP. This is the predominant building type in the area and is generally up to 12 storeys in height. The range of heights is possible depending on the width of streets, the size of landscaped setbacks and terracing. Typically buildings up to 12 storeys are possible.



Mid-rise buildings predominate in the Bessarion Leslie neighbourhood



Mid-rise building at Yonge and Merton with a 5 storey base building

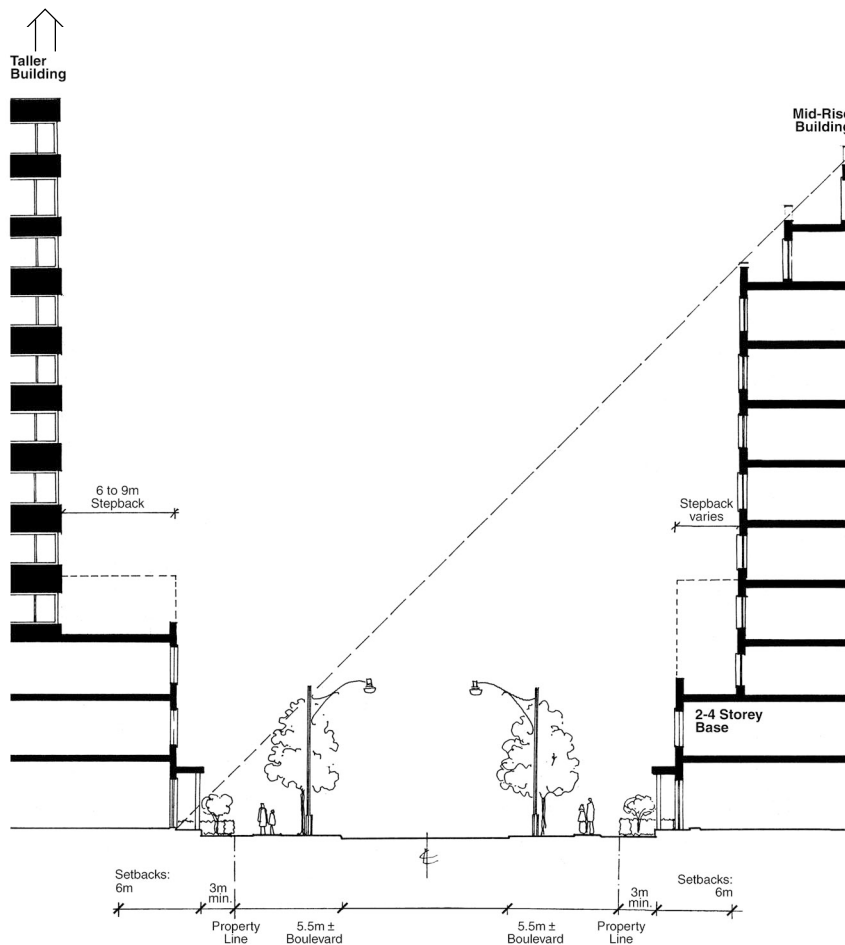
Guidelines

- Mid-rise building should generally be sited to align with streets, parks and accessible open spaces, framing these areas with building mass;
- The buildings should be sited and massed to form useable landscaped courtyards / open space either within the block or opening onto adjacent streets;
- Where appropriate, activity generating and public ground floor uses are encouraged including grade related residential units, lobbies and amenity areas to support the adjacent public realm;
- Generally, a 3 to 6 metre front yard setback from a street should be provided to allow for gardens and landscaped areas between the public sidewalk and the building;
- A well-defined 8 to 12m, or 2 to 4 storey base building should be provided on the lower floors of the street wall/mid-rise buildings;
- Front yard setbacks at grade together with step backs and terracing within the building mass may be used to reach the 12 storey height while remaining under the 1:1 street proportion (see diagrams on pages 11 and 14); and
- Minor building elements such as balcony railings and ornamental cornices may exceed the building height.

Taller Buildings

Taller Buildings

Taller buildings are those that rise above the 1:1 street proportion, being generally over 12 storeys in height. In keeping with the principles of the OP taller buildings may be located in the south portion of Oriole Village, adjacent to Highway 401 and on the east portion of Oriole Village, along the railway line and east of the railroad line that bisects these lands. Through the community consultation process, the expectation is that the taller buildings should not exceed 28 residential storeys.



Taller building with a defined base exceeds a 1:1 street proportion

Guidelines

- Buildings taller than 12 storeys should generally take the form of point towers above a 2 to 3 storey base building. The base building defines the streets and open spaces of the district and the slender proportions of the point tower casts fewer and smaller offending shadows, opens sky views to streets from neighbouring apartment buildings, and is easily absorbed in the skyline;
- In general, the foot print of a point tower should have a maximum dimension of 34m in either direction;
- A point tower should be located and oriented on the block above the building base to minimize shadow and wind conditions in adjacent streets, parks and open spaces;
- Base buildings for towers should be located so they align with the adjacent streets, parks and open spaces. The siting of the base buildings should follow the guidelines for grade related buildings. Setbacks and ground floor uses for the base building should be consistent with other building types and neighbouring structures;
- Generally, towers should be located with a minimum of 6 to 9m step back from the street and park face of its base building;
- The skyline character in relation to other buildings and open space should be considered for all taller buildings. An interesting and attractive skyline should be achieved for the district visible from Hwy. 401 and across the Don Valley; and
- Sun/shade and pedestrian comfort impacts of taller buildings on the public realm of streets and open space, and on existing residential areas should be considered for all tall buildings as per the Secondary Plan.

Large-Scale Retail Stores**Large-Scale Retail Stores**

Large scale retail stores are generally grade-related low-rise structures such as the existing IKEA store along Highway 401 and the proposed Canadian Tire store block at Sheppard Avenue, west of Provost Drive.



Example of large-scale retail store that incorporates glazing and entrances along the street, multiple storeys and building massing and siting that supports the public realm

Guidelines

- Large retail buildings should be located along the street to address and support the adjacent public street network;
- A direct pedestrian connection between the sidewalk and the front door of the store should be provided. If this is not possible, a clear and direct landscaped pedestrian path from the sidewalk to the front door of the store should be provided;
- Multiple entrances along a street promote its use by pedestrians. Grade related retail units and multiple entrances to larger stores are encouraged along the street frontages to complement and enhance the retail character.

Civic Buildings

Civic buildings in Oriole Village are anticipated to be no more than 4 stories in height. Civic buildings are planned for the neighbourhood park (community centre and schools), and a fire hall at the intersection of Oriole Vista Drive with Old Leslie Street and new transit facilities.

Guidelines

- Each building should occupy a prime, highly visible and accessible location;
- Civic buildings should be designed as neighbourhood landmarks;
- Building façades should address the street or park frontage with an appropriate degree of interest and animation, and include glazing and a main door to the street;
- The building footprint should be minimized. Multi storey building design is encouraged;
- Built form, massing, design and architecture should be based on a strong expression or interpretation of site / community heritage and is encouraged to include reference to an oriole theme either in colour, symbol or form; and
- Buildings should be sited, organized and designed, where possible, to promote environmentally friendly concepts.

Parking and Servicing

Sufficient parking and servicing facilities should be provided to serve new development and be oriented in such a way to have a minimal physical and visual impact on the public streets and open spaces. The amount of parking required should meet the essential demand of each development without encouraging discretionary auto use. Minimum and maximum parking requirements may be used to support this goal.

Guidelines

- Parking lots and service areas should be coordinated and organized for each development block, wherever possible, to limit their visual impacts on the public realm. Shared parking and servicing areas are encouraged and should be located such that their visibility to the public realm is minimized, paved area is minimized, and landscaped area is maximized;
- Adequate bicycle parking should be provided;
- The extent of surface parking and service areas should be minimized;
- Underground parking, and in certain cases such as those locations where it would not visually impact the public realm, structure parking should be encouraged;
- Parking structures should be integrated with the design of site buildings and may be located along the Highway 401 frontage, rail corridor or in the middle of the block;
- Access to parking, automobile drop off areas and servicing areas should be designed to minimize car-pedestrian conflict. The number and size of curb cuts should be kept to a minimum to reduce potential conflict points;
- The edge of parking facilities along streets or publicly accessible open spaces should be landscaped; and
- Divide large areas of surface parking or service areas with landscaping.

Traffic Infiltration

In order to discourage traffic from infiltrating adjacent stable residential areas two specific measures, to be implemented during the first phase of development in the node, were determined to be most appropriate – intersection design features and by-law restrictions.



Intersection diverter prohibiting through traffic at Sheppard Avenue and Parkway Forest Drive

Guidelines

- The intersections of Provost/Ambrose and Sheppard Avenue and Blue Ridge Road / Sheppard Avenue will be designed with diverters to physically restrict through movements both northbound and southbound; and
- The physical design should be reinforced with a by-law restricting through traffic 24 hours a day – bicycles and emergency vehicles excepted.

Transportation Demand Management

Existing and future development, especially commercial, are strongly encouraged to implement measures to minimize peak hour vehicular trips. Carpool programs, parking supply controls and pricing, flexible work schedules, and employer subsidized transit fares are among the measures that could be employed to reduce the traffic generated by commercial development in the area and their impact on the surrounding community and the environment. All developments are expected to provide bicycle parking for residents, employees and visitors and provide parking at ratios that encourage transit-riding households to locate in the area. Developers are also encouraged to explore innovative ways to reduce auto use among the users of their buildings such as partnerships with car sharing organizations or rental companies.

Implementation

The Context Plan provides a framework for a range of development options that ensure the Secondary Plan policies can be achieved in this area. The objectives of the Context Plan and the policies of the Secondary Plan are to be implemented through various mechanisms including environmental assessment studies, zoning by-laws, draft plans of subdivision and site plan approvals as development proceeds.

The proposed east-west road connecting Leslie Street with Provost Drive and the improvements to Leslie Street are the subject of a Class Municipal Environmental Assessment process and Environmental Study Report (ESR). These transportation improvements can be implemented following approval of the ESR by the Minister of Environment, and upon financing approval by City Council.

The translation of building heights with the various height categories shown on the Context Plan Height Diagram will be determined in detail through the enactment of zoning by-laws for the property subject to the policies of the Official Plan.

The distribution of density within the Context Plan development area is subject to the policies of the Secondary Plan which set out maximum densities, and additional density achievable through density incentives and transfers. The Secondary Plan provides for density incentives for the provision of specific uses and facilities, and also directs that density transfers are limited to those associated with a public benefit such as the provision of new roads and parkland. The assignment of gross floor area for individual lands will be determined through the enactment of zoning by-laws.

The exact size and configuration of new parkland, as well as potential school locations will be determined through the subdivision and rezoning process. The joint use park/community centre and school facility requires a more detailed review of how such a facility may be developed jointly, including financing, phasing and monitoring approach together with a detailed design process. City Council approval for such a facility will be required. The joint use facility can be secured through a joint use agreement. Approval of the joint facility agreement with one or both District School Boards is subject to the approval of the Minister of Education as required under the "Education Act".