

CP Railway Lands – St. Clair Avenue West and Jane Street – Official Plan and Zoning Applications - Preliminary Report

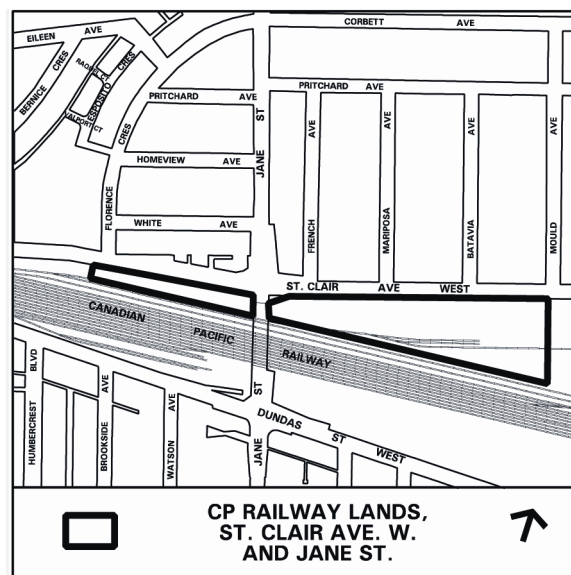
Date:	May 14, 2007
To:	Etobicoke York Community Council
From:	Director, Community Planning, Etobicoke York District
Wards:	Ward No. 11 – York South-Weston
Reference Number:	File No. 06 199631 WET 11 OZ

SUMMARY

1654316 Ontario Limited has submitted applications to amend the Toronto Official Plan and the former City of York Zoning By-law No. 1-83 to permit retail commercial uses and a car dealership with a motor vehicle repair shop on lands located on the south side of St. Clair Avenue West, between Florence Crescent and Mould Avenue.

This report provides preliminary information on the above-noted applications and seeks Community Council's directions on further processing of the applications, and on the community consultation process.

The applications have been circulated to agencies and City divisions for review and comment. A community consultation meeting will be scheduled for the second quarter of 2007, and a Final Report will be prepared once all the required information has been provided and when all the identified issues in this Report have been satisfactorily resolved.



RECOMMENDATIONS

The City Planning Division recommends that:

1. staff be directed to schedule a community consultation meeting together with the Ward Councillor;
2. notice for the community consultation meeting be given to landowners and residents within 120 metres of the site;
3. notice for the public meeting under the Planning Act be given according to the regulations under the Planning Act;
4. the applicant be requested to submit the following reports, studies and plans, which were not included with the submission application: a grading plan; a functional servicing plan/report showing public utilities; a surface water and storm management plan; a landscape plan; an arborist report/tree preservation plan; a functional road and intersection design; a complete set of floor plans and elevations for all of the proposed buildings; and a phasing plan.

Financial Impact

The recommendations in this report have no financial impact.

DECISION HISTORY

The applications were filed on December 22, 2006, and are not subject to the new provisions of the Planning Act and the City of Toronto Act, 2006.

ISSUE BACKGROUND

Proposal

The applicant has submitted an application to permit the development of the site with 6 commercial retail buildings, including restaurants, and a car dealership with a motor vehicle repair shop. The portion of the site located west of Jane Street, is proposed to be used as a parking and storage area for the car dealership's vehicle stock. The Application Data Sheet on Attachment 6 provides all the relevant project information.

Site and Surrounding Area

The proposed development will be located on a portion of the CPR lands on the south side of St. Clair Avenue West at Jane Street. The lands subject of these applications are surplus to the needs of CPR and are divided in two distinct areas by Jane Street. The two areas are connected by the CPR rail bridge over Jane Street. The portion of the site located west of Jane Street is currently vacant and has an approximate area of 5 050 square metres. The site located east of Jane Street is approximately 32 900 square metres in area and has a two-storey building that is proposed to be demolished.

Surrounding land uses include:

- North: along St. Clair Avenue West, a mix of low density commercial and residential uses, with some auto body shops and other automobile service uses
- South/West: Canadian Pacific Railway Lambton Yards
- East: Wal-Mart store and commercial uses

Provincial Policy Statement and Provincial Plans

The Provincial Policy Statement (PPS) provides policy direction on matters of provincial interest related to land use planning and development. The PPS sets the policy foundation for regulating the development and use of land. The key objectives include: building strong communities; wise use and management of resources; and protecting public health and safety. City Council's planning decisions are required to be consistent with the PPS.

The Growth Plan for the Greater Golden Horseshoe provides a framework for managing growth in the Greater Golden Horseshoe including: directions for where and how to grow; the provision of infrastructure to support growth; and protecting natural systems and cultivating a culture of conservation. City Council's planning decisions are required to conform, or not conflict, with the Growth Plan for the Greater Golden Horseshoe.

These applications are consistent with the provisions of the Provincial Policy Statement and conform to the Growth Plan for the Greater Golden Horseshoe.

Official Plan

With the exception of Section 3.1.2.5 (Built Form) and the Special Policy Areas (Floodplain) policies, all the policies of Volume 1 (Chapters 1 to 5 inclusive) of the Official Plan will now be in effect, except as they affect site specific matters identified on Attachments 3 to 7 of the Board Order. This property is not included among those site specific matters.

The lands west of Jane Street where the parking of vehicle stock is proposed, are designated Utility Corridors. Utility Corridors primarily consist of rail and hydro rights-of-way. Linear rail corridors will be protected for future use as public transportation routes, bicycle and pedestrian trails and telecommunications and electrical corridors. Where these corridors are declared surplus they may be used for other public services and amenities. In accordance with policies 4.4.5.a) and 4.4.5.b) of the Official Plan, redevelopment of lands adjacent to Utility Corridors, where appropriate, will protect for access to potential bicycle and pedestrian trails and open space; and ensuring that the abutting Utility Corridor is secured and screened through appropriate measures such as setbacks, fencing and landscaping.

The site located east of Jane Street is designated Employment Areas. Section 4.6.1 of the Plan provides that Employment Areas are places of business and economic activity, including among other uses: offices, manufacturing and small scale stores and services

that serve area businesses and workers. The site is not designated as an Employment District on the Urban Structure Map 2.

In addition, the Official Plan requires development to contribute to the creation of competitive, attractive, highly-functional areas by supporting the economic function of the Employment Areas and the amenity of adjacent areas; providing adequate parking and loading on-site; and providing landscaping to create attractive streetscapes and screening of parking, loading and service areas.

St. Clair Avenue West is listed on the Urban Structure Map 2 as an *Avenue*. The Avenues designation works with the underlying Employment land use designation for this site. Avenues are important corridors along major streets where development is encouraged to create job opportunities and new housing, while improving the pedestrian environment, streetscape, and transit service for the community. The growth and redevelopment of the Avenues should be supported by high quality transit services, combined with urban design and traffic engineering that promotes a safe, attractive and comfortable street for pedestrians and cyclists. Development within the Avenues should have appropriate massing, scale and organization of buildings; restrict parking at grade and driveways in front of the buildings; and provide streetscape improvements while implementing transit-supportive measures that restrict auto-oriented retailing and services.

Zoning

The lands on the west side of Jane Street are zoned Transportation and Utilities Districts (T) which permits Hydro and Railway installations and public parks. On the east side of Jane Street, the site is zoned Strategic Industrial Employment (SI) which permits industrial, retail and/or showroom uses only as an accessory use to industrial or office uses.

Site Plan Control

The proposed development is subject to site plan control. An application for site plan approval has not been submitted.

The submission of a site plan application will be required prior to approval of the Official Plan and Zoning By-law amendments, to be reviewed and processed in conjunction with these two applications.

Tree Preservation

The lands are subject to the City's Tree Preservation By-laws. The applicant has not submitted an Arborist Report/Tree Preservation Plan.

Archaeological Assessment

Portions of the site are within the Interim Screening Areas for Archaeological Potential identified in the Archaeological Master Plan of the City. The applicant may be required to submit an archaeological assessment of the lands following the City's terms of reference for the study.

Reasons for the Application

The proposed parking of the vehicle stock for the car dealership does not conform to the provisions of the City of Toronto Official Plan for “Utility Corridors”. In addition, retail uses are not permitted under the existing “Transportation and Utilities” and “Strategic Industrial Employment” zoning designations of the former City of York By-law No. 1-83.

The requirement for an Official Plan Amendment for the retail component of the application will be assessed based on ongoing discussions with the applicant regarding the nature of the intended uses for this part of the development.

COMMENTS

Issues to be Resolved

Land Use

The appropriateness of the land uses for the site will be reviewed within the context and character of the surrounding employment area, and will be based on discussions with the applicant regarding the nature of the intended uses proposed on the application. The uses which are to be permitted on the site and regulations applicable will be secured in a site specific by-law for the entire site.

In the event that an Official Plan Amendment is required for the retail component of the application, staff will undertake a comprehensive local area planning study to review the proposed retail uses within the context of the existing employment area in which this site is located. The study will assist staff in assessing the impact of the application on the surrounding employment area, and in addressing provincial and city policy objectives.

Urban Design, Built Form

There is a significant grade change along St. Clair at the intersection of Jane Street to accommodate the rail overpass. Creative solutions are required in order to successfully address the existing grade conditions on site. The proposed buildings should provide a better relationship to the street and create opportunities to improve the streetscape by reducing or eliminating the existing retaining wall. Corner site buildings should be located adjacent to both street frontages to give prominence to the corner.

Policies of the Official Plan require all new buildings to be located adjacent to the street with their main facades and entrances fronting the street. The buildings located along St. Clair Avenue should have their main access fronting the street, extending their walkways to the municipal sidewalk to provide direct pedestrian access.

Buildings are to be located parallel to the street rather than perpendicular, in order to provide a greater amount of street wall along the public street, resulting in an active and safe street edge. Consideration should be given to increase the building coverage/frontage

along St. Clair, and to provide direct pedestrian access from St. Clair, to help animate the public street and create a safe and attractive street edge.

Front yard setbacks along St. Clair Avenue West are to be consistent with the main building wall of the adjacent Wal-Mart site located to the east.

Access, Parking, Servicing

The applicant is proposing to have two driveway accesses from St. Clair Avenue West to the site.

City Planning staff have requested the applicant to continue exploring opportunities to share the existing signalized access to the Wal-Mart site on the east. This could help consolidate the access point along St. Clair, minimize the conflict between vehicular and pedestrian traffic, and more effectively use an existing signalized intersection.

In order to enhance the streetscape along St. Clair, and facilitate convenient pedestrian access between buildings, the street and transit services, parking should not be located in front of buildings. Service areas should be enclosed within the proposed building envelopes. The current proposal does not show any truck/garbage loading areas on the site.

Transportation

The Official Plan specifies a future Right-of-Way width of 27 metres for the section of St. Clair Avenue West between Scarlett Road and Runnymede Road, and 30 metres between Runnymede Road and Avenue Road. The 30 metres width was identified to accommodate streetcar operations within the corridor.

As part of the St. Clair Avenue West Transit Environmental Assessment (approved February 21, 2006), Council directed staff to protect for the future westerly extension of streetcar operations from their current terminus at the Gunns Road Loop to Scarlett Road. In order to comply with Council direction, staff have been actively protecting for a 30 metres right-of-way between Runnymede Road and Scarlett Road through the development review process.

The specific conveyance requirements for the subject lands will be identified through the review process.

Landscaping and Street Trees

The applicant is required to submit a landscape plan and arborist report. Existing street trees are to be preserved and protected where possible. Tree planting will be required within landscape islands throughout the parking areas and along the edge of parking areas which are visible from the street.

Environmental Issues

The site has been previously used by CPR for the storage of fuel tanks. The applicant will be requested to submit a Record of Site Condition, and soil remediation will be required

for the portion of the lands that will be conveyed to the City for road widening purposes along St. Clair Avenue. Further discussions are required with the applicant in regard to the timing of the remediation and conveyance of the required land to the City.

Studies/Reports

This application is considered incomplete under the City's STAR (Streamlining the Application Review) process and will not be subject to the City's standard timelines commitments and targets for review under the STAR process. The applicant was made aware of this condition at the time of the application. The applicant has not submitted a number of studies that have been requested to assist staff's review of the application including: a grading plan; a functional servicing plan/report showing public utilities; a surface water and storm management plan; a landscape plan, arborist report/tree preservation plan; a functional road and intersection design; a complete set of floor plans and elevations for all of the proposed buildings; and a phasing plan.

Additional issues may be identified through the review of the application, agency comments and the community consultation process.

Green Development Standard

Staff will be encouraging the applicant to review sustainable development opportunities by utilizing the City's Green Development Standard (GDS) adopted by Council in July 2006.

Comprehensive Development of the Site

In order to avoid piecemeal development and to ensure proper access and servicing of the site, it is recommended that the entire block be designed in a comprehensive manner, and developed in a phased manner that will provide for interim improvements on undeveloped or delayed phases, in the case that development on the entire site does not proceed concurrently.

CONTACT

Claudia La Rota, Assistant Planner

Tel. No. 416-394-6064

Fax No. 416-394-6063

E-mail: clarota@toronto.ca

Gregory Byrne, Senior Planner

Tel. No. 416-394-8238

Fax No. 416-394-6063

E-mail: gbyrne@toronto.ca

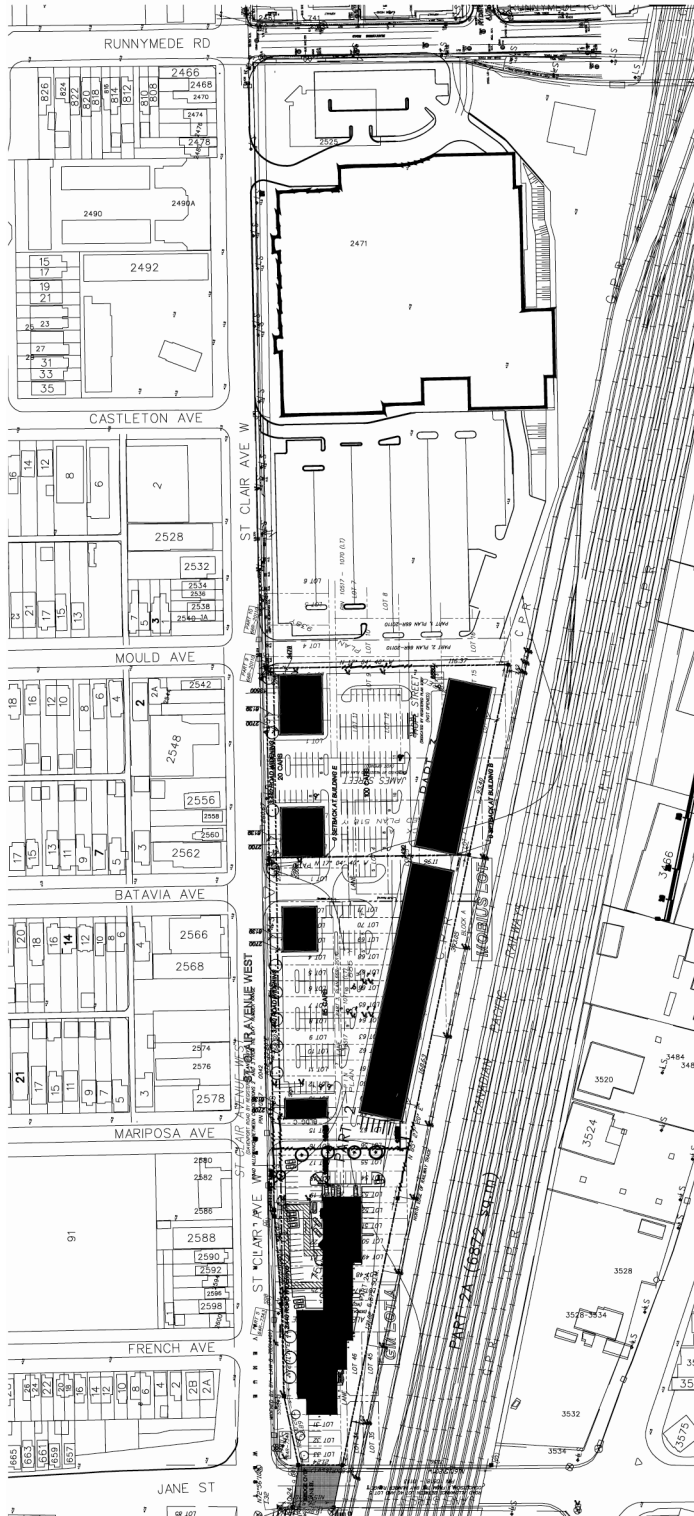
SIGNATURE

Gregg Lintern, MCIP, RPP
Director, Community Planning
Etobicoke York District

ATTACHMENTS

Attachment 1: Site Plan
Attachment 2: Elevations
Attachment 3: Zoning
Attachment 4: Official Plan
Attachment 5: Application Data Sheet

Attachment 1: Site Plan



CP Railway Lands,
St. Clair Ave. W. and Jane St.

File # 06_199631

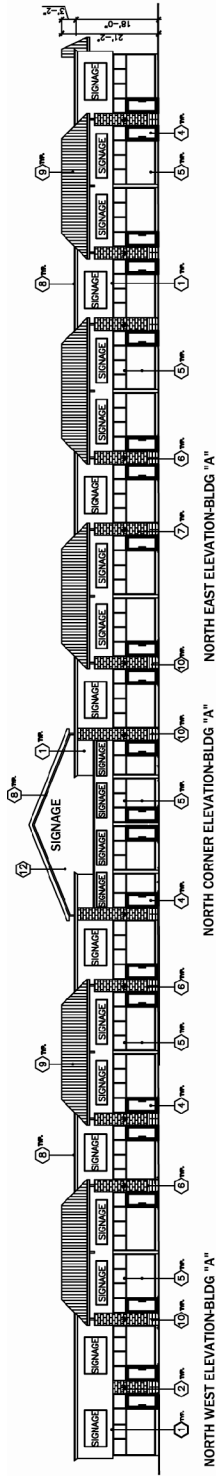
Site Plan

Applicant's Submitted Drawing

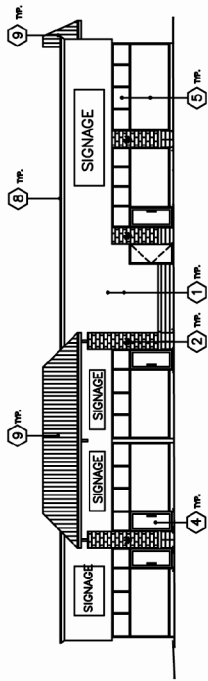
Not to Scale
03/01/07



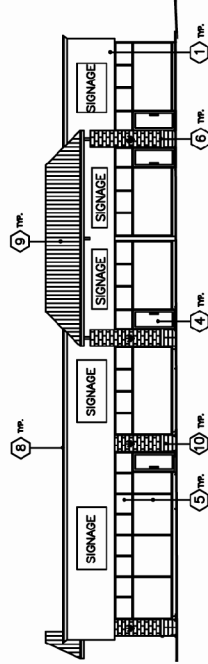
Attachment 2: Elevations



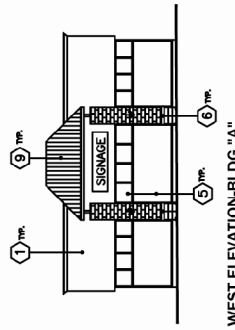
NORTH CORNER ELEVATION-BLDG. "A"



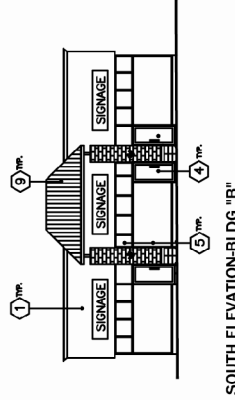
WEST ELEVATION-BLDG. "B"



EAST ELEVATION-BLDG. "B"



WEST ELEVATION-BLDG. "A"



SOUTH ELEVATION-BLDG. "B"

Elevations

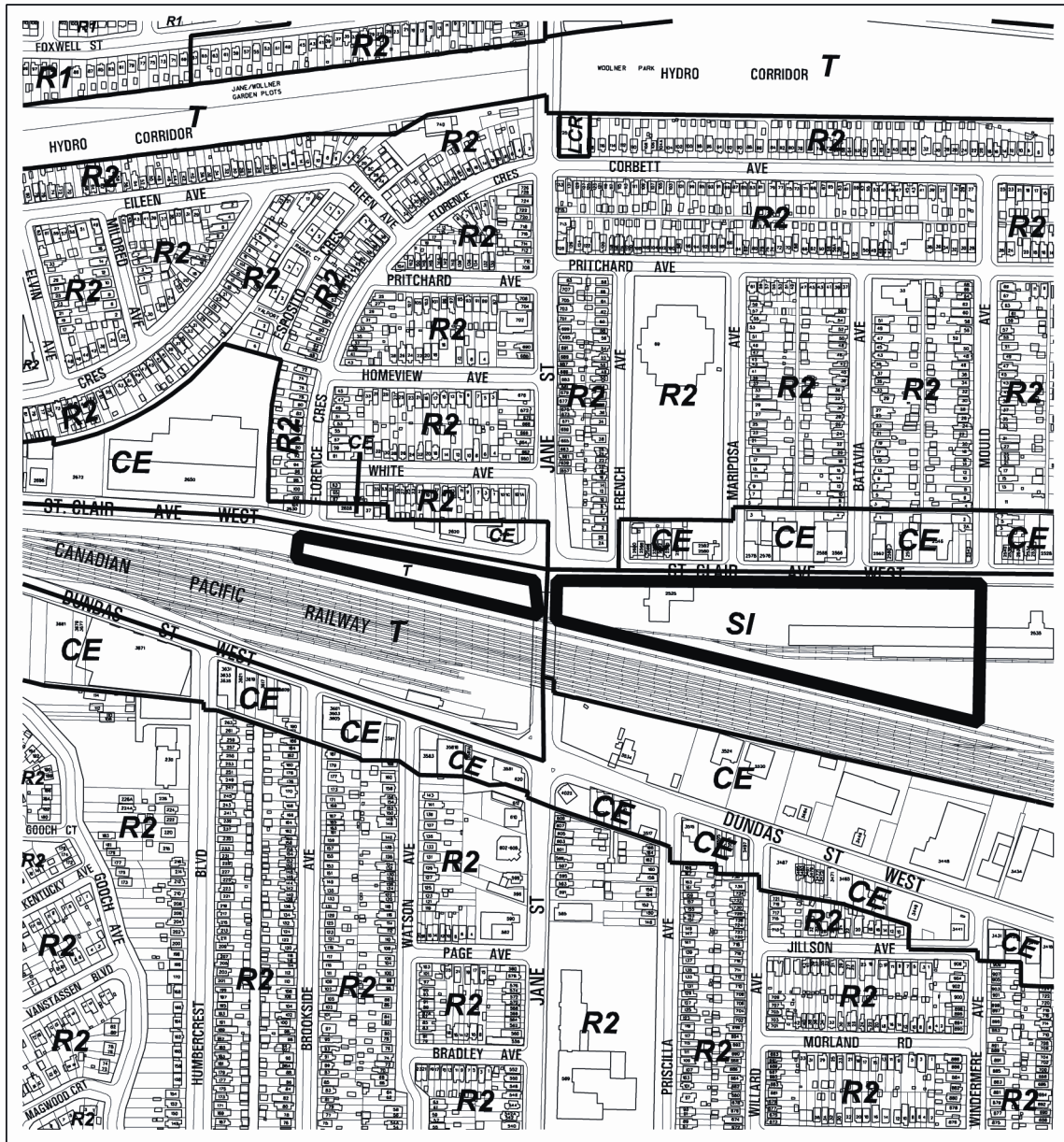
Applicant's Submitted Drawing

Not to Scale
03/01/07

**CP Railway Lands,
St. Clair Ave. W. and Jane St.**

File # 06 199631

Attachment 3: Zoning



**CP Railway Lands,
St. Clair Ave. W. and Jane St.**
File # 06_199631

Former York Zoning

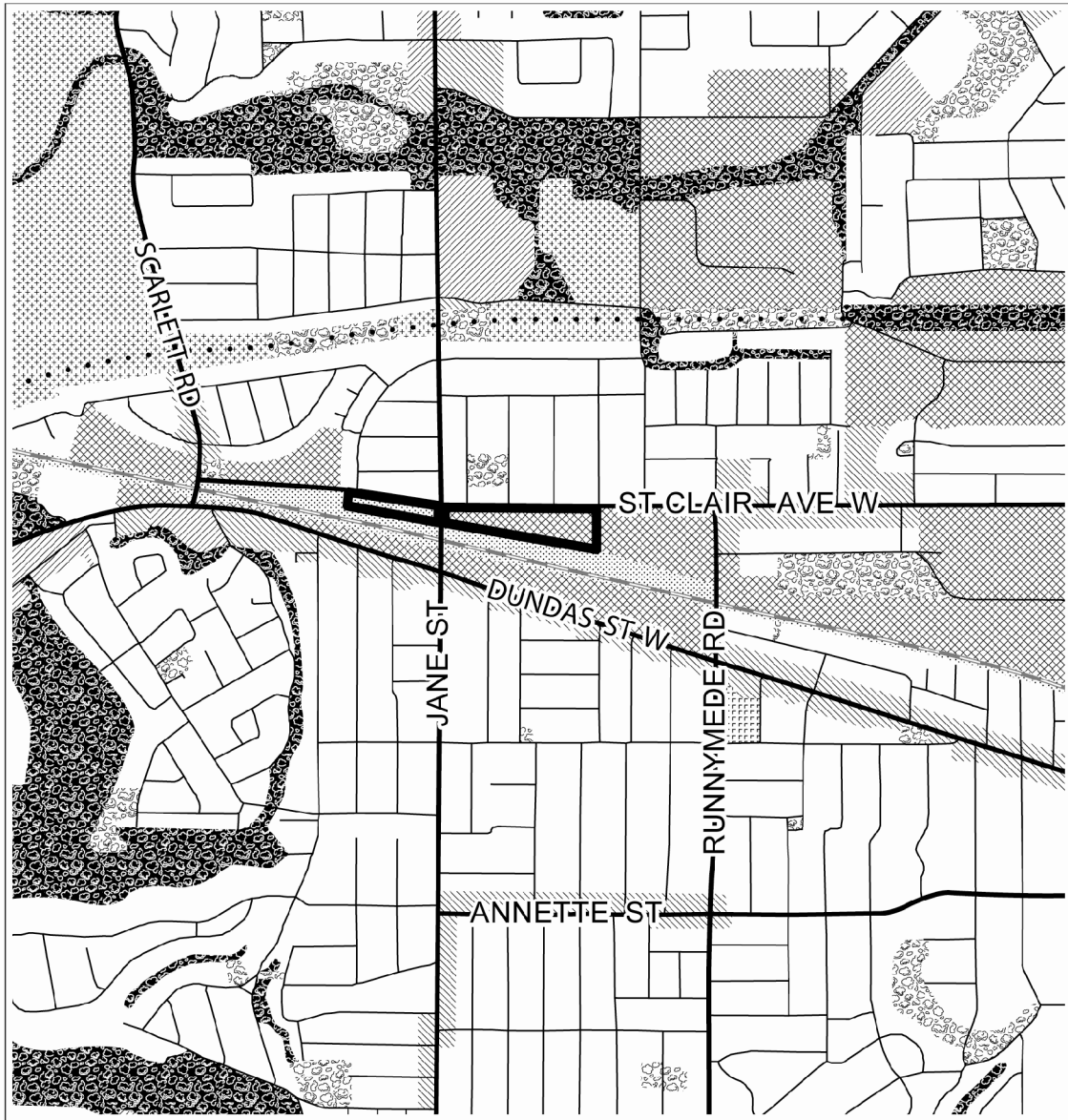
- R1 Residential Zone
- R2 Residential Zone
- CE Commercial Employment Zone

- LCR Local Commercial Residential
- SI Strategic Industrial Employment Zone
- T Transportation & Utilities



Not to Scale
Zoning By-law 1-83 as amended
Extracted 02/20/07 - MH

Attachment 4: Official Plan



TORONTO City Planning
Official Plan

Not to Scale
01/19/07

CP Railway Lands,
St. Clair Ave. W. and Jane St.

File # 06_199631

Attachment 6: Application Data Sheet

APPLICATION DATA SHEET

Application Type	Official Plan Amendment & Rezoning	Application Number:	06 199631 WET 11 OZ
Details	OPA & Rezoning, Standard	Application Date:	December 22, 2006

Municipal Address: 0 ST CLAIR AVE W S/S, TORONTO ON
 Location Description: RP 64R1408 PTS 1 TO 3 **GRID W1108
 Project Description: Official Plan Amendment for Parts 2,3 & 4 66R- for CP Railway lands on St. Clair Ave W. on west side of Jane Street, to redesignate the lands from Utility Corridors to Employment areas. Zoning By-Law Amendment for Parts 2,3,4,5,6 &7, 66R- for Cp Railway lands on St. Clair Ave., W. to rezone the lands from SI & T to CE zone together with amendments to th zone provision.

PLANNING CONTROLS

Official Plan Designation:	Utility Corridors	Site Specific Provision:	N
Zoning:	SI and T	Historical Status:	N
Height Limit (m):		Site Plan Control Area:	

PROJECT INFORMATION

Site Area (sq. m):	32900	Height:	Storeys:	1	
Frontage (m):	0		Metres:	7.5	
Depth (m):	0				
Total Ground Floor Area (sq. m):	8401				Total
Total Residential GFA (sq. m):	0		Parking Spaces:	325	
Total Non-Residential GFA (sq. m):	8401		Loading Docks	0	
Total GFA (sq. m):	8401				
Lot Coverage Ratio (%):	26				
Floor Space Index:	0.25				

DWELLING UNITS

Tenure Type:
 Rooms: 0
 Bachelor: 0
 1 Bedroom: 0
 2 Bedroom: 0
 3 + Bedroom: 0
 Total Units: 0

FLOOR AREA BREAKDOWN (upon project completion)

		Above Grade	Below Grade
Residential GFA (sq. m):	0	0	0
Retail GFA (sq. m):	8401	8401	0
Office GFA (sq. m):	0	0	0
Industrial GFA (sq. m):	0	0	0
Institutional/Other GFA (sq. m):	0	0	0

CONTACT: PLANNER NAME: Gregory Byrne, Senior Planner
TELEPHONE: (416) 394-8238