

6440 - 6452 Kingston Rd and 4035 – 4041 Ellesmere Rd – Rezoning, Draft Plan of Subdivision Applications – Final Report

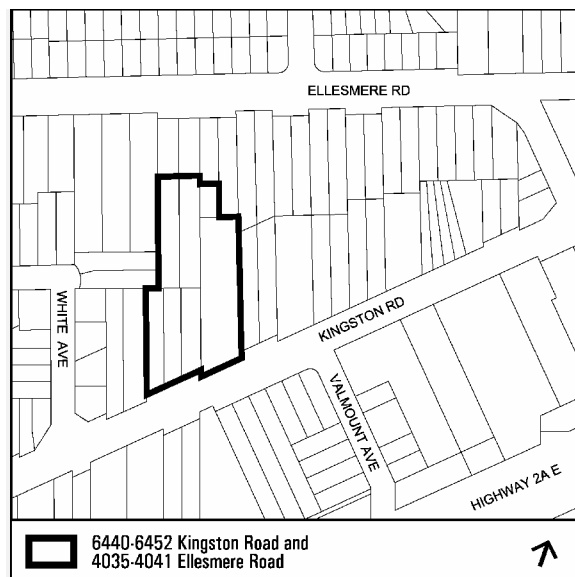
| | |
|--------------------------|---|
| Date: | May 22, 2008 |
| To: | Scarborough Community Council |
| From: | Director, Community Planning, Scarborough District |
| Wards: | Ward 44 – Scarborough East |
| Reference Number: | 07 106099 ESC 44 OZ, 07 106116 ESC 44 SB, 07 106108 ESC 44 SA |

SUMMARY

These applications were made on or after January 1, 2007 and are subject to the new provisions of the Planning Act and the City of Toronto Act, 2006.

The applications for rezoning and draft plan of subdivision approval propose a total of 33 lots, comprised of twenty five (25) single detached residential lots and eight (8) freehold townhouse lots on a new public road and public lane at 6440 - 6452 Kingston Road and the southerly portions of 4035 to 4041 Ellesmere Road.

The proposed land use is consistent with the Neighbourhoods policies of the Official Plan, and Site Specific Policy No. 6 of the Highland Creek Community Secondary Plan. The proposal is in keeping with the Tertiary Plan which forms part of the Highland Creek Secondary Plan and which ensures continuity of roads and a compatible lotting pattern. The development which includes lots for both single detached dwellings and townhouses is compatible with surrounding development.



This report reviews and recommends

approval of the application to amend the zoning by-law which will allow for the appropriate residential development on the subject lands and recommends the site plan control by-law be amended to include the lands on which the townhouses are proposed.

This report also advises that the Chief Planner may approve the draft plan of subdivision subject to the conditions contained in Attachment No. 7.

RECOMMENDATIONS

The City Planning Division recommends that:

1. City Council amend the zoning by-law for the Highland Creek Community substantially in accordance with the draft zoning by-law amendment attached as Attachment No. 5.
2. City Council amend the site plan control by-law substantially in accordance with the draft site plan control by-law amendment attached as Attachment No. 6.
3. City Council authorize the City Solicitor to make such stylistic and technical changes to the draft zoning by-law amendment and draft site plan control by-law amendment as may be required.
4. In accordance with the delegated approval under By-law 229-2000, City Council be advised that the Chief Planner may approve the draft plan of subdivision, as generally illustrated on Attachment No. 7, subject to:
 - a. the conditions of approval as generally listed in Attachment No. 7, which must be fulfilled prior to the release of the plan of subdivision for registration; and,
 - b. any such revisions to the proposed plan of subdivision or any such additional or modified conditions as the Chief Planner may deem to be appropriate to address matters arising from the on-going technical review of this development.

Financial Impact

The recommendations in this report have no financial impact.

DECISION HISTORY

In October 1992 Scarborough Council refused applications for official plan and zoning by-law amendments by Glen Park Cooperative Homes Inc. at 6440-6452 Kingston Road to permit 86 units within a four-storey apartment building and 4 blocks of 3 ½ storey stacked townhouses. The Ontario Municipal Board refused the applicant's appeal on February 3, 1994.

In 1994 Zapfe Holdings Inc. submitted applications to amend the Highland Creek Secondary Plan and the zoning by-law to permit the development of a maximum of 70 stacked townhouse units on 0.77 hectares (1.9 acres) of the subject lands comprised of 6440-6452 Kingston Road. The application was later amended to reduce the proposed development to 40 units of three-storey condominium townhomes. During the time the review of this application was taking place the City of Scarborough initiated the Highland Creek Community Secondary Plan Review. Two priority issues for the study were a review of the land use policies for the area bounded by the Ellesmere Road area, Meadowvale Road, Highways 2A and 401 and a review of the development caps for two of the neighbourhoods within the Secondary Plan. This study concluded with official plan and zoning by-law amendments in June 1997, which received subsequent approval from the Ontario Municipal Board in December 2000. Notwithstanding resulting changes to the official plan which were put into place to enable the redevelopment of these lands, the study did not rezone the lands for the specific development proposed. The applicant did not proceed with the zoning by-law amendment application and the files were subsequently closed in 2001.

The lands which comprise 4035 and 4037 Ellesmere Road were the subject of four separate consent applications approved through the Committee of Adjustment. The first two applications, B038/06SC and B037/06SC, were approved by the Committee in May 2006. These applications severed the southerly 60 metres (197 feet) of each property, to be conveyed to the adjacent properties to the south at 6440, 6444, 6448 and 6452 Kingston Road.

The third and fourth applications, B118/06SC and B119/06SC, were approved by the Committee of Adjustment in February 2007. These applications severed an additional 33.5 metres (110 feet) of each property, to be conveyed to the adjacent properties to the south. As a result of these applications, the depth of the retained lots at 4035 and 4037 Ellesmere Road are about 47 metres (154 feet).

The lands which comprise 4041 Ellesmere Road were severed by consent application B002/08SC. The application was approved in March 2008 and severed the southerly 32 metres (105 ft.) of the property, to be conveyed to the adjacent properties to the south and west at 6452 Kingston Road in order to complete the land assembly for the subject applications.

ISSUE BACKGROUND

Proposal

The draft plan of subdivision application for a residential infill development on this 1.15 hectare (2.8 acre) site was revised following consideration of the preliminary report. The revised proposal is for a total of 33 lots, comprised of 25 single detached residential dwellings and 8 street townhouse dwellings, a 16.5 metre public road, a 6.0 metre public laneway, and block for future development with the lands immediately to the east of the site fronting onto Ellesmere Road.

The original proposal described in the preliminary report was for 44 street townhouse dwellings on the same road configuration, and did not include the rear portion of the lands at 4041 Ellesmere Road. These lands were added to the plan in a revised submission made in March 2008.

The proposed lot frontages of the majority of the lots fronting onto the proposed public road would be between 9.14 and 9.46 metres (30 to 31 feet) with corner properties having wider frontages. The proposed depth of these lots would vary from about 25 metres (82 feet) to about 37 metres (121 feet), with the majority of the proposed lots having depths of about 25 metres (82 feet). The proposed single detached dwellings will have integrated garages, be two storeys in height, and have floor areas of approximately 169 square metres (1,800 square feet). Proposed rear yard building setbacks are 7.5 metres (24.6 feet) for all but the lots on the east side of proposed Street "A", which are 7.0 metres (23 feet).

The majority of the lots fronting onto Kingston Road and backing onto the proposed public laneway would have lot frontages of about 5 metres (16.4 feet), and lot depths of about 22.4 metres (73 feet). These units also have integrated garages which are accessed from the proposed public laneway at the rear of the townhouses. These units are proposed to be three storeys in height. The proposed floor area of interior units are approximately 140 square metres (1,500 square feet). The proposed floor area of end units is approximately 176.5 square metres (1,900 square feet).

Attachment No. 4, the application data sheet, contains relevant project information. Attachment No. 1 illustrates the proposed draft plan of subdivision and Attachment No. 2 illustrates the proposed elevations for the townhouses.

Site and Surrounding Area

The subject lands are comprised of four existing lots along Kingston Road and the backs of three lots having frontage on Ellesmere Road. These lands comprise a total area of 1.15 hectares (2.7 acres). The dwellings that were located along Kingston Road have been demolished. The site has 87.5 metres (287 feet) of frontage on Kingston Road and is flat in topography and supports many trees.

The abutting properties to the east of the site along Kingston Road contain single storey commercial buildings containing automotive uses, while the lands to the east of the site along Ellesmere Road contain one and two storey detached dwellings. The lands to the south of the site along Kingston Road contain detached dwellings on large lots. The lands to the west of the site both along Kingston Road and White Avenue contain one and two-storey detached dwellings, as do the lands to the north along Ellesmere Road. Further west of the site on the south side of Kingston Road east of Meadowvale Road a new infill development of 68 townhouse units and 6 detached residential dwellings has been constructed.

Provincial Policy Statement and Provincial Plans

The Provincial Policy Statement (PPS) provides policy direction on matters of provincial interest related to land use planning and development. The PPS sets the policy foundation for regulating the development and use of land. The key objectives include: building strong communities; wise use and management of resources; and, protecting public health and safety. City Council's planning decisions are required to be consistent with the PPS.

The Growth Plan for the Greater Golden Horseshoe provides a framework for managing growth in the Greater Golden Horseshoe including: directions for where and how to grow; the provision of infrastructure to support growth; and protecting natural systems and cultivating a culture of conservation. City Council's planning decisions are required by the Planning Act, to conform, or not conflict, with the Growth Plan for the Greater Golden Horseshoe.

The proposal is consistent with the PPS. The proposal conforms to and does not conflict with the Growth Plan for the Greater Golden Horseshoe.

Official Plan

The subject lands are designated Neighbourhoods within the Toronto Official Plan on Map 23 – Land Use Plan. These areas are physically stable areas providing for a variety of lower scale residential uses. Policies and development criteria aim to ensure that physical changes to established neighbourhoods be sensitive, gradual and generally “fit” the existing physical character.

Infill development policies are outlined in Section 4.1.5 – Neighbourhoods. These policies state that development in established neighbourhoods will respect and reinforce the existing physical character of the neighbourhood, including in particular: size and configuration of lots, setbacks of buildings from the street or streets; prevailing patterns of rear and side yard setbacks and landscaped open space; and continuation of special landscape or built form features that contribute to the unique physical character of a neighbourhood. No changes will be made through rezoning, minor variance, consent or other public action that are out of keeping with the physical character of the neighbourhood.

The property is also part of the Highland Creek Secondary Plan. The lands are subject to Policy No. 6 of the Secondary Plan which permits detached, semi-detached and street townhouse dwellings, and exempts the lands from the minimum lot requirement of 450 square metres.

Map 2-2 of the Secondary Plan is the Tertiary Plan which illustrates potential residential infill development to ensure continuity of roads and to provide for a compatible lotting pattern. The tertiary plan illustrates a new road running north from Kingston Road upon the subject site, terminating in a cul-de-sac on the rear portion of the adjacent lots on Ellesmere Road to the west of the subject site.

Zoning

The subject lands are zoned Single-Family Residential (S) Zone, and permit one single family detached dwelling per parcel of land with a minimum of 15 metres frontage on a public street, and a minimum area of 696 square metres. Permitted uses include single-family dwellings, correctional group homes and group homes. Ancillary uses permitted include domestic or household arts and private home day care.

Site Plan Control

The subject lands are not currently subject to site plan control, a draft amendment to the site plan control by-law is attached as Attachment No. 6. It is recommended that the lands fronting Kingston Road, upon which townhouse dwellings are proposed, be placed under site plan control. A related site plan control application, File No. 07 106108 ESC 44 SA has been submitted for the townhouse dwellings, and is currently being reviewed by staff.

Reasons for Application

The purpose of the rezoning application is to rezone the portion of the lands upon which townhouses are proposed from Single Family Residential (S) Zone to Street Townhouse Residential (ST) Zone with appropriate development standards, and to amend the zoning by-law for the remainder of the site to establish specific development standards for the development of 25 single detached residential dwelling units. The plan of subdivision application is necessary to create the proposed new lots, future development blocks, public road and public laneway.

Community Consultation

A community consultation meeting was held on November 21, 2007, attended by the Ward Councillor, the owner and their representatives, Urban Forestry staff and Community Planning staff, and about seventeen (17) members of the public.

Prior to the community meeting, and in response to issues raised in the Preliminary Report, the applicant revised their proposal from 44 street townhouse lots to 24 detached residential lots and 8 street townhouse lots. The proposed public road and laneway remained the same. Concerns were raised in regard to the proposed 4.87 metre frontage of the townhouse lots, and the size of the lots proposed when compared to the neighbourhood which is characterized by large lots. Several questions related to whether the turning circle illustrated on the lands to the west was necessary to support this development. Questions also related to the anticipated cost of the proposed homes. The need for additional parkland in this area was also raised. Correspondence received following the meeting included a comment that the proposal for townhouses will increase the population density and destroy the integrity of the neighbourhood.

Agency Circulation

The application was circulated to all appropriate agencies and City divisions. Responses received have been used to assist in evaluating the application and to formulate appropriate by-law standards and conditions of draft plan approval.

COMMENTS

Land Use

The proposed use for detached residential homes and townhomes is consistent with the City's Official Plan policy for this site. The area is subject to the Highland Creek Community Secondary Plan, and to Site and Area Specific Policy 6, which originated from a study undertaken by former City of Scarborough Planning for a portion of the Highland Creek Community, bounded by Ellesmere Road to the north, Meadowvale Road to the west, Highway 2A to the south and Ellesmere Road/Highway 2A westbound ramp in the east. This study concluded with official plan and zoning by-law amendments in June 1997, which received subsequent approval from the Ontario Municipal Board in December 2000. Site and Area Specific Policy 6 permits detached, semi-detached and street townhouse dwellings and exempts lands within the policy area from the minimum lot area requirement of 450 square metres that is otherwise applicable to lands designated Neighbourhoods within the Highland Creek Community Secondary Plan.

The area is characterized by residential lots which have frontages of generally 15.24 metres (50 feet) or greater, and lot areas of approximately 950 square metres (10,226 square feet) or greater, although smaller lots also exist in the surrounding area. Although the lots proposed by this plan are smaller in size than those of the surrounding neighbourhood, the proposed development contemplates lot sizes and dimensions that will support single detached residential dwellings and townhouses that will be sited in accordance with development standards that are commonly implemented for similar types of development. Recommended development standards for the single detached dwellings include a 6 metre building setback for garages, 3 metre building setback for the front wall of the dwelling. As well as rear yard minimum building setbacks of 7.0 metre and 7.5 metre, side yard setbacks of 1.2 metres and 0.6 metres, and 50% maximum lot coverage permissions. Recommended development standards for the townhouses include a garage setback of 6 metres, 3 metres building setback from Kingston Road, 1.2 metre side yards for the end units, and 65% maximum lot coverage permissions.

The proposed development addresses the official plan objective that development be on public streets. The public street is proposed to have a width of 16.5 metres, and the public lane a width of 6.0 metres, both of which are in keeping with the City's Development Infrastructure Policy Standards (DIPS).

The proposed development is compatible with surrounding development and is appropriate for this area.

Height

The applicant has not proposed any changes to the existing height permissions of the zoning by-law for the proposed single detached dwellings in the interior of the site. The zoning by-law currently permits a maximum height of 10 metres within a 2-storey structure, excluding the basement.

For the townhouses along Kingston Road the applicant has requested a maximum height permission of 12 metres (39 feet) with a 3-storey structure, excluding basements. The request is consistent with other townhouse developments, including the recently constructed Stone Manor project, south-west of the subject lands, and is considered appropriate for the Kingston Road frontage.

Streetscape

The eight proposed townhouses will front onto Kingston Road and will have vehicular access via the new public road and rear public laneway. As a result, the frontage along Kingston Road will not include any private driveways or garages, and will instead present front facades facing Kingston Road. This is desirable from a traffic safety standpoint as it reduces conflicts with pedestrians along the sidewalk on Kingston Road and also provides increased opportunity for landscaping and tree planting. The proposed lot frontages of 5.0 metres comply with the City's Infill Townhouse Guidelines which permit unit widths of 4.2 metres where parking for the units is accessed from the back. The height of the townhouses is 3-storeys (12 metres), and this along with a minimum 3 metre building setback from the Kingston Road street line and landscaping will provide an appropriate streetscape for the Kingston Road frontage. Refer to Attachment No. 2 for elevation illustrations. Further details related to the townhouse units will be addressed through the review of the related site plan control application.

Public Roads/Laneways

The proposed draft plan of subdivision includes a new 16.5 m wide public street (Street "A") and a 6.0 m wide public laneway (Street "B"). Both the street and the lane are consistent with the Official Plan policy which states that all new streets should be public. The street and laneway are designed to be consistent with DIPS guidelines.

Street "A" is proposed to terminate in a temporary turning circle near the north-west edge of this plan until such time as its future westward extension is proposed and constructed. In the interim, the temporary turning circle will preclude the development of proposed Lot 9. A condition addressing this has been included in the recommended conditions of draft plan of subdivision approval, Attachment No. 7. A 0.3 metre (1 foot) reserve is required along the temporary turning circle of Street 'A' adjacent to proposed Lot 10. Also included is a condition which requires the appropriate reinstatement of driveways and curbs for Lots 10 and 11 as may be required at the time the road is extended. The proposed right-of-way will accommodate a sidewalk on one side of the street. The City will provide garbage collection and snow clearing along this public street. As the right-of-way width is below 18.5 metres, the City will not be providing sidewalk snow clearing and driveway windrow clearing.

The public laneway (Street "B") is proposed to terminate in a permanent turning circle near the south-west edge of the plan. This public laneway will provide vehicular access to the proposed townhouses fronting onto Kingston Road. City policy regarding winter maintenance is that the laneway will not receive City snow clearing, but will be salted by the City.

Tertiary Plan

Through preliminary discussions with the applicant, Planning staff requested that a concept plan be prepared which shows the potential for adjacent infill development consistent with the Tertiary Plan. The draft plan of subdivision, Attachment No. 1, illustrates the possible continued westward extension of the proposed public road, terminating in a cul-de-sac on the abutting lands to the west. No development applications have been filed on these lands. Because the temporary turning circle encroaches on Lot 9, this lot should be identified as a block for future development, to be made available for development only after the temporary turning circle is removed and Street “A” is extended westward.

Traffic Impact and Parking

A traffic impact assessment prepared by Marshall Macklin Monaghan was submitted in support of this application. According to their findings, the traffic impacts of the proposed development will have a negligible impact on Kingston Road traffic operations, and the resulting traffic can readily be accommodated on Kingston Road. Further, there is no need for any turning lanes on Kingston Road at Street “A”, as the existing two-lane pavement cross-section will be sufficient to accommodate turning volumes without having any detrimental impact on through traffic operations on Kingston Road. City Transportation Planning and Transportation Services staff agree with their findings.

In regard to parking, an enclosed garage is proposed for each dwelling unit within the plan. The applicant proposes garages with a minimum dimension of 3 metres by 6 metres, and this requirement has been included in the draft zoning by-law, Attachment No. 5. The draft zoning by-law also requires that the garage be setback a minimum of 6 metres from the street line. This will enable additional parking for a second vehicle in front of the garage.

Tree Preservation and Plantings

An arborist report, and related plans have been submitted in support of this application. There are a total of approximately 83 private trees on the site that are protected under the Private Tree By-law, of which a total of approximately 19 trees are intended to be preserved. Urban Forestry staff has reviewed the plans and have indicated that a permit to destroy trees is required for the remaining 64 trees. These trees vary in size and condition and cannot be preserved due to their location within the future public road and proposed lots. A minimum of 3 replacement trees are required for every tree included in an application to destroy or injure trees.

There are also 21 street trees that are protected under the Street Tree By-law. Ten of these trees are proposed to be preserved. Eleven of these trees, which also vary in size, are located at the proposed entrance of Street ‘A’ adjacent to Kingston Road, and are proposed for removal.

Proposed plantings include 26 new street trees along proposed Street 'A', along with 36 trees on the proposed lots. The applicant proposes to provide cash-in-lieu value for 156 replacement trees to the City.

Urban Forestry staff have found the tree planting proposed on the Street Tree and Rear Yard Planting Plan to be satisfactory, however further revisions are required to the Utility Coordination drawings to ensure that all trees can be planted as proposed. In order to ensure that all matters related to tree protection and planting are addressed, pertinent conditions have been included as conditions of draft plan approval.

Servicing

The applicant has submitted a municipal servicing report in support of the proposal. The report and associated servicing drawings have been reviewed by relevant divisions, including the Technical Services Division.

The appropriate municipal servicing for the proposed development can be constructed and can be fully servicing in accordance with City of Toronto standards. A revised Municipal Servicing Report, in addition to a Stage II Stormwater Management Report are required as conditions of draft approval. Refer to Attachment No. 7 for recommended conditions of draft plan approval.

School Capacity

The Toronto District School Board (TDSB) advises that there is insufficient local elementary and junior school capacity to accommodate students anticipated from this proposed development. As part of the subdivision process the TDSB has requested standard conditions of approval be included, which require the applicant to post signs on site and include warning clauses on title advising purchasers of the status of local school accommodation. The Toronto Catholic District School Board was also circulated advising of the proposed development, however, they did not provide comments.

Parkland

The Official Plan contains policies to ensure that Toronto's system of parks and open spaces are maintained, enhanced and expanded. Map 8B of the Toronto Official Plan shows local parkland provisions across the City. The subject lands are located in an area with 1.57 to 2.99 hectares of local parkland per 1,000 people.

The subject lands are located within a Parkland Acquisition Priority Area, and are therefore subject to By-law No. 1420-2007, the Alternate Rate Parkland Dedication By-law. Parks staff find that based on the greater value of either 0.4 hectares for every 300 units proposed, or 5% of the net site area of the land to be developed, the resulting size would not be of a useable size as parkland. As a result, a cash-in-lieu of parkland dedication payment is required prior to building permit issuance. A condition to this effect has been included in the recommended conditions of draft plan approval.

Revisions Required Prior to Draft Plan Approval

The proposed draft plan of subdivision approval conditions address the technical requirements of the development including among other matters, the construction of streets, services, street tree planting and stormwater management facilities. Staff recommend that Council support the draft plan of subdivision, and recommend to the Chief Planner that the draft plan be approved.

Minor revisions will be required prior to draft plan of subdivision approval, reflective of comments received in response to the circulation of the proposed plan. The Official Plan establishes the right-of-way of Kingston Road at this location to be 27 metres, and as such the road widening shown as Block 35 in Attachment No. 1 is not required. Street “A” should be extended to connect through this block to Kingston Road, and the remainder of Block 35 should be combined with the adjoining proposed lots. A 0.3 metre (1 foot) reserve will be required along the entire Kingston Road frontage. This will preclude vehicular access to these lots directed from Kingston Road.

Conditions of draft approval for the subdivision are set out in Attachment No. 7.

Toronto Green Development Standard

The City’s Green Development Standard encourages the use of “green” development techniques with an aim to promote sustainable development. The proposed development includes a variety of techniques identified in the guideline including the use of local materials, urban heat island reduction with the use of light coloured roofing materials, various building features for the dwelling units and building orientation to take advantage of passive solar heating and natural light.

Development Charges

It is estimated that the development charges for this project will be \$347,602. This is an estimate. The actual charge is assessed and collected upon issuance of the building permit.

CONTACT

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Fax No. (416) 396-4265
E-mail: areaney@toronto.ca

SIGNATURE

Allen Appleby, Director
Community Planning, Scarborough District

ATTACHMENTS

Attachment 1: Draft Plan of Subdivision

Attachment 2: Elevations - Townhouses

Attachment 3: Zoning

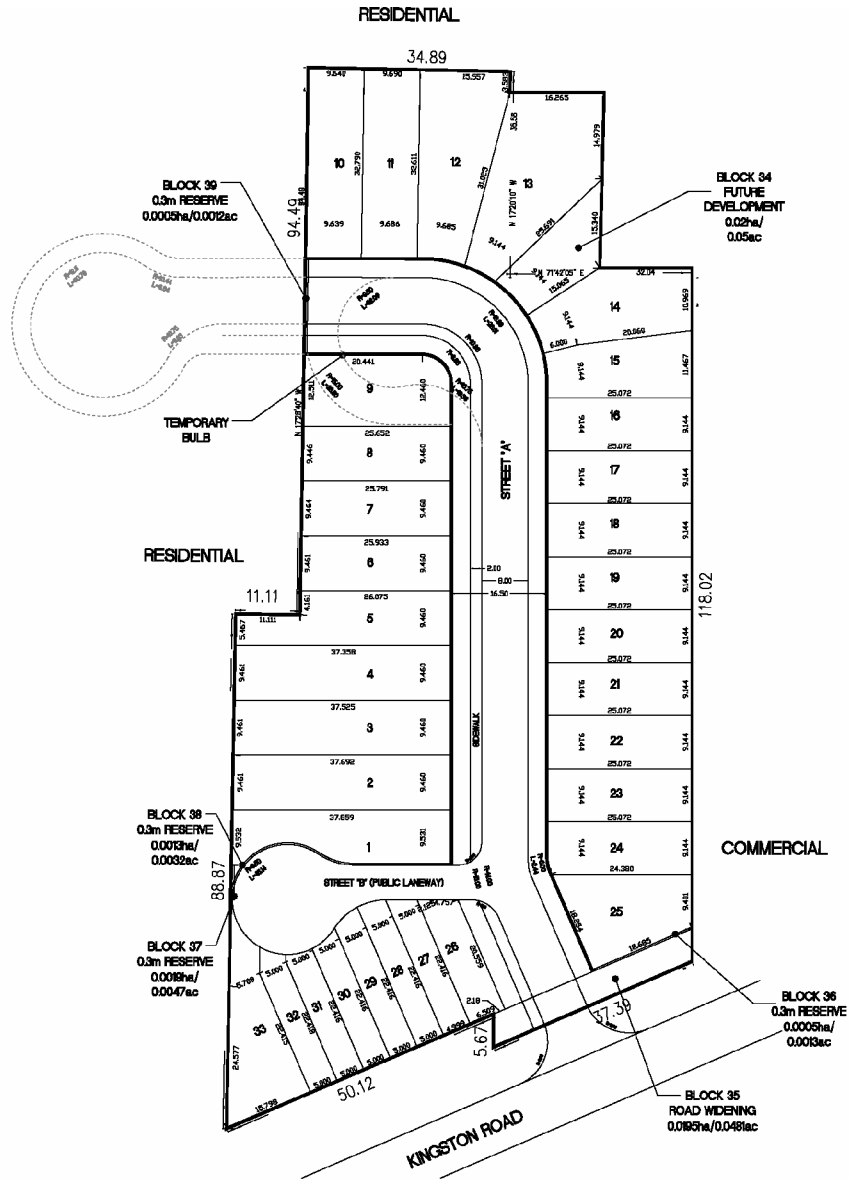
Attachment 4: Application Data Sheet

Attachment 5: Draft Zoning By-law Amendment

Attachment 6: Draft Site Plan Control By-law Amendment

Attachment 7: Recommended Conditions of Draft Plan of Subdivision Approval

Attachment 1: Draft Plan of Subdivision



Proposed Plan of Subdivision

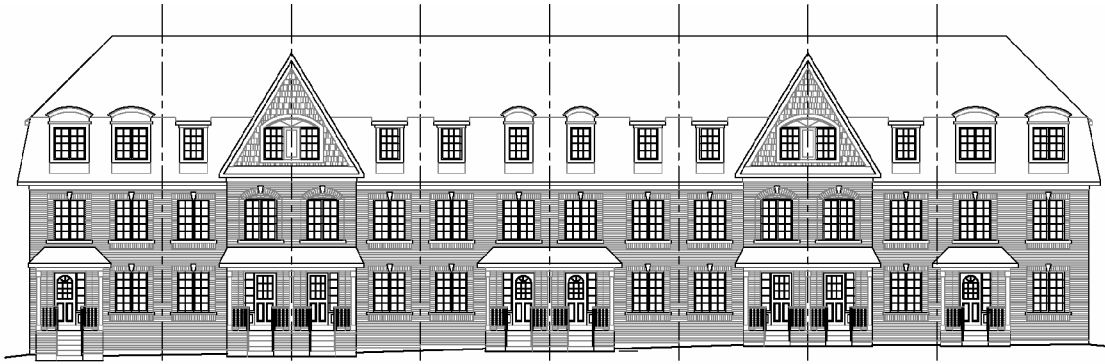
Applicant's Submitted Drawing

Not to Scale 

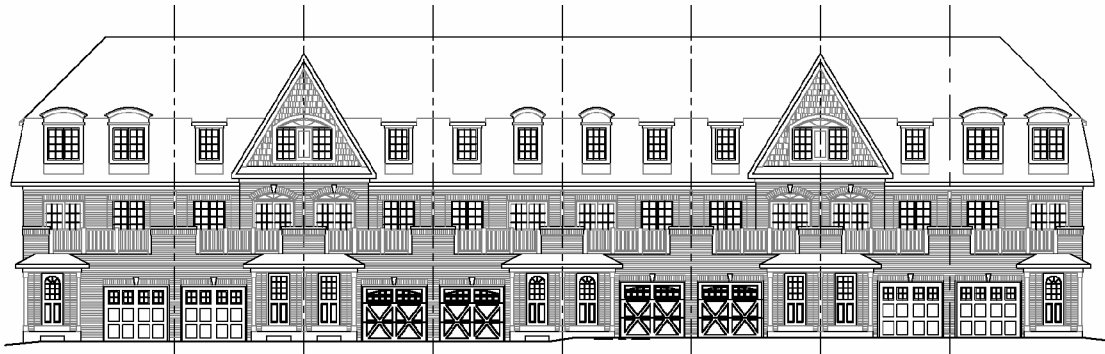
6440-6452 Kingston Road
& 4035-4041 Ellesmere Road

File # 07-106099_OZ, 07-106116_SB

Attachment 2: Elevations - Townhouses



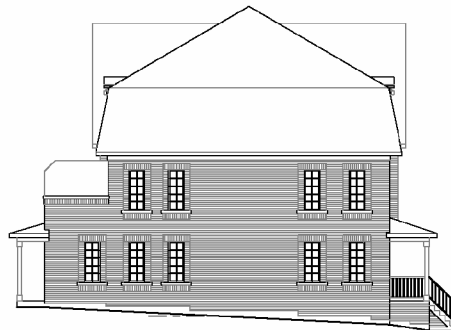
FRONT (KINGSTON ROAD)



LANEWAY



LOT 26 (EXTERIOR END)

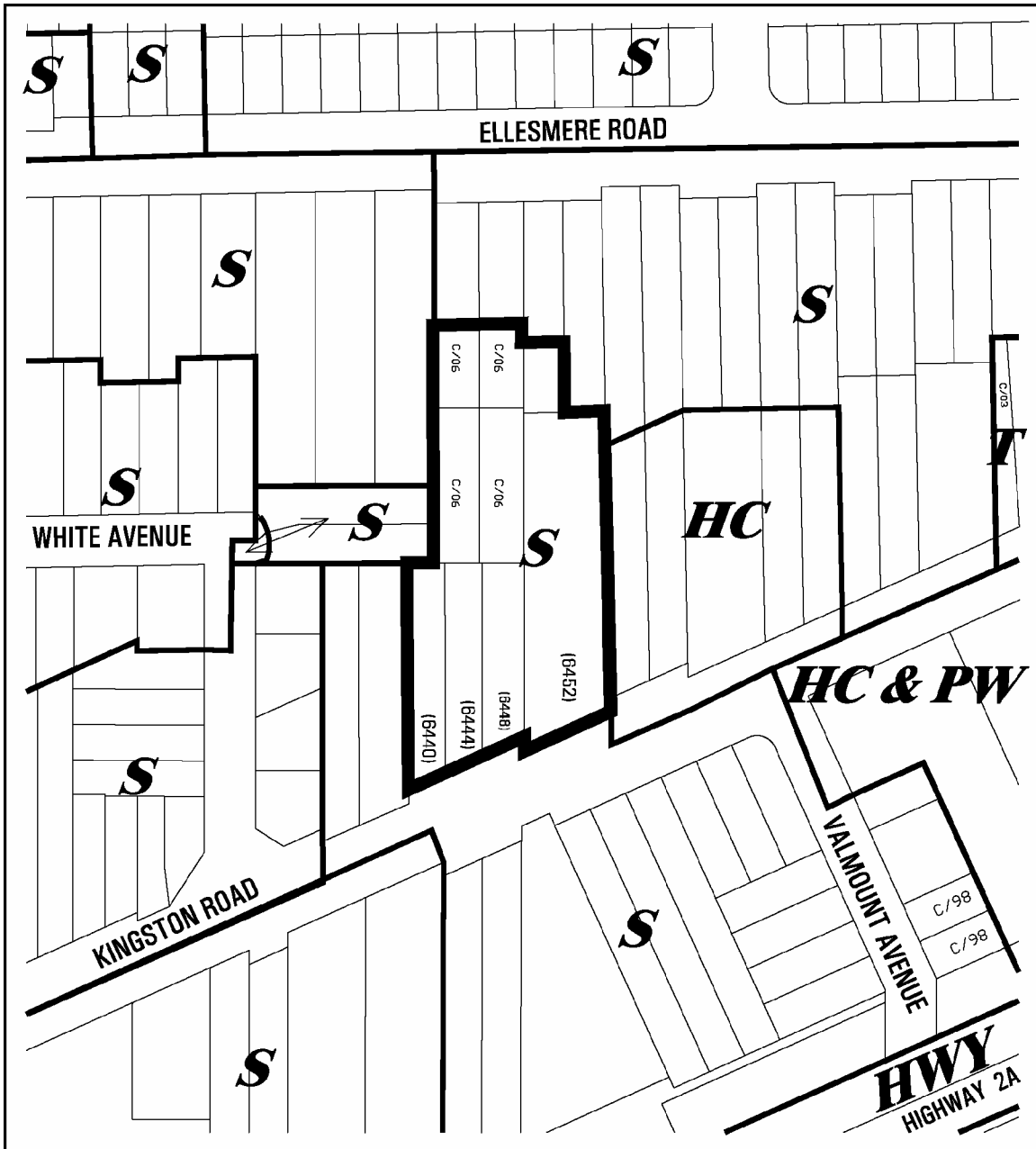


LOT 33 (INTERIOR END)

Elevation Plan
Applicant's Submitted Drawing
Not to Scale
5/22/08

**6440-6452 Kingston Road
& 4035-4041 Ellesmere Road**
File # 07-106099_OZ, 07-106116_SB

Attachment 3: Zoning



Toronto City Planning Division
Zoning

6440-6452 Kingston Rd, 4035-4041 Ellesmere Rd
 File # 07-106099_OZ, 07-106116_SB

- S** Single Family Residential
- T** Two Family Residential
- HC** Highway Commercial
- PW** Place(s) of Worship
- HWY** Highway

Highland Creek Community Bylaw
 Not to Scale
 5/22/08



Attachment 4: Application Data Sheet

| | | | |
|------------------|---------------------------------|---------------------|---|
| Application Type | Rezoning, Subdivision | Application Number: | 07 106099 ESC 44 OZ |
| Details | Rezoning, Standard, Subdivision | Application Date: | 07 106116 ESC 44 SB February 1, 2007 |

Municipal Address: 6440 - 6452 KINGSTON RD and rear portions of 4035 – 4041 Ellesmere Rd
 Location Description: Pt. Lot 8, RP 2732 and Pt. Lot 11, RP 3499 CON 1 PT LT4
 Project Description: 25 single family dwellings and 8 townhouse units on a new 16.5 m public road and a 6.0 m public laneway.

| | | | |
|---|---------------|-------------------|---|
| Applicant: | Agent: | Architect: | Owner: |
| MARSHALL MACKLIN MONAGHAN JAMIE BENNETT | | | BALLYMORE DEVELOPMENT (KINGSTON) CORP |

PLANNING CONTROLS

| | | | |
|----------------------------|----------------------------------|--------------------------|---|
| Official Plan Designation: | Neighbourhoods | Site Specific Provision: | Y |
| Zoning: | S-Single Family Residential Zone | Historical Status: | |
| Height Limit (m): | | Site Plan Control Area: | N |

PROJECT INFORMATION

| | | | | |
|------------------------------------|---------------------------------|---------|-----------------|----------------------------|
| Site Area (sq. m): | 11499.9 | Height: | Storeys: | 2 (singles) 3 (townhouses) |
| Frontage (m): | 87.51 | | Metres: | 10 m and 12 m |
| Depth (m): | 183.4 | | | |
| Total Ground Floor Area (sq. m): | 0 | | | Total |
| Total Residential GFA (sq. m): | 0 | | Parking Spaces: | 33 |
| Total Non-Residential GFA (sq. m): | 0 | | Loading Docks | 0 |
| Total GFA (sq. m): | 0 | | | |
| Lot Coverage Ratio (%): | 50 % (singles) 65% (townhouses) | | | |
| Floor Space Index: | 0 | | | |

DWELLING UNITS

| | |
|--------------|----------|
| Tenure Type: | Freehold |
| Rooms: | 0 |
| Bachelor: | 0 |
| 1 Bedroom: | 0 |
| 2 Bedroom: | 0 |
| 3 + Bedroom: | 0 |
| Total Units: | 33 |

FLOOR AREA BREAKDOWN (upon project completion)

| | Above Grade | Below Grade |
|----------------------------------|-------------|-------------|
| Residential GFA (sq. m): | 0 | 0 |
| Retail GFA (sq. m): | 0 | 0 |
| Office GFA (sq. m): | 0 | 0 |
| Industrial GFA (sq. m): | 0 | 0 |
| Institutional/Other GFA (sq. m): | 0 | 0 |

CONTACT: PLANNER NAME: Andrea Reaney, Senior Planner
TELEPHONE: (416) 396-7023

Attachment 5: Draft Zoning By-law Amendment

Authority: Scarborough Community Council Item ~ [or Report No. ~, Clause No. ~]
as adopted by City of Toronto Council on ~, 20~
Enacted by Council: ~, 20~

CITY OF TORONTO

Bill No. ~

BY-LAW No. ~-20~

**To amend the Highland Creek Community Zoning By-law No. 10827, as amended,
With respect to the lands municipally known as
6440, 6444, 6448, 6452 Kingston Road and the rear portions of 4035, 4037 and 4041
Ellesmere Road**

WHEREAS authority is given to Council by Section 34 of the *Planning Act*, R.S.O. 1990, c.P. 13, as amended, to pass this By-law; and

WHEREAS Council of the City of Toronto has provided adequate information to the public and has held at least one public meeting in accordance with the *Planning Act*;

The Council of the City of Toronto HEREBY ENACTS as follows:

1. Schedule “A” is amended by deleting the current zoning for the lands outlined on Schedule ‘1’ and adding the following Zoning to the outlined lands:

S – 1 – 60 – 95A – 129A – 138 – 268 - 270

S – 1 -60 – 95A – 138- 268 - 270

S – 1 - 95A – 138 – 151 – 268 - 270

S -1 – 95A – 138 – 150- 268 - 270

ST – 1 –60 C – 95 A – 129B – 129C – 141 – 269 – 270 -408

2. Schedule “B”, PERFORMANCE STANDARDS CHART, is amended by adding Performance Standards 60C, 129A, 129B, 129C, 150, 151, 268, 269, 270 and 408 as follows:

SIDE YARD

60C. Minimum 1.2 m for the end wall and for **flankage yards** of the **street townhouse dwellings** and 0 m for the common lot lines coincident with the common wall shared by **dwelling units**.

BUILDING SETBACK FROM LOT LINES OTHER THAN STREET LINES

- 129A. Minimum 7.0 m **rear yard** building setback.
- 129B. Minimum 3 m **rear yard** building setback.
- 129C. For **street townhouses**: balconies, unenclosed porches and canopies may project from the **main wall** a maximum of 1.55 m into a required **front yard** abutting a **street** or into a required **rear yard**.

BUILDING SETBACK FROM STREETS

150. Minimum **flankage yard** 1.5 m and minimum interior **side yard** 0.6 m.
151. Minimum **flankage yard** 3.0 m and minimum interior **side yard** 0.6 m.

MISCELLANEOUS

268. The maximum coverage for all buildings and structures shall be 50% of the lot or parcel.
269. The maximum coverage for all buildings and structures shall be 65% of the lot or parcel.
270. Within the lands shown on Schedule '1' attached to this By-law, no person shall use any land or erect or use any building or structure unless the following municipal services are provided to the lot line and the following provisions are complied with:
- (a) all new public roads have been constructed to a minimum of base curb and base asphalt and are connected to an existing public highway, and
 - (b) all water mains and sanitary sewers, and appropriate appurtenances, have been installed and are operational.

HEIGHT

408. Maximum **height** of buildings 12 m and 3 storeys, excluding **basements**.
3. Schedule "C" is amended by adding Exception Numbers 47, 48 and 49 to the lands outlined on Schedule '2' as follows:
- 47. On the lands identified on the accompanying Schedule '2' the following provisions shall apply:

Notwithstanding the definition of **street** in **CLAUSE V (f) Definitions**, a **street** shall have a minimum width of 16.5 metres.

48. On the lands identified on the accompanying Schedule ‘2’ the following provisions shall apply:

Notwithstanding the definition of **street** in **CLAUSE V (f) Definitions**, a public laneway having a minimum width of 6.0 metres shall be considered a **street**.

49. On the lands identified on the accompanying Schedule ‘2’ the following provisions shall apply:

Minimum 3.0 m side yard building setback for the side of the building closest to Kingston Road.

ENACTED AND PASSED this ~ day of ~, A.D. 20~.

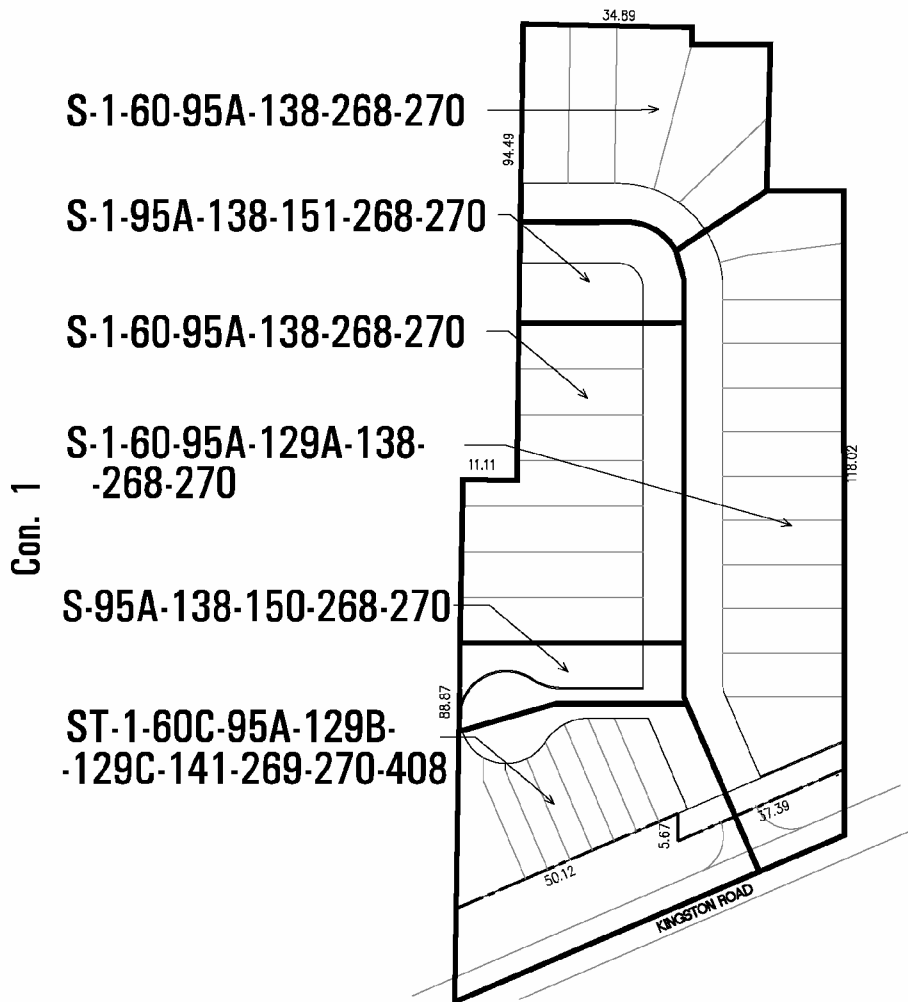
DAVID R. MILLER,
Mayor

ULLI S. WATKISS,
City Clerk

(Corporate Seal)

Schedule '1'

Lot 4



Zoning By-Law Amendment

6440-6452 Kingston Rd & 4035-4041 Ellesmere Rd

File # 07-106099 OZ, 07-106116 SB



Area Affected By This By-Law

Highland Creek Community Bylaw

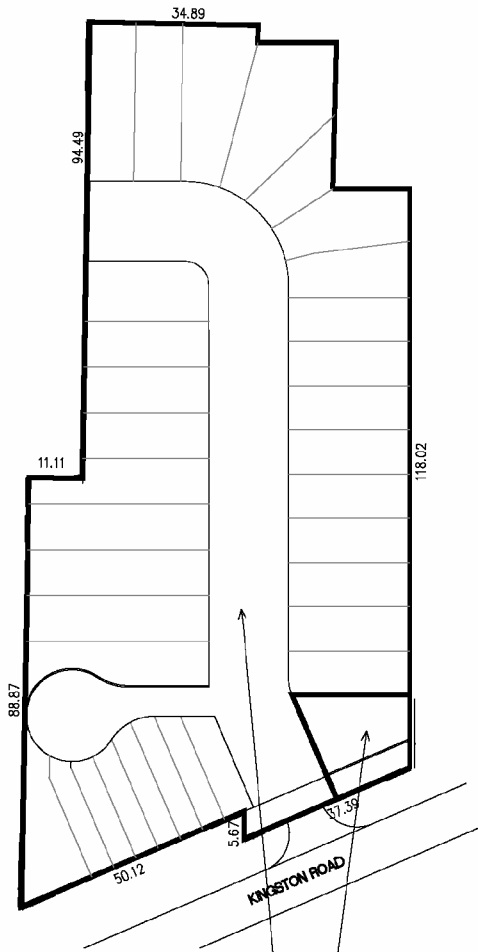


Not to Scale
5/22/08

Schedule '2'

Lot 4

Con. 1



Exception No. 47,48,
Exception No. 49



Zoning By-Law Amendment

6440-6452 Kingston Rd & 4035-4041 Ellesmere Rd

File # 07-106099 OZ, 07-106116 SB



Area Affected By This By-Law

Highland Creek Community Bylaw



Not to Scale
5/22/08

Attachment 6: Draft Site Plan Control By-law

Authority: Scarborough Community Council Item ~ [or Report No. ~, Clause No. ~]
as adopted by City of Toronto Council on ~, 20~
Enacted by Council: ~, 20~

CITY OF TORONTO

Bill No. ~

BY-LAW No. ~-2008

**To amend Site Plan Control By-law No. 21319, as amended, of the former City of Scarborough, to designate a Site Plan Control Area, with respect to a portion of the lands known municipally in the year 2007 as,
6440, 6444, 6448, 6452 Kingston Road**

WHEREAS authority is given to Council by Section 114 of the *City of Toronto Act*, S.O. 2006, Chapter 11, Schedule A, to designate the whole or any part of the area covered by an Official Plan as a Site Plan Control Area;

The Council of the City of Toronto HEREBY ENACTS as follows:

1. By-law no. 21319 is amended by designating the land shown outlined by a heavy black line on Schedule '1' of this By-law as a Site Plan Control Area.

ENACTED AND PASSED this ~ day of ~, A.D. 2008.

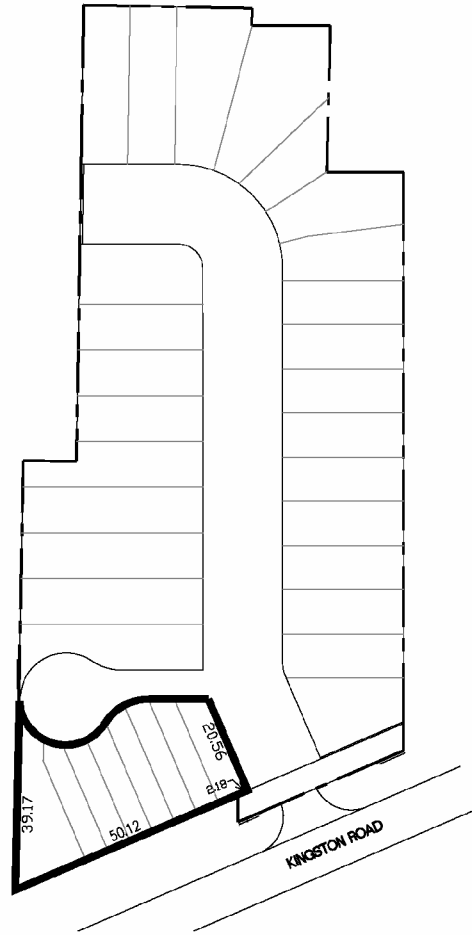
DAVID R. MILLER,
Mayor

ULLI S. WATKISS,
City Clerk

(Corporate Seal)

Schedule '1'
Lot 4

Con. 1



Site Plan Control Amendment

6440-6452 Kingston Rd & 4035-4041 Ellesmere Rd

File # 07-106099 OZ, 07-106116 SB



Area Affected By This By-Law

Highland Creek Community Bylaw
Not to Scale
5/22/08



Attachment 7: Recommended Conditions of Draft Plan of Subdivision Approval

**CITY OF TORONTO
CONDITIONS OF DRAFT PLAN OF SUBDIVISION APPROVAL**

**SUBDIVISION APPLICATION 07 106116 ESC 44 SB
DRAFT PLAN OF SUBDIVISION PREPARED BY MARSHALL MACKLIN
MONAGHAN LIMITED
JOB NO. 14.06212.01.P01 DATED JUNE 14, 2007
AS REVISED TO FEBRUARY 22, 2008**

**PART OF LOT 8 REGISTERED PLAN 2732, PART OF LOT 11, REGISTERED
PLAN 3499 AND PART OF LOT 1 CONCESSION 4
CITY OF TORONTO (FORMERLY CITY OF SCARBOROUGH)
BALLYMORE DEVELOPMENT (KINGSTON) CORPORATION**

THE DRAFT PLAN OF SUBDIVISION MUST BE REVISED TO REFLECT THE FOLLOWING COMMENTS PRIOR TO DRAFT PLAN APPROVAL:

1. 6.0 metre corner roundings (street line radius) must be illustrated on both sides of Street 'A' intersecting at Kingston Road with 8.0 metre curb radii.
2. Delete the proposed road widening along the Kingston Road frontage and incorporate these lands into the adjacent lots, and Street "A".
3. 0.3 m reserves shall be applied the Kingston Road frontage of the plan, excluding Street 'A'.
4. 0.3 m reserves shall be applied to a portion of the temporary turning circle on Street "A", in accordance with the requirements of the Transportation Services Division.
5. The curb line within the public laneway (Street 'B') must be detailed with a double line, identifying the curb radii at 5.0 m, at the intersection with Street 'A'.
6. The temporary turning circle on Street "A" shall be illustrated with solid lines.

**THE FOLLOWING CONDITIONS MUST BE FULFILLED PRIOR TO THE
REGISTRATION OF THE PLAN OF SUBDIVISION OR ANY PHASE
THEREOF FOR REGISTRATION UNLESS OTHERWISE SPECIFIED BELOW**

Subdivision Agreement

1. The Owner shall enter into the City of Toronto's standard residential Subdivision Agreement and satisfy all pre-registration conditions.

Zoning

2. Prior to the registration of the draft plan of subdivision, the subject land is to be zoned in accordance with the uses proposed on the draft plan to the satisfaction of the Chief Planner.

Tax Payment

3. Prior to the registration of the draft plan of subdivision, the Owner shall pay the City in full all taxes on all lands included in the said plan of subdivision for the current taxation year. The taxes shall be estimated by the City, if required and where such taxes have been estimated, they shall be subject to readjustment upon final determination thereof. The Owner shall provide to the Director of Community Planning, Scarborough District, a Statement of Account from the Revenue Services Division, confirmation of payment of all outstanding and current taxes.

Lapsing

4. The approval of this plan of subdivision will lapse if the subdivision is not registered within 5 years of the date of draft plan approval.

SERVICES

Easement Requirements

5. Prior to the registration of the draft plan of subdivision, the Owner shall make satisfactory arrangements to provide such easements to the City as deemed necessary by the Executive Director, Technical Services for utilities, drainage and servicing purposes to the appropriate authorities in order to service this plan of subdivision.

Servicing

6. Prior to the registration of the draft plan of subdivision, the Owner shall
 - a) construct all necessary municipal watermain, storm and sanitary sewers necessary to service the plan of subdivision, including all external services necessary to develop their lands at their sole cost, to the satisfaction of the Executive Director, Technical Services.

- b) construct all new public roads to a minimum of base curb and base asphalt and connect all new public roads to an existing public highway to the satisfaction of the Executive Director, Technical Services.
7. The Owner agrees to provide and maintain a minimum clearance of 1 metre from any hydro street light/poles. All costs of alteration, including any poles relocated and reinstatement of any other existing utilities are the responsibility of the Owner and at no cost to the City.

Hydroelectric

8. Prior to the registration of the draft plan of subdivision, the Owner shall make satisfactory arrangements, financial and otherwise (including providing easements) with a hydroelectric provider including, amongst other matters the following regarding the installation of an underground hydro and street lighting system to the satisfaction of Toronto Hydro:
- (i) underground civil and electrical system for the development;
 - (ii) the provision of adequate space within the development for transformers, tap boxes and any other hydro equipment of the local distribution company, and;
 - (iii) any existing electrical feeds to any existing or demolished buildings on the property must be removed at the developer's expense.

Street Lighting

9. The Owner shall agree to provide street lighting within the plan of subdivision at their sole cost. Such street lighting shall be installed prior to occupancy of any dwelling to the satisfaction of Toronto Hydro, in consultation with the Chief Planner.

Surveys, Mapping, Road Widenings, Fire Hydrant Maintenance, Etc.

10. Prior to the registration of the draft plan of subdivision, the Owner shall agree to pay the cost of all geodetic surveys, aerial mapping, traffic signs, upgraded street lighting and fire hydrant maintenance within the plan of subdivision satisfactory to the Executive Director, Technical Services.

Composite Utility Plan

11. Prior to the earlier of release for construction of services or the registration of the draft plan of subdivision, the Owner shall prepare a composite utility plan, prepared by a licensed municipal servicing engineer, who shall co-ordinate, on behalf of the City and to City standards, all utility company installation proposals within the public right-of-ways. This same consulting engineer shall provide a composite utility plan, indicating the utility company's installations of services

within the municipal road allowances created and/or extended to service this development. This plan shall be completed to the satisfaction of the utility companies, the General Manager of Parks Forestry and Recreation, the Chief Planner, and the Executive Director of Technical Services.

Preparation of Final Plan of Subdivision

12. The Owner shall prepare the final plan of subdivision in metric units related to the Ontario Co-ordinate System and shall submit a digital copy of the plan to the Executive Director, Technical Services

STREETS, DRIVEWAYS, SIDEWALKS AND 0.3 METRE RESERVES

Construction of Streets

13. Prior to the registration of the draft plan of subdivision, the Owner shall construct Street A as a fully serviced road and dedicate it as a public right of way at no cost to the City, and construct Street B and dedicate it as a public right of way free and clear of any encumbrances to the satisfaction of the Executive Director, Technical Services as follows:
 - (a) Street A shall be constructed with a 16.5 metre road allowance,
 - (b) The temporary turning circle on Street A and on Lot 9 shall be constructed with a minimum 9.5 metre curb radius, a street line radius of 15 metres, and a turning radius with a centreline radius of not less than 12.0 metres on the proposed public road.
 - (c) Street B shall be constructed with a 6.0 metre road allowance.

Naming of Streets and Street Signage

14. Prior to the registration of the draft plan of subdivision, the Owner shall apply to the City for approval of the naming of the road allowances shown on the draft plan to the satisfaction of the Executive Director, Technical Services. Further, the Owner shall be responsible for all costs associated with the installation/alternations of municipal street signage required for this development.

Driveways

15. The Owner shall agree that all lot layouts shall create street frontages such that no driveway entrance shall overlap with the adjacent driveway approach within the boulevard to the satisfaction of the Executive Director, Technical Services.

Sidewalks

16. The Owner shall agree to construct a municipal concrete sidewalk within the boundaries of the plan of subdivision along one side of Street A, in accordance with the approved engineering drawings, and along the frontage of the property at Kingston Road shall remove the 1.5 metre wide sidewalk and construct a sidewalk with a width of 1.7 metres, which shall be completed prior to the occupancy of any dwelling, to the satisfaction of the Executive Director, Technical Services.

0.3 m Reserves

17. Prior to the registration of the plan of subdivision, the Owner is required to provide 0.3 metre reserves along portions of the temporary turning circle of Street “A”, along the frontage of the property along Kingston Road on either side of Street “A”, and shall convey and dedicate all 0.3 metre reserves to the City of Toronto as a Public Highway, to the satisfaction of the Executive Director of Technical Services.

Roadway Temporary Turning Circle Street “A” – Lot 9, 10, 11

18. The Owner shall agree that Lot 9 shall be deemed to be a block for future development and will be made available for residential uses only when the temporary turning circle is removed from the lands, and the lands are reinstated for residential use to the satisfaction of the Chief Planner and the Executive Director, Technical Services.
19. The Owner shall agree to provide easements as may be required for access to Lots 10 and 11, and further to reinstate driveways for Lots 10 and 11 as may be necessary when Street “A” is extended to the satisfaction of the Executive Director, Technical Services.

Warning Clauses – Public Street and Public Laneway, Rear Yard Catchbasins, Swales

20. Prior to the registration of the plan of subdivision, the Owner shall agree to include the following warning clauses in all agreements of purchase and sale and/or lease agreements and registered on title to the satisfaction of the City Solicitor:

“Purchasers are advised that where sidewalks are located adjacent to the curb or where the right-of-way width is less than 18.5 metres, sidewalk snow clearing and driveway windrow clearing will not be provided by the City.”

“Purchasers are advised that snow clearing will not be provided by the City on the public laneway.”

21. Prior to registration of the plan of subdivision, the Owner agrees to provide its Solicitor's confirmation to the City advising that the clause set out above have been included in applicable offers of purchase and sale and/or lease agreements to ensure that future occupants are aware of the City's snow clearing practices on these lands.
22. Prior to the registration of the draft plan of subdivision the Owner shall agree to include the following warning clauses in all agreements of purchase and sale and/or lease agreements and registered on title to the satisfaction of the City Solicitor, informing the homeowner that they are responsible for the upkeep and maintenance of rear yard catchbasins, drainage swales and catchbasin lead (i.e. connections) located on private property.

STORM DRAINAGE

Storm Water Management Plan

23. Prior to the registration of the draft plan of subdivision and in conjunction with engineering drawing approvals, the Owner shall submit a Stage II Stormwater Management Report addressing details including the calculation of storage pipes and overland flow routes, and a revised Municipal Servicing Report all to the satisfaction of the Executive Director, Technical Services.

ENVIRONMENTAL SITE ASSESSMENT

Record of Site Condition and Remedial Action Plan

24. Prior to the earlier of the release for construction of services or registration of the draft plan of subdivision, the Owner agrees to provide an Environmental Report and Record of Site Condition for the land to be transferred to the City. The report must present the soil condition and compliance with the current criteria of the Ministry of the Environment guidelines for use at contaminated sites in Ontario.
25. Prior to the earlier of release for construction of services or registration of the draft plan of subdivision, the Owner agrees to pay all costs associated with the City retaining a third party peer reviewer and submit a certified cheque payable to the City of Toronto in the amount of Five Thousand Dollars (\$5,000,00) as a deposit towards the cost of the peer review, and to make further deposits in the specified amount as required by the City from time to time, all to the satisfaction of the Executive Director of Technical Services.

FENCING

26. Prior to the registration of the draft plan of subdivision, the Owner shall provide a fencing plan, which illustrates perimeter fencing around the site, as well as fencing within flankage yards for corner lots, to the satisfaction of the Chief

Planner, the Executive Director of Technical Services and the General Manager of Parks, Forestry and Recreation.

BLOCK 34 – FUTURE DEVELOPMENT

27. Block 34 of the proposed draft plan of subdivision shall not be used until combined with abutting lands to the east to the satisfaction of the Chief Planner.

PARKLAND

28. The Owner agrees to pay a cash-in-lieu of parkland dedication prior to the issuance of the building permits, to the satisfaction of the General Manager of Parks, Forestry and Recreation

TREES

Tree Preservation

29. The owner agrees that post holes for fences within tree protection zones must be hand dug, or excavated using the hydrovac method of excavation, and that roots larger than 2.5cm in diameter will not be cut. If necessary, post holes should be relocated to ensure significant roots are not cut, to the satisfaction of the General Manager of Parks, Forestry and Recreation.

Tree Removal

30. The Owner agrees to apply for and receive all necessary permits under the City of Toronto Tree By-laws to injure or remove trees on the property, as may be determined to be required by the General Manager, Parks, Forestry and Recreation. The Owner further agrees to provide all replacement tree plantings on the subject property or in such other manner as may be required through conditions of approval of such permits, satisfactory to the General Manager of Parks, Forestry and Recreation.
31. The Owner shall provide a Tree Preservation and Replanting Plan to the satisfaction of the General Manager of Parks, Forestry and Recreation.

Street Trees

32. The Owner shall provide a Street Tree Planting Plan to the satisfaction of the General Manager of Parks, Forestry and Recreation.
33. Prior to the removal of City owned street trees, the Owner agrees to submit a completed Agreement to Perform Arboricultural Work on City Owned Street Trees, to the satisfaction of the General Manager of Parks, Forestry and Recreation.

34. Prior to the acceptance of engineering drawings by Technical Services, the Owner agrees to submit composite utility plans, indicating the location of all existing and proposed underground and above ground utilities in relation to proposed tree planting locations, at a minimum scale of 1:250, to the satisfaction of Technical Services and the General Manager of Parks, Forestry and Recreation.
35. The owner agrees that trees will be planted by a qualified arborist or nursery person. They must determine how much settlement, if any, may occur, to ensure trees are not planted too high, or settle too deeply. Trees are to be planted after final grading, and once the sod has been laid.
36. Prior to tree planting, the Owner agrees to provide a two-year renewable guarantee for all trees planted within the road allowance.
37. Prior to tree planting, the owner agrees to submit to Urban Forestry, a watering schedule for newly planted trees during the guarantee period, acceptable to the General Manager of Parks, Forestry and Recreation.
38. The owner agrees to contact Urban Forestry one week prior to tree planting, and once the trees have been planted, in order to establish the anniversary dates for the planting(s).
39. Upon written request from the Owner, Parks, Forestry and Recreation agrees to inspect the newly planted trees and will advise the Owner if the trees are satisfactory. The Owner acknowledges and agrees to maintain the trees for a period of two (2) years after being advised in writing that Parks, Forestry and Recreation is satisfied with the newly planted trees. The Owner acknowledges that, should any newly planted tree require replacement during this guarantee period, the tree shall be replaced immediately and shall have an additional two (2) year maintenance period placed on this new planting. At the end of that period, upon acceptance, the City shall accept maintenance responsibilities and return the Letter of Credit.

Tree Planting Strategy Information Booklet

40. Prior to the registration of the plan of subdivision, the Owner shall prepare an information booklet outlining the tree planting strategy within the community and the ongoing responsibilities of the homeowners and the City in order to achieve successful urban tree plantings within the community. This booklet will be prepared to the satisfaction of the General Manager, Parks, Forestry & Recreation and will be distributed to all homeowners for all dwellings within the limits of this plan of subdivision.

WARNING CLAUSES – TDSB

41. Prior to the registration of the draft plan of subdivision, or the issuance of a building permit, the Owner shall enter into an agreement to erect and maintain signs, at points of egress and ingress of the development site advising that:

“The Toronto District School Board makes every effort to accommodate students locally. However, due to residential growth, sufficient accommodation may not be available in this area for all students. Students may be accommodated in facilities outside the area until adequate funding or space becomes available.

For information regarding designated school(s), please call (416) 394-7526.

42. The Owner shall agree in the Subdivision agreement, or in a separate agreement between the School Board and the Developer, to include the following warning clauses in all offers of purchase and sale of residential units (prior to registration of the plan and for a period of 10 years following registration), that:

“Despite the best efforts of the Toronto District School Board, sufficient accommodation may not be locally available for all students anticipated from the development area and that students may be accommodated in facilities outside the area, and further, that students may later be transferred.

Purchasers agree for the purpose of transportation to school, if bussing is provided by the Toronto District School Board in accordance with the Board’s policy, that students will not be bussed home to school, but will meet the bus at designated locations in or outside of the area.”

BELL CANADA

43. Prior to the registration of the draft plan of subdivision the Owner must confirm with Bell Canada that sufficient wire-line communication/telecommunication infrastructure is currently available within the proposed development to provide communication/telecommunication service to the proposed development. In the event that such infrastructure is not available, the Owner is hereby advised that the Owner may be required to pay for the connection to and/or extension of the existing communication/telecommunication infrastructure. If the Owner elects not to pay for such connection to and/or extension of the existing communication/telecommunication infrastructure, the Owner shall be required to demonstrate to the municipality that sufficient alternative communication/telecommunication facilities are available within the proposed development to enable, at a minimum, the effective delivery of communication/telecommunication services for emergency management services (i.e. 911 Emergency Services).

ENBRIDGE GAS DISTRIBUTION INC.

44. The Owner shall agree to grade all streets to final elevation prior to the installation of the gas lines and provide Enbridge Gas Distribution Inc with the necessary field survey information for the installation of the gas lines. Further, in the event that it is not possible for the natural gas distribution system to be installed within the proposed road allowance, that easements will be provided at no cost to Enbridge Gas Distribution Inc.
45. The Owner shall prepare an overall utility distribution plan to the satisfaction of Enbridge Gas Distribution.
46. The Owner shall agree in the subdivision agreement to grade all streets to final elevation prior to the installation of the gas lines and provide the necessary field survey information required for the installation of the gas lines, all to the satisfaction of Enbridge Gas Distribution.

CONSTRUCTION MANAGEMENT PLAN

47. Prior to registration of the plan of subdivision or prior to commencement of any construction activities, the Owner agrees to submit a Construction Management Plan for review and acceptance, to the satisfaction of the Executive Director of Technical Services. The construction management plan shall include the following:
 - i. Dust/mud/sediment control on and off-site;
 - ii. Location of truck loading points and trailer parking;
 - iii. Location and height of temporary storage areas;
 - iv. Access/truck routing;
 - v. Provision of hoarding and temporary fencing;
 - vi. Details of piling and shoring activities;
 - vii. Estimated period of construction;
 - viii. Control of storm run-off, including any siltation control devices which may be required;
 - ix. Parking management plan for employee/construction vehicle parking; and
 - x. Provisions for the removal of debris in a timely manner.

OTHER

48. The Owner shall agree in the subdivision agreement to complete or comply with any other requirements by the City acting reasonably that results from the City's examination and review of all requisite plans, drawings, reports and submissions required by the City to facilitate this development.