

2 RECOMMENDATIONS

2.1 The Vision

Cliffside Village has significant potential for redevelopment. Positive characteristics for redevelopment include deep land parcels that are generally underutilized; a wide right-of-way width (36 metre) which is appropriate for mid-rise buildings; views to Lake Ontario; proximity to downtown; proximity to stable neighbourhoods and local parks; and access to the Bluffs. These attributes in addition with improved transit will provide more direct links to downtown Toronto and will provide excellent conditions for new mixed use development in this area.

This redevelopment will occur in accordance with a vision that reflects the needs of the community and the city while providing enough flexibility and opportunity to encourage developers to reinvest.

Through the public engagement process for this study, several important community values emerged that should influence how change should occur in Cliffside Village. Key values include encouragement of mid-rise mixed use development balanced with beautiful, accessible and safe streets and public spaces; green and sustainable development; enjoyable and efficient active transportation system (transit, cycling and walking); enhancement of the village character; respect for the surrounding established neighborhoods and high quality, distinct and appropriate built form that promotes opportunities for revitalization.

The Cliffside Village Kingston Road corridor will be transformed to support a **vibrant, self-sustaining, mixed use community** with a **distinct character** and features that set it apart from other communities on Kingston Road.

Appropriate built form and **pedestrian priority environments** with **excellent facilities for transit and cycling** will form the basis of an **environmentally friendly, green and beautiful** community identity.



New development should enhance the unique character of Cliffside Village while providing context specific opportunities for revitalization.

EAST

Midland Avenue

Kingston Road





Kingston Road

WEST

Development as Good Neighbour



Transformation



Progressive Community



Safe, Secure & Accessible



2.2 Guiding Principles

The following eight guiding principles should direct change in Cliffside Village. They are based on the ideas and values of the Cliffside Community and the City of Toronto.

Development as Good Neighbour

All new development will demonstrate “good fit” with the immediate and adjacent areas, in particular, the stable, low-rise residential areas to the north and to the south. Plans for new development will include mitigating measures for the infiltration of traffic, parking, views, shade, noise, nighttime light and use conflicts. New mid-rise buildings will be appropriately massed and scaled to create a high quality pedestrian environment, and will coordinate expansion or location of additional community facilities to alleviate strain on existing services.

Transformation

Promote high quality urban design that reinforces the Vision, supporting Cliffside Village as a mixed use corridor. New development will create a vibrant, livable community that encourages residential intensification by reinforcing connections to existing community assets, providing desirable community related facilities and integrating new residential uses in a way that respects, preserves and enhances established neighbourhoods.

Progressive Community

Promote the use of transit, cycling and walking by minimizing vehicle related features such as surface parking and traffic lanes. Harmonize movement and connectivity within the study area and to adjacent areas to achieve a better balance between pedestrians, cyclists, motorists and transit.

Safe, Secure and Accessible

The physical environment will be designed to foster comfort and safety and will accommodate people of all abilities and ages. A balance will be achieved between pedestrian, cyclist, transit and vehicle movement that will maximize safe and controlled interfaces while a finer grain of streets and blocks will be created to improve connectivity to adjacent neighbourhoods and alleviate transportation bottlenecks.

Economic Health

New development will create a vibrant community that encourages private sector and business development by identifying key sites that will energize reinvestment in the area and create and reinforce a commercial identity for Cliffside Village. Promote high quality urban design throughout the corridor that reinforces the Vision for the area and promotes a mix of uses that expands population and diverse employment opportunities.

Pedestrian Place

Encourage a mix of uses and built form that promotes high quality, attractive and vibrant places that enhance the pedestrian experience. Reinforce the urban street wall, encourage retail, commercial or public uses at grade and reduce the dominance of vehicles in the corridor. Create an organized, beautiful and green infrastructure for pedestrian movement that addresses linkages to existing community assets and includes a new civic space.

Reflect Community Identity

Reflect the quality and nature of the surrounding community within the corridor. Extend the beautiful, lakefront community character into the corridor, provide opportunities for businesses that will meet the needs of the surrounding neighbourhoods and upgrade and maintain high quality streetscapes, open spaces and parks. Build on area distinctiveness by promoting and connecting existing and future community assets and protecting and enhancing heritage features.

Complete “Green” Community

Cliffside Village should evolve as a sustainable community contributing to the City's efforts to reduce its carbon footprint. The community will provide a diversity of land uses for living, shopping, working, recreating and reducing the need for vehicular travel. Opportunities for greening the area through private and public investment, including an increased tree canopy, green buildings, new parks, open spaces and streetscapes are optimized. Protection of natural systems will be prioritized and balanced to direct and guide the planning of future development activities. New buildings will be designed to exceed the City's Green Building Standards.

Economic Health



Pedestrian Place



Reflect Community Identity



Complete “Green” Community



2.3 Urban Design Concept Plan

The Urban Design Concept Plan has been developed to address the significant redevelopment opportunities that exist in the corridor while applying the values and ideas expressed in the guiding principles. The plan represents a synthesis of concepts and ideas that evolved throughout the study process and includes input from the public, a design charrette, various City departments (urban design, transportation and transit, policy and research) and the sub-consultants (itrans and UMC).

Although the entire Cliffside Village corridor has been studied, distinct differences between the north and south sides merit a separate evaluation. In many cases, recommendations in this report have been organized into “north side” and “south side” categories. Please refer to section 3.0 Background for area character and conditions.

The North Side

The Urban Design Concept Plan illustrates approximately one third of the developable area on the north side of Kingston Road redeveloped with the remaining fabric revitalized through adaptation of existing buildings. New development is likely to occur at locations that have existing buildings that are 1 storey in height, are currently underutilized or vacant and located at corners. Key urban design recommendations for the north side include: new mid-rise buildings with a maximum height of eight storeys (between Highview Avenue and Midland Avenue); a widened rear lane (Sandown Lane) to provide rear lot access and parking; mid-block connections; new public park/plaza opportunities; and widened tree lined sidewalks.

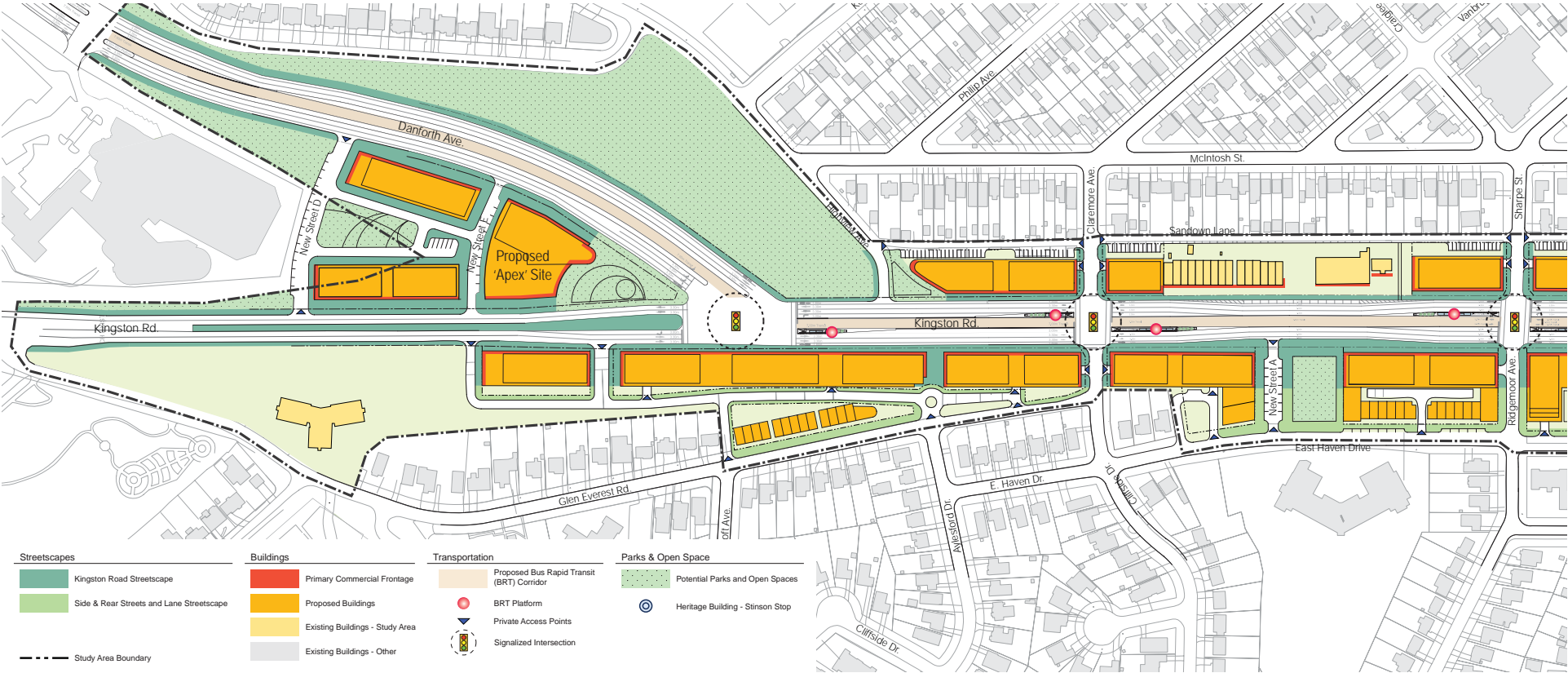


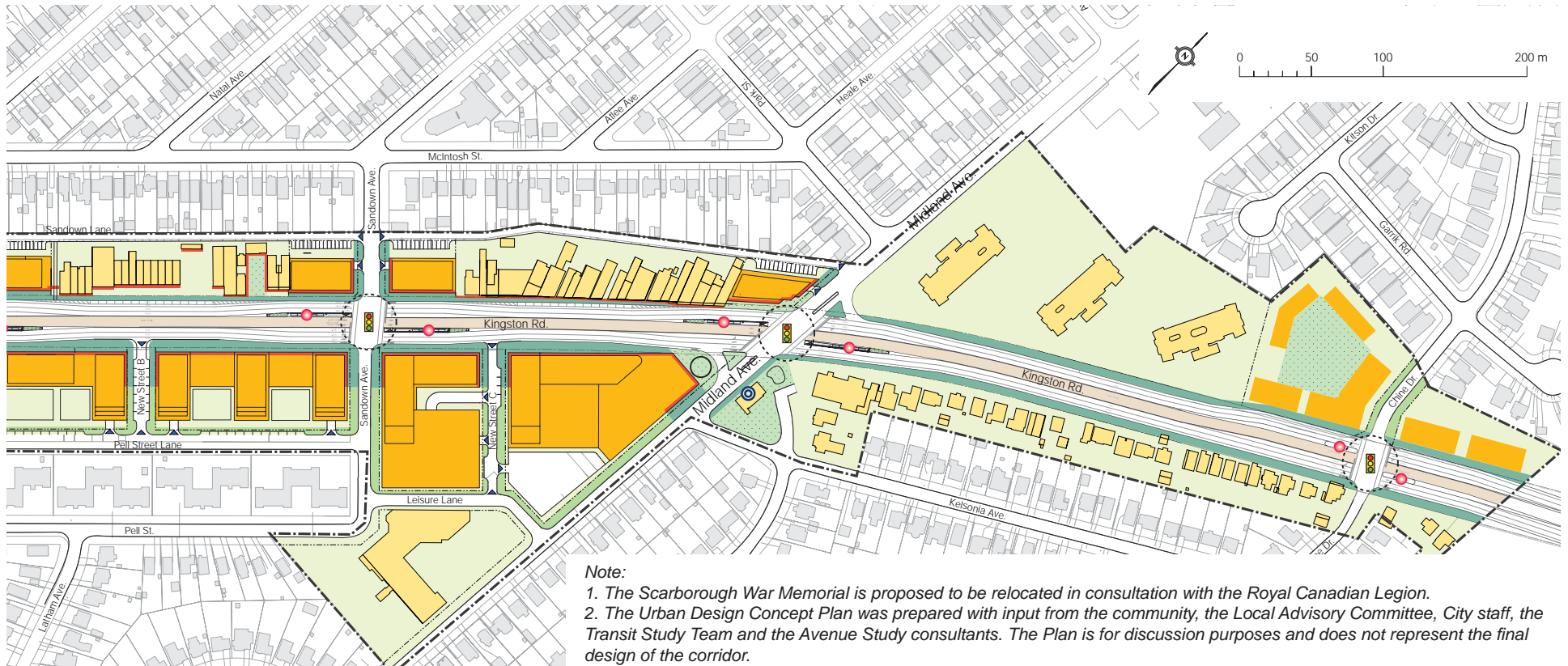
Figure 1 - Urban Design Concept Plan

The South Side

The Urban Design Concept Plan illustrates the redevelopment of all sites on the south side of Kingston Road between Danforth Avenue and Midland Avenue. Key urban design recommendations for the south side include: new buildings with a maximum height of eleven storeys (excluding areas to the east of Midland Avenue); the conversion of rear lanes to public streets to improve access; opportunities for on-street parking, the introduction of new streets and mid-block connections; widened tree lined sidewalks; new public park/plaza opportunities and the inclusion of a destination oriented commercial anchor at the south west corner of Kingston Road and Midland Avenue.

Kingston Road Right-of-Way

The Urban Design Concept Plan includes the proposed alignment and configuration for transit improvements along Kingston Road. The proposed transit improvements include a centred, dedicated transit right-of-way, two lanes of through traffic in each direction, turning lanes, bicycle lanes, tree lined sidewalks and intersection improvements at the Danforth Avenue / Kingston Road interchange and at Midland Avenue / Kingston Road.



2.4 Sub-Areas

Information gathered at the design charrette, Local Advisory Committee meetings and meetings with stakeholders indicated that improvements could be made to the corridor's organization to help invigorate revitalization. Three Sub-Areas were established including the West Residential Mixed use Area, Mixed use Core and East Residential Area. These Sub-Areas will help develop the implementation tools required to guide change. The recommended land uses and precinct organization are described in the following sections.

The focus of the **West Residential Mixed use Sub-Area** should be the new Danforth Avenue / Kingston Road intersection where transit, active transportation, public parks, community facilities and new residential and mixed use development create a gateway feature into the Cliffsides community. The West Residential Mixed use Area should primarily be a residential area with a mix of uses permitted on the ground floor. The 'Apex Mixed use Area' is a large triangular site that will be created as a result of the reconfiguration of the Danforth Avenue / Kingston Road grade separation. The 'Apex' site could be developed with a mix of uses including residential, retail, office, community and cultural uses and destination / entertainment commercial development. To the north of Danforth Avenue is a large continuous open space area between Kingston Road and Danforth Avenue that could be redesigned as a community park. The reconfiguration of the road in this area will require that the Scarborough War Memorial be relocated.

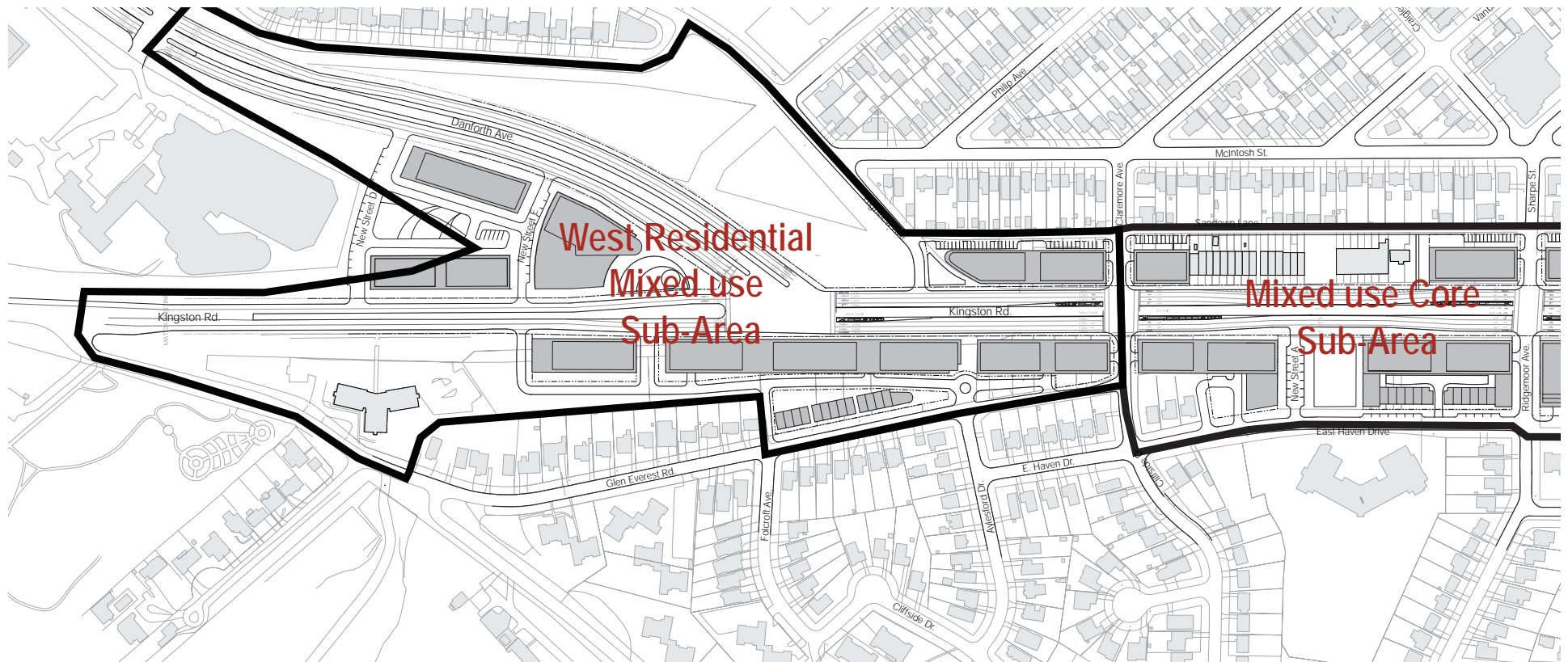
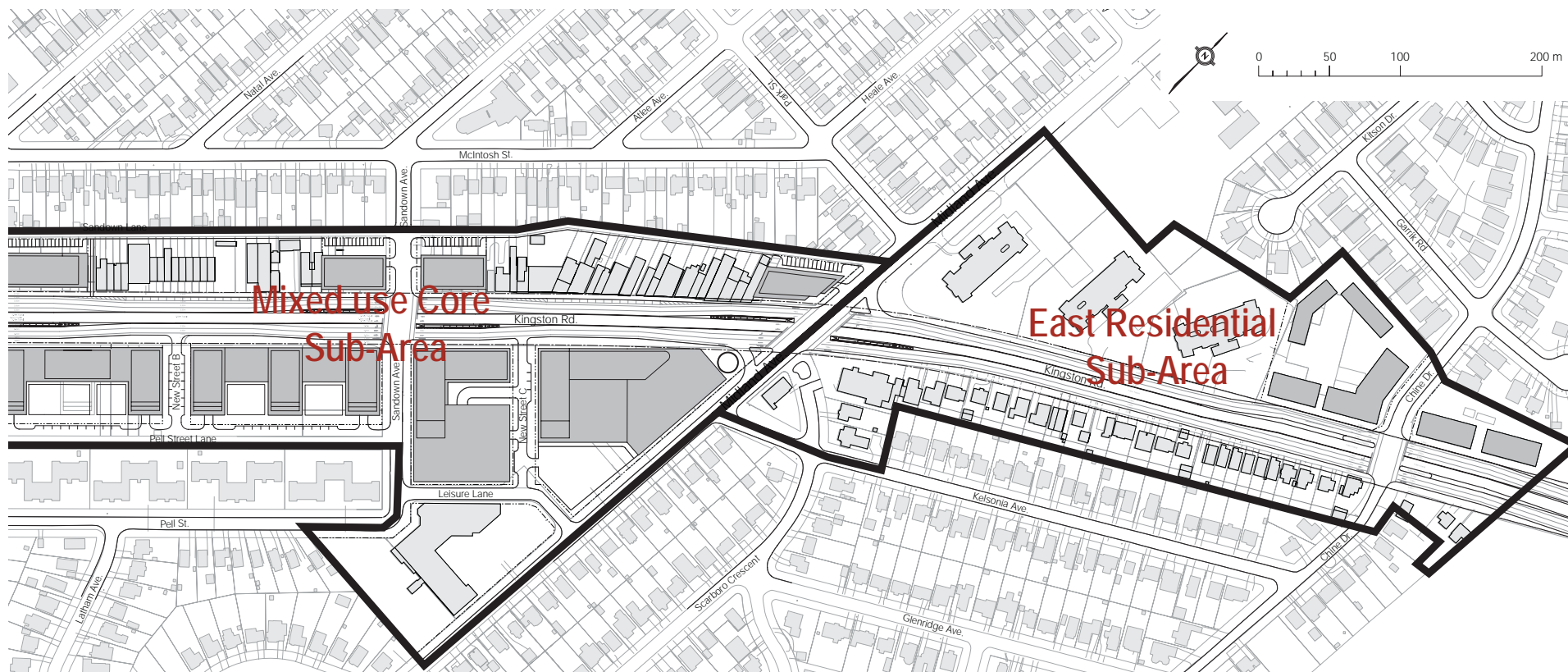


Figure 2 - Sub-Area Plan

The **Mixed use Core Sub-Area** should reinforce the unique village character of Cliffside Village and should be the focus for retail, commercial, cultural and institutional uses. New development in this area should create ideal conditions for a successful main street, encouraging pedestrian movement and attracting businesses.

The **East Residential Sub-Area** is an established residential area that includes both high-rise apartments and single family homes. New development in this area should respect the character of these existing land uses.





West Residential Mixed use Sub-Area: Buildings should primarily be residential development.



Rendering of the 'Apex' site at the corner of Danforth Road and Kingston Road.

West Residential Mixed use Sub-Area

The West Residential Mixed use Sub-Area includes properties along Kingston Road between Variety Village and Claremore Avenue, including the green space north of Kingston Road and Danforth Avenue to Pinegrove Avenue/Highview Avenue. Major improvements proposed to improve transit at the Danforth Avenue / Kingston Road interchange will have a large impact on the character of the area. The release of green spaces constrained by the current roadway configuration will provide opportunities for redevelopment. The Urban Design Concept Plan proposes that the property to the north become a new open space and that the 'Apex' site be considered as a redevelopment parcel.

As new development occurs in this area, the following should apply:

- Residential uses should be permitted on the ground floor, excluding townhouses.
- Retail / commercial / institutional uses should be permitted on the ground floor but are not required.
- Community uses such as daycares, community centres and fitness centres should be permitted.
- The building base should address Kingston Road and Danforth Avenue and should include features to improve the public environment along the street.
- The 'Apex' site should be encouraged to accommodate a range of mid-rise buildings that include office, residential, hotel, entertainment, institutional, and community uses.
- A continuous park area north of Danforth Avenue will serve the community.
- A public plaza, which might include the relocated Scarborough War Memorial, at the east end of the 'Apex' site should serve as a gateway and provide amenities such as a water feature, outdoor seating, terraces, tree planting and public art.
- Individual unit access to Kingston Road may be permitted.
- The relocated Scarborough War Memorial should include a public gathering space / parking.

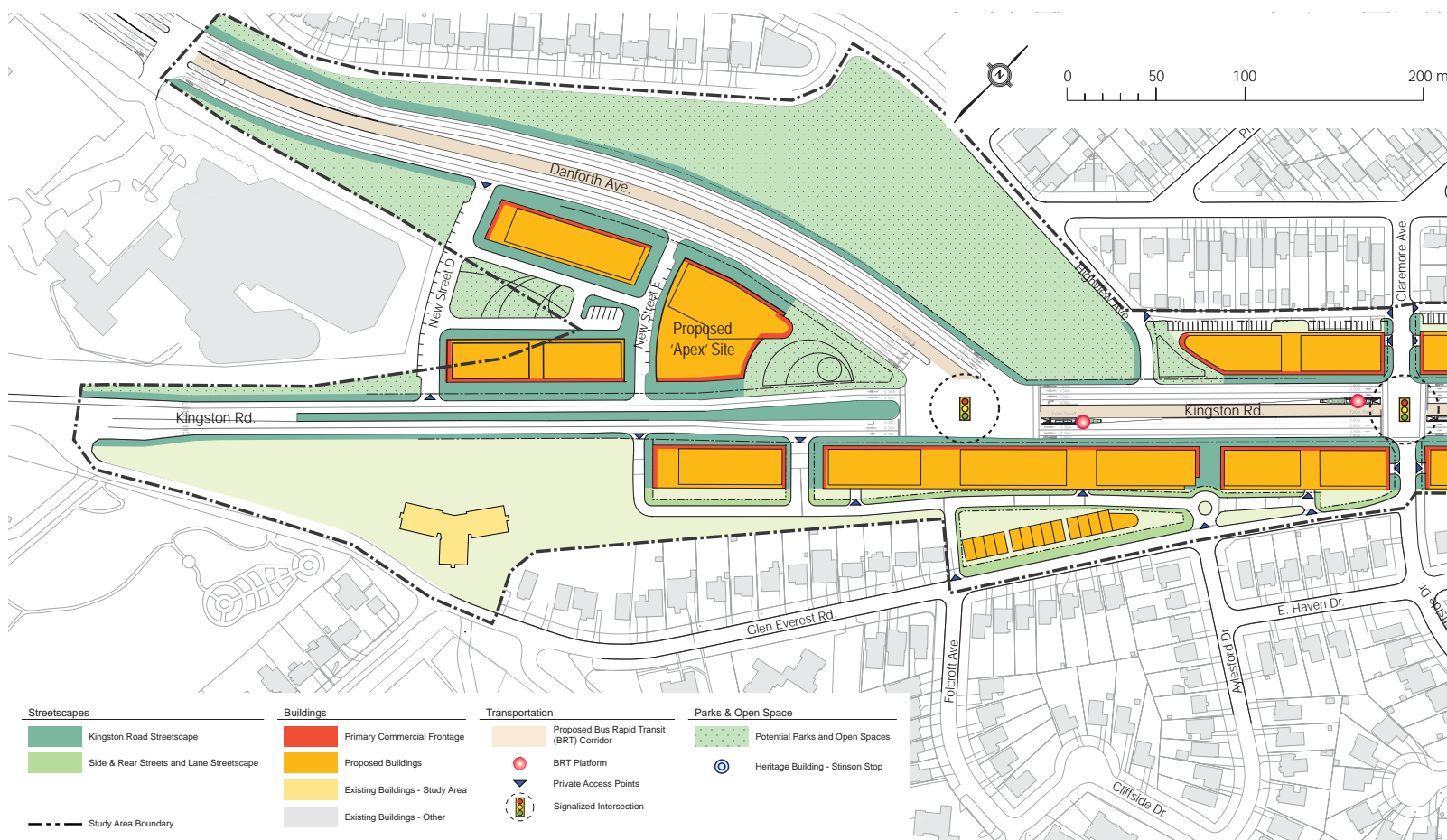


Figure 3 - West Residential Mixed use Sub-Area Enlargement



The Mixed use Core should encourage streetscape development that provides opportunities for patios, cafes and gathering spaces.



In the Mixed use Core, residential uses should be permitted starting on the second floor where buildings front onto Kingston Road.



Residential uses at the rear of large sites in the Mixed use Core should be permitted on the ground floor where the property abuts a rear lane of a large site



New development in the Mixed use Core should provide active uses on the ground floor in the evenings.

Mixed use Core Sub-Area

The Mixed use Core Sub-Area includes properties along Kingston Road between Claremore Avenue and Midland Avenue. Development should focus on creating a vibrant main street condition complete with walkable blocks, wide sidewalks, street trees and landscaping, pedestrian and cycling amenities and access to parking.

As new development occurs, the following should apply:

- A mix of mid-rise buildings should be encouraged including office and residential buildings with retail at grade, hotels, community uses, entertainment and institutional uses.
- The building base should address Kingston Road and include features to improve the public environment along the street, including patios, seating areas, canopies and awnings, transparent ground floor treatment, pedestrian scale articulation and detailing.
- Adjacent to Kingston Road, retail / commercial / institutional uses are required on the ground floor and should include establishments such as restaurants, retail stores, cafes and coffee shops, grocery stores, pharmacies, offices, daycares and galleries.
- Places of worship are prohibited on the ground floor of buildings in the Mixed Use Core.
- On the north side of the street, the majority of the ground floor should be retail and commercial uses. On the south side, the first 20 metres (min) of the ground floor area facing Kingston Road should be retail and commercial uses. Residential at grade should be permitted beyond the first 20 metres, on side and rear streets.
- Properties on the south side of Kingston Road that abut existing laneways (Pell Street Lane and Leisure Lane - which are proposed to be converted to public streets) should permit residential uses on the ground floor adjacent to the laneways / streets.
- Building and site design should incorporate appropriate transitions to existing residential areas.
- Driveways and entrances should be consolidated.
- New streets should be added to improve permeability of the blocks.
- Heritage properties are to be preserved.
- Opportunities for street trees will be pursued where possible.

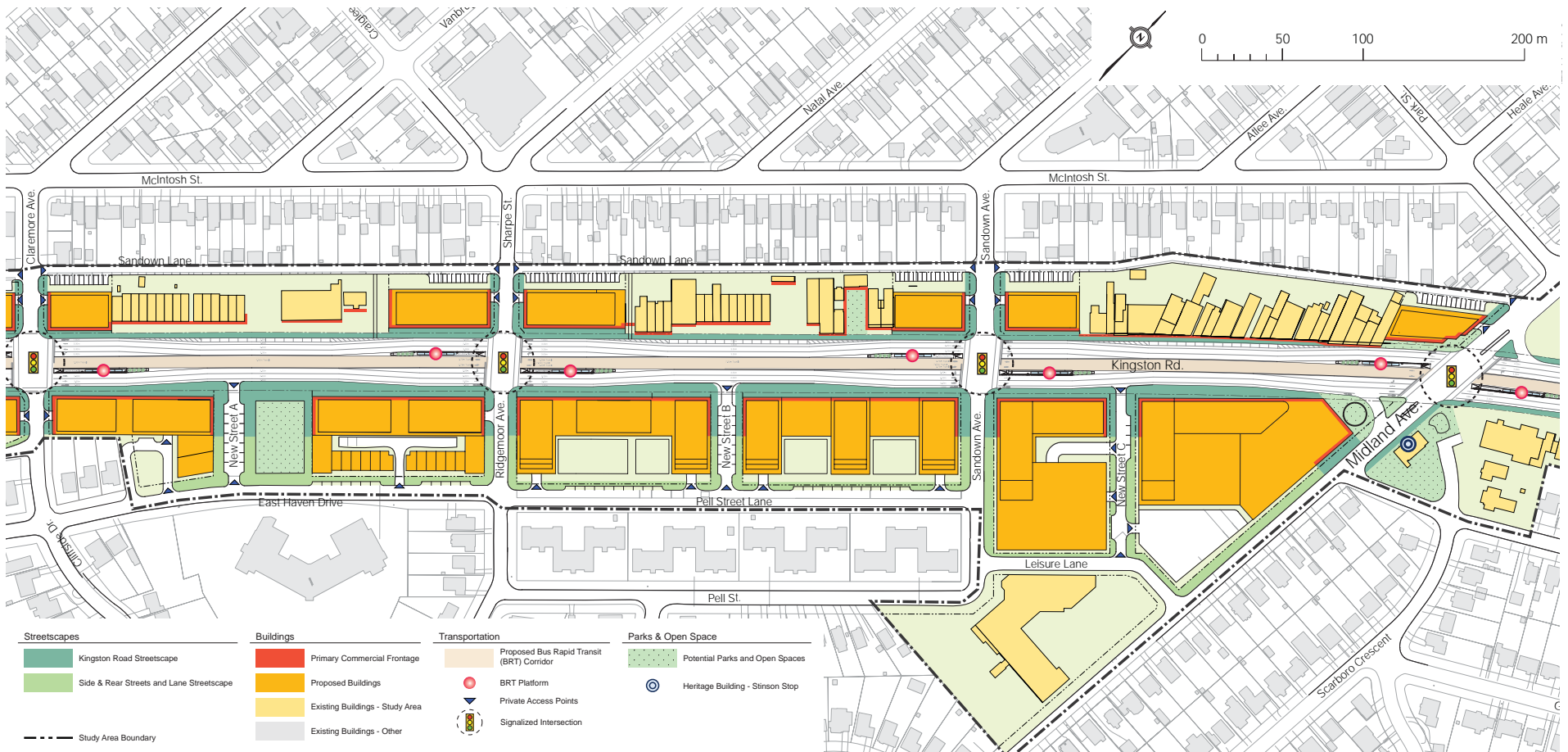


Figure 4 - Mixed use Core Sub-Area Enlargement



In the East Residential Sub-Area, infill within the apartment sites should be encouraged to relate well to the street.



In the East Residential Sub-Area, existing residential uses should be preserved on the south side. Long term redevelopment could include grade related low rise built form.

East Residential Sub-Area

The East Residential Sub-Area includes properties along Kingston Road between Midland Avenue and Chine Drive. Limited development is expected in this area as a majority of the south side of Kingston Road is designated Neighbourhoods and is not intended for intensification. The north side of Kingston Road is comprised of existing high-rise development and is designated Apartment Neighbourhood and may be revitalized in accordance with the City's Tower Renewal initiative. Properties located on the north side of Kingston Road at Chine Drive are designated as a Mixed use Area and could be redeveloped for primarily residential uses. Commercial uses at grade should be permitted but not required.

Limited development is expected in this area. However; as new development occurs, the following should apply:

- Residential uses on the ground floor should have individual unit access where appropriate.
- Privacy measures should be explored for properties fronting onto Kingston Road.
- Building base should address Kingston Road and include features to improve the public environment.
- Building and site design should incorporate appropriate transitions to existing residential areas.
- Driveways and entrances should be consolidated.
- Infill within the apartment sites (as part of the City's Tower Renewal program) should be encouraged closer to the street edge.

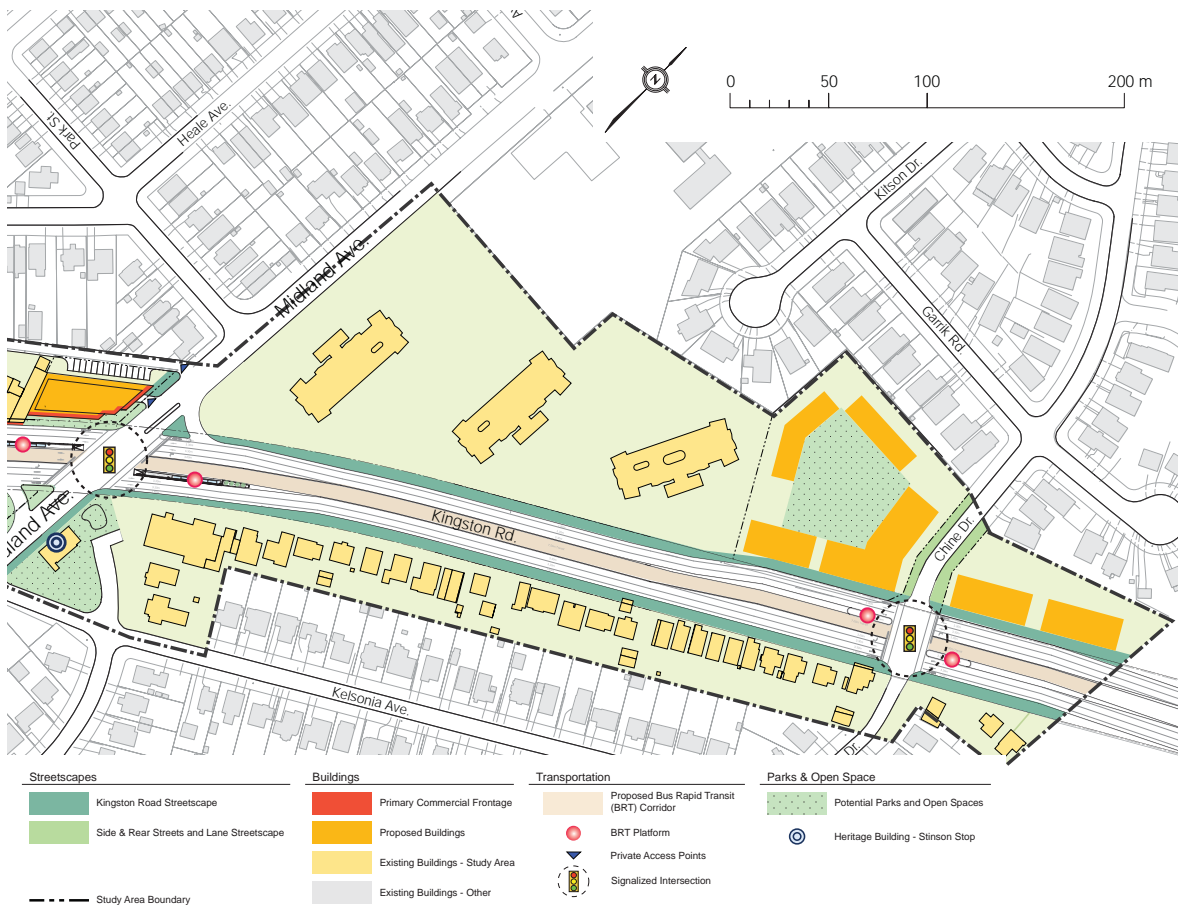


Figure 5 - East Residential Sub-Area Enlargement



The south side (left of the image) of Kingston Road provides different opportunities for development than the fine grain fabric of the north side. Recommendations for built form on the north and south sides are separated to address each side's specific opportunities and constraints.

2.5 Potential Built Form

Refer also to Section 2.7 Policy Recommendations for Official Plan and Zoning Recommendations.

Refer also to Section 4.0 Built Form of the Cliffside Village Urban Design Guidelines.

This study proposes 'building envelopes' that embody the development principles that have evolved over the course of the study. This is determined by site specific factors including site orientation as it relates to sunlight and shadow casting, adjacent land uses, existing built form, views to the lake, and the adjacent street and its scale, width and purpose. The built form recommendations proposed in this section represent the maximum build out of the 'building envelope' and does not necessarily represent a building design.

Cliffside Village has a different context and potential for built form on each side of the street, this study addresses each separately. However, as Kingston Road is redeveloped, the street will function as one.



Figure 6 - Building Height to ROW Ratio

Although the Kingston Road right-of-way width indicates that a maximum building height of 36 metre would be appropriate, the study recognizes that a 36 metre building height may not be appropriate in all areas.

2.5.1 North Side Development Principles (between Danforth Avenue and Midland Avenue)

Existing built form on the north side of Kingston Road establishes the village character of Cliffside Village. New built form should respect the existing mass, scale, rhythm and patterns while providing well designed and appropriate mid-rise infill buildings.

The north side of Kingston Road is generally comprised of many existing one and two storey buildings on narrow lots. The result is a traditional main street 'fine-grain' building fabric. The typical lot depth is in the range of 37 metres and is adjacent to a rear lane (Sandown Lane) that is located in a 4.5 metre wide right-of-way. It is likely that redevelopment would happen over a long period of time due to the fractured land ownership. The Urban Design Concept Plan illustrates a scenario wherein approximately one third of the north side is redeveloped. New development should fit well with the surrounding buildings providing adequate transition in scale and massing that complements the village character. This is particularly important where blank facades on new upper storeys are visible against an existing low-rise building.

North Side Building Height

The study has determined that new built form height on the north side of Kingston Road between Danforth Avenue and Midland Avenue be limited to a maximum 26.0 metres or 8 storeys. A 9th level could be constructed to accommodate mechanical equipment, however, no portion of the structure should be permitted to penetrate the visual angular plane.

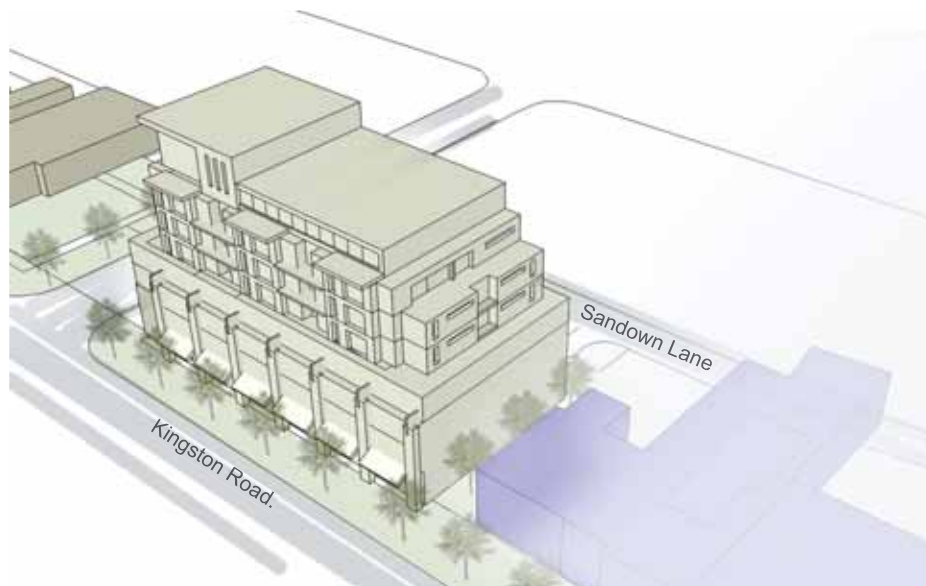
The minimum height for new development throughout the north side should be 11.0 metres.

North Side Building Shape

Building edges that face Kingston Road should have a 3-4 storey building base adjacent to the sidewalk, and should step back a minimum of 1.5 metre at the 4th or 5th floor. Refer to figure 8. To mitigate the impacts of large blank side walls, it is recommended that the blank side wall may be permitted up to a height of 6 storeys. A 5.5 metre side wall step-back should be required at the 7th floor. On narrow properties (20 metres in width or less) the 5.5 metre step-back may not be required. Refer to Figure 9.



The existing built form on the north side of Kingston Road establishes the character of Cliffside Village. New development should complement the existing built form while providing well designed and appropriate mid-rise buildings.



Potential built form on the north side of Kingston Road.

The north side of the buildings, or the building edge that faces the rear of the property, should be designed to minimize shadow and privacy impacts on the residential properties along McIntosh Street. A 45 degree visual angular plane applied to the rear of the properties that are adjacent to low-rise residential areas should ensure that the building is constructed within an appropriate envelope. Refer to Figure 7.

North Side Building Setbacks

Front Yard

A minimum 2.0 metre setback is recommended for all buildings adjacent to Kingston Road and side streets to provide additional space for public use, street tree planting and landscaping so that a minimum curb to building wall dimension is no less than 4.8 metres in width at both mid-block and intersection conditions. Refer to Figure 10.

Side Yard

A 0 metre setback for a depth of 20 metres is recommended for new buildings to encourage the construction of a continuous streetwall along Kingston Road.

Where a mid-block pedestrian connection is desired along a property line, a minimum 4.5 metre setback is recommended to accommodate a walkway and landscaping. Where a mid-block connection with vehicle access is desired along a property line, a minimum 7.5 metre setback is recommended to accommodate a 4.5 metre wide, one-way travel lane and a 3.0 metre sidewalk. It is intended that the mid-block connection with vehicle access would be perceived as a pedestrian priority place.

Rear Yard

A 9.0 metre setback from the south Sandown Lane property line is recommended to accommodate the 1.5 metre widening of the Lane and a 6.0 metre wide no-build zone where parking, servicing and landscaping could be accommodated.

Building Ground Floor

It is recommended that the ground floor of buildings on the north side of Kingston Road be entirely non-residential, with the exception of those properties east of Midland Avenue. The minimum floor to floor height recommended for ground floors with commercial/retail uses is 4.5 metres. Refer to Figure 11.

Ground floor design should incorporate recessed entries, storefront display windows, an area for signage and should reference the adjacent building facade rhythm. The ground floor should be a minimum of 4.5 metres (floor to floor) for adequate space

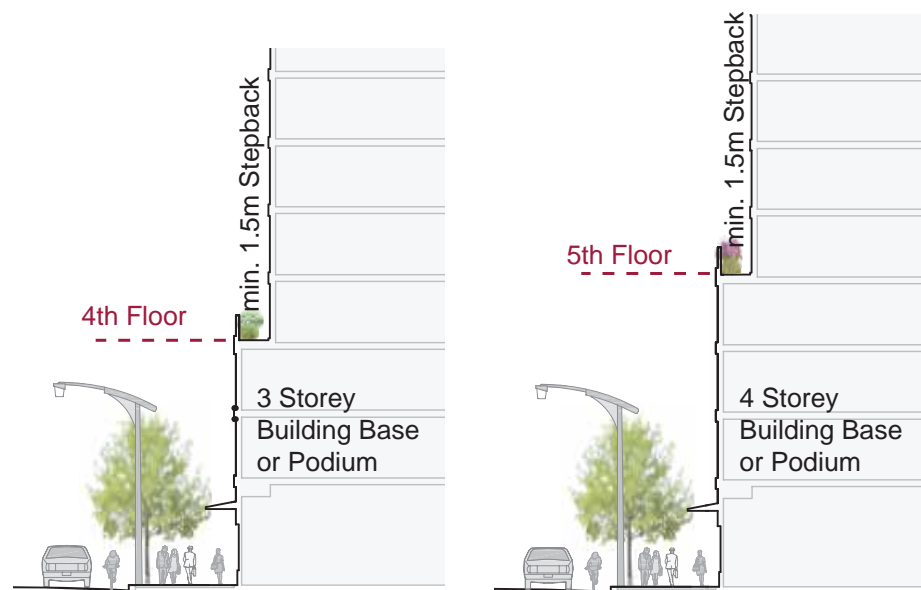


Figure 8 - Building Base Height

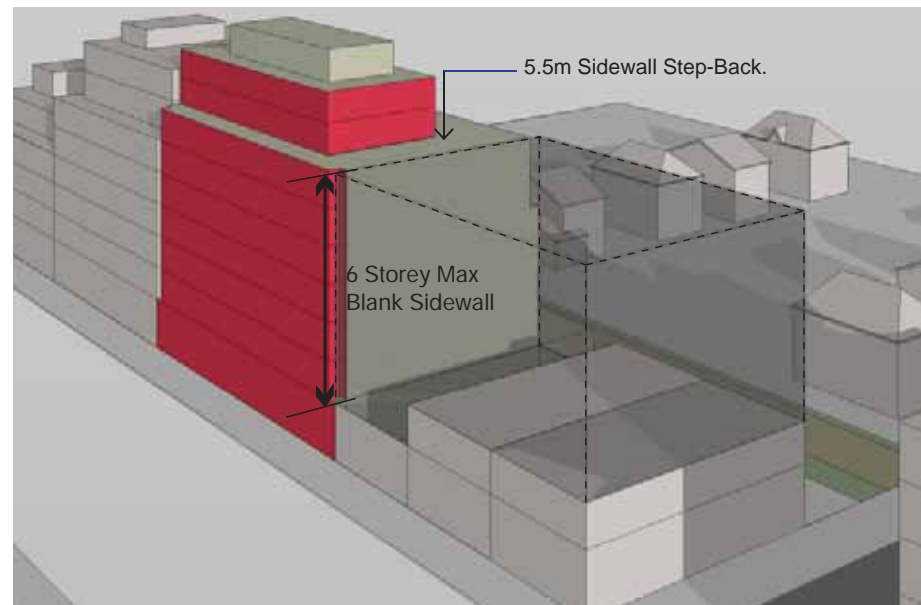


Figure 9 - Sidewalls and Side Step-Backs

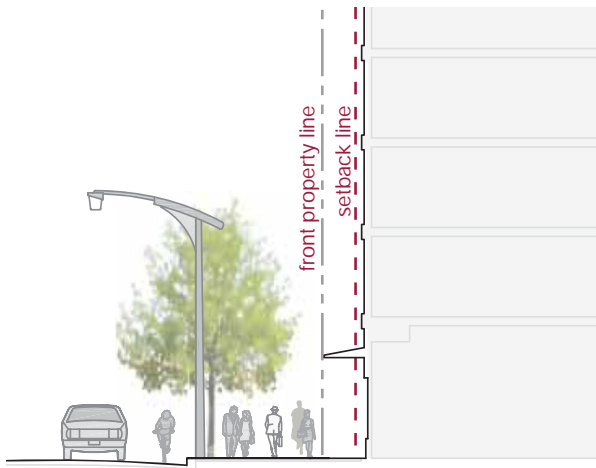


Figure 10 - Building Front Yard Setback

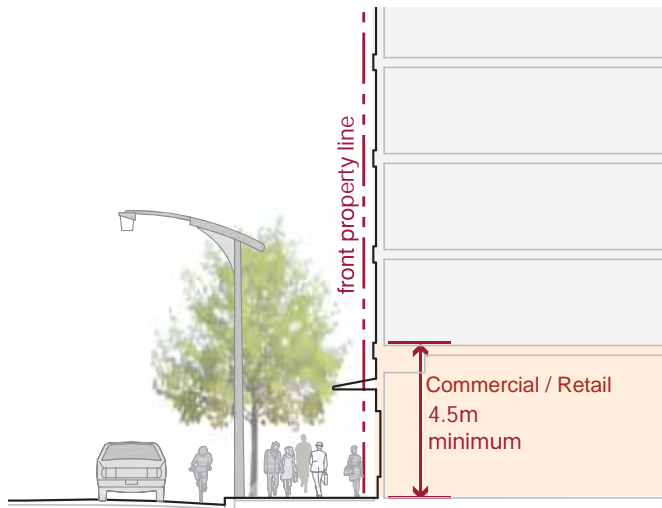


Figure 11 - Minimum Floor to Floor Height

to incorporate windows above the entry doors and a sign band strip. These horizontal elements are important aspects of the storefront facade and should form a continuous strip, interrupted only at the vertical pilasters or columns (part of the vertical banding of the building overall).

Façade Articulation

Facade articulation is a basic aspect of the pedestrian experience along a street. Utilization of facade articulation elements within new development will promote the Cliffside Village's character through the rhythm of recesses, openings, projections and horizontal and vertical demarcations. A minimum of 60% of the ground floor level should be glazed.

Currently, building widths are generally narrow and form the basis of the main street village scale. In order to maintain the village scale of the streetscape, the perceived building width of new buildings should remain narrow. New buildings with long street frontages should use vertical elements and window patterns and frequent entrances to maintain the appearance of narrow storefronts.

Vertical Demarcation

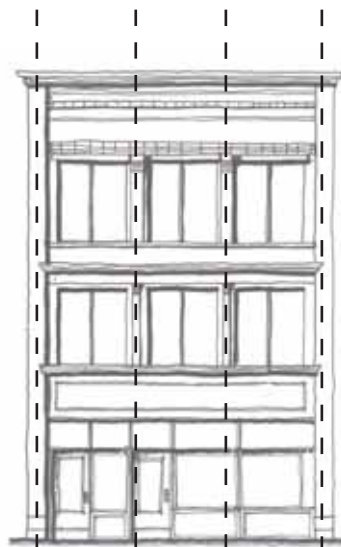
Buildings will be typically divided vertically into a symmetrical arrangement of facade units referred to as bays. In order to maintain the characteristic proportions and bay widths of the existing buildings, new building facades should have symmetrically arranged, narrow bays. The vertical divisions between bays may be demarcated using construction elements such as masonry coursing, changes in materials and colour, projecting piers, pilasters or columns.

Horizontal Demarcation

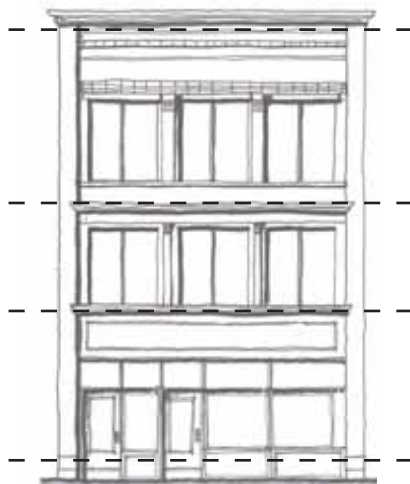
Buildings will be typically divided horizontally into storeys with retail/commercial uses generally found in the ground floor, and upper storeys containing a mix of office and residential uses. Each storey may be horizontally demarcated on the building facade through the use of projecting mouldings, intermediate cornices, changes in material, and colour and masonry coursing.

Transparency

It is recommended that a minimum of 60% of the ground floor facade be glazed to provide transparency and animation adjacent to the sidewalks fronting onto Kingston Road.

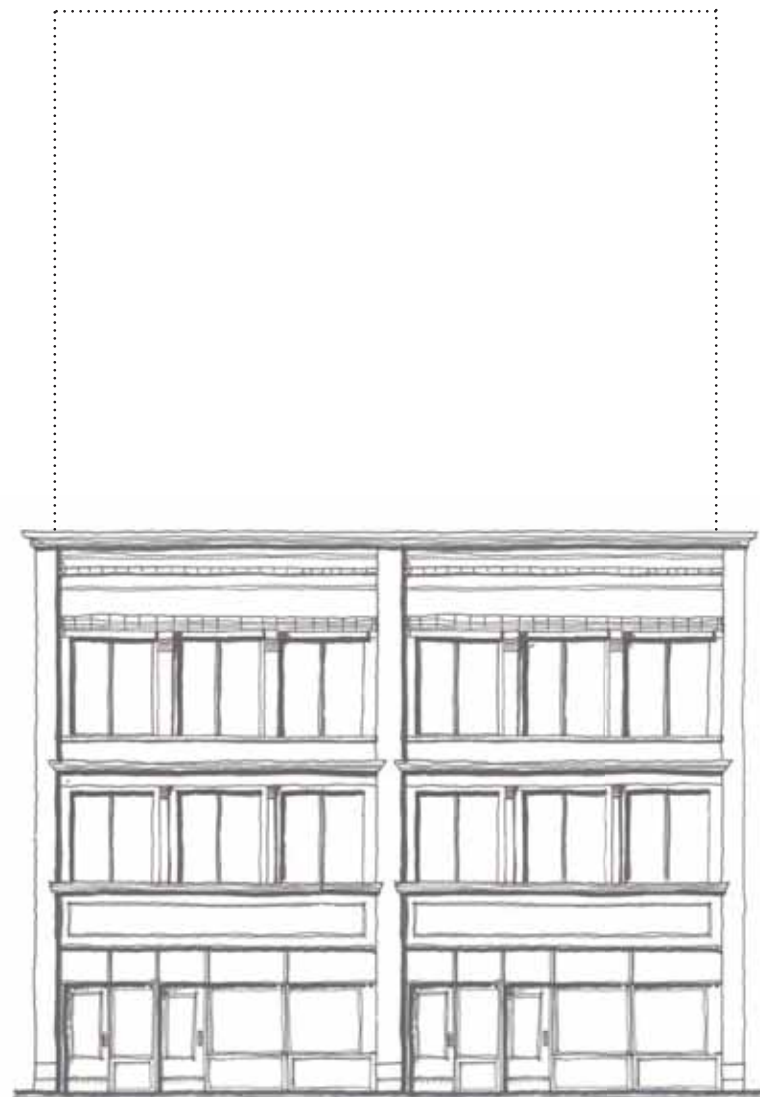


Vertical Demarcation of Primary Building Facades



Horizontal Demarcation of Primary Building Facades

Figure 12- Building Facade Articulation



Upper Floors:

- May utilize modern materials
- Should be architecturally distinguished from the podium
- Height restricted as recommended

Podium or Building Base:

- Articulated at 3 or 4 storeys
- Incorporate pedestrian scale patterns
- Utilize traditional building materials
- Contain typical village main street facade elements

New Building Composition Diagram

The first 3 or 4 storeys of a new building (podium) should be articulated using typical village main street patterns and appropriate materials. The upper floors should be distinguished from the podium by the use of step-backs or change in material. The purpose is to maintain the perception that the streets are composed of 3 to 4 storey buildings, without excluding the possibility of taller structures.

Figure 13 - New Building Composition



Lot sizes and access to rear lanes or streets should make properties on the south side of Kingston Road attractive to developers.



Rendering of buildings on the south side of Kingston Road.

2.5.2 South Side Development Principles (West of Danforth Avenue to Midland Avenue)

The south side of Kingston Road will likely provide more opportunities for redevelopment in the short and medium term than properties on the north side. The deep and wide lot sizes range from approximately 50 to 100 metres in depth and are conducive to redevelopment as minimal consolidation may be required. Properties on the south side are easily accessed and are typically not constrained by other buildings or sensitive land use areas.

South Side Building Height

The study has determined that buildings on the south side of Kingston Road between Glen Everest Road and Midland Avenue should be limited to 11 storeys. A 12th level could be constructed to accommodate mechanical equipment, however; no portion of the structure should penetrate the visual angular plane.

The minimum height throughout the study area should be 11.0 metres.

South Side Building Shape

Building edges that face Kingston Road should have a 3-4 storey building base adjacent to the sidewalk and should step-back a minimum of 1.5 metres at the 4th or 5th floor. Refer to Figure 8. An additional 1.5 metre step-back should be required for buildings taller than 22.5 metres (7 storeys).

A 45 degree visual angular plane applied to the rear and sides of the property that are adjacent to low-rise residential areas should ensure that the building is constructed within an appropriate envelope and that shadow and privacy impacts are minimized on adjacent properties. Refer to Figure 14.

South Side Setbacks

Front Yard

A 3.0 metre setback is recommended for all buildings adjacent to Kingston Road and to provide additional space for public use, street tree planting and landscaping to accommodate a minimum curb to building wall dimension of 6.0 metres in width at mid-block and 4.8 metres at intersections. The same setback applies to both residential and retail / commercial uses at grade.

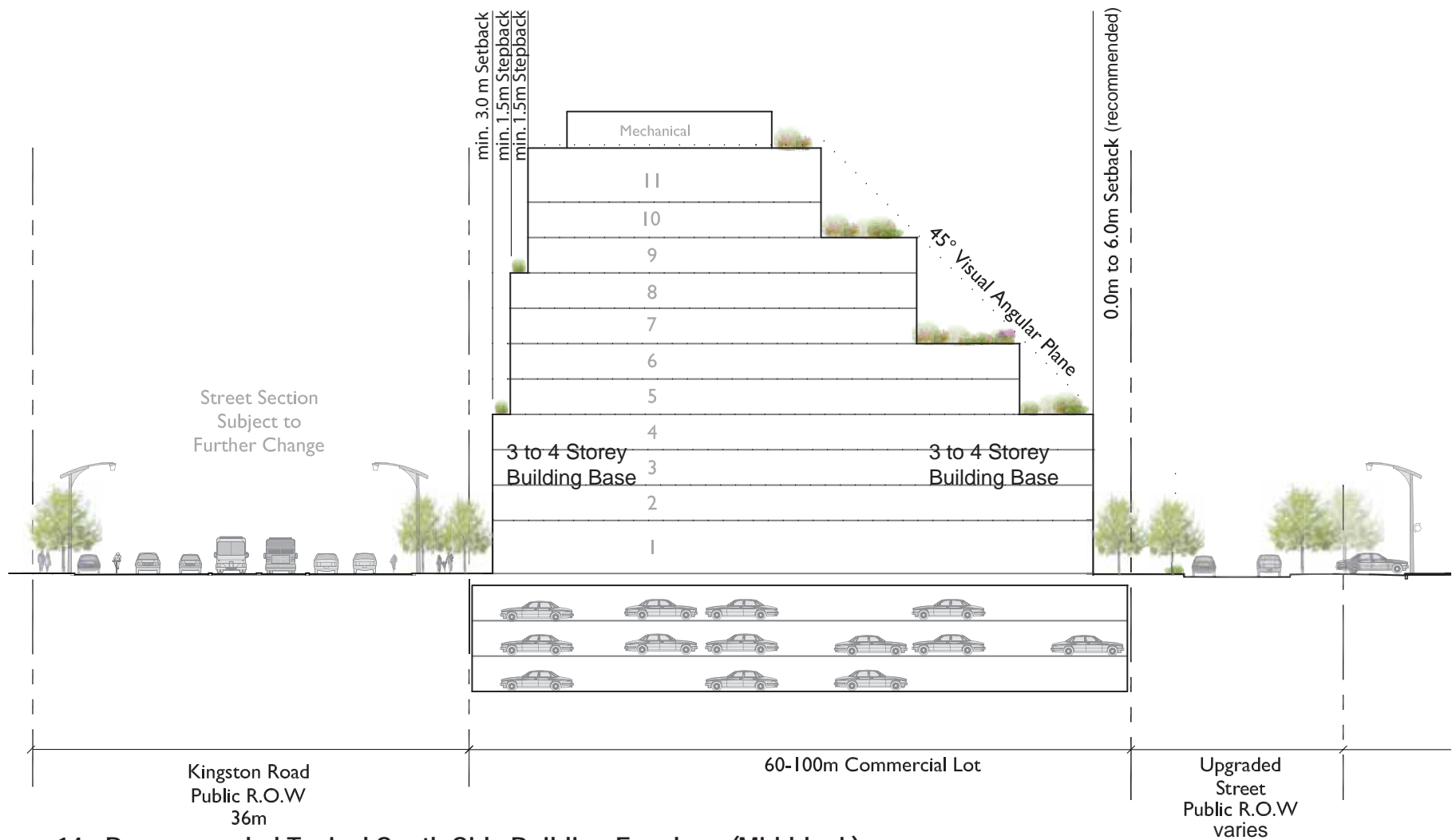


Figure 14 - Recommended Typical South Side Building Envelope (Mid-block)



Ground floors intended for commercial / retail uses should incorporate tall floor to ceiling heights, and include large store front display windows and recessed entries.

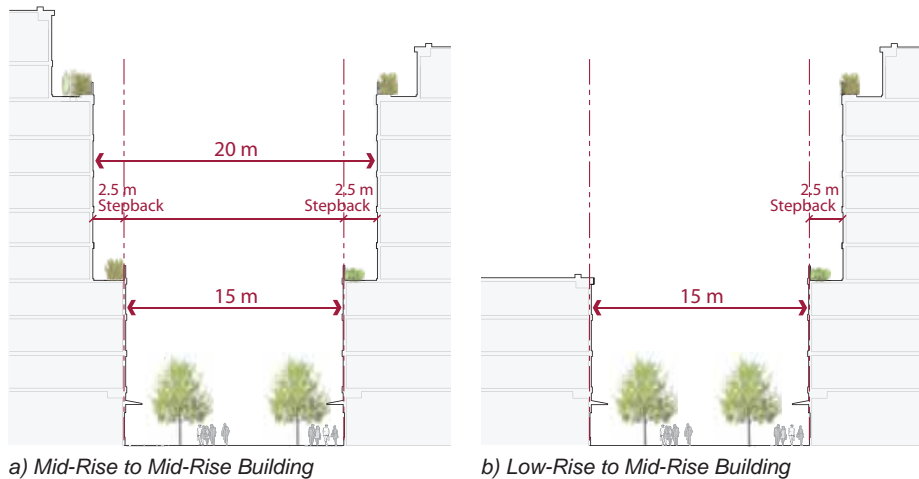


Figure 15 - Separation Distances
(Long Dimension Perpendicular to Street)

For new buildings facing side and rear streets, a setback of 0-3.0 metres for retail / commercial spaces is recommended. For residential uses at grade facing side or rear streets, a setback ranging from 3.0 to 6.0 metres is recommended.

Side Yard (Retail / commercial at grade)

A 0 metre setback is recommended for the first 20.0 metres of building depth to encourage a continuous streetwall along Kingston Road. Where a mid-block pedestrian connection is desired (generally recommended every 100 - 130 metres) a minimum setback of 4.5 metres is recommended to accommodate a walkway and landscaping. The inclusion of a mid-block connection will be dependant on the width of the property and/or the width of the proposed building.

Separation Distances

Where Long Dimensions are Perpendicular to the Length of the Street

Where the long dimension of a proposed building, or portion of a building is perpendicular to Kingston Road, the following separation distances are recommended:

- Where desirable, for buildings or portions of buildings up to 4 storeys, a 15 metre separation from adjacent buildings should be applied.
- Where desirable, for portions of buildings 5 storeys or more, a minimum 2.5 metre step-back should be applied. This step-back would ensure that a 20.0 metre separation would occur between portions of buildings that are 5 storeys in height or more. Refer to Figure 15.

Where Long Dimensions are Parallel to the Length of the Street

To promote a continuous street front along Kingston Road, breaks in the street wall are only recommended where mid-block connections are desired. Refer to "Mid-Block Connections", page 52. Generally, this would place breaks in the street front every 100 to 130 metres.

- Where mid-block connections are provided outside of the building envelope, a minimum of 4.5 metres (pedestrian access only) and 7.5 meters (pedestrian and vehicle access permitted) separation distance is recommended for the first 3 to 4 storeys or what is defined as the building base.
- A minimum separation distance of 11m between adjacent buildings is recommended for the portion of buildings that are above the building base (4 -5 storeys) to allow for the provision of windows and appropriate façade articulation. This may be implemented through a 5.5 metre step-back from the shared side property line.

- Where the mid-block connection is provided within the building envelope, no separation distances apply.

Building Ground Floor - Retail / Commercial Uses

The minimum floor to floor height recommended for ground floors with commercial/retail uses is 4.5 metres. Refer to Figure 11.

The ground floor design for infill and new development should incorporate recessed entries, large storefront display windows, an area for signage and should reference the adjacent building facade rhythm. The ground floor should be of a taller floor to ceiling height to allow for adequate space to incorporate a transom window above the entry doors and sign band strip between the transom and the intermediate cornice. These elements are important aspects of the storefront facade and should form a continuous strip, interrupted only at the vertical pilasters or columns (part of the vertical banding of the building overall).

Building Ground Floor - Residential Uses

Where residential uses are recommended on the ground floor, special design standards should be applied to ensure that:

- there is a suitable transition from the public sidewalk to private residential units;
- that landscaping and other design features are used to augment this transition zone, and;
- ground floor residential uses can transition to commercial uses in the future.

Where residential at grade faces Kingston Road, building setbacks and floor to floor ground floor heights should be set to accommodate the potential conversion of the unit to retail / commercial space. A 3.0 metre setback is recommended where a grade separation occurs between the sidewalk and the finished floor of the unit. A minimum 3.6 metre floor to floor height and a 0.9 - 1.2 metre grade separation is recommended. Refer to Figure 17 - A. Where the ground floor unit is level with the sidewalk, a minimum 4.5 metre setback and 4.5 metre floor to floor height is recommended. Refer to Figure 17 - B.

Where residential at grade faces a rear or side street, a setback of 6.0 metres, a floor to floor height of 3.6 metres and a grade separation of 0.6 - 0.9 metres is recommended. Refer to Figure 16.

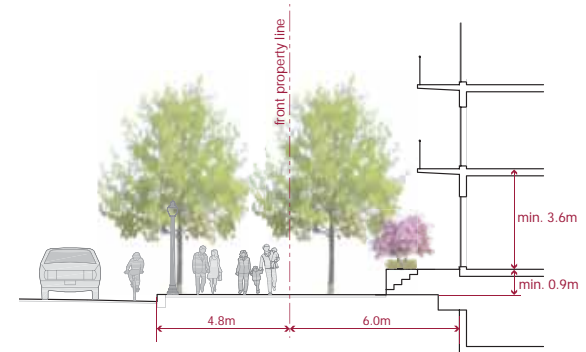
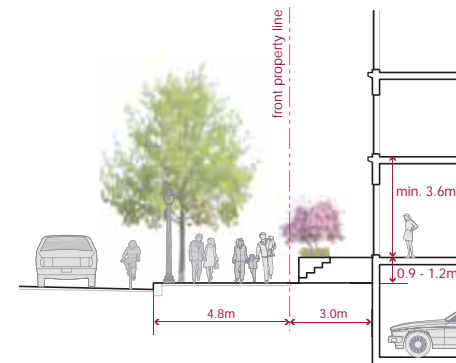
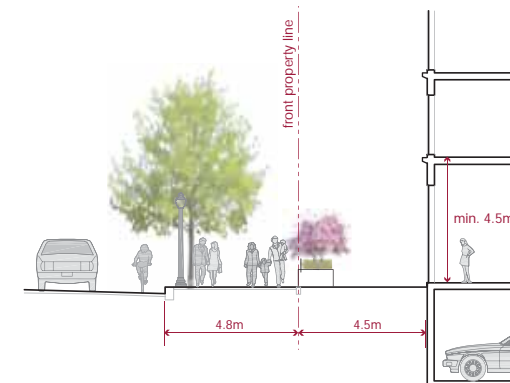


Figure 16 - Residential Facing Rear Streets



a) Grade Separated Finished Floor Level



b) Finished Floor Level With Sidewalk

Figure 17 - Residential Facing Kingston Road



New streets have been proposed between Kingston Road and rear streets and lanes to create a shorter pattern for development. New streets should include parallel parking, wide sidewalks and street tree planting.

Facade Articulation

Refer to Section 2.5.1 North Side Development Principles as the same facade articulation recommendations should apply.

2.6 Proposed Streets and Lanes

The existing street pattern in Cliffside Village presents an excellent framework for redevelopment. The network of streets create a predictable block structure complete with rear lane access. Generally the north-south block dimensions are walkable. However, the east-west blocks are too long to encourage pedestrian movement.

New mid-block streets and lanes are proposed to improve vehicle and pedestrian permeability and to provide opportunities for potential retail/commercial activities at corners. Refer to Figure 18. The shorter blocks should provide an improved pedestrian environment. Locations for new streets and lanes should to be coordinated with property ownership to facilitate consolidation or acquisition of the lands for access.

New Streets should:

- Provide parallel parking on both sides of the street;
- Be developed in conjunction with pedestrian facilities such as wide sidewalks; seating, lighting, landscaping and orientation signage;
- Be designed with right-in / right-out access at Kingston Road; and
- Be public.

New Lanes should:

- Provide pedestrian facilities as described above;
- Provide one-way vehicle access to the rear lane where warranted. Vehicle access would be warranted where the mid-block connection is located near the middle of a block but should be limited to one per block;
- Be designed as a right-in only from Kingston Road. Exiting would occur at an existing side street at a signal; and
- Become City property or remain privately owned and used through access agreements or easements.

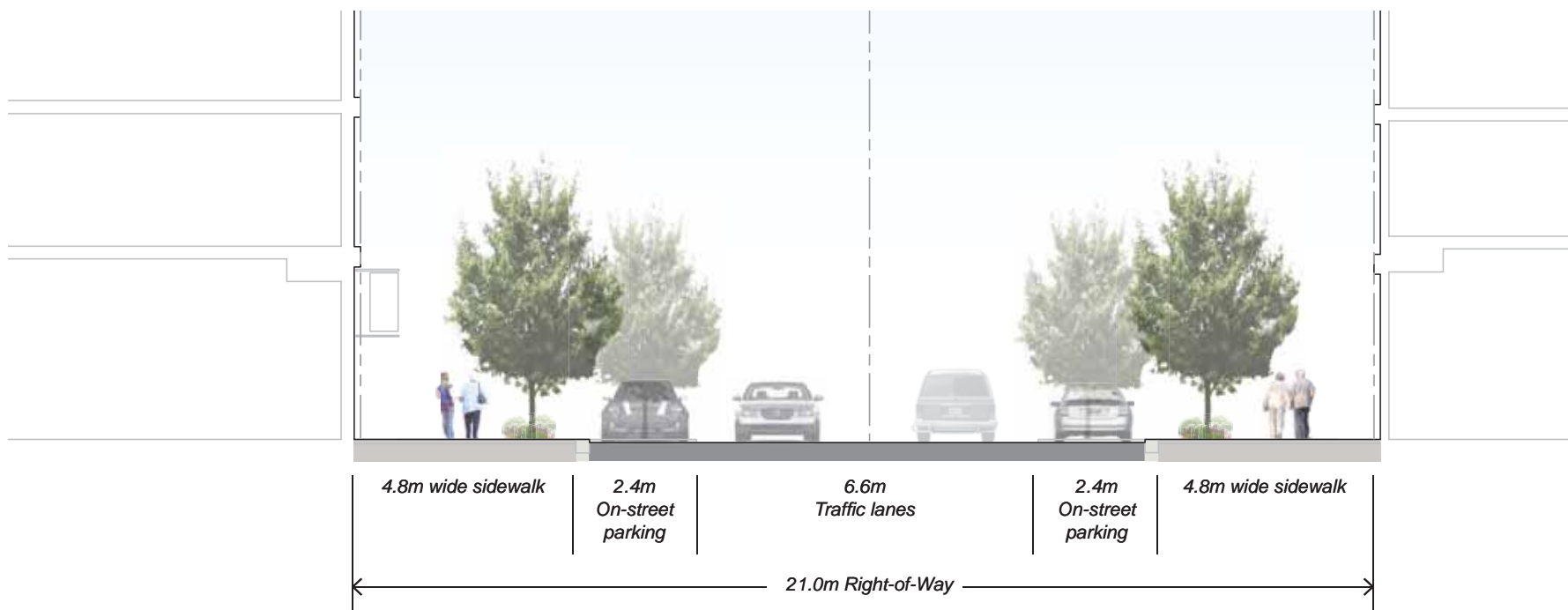


Figure 18 - New Street Typical Section

Pell Street and Leisure Lanes (South Side of Kingston Road)
Pell Street Lane and Leisure Lane should be upgraded to City streets and be constructed to meet City standards. The recommended new width is 13.3 metres and 14.8 metres respectively and should include two traffic lanes, one in each direction, parallel parking on the north side and a 5.3 metre wide sidewalks on the north side. The 5.3 metre wide sidewalks have been proportioned to potentially accommodate future underground servicing. An additional 3.2 metres would be required and should be acquired from the south side of the lane. Refer to Figures 19 and 20.

Sandown Lane (North Side of Kingston Road)
Sandown Lane should be widened to accommodate two lanes of vehicle traffic with the purpose of providing access to potential rear lot parking. The current 4.5 metre wide public right-of-way would be widened to 6.0 metre, which would require approximately 1.5 metres of private property on the south side of the lane.

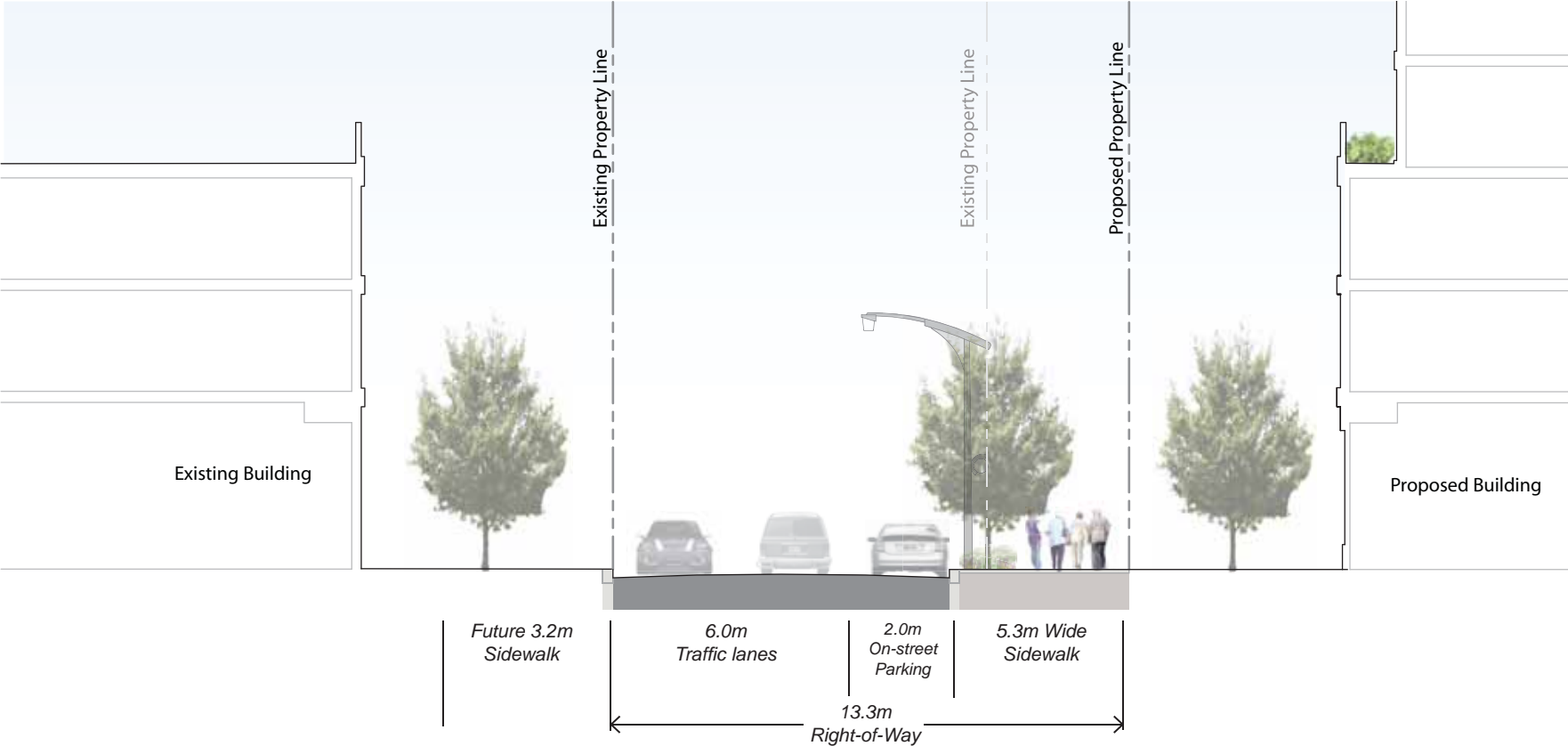


Figure 19 - Proposed Upgraded Pell Street Lane Typical Section



Figure 20 - Proposed Upgraded Leisure Lane

New Mid-Block Connections / Lanes, North Side of Kingston Road

A number of mid-block connections have been proposed on the north side of Kingston Road to improve access to Sandown Lane and to improve permeability of the long blocks. Two concepts are proposed for the new mid-block connections; a pedestrian only connection and a shared pedestrian and vehicle access connection. The pedestrian only mid-block connection should be a minimum of 4.5 meters in width to allow for a 2.1 metre minimum clearway and a 2.4 metre landscape and furnishing

zone. Refer to Figure 21. The shared pedestrian / vehicle use mid-block connection should be a minimum of 7.5 metres in width to permit a 6.0 metre wide clear access lane and a minimum 1.5 metre wide pedestrian clearway. The mid-block connections should be pedestrian priority spaces whereby movement of vehicles is secondary. It is intended that vehicle access to the mid-block lanes would be right-in only from Kingston Road and accommodate one-way movement to the north. Refer to Figure 22.

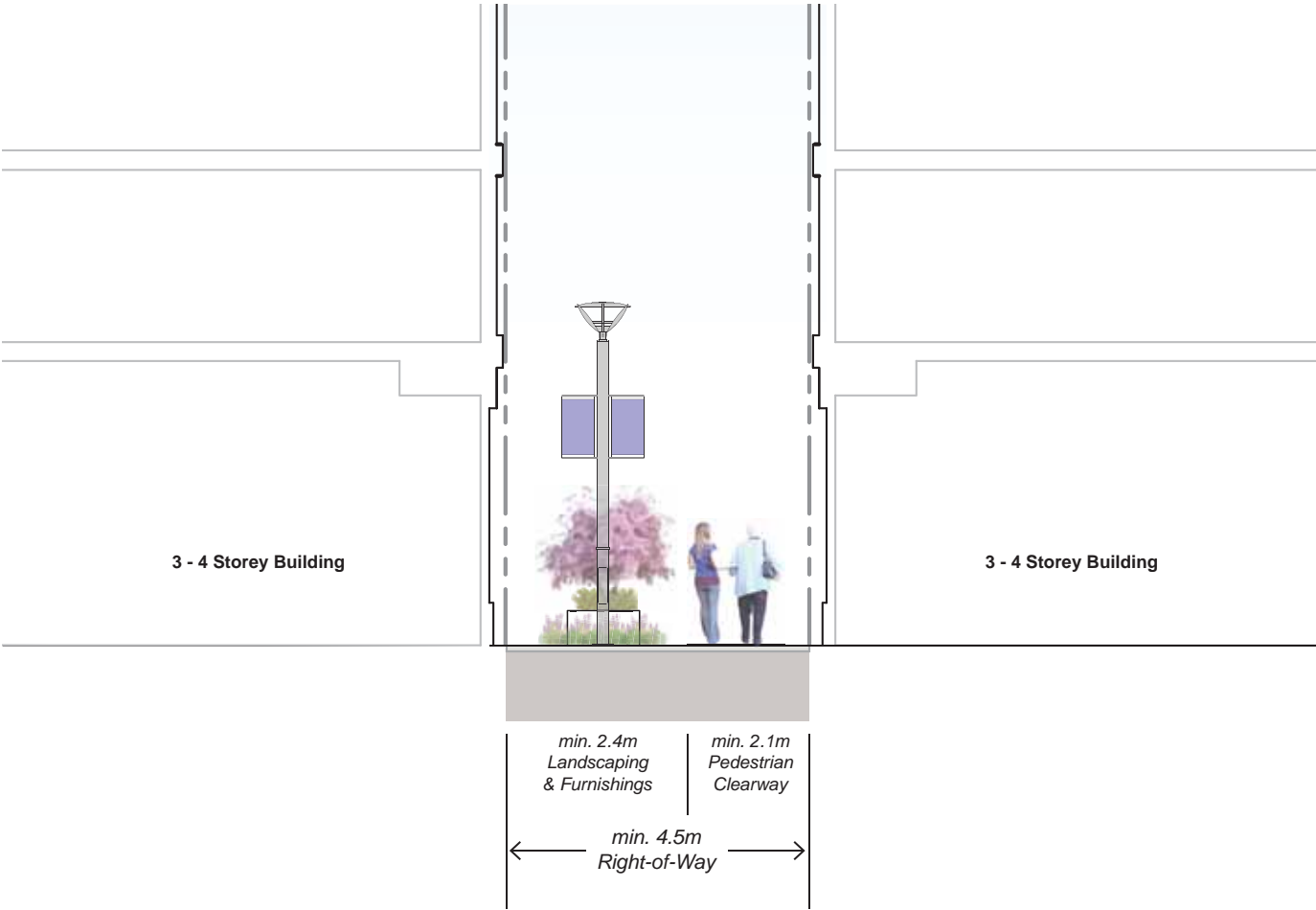


Figure 21 - Pedestrian Access Only (Mid-Block Connection)

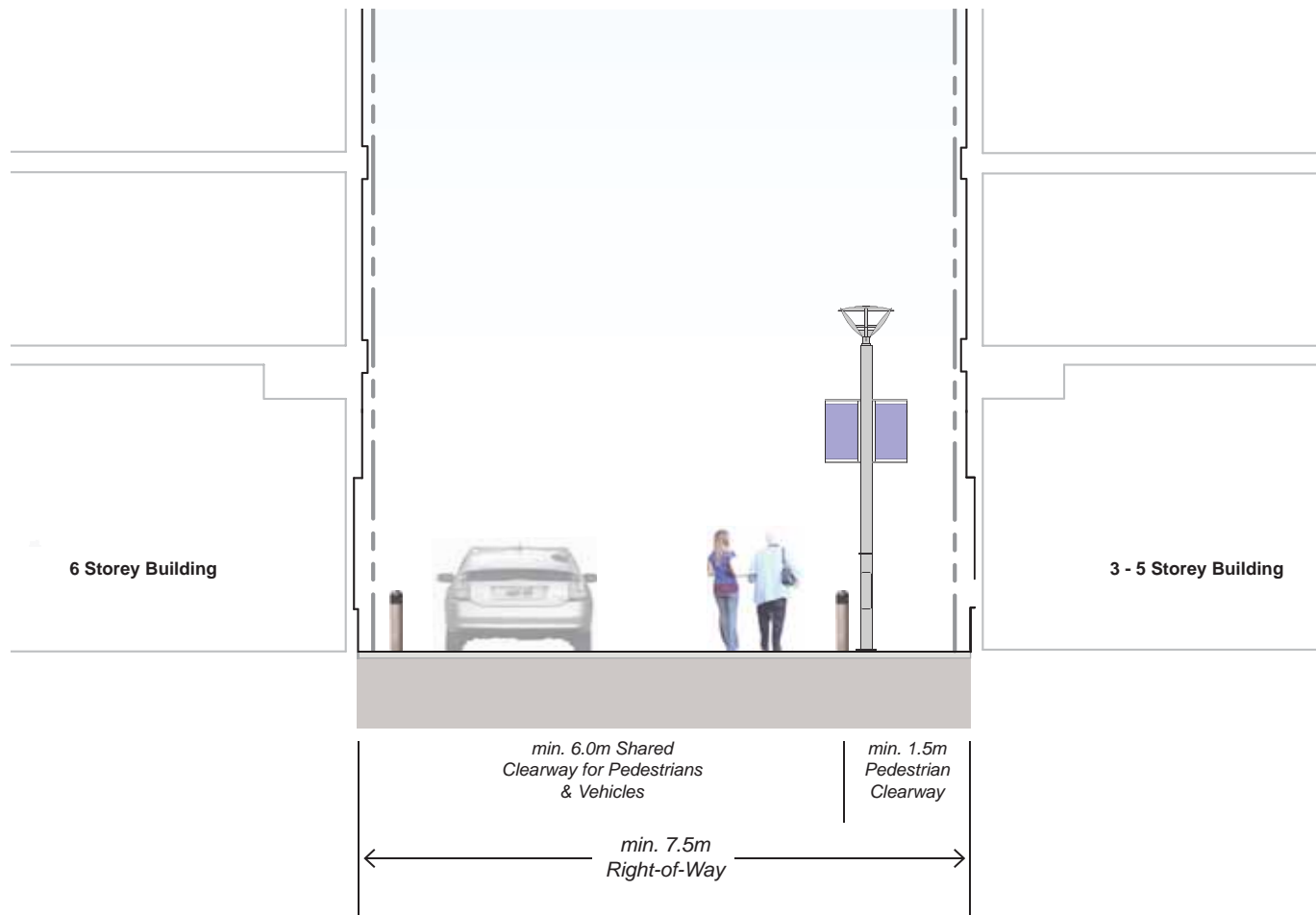


Figure 22 - Pedestrian Access with One-Way Vehicle Traffic