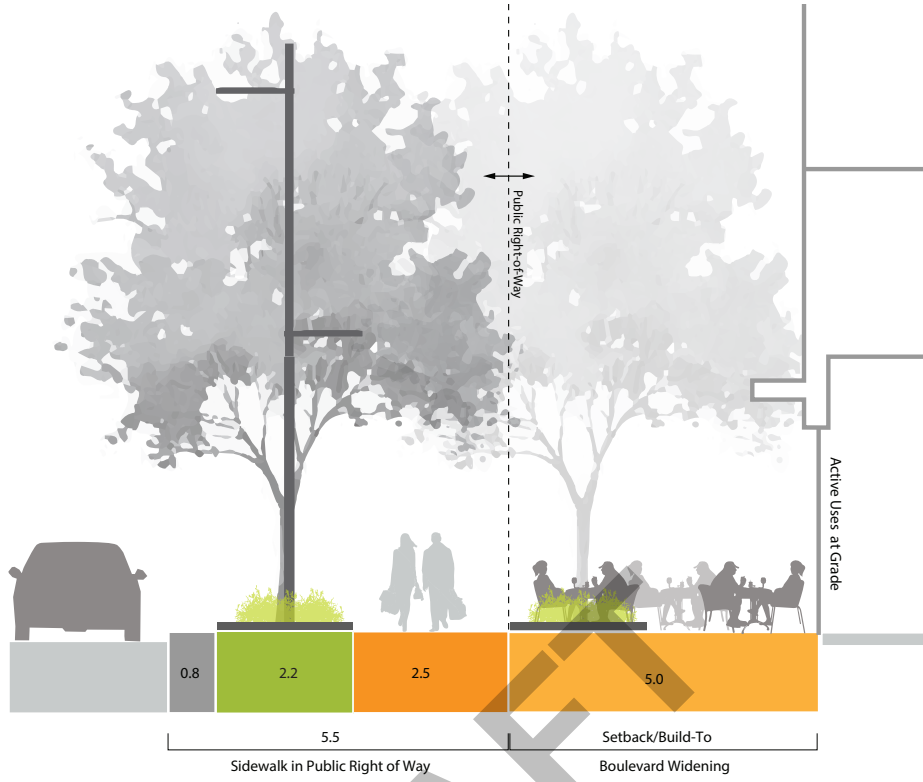
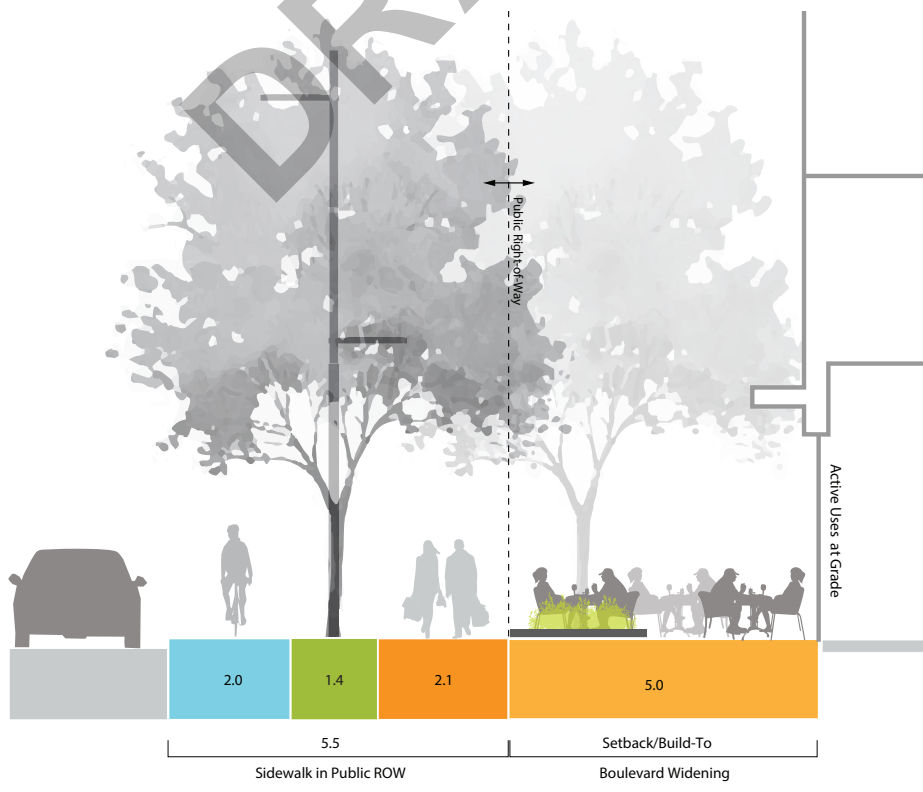


**Queensway Northside - Option 1:
No Cycling Facility (on southside)**



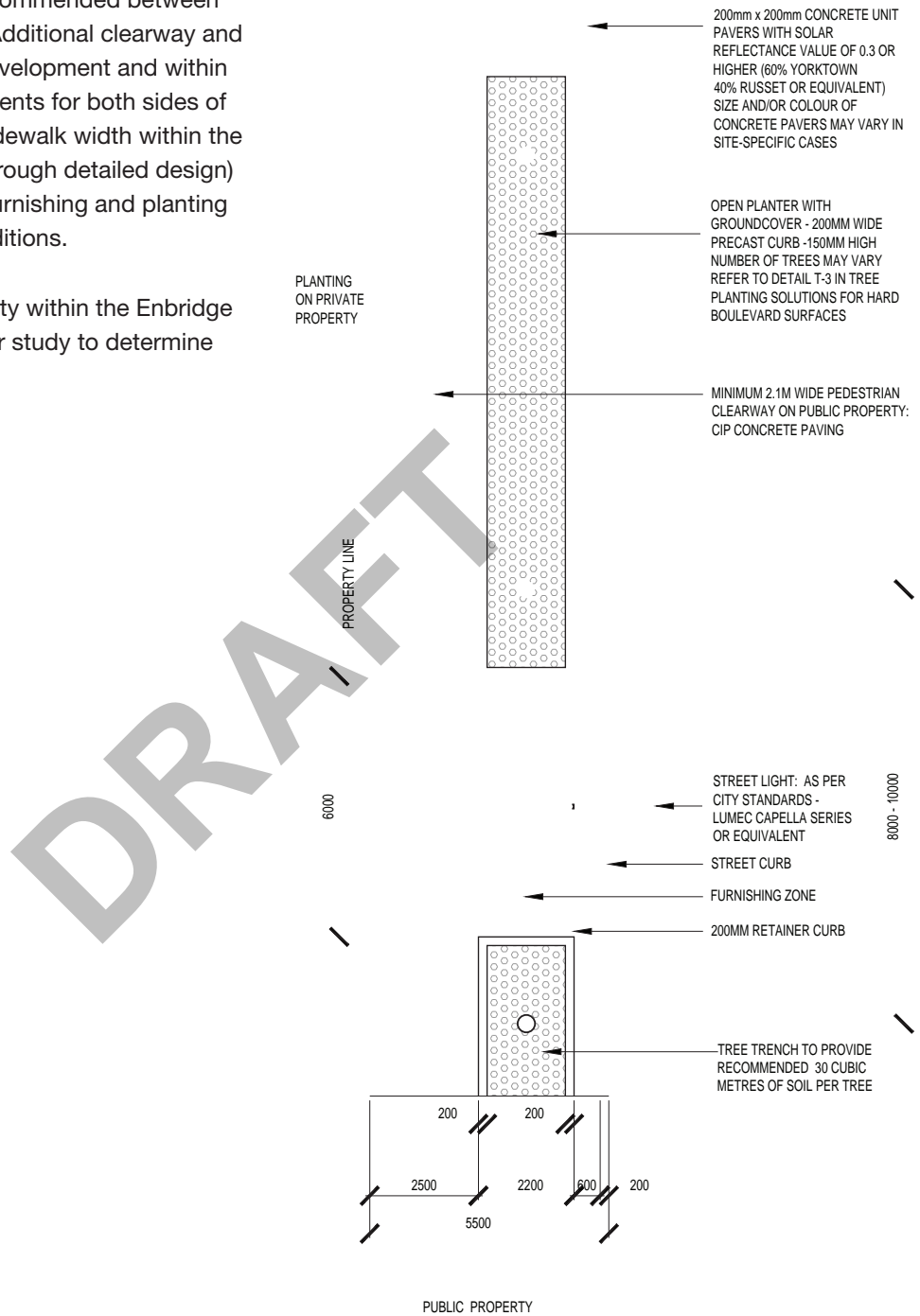
**Queensway Northside - Option 2:
Sidewalk Cycle Track**



Option 1

This detail illustrates the minimum improvement for the north and south sidewalks, and includes a 2.5m pedestrian clearway, 2.2m open planter and 0.8m edge zone. Concrete unit pavers are recommended between the clearway and back of curb. Additional clearway and tree planting is anticipated with development and within the generous setbacks and easements for both sides of The Queensway. Any additional sidewalk width within the public right-of-way (determined through detailed design) should go to the widening of the furnishing and planting zone to improve tree planting conditions.

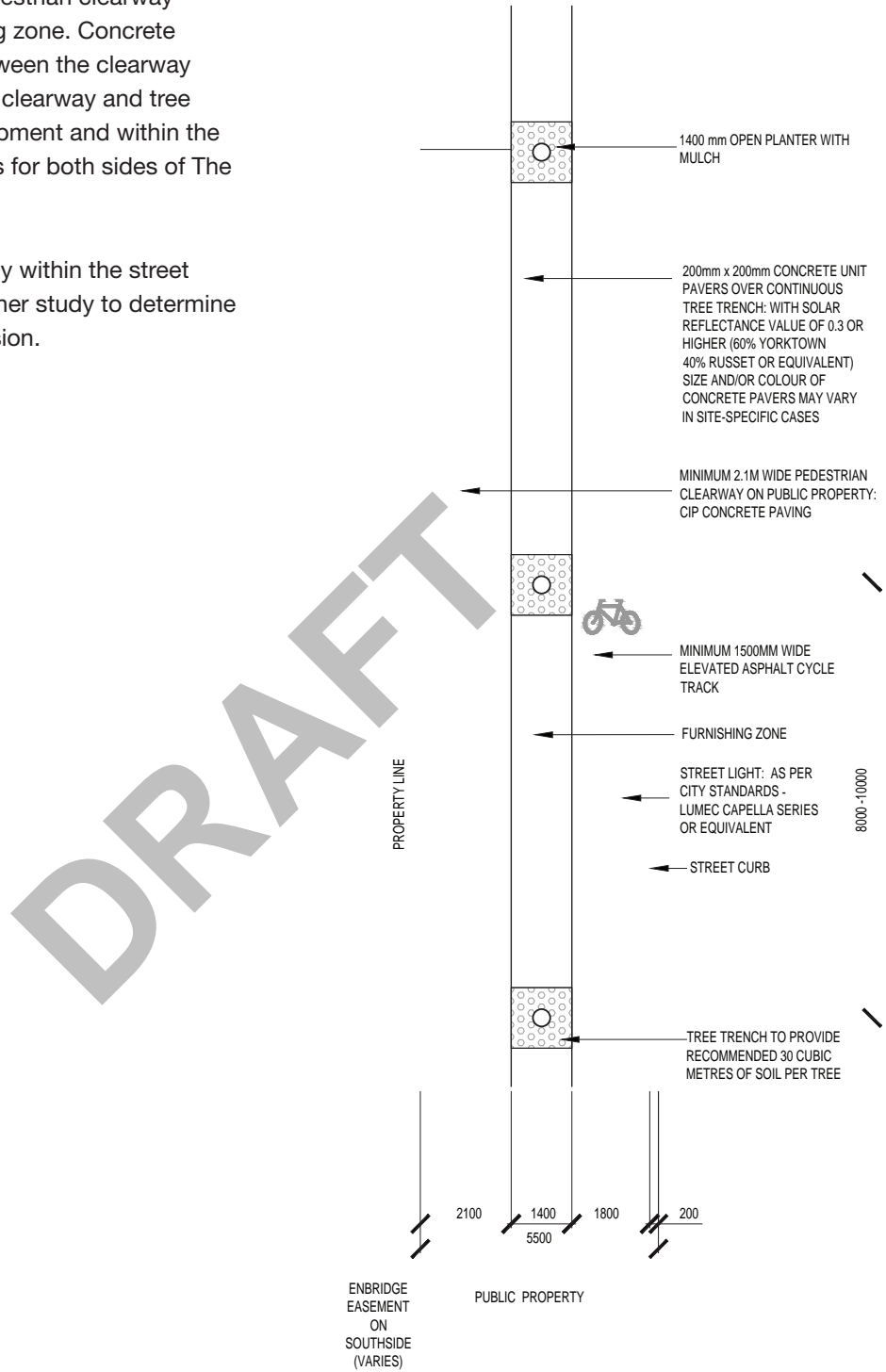
This detail imagines a cycling facility within the Enbridge Easement which will require further study to determine its appropriateness.



Option 2

This detail illustrates wider north and south sidewalks, and includes a minimum 2.1m pedestrian clearway with a 1.4m planting and furnishing zone. Concrete unit pavers are recommended between the clearway and back of cycletrack. Additional clearway and tree planting is anticipated with development and within the generous setbacks and easements for both sides of The Queensway.

This detail includes a cycling facility within the street right-of-way which will require further study to determine the appropriate facility and dimension.



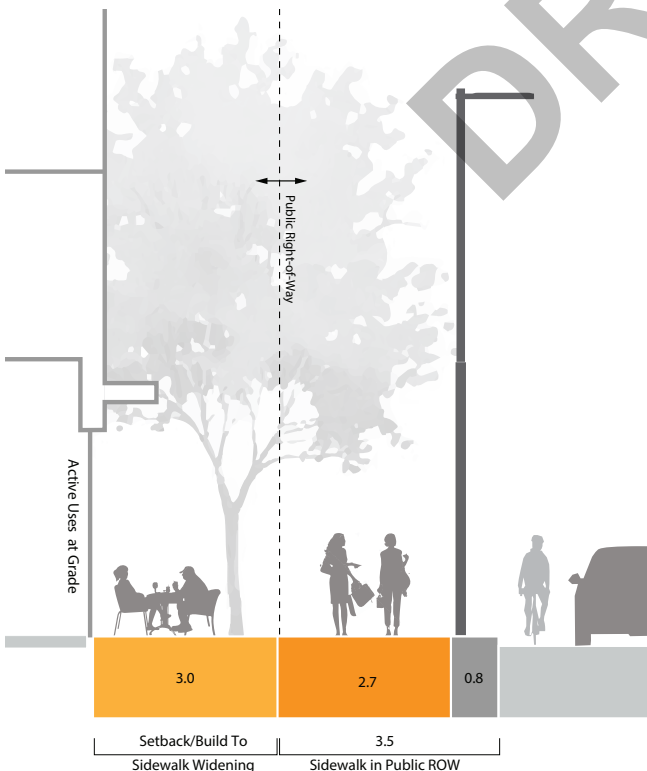
The West Mall

This detail illustrates the typical minimum sidewalk improvement for both sides of the West Mall north and south of the Queensway. The sidewalk design includes a 2.7m pedestrian clearway and 0.8m edge zone. Given the higher volume and speed of vehicles on this street, tree planting is not recommended in the public right of way unless sufficient clearance is provided to ensure proper growing conditions and healthy growth.

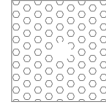
The pedestrian clearway will include standard cast-in-place concrete. Decorative concrete unit pavers are recommended between the clearway and back of curb. Private setbacks are anticipated with development, which can increase the pedestrian clearway and introduce trees and other planting and other greening. Any additional sidewalk width within the public right-of-way determined through detailed design should go to the pedestrian clearway.

This detail does not include any cycling infrastructure which will require further study to determine the appropriate facility and dimension.

The West Mall: Sidewalk with Tree Planting in Private Setback



← 200mm x 200mm CONCRETE UNIT PAVERS WITH SOLAR REFLECTANCE VALUE OF 0.3 OR HIGHER (60% YORKTOWN 40% RUSSET OR EQUIVALENT) SIZE AND/OR COLOUR OF CONCRETE PAVERS MAY VARY IN SITE-SPECIFIC CASES



← MINIMUM 2.1M WIDE PEDESTRIAN CLEARWAY ON PUBLIC PROPERTY; CIP CONCRETE PAVING

PLANTING ON PRIVATE PROPERTY

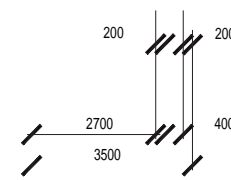


PROPERTY LINE

← STREET LIGHT: AS PER CITY STANDARDS - LUMEC CAPELLA SERIES OR EQUIVALENT

← STREET CURB

← 200MM RETAINER CURB



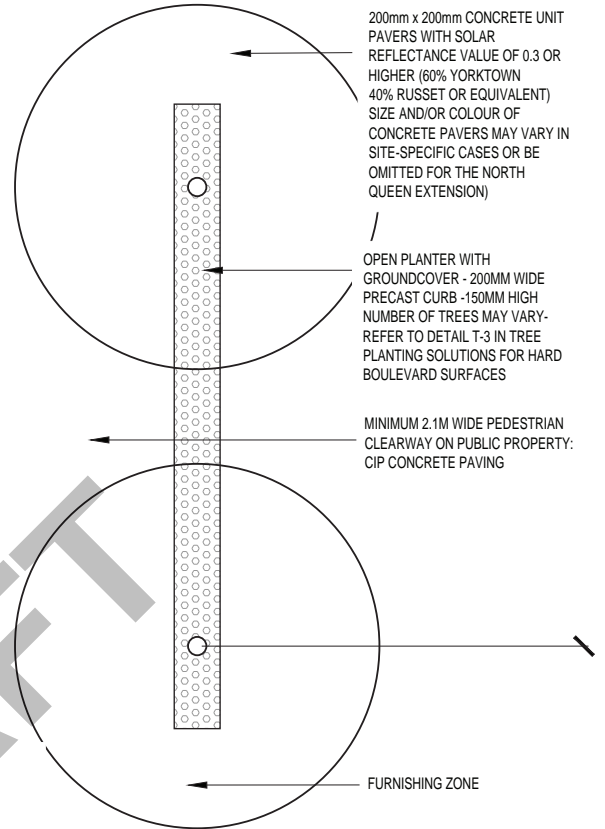
PRIVATE PROPERTY SETBACK

PUBLIC PROPERTY

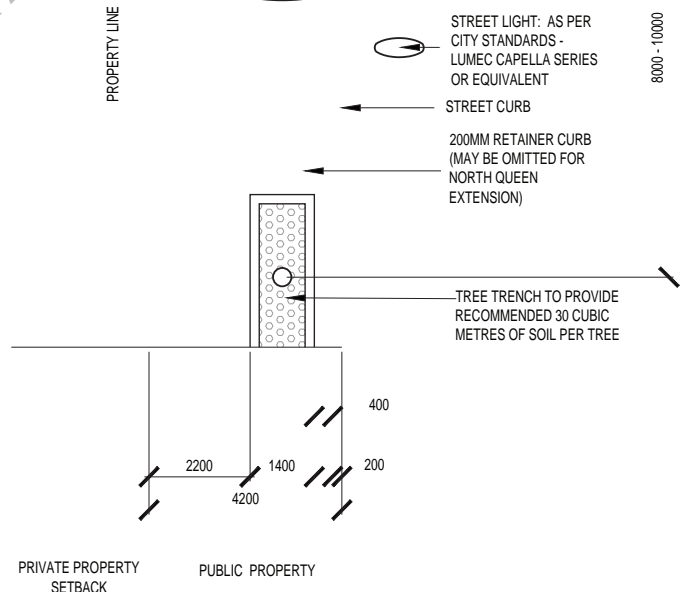
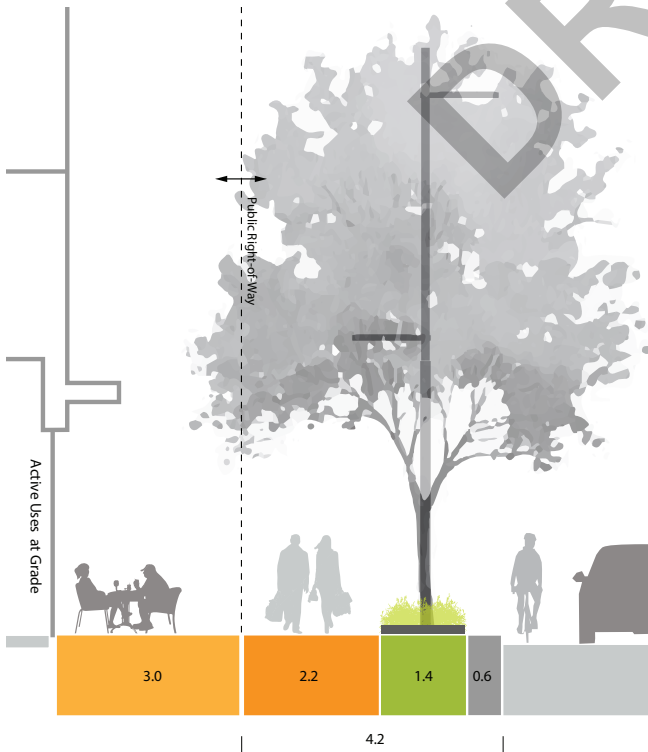
North Queen Street & Sherway Gardens Road

This detail illustrates the North Queen and North Queen Extension sidewalks as well as the re-aligned Sherway Gardens Road, and includes a 2.2m pedestrian clearway, 1.4m open planter, and 0.6m edge zone. Additional clearway and tree planting is anticipated with development and within setbacks. Any additional sidewalk width within the public right-of-way (determined through detailed design) should go to the widening of the furnishing and planting zone to improve tree planting conditions.

This detail does not include any cycling infrastructure which will require further study to determine the appropriate facility and dimension.



North Queen Street: Sidewalk with Trees in Open Planters

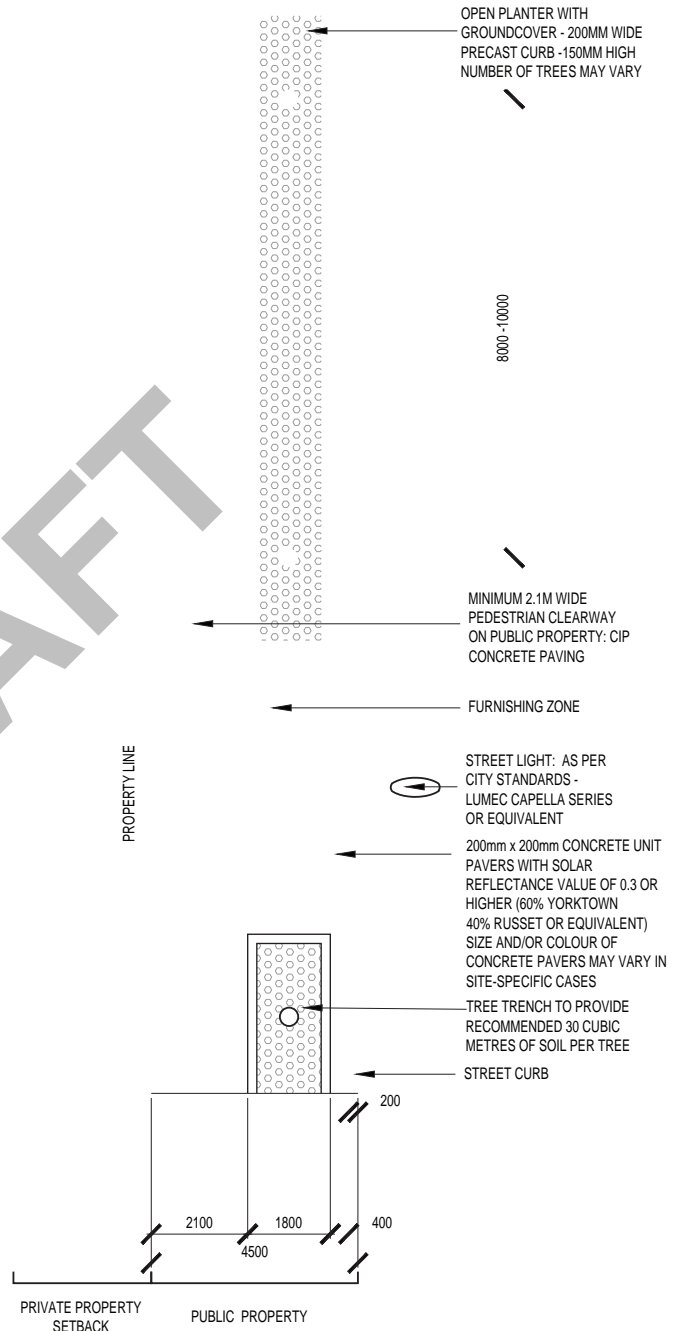
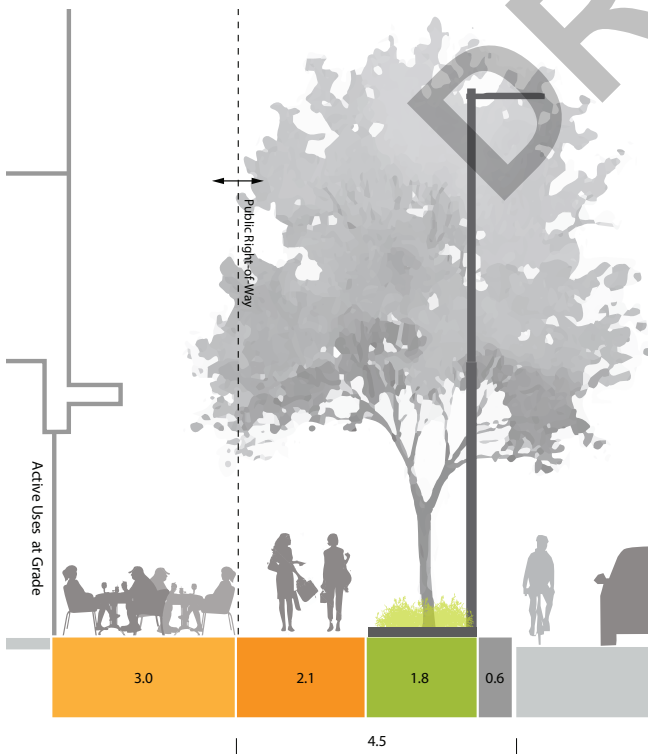


Nova Road, Boncer Drive, New Street A

This detail illustrates the minimum improvement for sidewalks on Nova Road, Boncer Drive and Street B. It includes a minimum 2.1m pedestrian clearway, 1.8m open planter, and 0.6m edge zone. Concrete unit pavers are recommended between the clearway and back of curb. Any additional sidewalk width within the public right-of-way (determined through detailed design) should go to the widening of the furnishing zone to improve tree planting conditions.

This detail does not include any cycling infrastructure which will require further study to determine the appropriate facility and dimension.

Nova Road, Boncer Drive, New Street A Sidewalk with Tree Planting in Open Planters

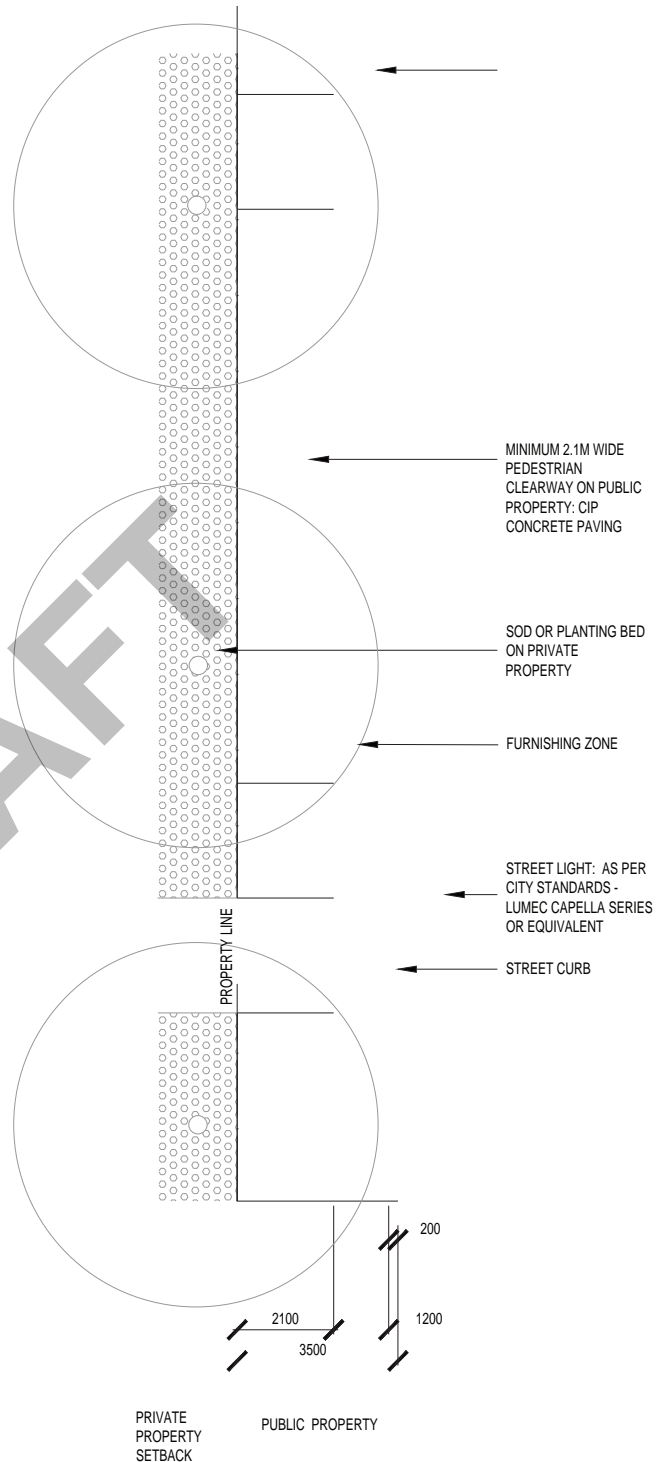
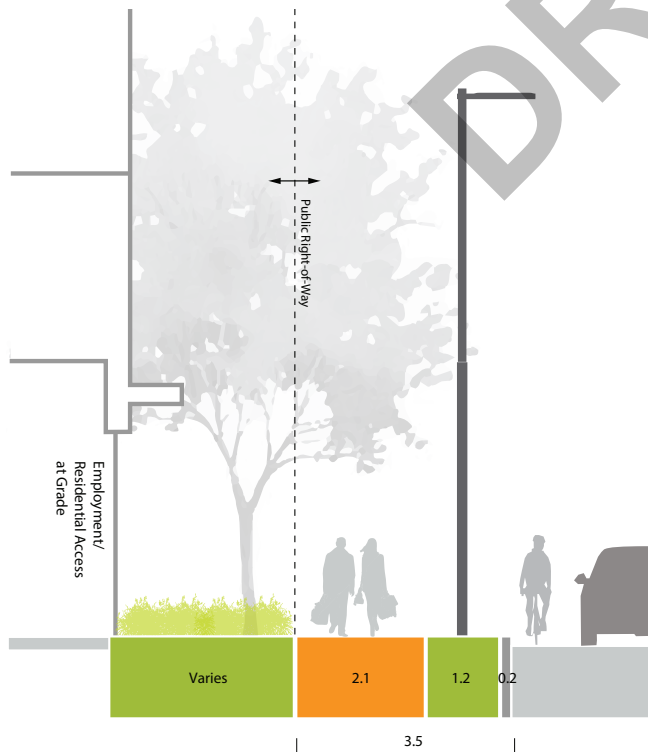


Evans Avenue

This detail illustrates the minimum improvement for sidewalks on Evans Avenue. It includes a minimum 2.1m pedestrian clearway. The furnishing and planting zone can either have vegetation or decorative concrete pavers. Additional frontage and marketing zone or planting is anticipated with development setbacks. Any additional sidewalk width within the public right-of-way (determined through detailed design) should go to the pedestrian clearway. Given the higher volume and speed of vehicles on this street, tree planting is not recommended in the public right of way unless sufficient clearance is provided to ensure proper growing conditions and healthy growth.

This detail does not include any cycling infrastructure which will require further study to determine the appropriate facility and dimension.

Evans Avenue Sidewalk with Trees Planted in Private Setbacks



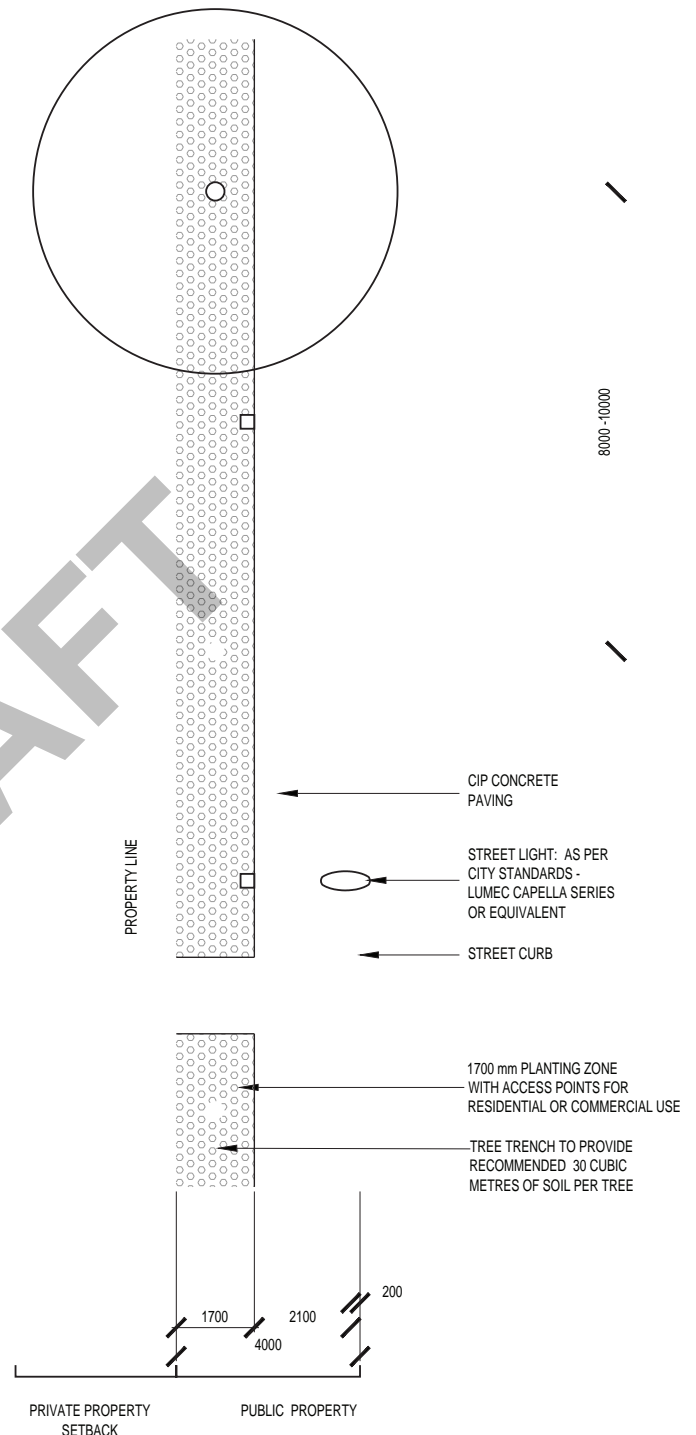
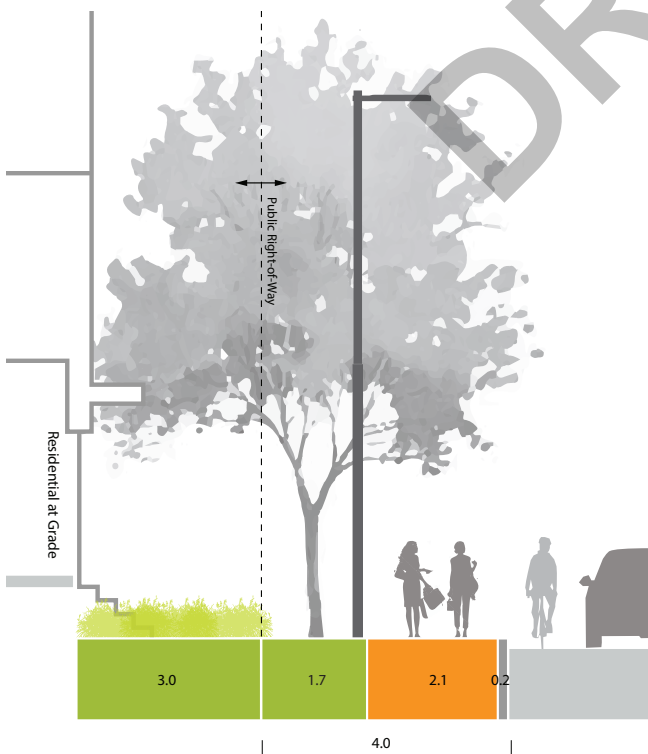
Local Neighbourhood Streets

Option 1

This detail illustrates the minimum sidewalk for Neighbourhood Street with 16.5m rights of way in case a 20m right-of-way cannot be accommodated. This detail includes a minimum 2.1m pedestrian clearway at the curb and a 1.7m planting zone at the back of sidewalk. Additional landscaping is possible in the minimum 3.0m private setbacks. This sidewalk detail is often considered on streets with residential uses at grade. Any additional sidewalk width within the public right-of-way (determined through detailed design) should go to the widening of the furnishing and planting zone to improve tree planting conditions.

This detail does not include any cycling infrastructure which will require further study to determine the appropriate facility and dimension.

Local Neighbourhood Street - 16.5m ROW: Sidewalk at Curb (illustrating Residential At Grade)

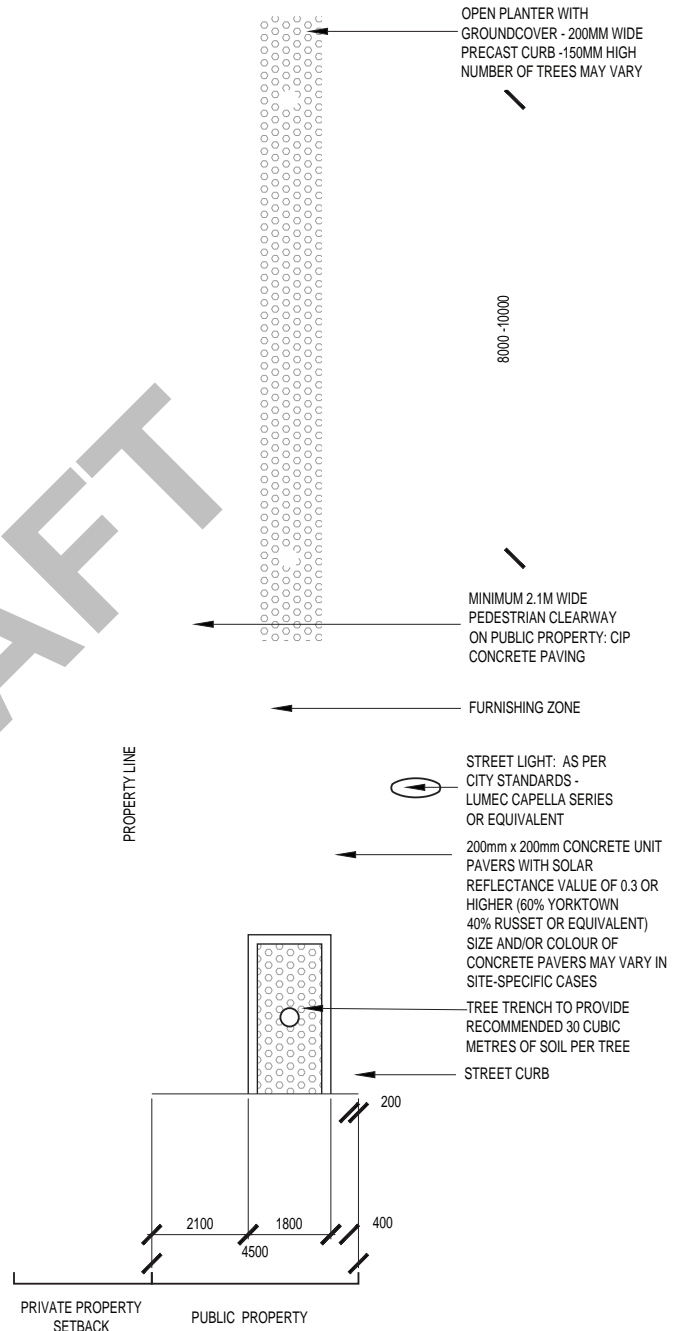
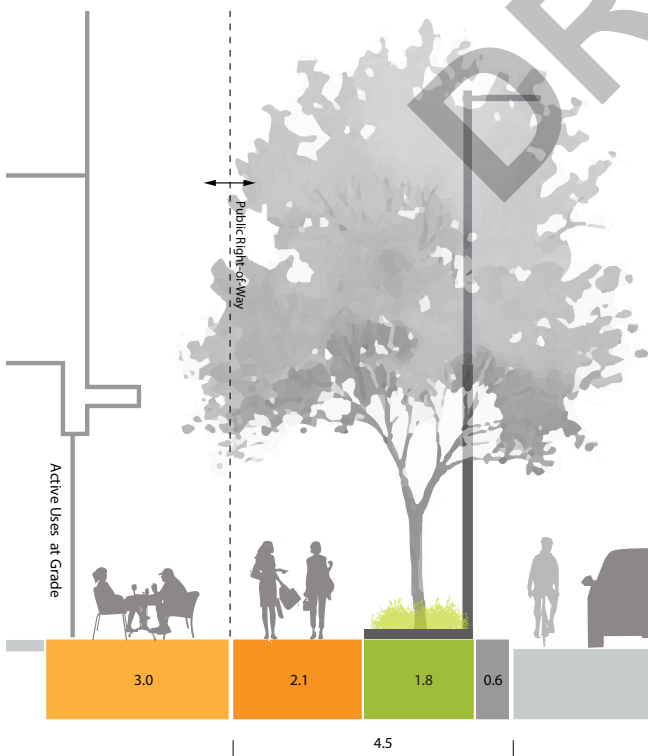


Option 2

This detail illustrates the minimum sidewalk for Neighbourhood Street with 20.0m rights of way. This detail includes a minimum 2.1m pedestrian clearway, a 1.8m planting zone, and a 0.6m edge zone. Concrete unit pavers are recommended between the clearway and back of curb. Additional landscaping is possible in the minimum 3.0m private setbacks. This sidewalk detail is often considered on streets with active retail or residential uses at grade. Any additional sidewalk width within the public right-of-way (determined through detailed design) should go to the widening of the furnishing and planting zone to improve tree planting conditions.

This detail does not include any cycling infrastructure which will require further study to determine the appropriate facility and dimension.

Local Neighbourhood Street - 20.0m ROW: Sidewalk at Curb (illustrating Active Uses At Grade)





Appendix



