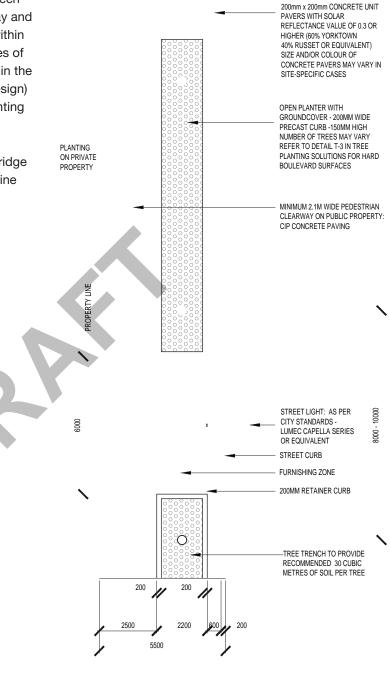


Option 1

This detail illustrates the minimum improvement for the north and south sidewalks, and includes a 2.5m pedestrian clearway, 2.2m open planter and 0.8m edge zone. Concrete unit pavers are recommended between the clewarway and back of curb. Additional clearway and tree planting is anticipated with development and within the generous setbacks and easements for both sides of The Queensway. Any additional sidewalk width within the public right-of-way (determined through detailed design) should go to the widening of the furnishing and planting zone to improve tree planting conditions.

This detail imagines a cycling facility within the Enbridge Easement which will require further study to determine its appropriateness.



Option 2

This detail illustrates wider north and south sidewalks, and includes a minimum 2.1m pedestrian clearway with a 1.4m planting and furnishing zone. Concrete unit pavers are recommended between the clearway and back of cycletrack. Additional clearway and tree planting is anticipated with development and within the 1400 mm OPEN PLANTER WITH Ô generous setbacks and easements for both sides of The Queensway. This detail includes a cycling facility within the street 200mm x 200mm CONCRETE UNIT PAVERS OVER CONTINUOUS right-of-way which will require further study to determine TREE TRENCH: WITH SOLAR REFLECTANCE VALUE OF 0.3 OR the appropriate facility and dimension. HIGHER (60% YORKTOWN 40% RUSSET OR EQUIVALENT) SIZE AND/OR COLOUR OF CONCRETE PAVERS MAY VARY IN SITE-SPECIFIC CASES MINIMUM 2.1M WIDE PEDESTRIAN CLEARWAY ON PUBLIC PROPERTY: CIP CONCRETE PAVING Ô MINIMUM 1500MM WIDE ELEVATED ASPHALT CYCLE TRACK FURNISHING ZONE STREET LIGHT: AS PER CITY STANDARDS -10000 LUMEC CAPELLA SERIES 8000 OR EQUIVALENT - STREET CURB Ô TREE TRENCH TO PROVIDE RECOMMENDED 30 CUBIC METRES OF SOIL PER TREE 1400 5500 ENBRIDGE PUBLIC PROPERTY EASEMENT SOUTHSIDE

(VARIES)

The West Mall

The West Mall:

This detail illustrates the typical minimum sidewalk improvement for both sides of the West Mall north and south of the Queensway. The sidewalk design includes a 2.7m pedestrian clearway and 0.8m edge zone. Given the higher volume and speed of vehicles on this street, tree planting is not recommended in the public right of way unless sufficient clearance is provided to ensure proper growing conditions and healthy growth.

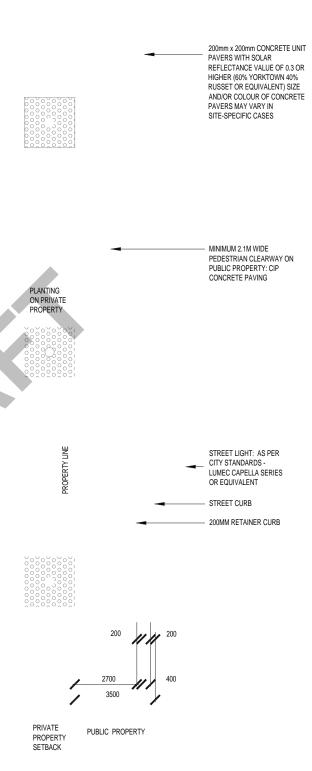
The pedestrian clearway will include standard cast-inplace concrete. Decorative concrete unit pavers are recommended between the clearway and back of curb. Private setbacks are anticipated with development, which can increase the pedestrian clearway and introduce trees and other planting and other greening. Any additional sidewalk width within the public right-ofway determined through detailed design should go to the pedestrian clearway.

This detail does not include any cycling infrastructure which will require further study to determine the appropriate facility and dimension.

Sidewalk with Tree Planting in Private Setback

Public Right-of-Way

Active Uses at Grade



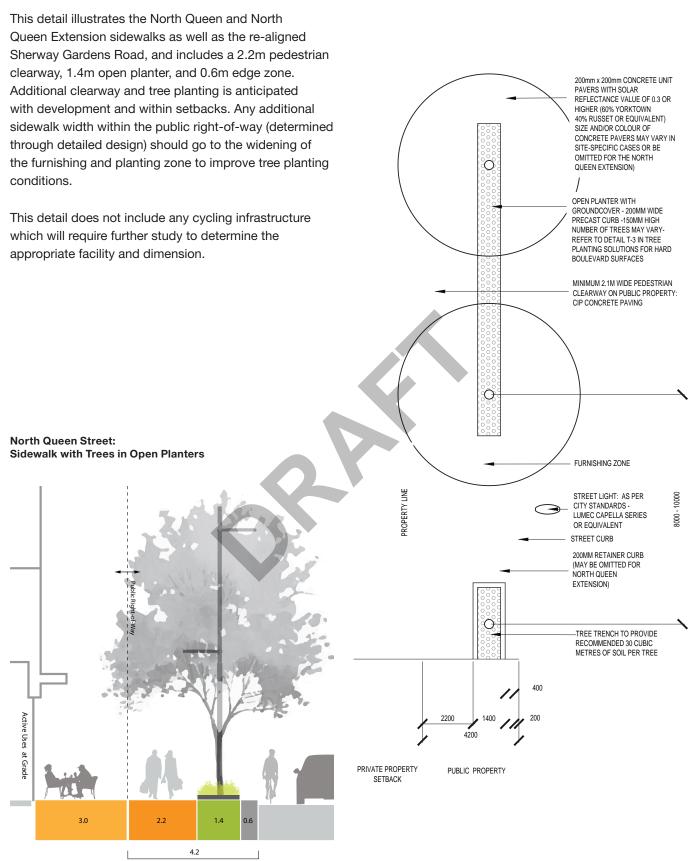
3.5

Sidewalk in Public ROW

Setback/Build To

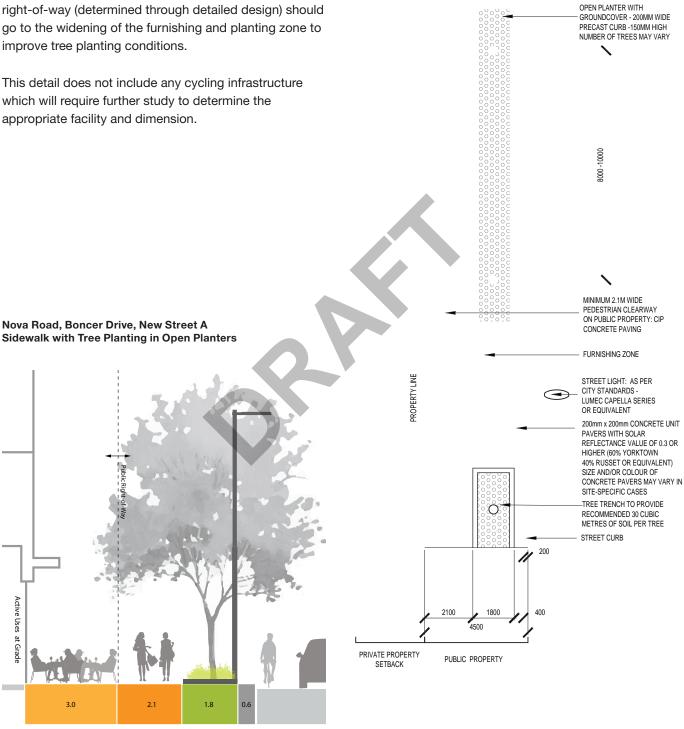
Sidewalk Widening

North Queen Street & Sherway Gardens Road



Nova Road, Boncer Drive, New Street A

This detail illustrates the minimum improvement for sidewalks on Nova Road, Boncer Drive and Street B. It includes a minimum 2.1m pedestrian clearway, 1.8m open planter, and 0.6m edge zone. Concrete unit pavers are recommended between the clearway and back of curb. Any additional sidewalk width within the public right-of-way (determined through detailed design) should go to the widening of the furnishing and planting zone to improve tree planting conditions.



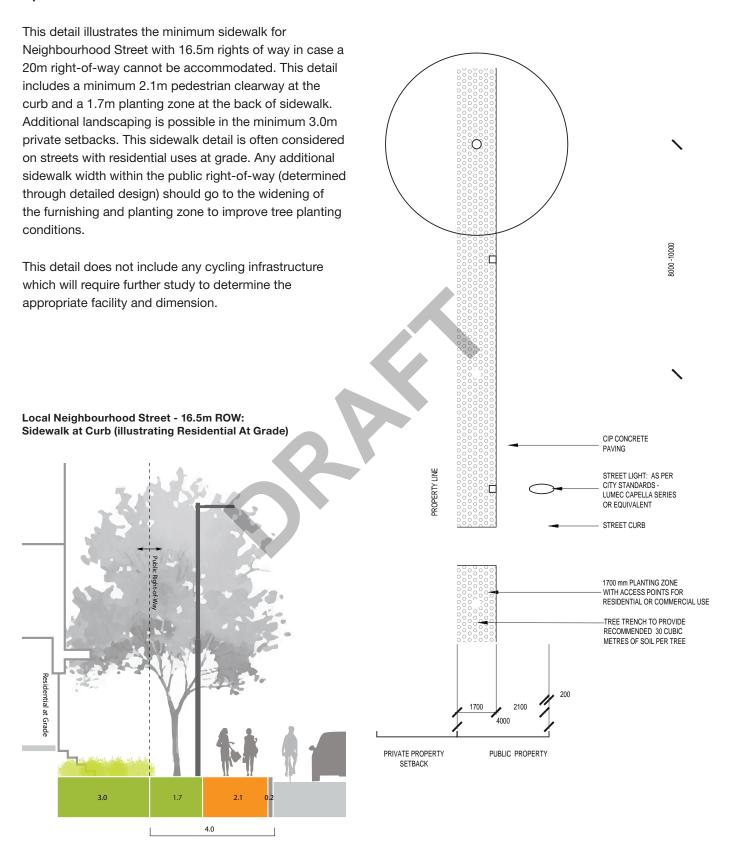
4.5

Evans Avenue

This detail illustrates the minimum improvement for sidewalks on Evans Avenue. It includes a minimum 2.1m pedestrian clearway. The furnishing and planting zone can either have vegetation or decorative concrete pavers. Additional frontage and marketing zone or planting is anticipated with development setbacks. Any additional sidewalk width within the public rightof-way (determined through detailed design) should go to the pedestrian clearway. Given the higher volume and speed of vehicles on this street, tree planting is not recommended in the public right of way unless sufficient clearance is provided to ensure proper growing conditions and healthy growth. This detail does not include any cycling infrastructure which will require further study to determine the MINIMUM 2.1M WIDE PEDESTRIAN appropriate facility and dimension. CLEARWAY ON PUBLIC PROPERTY: CIP CONCRETE PAVING SOD OR PLANTING BED ON PRIVATE PROPERTY FURNISHING ZONE **Evans Avenue** Sidewalk with Trees Planted in Private Setbacks STREET LIGHT: AS PER CITY STANDARDS -LUMEC CAPELLA SERIES 뾜 OR EQUIVALENT STREET CURB 200 Employment/ Residential Access at Grade 1200 2100 PRIVATE PUBLIC PROPERTY PROPERTY SETBACK Varies 3.5

Local Neighbourhood Streets

Option 1



Option 2

This detail illustrates the minimum sidewalk for Neighbourhood Street with 20.0m rights of way. This detail includes a minimum 2.1m pedestrian clearway, a 1.8m planting zone, and a 0.6m edge zone. Concrete unit pavers are recommended between the clearway and back of curb. Additional landscaping is possible OPEN PLANTER WITH GROUNDCOVER - 200MM WIDE PRECAST CURB -150MM HIGH in the minimum 3.0m private setbacks. This sidewalk detail is often considered on streets with active retail NUMBER OF TREES MAY VARY or residential uses at grade. Any additional sidewalk width within the public right-of-way (determined through detailed design) should go to the widening of the furnishing and planting zone to improve tree planting conditions. This detail does not include any cycling infrastructure which will require further study to determine the appropriate facility and dimension. MINIMUM 2.1M WIDE PEDESTRIAN CLEARWAY ON PUBLIC PROPERTY: CIP Local Neighbourhood Street - 20.0m ROW: CONCRETE PAVING Sidewalk at Curb (illustrating Active Uses At Grade) FURNISHING ZONE PROPERTY LINE STREET LIGHT: AS PER CITY STANDARDS LUMEC CAPELLA SERIES OR EQUIVALENT 200mm x 200mm CONCRETE UNIT PAVERS WITH SOLAR REFLECTANCE VALUE OF 0.3 OR HIGHER (60% YORKTOWN 40% RUSSET OR EQUIVALENT) SIZE AND/OR COLOUR OF CONCRETE PAVERS MAY VARY IN SITE-SPECIFIC CASES TREE TRENCH TO PROVIDE \circ RECOMMENDED 30 CUBIC METRES OF SOIL PER TREE STREET CURB Active Uses at Grade 4500 PRIVATE PROPERTY PUBLIC PROPERTY SETBACK 3.0 0.6

4.5





Table 1. Guideline Sections and their Reinforcing Principles.

P <u>r</u>	Principle/Guideline Section	Streets & Blocks	Parks & Open Space	Built Form (Height & Transitions)	Grade related- uses & Setback	View corridors and Vistas	Mobility Network	Parking, access & Servicing	Sustainability
-	Economic Vitality Principle: Support activity and redevelopment that provides a healthy economic foundation for the area.	\Diamond	\langle		\Diamond		\langle		\Diamond
6	Mix of Uses Principle: Shift to a more broadly balanced mix of land uses that are part of a clear strategy, which responds appropriately to existing and planned conditions.				\Diamond				
က်	Compatibility and Sustainability Principle: Minimize adverse impacts between new development and adjoining Employment Areas and transportation infrastructure	\Diamond			\Diamond		\Diamond	\Diamond	
4	Cohesive Urban Appearance Principle: Establish a cohesive, urban appearance, with buildings that frame the streets as the dominant built form.			\$				\Diamond	
ပ်	Safe and Convenient Movement Principle: Enhance safe and convenient movement through the area and to surrounding areas by providing greater opportunities for walking, cycling and public transit use, addressing traffic and congestion issues, and creating new streets and connections as redevelopment occurs.				\Diamond	\langle	\Diamond		
9	Connected and Attractive Public Spaces Principle: Create a green, safe and attractive place consisting of public parks, promenades, streetscape improvements and privately owned publicly-accessible open spaces that create a connected system and support a range of local social and recreational activities.	\Diamond	\Diamond		\Diamond	♦	\Diamond	\Diamond	\Diamond
	Responsible Use of Natural Heritage Principle: Incorporate access to natural heritage areas as part of the green and open space networks while preserving their environmental integrity.		\Diamond		\ \ \ \				\Diamond
ထံ	Local Community Services and Facilities Principle: Coordinate the introduction of new residential development in the area with the provision of community services and facilities.				\Diamond		\Diamond		
<u>ဝ</u> ်	Sustainable Infrastructure Principle: Ensure that adequate water and sewage services exist to accommodate new development and that stormwater is managed in a sustainable way.	\Diamond					\Diamond		♦
10.	Logical Phasing Principle: Phase any new residential development to incrementally add to the creation of a complete community that can coexist with adjacent uses.	\Diamond	\Diamond		\Diamond		\Diamond	\Diamond	