

Midtown Public Realm Implementation Strategy

April 2022

Acknowledgements

Land Acknowledgement

We acknowledge the land within Midtown is the traditional territory of many nations including the Mississaugas of the Credit, the Anishnabeg, the Chippewa, the Haudenosaunee and the Wendat peoples and is now home to many diverse First Nations, Inuit and Métis peoples. We also acknowledge that Toronto is covered by Treaty 13 with the Mississaugas of the Credit.

African Ancestral Acknowledgement

The City of Toronto acknowledges all Treaty peoples – including those who came here as settlers – as migrants either in this generation or in generations past – and those of us who came here involuntarily, particularly those brought to these lands as a result of the Trans-Atlantic Slave Trade and Slavery. We pay tribute to those ancestors of African origin and descent.

City Planning Division Study Team

Public Realm Lead: Svetlana Lavrentieva

Abdullah Diab

Anson Ma

Jana Neumann

Matt Armstrong

Melanie Melnyk

Sabrina Machry

The study team would like to thank

Technical Advisory Committee

City Planning

Alex Teixeira

Al Rezoski

Arthur Lo

Dawn Hamilton

Itai Peleg

Jason Brander

Julie Bogdanowicz

Justin Matthews

Kathryn Moore

Meredith Vaga

Michael Sakalauskas

Nigel Tahair

Oren Tamir

Robert Ursini

Parks Forestry & Recreation

Corinne Fox

Nicholas Trevisan

Teresa Liu

Xin (Will) Zhou

Toronto Water

Diane Sertic

Dhiren Barot

Les Arishenkoff

Mike Collins

Transportation Services

Anson Yuen

Becky Katz

Bill Tsomokos

Cai de Ridder

Daniel Reynolds

Julius Aquino

Katie Wittmann

Kristina Hausmanis

Leigh Sherkin

Luigi Nicolucci

Mark Berkovitz

Matthew Davis

Michelle Berquist

Patricia Pearsall-Mills

Robert Mays

Sean Wheldrake

Sheldon Koo

Table of Contents

Acknowledgements	i
1.0 Overview and Background	1
Overview and Background	2
2.0 Implementation Toolbox	23
Overview	24
Section 37 and Community Benefits Charge	24
Development Charges (DCs)	24
Other Implementation Tools	26
3.0 Considerations for Public Realm Phasing	27
Development Activity	28
Soft Sites, Heritage Properties and Opportunities and Constraints	30
4.0 Public Realm Elements	33
Public Realm Elements	34
Landscape Setbacks	37
Streetscape Improvements	40
4.1 Eglinton Green Line	43
Overview	44
4.2 Park Street Loop	47
Overview	48
Background Analysis	50
Park Street Loop Concept	59
Final Condition Recommendations	60
Design Guidelines	70
Implementation	75

4.3 Davisville Community Street	77
Overview	78
Background Analysis	80
Final Condition Recommendations	86
Implementation	91
4.4 Merton Street Promenade	93
Overview	94
Background Analysis	95
Final Condition Recommendations	100
Implementation	105
4.5 Pailton Crescent Connector	107
Overview	108
Background Analysis	109
Final Condition Recommendations	114
Implementation	117
4.6 Balliol Green Street	118
Overview	119
Background Analysis	120
Final Condition Recommendations	124
Implementation	127
4.7 The Midtown Greenways	128
Overview	128
Keewatin Greenway	128
Erskine Greenway	128
The Greenways	128
Overview	129
Keewatin Greenway	131
Background Analysis	132
Final Condition Recommendations	138
Implementation	143

Erskine Greenway	144
Background Analysis	144
Final Condition Recommendations	148
Implementation	153
The Greenways	154
Background Analysis	154
Final Condition Recommendations	160
Implementation	166
4.8 Redpath Revisited	167
Overview	168
Background Analysis	169
Final Condition Recommendations	177
Implementation	182
4.9 Yonge Street Squares	183
Overview	184
Background Analysis	186
Final Condition Recommendations	193
Implementation	204
5.0 Parks and Open Spaces Summary	205
Summary	206
Intersection of Public Realm Moves with Parks and Other Open Spaces	206
6.0 Phasing and Implementation Recommendations	209
Summary	210
Public Realm Implementation Matrix	212
A Appendix - List of Figures	226
B Appendix - Eglinton Green Line Landscape and Public Realm Standards	235

1.0

Overview and Background

- 1.1 Purpose of Implementation Strategy
- 1.2 How to Read this Strategy
- 1.3 Summary of the Midtown Public Realm Plan
- 1.4 Aligned Major Initiatives and Studies
- 1.5 Additional Midtown Public Realm Elements
- 1.6 Public Realm Context
- 1.7 Recent Achievements

Overview and Background

1.1 Purpose of Implementation Strategy

The purpose of the Midtown Public Realm Implementation Strategy is to realize a long term vision for the provision of high quality streetscapes and open spaces as outlined in the Yonge-Eglinton Secondary Plan and supporting studies, to create a vibrant, active, resilient and connected public realm.

In July 2018, Council adopted the Yonge-Eglinton Secondary Plan through Official Plan Amendment 405 (OPA 405), setting out a 25-year vision with a policy framework to guide growth and priority infrastructure improvements. Supporting the Secondary Plan were a series of infrastructure assessments, including the Midtown Parks and Public Realm Plan, 2018 (PPR Plan). The PPR Plan sets out detailed direction for expanding and enhancing the network of parks, publicly-accessible open spaces, streetscapes, cycling and mid-block connections to maintain a high quality of life for Midtown's growing population. In June 2019 the Provincial Minister of Municipal Affairs approved OPA 405, which includes area-wide parks and public realm policies, priorities for parkland acquisition and eleven Public Realm Moves along key streets in the Midtown area.

In adopting OPA 405, Council directed staff to develop strategies to advance the inter-divisional implementation of initiatives identified in the Secondary Plan policies and infrastructure assessments. This document addresses the public realm strategies outlined in the PPR Plan and related parks initiatives, providing a detailed road map for their planning and implementation through development and/ or capital projects over the near, mid and long term.

This Public Realm Strategy:

- Provides direction for street and open space design to design professionals, developers and city staff;
- Describes and illustrates concept plans for the Midtown Public Realm Moves, including potential interim and final conditions, as applicable;
- Identifies phasing and implementation opportunities, including coordinated parks projects; and
- Summarizes financial considerations, including the funding and implementation tools available to deliver Public Realm Moves.

1.2 How to Read this Strategy

The Public Realm Implementation Strategy is intended to be utilized by City staff and applicants in the proposal and review of new developments, the design of major infrastructure updates such as street reconstruction and municipal servicing upgrades, as well as when undertaking street and public realm improvements. This Strategy forms part of the overall Midtown Infrastructure Implementation Strategy, a document intended to coordinate City Divisions in the prioritization and phasing of infrastructure improvements to respond to growth in the Yonge-Eglinton Secondary Plan area. All background documentation to provide base information and guide implementation recommendations is current to December 31, 2021.

The document is structured as follows:

1.0 Overview and Background

A general overview of the document, a summary of the initiatives for Midtown public realm and aligned initiatives, and improvements completed to December 31st, 2021 are provided in this section.

2.0 Implementation Toolbox

This section includes a summary of various implementation and funding tools that may be used to realize public realm improvements.

3.0 Considerations for Public Realm Phasing

This section outlines aligned priorities to further inform funding and phasing strategies.

4.0 Public Realm Moves

The Public Realm Moves section generally describes the public realm elements and landscape setbacks, and provides concept designs for applicable interim and final conditions for the Public Realm Moves identified in the Yonge-Eglinton Secondary Plan. These concepts are intended to guide design, and are subject to feasibility analysis and detailed evaluation. They will be implemented using standards and best practices current at the time of implementation.

5.0 Parks and Open Spaces Summary

The Public Realm Moves intersect with parks in the area to establish a cohesive green network for Midtown. This section provides a summary of the Midtown Infrastructure Implementation Strategy related to parks, including an overview of planned projects, funding and delivery tools, and phasing approach.

6.0 Phasing and Implementation Recommendations

This section includes a Phasing and Implementation Recommendations Matrix, providing a strategy for the delivery of public realm initiatives through development review, capital planning and/ or inter-divisional partnerships, funding sources and projected timelines.

1.3 Summary of the Midtown Public Realm Plan

The Midtown Parks and Public Realm Plan (PPR Plan) was completed in 2018 to support the development of the Yonge-Eglinton Secondary Plan. The PPR Plan sets out a comprehensive vision for parks and the public realm in Midtown and establishes a path to achieving an expanded, connected and improved system within Midtown’s mature fabric. It presents a shared vision to significantly improve the quality of life as growth continues to ensure that the dual qualities of Midtown – lush and green, and dense and urban - remain central character-defining elements of the area. The PPR Plan provides a framework to inform decisions about the acquisition of new parks, parks expansions, increased accessibility and improvements to existing parks, the reclamation of city streets as a series of community spaces, and creatively capitalizing on the variety of other open spaces found throughout Midtown.

The vision set out in the Midtown PPR Plan is guided by four aspirational objectives:

Create New and Expanded Parks and Open Spaces

As Midtown’s population continues to grow, acquisitions and expansions will be undertaken to create new parkland and making existing parks bigger, where possible, and accommodate a range of programming and functionality.

Treasure and Enhance our Parks and Open Spaces

Existing parks can be improved and repurposed to increase their utility, effectively enabling these spaces to serve a wider range of parks users and to improve their appearance and image, making a positive contribution to Midtown’s evolving identity.



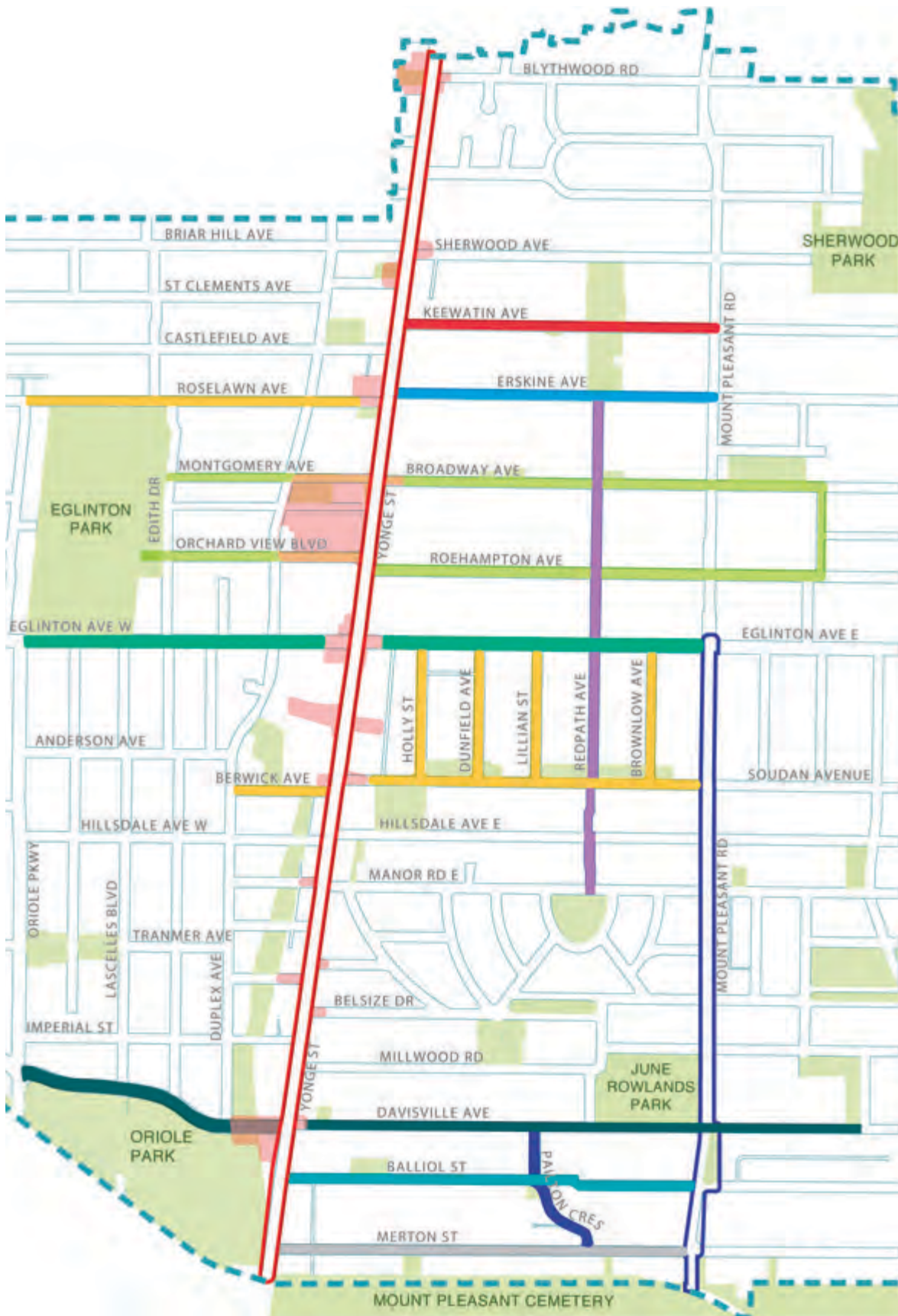


Figure 1 Map of the Public Realm Moves

Share our Parks and Open Spaces

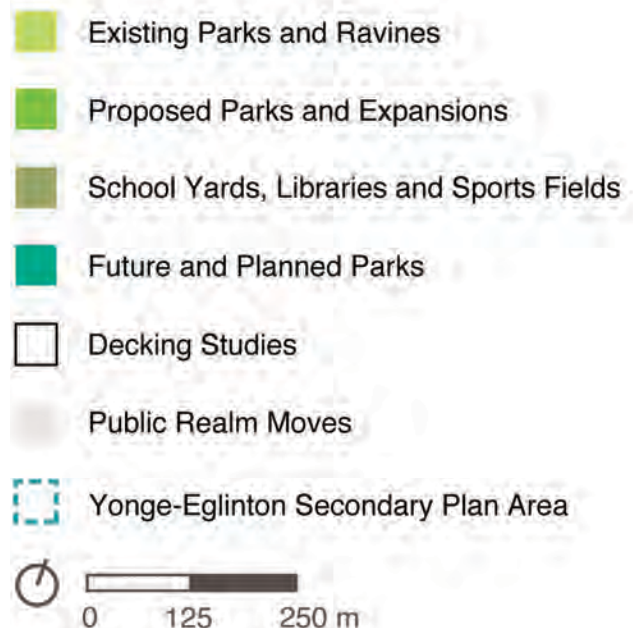
The high quality open space assets owned and operated by other public agencies and private landowners supplement and further contribute to the character of the area. Collaboration and partnerships are needed to ensure the existing and future population has access to these spaces, while securing new publicly-accessible open spaces through development, for example, to contribute to expansion of the public realm.

Connect our Parks and Open Spaces

The existing street grid presents the opportunity to establish a network of active transportation routes and pedestrian connections that provide better access to existing parks and open spaces, to expand the park experience and create a cohesive green network to support community life.

To bolster the green network, the PPR Plan outlines a series of Public Realm Moves, which build on those identified in the 2014 Midtown In Focus: Parks, Open Space and Streetscape Master Plan, creating the framework of a transformed public realm in Midtown. The eleven Moves are large-scale public space proposals that bring together changes in the design of public streets and parks with privately-owned open spaces.

Parks and Open Spaces Network



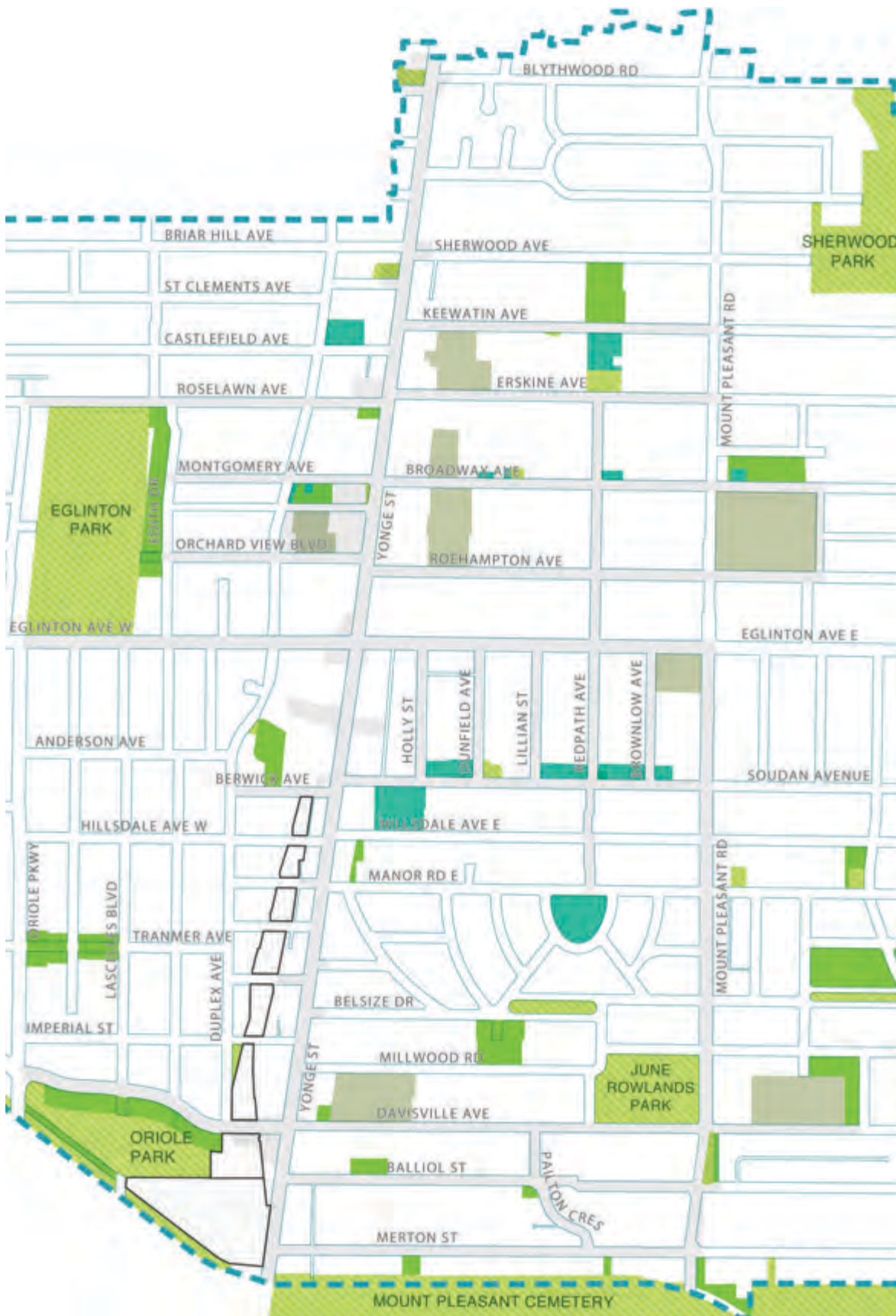


Figure 2 Map of Parks and Open Spaces Network relative to Midtown Public Realm Moves

1.4 Aligned Major Initiatives and Studies

This Strategy has been developed in parallel with other provincial and municipal planning initiatives and studies. The projects outlined below play a role in shaping the future of Midtown to ensure that development is appropriate and sustainable, and to provide the necessary infrastructure and services to support recent and ongoing growth.

I. Cycling Network Plan

The 10 Year Cycling Network Plan was approved in 2016 as a strategy to improve and expand the cycling infrastructure network across the city, with projects identified up to 2025.

Within Midtown, Yonge Street is highlighted as a suggested road for a major corridor study. Eglinton Avenue is also recommended for dedicated cycling infrastructure and connects with designated quiet street routes on local roads such as Broadway Avenue, Duplex Avenue and Millwood Road.

The plan was further adjusted following the adoption of the 10 Year Cycling Network Plan Update in 2019. As part of this update, Council directed a study on identifying additions to Midtown's cycling network and north-south corridors between Midtown and Downtown. The 2019-2021 Implementation Program will study Eglinton Avenue, Yonge Street, Oriole Parkway, Avenue Road and Mount Pleasant Road as potential corridors for dedicated cycling infrastructure to connect Midtown and Downtown.

In April 2021, as part of the ActiveTO program, Council directed staff to pursue opportunities to implement Cycling Network Expansion Projects. This included a complete street pilot on Yonge Street, connecting Bloor Street up to Davisville Avenue. The pilot project is continuing to be monitored and a report with recommendations is expected in early 2022.

Cycling Network Plan



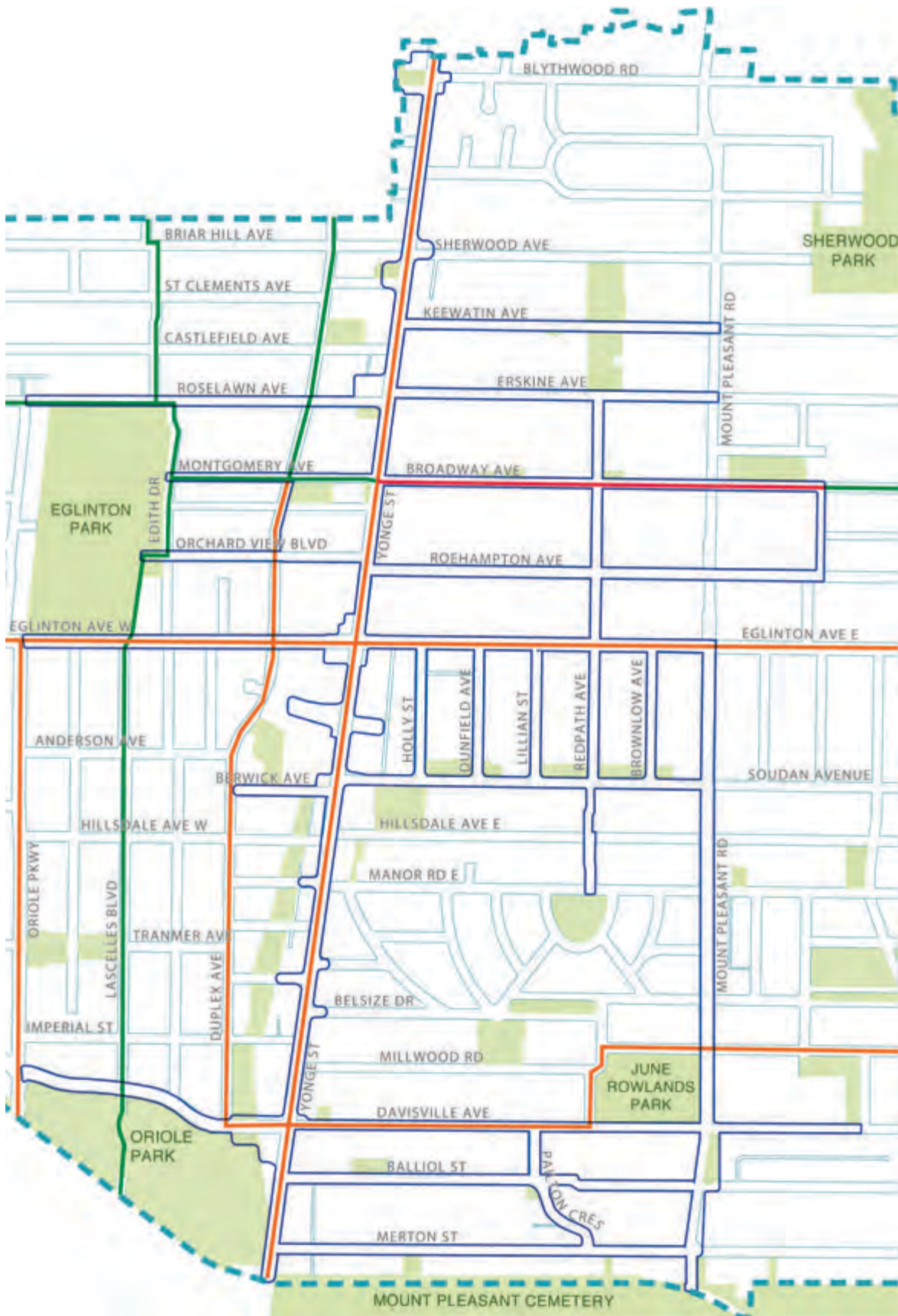


Figure 3 Map of the Cycling Network Plan within the Public Realm Moves area

II. Eglinton Crosstown

The Eglinton Crosstown, or Line 5 Eglinton, is a new rapid transit line currently under construction running along Eglinton Avenue from Weston Road to Kennedy Road. In Midtown, the line will operate underground with five stations in the area, including a connection with Line 1-Yonge-University at Yonge Street and Eglinton Avenue. It will provide fast and reliable service across one of Toronto's fastest-growing regions and match the rapid growth in Midtown with its need for increased transit capacity. This will ensure that transportation needs continue to be met sustainably as more people choose to live, work and play in Midtown.

As part of the Eglinton Crosstown, Metrolinx will be responsible for delivering some of the cycling infrastructure and public realm improvements in Midtown, with funding from the City. Within Midtown, these include the portions of Eglinton Avenue along the front of the five stations in the area, as well as the segment of Eglinton Avenue between Yonge Street and Avenue Road.

III. Transit Initiatives

In addition to the Eglinton Crosstown, there are also ongoing studies and initiatives towards improving the transit network across Midtown. The Yonge-Eglinton Secondary Plan directs the study of major north-south streets for surface transit priority corridors.

Midtown is served by higher-order transit and local bus routes, as shown in Figure 3. The Line 1 subway runs north-south along Yonge Street, with two stations at Eglinton Avenue and Davisville Avenue. These are integrated with local bus routes that provide transit access within the area as well as connecting Midtown with other neighbourhoods.

The Line 5 Eglinton Crosstown LRT is under construction and scheduled to be completed by 2022. This will boost transit east-west capacity by linking Mount Dennis with Kennedy via Eglinton Avenue and provide a connection with the Line 1 subway. There will be five stops serving Midtown: at Chaplin Crescent, Avenue Road, Yonge Street, Mount Pleasant Road and Bayview Avenue (Leaside).





Figure 4 Map of transit network within the Public Realm Moves area

IV. Midtown Zoning Review

Following Council's adoption of the Yonge-Eglinton Secondary Plan in July 2018, the Midtown Zoning Review was launched to assess and update the zoning by-laws in Midtown's 22 Character Areas. This study will amend the zoning in the area to implement the policy directions of the approved Yonge-Eglinton Secondary Plan. An updated zoning framework for the area will guide appropriate development through regulating land uses, height, built form and performance standards.

V. Parkland Strategy

The Parkland Strategy was adopted in 2019 to guide city-wide planning for new parks and expanding and improving access to the existing parks over the next 20 years. The strategy analyzes parkland provision and trends, and provides strategic actions to guide decision-making and implementation tools to advance these objectives.

Within Midtown, the area generally has below-average parkland provision that is decreasing due to population growth. It is also designated as an Area of Parkland Need, which prioritizes future new parkland and parks improvements through a locally-oriented parks plan.

The Parkland Strategy continues to be implemented in Midtown. City staff are working on detailed analysis of local parkland provision. Staff are also improving local parks and have secured parkland from recent development applications and City-initiated acquisitions on an ongoing basis.

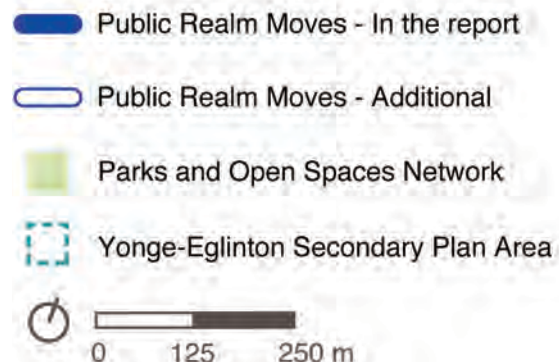
1.5 Additional Midtown Public Realm Elements

With the increasing density in the Midtown area, conceptual street and open space studies and recommendations addressed in this report focus on areas with a high number of development applications, which generally fall between Yonge Street and Mount Pleasant Road. The remaining east and west extensions of these Public Realm Moves will be designed based on the recommendations provided in their applicable chapters of this document.

Additional items that are to be delivered beyond the scope of this report include the following:

- The Yonge Street Square Extensions will be implemented through development or planned capital works, subject to the outcome of the Yonge Street ActiveTO Pilot;
- The Mount Pleasant Arboretum will be prioritized within the long-term Transportation Services Capital Work Program relative to the outcome of the Yonge Street ActiveTO Pilot;
- Future mid-block connections, new laneways and new local public streets identified in the Yonge-Eglinton Secondary Plan will be achieved through the development review process;
- Proposed street realignments identified in the Yonge-Eglinton Secondary Plan will be implemented as part of the development review process;
- Construction of portions of the Eglinton Green Line within the City's right-of-way (ROW), west of Yonge Street, will be delivered by Metrolinx as part of the Eglinton Crosstown construction.

Additional Midtown Public Realm Elements



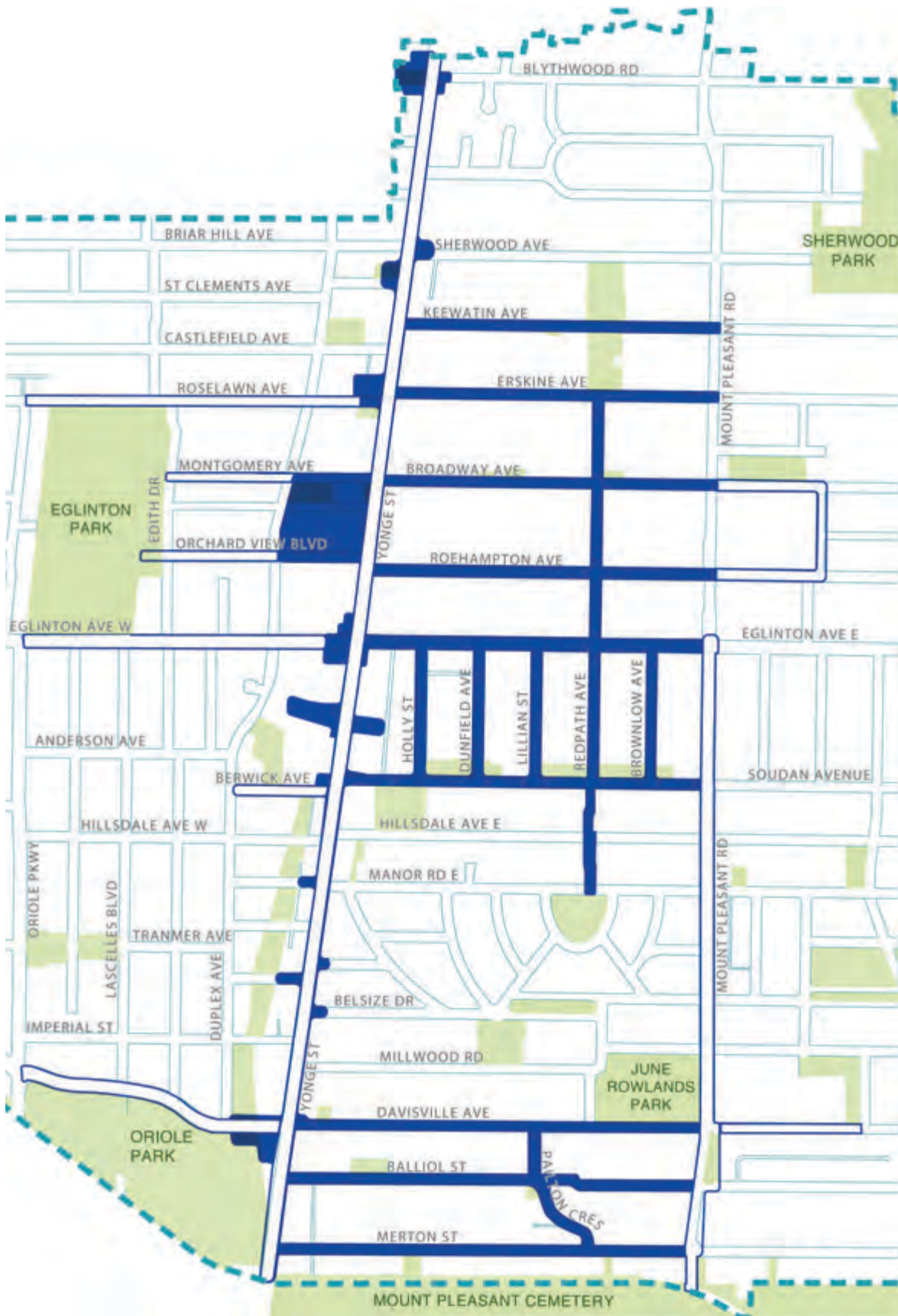


Figure 5 Scope of Public Realm Elements addressed in this report

1.6 Public Realm Context

Roadway Width

Figure 6 shows the range of existing roadway widths on the streets between Yonge Street and Mount Pleasant Road which are part of a Public Realm Move. Nearly all streets in the Apartment Neighbourhoods are 8.6m in width. There is a portion of Broadway Avenue east of Redpath Avenue which widens from 9.8m to 12.0m.

Some remaining east-west streets also have a wider roadway width. Merton Avenue has a width of 11.4m to 12.0m. The widest roadways in the Midtown area are the arterial roads of Eglinton Avenue East and Davisville Avenue, with a span of 12.8m to 13.0m.



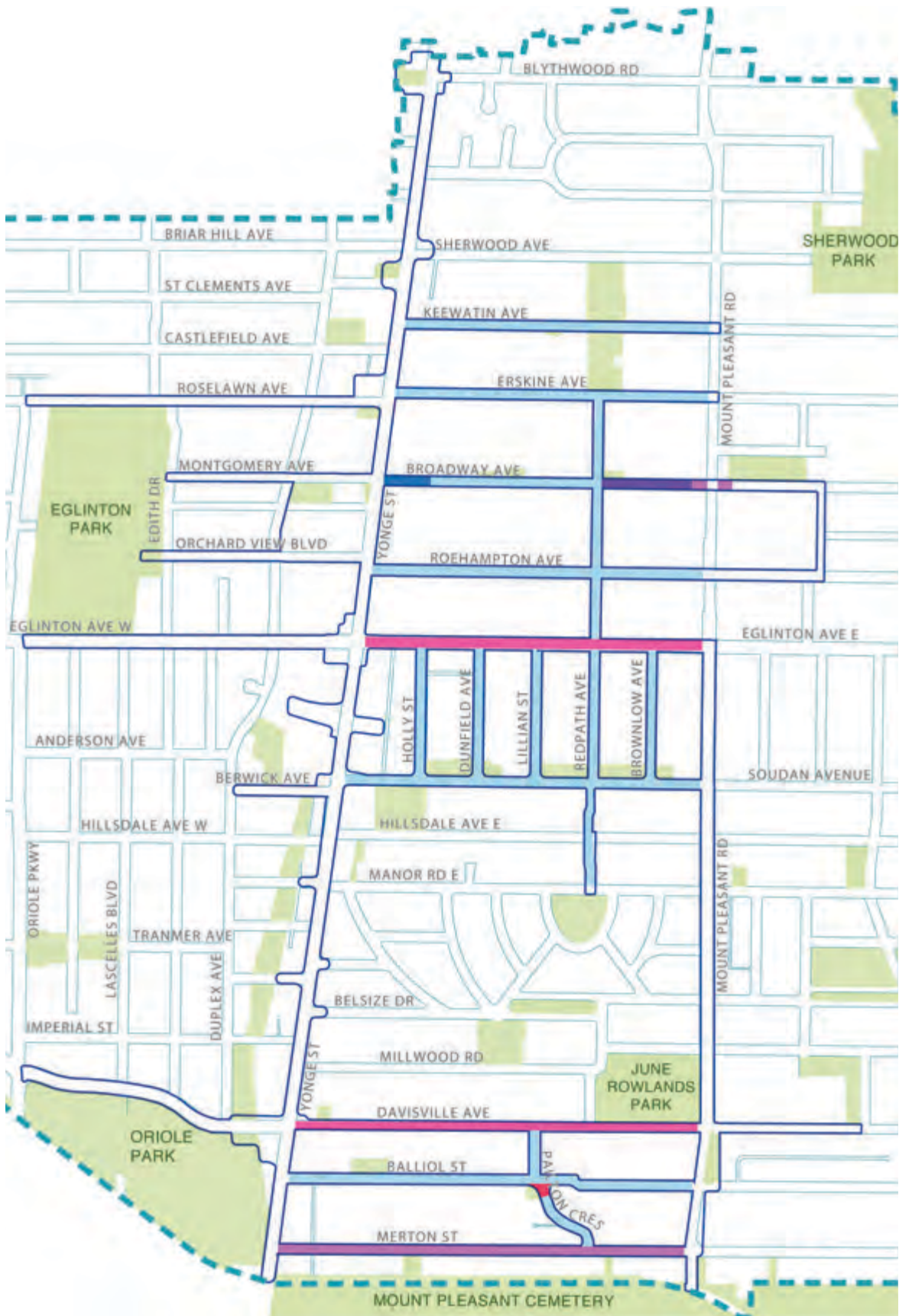


Figure 6 Map of existing roadway widths along the Public Realm Moves

Sidewalk Width

Along streets that are planned for improvements as a Public Realm Move, there is a range of existing sidewalk widths, as shown in Figure 7. Eglinton Avenue currently has the widest sidewalks, with over 4.0m on the north side and generally over 3.0m - 4.0m on the south side.

In the residential areas north of Eglinton Avenue, sidewalks are generally narrow with a width of 1.6m - 2.0m, narrowing further to 1.2m along Keewatin Avenue. For the areas south of Eglinton Avenue to Soudan Avenue, there is some local variation with wider sidewalks that are located closer to the Yonge-Eglinton intersection, such as sections of Holly Street and Dunfield Avenue. Most of the other north-south streets and Soudan Avenue fall within a range of 1.0m - 2.0m.

In the Davisville area, the widest sidewalks fall within a range of 2.6m - 3.0m and are concentrated along a stretch of Balliol Street from Yonge Street to Pailton Crescent. This is followed by the north side of Merton Street and a portion of Davisville Avenue where sidewalk widths are 2.1m - 2.5m. The remaining streets are generally 1.0m - 2.0m.

There are no existing sidewalks on the west side of Redpath Avenue running south of Soudan Avenue, and along part of Pailton Crescent.



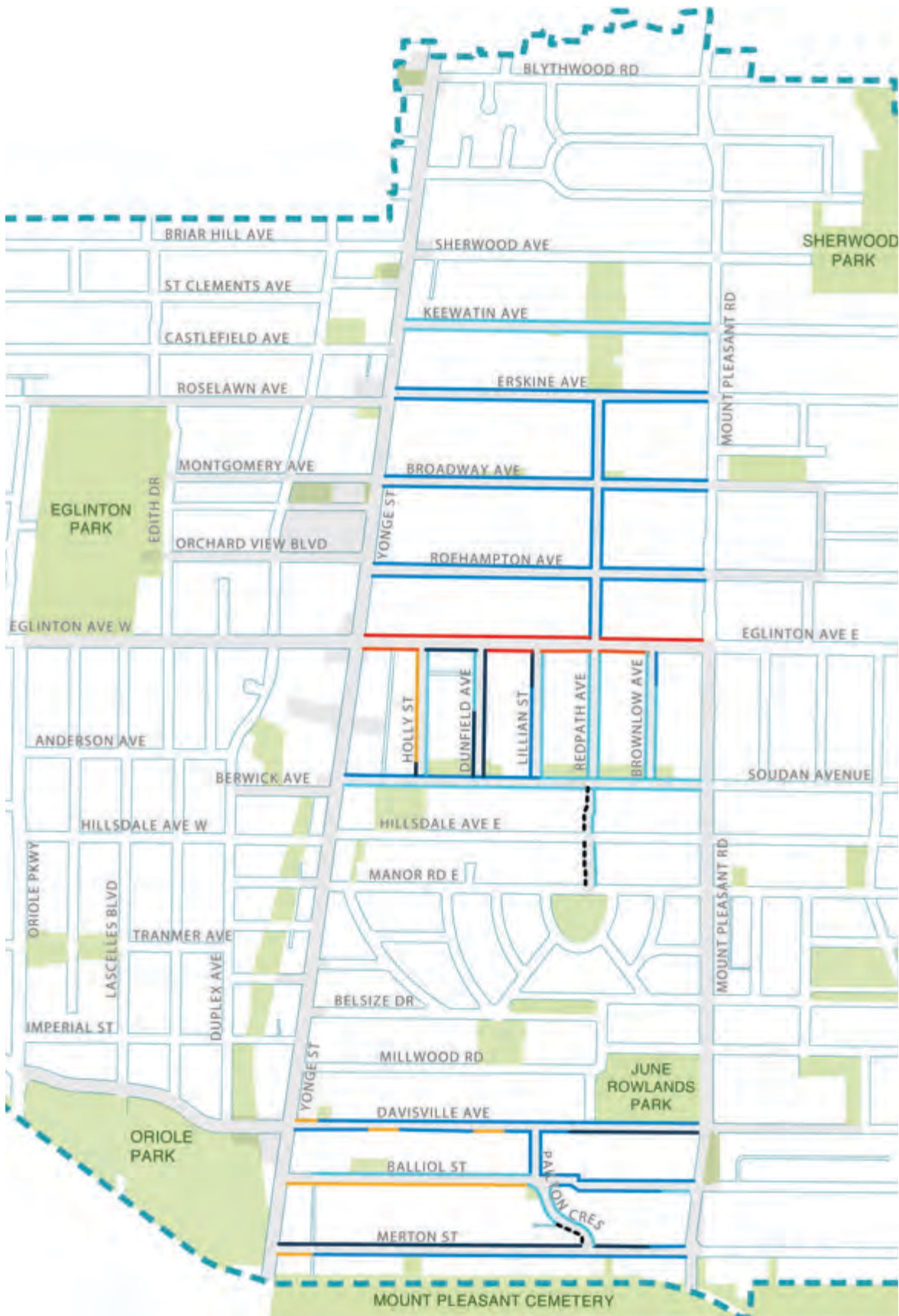


Figure 7 Map of existing sidewalk widths along the Public Realm Moves

On-street Parking

All of the streets where Public Realm Moves are planned provide on-street parking. Additionally, private parking driveways and boulevard parking commonly occur in the public ROW. For the purposes of this study, Figure 8 identifies the general locations and extent of the existing on-street and boulevard parking areas. The report does not take into consideration any temporary parking relocation or removal due to the on-going construction in Midtown.

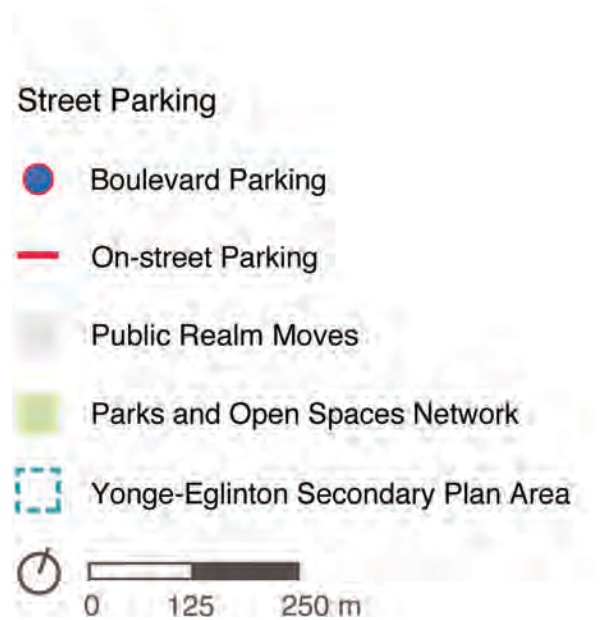




Figure 8 Map of on-street and boulevard parking along the Public Realm Moves

Bike Share Toronto Stations

Figure 9 shows the comprehensive bike share network in Midtown. There are 16 stations in total. Most of these stations are located along Yonge Street and Eglinton Avenue, with some in residential areas such as Merton Street and Roehampton Avenue. Each station has a capacity that ranges from 13 to 23 docks.

In addition to the existing bike share station network, there are plans to expand the number of stations in Midtown. Figure 9 identifies the approximate location of existing and desired bike share stations in Midtown based on proximity to transit, future developments, and user demand. The location of bike share stations is subject to change due to capital construction, property development, councillor direction, and programming like CafeTO. Where work, by the City or private developers, impacts station locations, project teams are advised to contact Bike Share Toronto and arrange to reinstall the station on site that is an equal or greater size.

Stations may be temporarily or permanently relocated as close as feasible if they are removed or cannot be installed in a desired location. When planning a station within the right-of-way, ensure the station placement complements public realm design, while allowing for tree planning, seating elements, and outdoor cafes.

Bike Share Toronto must be contacted early in the design phase to ensure the proposed station meets their size requirements and the local demand.

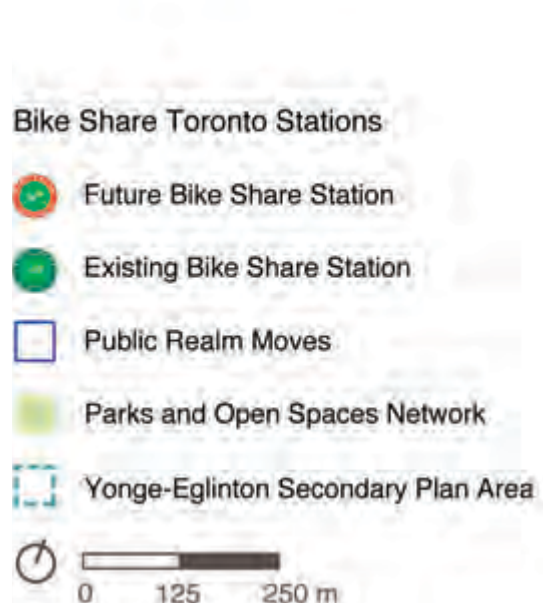




Figure 9 Map of existing and future Bike Share Toronto stations along and in close proximity to the Public Realm Moves

1.7 Recent Achievements

Since the adoption of the Midtown In Focus Plan in 2014 and the development of the Yonge-Eglinton Secondary Plan, some portions of the Public Realm Moves have been secured through development and detailed studies have been undertaken. Recent achievements completed to December 31st, 2021 include:

- Eglinton Green Line
 - Eglinton Green Line Public Realm and Landscape Standards adopted by Council in July 2021
- Park Street Loop
 - Numerous public realm improvements secured through the development review process, to be constructed by private development
- Yonge Street Squares and Yonge Street

Partial improvements to the Yonge Street Squares and Yonge Street streetscape, adjacent to developments as follows:

- Yonge and Eglinton Crossroads north-east corner square
- Yonge and Eglinton Crossroads north-west corner square
- Montgomery Square – part of Montgomery Avenue, south side, Helendale Avenue and Yonge Street, with partial mid-block connection
- Yonge Street and Soudan Avenue south square
- Yonge streetscape between Soudan Avenue and Hillside Avenue
- Glebe Road West, south side
- Belsize Drive Square, south side and Yonge streetscape between Belsize Drive and Millwood Road

- Additionally:
 - St. Clements Avenue Square, Roselawn Avenue Square, Soudan Avenue Square and Manor Road Square have been secured for construction by private development
 - TTC McBrien Building (1900 Yonge Street) and Davisville Yard Feasibility Study has been initiated

2.0

Implementation Toolbox

Overview

Section 37 and Community Benefits Charge

Development Charges

Overview

This section provides a summary of the financial tools available to implement this Strategy. Some of these tools are broadly administered by the City such as property taxes and Development Charges. There are also tools that are specific to the development process and that are provided to the City under the authority of the Planning Act and City of Toronto Act. These include height and density bonusing (Section 37 of the Planning Act) and site plan control (Section 114 of the City of Toronto Act). Site plan control allows to secure public realm improvements on-site and on the abutting street as a base requirement at the owner's expense. These tools will be modified by the introduction of the Community Benefits Charge (CBC) framework, to take effect in 2022. Finally, there is potential to collaborate with other parties. This includes a philanthropic approach with donors, and leveraging a variety of cost-sharing arrangements with local third parties, such as landowners and BIAs.

Section 37 and Community Benefits Charge

Through Provincial Bill 197, Section 37 will be replaced with a new Community Benefits Charge (CBC) regime. Once the City adopts a CBC by-law in 2022, fees paid by new development for a specific list of growth-related capital facilities and services will be levied on new developments with 5+ storeys and 10+ residential units, to be capped at 4% of the land value. The CBC may be used to fund projects such as public art, park improvements and streetscape improvements. However, they cannot fund the same project costs as Development Charges or Parkland Dedication. Municipalities must prepare a CBC strategy that identifies the facilities, services and matters that will be funded before passing the CBC By-law.

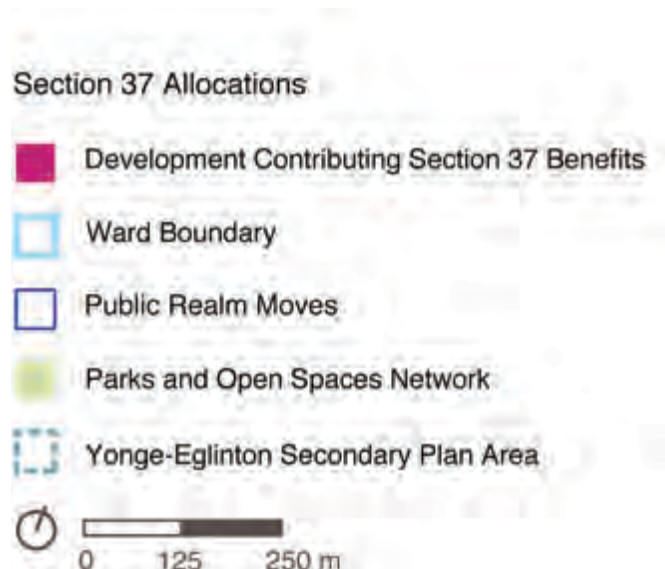
Under Section 37, the City had the ability to secure benefits related to the approval of increased height and density development applications, greater than 10,000 sq.m. in area, to support additional growth in accordance with Policy 5.1.1 of the Official Plan.

The City has already collected funds and in-kind contributions through recently approved developments, which can be applied to public realm and streetscaping initiatives in the vicinity of the proposal.

This Strategy identifies existing Section 37 funds collected from recent development that can be directed toward the delivery of the Midtown Public Realm Moves, as well as current proposals under review that can contribute further funding and implementation of their objectives.

Development Charges (DCs)

Development Charges are charges levied on development by a municipality to ensure the provision of growth-related infrastructure, which is primarily delivered through the municipality's capital programs. While streetscaping initiatives are eligible for DCs, there are no Midtown public realm projects identified in the current DC capital list.



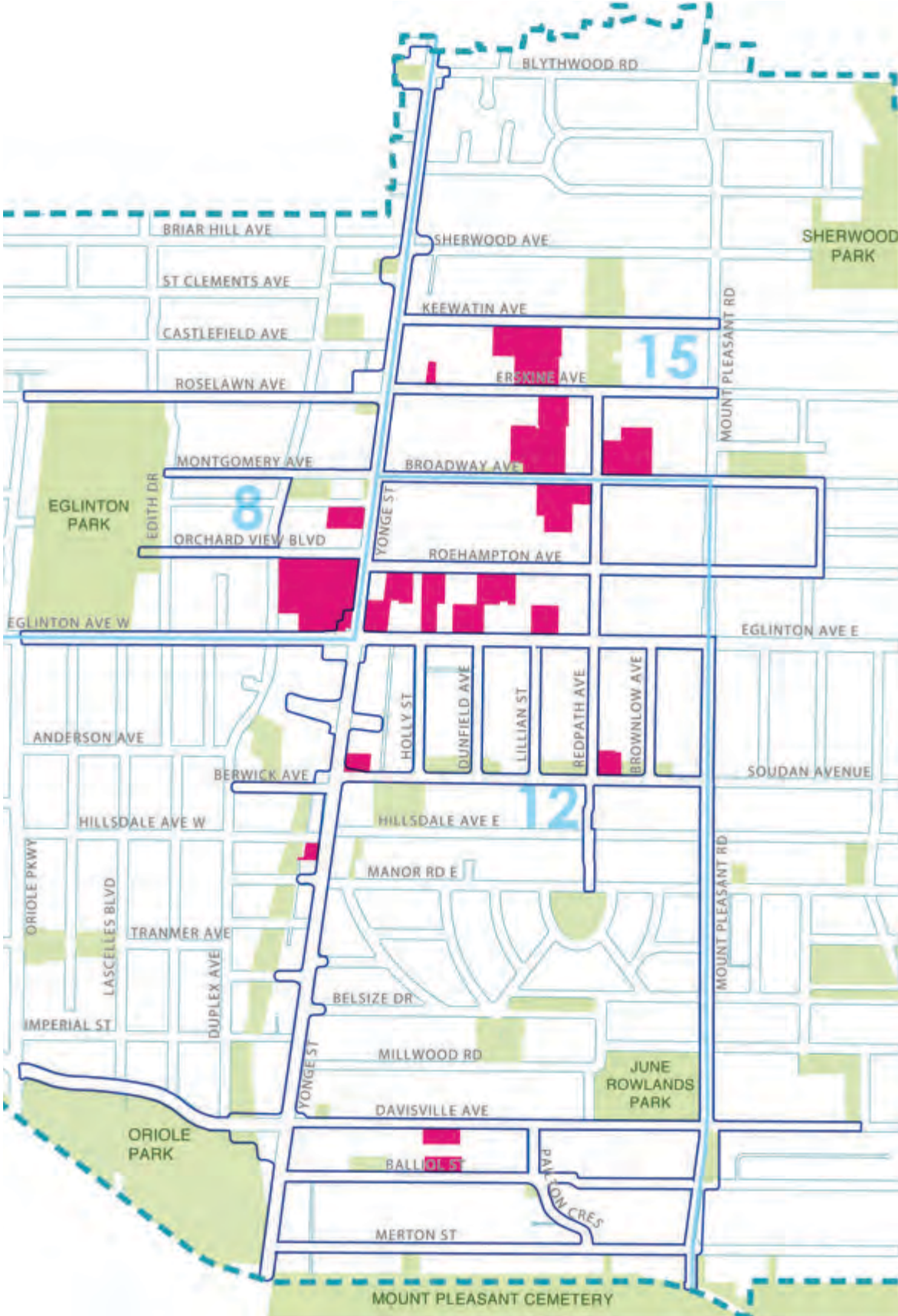


Figure 10 Map of Developments contributing Section 37 benefits along the Public Realm Moves

Other Implementation Tools

Additional financial tools for delivering parks and public realm improvements, outlined in the Midtown Infrastructure Implementation Strategy, are summarized below:

Parkland Dedication is legislated by Section 42 of the Planning Act to allow the City to require dedication of parkland associated with the development and redevelopment of land. Parkland may be provided on the development site, off-site, or through cash-in-lieu contributions. Dedication rates are dependent upon the land use, site size and location, as outlined in the Planning Act and the City's implementing Parkland Dedication By-law.

Capital Programs are funded through DCs and property taxes to maintain, improve and expand municipal infrastructure. The City's capital budget includes funding for streets, parks, recreation facilities, libraries, watermains, stormwater and sewer servicing, among other infrastructure types. Projects are prioritized city wide and generally planned over a five-year or ten-year time horizon.

Partnerships can be sought with owners of open space assets to provide community access to green space; with local Business Improvement Areas (BIAs) through cost-sharing agreements to deliver streetscaping initiatives; with school boards through shared use agreements to provide access to parks and school yards; and with development through the establishment of POPS (Privately Owned, Publicly-accessible Spaces), among other possibilities.

Philanthropy refers to charitable actions, often in the form of donations, towards improving public welfare. The source and scale of donations may range across the city, and Toronto has been fortunate to receive private funding to support local park and public realm improvements.

Property Taxes are used to fund non-growth related capital projects, such as those related to state of good repair, as well as ongoing operations.

The Midtown Infrastructure Implementation Strategy provides a coordinated approach to prioritizing and phasing the various infrastructure projects identified for the YESP area over the near, medium and long term. This inventory of projects will help inform ongoing implementation through the City's future Capital Budget planning processes, as well as DC By-law reviews at regular intervals over the life of the Plan. Detailed infrastructure timing and funding will need to be prioritized against other city-wide priorities, financial and resource capacity to deliver these works.

For reliability at the time of implementation, infrastructure costs will be determined at the time of detailed design. To optimize the efficiency and cost-effectiveness of the provision of infrastructure, multiple projects will be considered for bundling, where feasible.

3.0

Considerations for Public Realm Phasing

Overview

Development Activity

Soft Sites, Heritage Properties and Opportunities and Constraints

Overview

Wherever feasible, public realm improvements will be delivered through development on immediately adjacent properties, to the extent of the development site, through the site plan approval process. Public Realm Moves that are not delivered through development will be evaluated and prioritized by Transportation Services as part of ongoing capital program planning. As described in the Midtown Infrastructure Implementation Strategy, the State Of Good Repair (SOGR) program is one of the main channels for project delivery and plays a significant role in the prioritization of projects and bundling of aligned initiatives.

In addition, City Planning’s Civic Improvement Program is a modest, but high value-added capital program that significantly enhances ongoing projects of other Divisions, Business Improvement Area initiatives and private city-building activities.

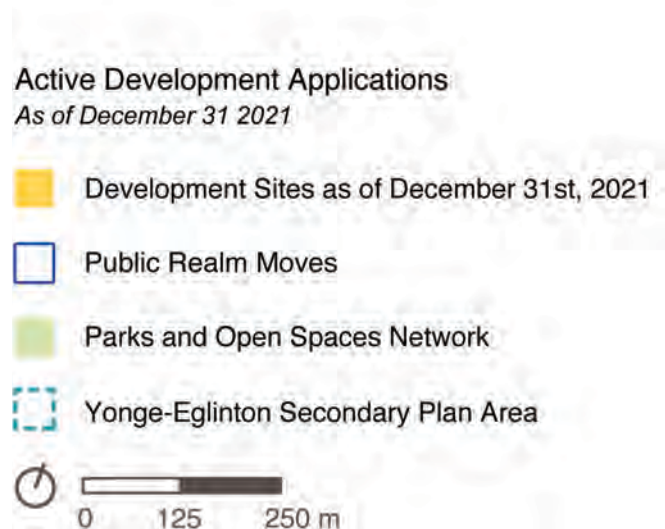
Development Activity

In addition to capital improvements, an upgraded public realm will be implemented as part of proposed private development. Midtown is one of Toronto’s fastest-developing areas, representing around one percent of Toronto’s land area while accounting for 10 percent of the City’s development applications from 2014 to 2018. As of December 31st 2021, there are 59 active development applications in this area, with 37 that are in the review process and 22 that are approved for construction.

This high volume of development applications results in increased pedestrian, cycling and vehicular movement.

A large number of proposals are located next to or near Public Realm Moves. Areas with the highest concentration of development activity will see earlier, though incremental, implementation of associated Public Realm Moves. The pattern of recent development applications generally propose mixed-use towers along major streets and residential buildings further into the neighbourhoods. With numerous proposed developments, often along the same street, a coordinated and expanded public realm will provide a safe, integrated outdoor environment for all users.

Figure 11 shows a map of current development sites. This map shows locations with proposed applications that are under review, applications approved by City Council or the Ontario Land Tribunal, and applications where buildings are under construction.



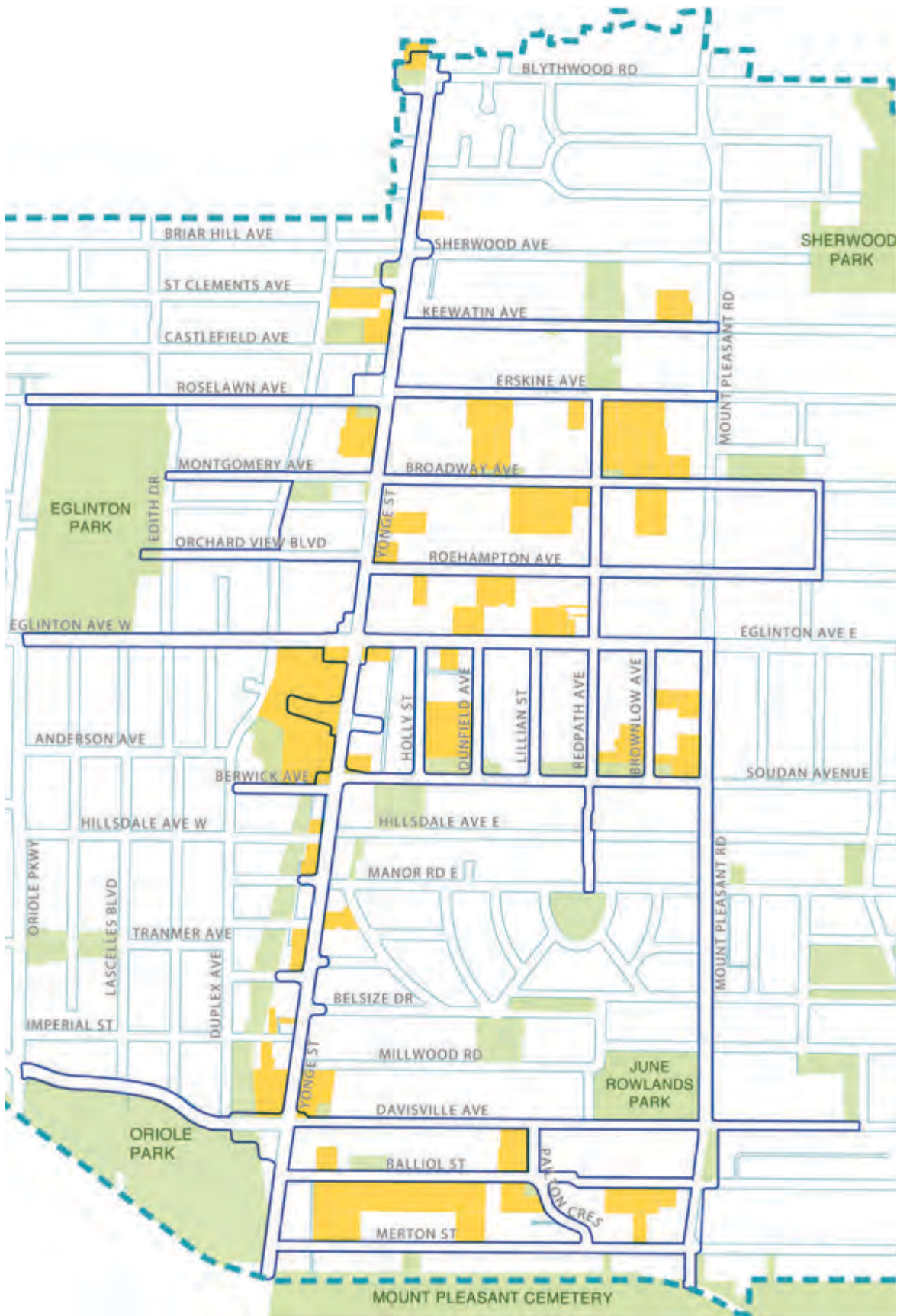


Figure 11 Map of current development sites along the Public Realm Moves

Soft Sites, Heritage Properties and Opportunities and Constraints

A list of “soft sites” were identified as part of the 2018 Yonge-Eglinton Built Form Study. Soft sites are those assessed as having future development potential, based on research on market conditions, development feasibility (such as existing land uses and potential lot consolidation) and ongoing development activity in Midtown. The list of soft sites is not exhaustive and does not account for development proposals where growth is not anticipated in accordance with Yonge-Eglinton Secondary Plan policies.

Midtown contains a diverse range of heritage properties that are located within the Public Realm Moves. Most of these properties are low-rise buildings that accommodate local retail and service shops, generally located along the historic main streets of Yonge Street and Mount Pleasant Road. There are also some larger landmarks in the area with a heritage designation, such as John Fisher Junior Public School and North Toronto Collegiate Institute to the north of Eglinton Avenue, and Mount Pleasant Cemetery that forms the south edge of Midtown.

In cases where a heritage property is a part of a soft site, minimum setbacks and landscape improvements within the setback may not be achievable in front of an existing building. The rest of the public realm within the City’s ROW immediately adjacent to both constrained and unconstrained soft sites will provide opportunities for improvements.

In the Public Realm Moves section of this report, each Public Realm Move includes an analysis of the current development context for each street. This analysis highlights sites that may experience future infill development or redevelopment and represents where development can implement public realm improvements along segments of the street near their sites.

In this document, these locations are identified as Potential Development Sites, which include all soft sites, as well as any other site that has not experienced redevelopment or infill development activity since 2010. These sites are also not constrained by other factors such as heritage status. This analysis is used to provide a snapshot of where potential and unconstrained development activity may provide an opportunity to advance public realm improvements.

Conversely, constrained sites may be limited in being able to fully implement public realm improvements, as they are not expected to redevelop in the near future or may be unable to provide a full landscape setback due to heritage designations. The streetscapes along these sites will be enhanced through nearby development where possible, and as part of City capital projects.

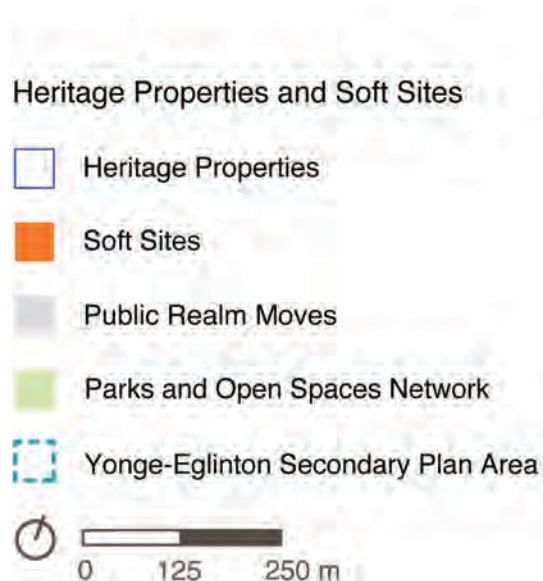




Figure 12 Map of heritage properties and soft sites along the Public Realm Moves

4.0

Public Realm Elements

Public Realm Elements
Landscape Setbacks
Streetscape Improvements

Public Realm Elements

"Public Realm" refers to publicly accessible open space, including parks, streets, plazas, open spaces, walkways, laneways and POPS (Privately-Owned Publicly Accessible Spaces). Elements of public realm include: sidewalks, street trees, planters and green infrastructure, street furniture, bikeways and bike parking areas. An enhanced public realm in Midtown will create a safe and pleasant pedestrian experience by providing wider sidewalks, adding designated bikeways and improving planting conditions for trees.

1.0 Sidewalks

All existing sidewalks should be widened through the course of new development and future City capital project improvements. To support the growing population, minimum 2.5m wide sidewalks should be provided. In more prominent areas, such as Davisville Community Street, Park Street Loop and Eglinton Green Line, the pedestrian clearway should be further widened to allow for comfortable pedestrian activity and circulation.

2.0 Green Boulevards and Open Planter Areas

To create a safe and pleasant pedestrian experience, large planted areas should be incorporated to provide separation between the sidewalk and vehicular traffic. Green boulevards should be designed to allow for stormwater run-off and be planted with resilient native plant materials able to thrive in an urban habitat. For the latest standards on rain garden design, refer to the *Green Streets Guidelines*.



Figure 13 Yonge Street



Figure 14 York Street and Lake Shore Boulevard West



Figure 15 Farquhars Lane and The Esplanade



Figure 16 Ossington Avenue

3.0 Street Trees

To continue growing Midtown's tree canopy, large shade casting street trees will be planted in the open planter areas along all Public Realm Moves. The interface between trees and utilities and provision of adequate soil volume (min. 30.cu.m/ tree) will be coordinated to create a suitable environment for tree growth.

In denser pedestrian environments tree grates can be incorporated, supported by a soil cell system to provide required soil volumes.



Figure 17 Montreal, QC

4.0 Bikeways

Bikeways will be integrated into the public realm along Broadway Avenue and Davisville Community Street. The width of the lanes will vary based on the location and surrounding context.



Figure 18 Raised cycle lanes on Bloor Street West

5.0 Street Furniture

City of Toronto street furniture, including benches, trash receptacles, bike rings and information pillars should be incorporated into the public realm between open planted areas. On Davisville Avenue, transit shelters should be located to minimize conflicts between pedestrians and transit users. For the standards, refer to *City of Toronto Streetscape Manual User Guide*. The design of Yonge Street Squares should incorporate street furniture iconic to the Midtown Yonge BIA and Uptown Yonge BIA, where possible.



Figure 19 Yonge Street

6.0 Bump-outs

Sidewalk bump-outs will be added where possible in front of all parks, at specific intersections and at mid-block raised crosswalks. The vehicular roadway will be narrowed to allow for bump-outs. Where possible, bump-outs should be designed as rain gardens with lush native plantings. For the latest standards on rain garden design, refer to the *Green Streets Guidelines*.



Figure 20 Bump-out at Gerrard and Mutual Streets

7.0 Raised Crosswalks

Where mid-block connections are incorporated, a raised crosswalk will be considered to provide a safe pedestrian experience and act as a traffic calming element. Raised crosswalks can be designed to include a planted bump-out with integrated rain garden. For latest standards, refer to 11.0 *Raised Crosswalk and Intersection Guideline*.

8.0 Sidewalk Transition Zones

In instances where existing and proposed sidewalks do not align, a sidewalk transition zone should be included to create a continuous pedestrian experience. Any deflection in a sidewalk must not exceed 20 degrees.

9.0 Bike Share Toronto Stations

Where suitable, Toronto Bike Share Stations should be integrated into the public realm design. The stations should be highly visible and easily accessible. When located in the public boulevard, stations should not obstruct the minimum pedestrian clearway. The stations should not replace trees and planted areas, but be integrated in the overall streetscape design. For streets with proposed bump-outs, stations should be incorporated into the bump-out design where applicable and possible.

10.0 Utility locations

Existing above ground utilities, such as hydro poles, fire hydrants, vaults, etc. located immediately adjacent to or on the existing sidewalks, obstructing pedestrian movement, should be relocated away from pedestrian circulation to maximize sidewalk width as part of public realm improvements. Undergrounding of hydro transmission wires should occur for the final condition for all public realm moves, where possible.



Figure 21 Raised Crosswalk at St. George Street



Figure 22 Sidewalk Transition at Stewart Street



Figure 23 Bike Share Station at Yonge Street and Bloor Street



Figure 24 Utilities locations on Broadway Avenue

Landscape Setbacks

The setbacks are lush landscaped areas that act as an extension of the public sidewalk with amenities for public and private users. Forecourts featuring decorative paving, gardens, seating, lighting and sometimes public art or other architectural or landscape features can be incorporated into the setbacks. The landscape setbacks will be planted with high-branching deciduous trees and understory plantings. All landscape setbacks will be designed as welcoming spaces for the community and passers-by. The use of fencing is discouraged.

1.0 Underground Facilities

The location and design of underground facilities, such as parking, on properties where landscape setbacks are required will provide a minimum of 1.5m soil depth to provide healthy tree growth. Above grade planters will not be acceptable.

2.0 Site Servicing

Transformers or gas regulators will be screened with architectural features. No siamese connections, transformers or gas regulators will be located within the landscape setback. All proposed visible, in-ground utilities, such as exhaust vents will be located away from pedestrian circulation areas and screened with vegetation where possible. Additionally, all in-ground utilities such as access holes, chambers and sampling ports, etc. will be located away from pedestrian circulation routes. These utilities are to be located in the driveways away from the landscape setback.

3.0 Mid-block connection termini

Where mid-block connections are proposed, a generous forecourt will be provided where the mid-block connection meets the public realm. It should include seating, bicycle parking, lighting and design features which identify the mid-block walkway to ensure that it is legible as a publicly accessible space and part of a connected pedestrian network. The mid-block connection will be complementary to the termini and will be designed as a 6.0m wide linear open space, including a 3.0m wide pedestrian clearway. The remaining space will incorporate similar paving, lighting, planting areas, bike parking and furniture elements, while providing legibility as a publicly accessible space.



Figure 25 Duplex Avenue



Figure 26 Vent Screening, Davenport Road



Figure 27 Transformer Screening, Sorauen Avenue



Figure 28 Mid-block connection, Broadway Avenue

Landscape Setbacks

Figure 29 illustrates the location and widths of the proposed landscape setbacks along the Public Realm Moves. The setbacks will provide publicly accessible, green, open spaces for the community to enjoy. Larger setbacks, between 6.0-7.5m, will be programmed to include children's play elements, public art, lush, planted areas, such as butterfly gardens and rain gardens as well as seating elements. Smaller setbacks between 3.0-5.0m, will prioritize green open spaces with trees, native planting, rain gardens and ample seating facing the streets. The setbacks will extend from the property line to the building face and will act as an extension of the public realm.

For 12.0m setback guidelines along Eglinton Avenue East, please refer to Appendix B: The Eglinton Green Line Landscape and Public Realm Standards.



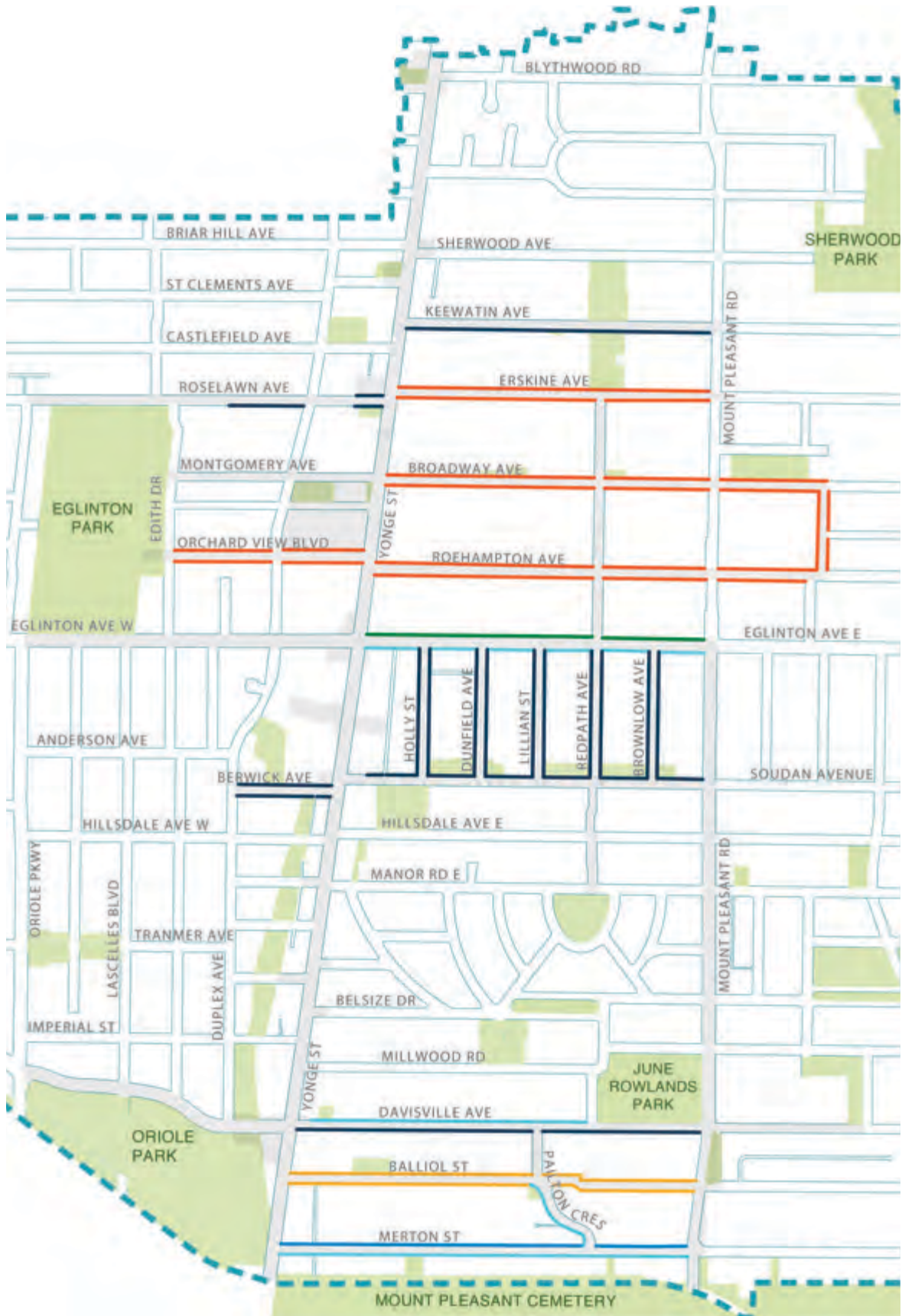
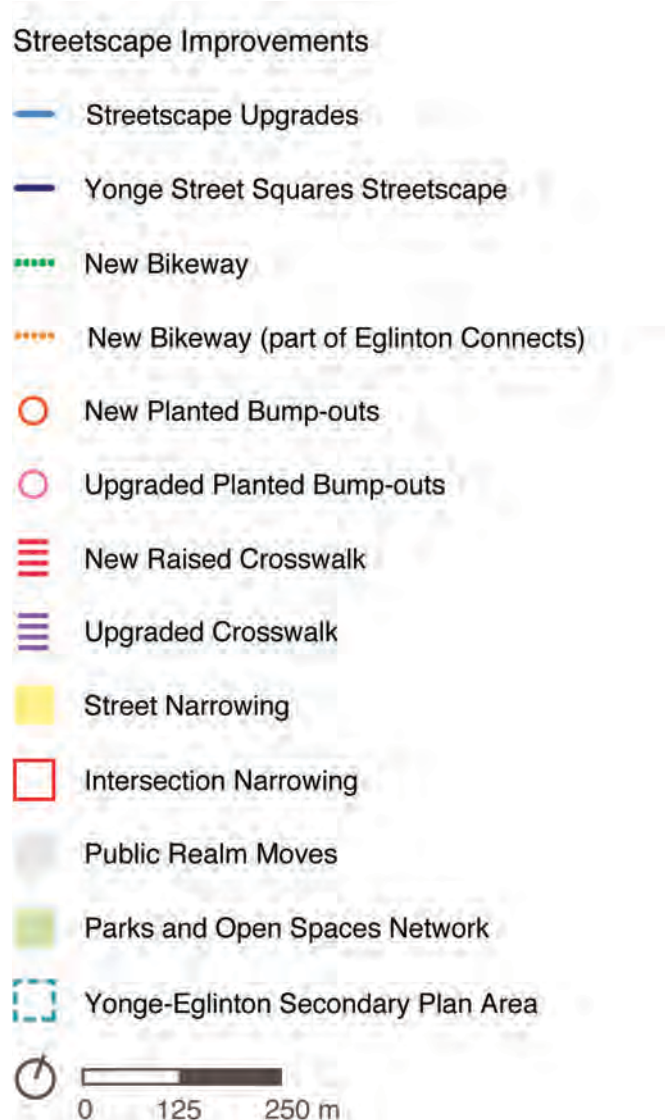


Figure 29 Map of landscape setbacks along the Public Realm Moves

Streetscape Improvements

Figure 30 shows a summary of streetscape improvements along the Public Realm Moves. Some of the streets will require general improvements, such as new sidewalks, sidewalk widening or bump-outs, while others, such as Broadway Avenue and Davisville Avenue, will include new bikeways and roadway and intersection narrowing. Additional improvements may consist of rain gardens, tree planting and new mid-block crosswalks.



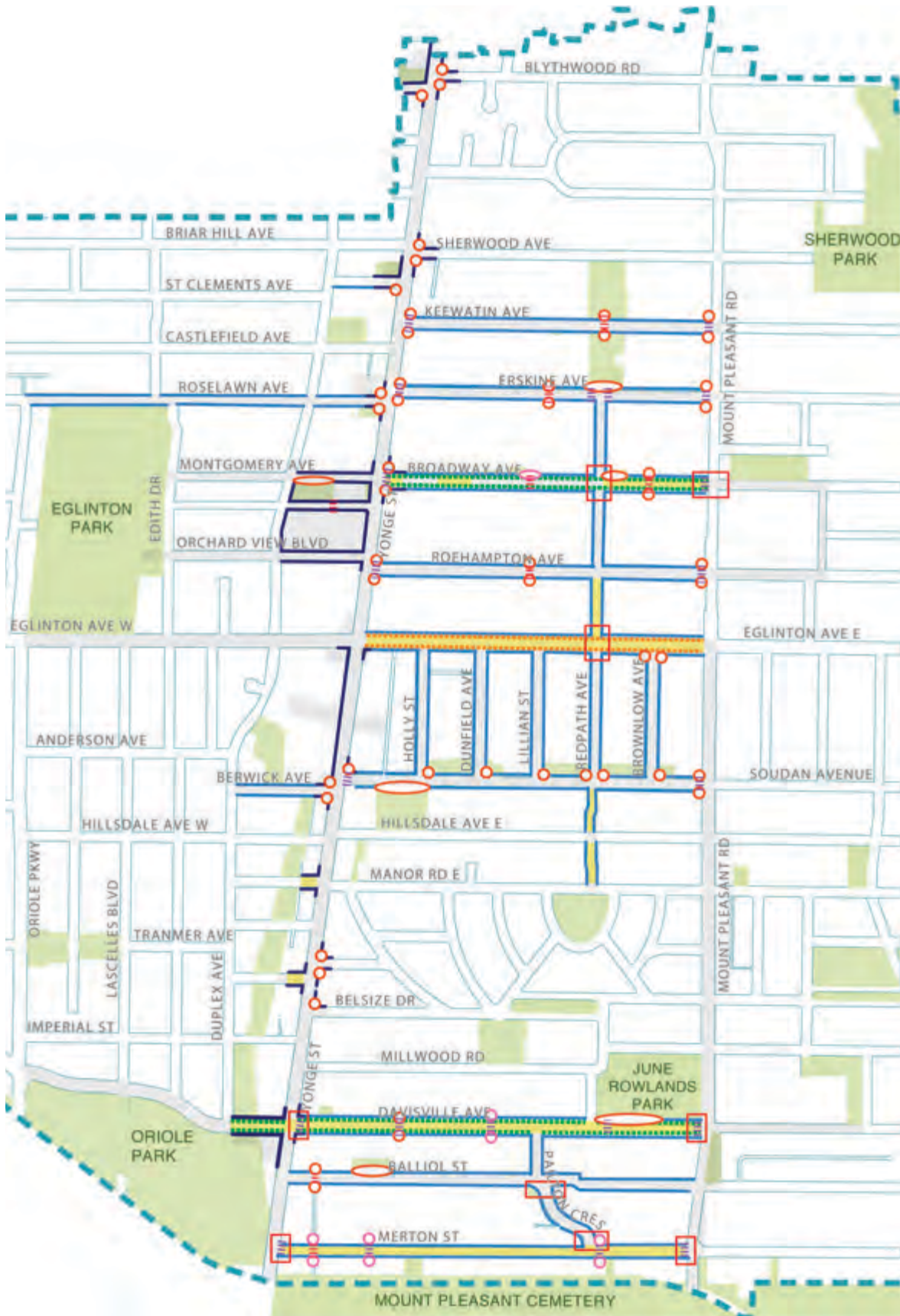


Figure 30 Map of streetscape improvements along the Public Realm Moves

4.1

Eglinton Green Line

Official Plan Policy Direction
Overview

Overview

Policy Direction

The Yonge-Eglinton Secondary Plan identifies the Eglinton Green Line as a Public Realm Move and provides detailed direction in the following policies:

3.2.2 Eglinton Green Line

- a. The Eglinton Green Line will be a major linear, publicly-accessible green open space on Eglinton Avenue, extending from Eglinton Park to Mount Pleasant Road.
- b. All buildings on the north side of Eglinton Avenue between Yonge Street and Mount Pleasant Road will be reasonably set back at grade and above established grade from the property line adjacent to Eglinton Avenue. The setback will be provided to establish high-quality pedestrian and landscaped publicly-accessible open spaces, plazas and forecourts. Space for community gatherings, patios and public art will be encouraged within the Eglinton Green Line.
- c. The location and design of underground facilities, such as parking, on properties abutting the north side of Eglinton Avenue East between Yonge Street and Mount Pleasant Road will provide sufficient space and soil depth to establish and maintain a permanent, high-branching tree canopy.

- d. On Eglinton Avenue West between Yonge Street and Eglinton Park, continuous landscaping that includes high-branching trees and plantings will be provided in the right-of-way and in the setbacks.

As identified in the Yonge-Eglinton Secondary Plan, the Eglinton Green Line is a major linear, publicly-accessible green space on Eglinton Avenue, extending from Eglinton Park to Mount Pleasant Road. In the segment east of Yonge Street, this Public Realm Move will dramatically expand the open space network of Midtown through the provision of a 12.0m building setback on all properties along the north side of the street. This is a new typology of linear open space, mixing green infrastructure with urban culture, commerce and vibrant street life. The significant setback will allow for the establishment of high-quality, pedestrian-focused, and landscaped publicly-accessible open spaces, plazas, forecourts and spaces for community gatherings, patios and public art.

The Eglinton Green Line is a long-term vision to create a major linear, publicly accessible open space to be delivered site by site, with each new development on the north side of Eglinton setting



Figure 31 From Eglinton Green Line Landscape and Public Realm Standards

its building back 12.0m from the property line. The space of each setback will be designed, constructed and maintained by the developer to both enhance the development itself and to fit within a larger pattern of open spaces which together provide an identity and focus for residents, workers and visitors to the district. The design and programming of these spaces will support public life throughout the year. Each site and its associated open space will contribute to the evolution and ultimate realization of the complete Eglinton Green Line. The goal over time is to create a connected whole, linking this varied and diverse open space.

The location and design of underground facilities, such as parking, will provide sufficient space and soil depth to establish and maintain a permanent, high-branching tree canopy.

In the segment west of Yonge Street, the public realm elements of the Eglinton Green Line will be implemented through the construction of the Eglinton Crosstown, a new Light Rapid Transit service that delivers higher order transit.

The Eglinton Green Line Landscape and Public Realm Standards, for the portion between Yonge Street and Mount Pleasant Road, are included in Appendix B.

SECTION 4.1

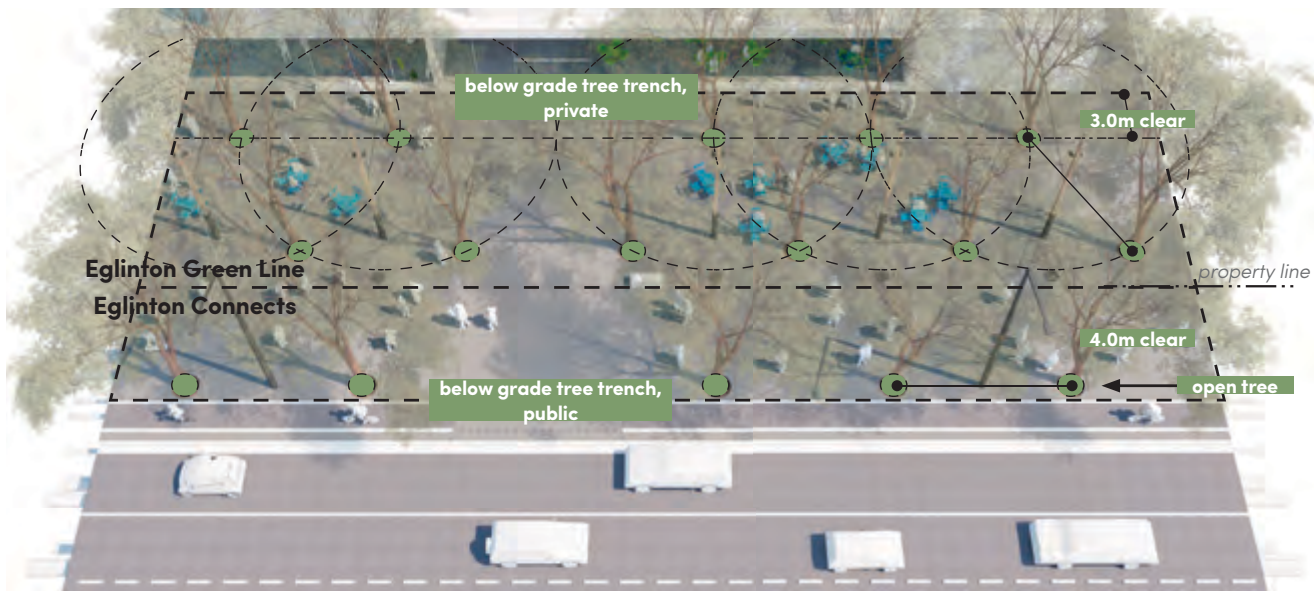


Figure 32 Planting arrangement diagram from Eglinton Green Line Landscape and Public Realm Standards

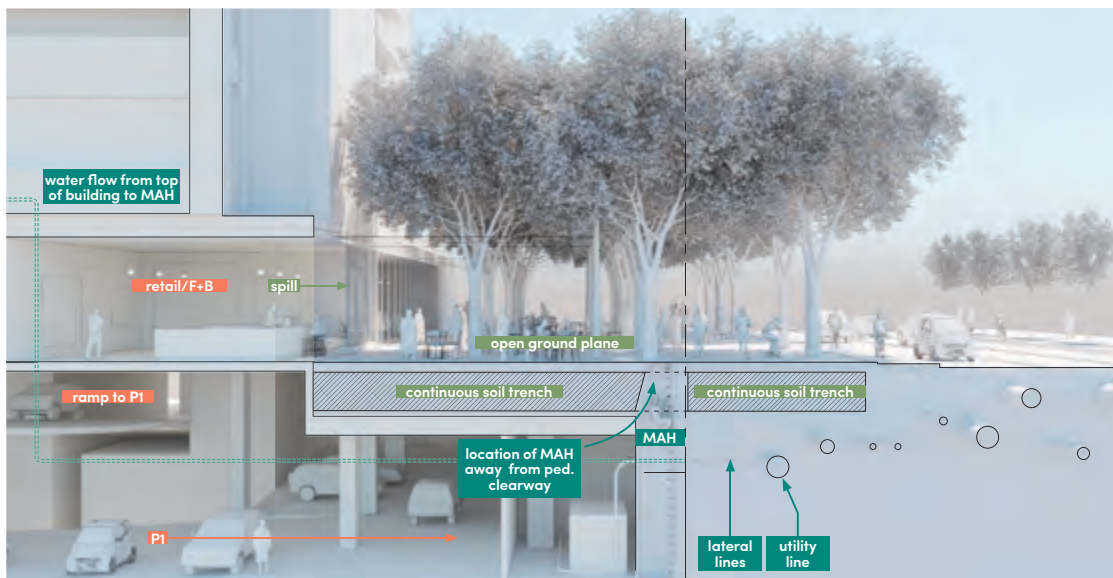


Figure 33 Conceptual section from Eglinton Green Line Landscape and Public Realm Standards

4.2

Park Street Loop

- Overview
- Background Analysis
- Park Street Loop Concept
- Final Condition Recommendations
- Design Guidelines
- Implementation

Overview

The Park Street Loop links a number of existing recreational and community amenities with its multi-purpose green promenade. Anchored by Eglinton Park to the west, passing through Montgomery Square and alongside the Northern District Library, and integrating two institutional sports grounds at North Toronto Collegiate Institute and Northern Secondary School, the Park Street Loop spans a 6-block cross-section of Midtown to create a 3-kilometre linear promenade.

The Park Street Loop, along Broadway Avenue and Roehampton Avenue, requires the integration of private front yard open spaces of adjacent properties with the streetscape to provide an extension of the public realm. The development of neighbourhood amenities such as play structures, public art, community gardens and shady green spaces for seating and gathering adjacent to the multi-use promenade is encouraged to promote social and recreational activities. West of Yonge Street, on Orchard View Boulevard and Montgomery Avenue, elements of the Park Street Loop will be carried over to Eglinton Park on a smaller scale, by employing consistent landscape and surface treatments and by extending large scale tree planting into the abutting private gardens.

Policy Direction

The Yonge-Eglinton Secondary Plan provides the following detailed policy direction for the Park Street Loop Public Realm Move:

3.4.5 Park Street Loop

- a. The Park Street Loop will be designed as a publicly-accessible, multi-purpose green promenade with wide pedestrian clearways, cycling facilities and landscaping that provides green linkages connecting Eglinton Park to community amenities and open spaces in neighbourhoods to the east.
- b. On Broadway Avenue and Roehampton Avenue between Yonge Street and Rawlinson Avenue and on Orchard View Avenue between Duplex Avenue and Yonge Street, all buildings will be reasonably set back at grade and above established grade from the property line.
- c. The location and design of underground facilities, such as parking, on properties abutting the Park Street Loop will provide sufficient space and soil depth to establish and maintain a permanent, high-branching tree canopy.

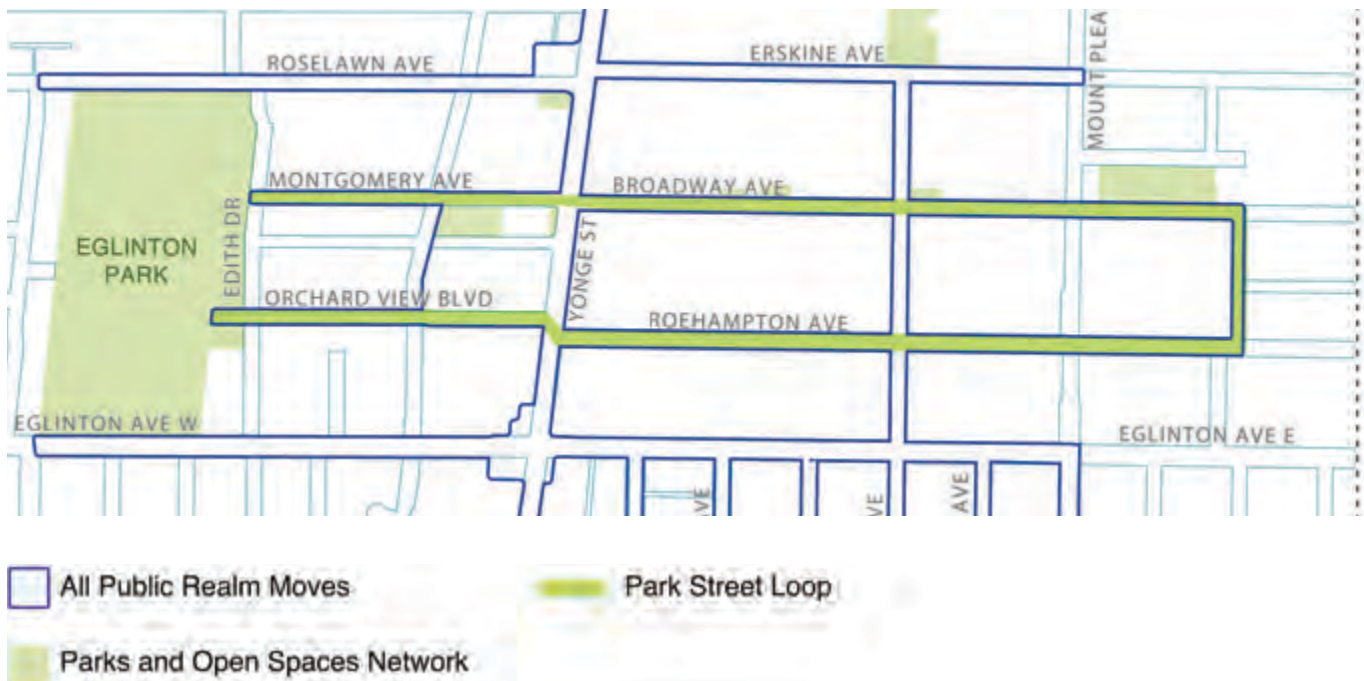


Figure 34 Map showing the location of the Park Street Loop Public Realm Move

Cycling Network Plan

The Cycling Network Plan identifies Broadway Avenue as part of the City's cycling network. Broadway Avenue is designated as a signed route from Eglinton Park throughout the length of the Park Street Loop and continuing to Brentcliffe Road. A signed route has wayfinding signage for cyclists on low-volume streets, but is otherwise identical to streets without cycling infrastructure. Cyclists continue to travel in mixed traffic with vehicles.

The Park Street Loop concept proposes the wayfinding signage to be upgraded for new dedicated bikeways on this segment of Broadway Avenue as the corridor redevelops. The revamped street will introduce an integrated bikeway as part of the promenade, which will separate cyclists from vehicular traffic and support a safer cycling network.

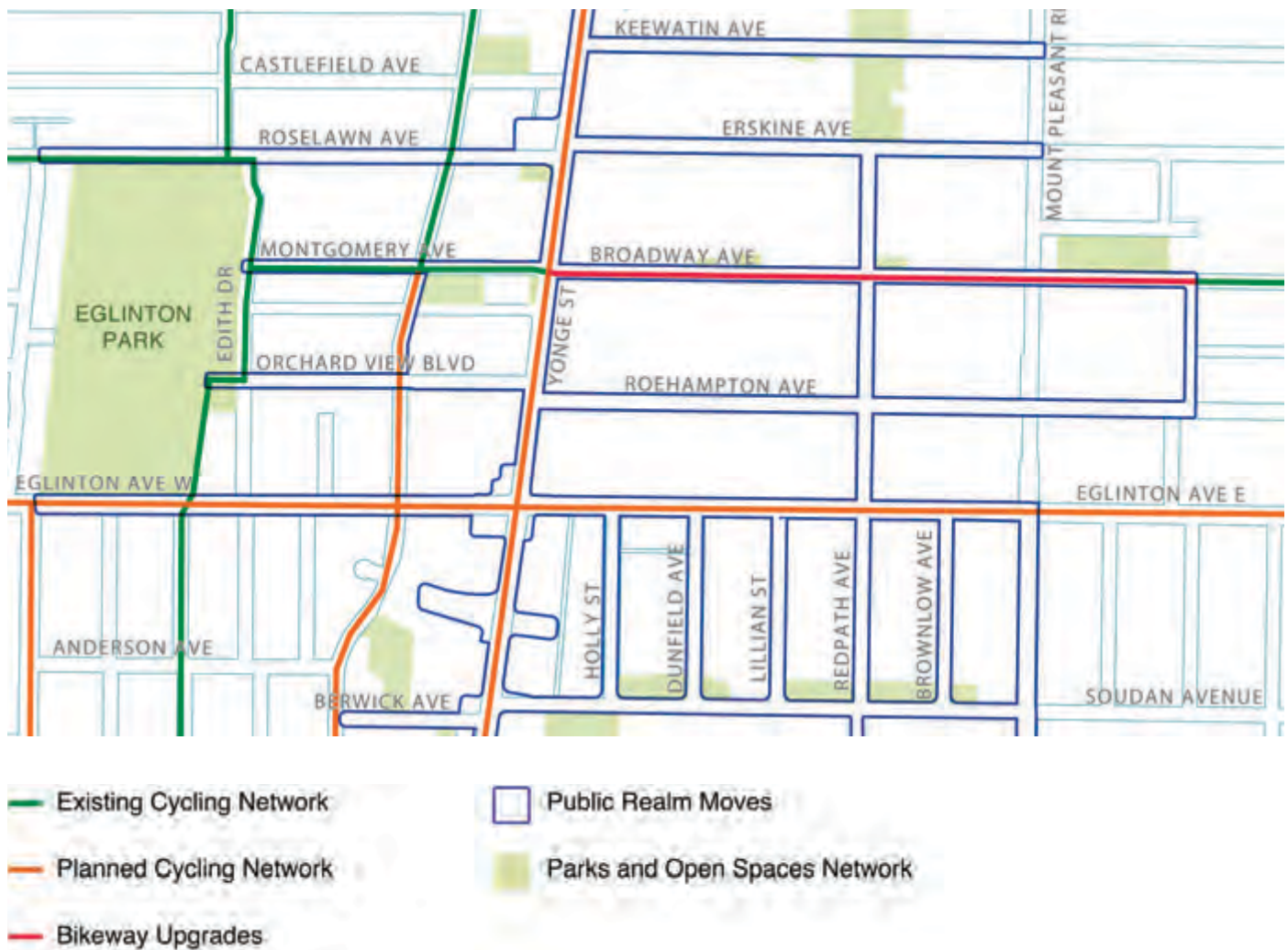


Figure 35 Map of the Cycling Network Plan within the Park Street Loop area

Background Analysis

Existing Street Cross-Sections

Existing Condition along Roehampton Avenue

- 22.0m right-of-way (ROW)
- 8.6m vehicular ROW, including 2 vehicular travel lanes and a parking lane
- Two-way street with parking on the south side of the street
- Substandard pedestrian clearway on narrow sidewalks on both sides of the street
- Pedestrian north-south and east-west crosswalks at the intersections with Yonge Street and Mount Pleasant Road

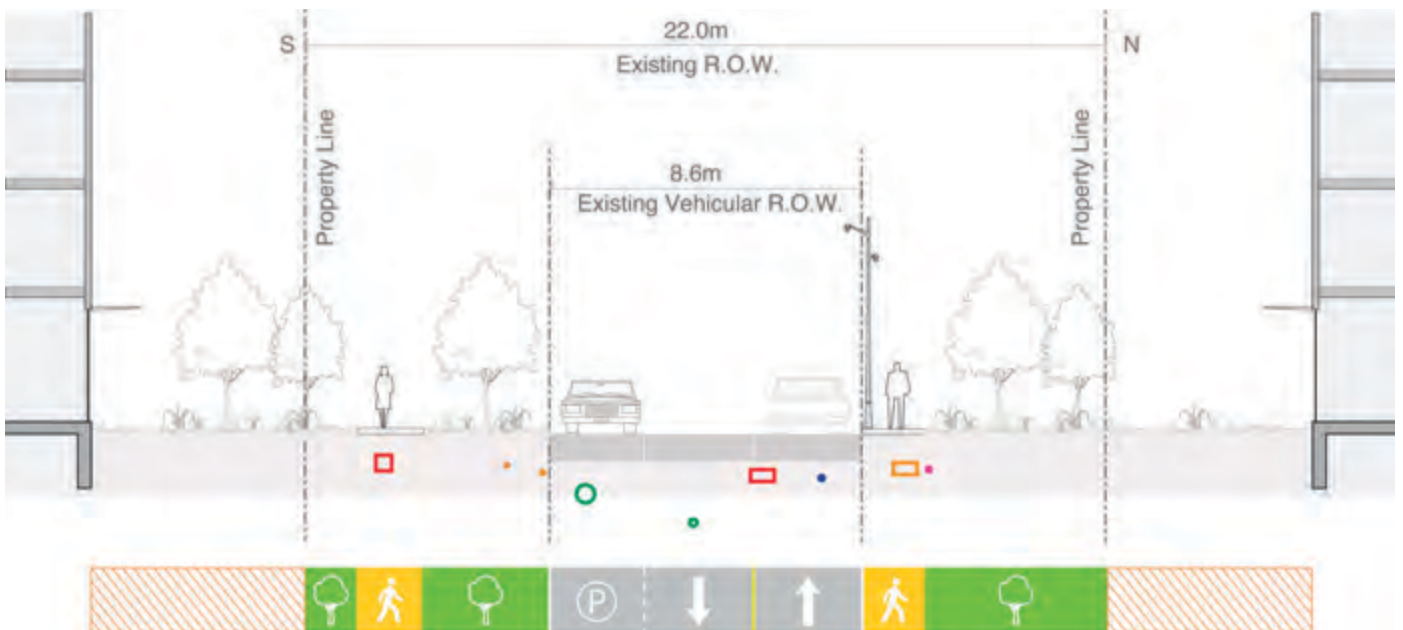
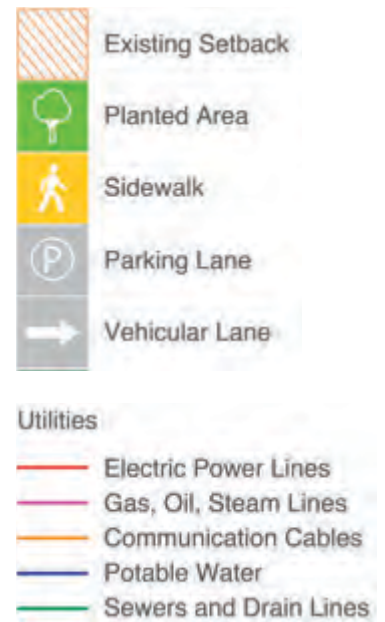


Figure 36 Section of the Existing Conditions along Roehampton Avenue



Figure 37 Existing conditions along south side of Roehampton Avenue

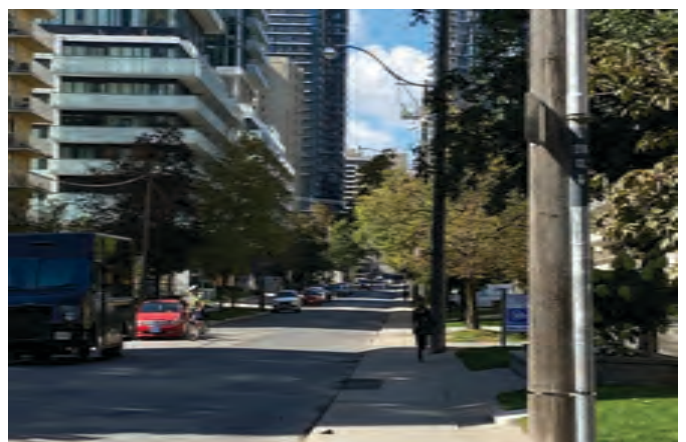


Figure 38 Existing conditions along north side of Roehampton Avenue

Existing Condition along Broadway Avenue

- 20.0m ROW
- Two-way street with parking on the north side of the street
- Yonge Street to Redpath Avenue - 8.6m Vehicular ROW including 2 vehicular travel lanes and a parking lane
- Redpath Avenue to Mount Pleasant Avenue – 9.8m Vehicular ROW, including 2 vehicular travel lanes and a wide parking lane
- Broadway Avenue and Yonge Street Intersection – 9.7m Vehicular ROW, no turning lane
- Broadway Avenue and Mount Pleasant Avenue intersection – 12.0m Vehicular ROW, including west travel lane, left turning lane and east travel lane
- Substandard pedestrian clearway on narrow sidewalks on both sides of the street
- Pedestrian north-south and east west crosswalks at the intersections with Yonge Street and Mount Pleasant Road

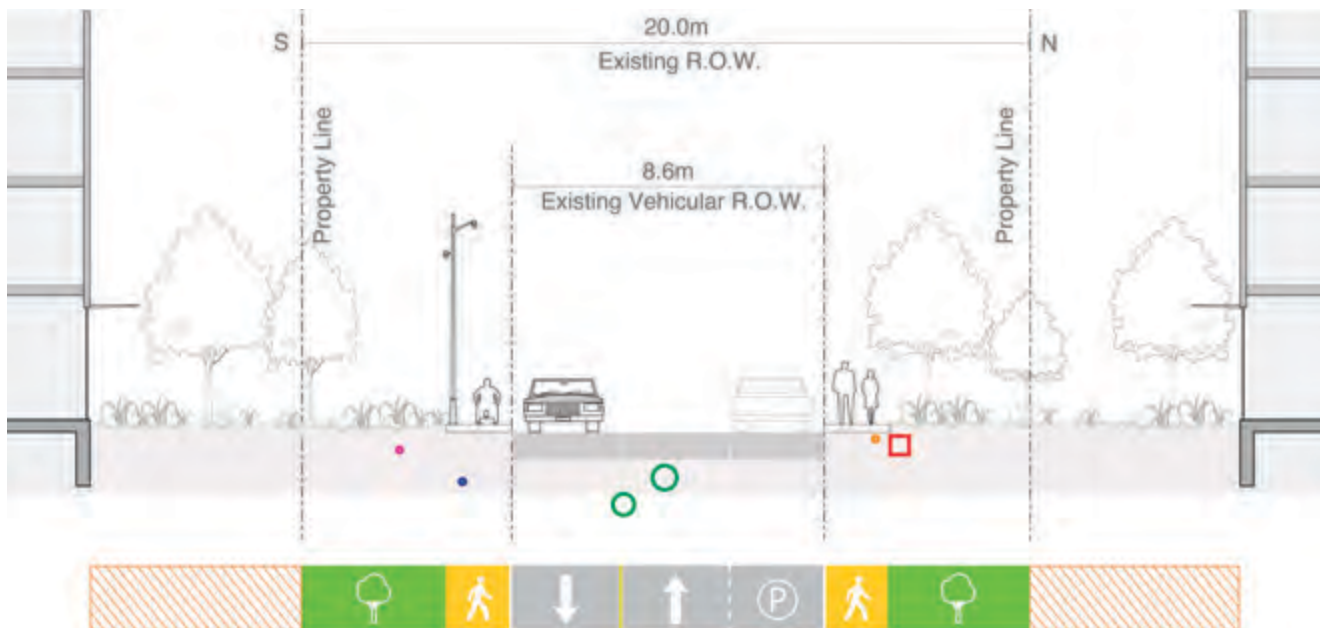


Figure 39 Section of the Existing Conditions of Broadway Avenue between Yonge Street and Redpath Avenue



Figure 40 Existing conditions along Broadway Avenue



Figure 41 Existing conditions along Broadway Avenue

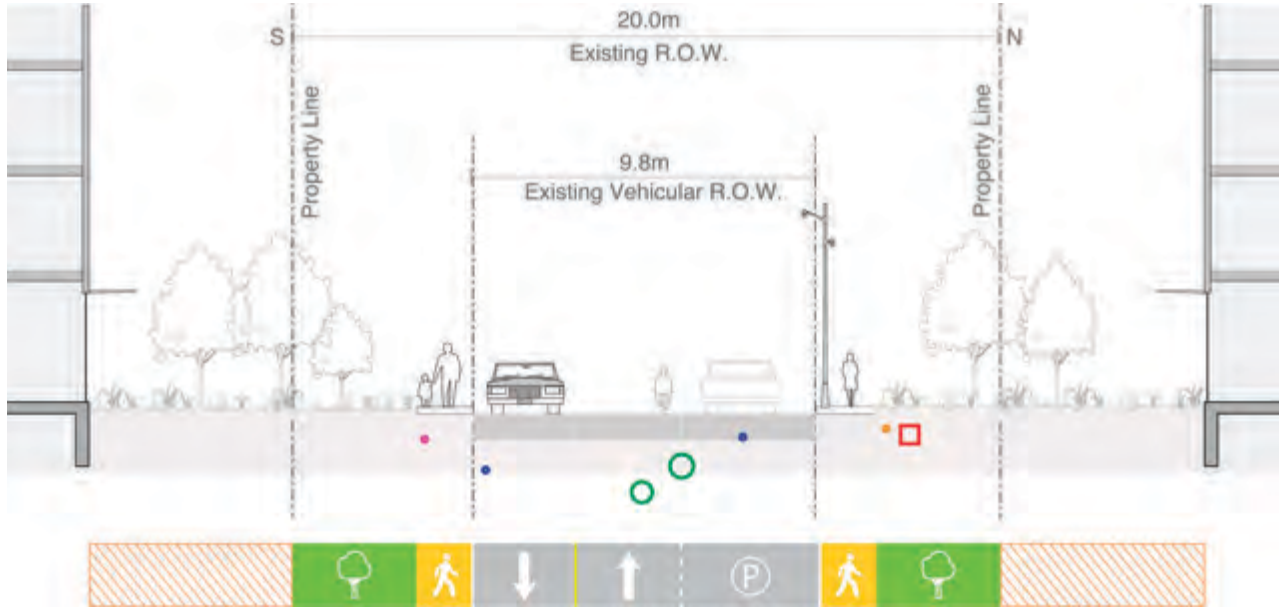


Figure 42 Section of the Existing Conditions of Broadway Avenue between Redpath and Mount Pleasant Road



Figure 43 Existing condition at the intersection of Yonge Street and Roehampton Avenue



Figure 45 Existing condition at the intersection of Broadway Avenue and Redpath Avenue



Figure 44 Existing condition at the intersection of Broadway Avenue and Mount Pleasant Road



Figure 46 Existing condition at the intersection of Broadway Avenue and Mount Pleasant Road

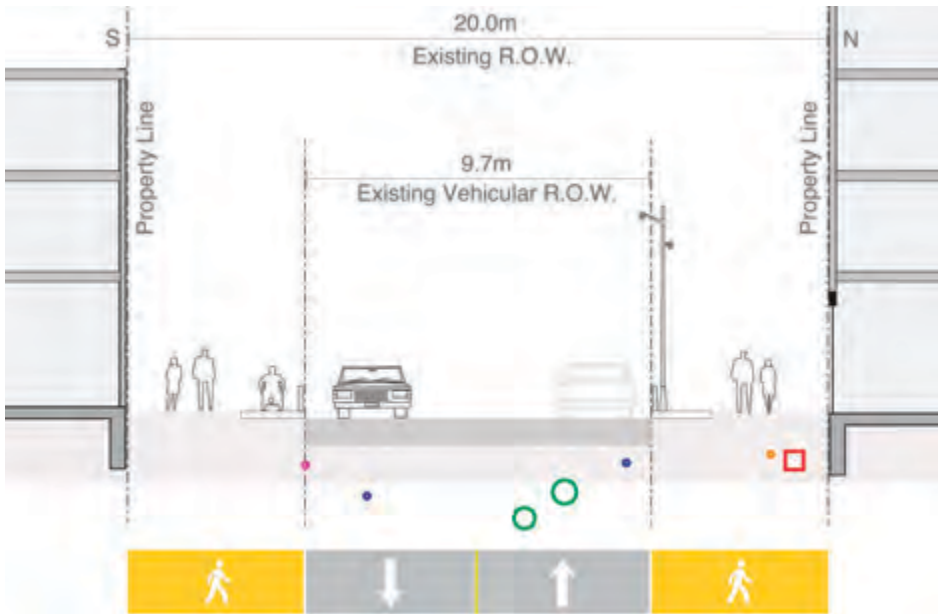


Figure 47 Section of the Existing Conditions of Broadway Avenue intersection with Yonge Street

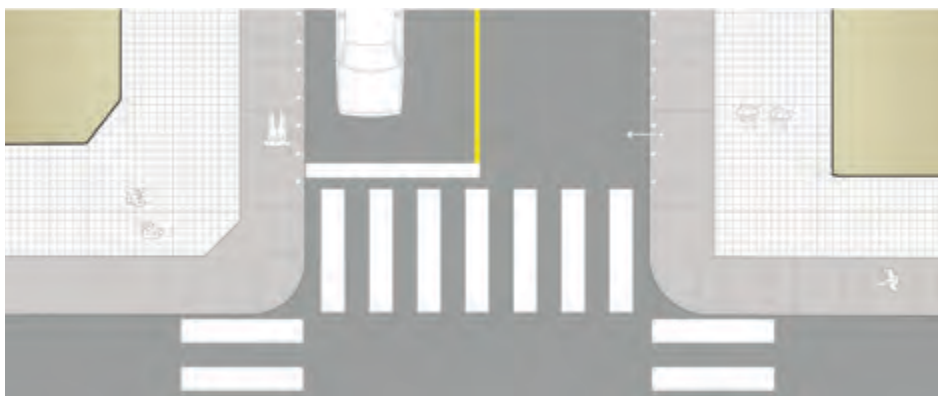


Figure 48 Plan of the Existing Conditions of Broadway Avenue intersection with Yonge Street

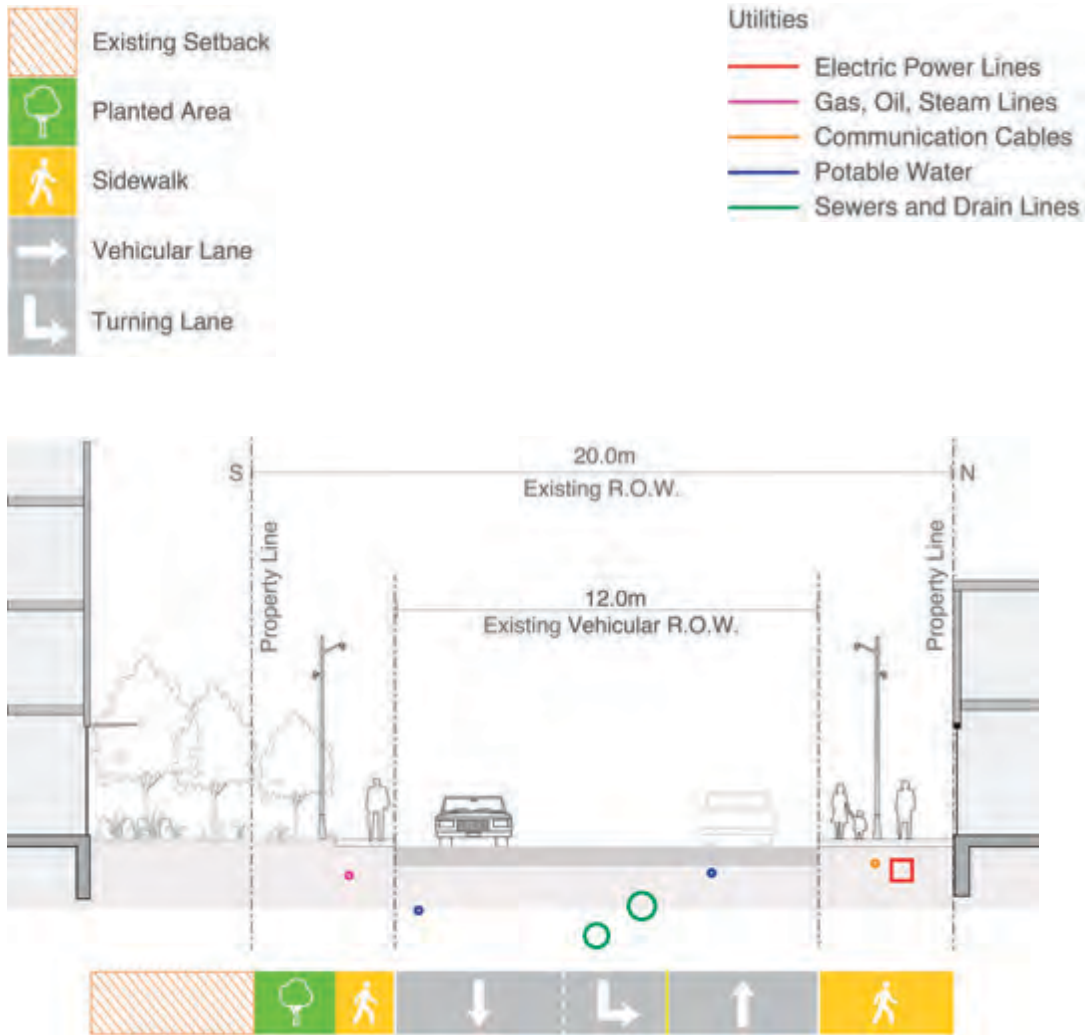


Figure 49 Section of the Existing Conditions of Broadway Avenue intersection with Mount Pleasant Road

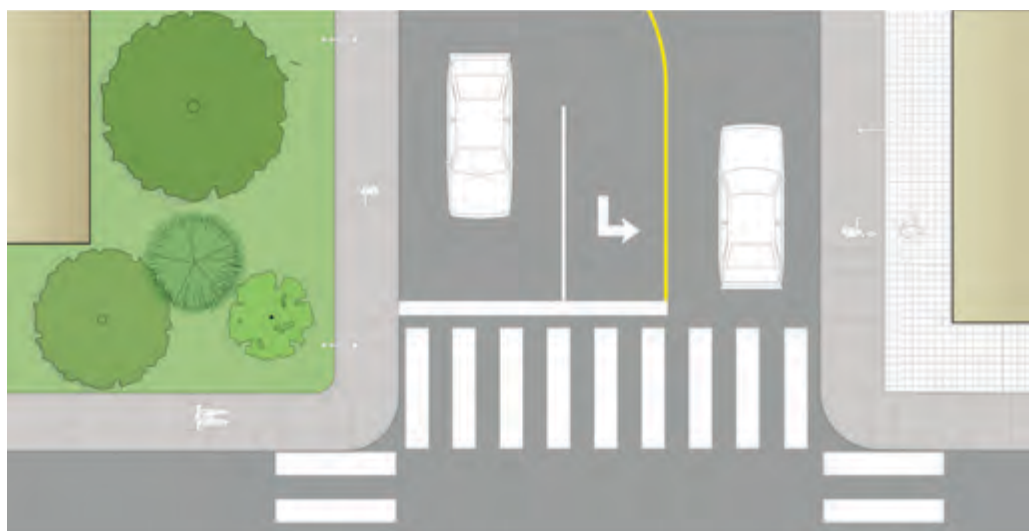


Figure 50 Plan of the Existing Conditions of Broadway Avenue intersection with Mount Pleasant Road

Secured Section 37 Contributions

There are eight development sites located along Roehampton Avenue and Broadway Avenue where Section 37 contributions have been secured, currently totalling up to \$11.4 million. All, or a portion of these funds can be used to implement the Park Street Loop or other public realm improvements. Of this, \$8 million has been secured from sites where the Park Street Loop is the only Public Realm Move that is adjacent to the site.

The eight development sites adjacent to the Park Street Loop are located in Wards 12 and 15, where Section 37 funds in each ward could provide a roughly equal contribution if the potential funding is entirely dedicated to the Park Street Loop.

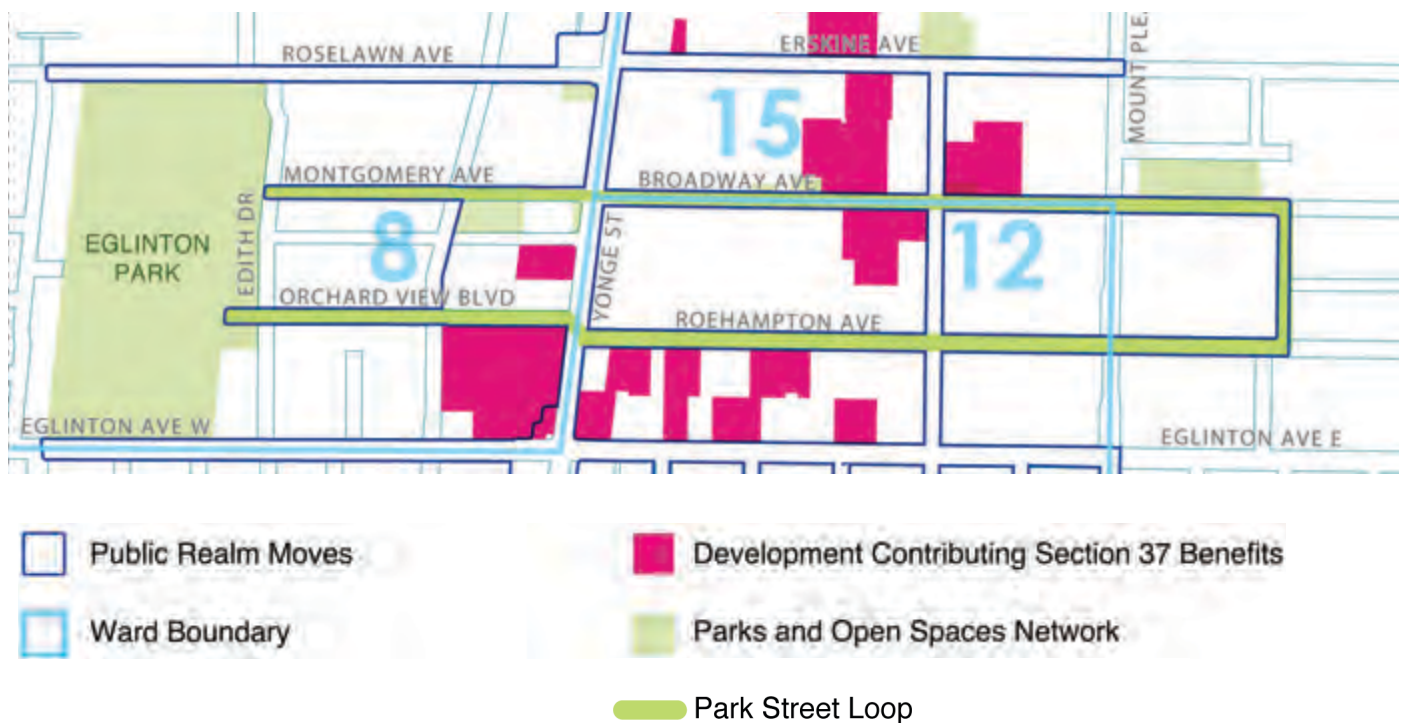


Figure 51 Map of the Section 37 secured contribution locations along the Park Street Loop Public Realm Move

Summary of Opportunities and Constraints

Sites with Redevelopment Potential

Both Roehampton and Broadway Avenue have been developing rapidly. There are several approved applications moving towards the construction phase, clustered around the intersection with Redpath Avenue on Broadway Avenue and west of Redpath Avenue on Roehampton Avenue. These approved applications have a secured landscape setback of at least 7.5m from the building face to the property line. There is also a proposed development at the northeast corner of Yonge Street and Roehampton Avenue, where staff are working to secure a landscape setback as part of the development review process.

Some of the remaining sites fronting these streets may be able to accommodate redevelopment and infill, particularly along both sides of Roehampton Avenue. As these sites come forward for development, they will contribute to the overall vision of the public realm through implementation by the developer, where possible. Additionally, public realm improvements can be expanded beyond the developer's property line in front of properties that have been recently constructed and will not be redeveloped in the near future.

Both Broadway Avenue and Roehampton Avenue present opportunities and constraints to public realm improvements through development. A constrained site may include heritage properties or recent developments that provide less than 7.5m of space from the building face to the property line. These sites are generally located west of Redpath Avenue and are either large lots or clustered small lots. However, public realm improvements, including a promenade loop (with bikeway along Broadway Avenue) and green furniture zones, can still be implemented either by the City or through redevelopment on these sites.

To accommodate all proposed public realm elements, narrowing the following roadways will be required:

- The vehicular travelway of Broadway Avenue, east of Redpath Avenue
- Broadway Avenue and Mount Pleasant Road intersection
- Broadway Avenue and Yonge Street intersection
- Lay-by on the north side on Broadway Avenue

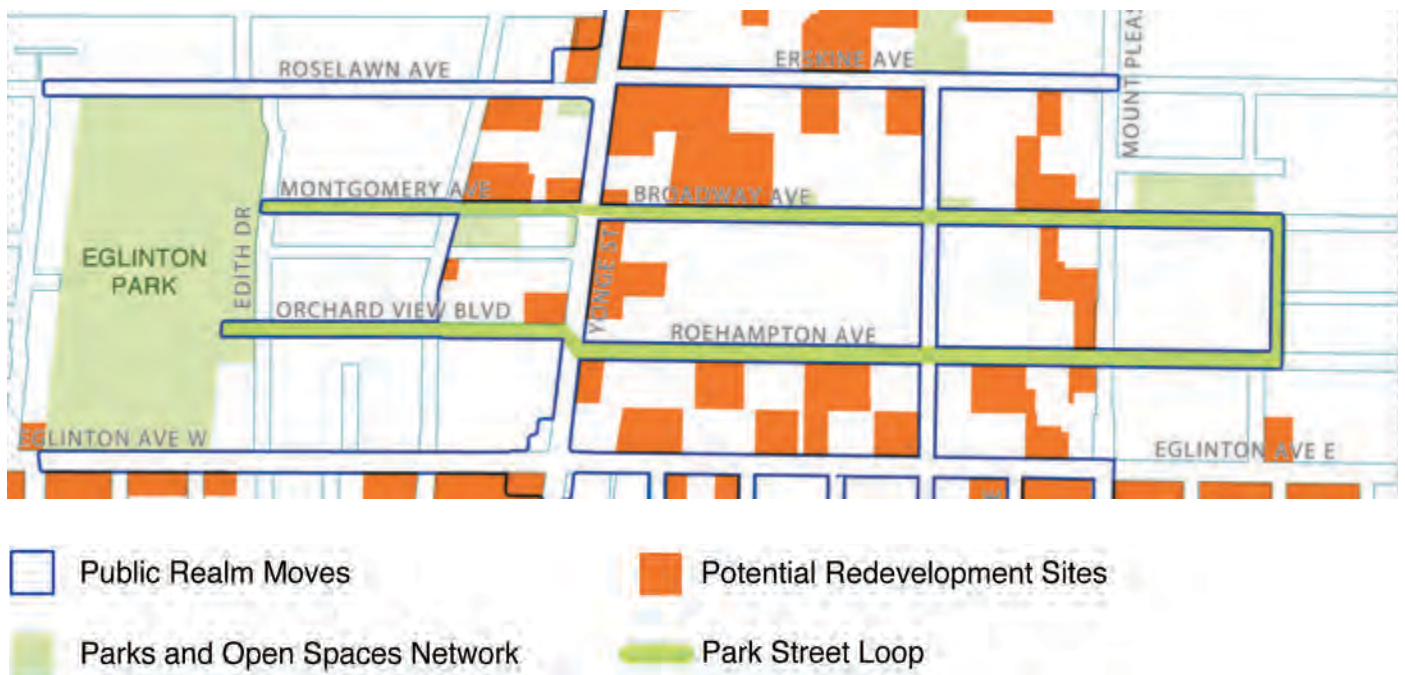


Figure 52 Map of potential redevelopment sites along the Park Street Loop



Figure 53 Diagram of opportunities and constraints at Broadway Avenue



Figure 54 Diagram of opportunities and constraints at Roehampton Avenue

Utility Locations and Key Issues

Both Broadway and Roehampton Avenue have a variety of above and below ground utilities. Many of the above-ground facilities such as hydro poles, fire hydrants and utility vaults are located immediately adjacent to the sidewalks and within the public realm.

On Broadway Avenue, an existing watermain runs below the sidewalk on the south side of the street. On the north side, another watermain lies below the travel portion of the street.

On Roehampton Avenue, an existing watermain is located on the north side below the sidewalk. Refer to existing condition sections for utility locations.

The current location of numerous above and below ground utilities conflicts with the intended expansion and beautification of the public realm. To create a continuous, unobstructed sidewalk and bikeway space, the relocation of many of these facilities will be required. Where bump-outs are proposed, existing catch basins will need to be relocated or incorporated into the bump-out design.



Figure 57 Utility Challenges along Roehampton Avenue



Figure 58 Utility Challenges along Broadway Avenue



Figure 55 Utility Challenges along Roehampton Avenue



Figure 59 Constrained Condition at north west corner of Broadway and Redpath Avenues



Figure 56 Utility Challenges along Broadway Avenue



Figure 60 Constrained Condition on Roehampton Avenue

Park Street Loop Concept

Park Street Loop Elements

The Park Street Loop will consist of four zones:

Green Boulevard/ Street Furniture

The 'Green Boulevard/ Furniture Zone' along the curb will prioritize soft landscape areas and street trees. Benches and bike rings could be provided to augment the furniture provided in the 7.5m setback.

Promenade Loop

The Promenade Loop is located between the Green Boulevard and Publicly-Accessible Open Space and will consist of a widened pedestrian sidewalk. The Promenade Loop is intended to be a casual recreational walkway for people of all ages and abilities, parents with strollers as well as joggers.

Broadway Bikeway

The Broadway bikeway is located between the Green Boulevard and Promenade Loop and is intended for casual recreational cycling activity.

Publicly-Accessible Open Space

Within the 7.5m setback, 5.5m of programmed space will promote public use and have a high quality design. Seating areas will be distinct, intimate spaces tucked into the setback and fronting onto the Promenade Loop. The design of the setback will incorporate seating to allow people-watching on the sidewalks and provide places to stop and mingle. A four season planting palette will provide visual interest all year long.

Private Amenity Area

Private stoops, or entries to individual at-grade units or townhouses, may also be located within the 2.0m walkway area of the building setback. These front yard areas may be defined with landscaping, low fences, hedges or similarly low-scaled front yard features to provide a private threshold and preserve visual connection into the public space.

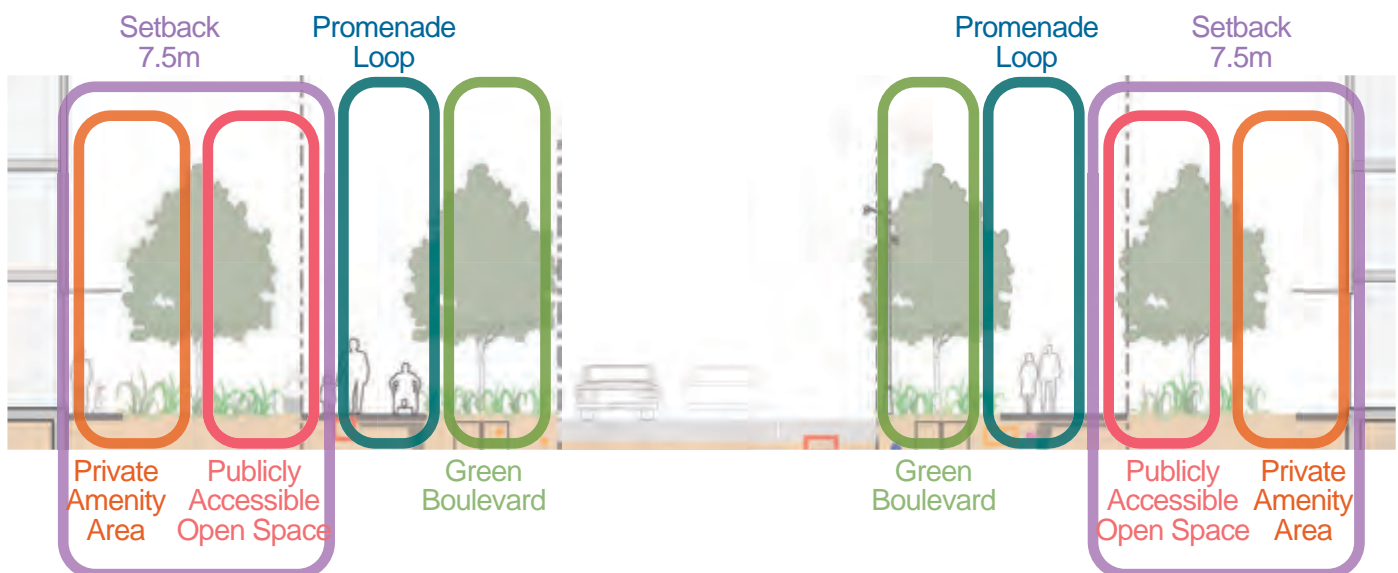


Figure 61 Diagram of the Street Section showing Park Street Loop Zones

Where possible, the design of each setback area beyond the Promenade Loop will be developed with its own concept, program and identity as one in a series of outdoor rooms with a unique character, within the 7.5m setback.

The character of both public and private areas together will result in a lush park-like setting by maximizing tree, shrub and native grass planting, and generous paving surfaces for a variety of passive recreational activities. The shrubs and native grasses will provide year round seasonal interest through a diversity of species.

In the short term, the Promenade Loop will be upgraded incrementally to provide wider areas for pedestrian movement and enhanced amenity, between the pinch points where properties are constrained. Ultimately, the public right-of-way of the Park Street Loop will be a continuous and cohesive element.

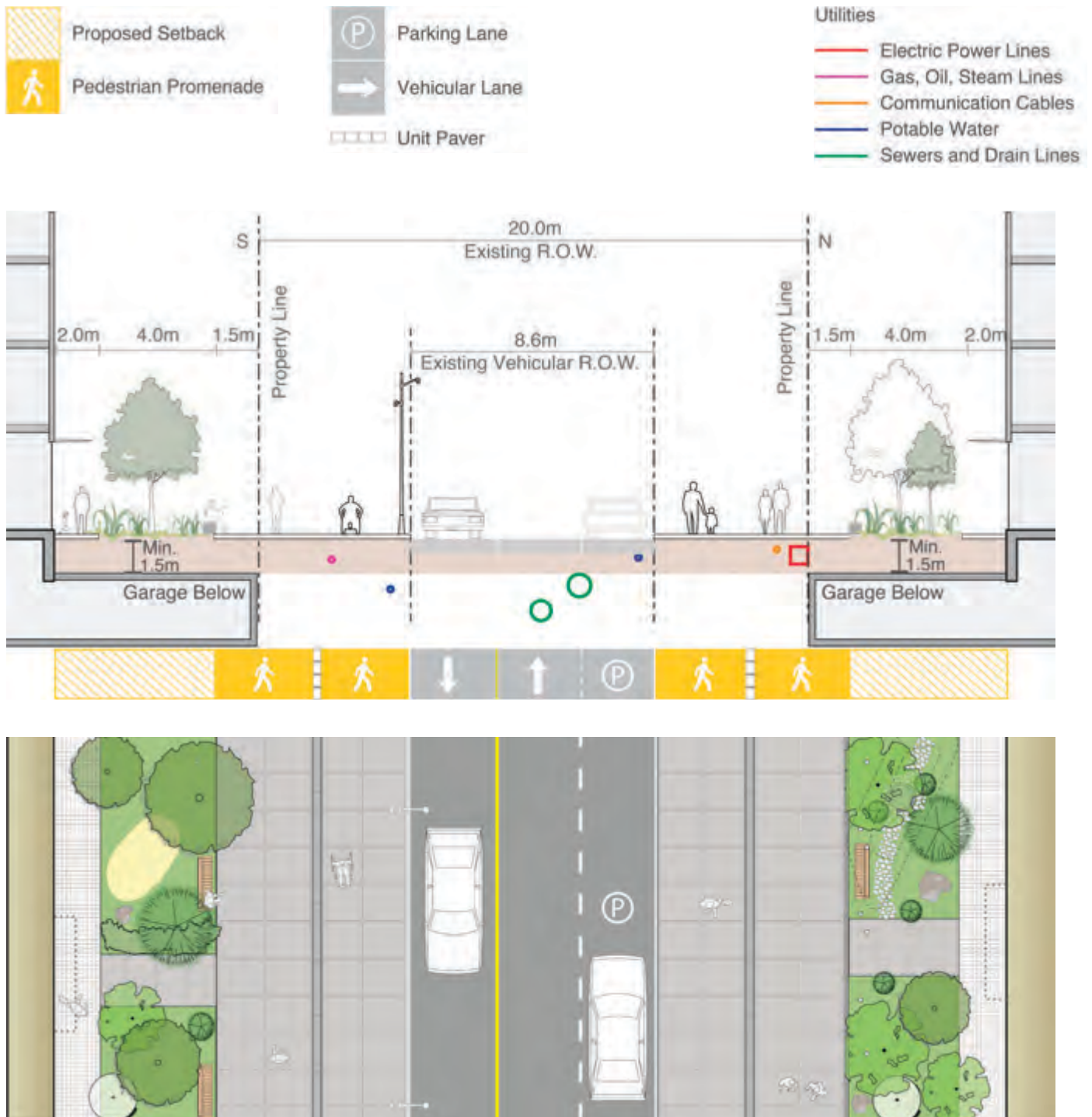
Final Condition Recommendations:

Broadway Avenue

Interim Condition

- It is anticipated that the existing 150mm watermain will be upgraded and relocated in 2025. Any development that will be constructed prior to the watermain upgrades will require the implementation of an interim condition given the location of the existing watermain. An interim condition that protects the space identified for cycling lanes is also required. The interim condition will include concrete paving from the edge of the curb to the property line.

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.



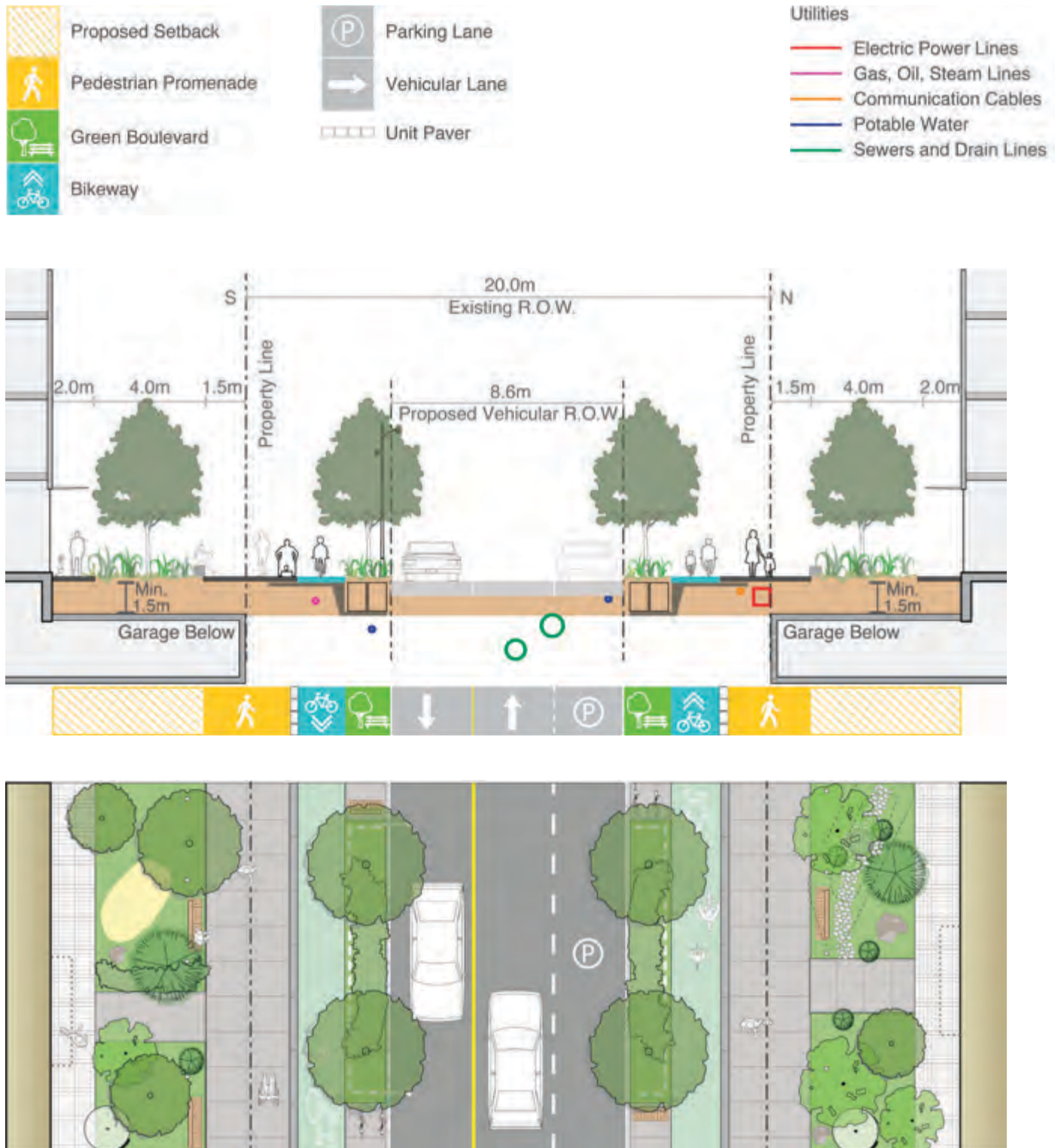
SECTION 4.2

Figure 62 Section and Plan of Broadway Avenue - Recommended Interim Condition

Final Condition

- Narrow the ROW from 9.8m to 8.6m between Redpath Avenue and Mount Pleasant Road
- Narrow the ROW from 9.7m to 8.6m between Yonge Street and Broadway Avenue Intersections
- Maintain two-way travel with parking on the north side of the street
- Provide a 7.5m building setback from the property line to the building face on both sides of the street
- Provide generous concrete sidewalk to create the Promenade Loop (with 1.5m wide strip on private property)
- Provide generous Green Boulevard/ Furniture Zone along the curb
- Provide Bikeway
- Provide unit paver buffer between the Bikeway and the Promenade Loop
- Provide planted bump-outs at Yonge Street and Broadway Avenue Intersections
- Vehicular drop-off is not be located in the 7.5m landscape setback
- Narrow vehicular ROW from 12.0m to 9.0m, including 2 travel lanes and a left turning lane at the intersection of Broadway Avenue and Mount Pleasant Avenue
- Relocate existing light standards/ hydro poles outside of the Promenade Loop into the Green Boulevard Furniture Zone (or, if possible, relocate hydro underground)
- Relocate on-street parking impacted by the size and locations of the future bump-outs
- Design north-east and south-east side of the intersection with Yonge Street as a Yonge Street Square. (Refer to Section 4.9)

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.



SECTION 4.2

Figure 63 Section and Plan of Broadway Avenue, between Yonge Street and Mount Pleasant Road - general Recommended Final Condition

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.



Figure 64 Section and Plan of Broadway Avenue - Recommended Final Condition at intersection with Mount Pleasant Road

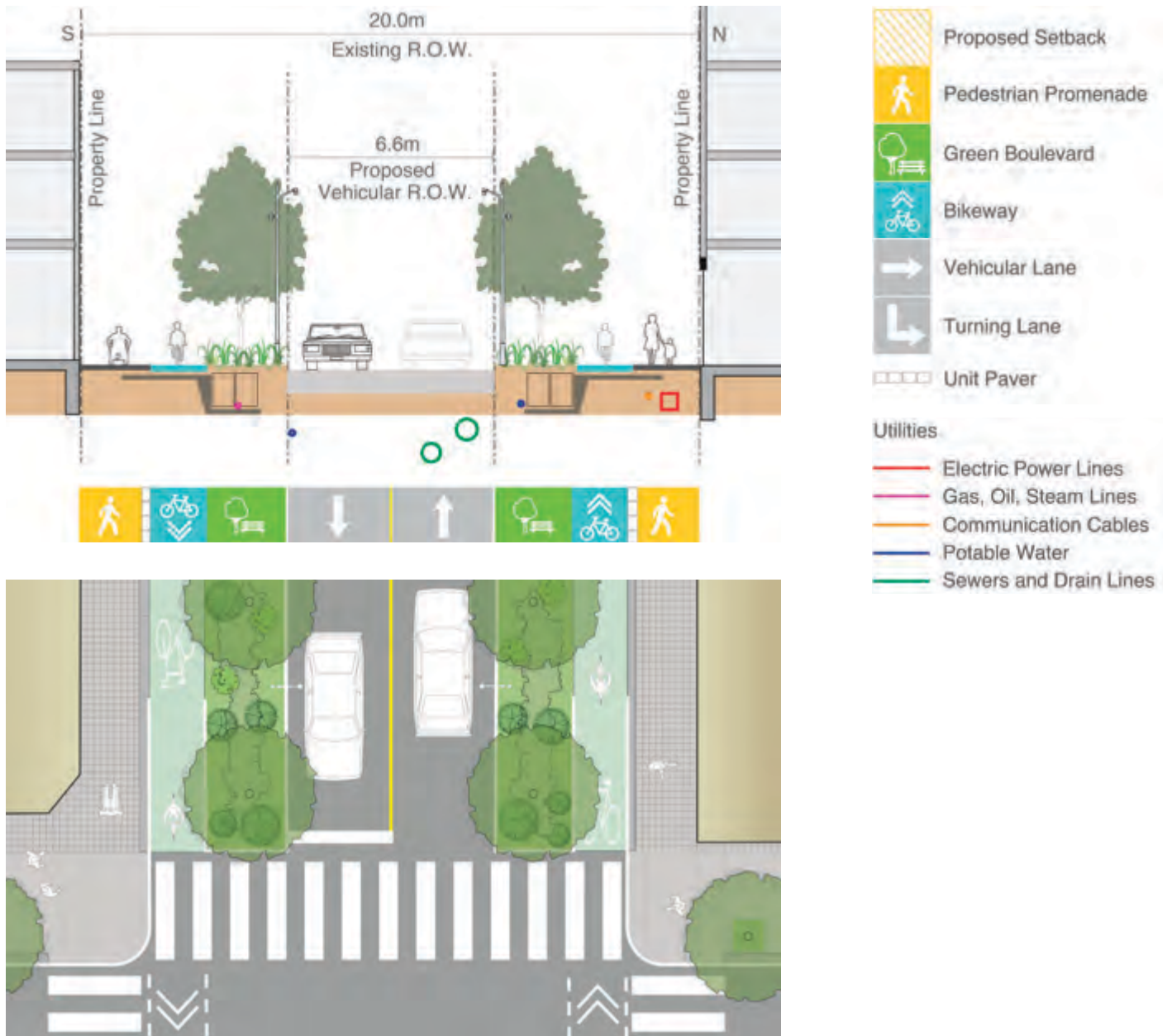


Figure 65 Section and Plan of Broadway Avenue - Recommended Final Condition with Bump-out at intersection with Yonge Street

Roehampton Avenue

Final Condition

- Maintain 8.6m vehicular ROW as a two-way street with parking on the south side of the street
- Provide a 7.5m building setback from the property line to the building face on both sides of the street
- Provide generous concrete sidewalk called the Promenade Loop
- Provide generous Green Boulevard/ Furniture

Zone along the curb

- Provide planted bump-outs at Yonge Street and Roehampton Avenue Intersections
- Vehicular drop-off is not be located in the 7.5m landscape setback
- Relocate on-street parking impacted by the size and locations of the future bump-outs
- Relocate existing light standards/ hydro poles outside of the Promenade Loop into the Green Boulevard Furniture Zone (or, if possible, relocate hydro underground)

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

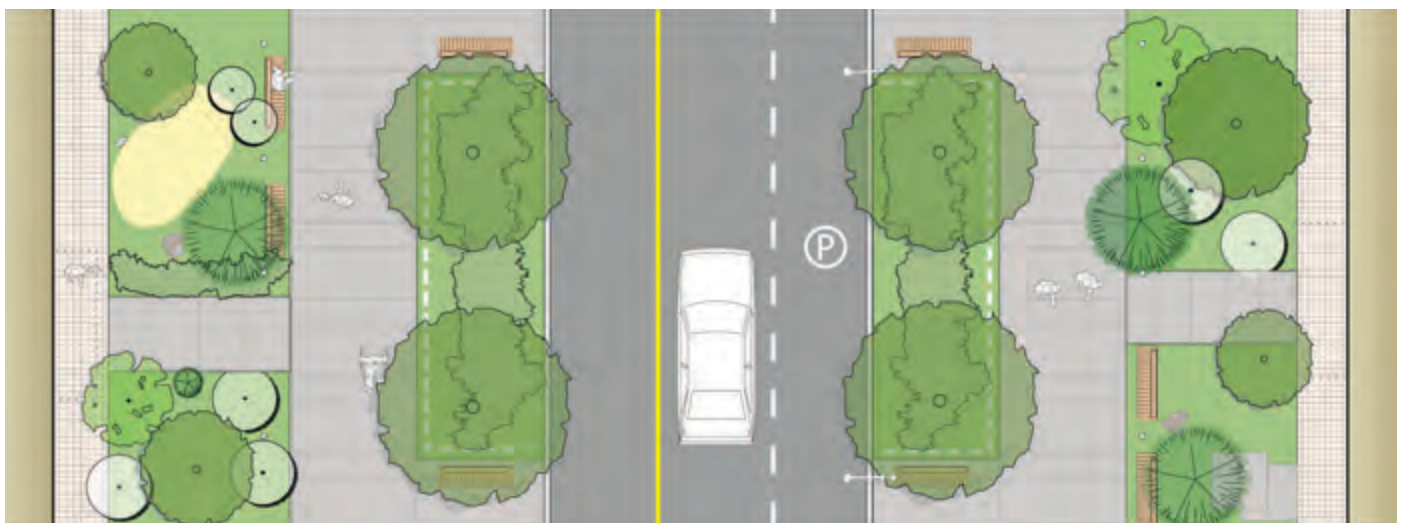
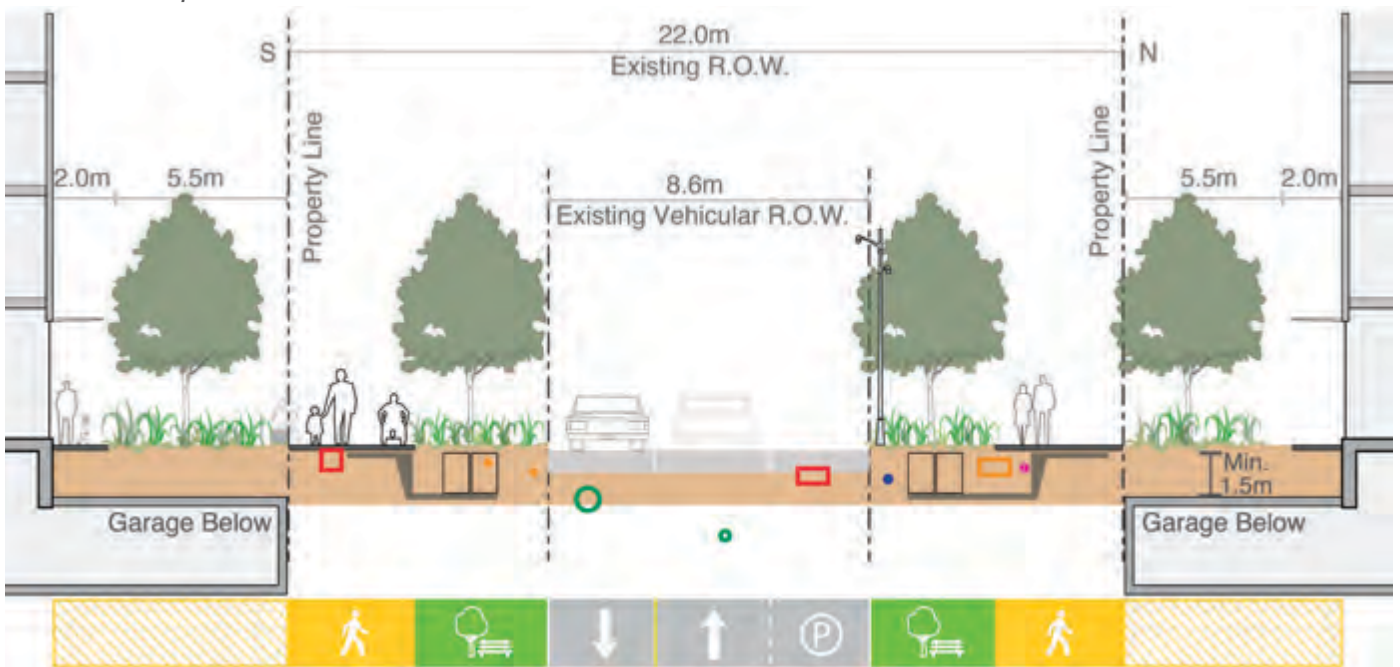
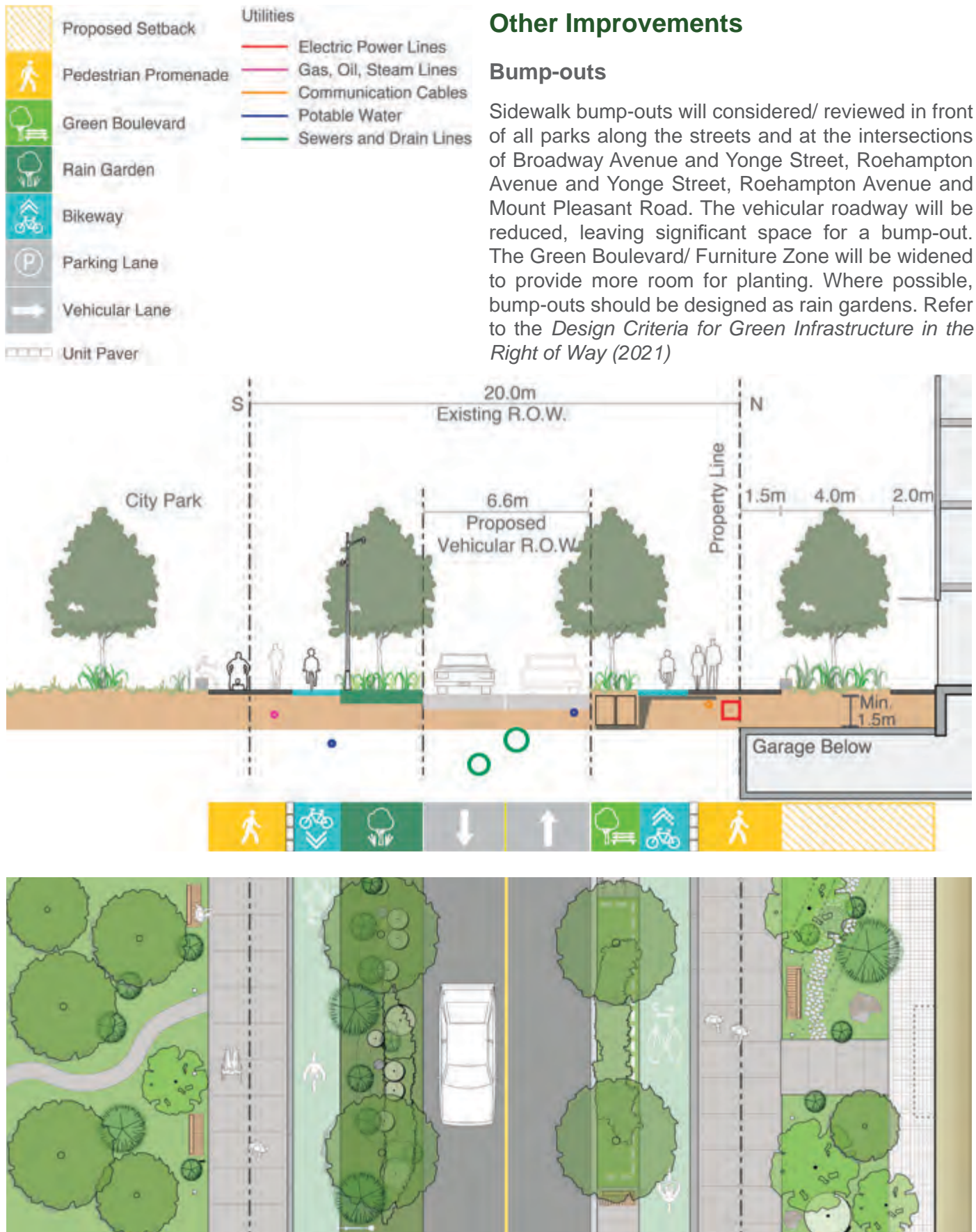


Figure 66 Section and Plan of Roehampton Avenue - Recommended Final Condition

Other Improvements

Bump-outs

Sidewalk bump-outs will be considered/ reviewed in front of all parks along the streets and at the intersections of Broadway Avenue and Yonge Street, Roehampton Avenue and Yonge Street, Roehampton Avenue and Mount Pleasant Road. The vehicular roadway will be reduced, leaving significant space for a bump-out. The Green Boulevard/ Furniture Zone will be widened to provide more room for planting. Where possible, bump-outs should be designed as rain gardens. Refer to the *Design Criteria for Green Infrastructure in the Right of Way (2021)*



SECTION 4.2

Figure 67 Section and Plan of Broadway Avenue - Recommended Final Condition of bump-out in front of parks

Raised Crosswalks

Where mid-block connections are incorporated, a raised crosswalk with planted bump-outs will be considered/ reviewed to provide a safe pedestrian experience and act as a traffic calming element. For the latest standards, refer to *Road Engineering Design Guidelines, 11.0 Raised Crosswalk and Intersection Guideline, Version 1.0, January 2020, City of Toronto Transportation Services.*

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

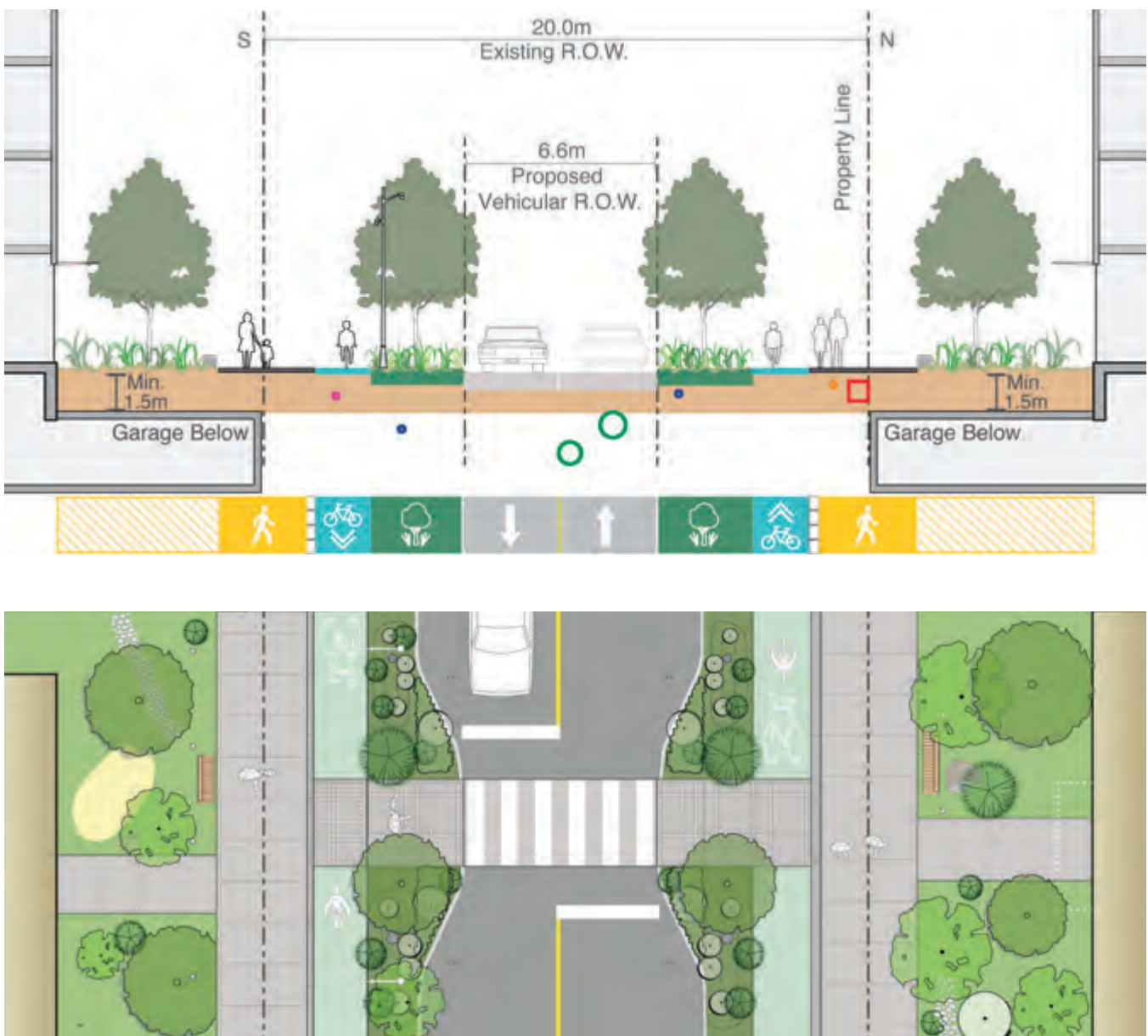


Figure 68 Section and Plan of Broadway Avenue - raised crossing Recommended Final Condition

Constrained Sites and Sidewalk Transitions

In instances where a 7.5m setback cannot be achieved - for example, if the site has heritage value or has been recently constructed - public realm improvements including the Green Boulevard and Promenade Loop should be implemented to the extent possible to create a continuous pedestrian experience. The interim condition will include a sidewalk transition zone to connect existing sidewalks to the Promenade Loop.



Figure 69 Plan of existing constrained and sidewalk transition - Recommended Final Condition

Design Guidelines

Green Boulevard/ Furniture Zone Guidelines

- Limit planter length to one or two trees in order to provide paved pathways between the street and the sidewalk and avoid long runs of curbs which limit pedestrian movement. Large canopy trees will be: American Elm (*Ulmus americana*), Black Gum (*Nyssa sylvatica*), Black Walnut (*Juglans nigra*), Sugar Maple (*Acer saccharum*), Freeman Maple (*Acer freemani*) or Red Oak (*Quercus rubra*) with soil volumes of 30 cu. m. per tree to achieve legacy trees;
- Install a 400mm high iron fence on metal frames around the open planter flush with the ground to allow for stormwater runoff. Refer to City of Toronto Streetscape Manual details T-CIP- 3.0 to 3.0-6a as a guide;
- Provide sustainable understory planting that provides seasonal colour and textural interest. The understory planting will be resilient, support biodiversity objectives and provide natural habitat including pollinator habitat and food sources.



Figure 70 Fort York Boulevard



Figure 71 Harbour Street

Promenade Loop Guidelines

- Provide an extra wide concrete sidewalk. The concrete is to be continuous across driveways. If a portion of the sidewalk is on private property, provide an expansion joint at the property line to anticipate different grade settlement;
- Provide a wide concrete sidewalk on public property and 1.5m on private property (part of the 7.5m landscape setback on Broadway Avenue only);
- Create sidewalk transitions in front of the development site to meet the existing sidewalk on adjacent sites by designing sidewalk transition zones, which will accommodate a minimum 2.1m wide diagonal connection. The adjacent sites are to be shown on the site plans as part of the local context. The transition zone is to be wide enough to accommodate benches and create a small plaza space.



Figure 72 Queens Quay West



Figure 73 York Street

Publicly-Accessible Open Space and Building Forecourts

- Design a 7.5m setback area with the adjacent public boulevard as one well-defined, comprehensive landscape concept;
- Provide a variety of seating options including movable tables and chairs;
- Prioritize lush planting by providing 60% of the area as soft landscape. Provide a variety of layered planting including canopy, understory and groundcover that maintains interest throughout the season. Planting will be designed to maintain and enhance safety by ensuring open views and avoiding the creation of entrapment areas.
- Refer to the Green Streets Guidelines when designing planting areas to allow for stormwater infiltration. This improves water quality, which conventional stormwater storage does not;
- Limit the height of the above grade planters to 450mm to create a visually and physically open landscape;
- Match the setback grade with the sidewalk grade at the boundary to promote accessibility;
- Provide a minimum 1.5m of soil depth for tree planting on top of underground garages;
- Provide pedestrian-scaled lighting along the Promenade Loop, within the 7.5m setback. The lighting will define both the Promenade Loop and the private setback which will be distant from street lighting. To achieve consistency along the Park Street Loop, the light fixture is to be the 3.4 m (12') tall Exelia Column by S'elux, or equivalent. Light levels should be between 100-30 lux (10-3 ft. candles). The setback can also be augmented by a bollard like Viviana by Ares at a height of 900 mm.



Figure 74 St. James Park



Figure 75 Duplex Avenue



Figure 76 Adelaide Place

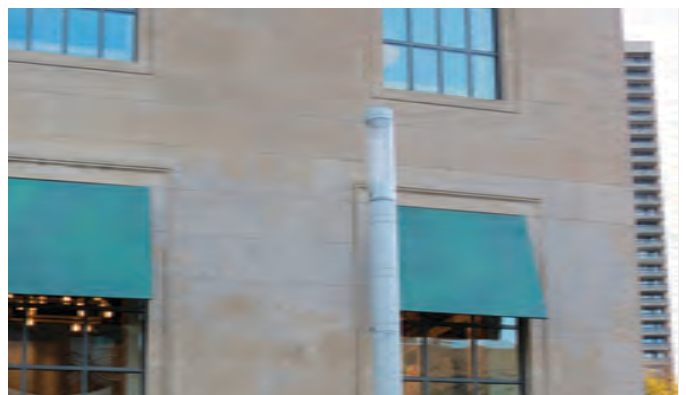


Figure 77 Light Column at Montgomery Square Park

- Incorporate a variety of programming. Options for the programming of the setback include:
 - *Natural Play* which includes child-friendly, flexible play elements such as boulders, natural seating and elements as well as vertical and whimsical elements. Prioritize design features that promote children's play, as per the Growing Up Guidelines;
 - *Public Art* to include whimsical elements and provide a sense of play and interaction;
 - *Pollinator/ Butterfly Gardens* to include appropriate plant materials to attract pollinators and butterflies;
 - *Rain Gardens* to include appropriate plant materials to retain rain water and to provide visual interest;
 - *Green Rooms* to include large trees and native understory species with quiet seating areas.
- Provide POPS signage for areas secured as public easements.
- Locate "paid seating areas", such as cafes, adjacent to the building, within 2.0m of the building setback. The remaining 5.5m will be secured for public use and will be appropriately furnished for the intended public use. Consideration for larger paid seating areas at corner locations or at mid-block walkways may be considered on a site-specific basis where warranted by the context. "Paid seating areas" are not permitted in POPS.



Figure 78 Yorkville Park



Figure 79 City Place



Figure 80 College Park

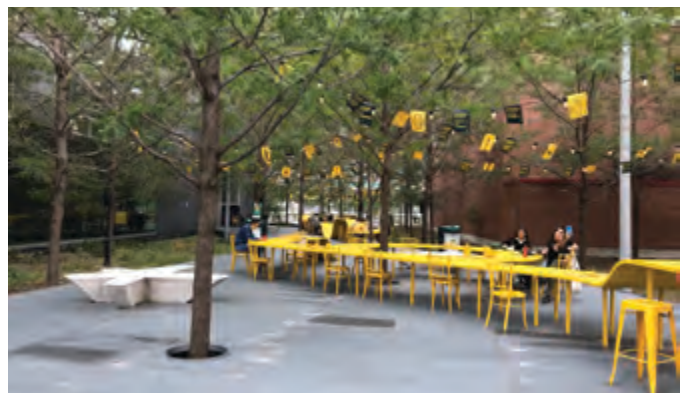


Figure 81 Hydro - Quebec Parc, Montreal

Site Servicing

- Screen transformers or gas regulators with architectural features. No Siamese connections, transformers or gas regulators will be located within the 7.5m landscape setback;
- Place all proposed visible, in-ground utilities away from pedestrian circulation areas and screen with vegetation where possible;
- Locate all new exhaust vents outside of the 7.5m landscape setback;
- Locate all in-ground utilities such as man access holes, chambers and sampling ports, etc. away from pedestrian circulation routes. These utilities are to be located in the driveways away from the programmed 7.5m landscape setback;
- Relocate all existing above ground infrastructures, such as hydro poles, fire hydrants, vaults, etc. away from pedestrian Promenade Loop sidewalk into the Green Boulevard/ Furniture Zone.

Mid-block connection termini

On blocks with mid-block connections:

- Provide a generous forecourt where the mid-block connection meets the Park Street Loop. Provide additional seating, bicycle parking, lighting and design features which identify the mid-block walkway to ensure that it is legible as a publicly accessible space and part of a connected pedestrian network.



Figure 82 Transformer Screen at Adelaide Place



Figure 83 Mid-block connection at Broadway Avenue



Figure 84 Mid-block connection at Wellington Avenue

Private Amenity Zone

- Design ground floor units with private amenity space as a front patio;
- Provide low fencing, no higher than 900mm, between private amenity spaces and public space, if desired.

Building Entrances

- Recess main entrances in from the building face in order to accommodate an exterior entry forecourt;
- Highlight building entrances through the use of a signature architectural canopy. The canopies should be appropriately scaled, large and sculptural with added interest, to announce the entrance and provide weather protection;
- Maintain clear sight lines from the public sidewalk to the entrance to the lobby;
- Design forecourts to include ample seating, waste baskets, bicycle parking, lush planting and movable planting pots of various sizes.



Figure 86 Niagara Street



Figure 87 St. James Park



Figure 88 Roehampton Avenue



Figure 85 Adelaide Place



Figure 89 Roehampton Avenue

Implementation

Development-related Improvements

With numerous redevelopment sites along both Roehampton and Broadway Avenue, partial implementation of the Park Street Loop will be secured through development review process.

Broadway Avenue Interim Improvements and Cash-in-Lieu

For the sites where an existing watermain conflicts with proposed tree planting, the developer will implement an interim condition. A cash-in-lieu payment will be secured for the final streetscape condition at the Site Plan Approval stage as a pre-approval condition secured in the Notice of Approval Conditions (NOAC). The cash-in-lieu will be used for future implementation of the streetscape by the City at the time that Broadway Avenue is reconstructed.

Capital Works and Coordination

Opportunities for public realm improvements may arise through upcoming capital works projects led by Toronto Water and Transportation Services. Taking advantage of other planned works represents an opportunity to coordinate and implement public realm improvements in the near-term and avoids disruption from additional construction work in the future.

Note: The implementation timeline represents information available from divisional partners and data from T.O. Inview as of December 31st, 2021 and is subject to change.

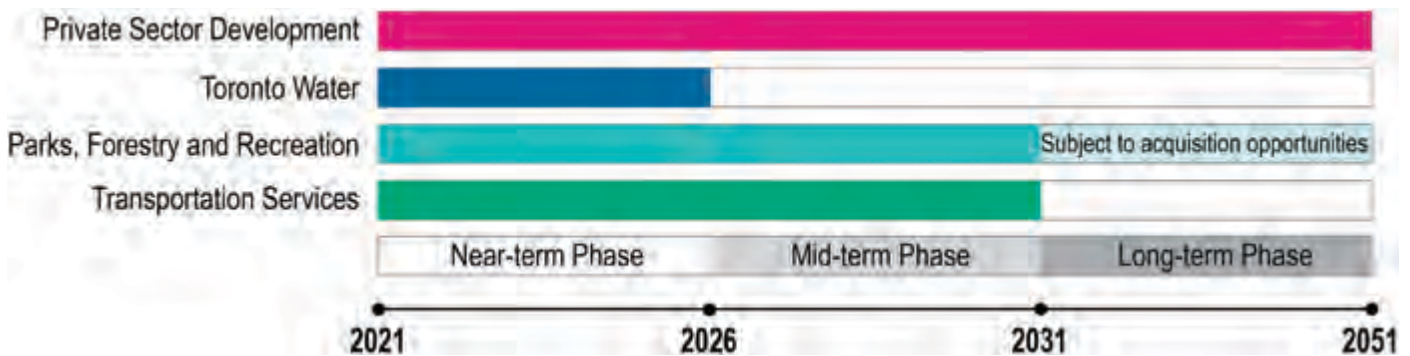


Figure 90 Projected Implementation Timeline for the Park Street Loop



Figure 91 Checklist of Public Realm Elements to be implemented for the Park Street Loop

4.3

Davisville Community Street

Overview

Background Analysis

Final Condition Recommendations

Implementation

Overview

The Davisville Community Street, together with Chaplin Crescent, is a key civic connector in the Davisville neighbourhood. It links the existing June Rowlands Park with existing schools, the future Midtown Community Hub and Aquatic Centre, and Davisville station, all served by an existing bus route. The potential redevelopment of the TTC McBrien building and adjacent transit yard will create further westward links to Oriole Park and the broader public realm network.

Davisville Avenue is envisioned as a generous pedestrian promenade with cycling lanes and large planting areas with wide-canopied trees, with ample space for street furniture and other landscape elements.

Policy Direction

The Yonge-Eglinton Secondary Plan identifies the Davisville Community Street as a Public Realm Move and provides detailed direction in the following policies:

3.2.8 Davisville Community Street

- a. Davisville Avenue, together with Chaplin Crescent, is an important local neighbourhood civic street connecting parks, schools, transit stations and a future community hub.
- b. Davisville Avenue will be transformed into a vibrant, complete street. Improvements will include reducing vehicle travel lanes and widths and creating a multi-modal street that prioritizes walking and cycling with space to grow trees, supports continued bus service and accommodates other landscaping features.
- c. Setbacks at and above grade from the expanded Davisville Avenue right-of-way will be in accordance with the following:

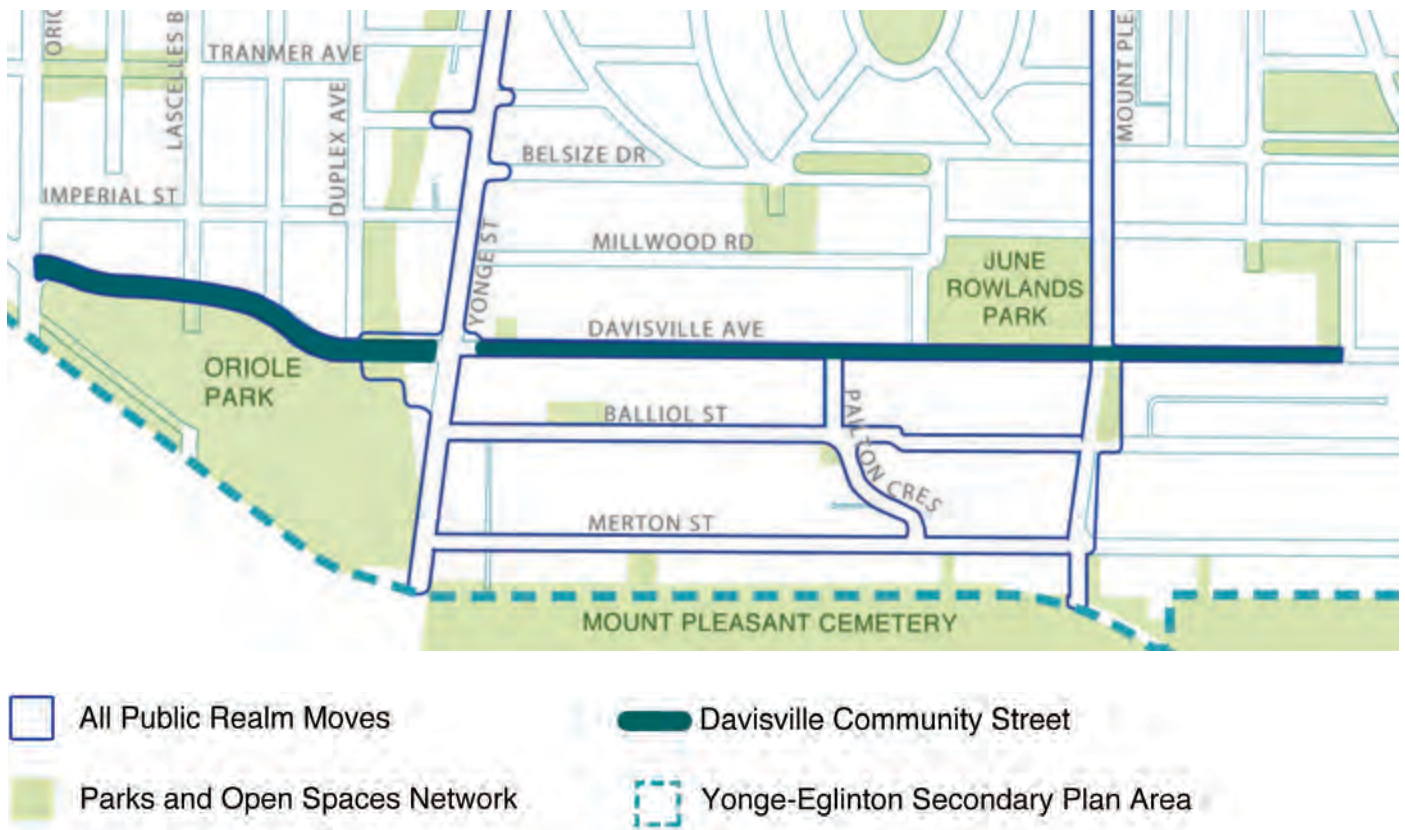


Figure 92 Map showing the location of the Davisville Community Street Public Realm Move

Cycling Network Plan

- i. A reasonable setback on the north side of Davisville Avenue between Yonge Street and Mount Pleasant Road; and
 - ii. A reasonable setback on the south side of Davisville Avenue between Yonge Street and Mount Pleasant Road.
- d. Primary entrances to new buildings will be located on Davisville Avenue and provide appropriately scaled forecourts, plazas and gardens along the street edge.
- e. Vehicular access to new residential developments will be from new streets or existing laneways. Interruptions to the Davisville Avenue streetscape will be minimized by consolidating and coordinating accesses for development and limiting curb cuts and surface parking.

The Cycling Network Plan identifies Davisville Avenue as a future bikeway in the City's cycling network. The Davisville Community Street proposes to realize the bikeway identified in the Cycling Network Plan by installing bikeways along Davisville Avenue, prioritizing the section from Duplex Avenue to Acacia Road. This will separate cyclists from vehicular traffic and support a safer cycling network.

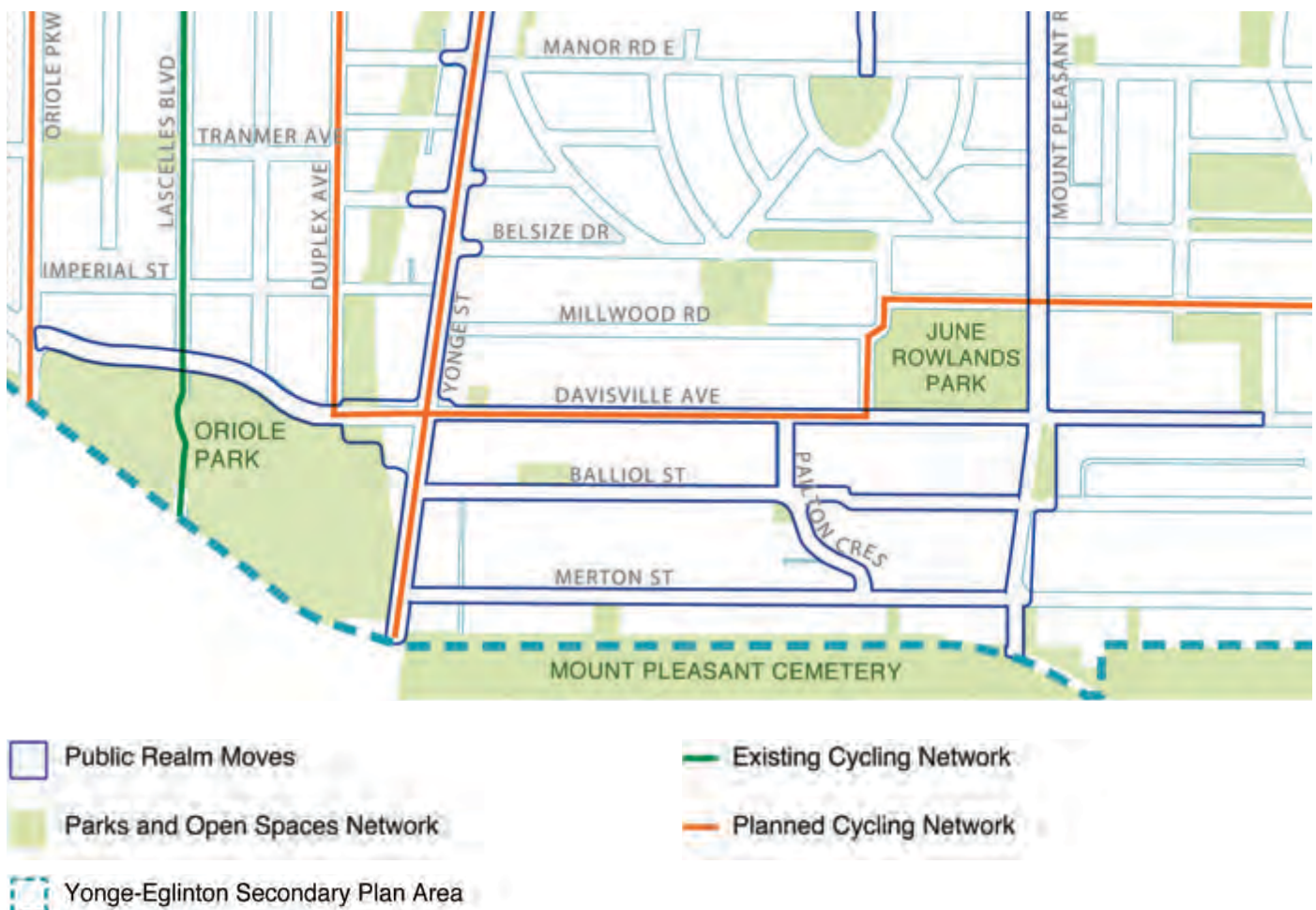


Figure 93 Map of the Cycling Network Plan within the Davisville Community Street area

Background Analysis

Existing Street Conditions

- 18.3m right-of-way (ROW)
- 13.0m vehicular ROW, including two vehicular travel lanes a parking lane
- Two-way street with parking on the south side of the street
- Substandard pedestrian clearway on narrow sidewalks on both sides of the street
- Building setbacks range from 1.6m-11.2 m
- TTC Transit Stop
- Bike Share Toronto station on the north side of the street
- Two signalized crosswalks mid-block
- Pedestrian north-south and east-west crosswalks at the intersections with Mount Pleasant Road
- No cycling infrastructure and cyclists travel in mixed traffic with vehicles



Figure 94 Existing crosswalk along Davisville Avenue



Figure 96 Existing conditions along Davisville Avenue



Figure 95 Existing Bike Share Station



Figure 97 Existing TTC shelter on the south side

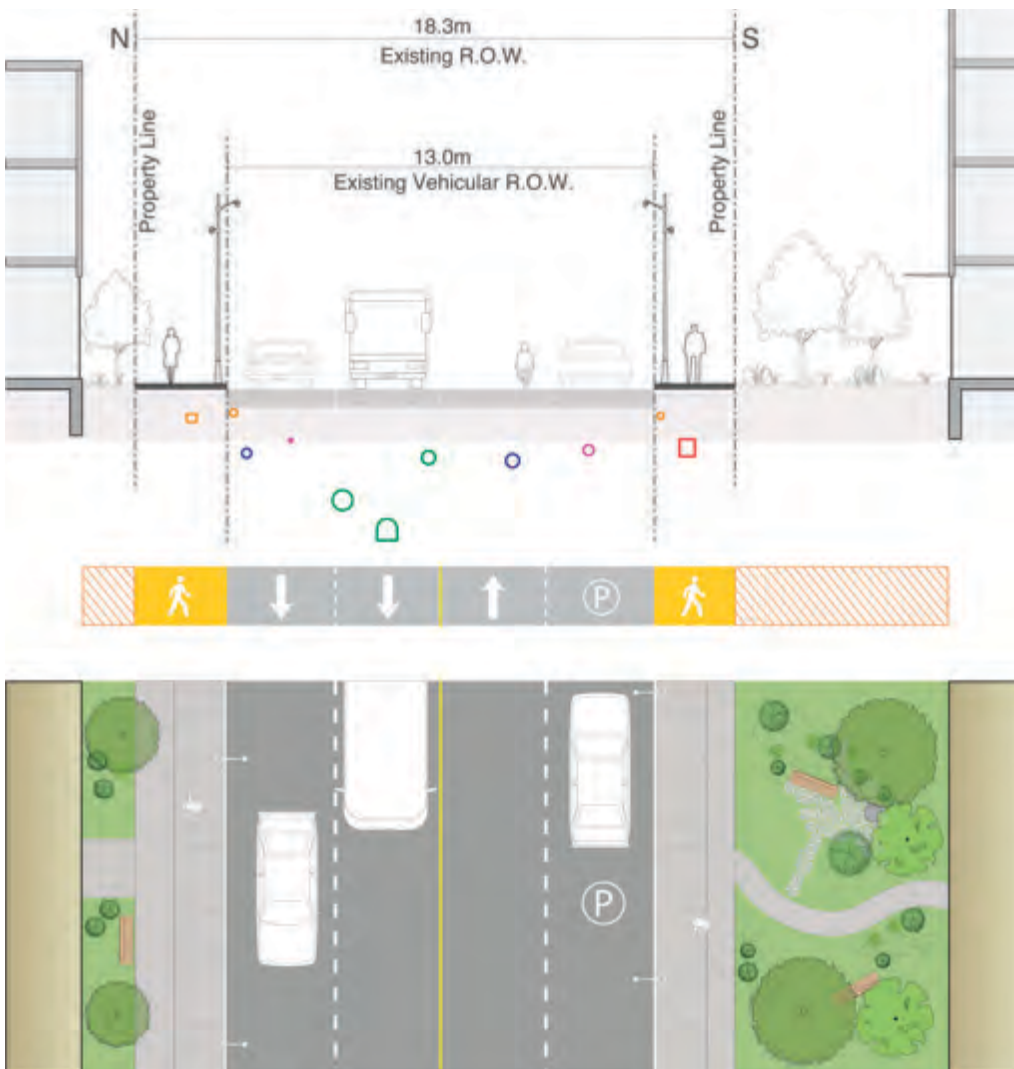


Figure 98 Section and Plan of the Existing Conditions along Davisville Avenue

Secured Section 37 Contributions

Recent development in the area has provided community benefits through Section 37 contributions to the City. There is one development application recently completed and built in Ward 12, comprising two sites along 87-107 Davisville Avenue and 108-128 Balliol Street. With the approval of this application, the City secured \$100,000, adjusted for interest to \$105,401 as of December 31st, 2021, towards widening the public sidewalk in front of 87-107 Davisville Avenue.

Summary of Opportunities and Constraints

Sites with Redevelopment Potential

With development applications under review, public realm improvements will occur incrementally. In the near future, a few limited soft sites with redevelopment potential have been identified. It is possible that in the future, other sites may come forward through consolidation and infill which could present opportunities for extending the public realm. As these sites come forward for development, the adjacent public realm improvements will be provided by the developer, where possible. Where there are potential sites for infill development and the existing building is located within the setback requirement,

development will be required to maintain the existing setback as a minimum. This is in addition to the expectation to provide public realm improvements within the setback.

With only a few current development applications under review or approved, it will be challenging to implement a complete public realm upgrade solely through development in the near future. There is one recently constructed development at 87-107 Davisville Avenue which includes a landscape setback. As well, there is one proposed application at the southwest corner of Davisville Avenue and Pailton Crescent, where staff are working to secure a landscape setback as part of the development review process.

Development activity is otherwise generally limited on Davisville Avenue. Two sites are designated as constrained and are located near the intersection with Yonge Street and on the site of the redeveloping Davisville School. This is due to a heritage designation and recent development activity. Constrained sites have limited development potential to provide landscape setbacks that are at least 3.0m on the north side of Davisville Avenue and 5.0m on the south side. The remaining majority of the lots along Davisville Avenue may experience future development that includes public realm improvements on their sites, implemented through the development review process.

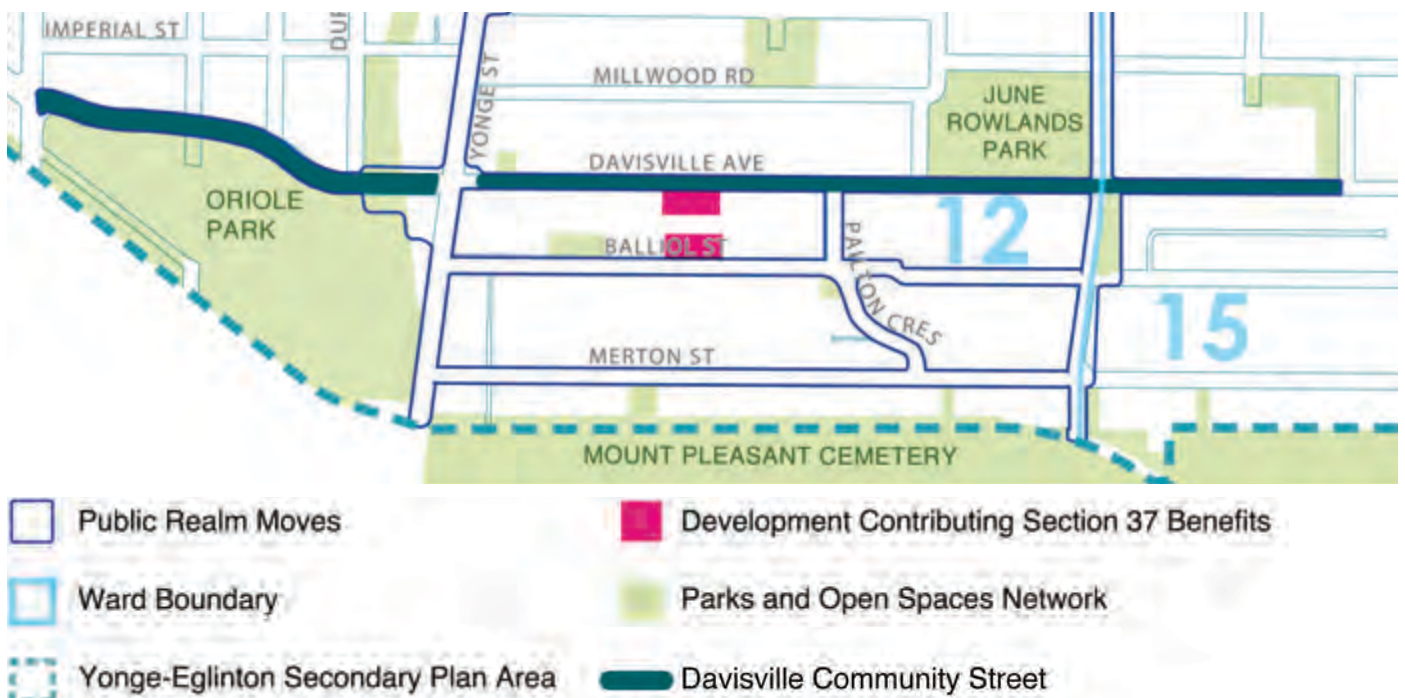


Figure 99 Map of the Section 37 secured contribution locations along the Davisville Public Realm Move

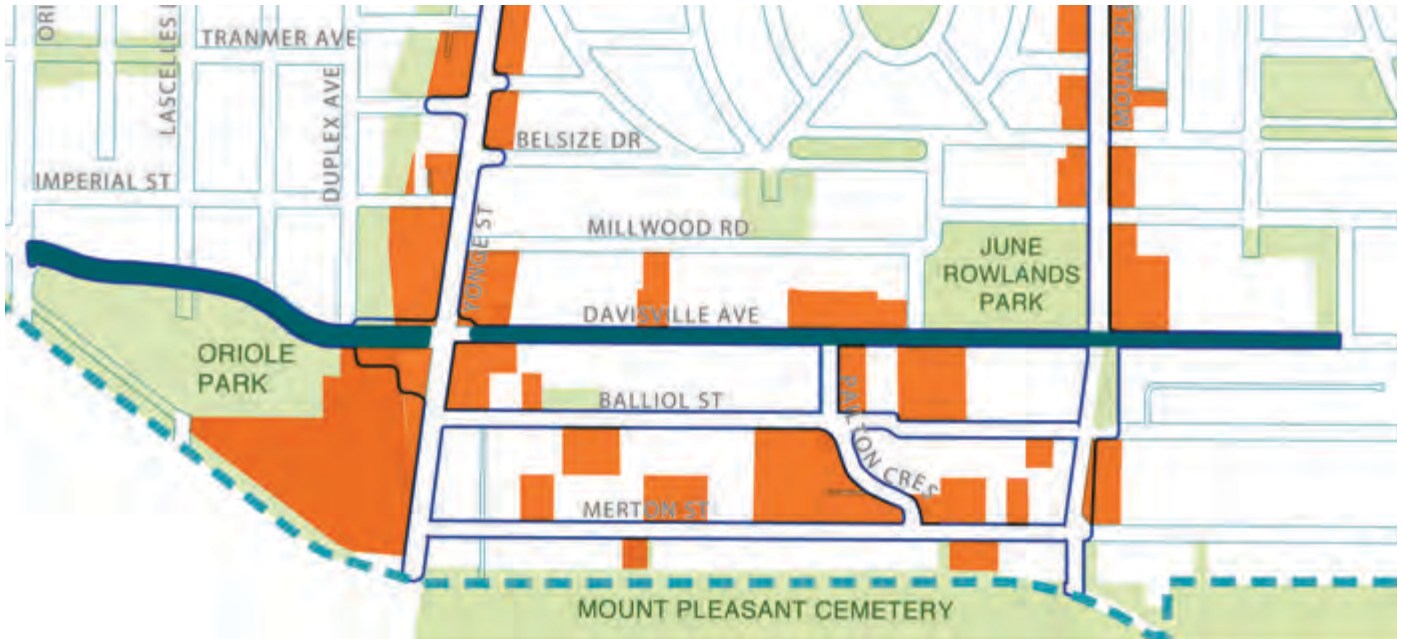


Figure 100 Map of potential redevelopment sites along the Davisville Community Street



Figure 101 Diagram of opportunities and constraints at Davisville Avenue

Utility Locations and Key Issues

Many of the above-ground facilities such as hydro poles, fire hydrants and utility vaults are located immediately adjacent to the sidewalks, hindering opportunities for widening.

Where planted bump-outs are proposed, existing catch basins may be in conflict and will need to be relocated or incorporated into the bump-out design.

Both existing 150mm and 300mm watermains are located in the vehicular portion of the right-of-way, limiting future conflicts with proposed street trees. Refer to existing condition sections for utility locations.

Additional grading challenges exist in front of June Rowlands Park with stairs, hand railings and retaining walls located in the public realm. Some of the retaining walls are protecting the roots of the existing mature trees.

The existing TTC shelter in front of June Rowlands Park will need to be relocated to accommodate a new bump-out and create safe pedestrian, cycling and ridership conditions.



Figure 102 Existing stairs at June Rowlands Park



Figure 103 Existing retaining walls at June Rowlands Park



Figure 104 Existing TTC Shelter at June Rowlands Park



Figure 105 Existing sidewalk transition at June Rowlands Park



Figure 106 Existing hydro poles along Davisville Avenue



Figure 107 Existing fire hydrants along Davisville Avenue



Figure 108 Existing utility conflicts along Davisville Avenue



Figure 109 Existing utilities along Davisville Avenue

Final Condition Recommendations:

- Expand existing ROW from 18.3m to 23.0m to accommodate raised bikeway and widened sidewalks
- Provide a bump-out on the north side in front of June Rowlands Park to further extend park onto the street. Relocate on-street parking to the north side to allow for bump-outs in front of June Rowlands Park
- Reduce the vehicular ROW to 8.8m to include two vehicular lanes, to accommodate bus service and one on-street parking lane
- Provide raised bikeway with an additional raised buffer between bikeway and parking lane
- Provide a building setback from the property line to the building face - a minimum 3.0m on the north side and a minimum 5.0m on the south side of the street
- Provide a minimum 1.5m depth of planting soil on top of any underground structures in the setback to support new, healthy tree growth
- Provide above standard width sidewalk on both sides of the street
- Provide a wide green boulevard to include urban rain garden with tree planting and soil cells, except at intersections where existing buildings to remain
- Relocate on-street parking impacted by the size and locations of the future bump-outs
- Provide planted bump-outs at existing and future pedestrian crossings
- Relocate existing Bike Share Toronto station into the green boulevard area between planted areas with trees
- Relocate existing TTC shelters away from pedestrian circulation, while allowing access to the shelter
- Relocate existing light standards/ hydro poles outside of the new pedestrian clearway (or, if possible, relocate hydro underground)
- Design the intersection of Yonge Street and Davisville Avenue as a Yonge Street Special Square (Refer to Section 4.9)



Figure 110 TTC Shelter and raised Bikeway on Roncesvalles Avenue



Figure 112 Raised Bikeway on Bloor Street

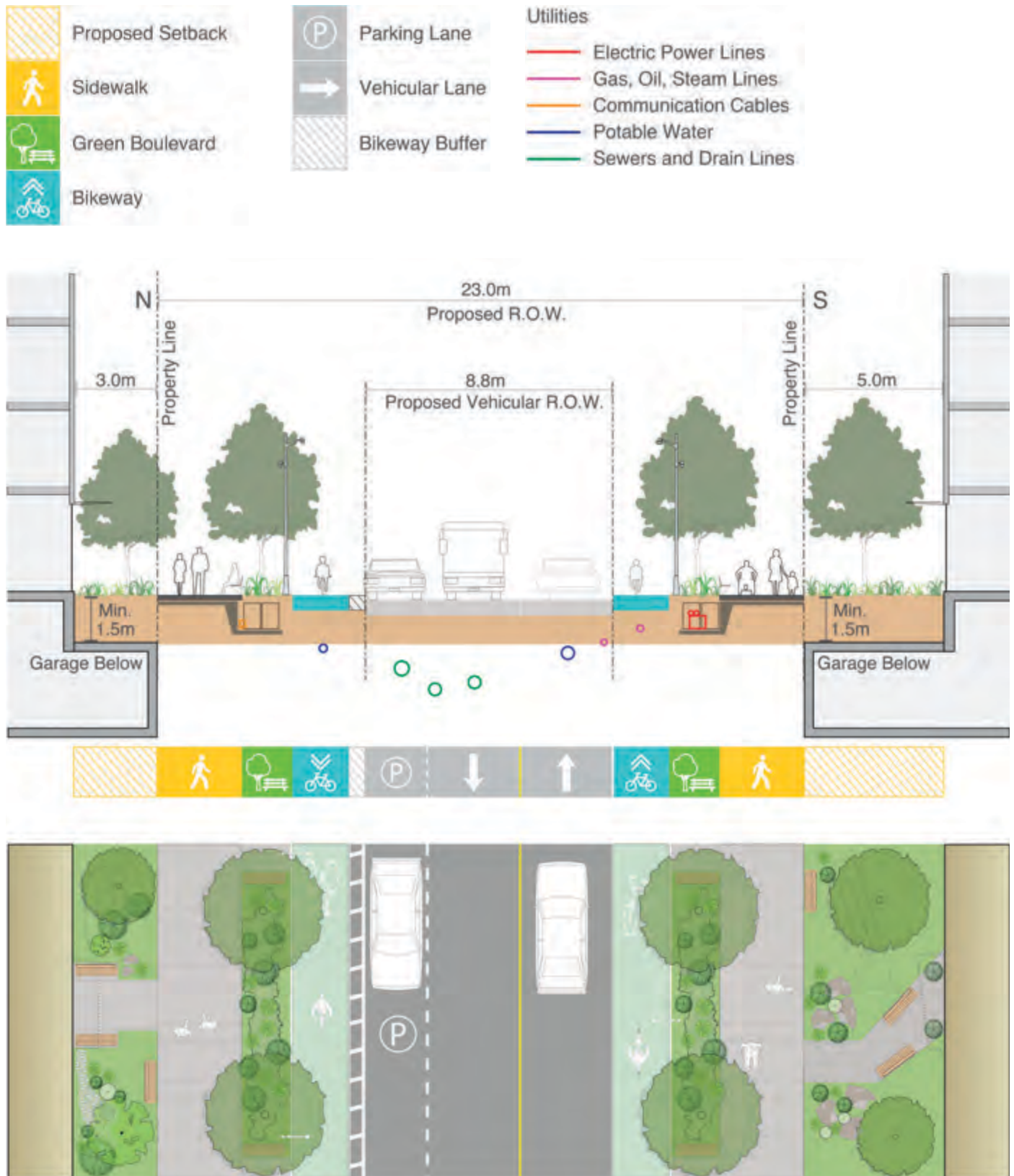


Figure 111 Open planters with seating along Roncesvalles Avenue



Figure 113 Planted bump-out at Gerrard Street East and Pembroke Street

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.



SECTION 4.3

Figure 114 Section and Plan of Davisville Community Street - Recommended Final Condition

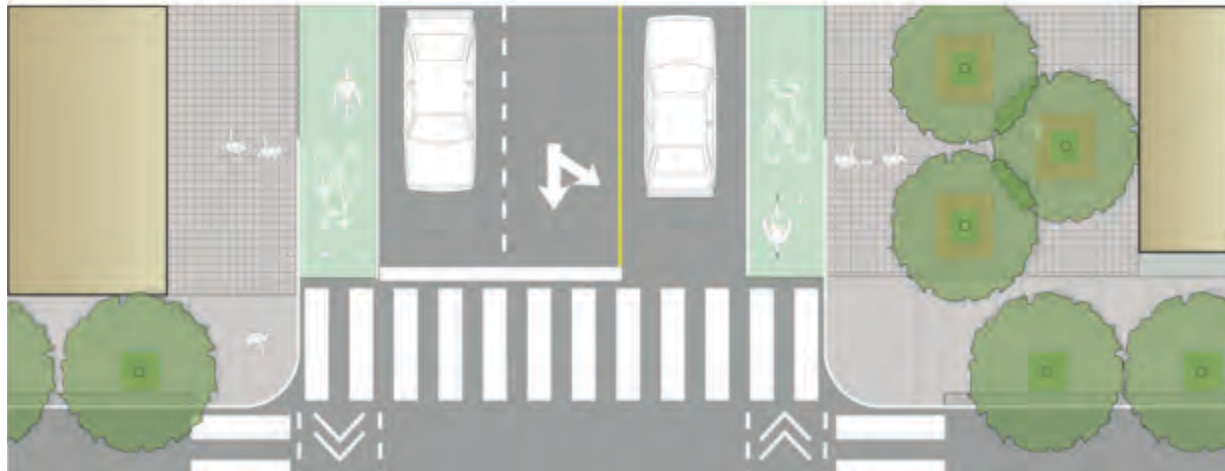
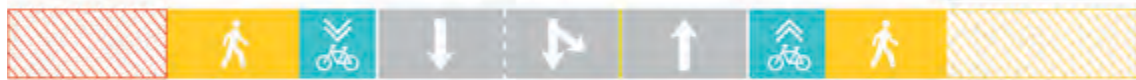
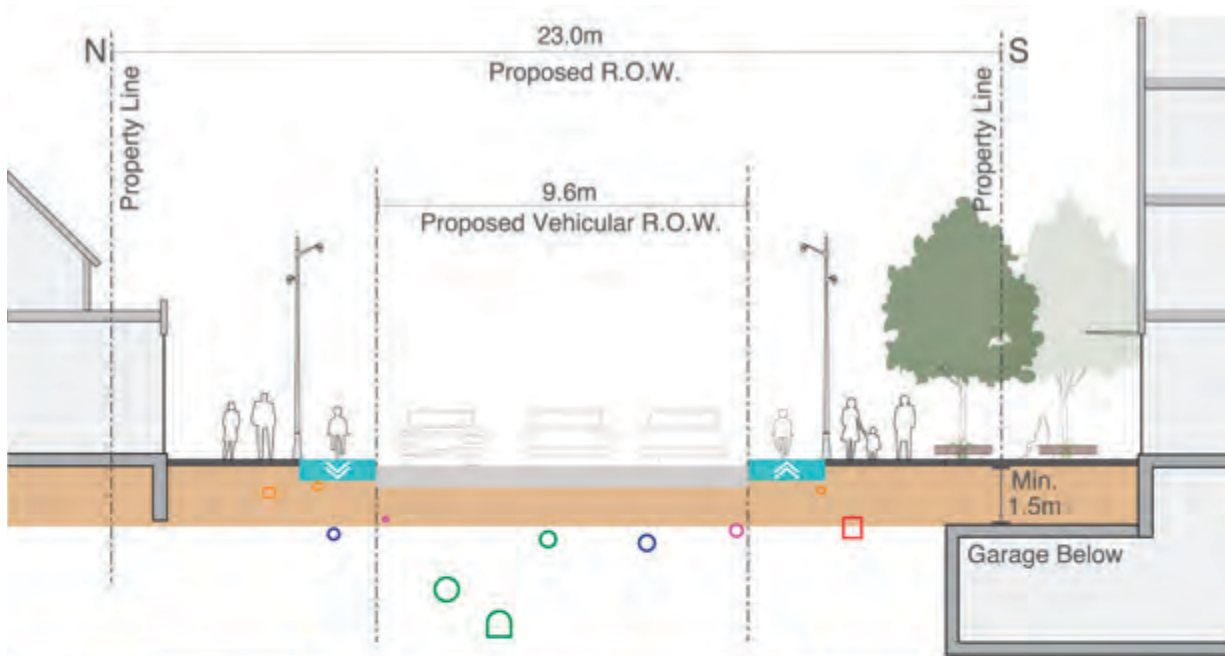


Figure 115 Section and Plan of Davisville Community Street - Recommended Final Condition at Yonge intersection

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.



SECTION 4.3

Figure 116 Section and Plan of Davisville Community Street - Recommended Final Condition at bump-out in front of June Rowlands Park



Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

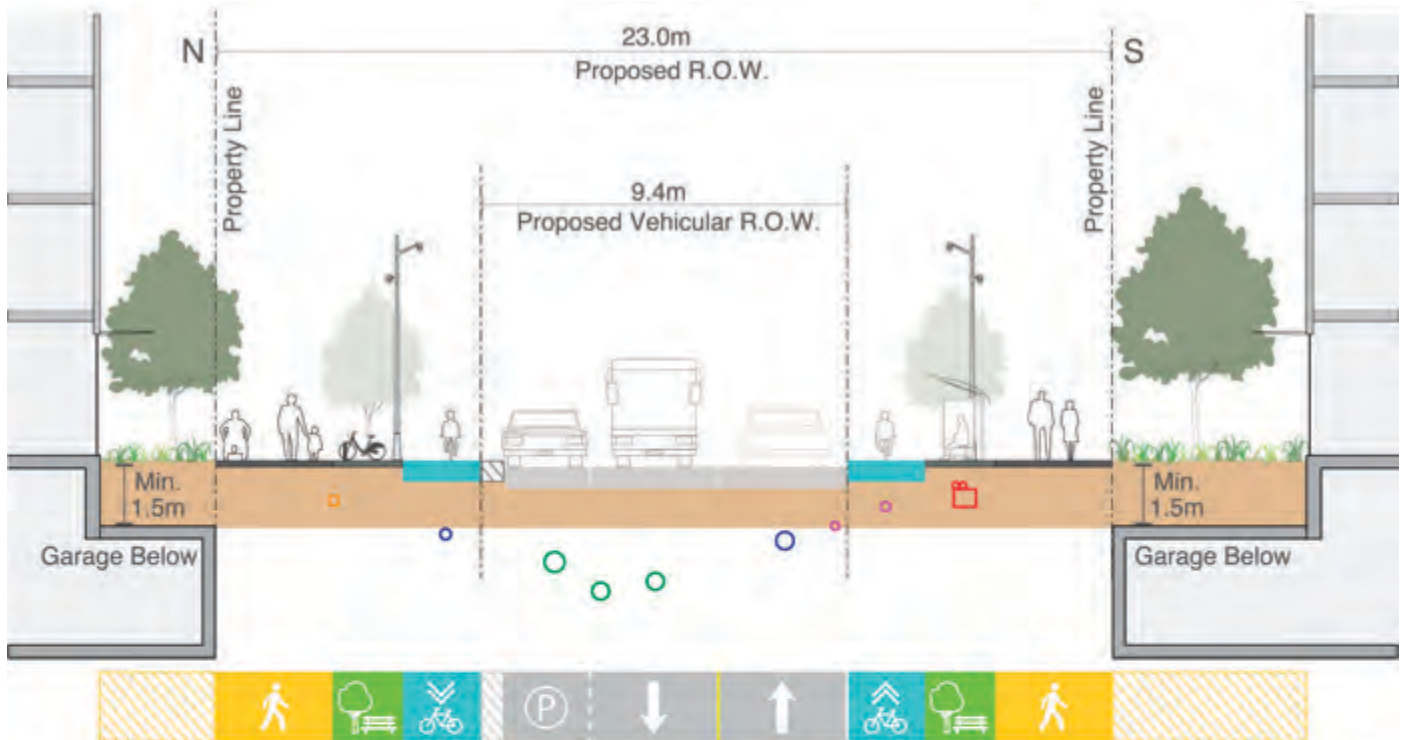


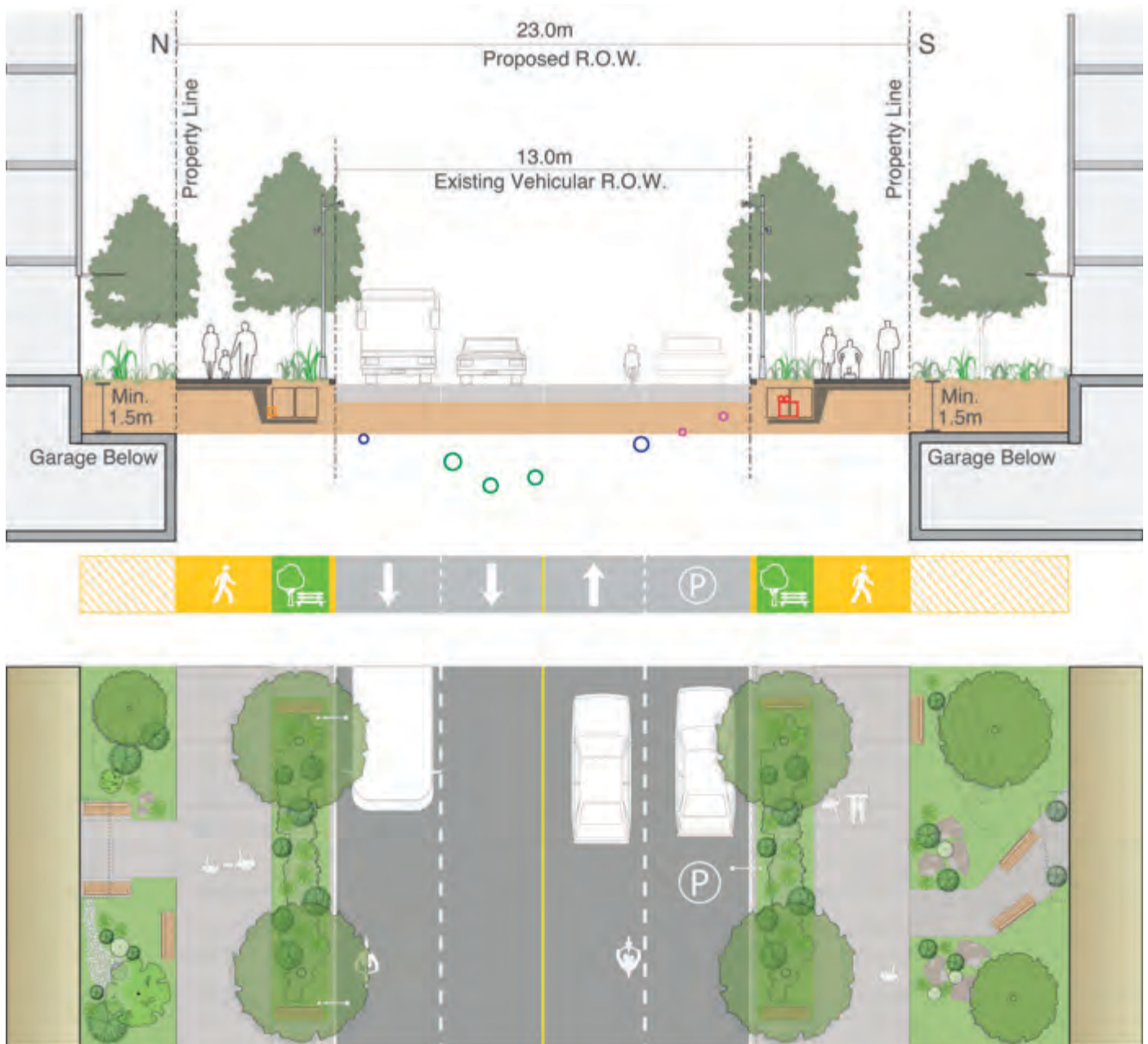
Figure 117 Section and Plan of Green Boulevard and Bike Share Station (North side), and Green Boulevard and TTC Shelter (South side) - Recommended Final Conditions

Implementation

Development-related Improvements

With few potential redevelopment sites along Davisville Avenue, partial implementation of the public realm can be secured through the development review process. Pending future ROW widening, and in cases where raised bikeway construction and roadway narrowing have not occurred, but new development is being proposed, it is the developer's

responsibility to upgrade the public realm and to futureproof for the final condition. The upgrade will include a minimum 3.0m setback on the north side and a 5.0m setback on the south side of the street, 3.0m wide sidewalk and 1.8m wide green boulevard with trees and planters.



SECTION 4.3

Figure 118 Section and Plan of Davisville Community Street - Recommended Interim Condition to be implemented by Developer

Capital Works and Coordination

Opportunities for public realm improvements may arise through upcoming capital works projects led by Toronto Water and Transportation Services. Taking advantage of other planned works represents an opportunity to coordinate and implement public realm improvements in the near-term and avoids disruption from additional construction work in the future. An internal ECS project will implement streetscape improvements at the intersection of Oriole Parkway and Chaplin Crescent in 2022.

Note: The implementation timeline represents information available from divisional partners and data from T.O. Inview as of December 31st, 2021 and is subject to change.

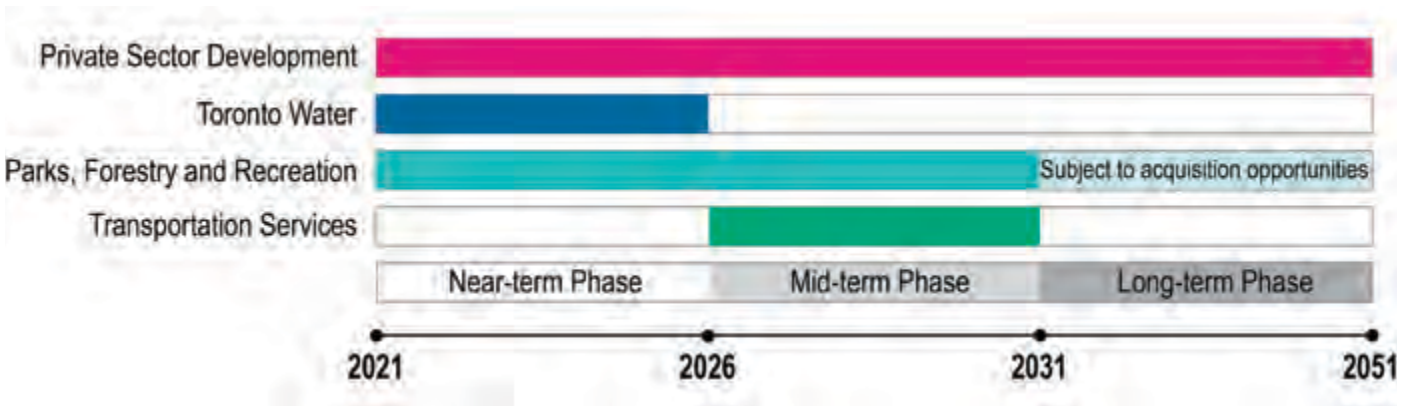


Figure 119 Projected Implementation Timeline for the Davisville Community Street



Figure 120 Checklist of Public Realm Elements to be implemented for the Davisville Community Street

4.4

Merton Street Promenade

Overview

Background Analysis

Final Condition Recommendations

Implementation

Overview

Merton Street Promenade is an east-west street in Davisville between Yonge Street and Mount Pleasant Road with multiple connection points to the Beltline Trail. Merton Street is envisioned to have a wide, tree-lined pedestrian promenade to support everyday activities and reinforce the Pailton Crescent commercial cluster as a neighborhood destination.

Policy Direction

The Yonge-Eglinton Secondary Plan includes the following policies related to the Merton Street Promenade Public Realm Move:

3.2.10. Merton Street Promenade

- a. The Merton Street Promenade will consist of wide sidewalks and landscaped boulevards to support pedestrian activity, the eclectic mix of uses along the street and the local commercial hub at Pailton Crescent.
- b. Development will provide a reasonable setback at grade and above grade on both sides of the street. Additional setbacks will be provided on large sites and publicly-owned sites.
- c. Development will provide and improve connections to the Kay Gardner Beltline Trail.

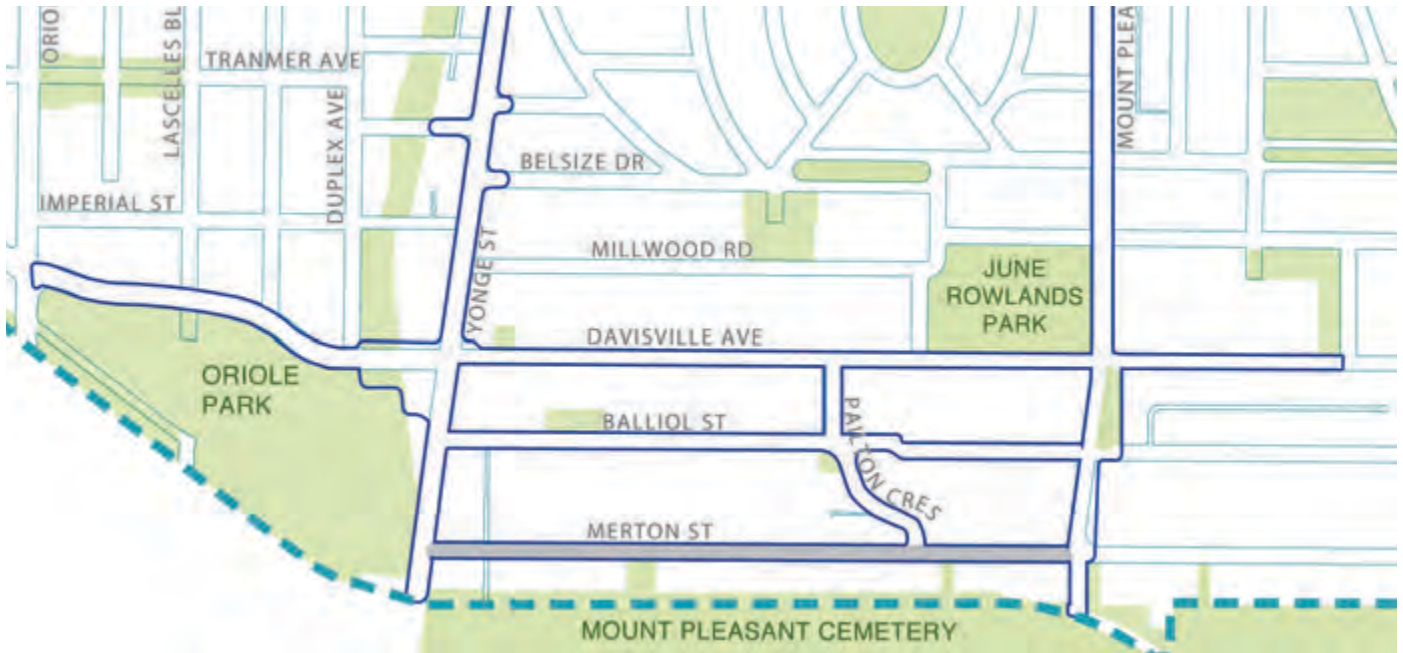


Figure 121 Map showing the location of the Merton Street Promenade Public Realm Move

Background Analysis

Existing Street Cross-Sections

- 20.1m right-of-way (ROW)
- Two-way street with on-street parking on the north side
- 11.4m vehicular ROW, including two travel lanes and a parking lane
- Substandard pedestrian clearway on narrow sidewalk on the south side and a wider sidewalk on the north side
- Building setbacks range from 0.0m - 8.9m
- Existing signalized crosswalks mid-block
- Bump-outs on both sides of street closer to the Yonge street intersection and at the signalized crosswalk at Pailton Crescent
- Boulevard parking on some properties
- Two Bike Share Toronto Stations - one on each of the north and south sides
- Pedestrian north-south and east-west crosswalk at the intersection with Pleasant Road and north-south crosswalk at Yonge Street



Figure 122 Existing bump-out at Merton Street and Pailton Crescent



Figure 123 Existing boulevard parking



Figure 124 Existing Toronto Bike Share Station



Figure 125 Existing crosswalk

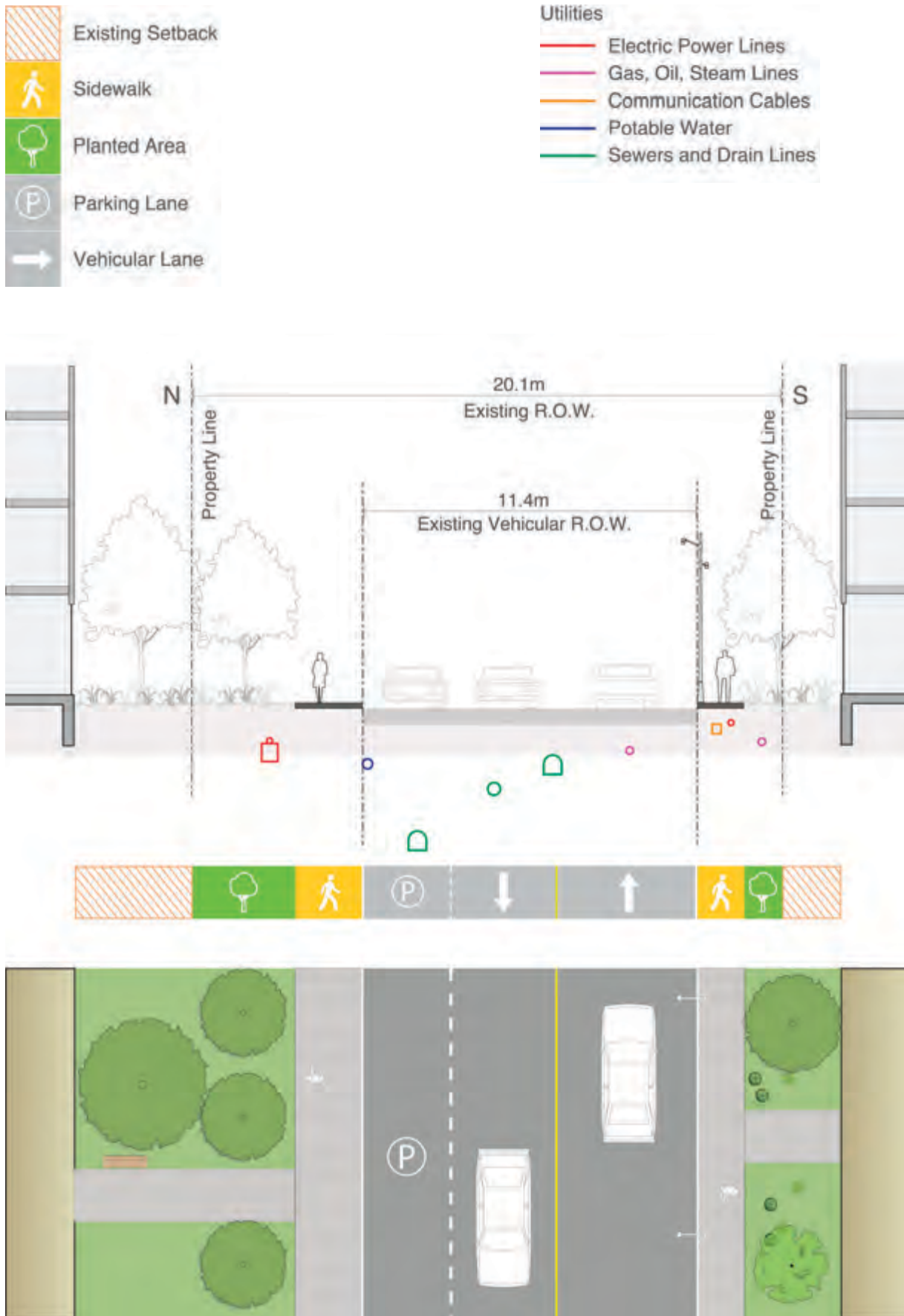


Figure 126 Section and Plan of the Existing Conditions along Merton Street

Secured Section 37 Contributions

There are currently no development applications along Merton Street where Section 37 contributions have been secured for the Merton Street Promenade. However, development applications will continue to implement streetscape improvements in front of their sites through the development review process.

Summary of Opportunities and Constraints

Sites with Redevelopment Potential

With only a few current development applications under review, it will be challenging to implement a complete public realm upgrade through development only in the near future. As the remaining soft sites come forward for development, the public realm improvements will be provided by the developer, where possible. Additionally, public realm upgrades can be expanded beyond the property line to provide enhancements in front of properties that have been recently constructed and will not be redeveloped in the near future.

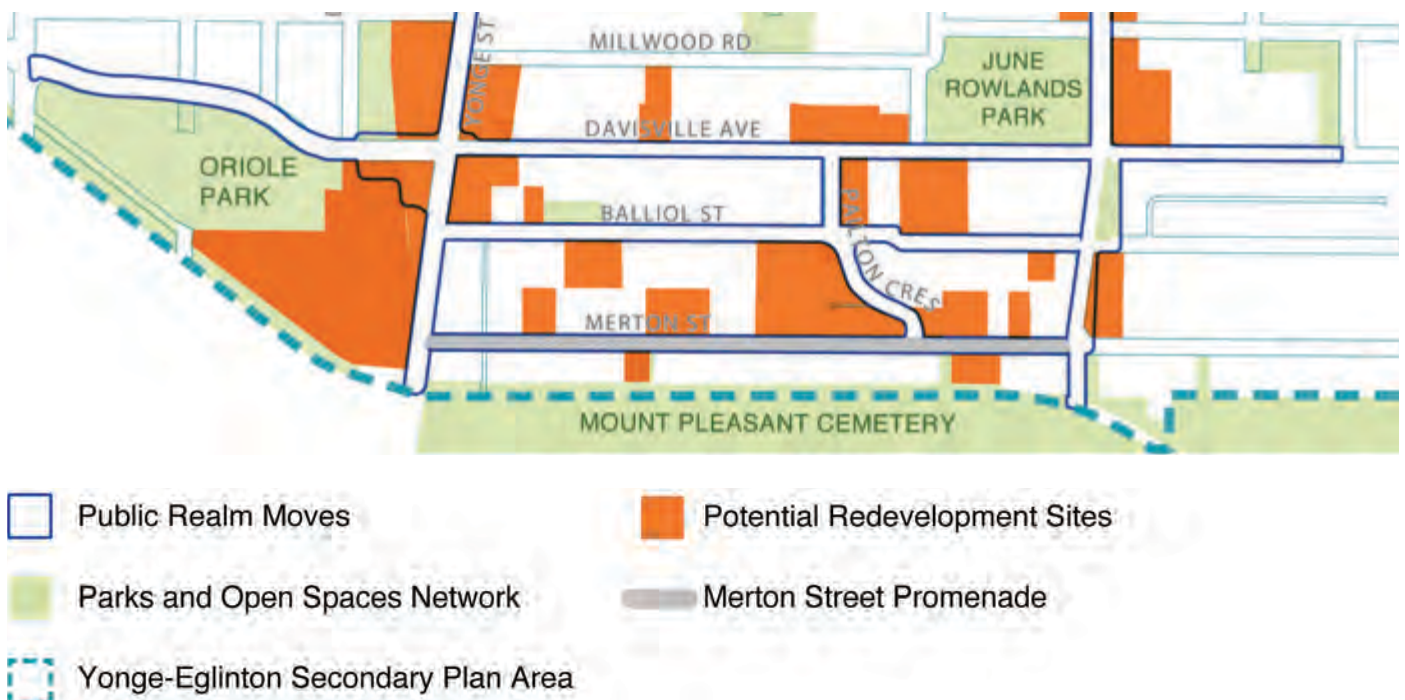


Figure 127 Map of potential redevelopment sites along the Merton Street Promenade

There are three active development applications which are located on the north side of Merton Street. This includes two approved development applications, which has secured landscape setbacks of at least 4.0m. There is also one proposed application where staff are working to secure landscape setbacks through the development review process.

There is relatively little development activity on Merton Street. There are two lots that are constrained, with limited potential to provide landscape setbacks of at least 4.0m on the north side of Merton Street and 3.0m on the south side. This is due to the recent construction of new buildings on these sites. The remaining lots may experience future development activity that includes public realm improvements on their sites, implemented through the development review process.



Figure 128 Opportunities for connection with Kay Gardner Beltline Trail



Figure 129 Opportunities for pedestrian crossing at mid-block connections



Figure 130 Diagram of opportunities and constraints at Merton Street

Utility Locations and Key Issues

Similar to the rest of the Midtown Public Realm Moves, above-ground facilities such as hydro poles, fire hydrants and utility vaults are located immediately adjacent to or on the sidewalks, hindering sidewalk widening.

Where planted bump-outs are proposed, existing catch basins may be in conflict and will need to be relocated or incorporated into the bump-out design.

An existing 150mm watermain is located below the north sidewalk in close proximity to the existing curb. With vehicular roadway narrowing, introduction of the bump-outs with rain gardens and strategic placement of the street trees, conflicts between tree planting and existing watermain can be avoided.

Additional constraints are similar to those found on other streets, such locations of hydro poles, parking meters and other above ground utilities in the middle of or immediately adjacent to sidewalks, limiting sidewalk widening opportunities.

Grading challenges occur throughout the length of the street, resulting in stairs, ramps and retaining walls encroaching onto the public realm.

Existing boulevard parking areas create an unsafe condition with additional curbs cuts and cars making turns across the sidewalk, limiting pedestrian use.

There are multiple opportunities along the south side of the street to provide to connections to the Kay Gardner Beltline Trail. With on-street parking located on the north side of the street, north side public realm expansion can occur by decreasing parking lane width without reducing parking capacity.



Figure 131 Existing hydro poles along Merton Street



Figure 132 Existing utility challenges along Merton Street



Figure 133 Existing utilities along Merton Street



Figure 134 Existing utility conflicts along Merton Street

Final Condition Recommendations:

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

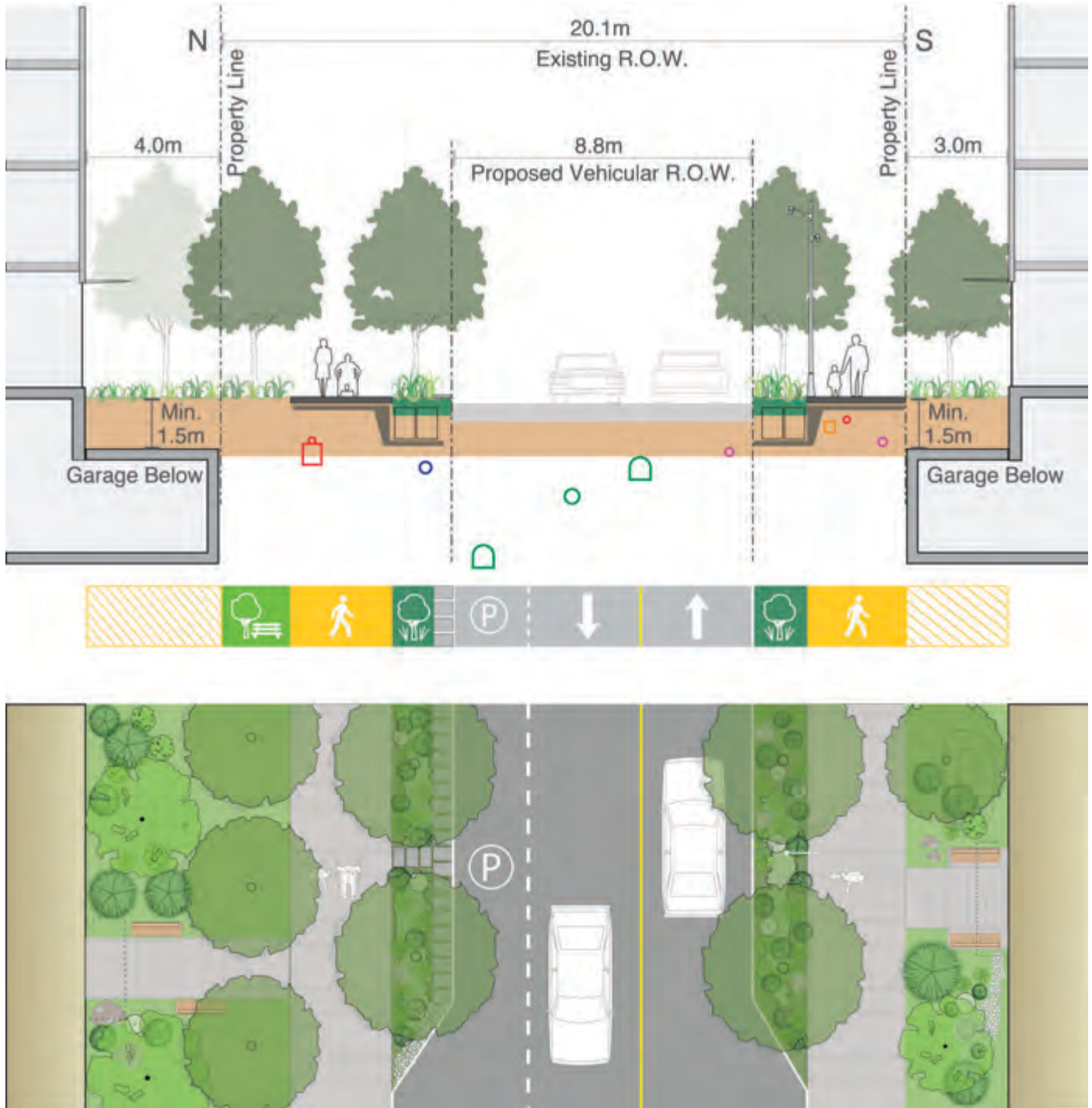







Figure 135 Section and Plan of Merton Street Promenade - Recommended Final Condition

- Narrow the vehicular right-of-way from 11.4 to 8.8m with two lanes of traffic and on-street parking, with bump-outs in strategic locations
- Narrow the vehicular right-of-way from 11.4 to 9.6m with two lanes of traffic and a turning lane at intersections with Yonge Street and Mount Pleasant Road
- Create an allee of trees on the north side of the street and a pedestrian promenade above standard width
- Introduce rain gardens capable of growing large trees adjacent to the curb on both sides of the street
- Provide sidewalks above standard width on both sides of the street
- Provide a building setback from the property line to the building face - a minimum 4.0m on the north side and a minimum 3.0m on the south side of the street, or maintain existing landscape setbacks with any new redevelopment if they exceed the above-mentioned setbacks
- Provide a minimum 1.5m of planting soil on top of any underground structures in the setback to support new, healthy tree growth
- Relocate existing Bike Share Toronto station into future bump-out
- Integrate existing bump-outs with the future continuous bump-outs along the street
- Remove all existing boulevard parking to upgrade, green and expand the public realm
- Relocate on-street parking impacted by the size and locations of the future bump-outs
- Upgrade existing bump-outs into planted areas and provide raised pedestrian crossings to upgrade existing crossings and create new mid-block connections
- Enhance pedestrian connections to Beltline Trail and Mount Pleasant Cemetery
- Relocate existing light standards/ hydro poles outside of the new pedestrian clearway (or, if possible, relocate hydro underground)

Utilities

	Electric Power Lines
	Gas, Oil, Steam Lines
	Communication Cables
	Potable Water
	Sewers and Drain Lines

	Proposed Setback
	Sidewalk
	Promenade Garden
	Rain Garden
	Parking Lane
	Vehicular Lane
	Buffer Strip

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

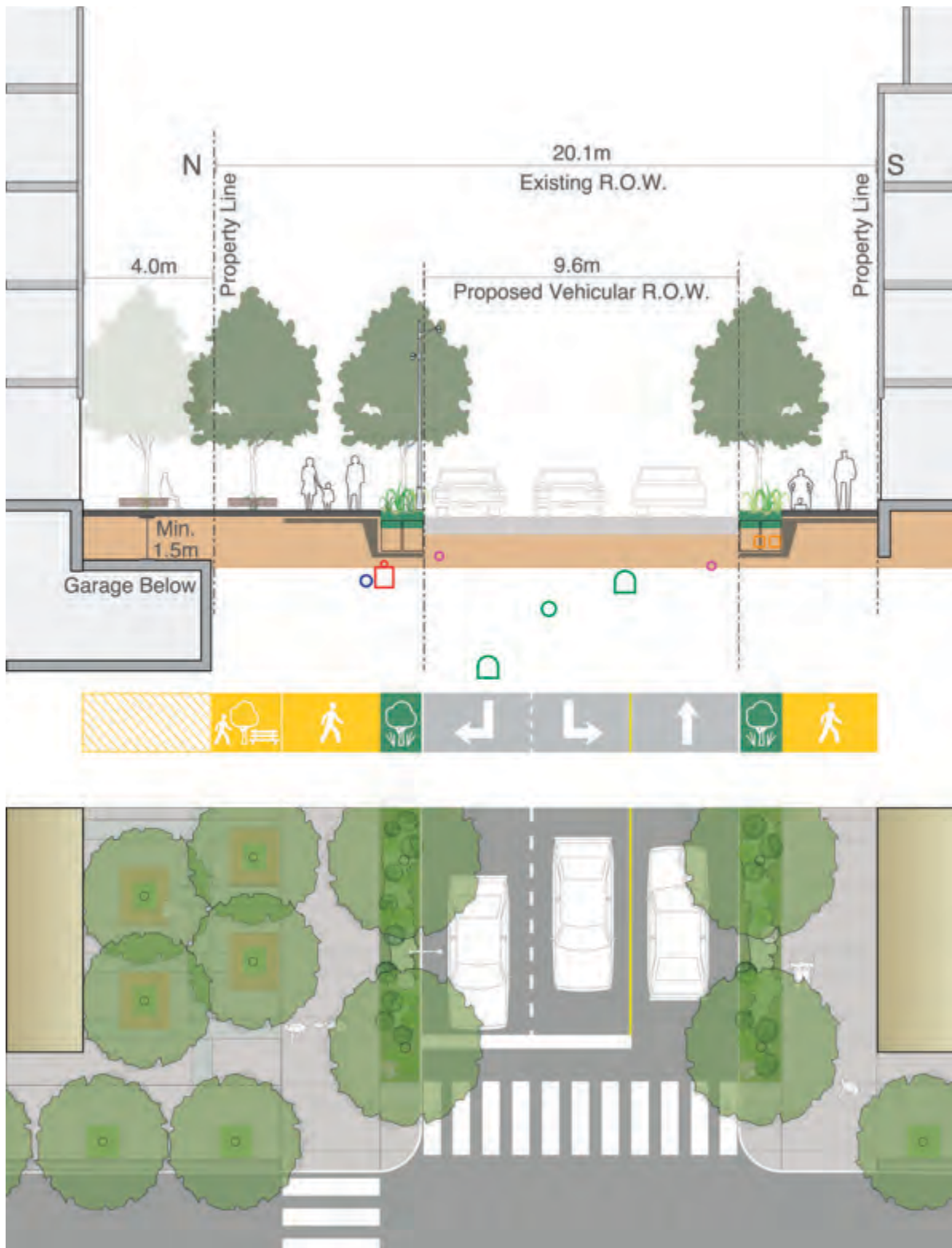


Figure 136 Section and Plan of Merton Street Promenade - Recommended Final Condition at intersection with Yonge Street

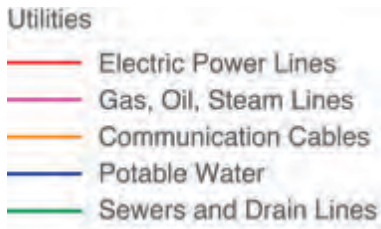


Figure 137 Plan of Merton Street Promenade - Recommended Final Condition of bump-out and raised crossing at mid-block connection

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.



Figure 138 Section and Plan of Merton Street Promenade - Recommended Interim Condition on the north side of the street to be implemented by development

Implementation

Development-related Improvements

With only a few development applications under review, public realm improvements will happen incrementally. As soft sites start to redevelop, the upgraded public realm will become more continuous.

Pending the entire street narrowing, the developer will be responsible for providing an interim condition with a narrow roadway in front of their property on the north side of the street, resulting in a temporary bump-out. Additional improvements include a double row of trees with 3.0m sidewalk and a sidewalk transition zone to meet the existing condition. The interim condition on the south side will include a 2.9 m expanded sidewalk.

New development will be responsible for providing a 4.0m building setback on the north side and a 3.0m building setback on the south side of the street; or maintain existing landscape setbacks with any new redevelopment that exceed the aforementioned setbacks.

Capital Works and Coordination

Opportunities for public realm improvements may arise through upcoming capital works projects led by Toronto Water and Transportation Services. Taking advantage of other planned works represents an opportunity to coordinate and implement public realm improvements in the near-term and avoids disruption from additional construction work in the future.

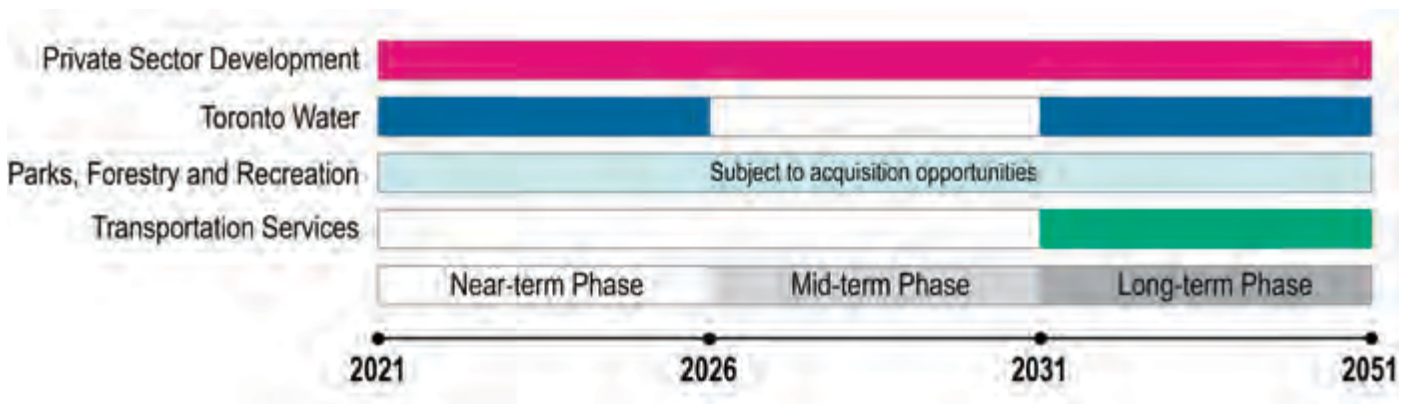


Figure 139 Projected Implementation Timeline for the Merton Street Promenade

Note: The implementation timeline represents information available from divisional partners and data from T.O. Inview as of December 31st, 2021 and is subject to change.



Figure 140 Checklist of Public Realm Elements to be implemented for the Merton Street Promenade

4.5

Pailton Crescent Connector

Overview

Background Analysis

Final Condition Recommendations

Implementation

Overview

This north-south, local street will connect an improved Pailton Crescent's commercial, retail and social hub at Merton Street to the future urban park at the southwest corner of Balliol Street and Davisville Avenue. The street is envisioned to have expanded sidewalks with large green boulevards to create a safe and pleasant pedestrian experience. Boulevard parking at Merton will be removed to continue the sidewalk and create a gathering space with patios and planted areas with seating.

Intersection narrowing at both Balliol and Merton Streets will provide additional space for new and improved destinations at Pailton's commercial hub and Balliol's new park.

Policy Direction

The Yonge-Eglinton Secondary Plan includes the following policy related to the Pailton Crescent Connector Public Realm Move:

3.2.12. Pailton Crescent Connector

- a. Pailton Crescent is one of the few north-south streets in the Davisville area. It will continue to support a lively local commercial hub at Merton Street. Sidewalks will be widened and extended on both sides of the street. Landscaping along the street and the reclamation of excess pavement width at Balliol Street to create a plaza or square will be undertaken as part of development to enhance the street and the local commercial hub.

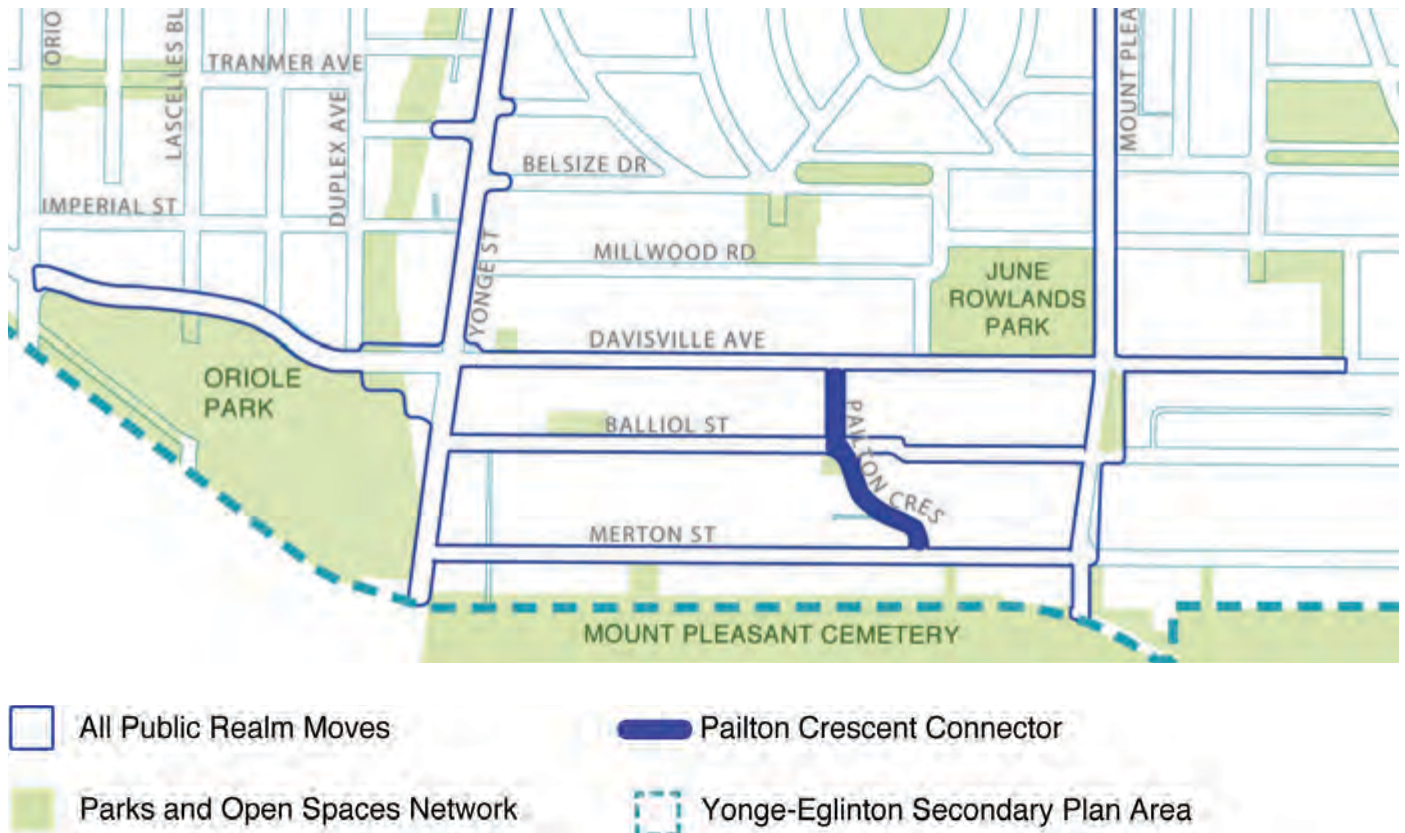


Figure 141 Map showing the location of the Pailton Crescent Connector Public Realm Move

Background Analysis

Existing Street Cross-Sections

- 20.0m right-of-way (ROW)
- 8.6m vehicular ROW
- Two-way street with on-street parking on the east side of the street between Balliol Street and Davisville Avenue
- Two-way street with no on-street parking between Balliol Street and Merton Street
- The ROW at Balliol Street intersection is approximately 46.0m wide, with a 22.0m vehicular ROW at the Merton Street intersection
- Substandard pedestrian clearway on narrow sidewalk on both side of the street
- No sidewalk exists on the west side of the street, half a block south of Balliol Street towards Merton Street, due to existing boulevard parking
- Building setbacks range from 0.0m – 38.0m
- Existing boulevard parking on the west side on the street
- Setbacks range from 0.0m – 38.0m



Figure 142 Existing extra wide ROW at intersection with Balliol Street



Figure 143 Existing condition with no sidewalk along west side of Pailton Crescent



Figure 144 Existing condition between Balliol Street and Davisville Avenue



Figure 145 Existing condition along Pailton Crescent

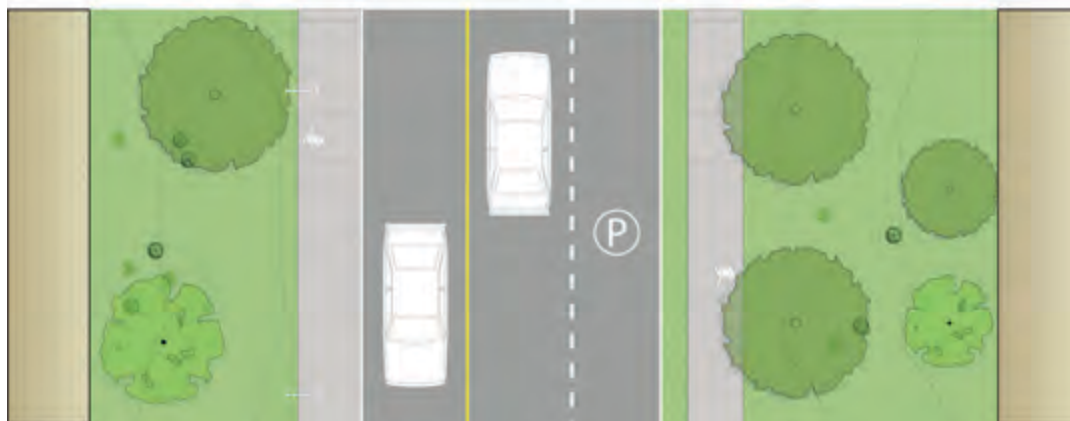
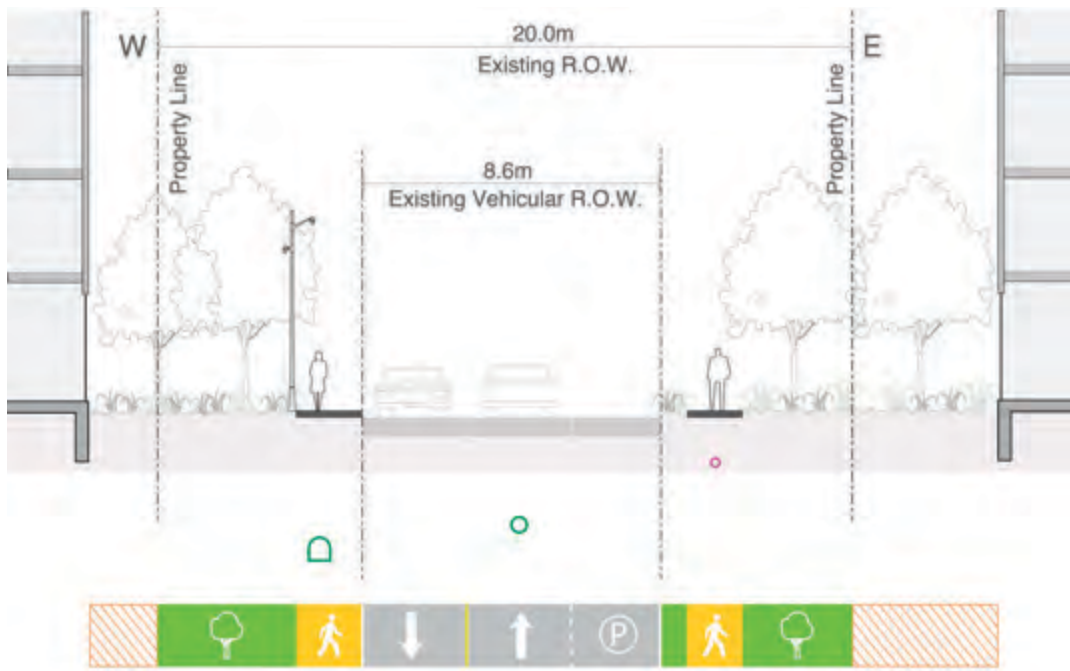


Figure 146 Section and Plan of the Existing Conditions along Pailton Crescent between Davisville Avenue and Balliol Street



Figure 147 Section and Plan of the Existing Conditions along Pailton Crescent between Balliol and Merton Streets

Secured Section 37 Contributions

There are currently no development applications along Pailton Crescent with secured Section 37 contributions for the Pailton Crescent Connector. However, development applications will continue to deliver streetscape improvements through the development review process.

Summary of Opportunities and Constraints

Sites with Redevelopment Potential

There are two active applications on the west side of Pailton Crescent. Between Davisville Avenue and Balliol Street, there is an approved application which includes a landscape setback. There is also a proposed application on the southwest corner of the intersection with Balliol Street, where staff are working to secure a landscape setback through the development review process.

The remaining sites on Pailton Crescent are still available for redevelopment and infill. As these sites come forward for development, the public realm improvements will be provided by the developer, where possible. Additionally, public realm upgrades can be expanded beyond the property line to provide enhancements in front of properties that have been recently constructed and will not be redeveloped in the near future.

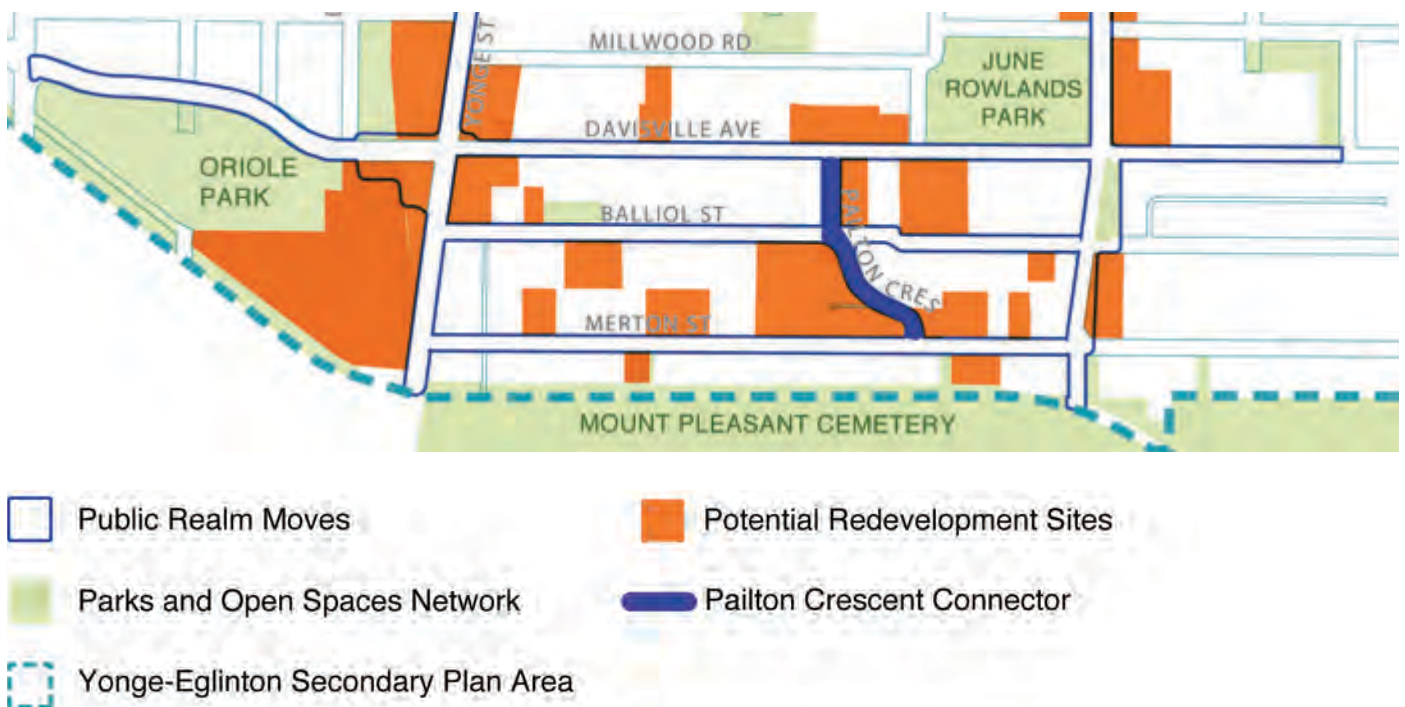


Figure 148 Map of potential redevelopment sites along the Pailton Crescent Connector

Utility Locations and Key Issues

Similar to the rest of Midtown Public Realm Moves, above-ground facilities such as hydro poles, fire hydrants and utility vaults are located immediately adjacent to the sidewalks and hinder sidewalk widening.

Where intersection narrowing is contemplated, existing catch basins may be in conflict and will need to be relocated.

An existing 150mm watermain is located on the west side of the street, in the roadway portion, and does not create any conflict between proposed street trees. Once the intersection at Balliol Street is narrowed, part of the roadway on the west side will offer an improved pedestrian realm for a future plaza park. Proposed trees may come in conflict with the watermain below and will need to be located carefully.

A few mature existing trees are located in close proximity to the existing sidewalks, limiting sidewalk widening. In those instances, healthy trees should be protected with minimal encroachments into the Tree Protection Zones.

On the west side of the street at Balliol Street, existing boulevard parking and the lack of a sidewalk on Pailton Crescent create an unsafe pedestrian condition and limit public realm use.

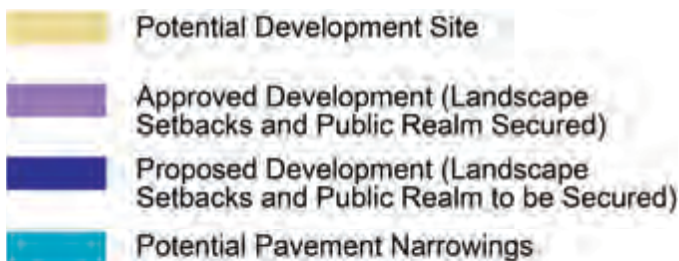


Figure 149 Diagram of opportunities and constraints at Pailton Crescent



Figure 150 Existing mature trees along Pailton Crescent



Figure 151 Existing conflicts with hydro poles along Pailton Crescent



Figure 152 Missing sidewalk along Pailton Crescent south of Balliol Street



Figure 153 Missing sidewalk and boulevard parking along Pailton Crescent near the intersection with Merton Street

Final Condition Recommendations:

- Maintain the 8.6m vehicular right-of-way
- Narrow the vehicular portion of the ROW to 8.6m at the Merton and Balliol Street intersections
- Provide a park/ plaza at the southwest corner of Balliol Street and Pailton Crescent
- Relocate boulevard parking between Balliol Street and Merton Street from the west side of the ROW onto the street to allow for sidewalk extension and a gathering place at the corner of Merton Street and Pailton Crescent
- Provide a minimum 3.0m setback from the property line to the building face on the west side
- Provide a minimum 1.5m of planting soil on top of any underground structures in the setback to support new, healthy tree growth
- Provide sidewalks above standard width, on both sides of the street
- Provide a generous, above standard width green boulevard to accommodate rain gardens and street trees on both sides of the street. Incorporate unit paver buffer and stepping stones to allow for easy on-street parking use
- Relocate existing light standards/ hydro poles outside of the new pedestrian clearway (or, if possible, relocate hydro underground)

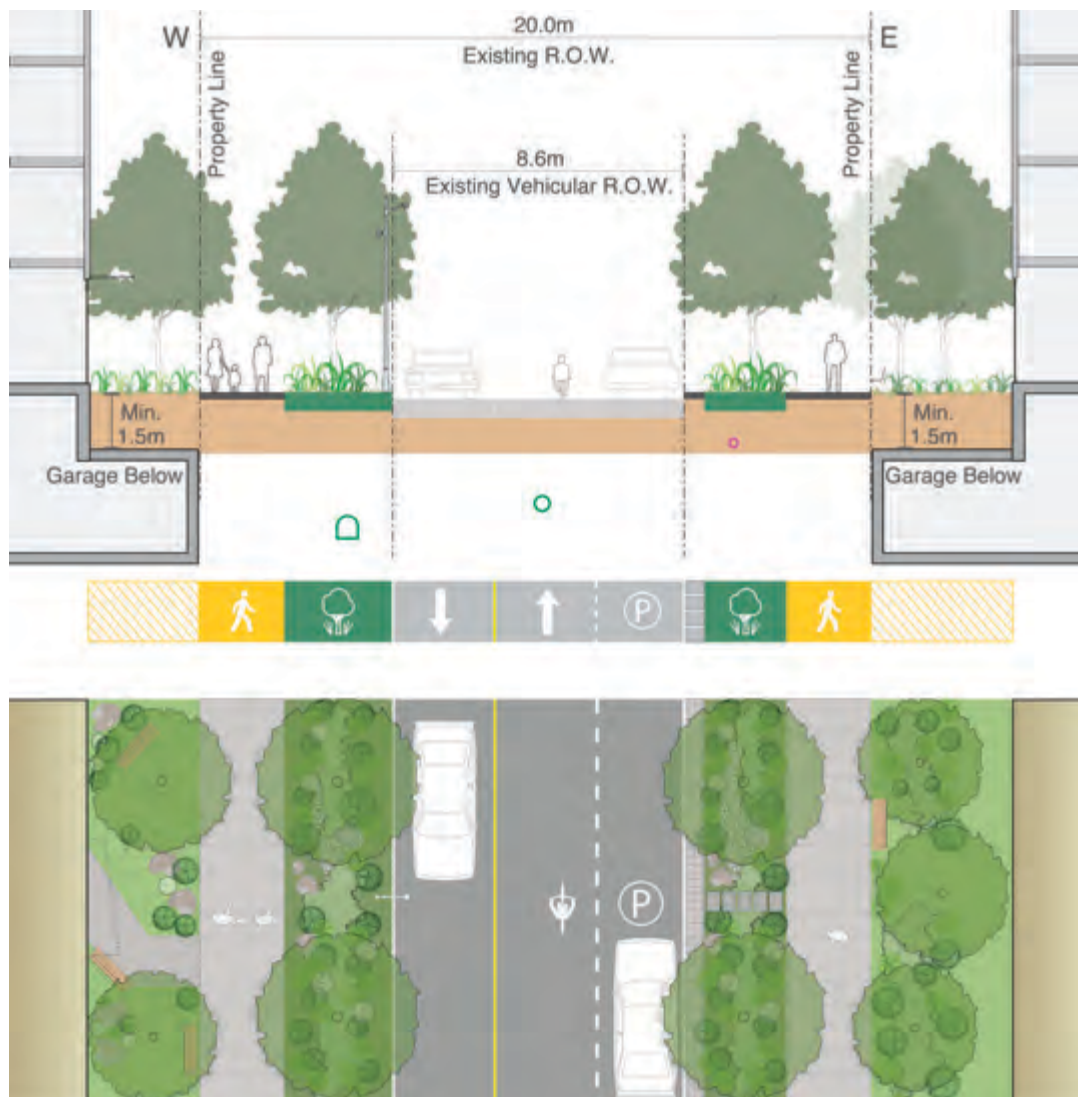


Figure 154 Section and Plan of Pailton Crescent Connector - Recommended Final Condition

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

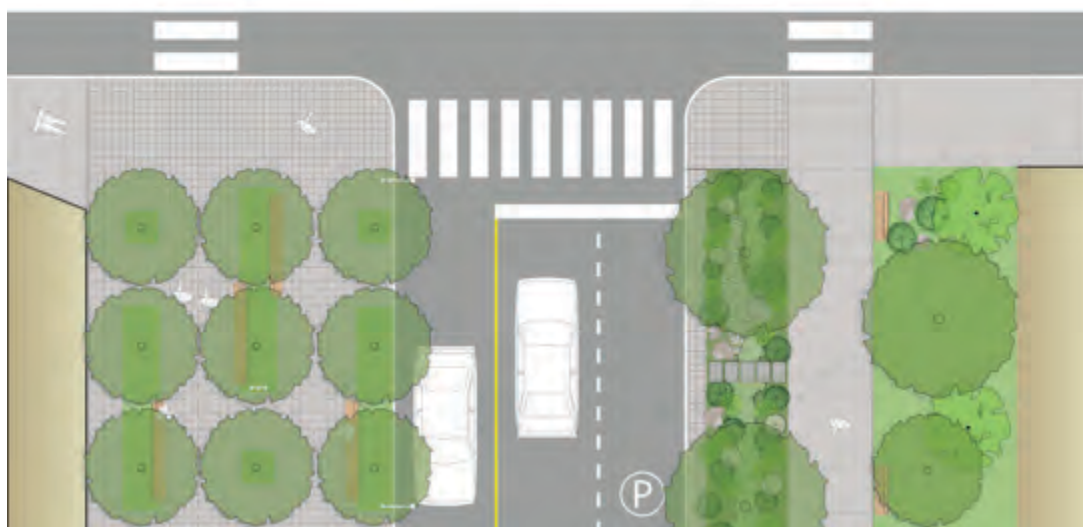
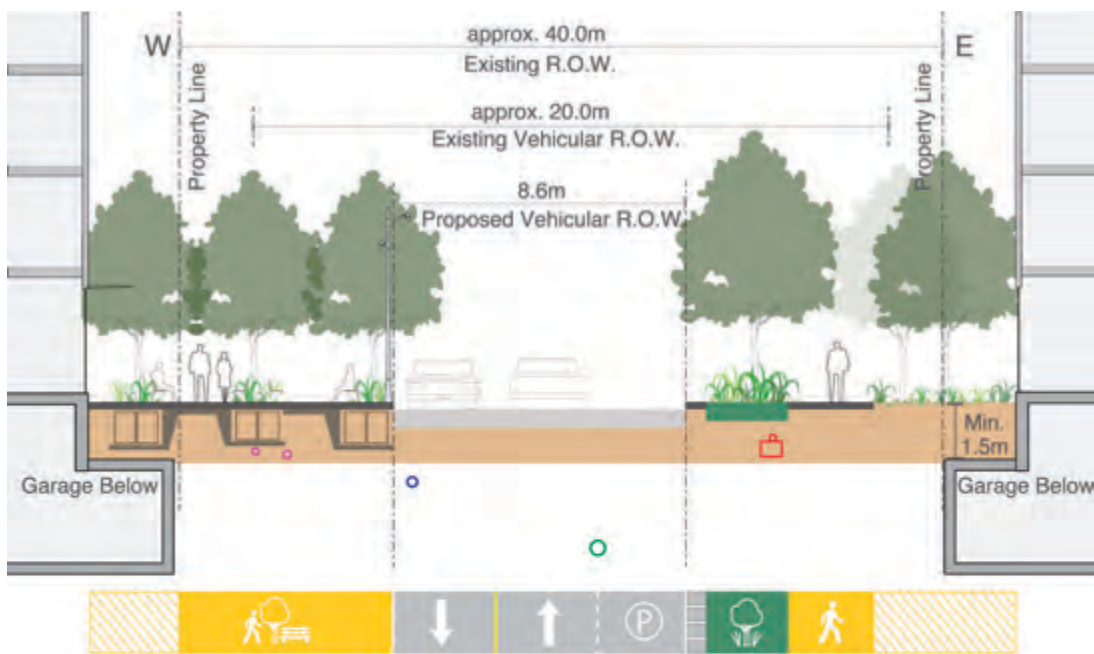


Figure 155 Section and Plan of Pailton Crescent Connector - Recommended Final Condition of public square at intersection with Balliol Street

SECTION 4.5

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.



Figure 156 Section and Plan of Pailton Crescent Connector - Recommended Final Condition of public square at intersection with Merton Street

Implementation

Development-related Improvements

A significant portion of the public realm will be improved through the development review process. In particular, these improvements will be implemented through current applications under review: 141 Davisville Avenue, which spans the entire block from Davisville Avenue to Balliol Street on the west side; and 185 Balliol Street, located at the southwest corner of the intersection.

Capital Works and Coordination

Opportunities for public realm improvements may arise through upcoming capital works projects led by Toronto Water and Transportation Services. Taking advantage of other planned works represents an opportunity to coordinate and implement public realm improvements in the near-term and avoids disruption from additional construction work in the future.

Note: The implementation timeline represents information available from divisional partners and data from T.O. Inview as of December 31st, 2021 and is subject to change.

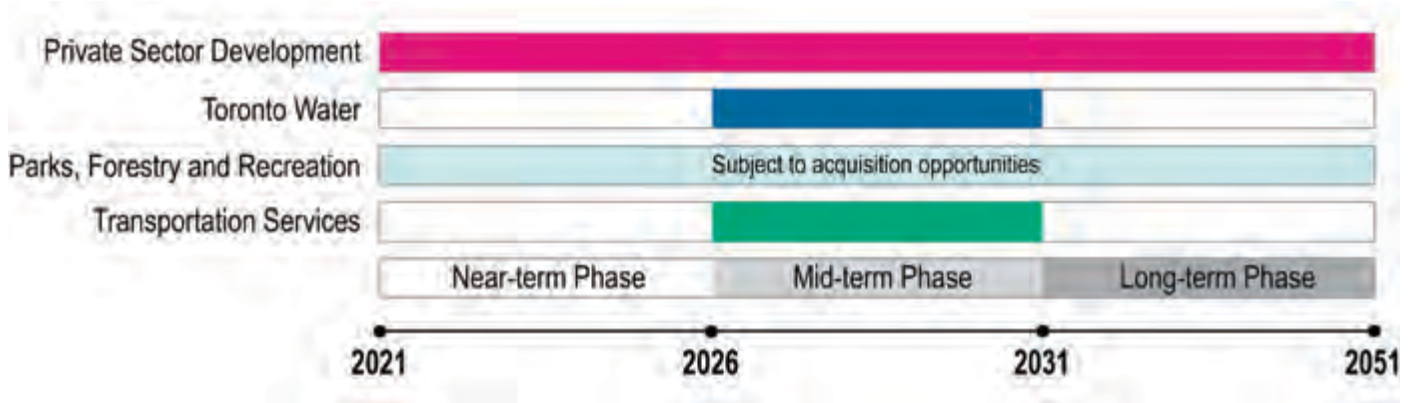


Figure 157 Projected Implementation Timeline for the Pailton Crescent Connector

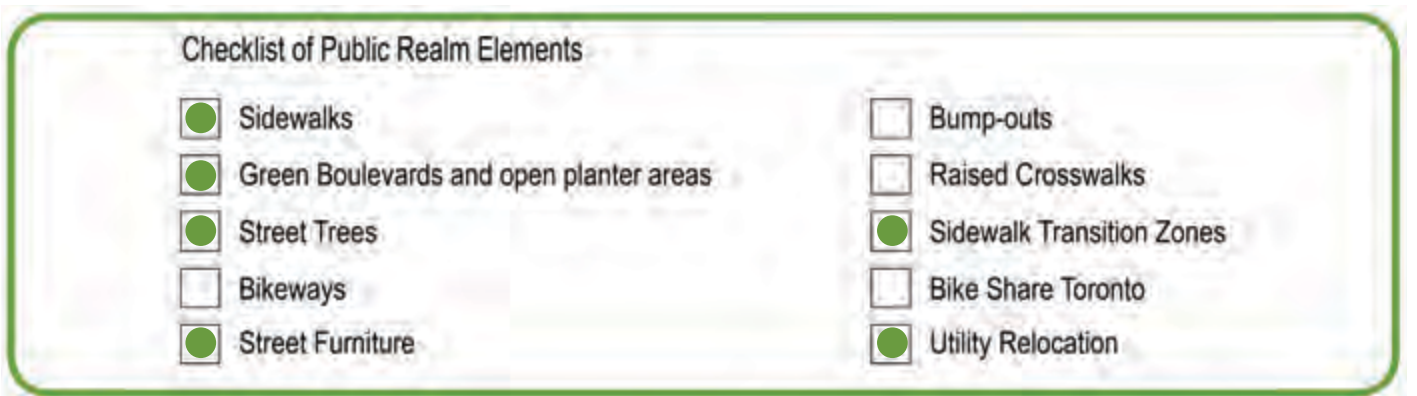


Figure 158 Checklist of Public Realm Elements to be implemented for the Pailton Crescent Connector

4.6

Balliol Green Street

Overview

Background Analysis

Final Condition Recommendations

Implementation

Overview

Balliol Street is a local street in the Davisville neighbourhood that runs between Yonge Street and Bayview Avenue. Existing open spaces associated with "tower-in-the-park" style high-rise apartment buildings will continue to emphasize the unique character of the street.

Existing and future landscape setbacks will be enhanced and expanded, creating a prominent green, open character along the street. Additional trees and understory plantings will create a comfortable pedestrian experience, while tying together new patios, residential forecourts and publicly-accessible open spaces that support the community's street life.

Policy Direction

The Yonge-Eglinton Secondary Plan includes the following policy related to the Balliol Green Street Public Realm Move:

3.2.11 Balliol Green Street

- Balliol Street is characterized by tower in the park open spaces and will continue to be a pleasant and tranquil connection for pedestrians through the Yonge-Davisville area and a landscaped setting for apartment buildings.
- Development on both sides of the street will provide a reasonable setback at grade and above grade to support landscaping, including tree and understory plantings, forecourts and POPS.

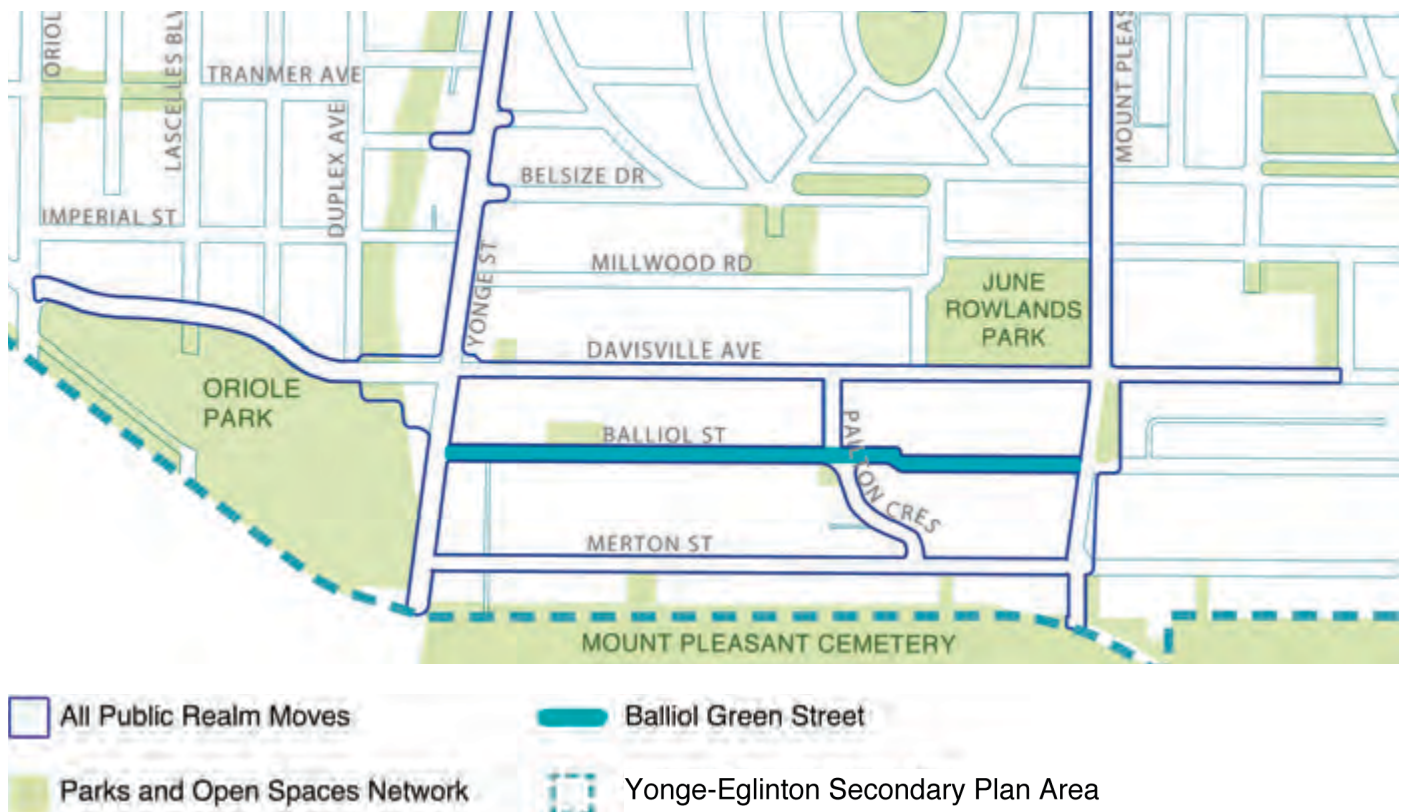


Figure 159 Map showing the location of the Balliol Green Street Public Realm Move

Background Analysis

Existing Street Cross-Sections

- 20.1m right-of-way (ROW)
- Two-way street with parking on the north side of the street
- Sidewalk widths range from 2.6m-3.7m
- Building setbacks range from 2.0m-14.8m
- Bike Share Toronto station on the north side of the street in front of Balliol Parkette
- Multiple boulevard parking spaces in front of existing grocery store



Figure 160 Existing Bike Share Toronto station in front of Balliol Parkette



Figure 161 Painted bump-out at intersection with Yonge Street



Figure 162 Existing mid-block connection

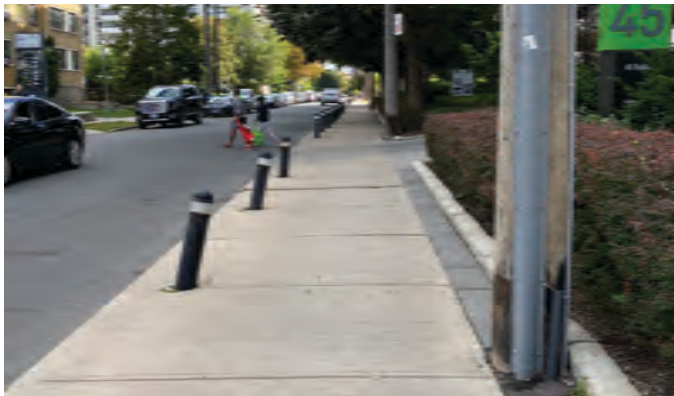
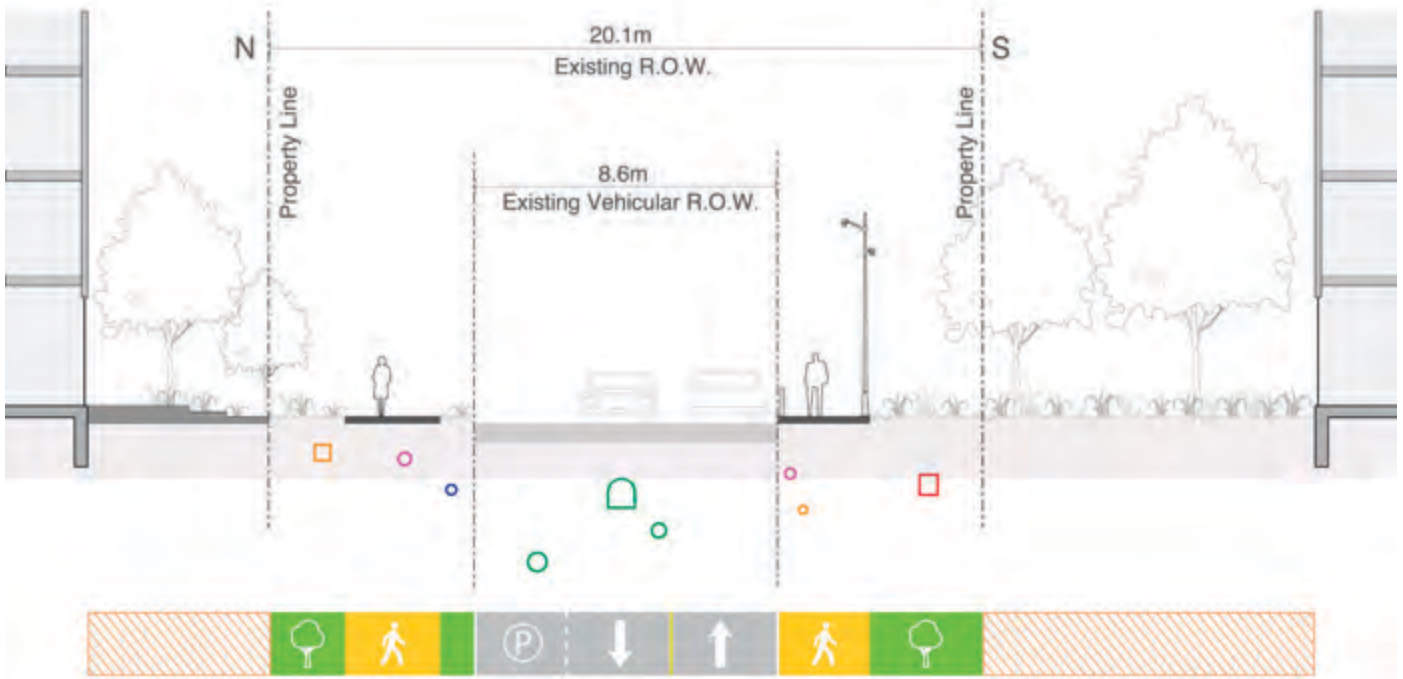


Figure 163 Existing sidewalk condition on the south side



SECTION 4.6

Figure 164 Section and Plan of the Existing Conditions along Balliol Street

Secured Section 37 Contributions

There are currently no development applications along Balliol Street with secured Section 37 contributions for the Balliol Green Street. However, development applications will continue to deliver streetscape improvements through the development review process.

Summary of Opportunities and Constraints

Sites with Redevelopment Potential

With numerous approved and under review development applications, public realm improvements will start to occur incrementally. Many sites fronting Balliol Street may still see redevelopment and infill. As these sites come forward for development, the public realm improvements will be provided by the developer, where possible. Additionally, public realm

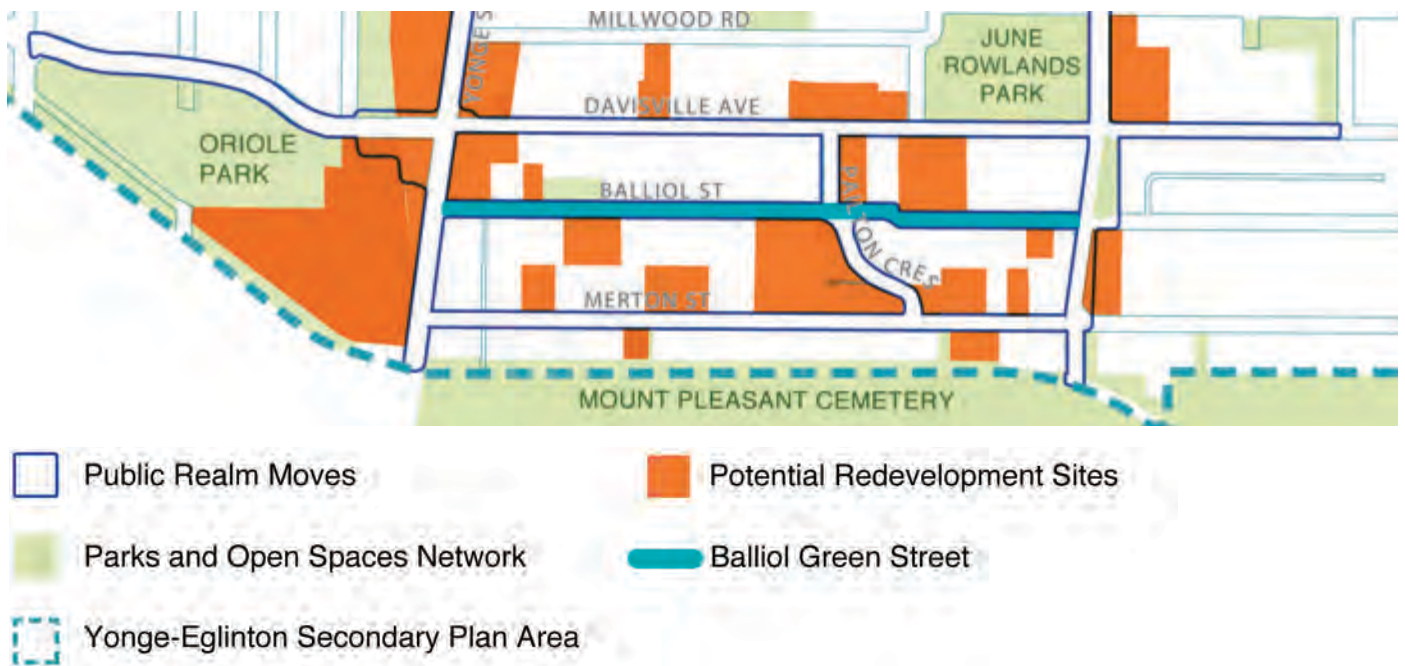


Figure 165 Map of potential redevelopment sites along the Balliol Green Street



Figure 166 Diagram of opportunities and constraints at Balliol Street

upgrades can be expanded beyond the property line to provide enhancements in front of properties that have been recently constructed and will not be redeveloped in the near future.

There are eight active development applications along both sides of Balliol Street. This includes two approved development applications, which have secured landscape setbacks of at least 6.0m. There are also six proposed applications where staff are working to secure landscape setbacks through the development review process.

There are also some lots throughout the area and clustered on the north side of Balliol Street which may experience future development activity. Public realm improvements would be implemented on these sites through the development review process.

With only a few current development applications under review, it will be challenging to implement a complete public realm upgrade through development only in the near term. As public realm gets reconstructed through development, sidewalk transition will be required as an interim condition.

Utility Locations and Key Issues

Similar to the rest of the Midtown Public Realm Moves, above-ground facilities such as hydro poles, fire hydrants and utility vaults are located immediately adjacent to the sidewalks and hinder sidewalk widening.

Where planted bump-outs are recommended, existing catch basins may be in conflict and will need to be relocated or incorporated into the bump-out design.

Both sides of the street have grading challenges including existing low retaining walls, fences, stairs and railings that encroach into the public realm.

Existing boulevard parking with numerous spaces in front of the existing grocery store creates an unsafe pedestrian condition with multiple cars maneuvering in and out across the sidewalk.

Additionally, in some areas of the street, mature existing trees with exposed roots are growing immediately adjacent to the existing narrow sidewalk, limiting sidewalk widening.

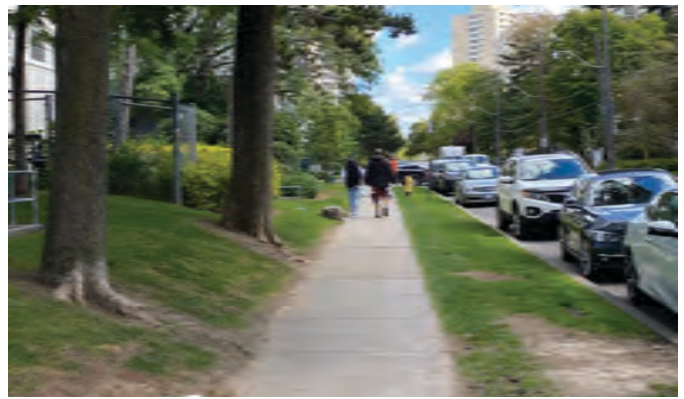


Figure 167 Existing mature trees along Balliol Street



Figure 168 Existing grading challenges along Balliol Street



Figure 169 Existing utility conflicts along Balliol Street



Figure 170 Existing utility conflicts along Balliol Street

Final Condition Recommendations:

- Maintain 8.6m vehicular ROW
- Provide rain gardens on both sides of the street
- Introduce pedestrian light standards on the north side of the street
- Provide a 6.0m building setback from the property line to the building face on both sides of the street
- Provide a minimum 1.5m depth of planting soil on top of any underground structures in the setback to support new, healthy tree growth
- Provide sidewalks above standard width on both sides of the street
- Provide planted bump-outs and raised pedestrian crossings at mid-block connections
- Provide a bump-out in front of Balliol Parkette
- Remove all existing boulevard parking to upgrade, green and expand the public realm
- Relocate on-street parking impacted by the size and locations of the future bump-outs
- Relocate existing light standards/ hydro poles outside of the new pedestrian clearway (or, if possible, relocate hydro underground)

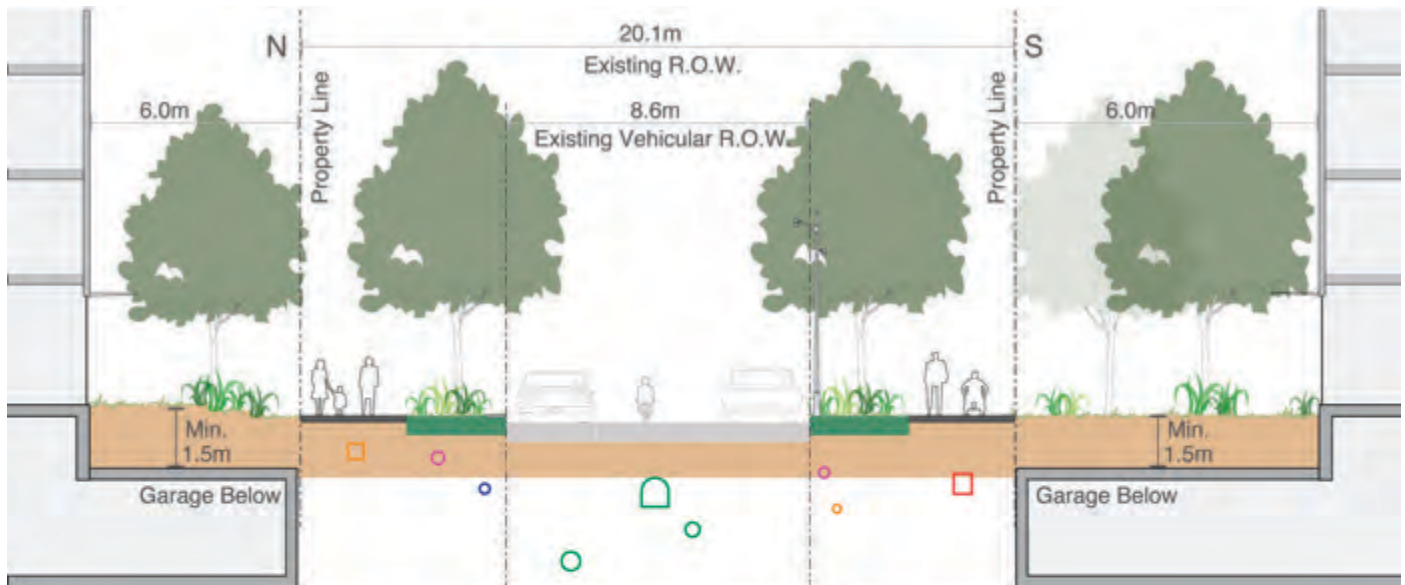


Figure 171 Section and Plan of Balliol Green Street - Recommended Final Condition



Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

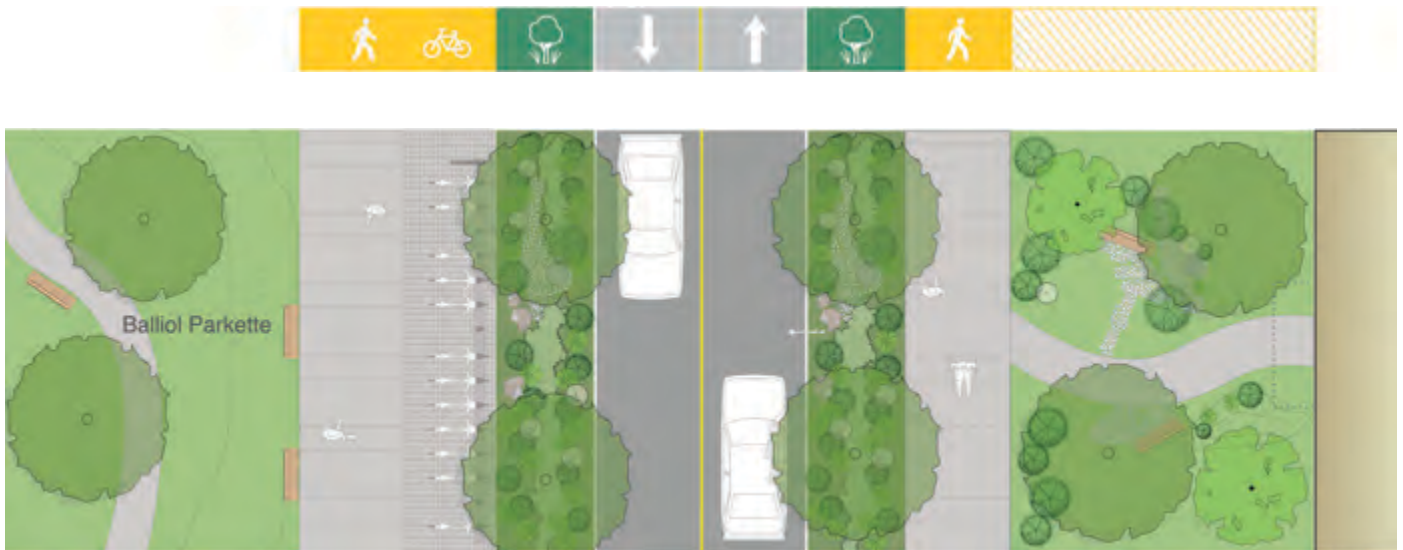


Figure 172 Plan of Balliol Green Street - Recommended Final Condition of bump-out of Bike Share station in front of Balliol Parkette

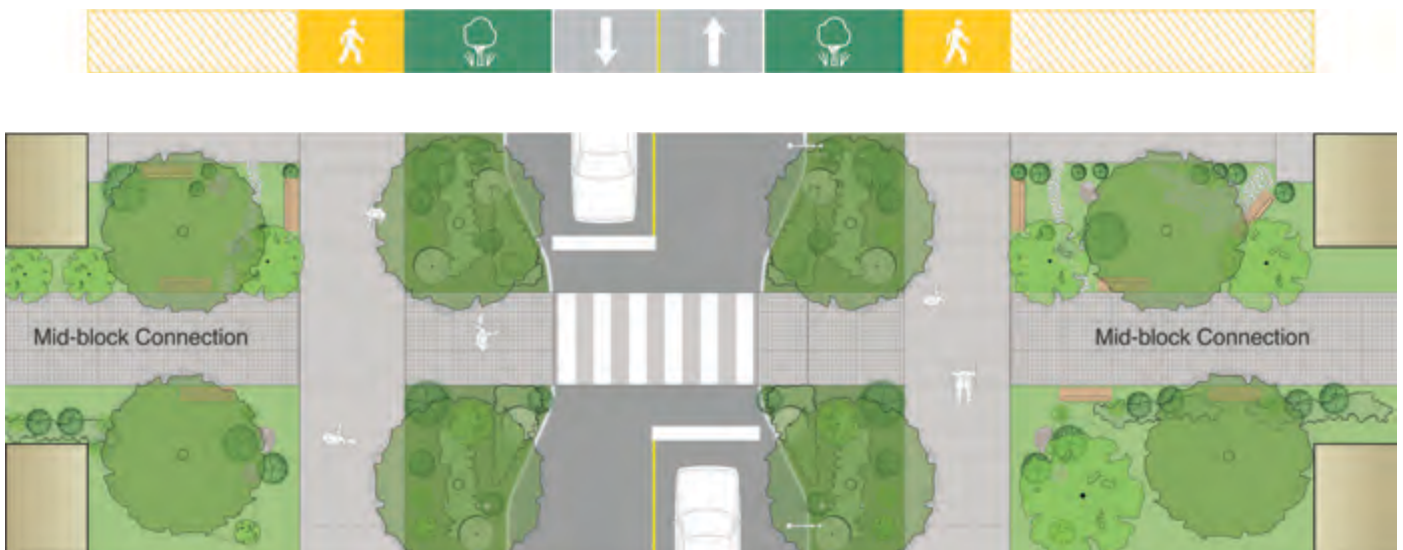


Figure 173 Plan of Balliol Green Street - Recommended Final Condition of bump-out and raised pedestrian crossing at mid-block connection

Other Improvements

Tree Protection

Healthy mature trees immediately adjacent to the existing sidewalk will be protected. Tree Protection Zones will be implemented, while providing continuous sidewalks. Paths of travel must be as linear as possible, deflections should not exceed 20 degrees.

Sidewalk Transitions

In instances where existing and proposed sidewalks do not align, a sidewalk transition zone should be included to create a continuous pedestrian experience.

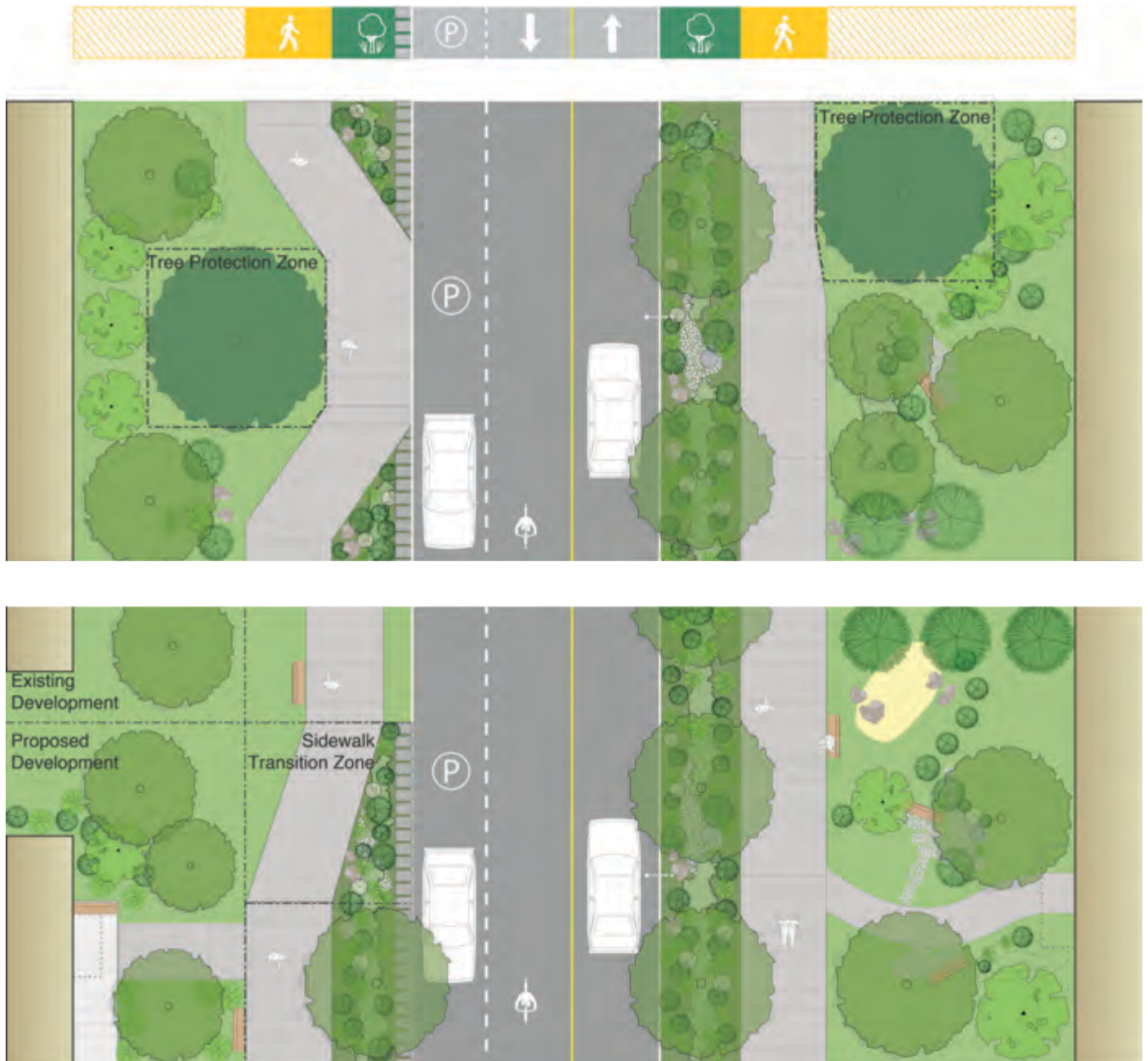


Figure 174 Tree Protection Zones (above) and Sidewalk Transition Zones (below) along Balliol Green Street

Implementation

Development-related Improvements

With numerous applications along Balliol Street, a good portion of the public realm will be improved through the development review process.

The developer will be responsible for providing a minimum 6.0m building setback on both sides of the street or maintain existing landscape setbacks with any new redevelopment if they exceed 6.0m.

Capital Works and Coordination

Opportunities for public realm improvements may arise through upcoming capital works projects led by Toronto Water and Transportation Services. Taking advantage of other planned works represents an opportunity to coordinate and implement public realm improvements in the near-term and avoids disruption from additional construction work in the future.

Note: The implementation timeline represents information available from divisional partners and data from T.O. Inview as of December 31st, 2021 and is subject to change.

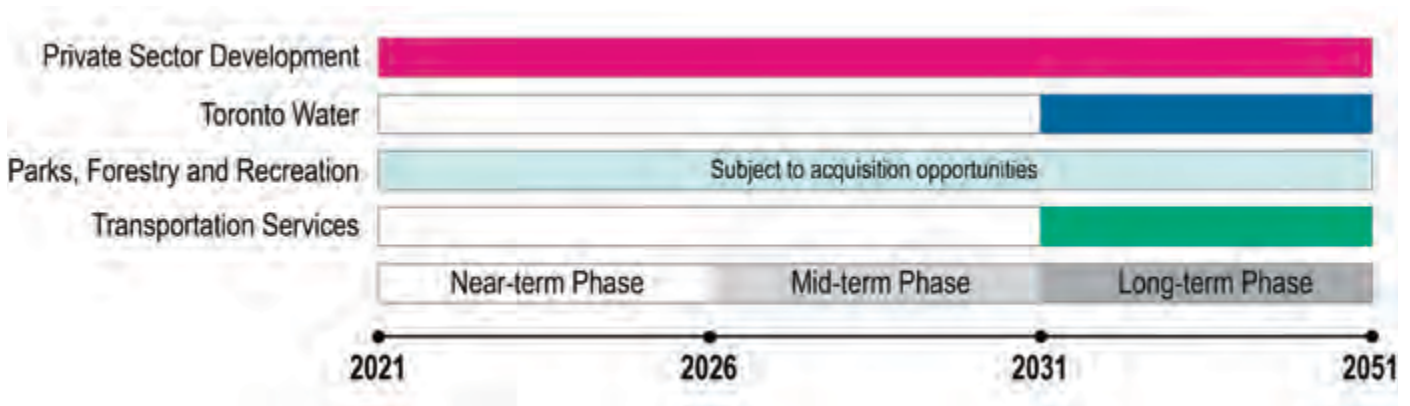


Figure 175 Projected Implementation Timeline for the Balliol Green Street



Figure 176 Checklist of Public Realm Elements to be implemented for the Balliol Green Street

4.7

The Midtown Greenways

Overview

Keewatin Greenway

Background Analysis

Final Condition Recommendations

Implementation

Erskine Greenway

Background Analysis

Final Condition Recommendations

Implementation

The Greenways

Background Analysis

Final Condition Recommendations

Implementation

Overview

The Midtown Greenways are a network of residential streets that provide pedestrian and active transportation connections through Yonge-Eglinton Centre. Keewatin Avenue, Erskine Avenue, Holly Street, Dunfield Avenue, Lillian Street, Brownlow Avenue and Soudan Avenue are a part of the Midtown Greenways network.

The Greenways are envisioned to have generous landscape setbacks to allow for publicly accessible open spaces, such as gardens and play spaces.

Additionally, the greenways will have widened sidewalks framed by a high-branching tree canopy to establish the Greenways as comfortable, attractive and active places. New street elements will include planted bulb-outs at park frontages, intersections and mid-block walkways. Stormwater features such as rain gardens will extend the landscape to the street edge, shorten pedestrian crossing distances and contribute to traffic calming. To improve pedestrian safety and experience, building access and driveway entrances will be consolidated to minimize disruptions in the sidewalk.

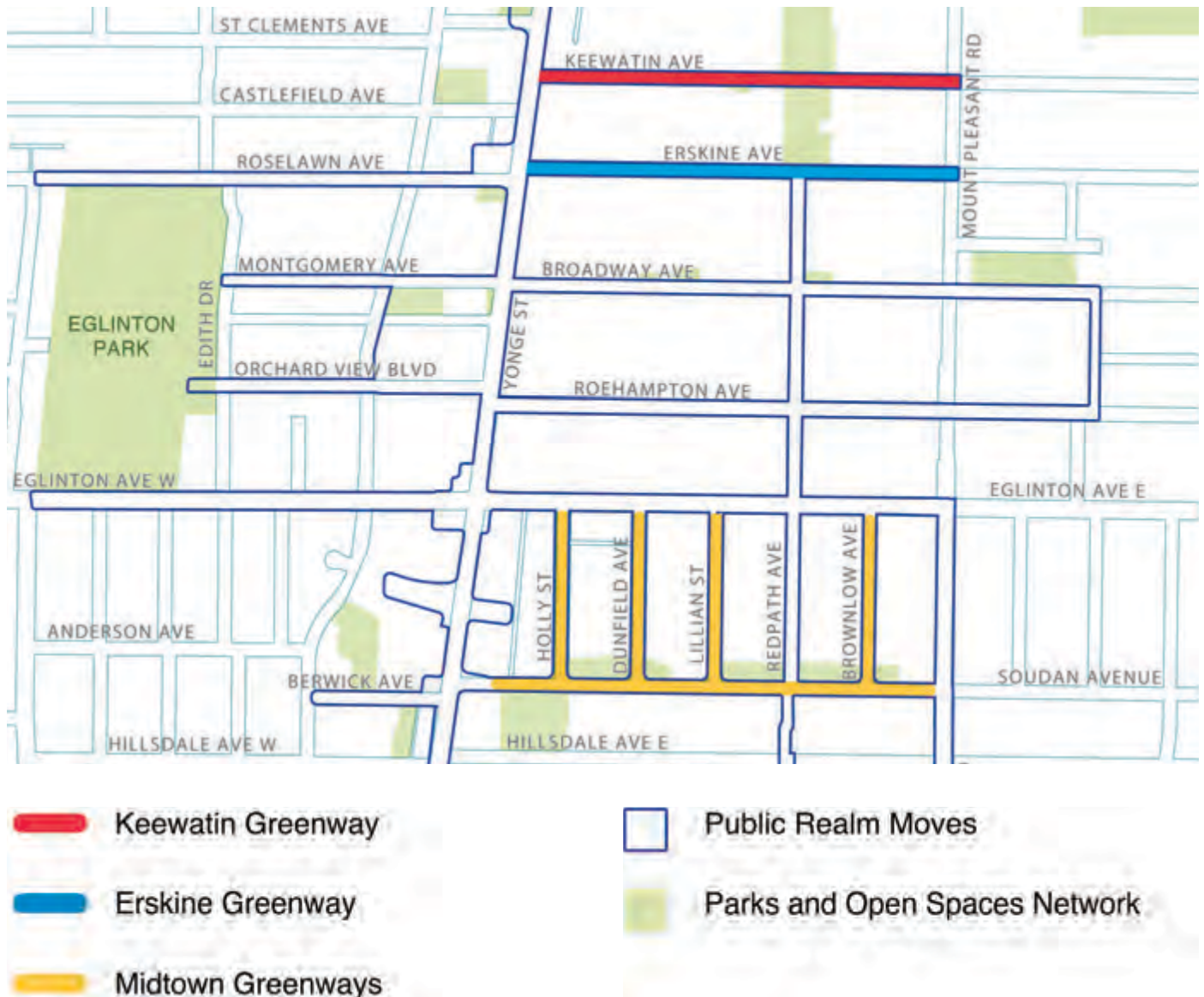


Figure 177 Map showing the location of the Midtown Greenways Public Realm Moves

Policy Direction

The Yonge-Eglinton Secondary Plan includes the following policies related to the Midtown Greenways Public Realm Move:

3.2.6 Midtown Greenways

- a. The Midtown Greenway streets will provide improved pedestrian amenity and support the continuation and expansion of the landscaped setbacks characteristic of the Secondary Plan area.
- b. A reasonable setback at grade and above established grade will be provided on all Greenway street frontages in Mixed Use Areas and Apartment Neighbourhoods.
- c. A reasonable setback at grade and above established grade from the property line will be provided on both sides of Erskine Avenue between Yonge Street and Mount Pleasant Road.
- d. The landscaped open space setbacks will provide a range of publicly accessible open spaces such as entry forecourts, gardens, and other forms of landscaped open space. The landscaped setbacks will be planted with high-branching deciduous trees and understory plantings.
- e. The location and design of underground facilities, such as parking, on properties abutting the Midtown Greenways will provide sufficient space and soil depth to maintain a permanent, high-branching tree canopy.



Figure 178 Existing condition along Keewatin Avenue



Figure 179 Existing condition along Erskine Avenue



Figure 180 Existing condition along Soudan Avenue



Figure 181 Existing condition along Dunfield Avenue

Keewatin Greenway

Keewatin Avenue is the most northern east-west Greenway street, consisting of unique built form character with Tower in the Park mid- and high-rise apartment buildings across the street from low-rise residential houses.



Figure 182 Existing sidewalk transition on the south side of Keewatin Avenue



Figure 183 Existing condition on the south side of Keewatin Avenue



Figure 184 Existing condition on the south side of Keewatin Avenue at sports field



Figure 185 Existing intersection at Yonge Street and Keewatin Avenue

Background Analysis

Existing Street Conditions

- 20.0m right-of-way (ROW)
- 8.6m vehicular ROW with two travel lanes and a parking lane
- Two-way street with parking on the north side of the street
- Substandard pedestrian clearway on narrow sidewalks on both sides of the street
- Multiple driveways and front yards on both sides of the street
- Various building setbacks
- Boulevard parking at Yonge Street intersection
- Pedestrian north-south crosswalks at the intersections with Yonge Street and Mount Pleasant Road

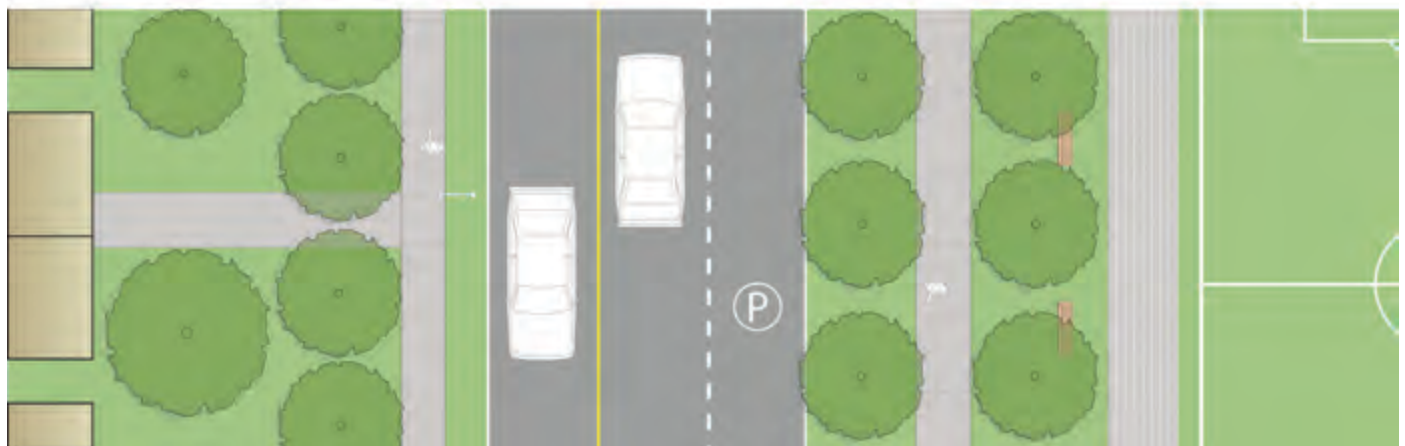
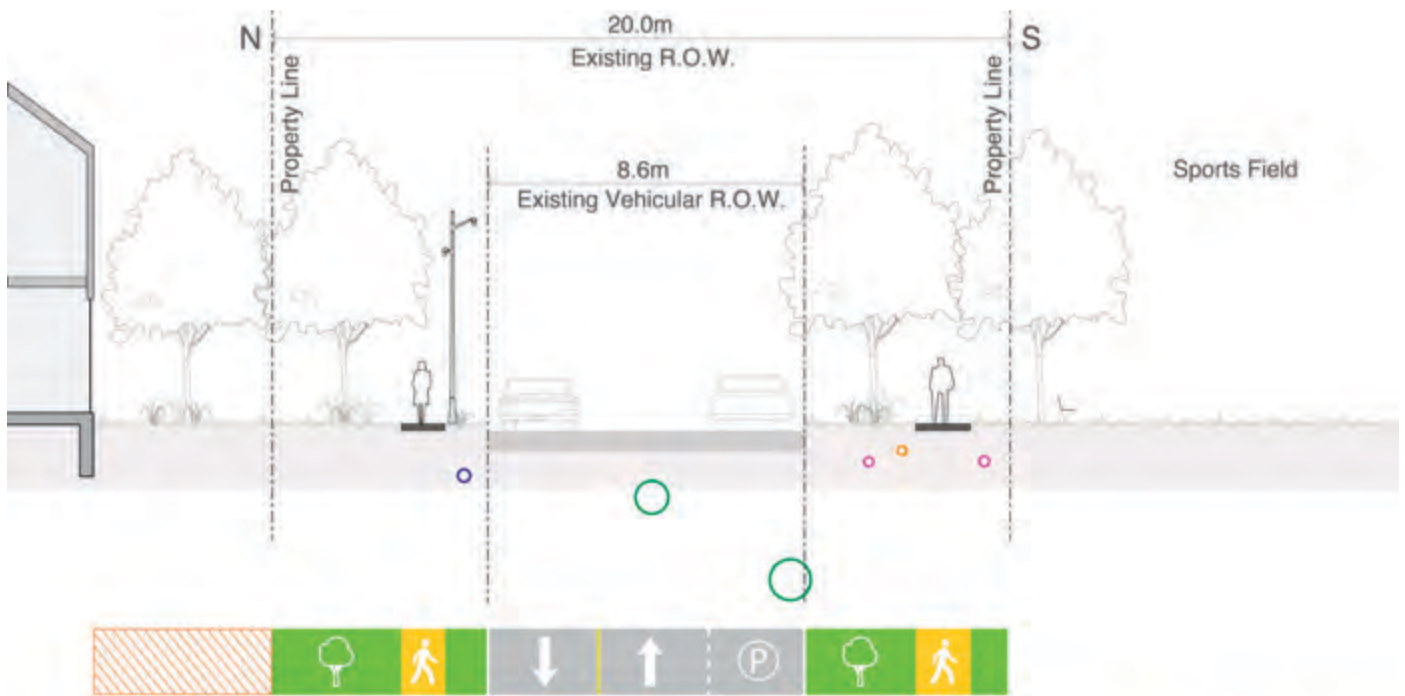
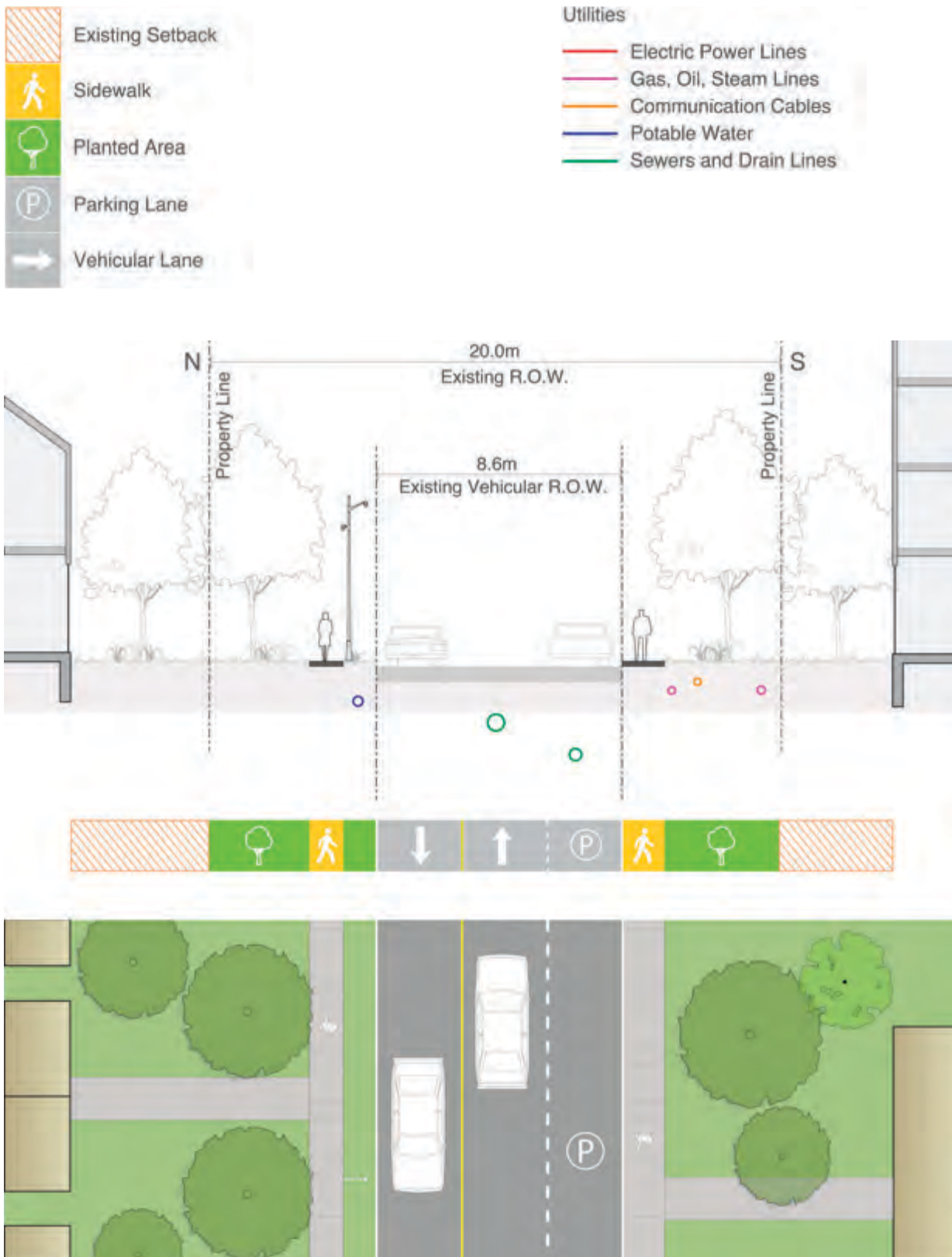


Figure 186 Section and Plan of the Existing Conditions of Keewatin Avenue at sports field



SECTION 4.7

Figure 187 Section and Plan of the Existing Conditions of Keewatin Avenue

Secured Section 37 Contributions

Recent development in the area has provided community benefits through Section 37 contributions to the City. There are two development applications recently completed and built in Ward 15, comprising two sites along 18-30 Erskine Avenue and 73-79 Keewatin Avenue. With the approval of these applications, the City secured \$1,149,512, adjusted for interest to \$1,192,488 as of December 31st, 2021, towards streetscape and public realm improvements on Keewatin Avenue and in the Yonge-Eglinton Apartment Neighbourhoods.

Summary of Opportunities and Constraints

Sites with Redevelopment Potential

With a few available soft sites on the south side of the street, public realm improvements will start to occur incrementally. As these sites come forward for development, the public realm improvements will be provided by the developer, where possible. Additionally, public realm upgrades can be expanded beyond the developer's property line and be implemented by developers in front of other properties that will not be redeveloped in the near future.

With no current development applications under review, it will be challenging to implement a complete public realm upgrade through development only, in the near-term. All lots on the north side of Keewatin Avenue are constrained as they are designated Neighbourhoods in the Official Plan and do not permit large-scale intensification, or are designated as a heritage property. The lots on the south side of Keewatin Avenue is a mix of large sites that are either constrained or may have potential infill development. These constrained sites are limited due to being designated a heritage property, having recent redevelopment or infill development activity, or being located on the site of the future of Redpath Parkette Expansion. For these reasons, constrained sites have low development potential to provide landscape setbacks that are at least 5.0m on the south side of Keewatin Avenue.

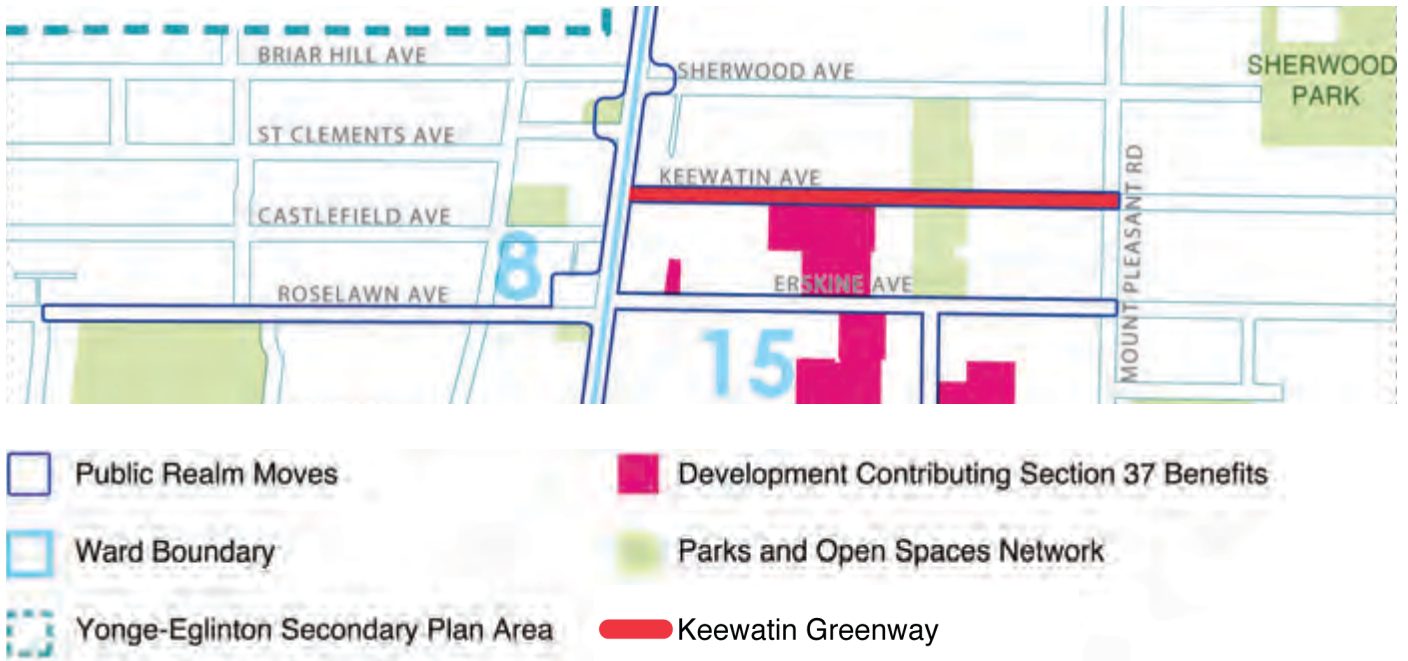


Figure 188 Map of the Section 37 secured contribution locations along the Keewatin Greenway

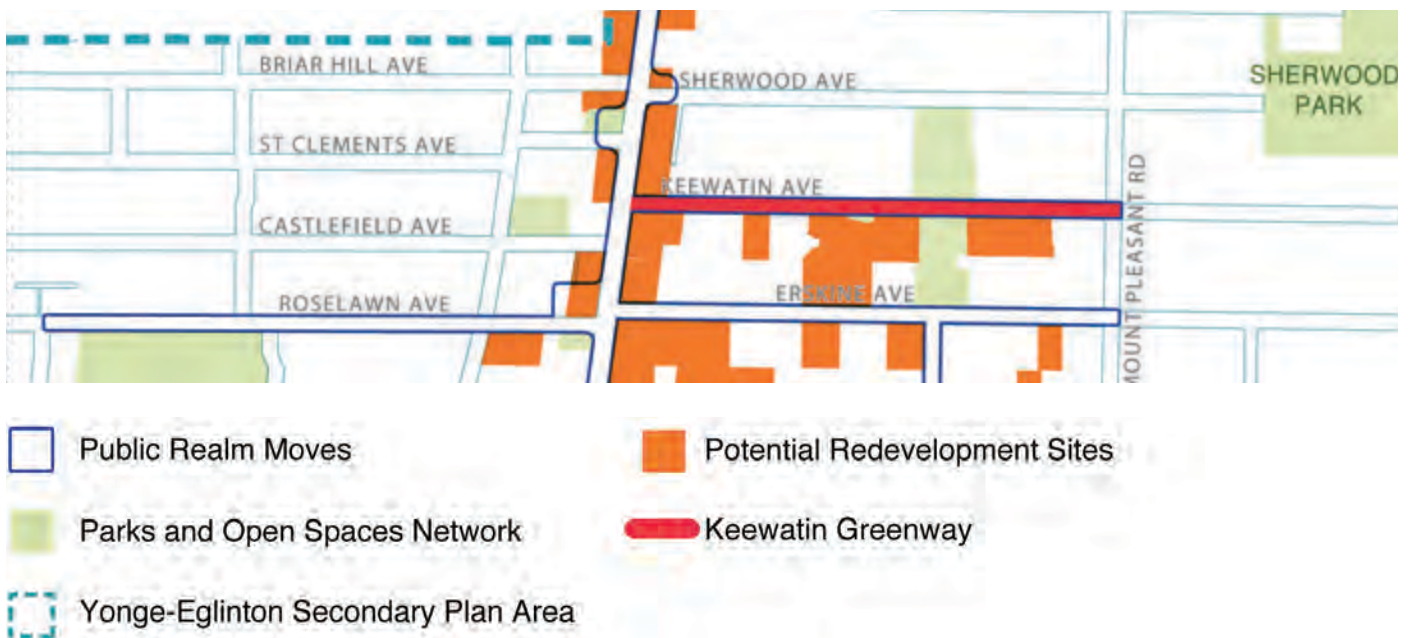


Figure 189 Map of potential redevelopment sites along the Keewatin Greenway

Utility Locations and Key Issues

Key utility issues are very similar to the rest of the Midtown Public Realm Moves. Above-ground facilities such as hydro poles, fire hydrants and various utility infrastructure are located immediately adjacent to the sidewalks and hinder sidewalk widening and general public realm improvements.

Where planted bump-outs are proposed, existing catch basins may be in conflict and will need to be relocated or incorporated into the bump-out design.

Numerous access driveways, private retaining walls, railings and steps provide grading challenges to create a continuous cohesive streetscape on the north side of the street. Existing boulevard parking areas create an unsafe pedestrian condition and limit public realm experience.

Additionally, in some areas of the street, mature existing trees with exposed roots are growing immediately adjacent to the existing narrow sidewalk. In those instances, healthy trees should be protected with minimal encroachments into the Tree Protection Zones.

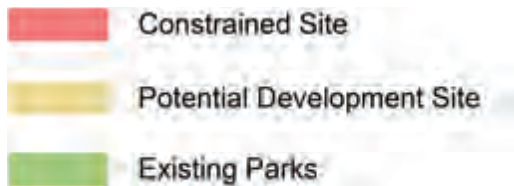


Figure 190 Diagram of opportunities and constraints at Keewatin Avenue



Figure 191 Existing fencing along Keewatin Avenue



Figure 194 Existing garden walls along north side of Keewatin Avenue



Figure 192 Existing grading challenges along Keewatin Avenue



Figure 195 Existing utility conflicts along Keewatin Avenue



Figure 193 Existing boulevard parking on the north side of Keewatin Avenue

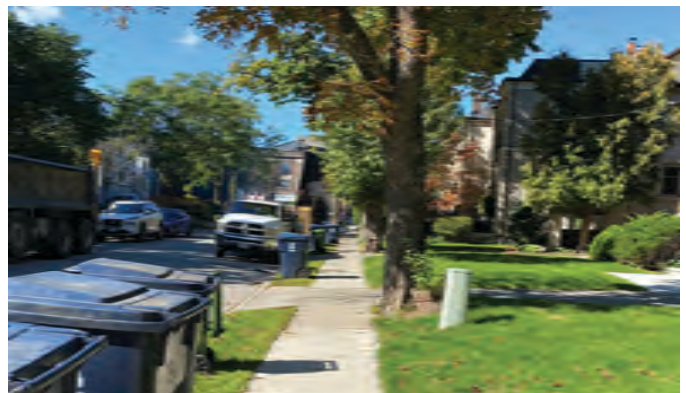


Figure 196 Existing mature trees along Keewatin Avenue

Final Condition Recommendations:

- Maintain 8.6m vehicular ROW
- Provide planted bump-outs at Yonge Street and Mount Pleasant Road intersections
- Provide planted bump-out at future parks fronting the street
- Maintain existing setback on the north side and provide a 5.0m building setback from the property line to the building face on the south side of the street
- Provide a minimum 1.5m depth of planting soil on top of any underground structures in the setback to support new, healthy tree growth
- Provide above standard width sidewalk on both sides of the street
- Convert and expand existing sod strip on the north side of the street into rain garden
- Provide generous rain gardens on the south side of the street, next to the setback to maximize publicly accessible open space
- Provide sidewalk transition zones to protect existing mature trees
- Provide planted bump-outs at mid-block pedestrian crossing associated with mid-block connections
- Remove all existing boulevard parking to upgrade, green and expand the public realm
- Relocate on-street parking impacted by the size and locations of the future bump-outs
- Relocate existing light standards/ hydro poles outside of the new pedestrian clearway (or, if possible, relocate hydro underground)



Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

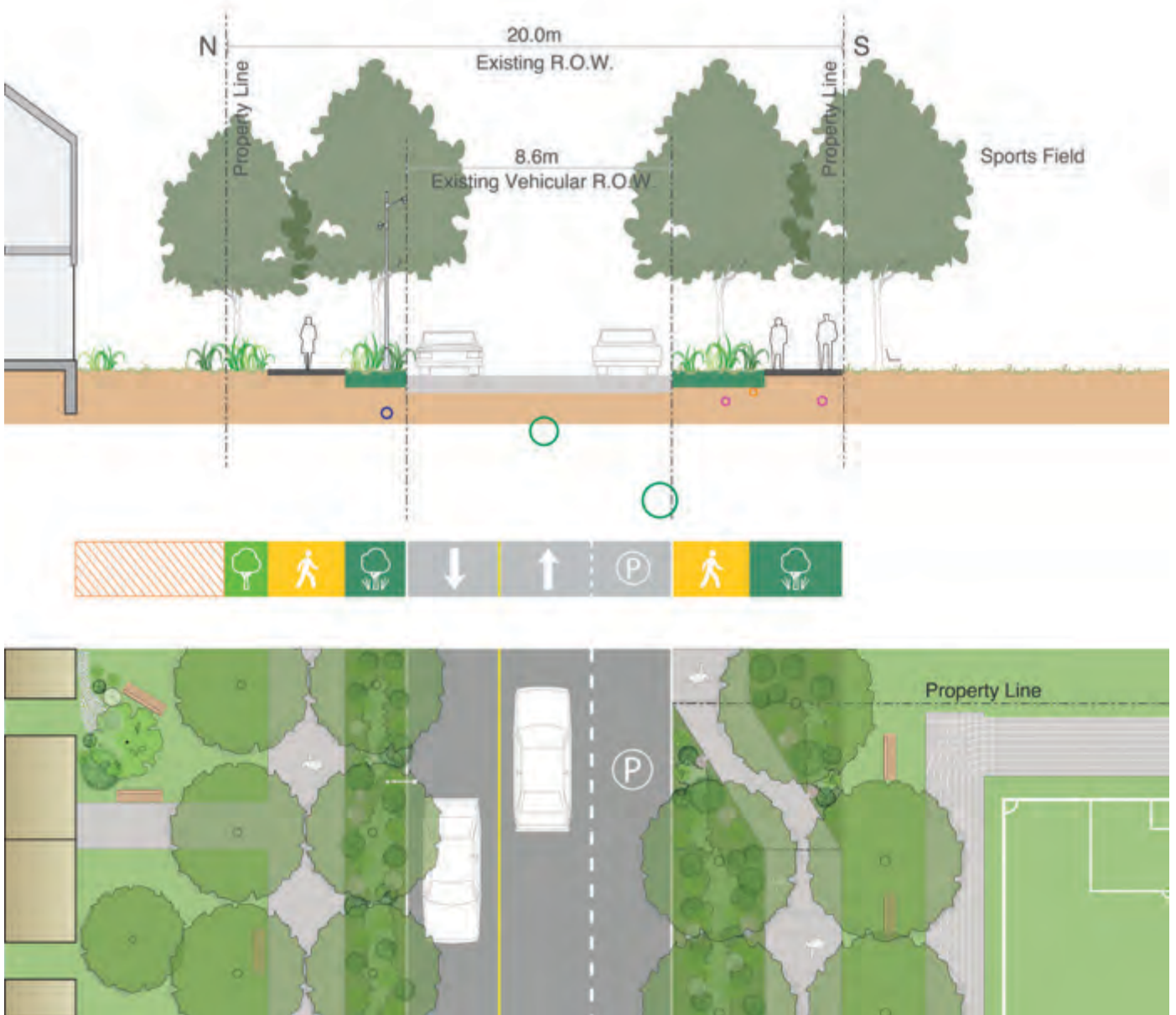


Figure 197 Section and Plan of Keewatin Greenway - Recommended Final Condition at sports field

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

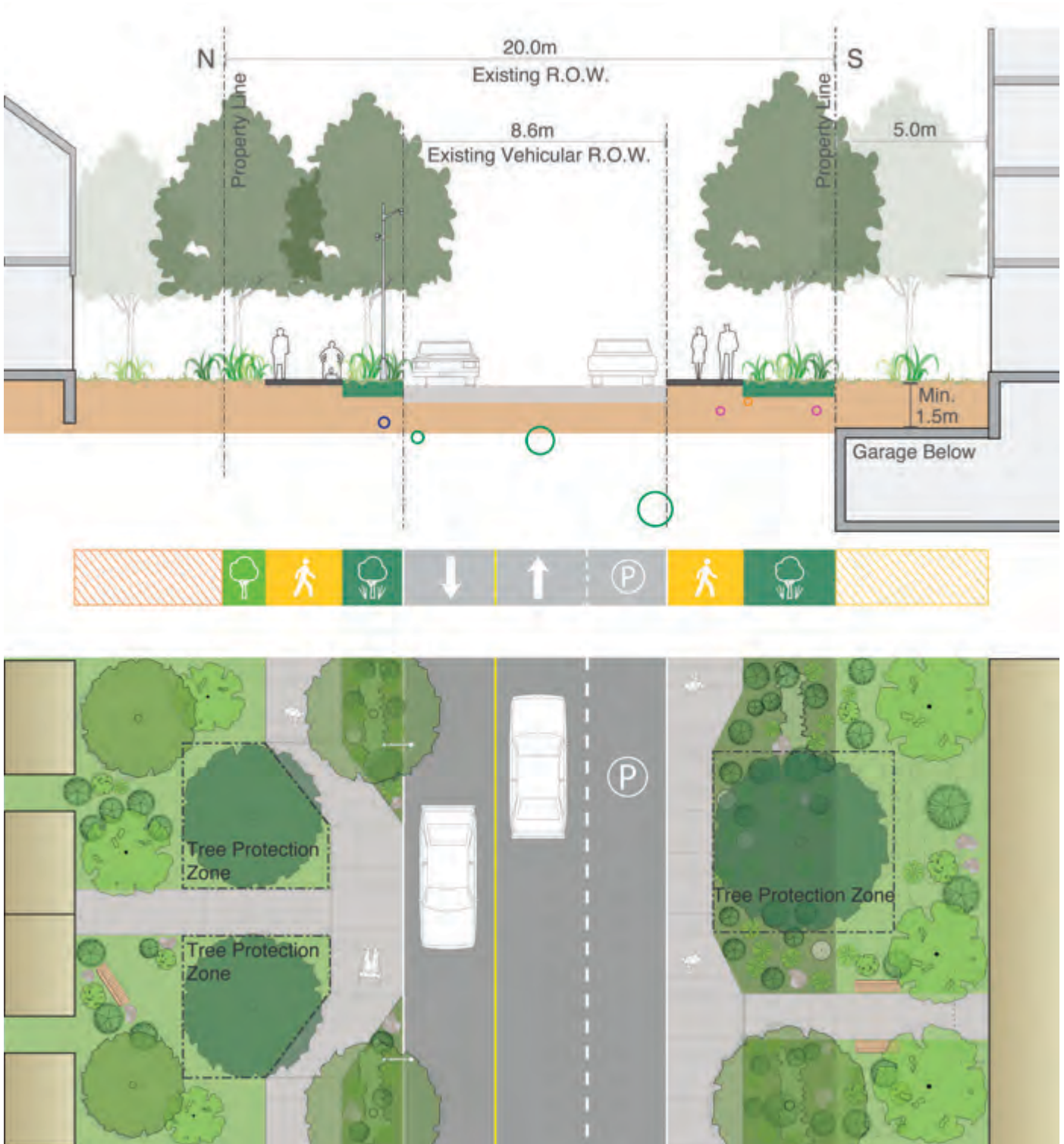
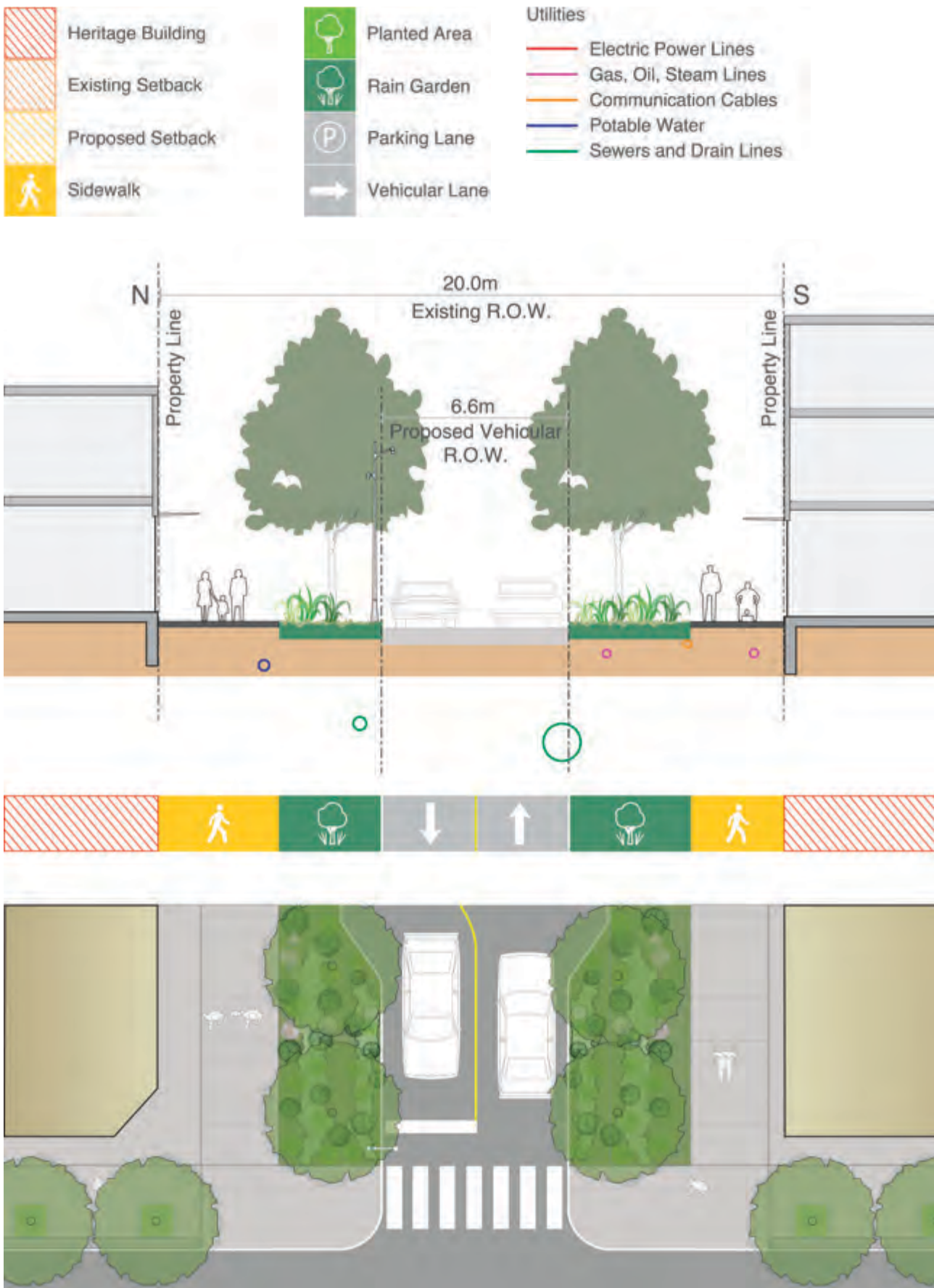


Figure 198 Section and Plan of Keewatin Greenway - mid-block Recommended Final Condition



SECTION 4.7

Figure 199 Section and Plan of Keewatin Greenway - Recommended Final Condition at major intersections with constrained sites

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

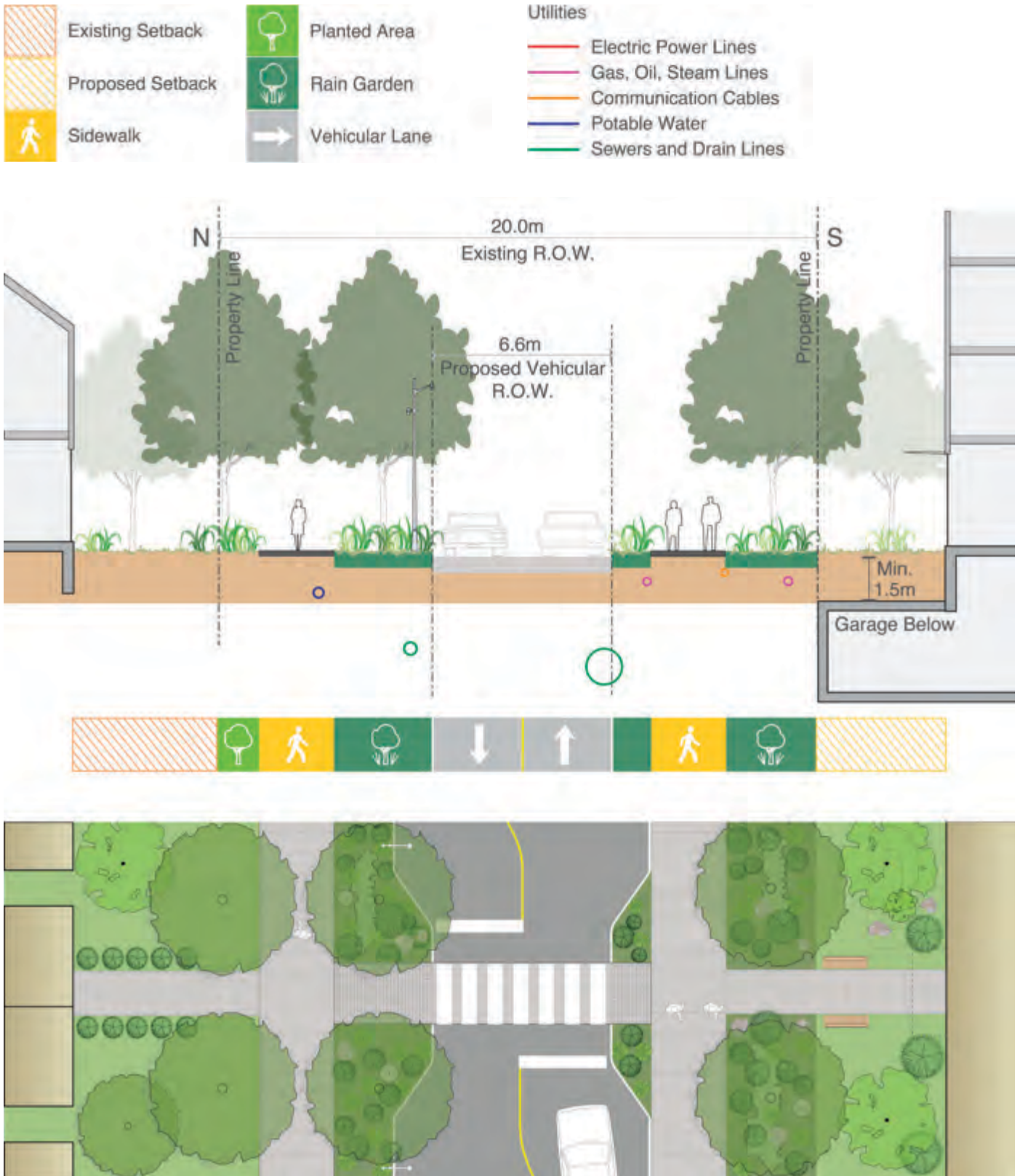


Figure 200 Section and Plan of Keewatin Greenway - Recommended Final Condition at mid-block raised crossing bump-out

Implementation

Development-related Improvements

New development will be responsible for providing a 5.0m building setback on the south side of the street; or maintaining existing landscaped setbacks with any new redevelopment that exceeds the aforementioned setbacks.

Capital Works and Coordination

Opportunities for public realm improvements may arise through upcoming capital works projects led by Toronto Water and Transportation Services. Taking advantage of other planned works represents an opportunity to coordinate and implement public realm improvements in the near-term and avoids disruption from additional construction work in the future.

Note: The implementation timeline represents information available from divisional partners and data from T.O. Inview as of December 31st, 2021 and is subject to change.

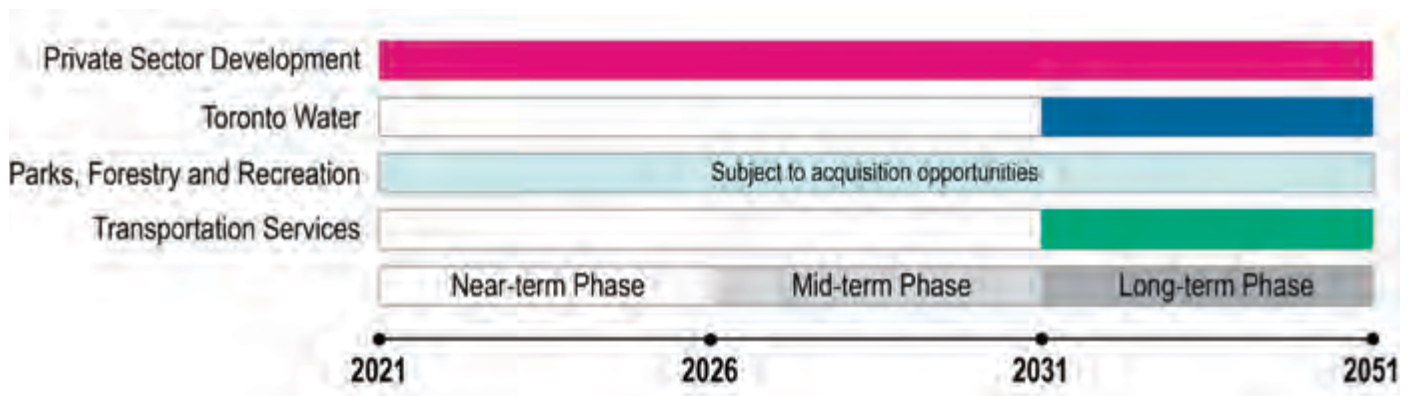


Figure 201 Projected Implementation Timeline for the Keewatin Greenway



Figure 202 Checklist of Public Realm Elements to be implemented for the Keewatin Greenway

Erskine Greenway

Erskine Avenue is another east-west Greenway street located south of Keewatin Avenue, consisting of Tower in the Park high-rise buildings.

Background Analysis

Existing Street Conditions

- 20.0m right-of-way (ROW)
- 8.6m vehicular ROW with two travel lanes and a parking lane
- Two-way street with parking on the south side of the street
- Substandard sidewalks on both sides of the street
- 2 pedestrian crosswalks at the intersection with Redpath Avenue in front of Redpath Avenue Parkette
- Various Building Setbacks
- Boulevard parking at Yonge Street intersection
- Bike Share Toronto station on the north side of the street at the intersection with Yonge Street



Figure 203 Existing Bike Share Toronto Station on the north side of Erskine Avenue



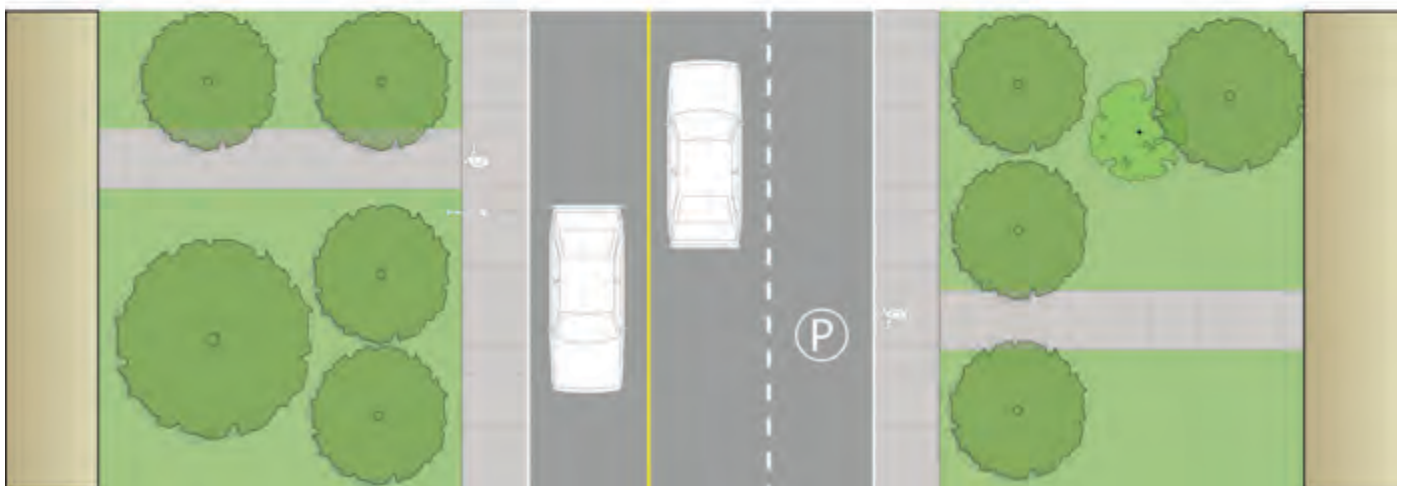
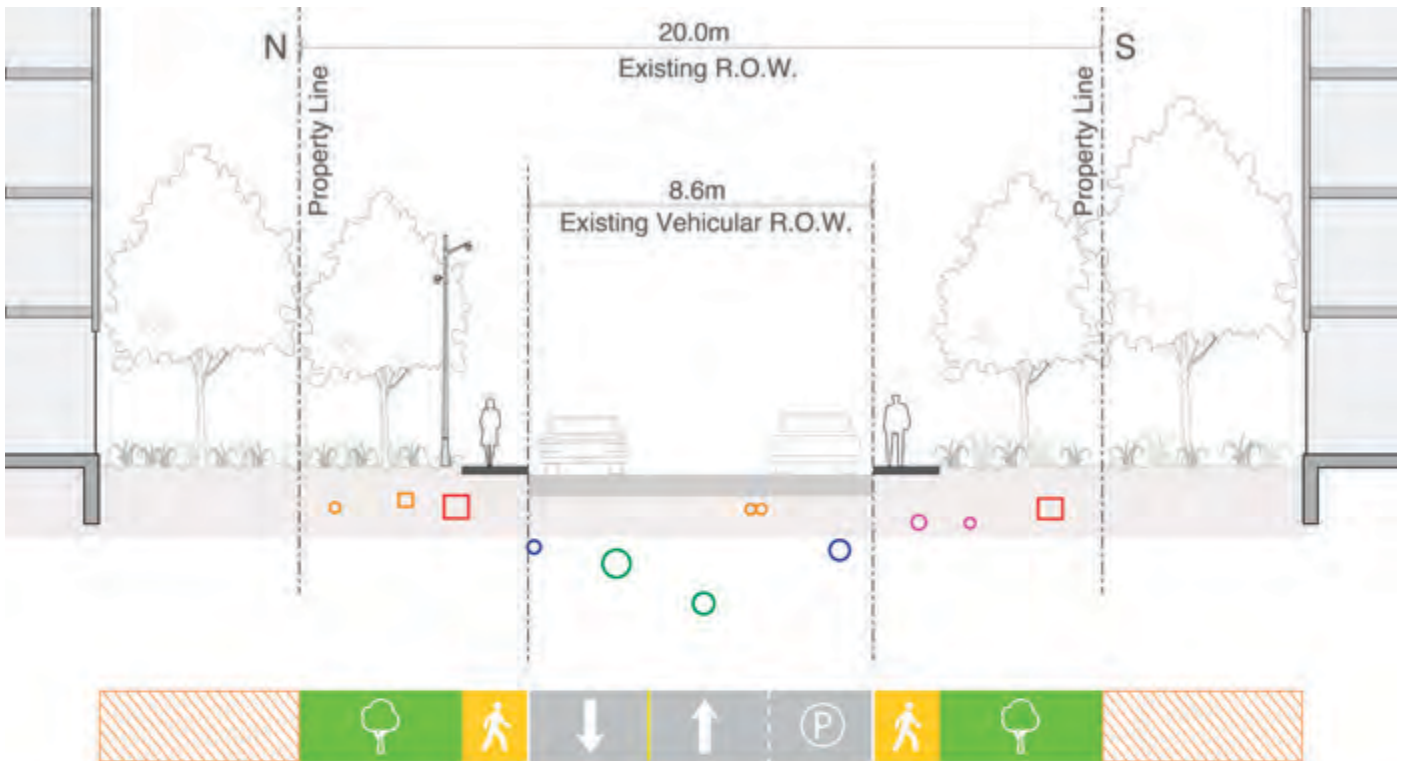
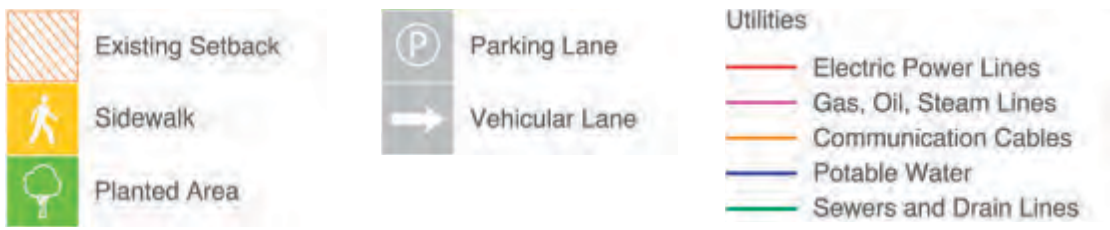
Figure 204 Existing condition at Redpath Avenue Parkette



Figure 205 Existing intersection at Redpath Avenue and Erskine Avenue



Figure 206 Existing intersection at Yonge Street and Erskine Avenue



SECTION 4.7

Figure 207 Section and Plan of the Existing Conditions along Erskine Avenue

Section 37 Contributions

Recent development in the area has provided community benefits through Section 37 contributions to the City. There are three development applications recently completed and built in Ward 15, comprising three sites along Erskine Avenue. With the approval of these applications, the City secured \$2,177,820, adjusted for interest to \$2,279,242 as of December 31st, 2021, towards streetscape and public realm improvements on Keewatin Avenue and in the Yonge-Eglinton Apartment Neighbourhoods.

Summary of Opportunities and Constraints

Sites with Redevelopment Potential

With a few available soft sites along the street, public realm improvements will start to occur incrementally. As these sites come forward for development, the public realm improvements will be provided by the developer, where possible. Additionally, public realm upgrades can be expanded beyond the developer's property line and be implemented by developers in front of other properties that will not be redeveloped in the near future.

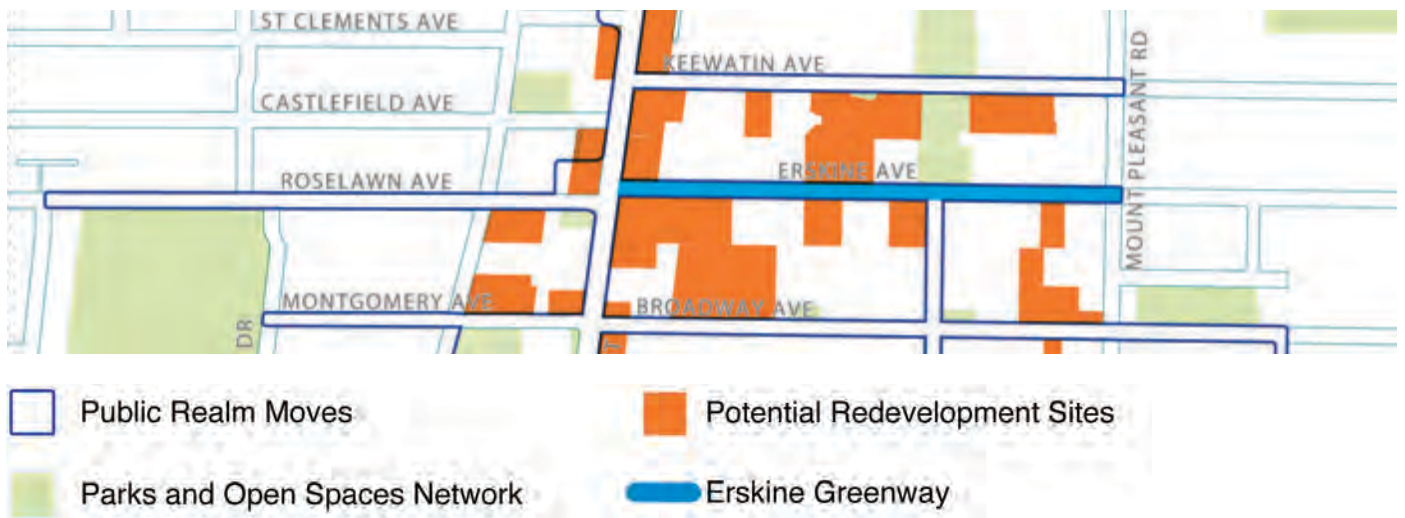


Figure 208 Map of potential redevelopment sites along the Erskine Greenway



Figure 209 Map of the Section 37 secured contribution locations along the Erskine Greenway

As there is limited additional development activity along Erskine Avenue, it will be challenging to implement a complete public realm upgrade through development only, in the near-term. There are two proposed development applications at 109 and 133 Erskine Avenue, where staff are working to secure landscape setbacks of at least 7.5m. On both sides of Erskine Avenue, sites are generally large and made up of both individual constrained sites and clusters of multiple lots that may experience potential redevelopment or infill development. The constrained sites have limited potential to provide landscape setbacks that are at least 7.5m due to being designated a heritage property, or having recent redevelopment or infill development activity.

Utility Locations and Key Issues

Key utility issues are very similar to the rest of the Midtown Public Realm Moves. Above-ground facilities such as hydro poles, fire hydrants and various utility infrastructure are located immediately adjacent to the sidewalks and prevent sidewalk widening and general public realm improvements.

Where planted bump-outs are proposed, existing catch basins may be in conflict and will need to be relocated or incorporated into the bump-out design.



Figure 210 Existing boulevard parking on the south side of Erskine Avenue



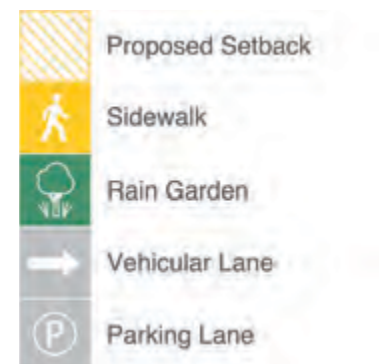
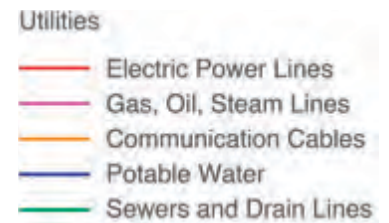
Figure 211 Existing utilities along Erskine Avenue



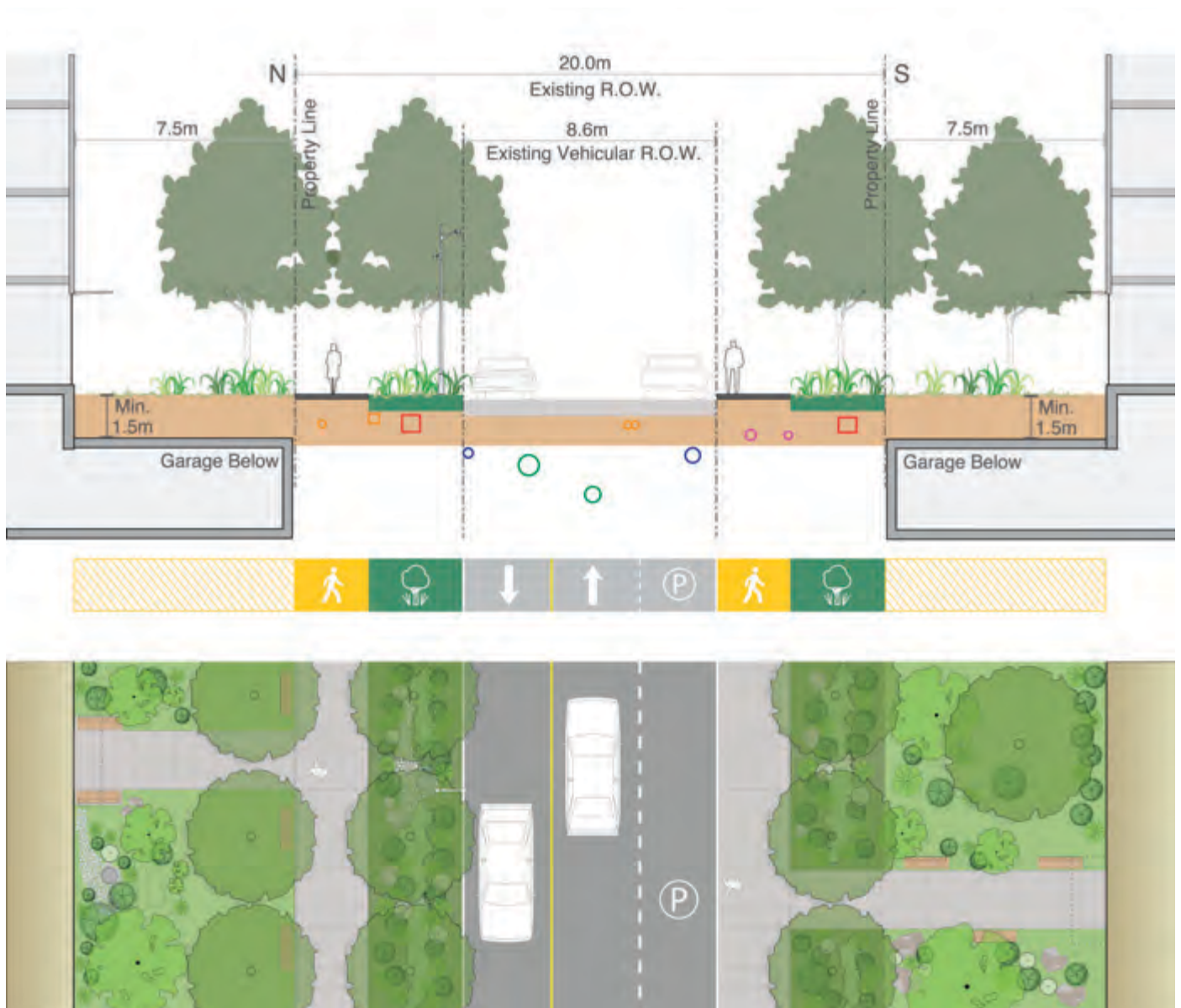
Figure 212 Diagram of opportunities and constraints at Erskine Avenue

Final Condition Recommendations:

- Maintain 8.6m vehicular ROW
- Provide planted bump-outs at Yonge Street and Mount Pleasant Road Intersections
- Provide planted bump-out at future and existing parks fronting the street
- Provide a 7.5m building setback from the property line to the building face on both sides of the street
- Provide a minimum 1.5m depth of planting soil on top of any underground structures in the setback to support new, healthy tree growth
- Provide above standard width sidewalk on both sides of the street
- Provide generous rain gardens on the both sides of the street
- Provide sidewalk transition zones to protect existing mature trees
- Provide planted bump-outs at mid-block pedestrian crossing associated with mid-block connections
- Remove all existing boulevard parking to upgrade, green and expand the public realm
- Relocate on-street parking impacted by the size and locations of the future bump-outs
- Relocate existing light standards/ hydro poles outside of the new pedestrian clearway (or, if possible, relocate hydro underground)



Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.



SECTION 4.7

Figure 213 Section and Plan of Erskine Greenway - Recommended Final Condition

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

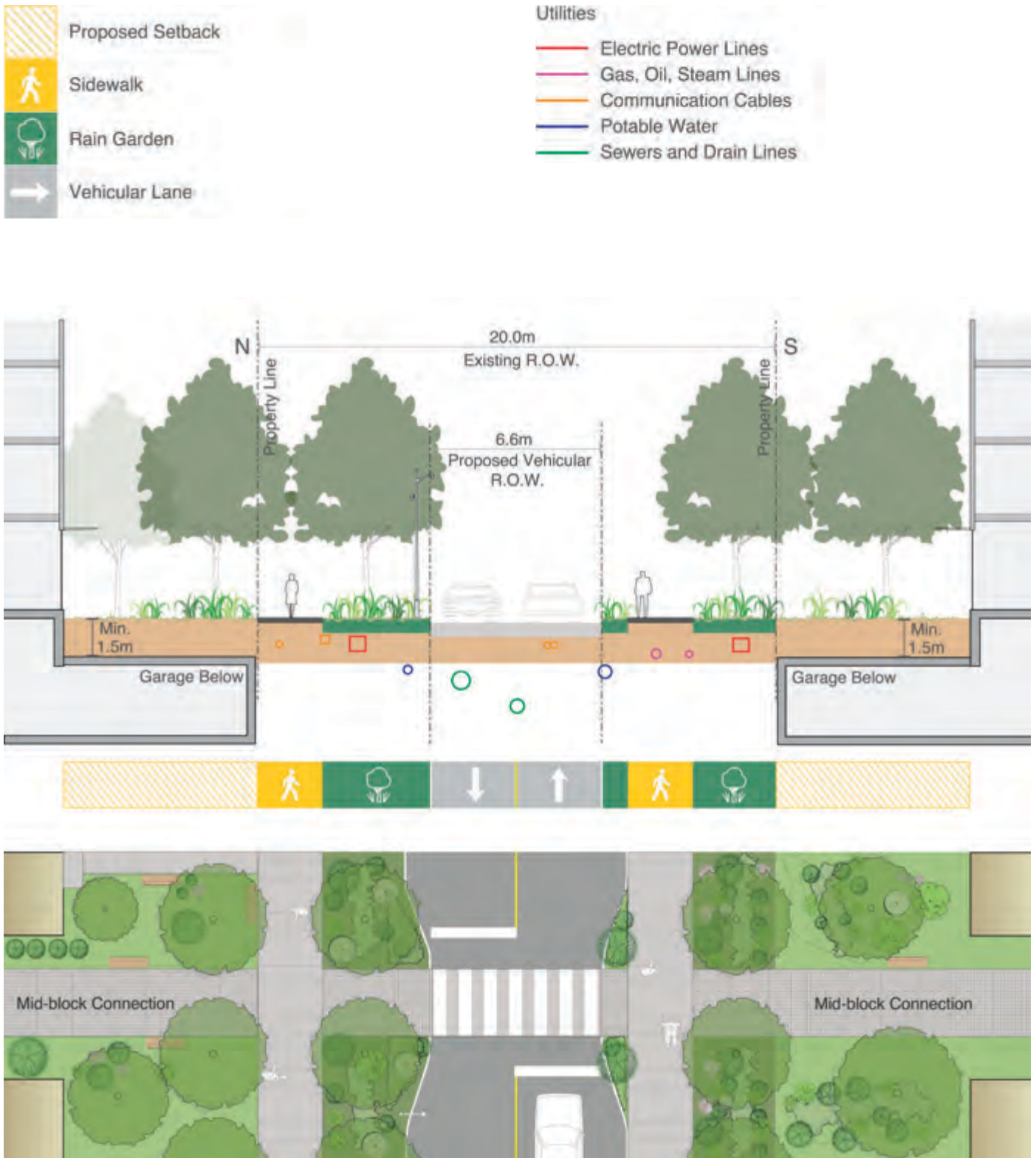
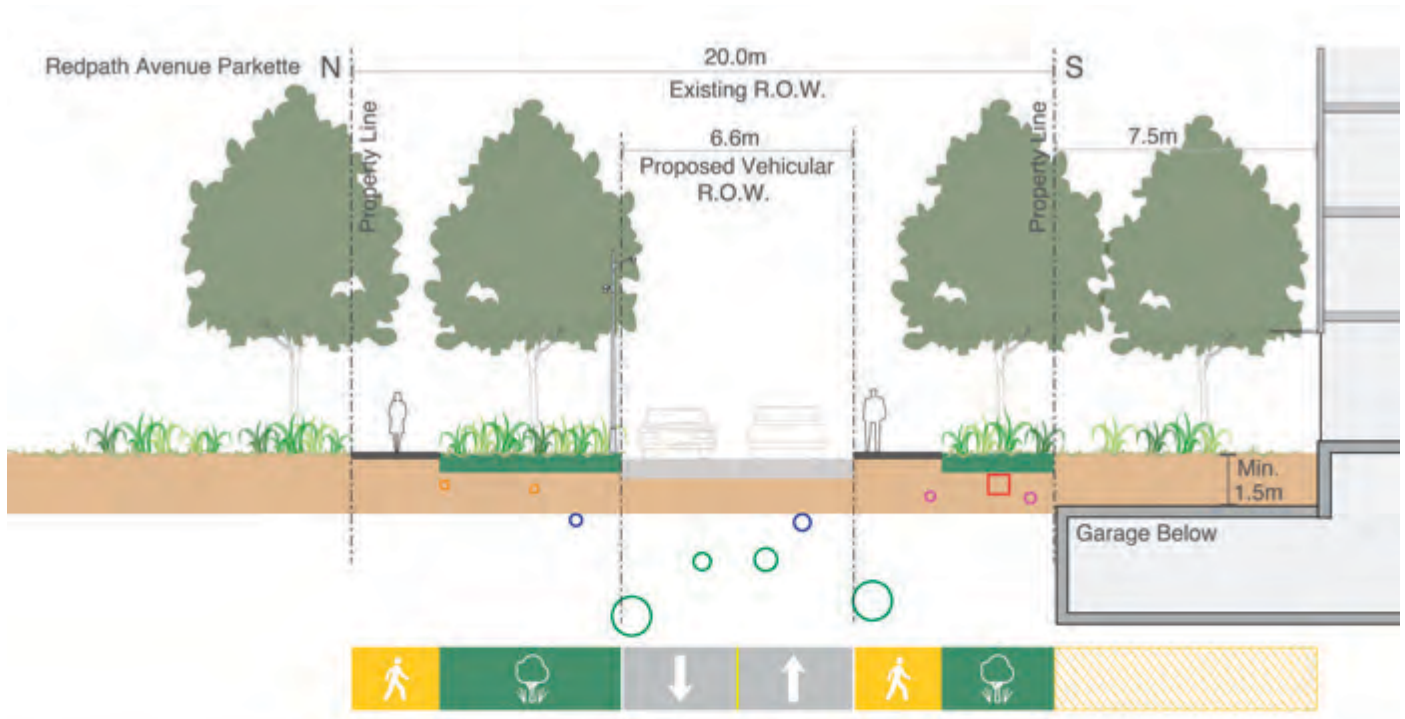


Figure 214 Section and Plan of Erskine Greenway - Recommended Final Condition at mid-block raised crossing bump-out



SECTION 4.7

Figure 215 Section and Plan of Erskine Greenway - Recommended Final Condition at bump-out in front of parks

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.



Figure 216 Section and Plan of Erskine Greenway - Recommended Final Condition at major intersections with constrained sites

Implementation

Development-related Improvements

New development will be responsible for providing a 7.5m building setback on both sides of the street; or maintain existing landscape setbacks with any new redevelopment that exceed the aforementioned setbacks.

Capital Works and Coordination

Opportunities for public realm improvements may arise through upcoming capital works projects led by Toronto Water and Transportation Services. Taking advantage of other planned works represents an opportunity to coordinate and implement public realm improvements in the near-term and avoids disruption from additional construction work in the future.

Note: The implementation timeline represents information available from divisional partners and data from T.O. Inview as of December 31st, 2021 and is subject to change.

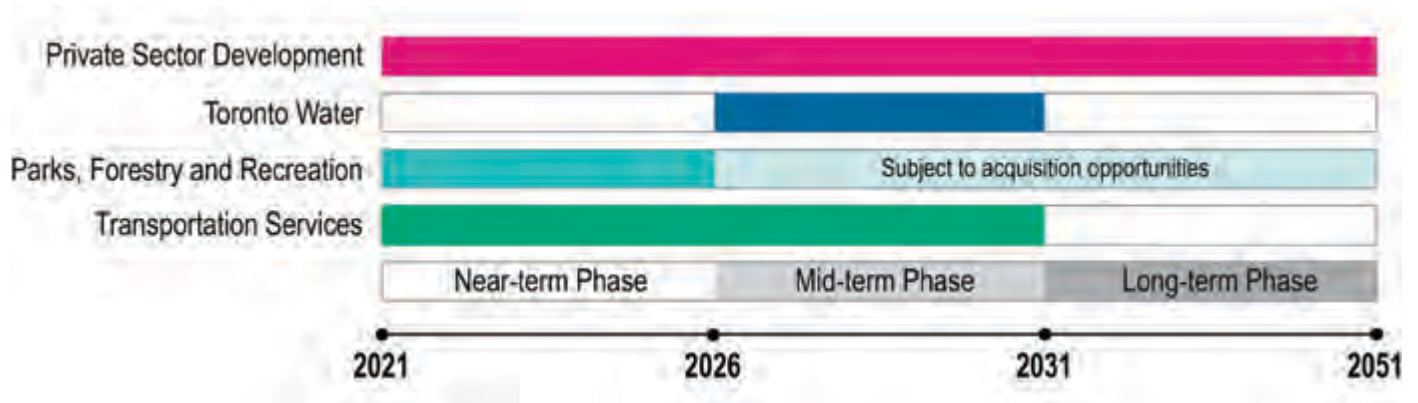


Figure 217 Projected Implementation Timeline for the Erskine Greenway



Figure 218 Checklist of Public Realm Elements to be implemented for the Erskine Greenway

The Greenways

Holly Street, Dunfield Avenue, Lillian Street and Brownlow Avenue run north-south between Eglinton East and Soudan Avenues and connect large apartment blocks.

Soudan Avenue is the most southern east-west Greenway street, consisting of predominantly new development with parks and open spaces on the north side and low-rise residential houses on the south side.

Background Analysis

Existing Conditions for 5 Greenway Streets

- 20.0m right-of-way (ROW)
- 8.6m vehicular ROW with two travel lanes and a parking lane
- Holly Street:
 - Two-way street with parking on the west side of the street
 - Pedestrian east-west crosswalk at the intersection with Eglinton Avenue East
- Dunfield Avenue:
 - One-way street south with parking on the east side of the street
 - Pedestrian east-west crosswalk at the intersection with Eglinton Avenue East
- Lillian Street:
 - One-way street south with parking on the west side of the street
- Brownlow Avenue:
 - Two-way street south with parking on the west side of the street
 - Pedestrian north-south and east-west crosswalks at the intersections with Soudan Avenue and east-west crosswalk at Eglinton Avenue East
- Soudan Avenue:
 - Two-way street south with parking on the north side of the street, with multiple driveways and residential front yards on both sides of the street

- Pedestrian north-south and east-west crosswalks at the intersections with Mount Pleasant Road
- Substandard pedestrian clearway on narrow sidewalks width range from 1.5m-2.5m on both sides of all the street in various locations along the existing public realm
- Existing trees planted in various conditions, such as planted boulevards and hardscape areas
- Various building setbacks

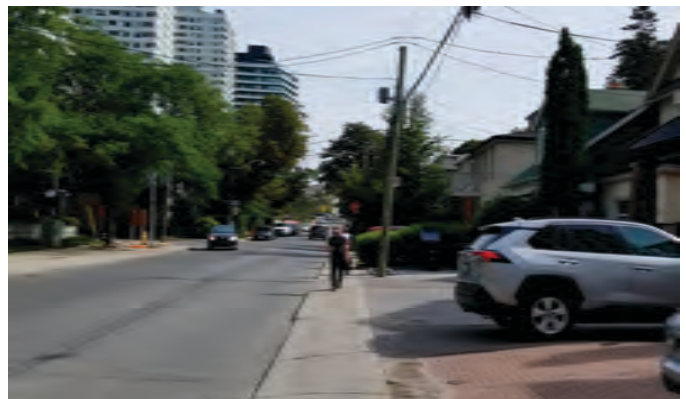


Figure 219 Existing conditions along Soudan Avenue



Figure 220 Existing conditions along Lillian Street



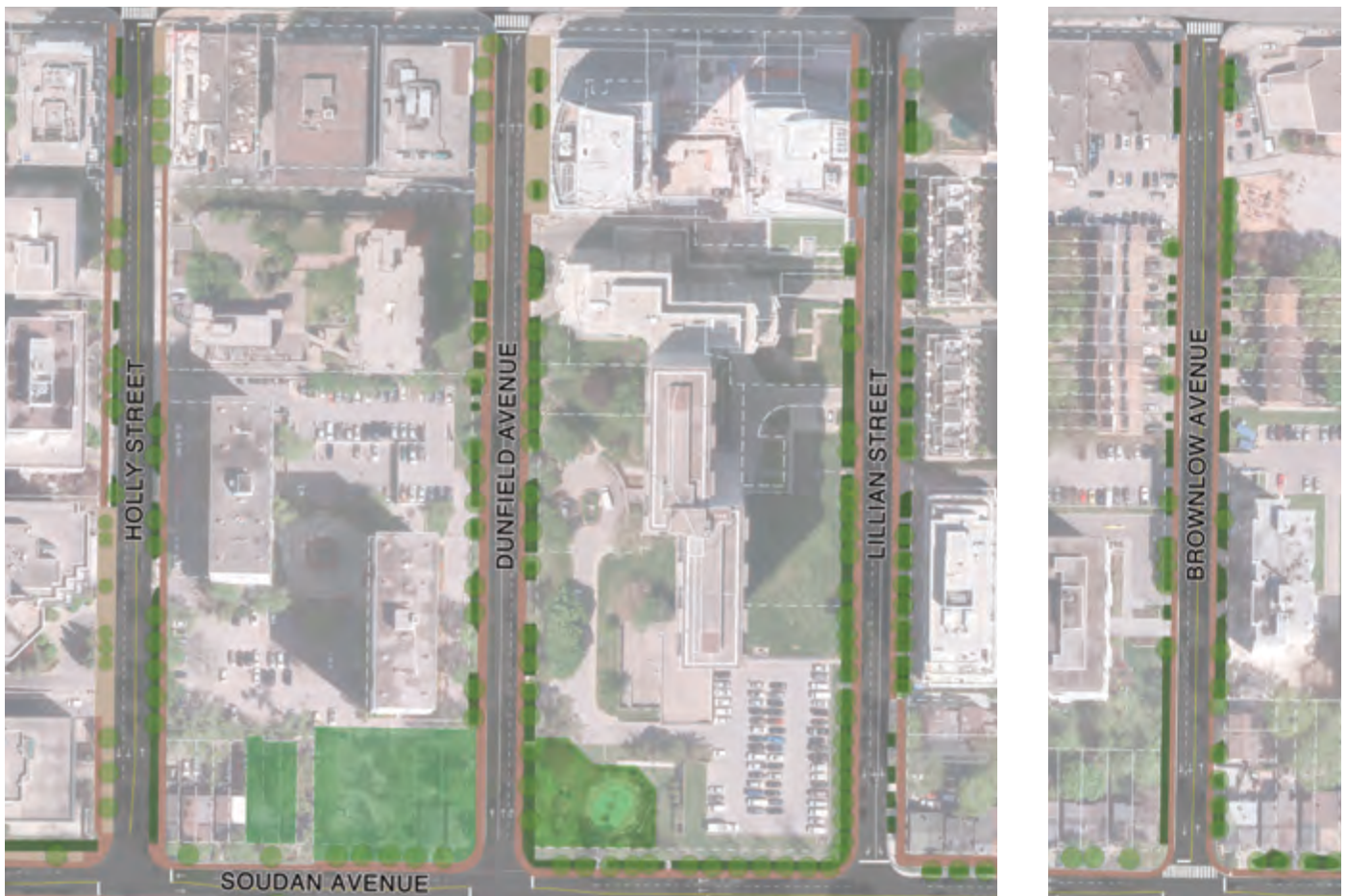


Figure 221 Plan of Existing Conditions along Holly Street, Dunfield Avenue, Lillian Street, and Brownlow Avenue

SECTION 4.7



Figure 222 Plan of Existing Conditions along Soudan Avenue between Yonge Street and Lillian Street

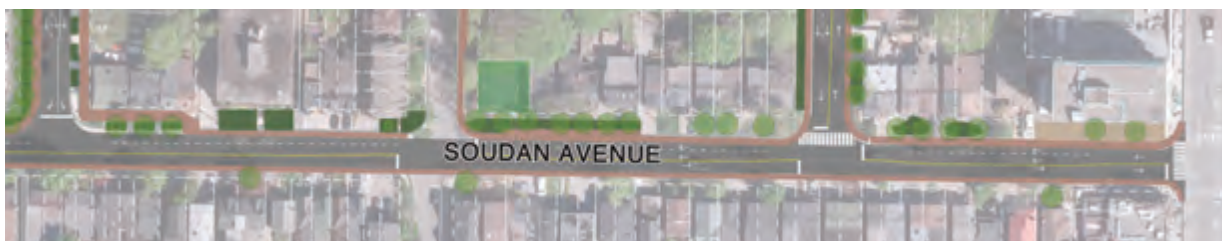


Figure 223 Plan of Existing Conditions along Soudan Avenue between Lillian Street and Mount Pleasant Road

Secured Section 37 Contributions

Recent development in the area has provided community benefits through Section 37 contributions to the City. There is one development application under construction in Ward 12, at 174-188 Soudan Avenue. With the approval of this application, the City secured \$940,854 as of December 31st, 2021, towards streetscape improvements in the area.

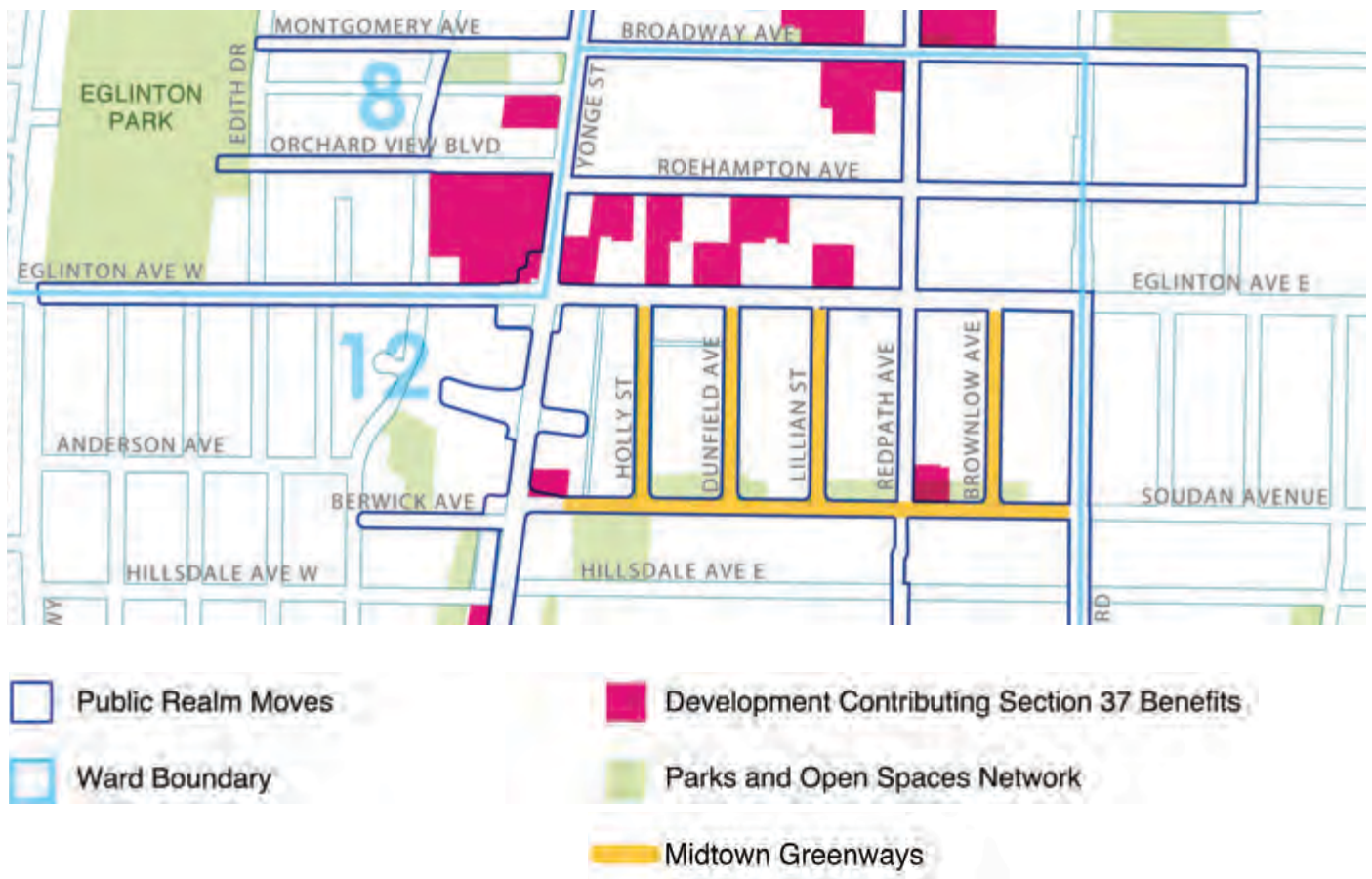


Figure 224 Map of the Section 37 secured contribution locations along the Midtown Greenways

Summary of Opportunities and Constraints

Sites with Redevelopment Potential

With a few soft sites available and a number of approved and under review development applications, public realm improvements will start to occur incrementally. As these sites come forward for development, the public realm improvements will be provided by the developer, where possible. Additionally, public realm upgrades can be expanded beyond the property line to provide enhancements in front of properties that have been recently constructed and will not be redeveloped in the near future.

With several development applications under review or approved, there are opportunities to implement a complete public realm upgrade through development in the near-term. Approved applications are located west of Redpath Avenue and near Soudan Avenue, where landscape setbacks were secured to the previous public realm standards prior to the Yonge-Eglinton Secondary Plan. Proposed applications are concentrated along Brownlow Avenue, where staff are working to secure landscape setbacks through the development review process. There are also multiple sites spread along the Midtown Greenways where there may be future redevelopment.

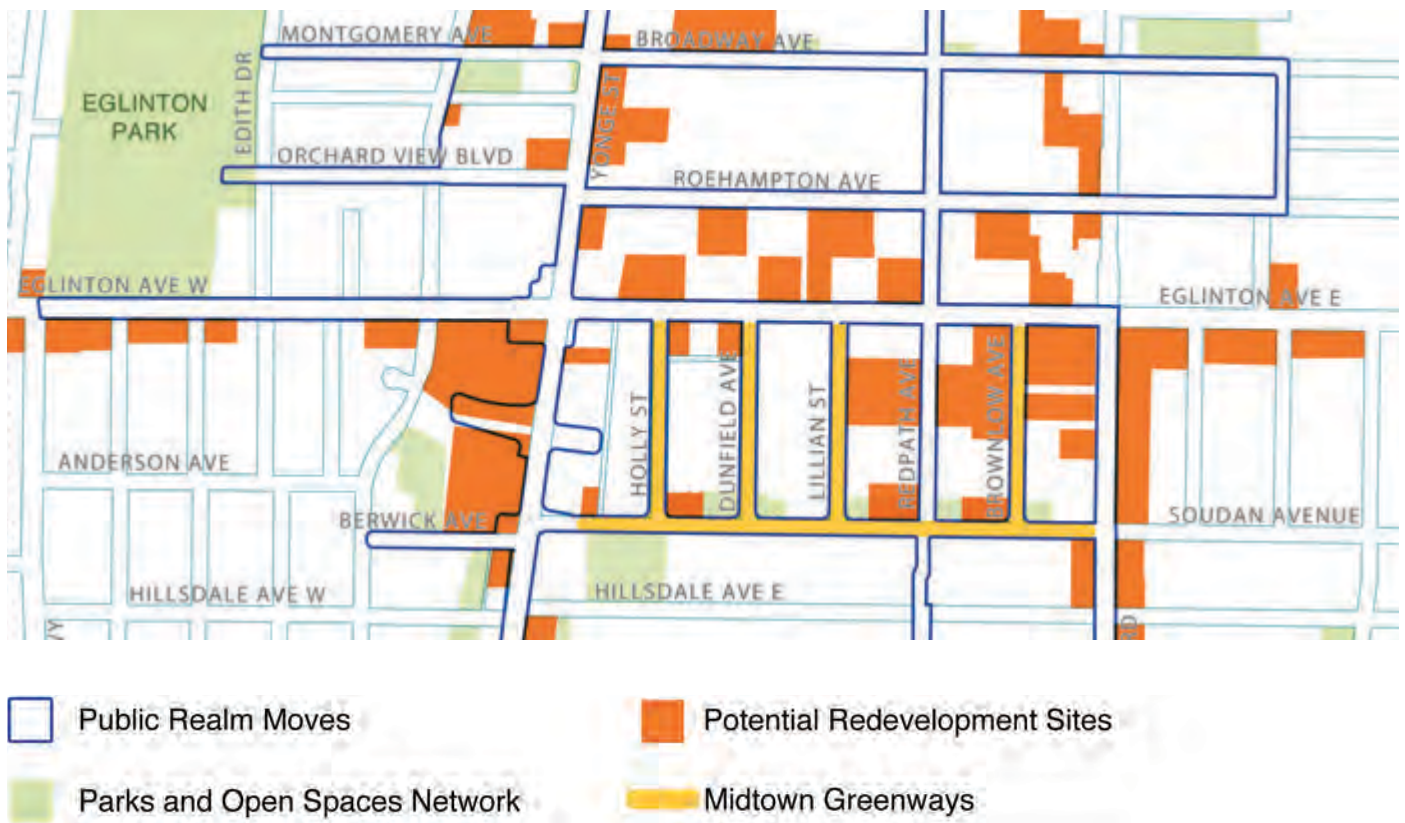


Figure 225 Map of potential redevelopment sites along the Midtown Greenways

Constrained sites are located in three areas. Between Dunfield Avenue and Lillian Street, lots are generally constrained due to having recent infill development activity. Sites on the north side of Soudan Avenue are also constrained as they are located on the sites of future and planned parks. These constrained sites have limited development potential to provide landscape setbacks that are at least 5.0m on both sides of streets in the area, except along the south side of Soudan Avenue where this standard does not apply.

On the south side of Soudan Avenue, nearly all lots are constrained as they are designated *Neighbourhoods* in the Official Plan and do not permit large-scale intensification.

With a number of newly built developments, the public realm has been constructed to the previous standards. Approved and under review applications will start to implement public realm improvements in the near-term.



Figure 226 Diagram of opportunities and constraints at Holly Street, Dunfield Avenue, Lillian Street, Brownlow Avenue, and Soudan Avenue

Utility Locations and Key Issues

Similar to the rest of the Midtown Public Realm Moves, above-ground facilities such as hydro poles, fire hydrants and utility vaults are located immediately adjacent to the sidewalks and hinder sidewalk widening.

Where planted bump-outs are recommended, existing catch basins may be in conflict and will need to be relocated or incorporated into the bump-out design.

Along north-south Greenways, Holly Street and Dunfield Avenue, existing 150mm watermains run below the sidewalks and planted areas on the west side of the street. The current condition along Holly Street indicates existing mature trees in very close proximity to the existing watermain. If the existing trees are to remain in their current locations, proposed trees should follow the same alignment, minimizing conflict with the watermain as much as possible. On Lillian Street, an old watermain has been abandoned below the west sidewalk and a current existing watermain is located in the vehicular portion. On Brownlow Avenue, the existing watermain is located in the vehicular portion of the street.

The lack of consistent public realm language along the Greenways, plus numerous access driveways, private curbs, fences, bollards, railings, steps and planters, create challenges to implement a continuous cohesive streetscape.

Along all the streets, existing mature trees are planted in various patterns and are often immediately adjacent to the existing substandard wide sidewalk. Where possible, healthy trees should be protected with minimal encroachments into the Tree Protection Zones.



Figure 227 Utility conflicts along Brownlow Avenue



Figure 228 Utility conflicts along Holly Street



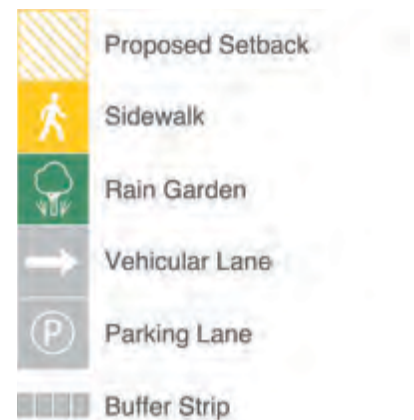
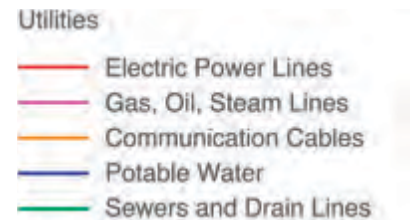
Figure 229 Utility conflicts along Lillian Street



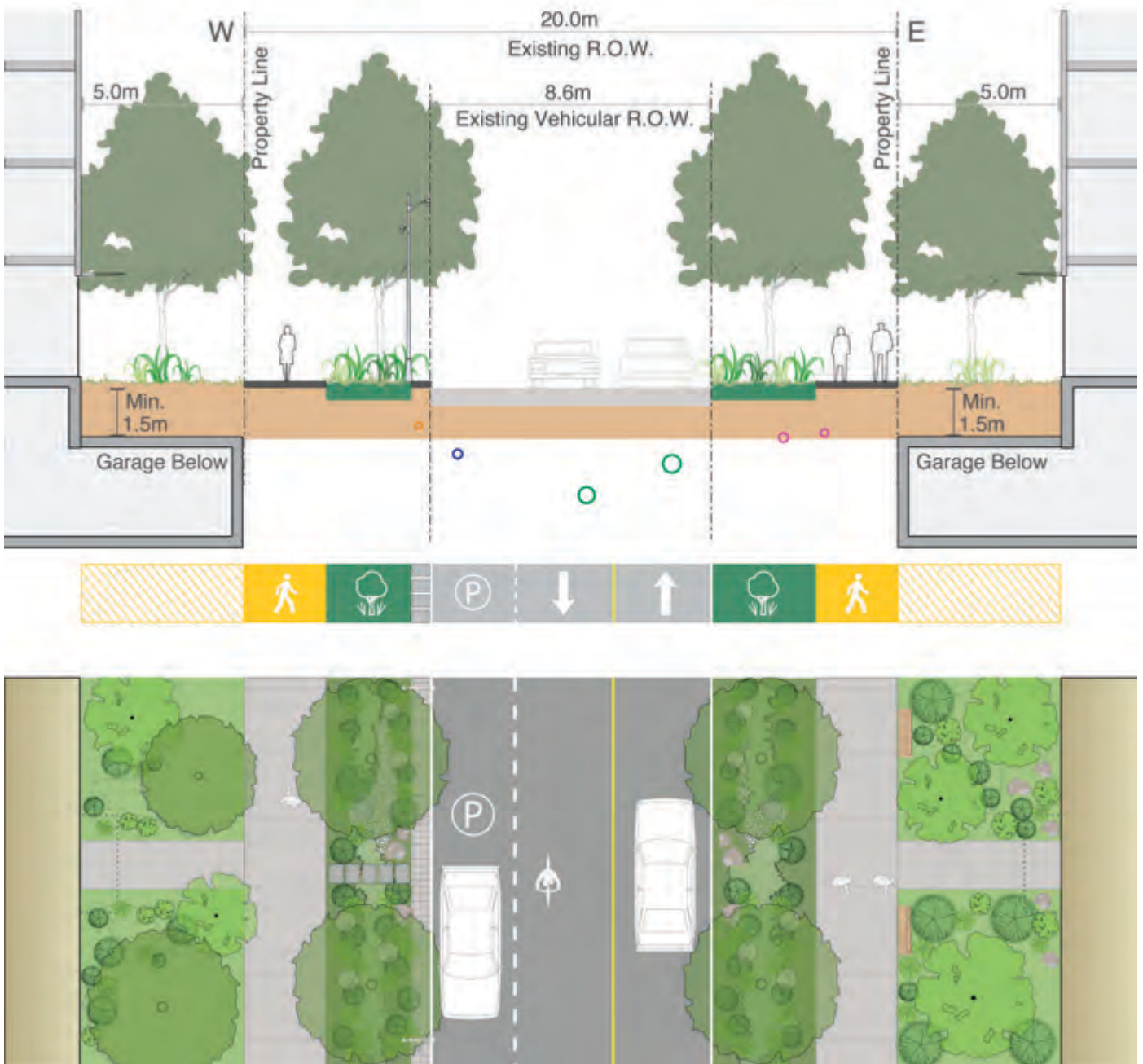
Figure 230 Utility and existing tree conflicts along Soudan Avenue

Final Condition Recommendations:

- Maintain 8.6m vehicular ROW
- Provide planted bump-outs at future and existing parks fronting the street
- Provide a 5.0m building setback from the property line to the building face along both sides of all Greenways, except for the south side of Soudan Avenue
- Provide a minimum 1.5m depth of planting soil on top of any underground structures in the setback to support new, healthy tree growth
- Provide above standard width sidewalk on both sides of the street
- Provide generous rain gardens on both sides of the street. Incorporate a unit paver buffer and stepping stones to allow for easy on-street parking access
- Provide sidewalk transition zones to protect existing mature trees
- Provide planted bump-outs at the intersections of Brownlow Avenue and Eglinton Avenue, Brownlow Avenue and Soudan Avenue and Soudan Avenue and Mount Pleasant Road
- Relocate on-street parking impacted by the size and locations of the future bump-outs
- Relocate existing light standards/ hydro poles outside of the new pedestrian clearway (or, if possible, relocate hydro underground)



Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.



SECTION 4.7

Figure 231 Section and Plan of Holly and Brownlow Greenways - Recommended Final Condition

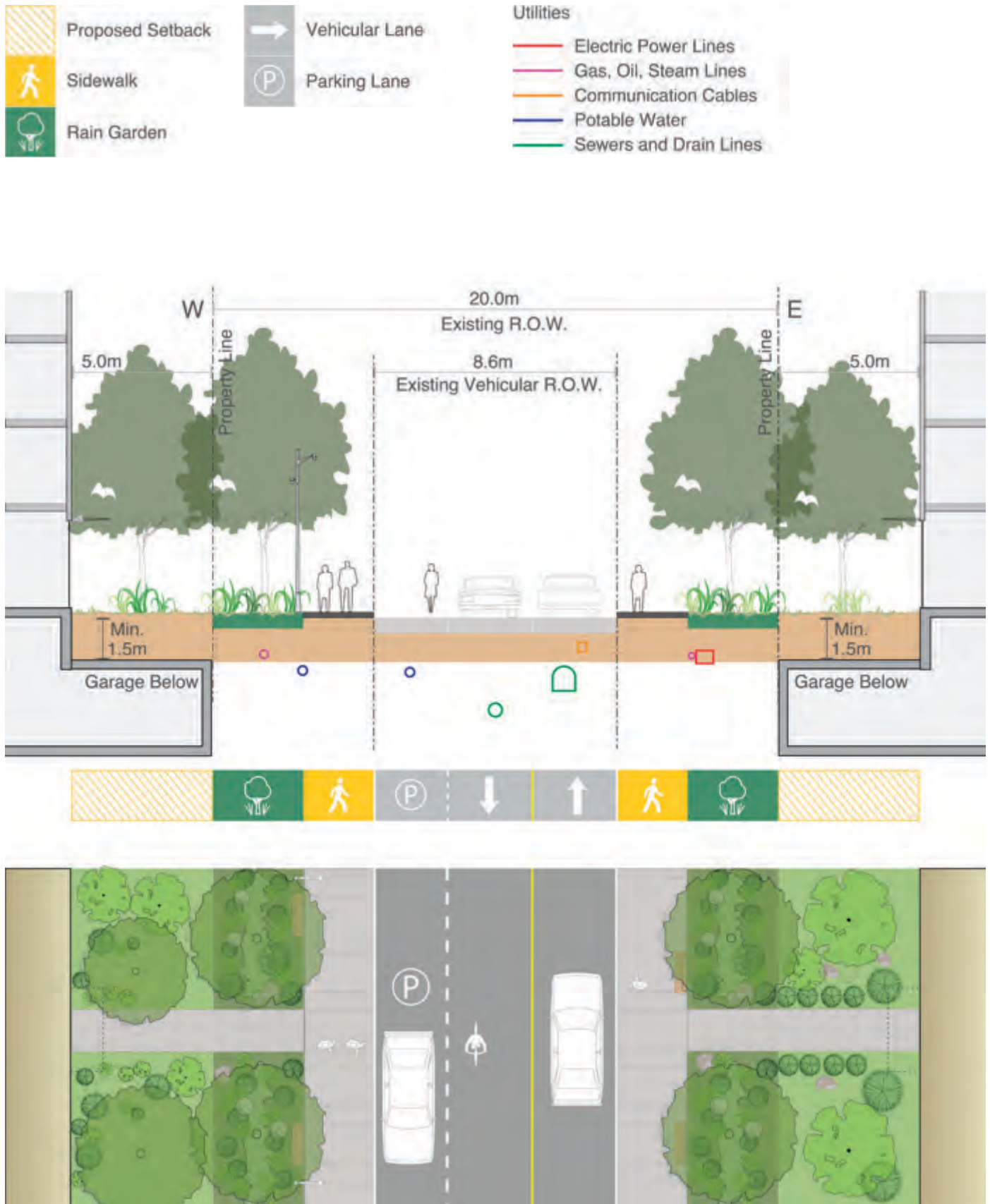
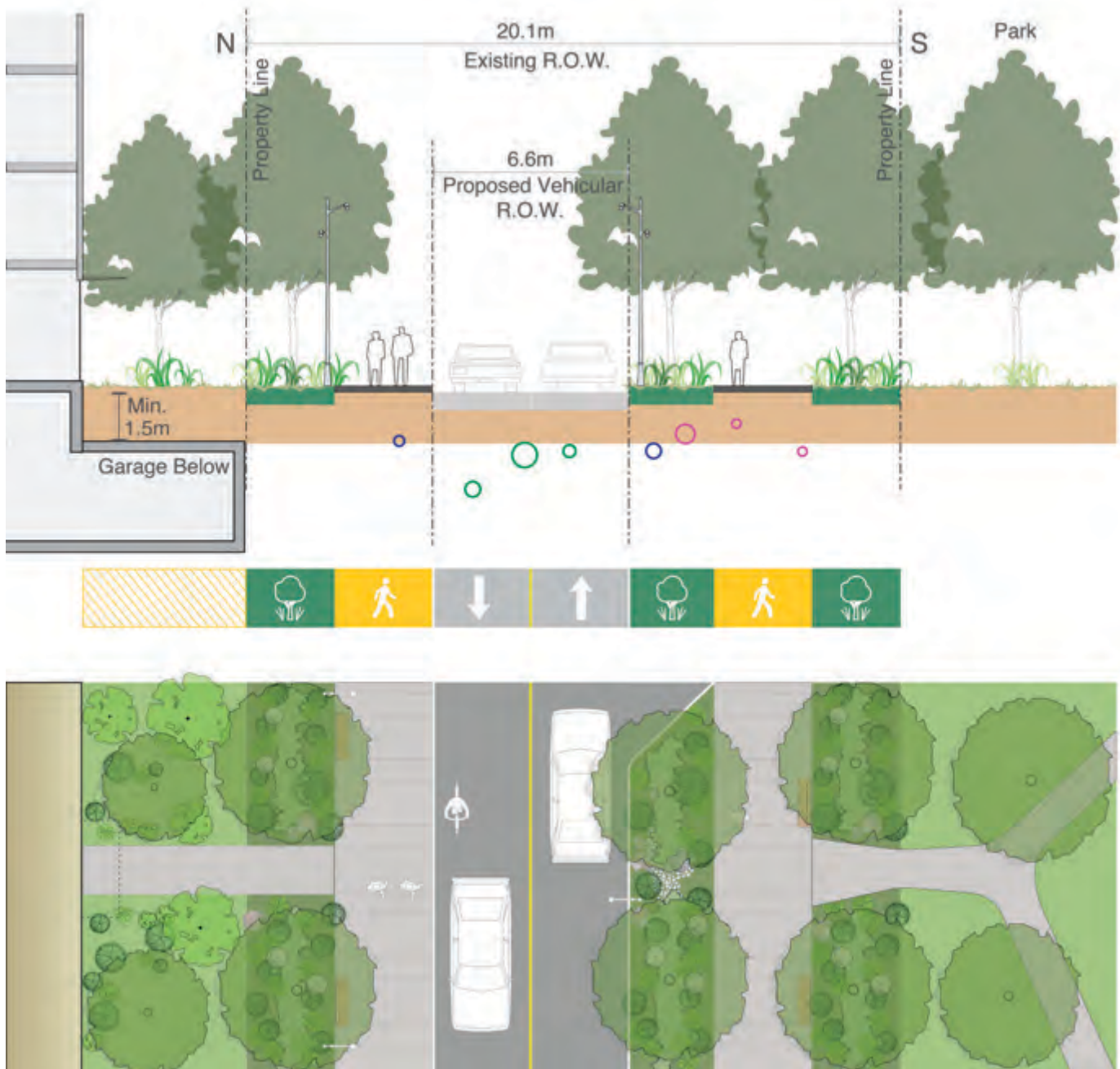


Figure 232 Section and Plan of Lillian and Dunfield Greenways - Recommended Final Condition

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.



SECTION 4.7

Figure 233 Section and Plan of Soudan Greenway - bump-out in front of parks Recommended Final Condition

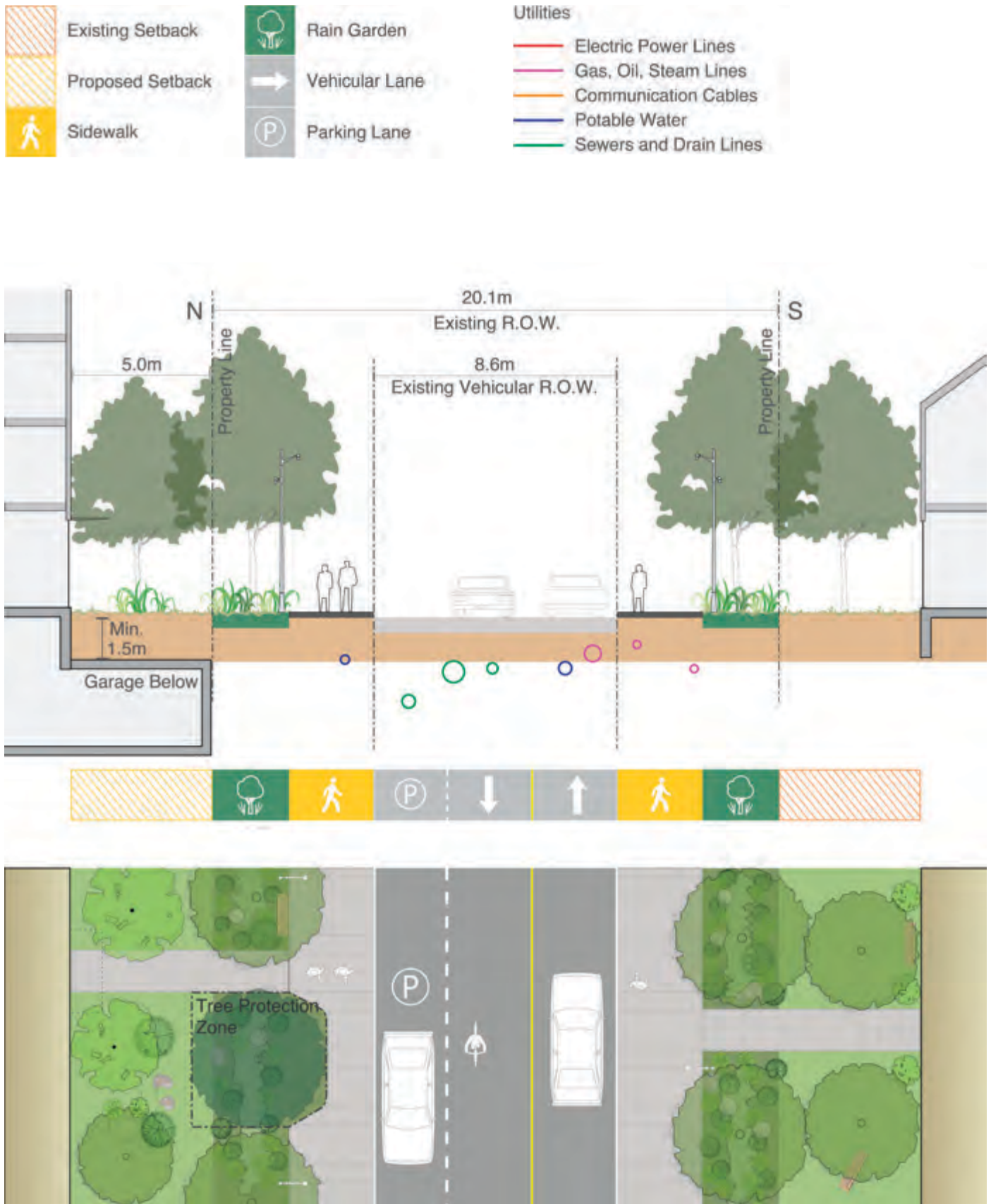
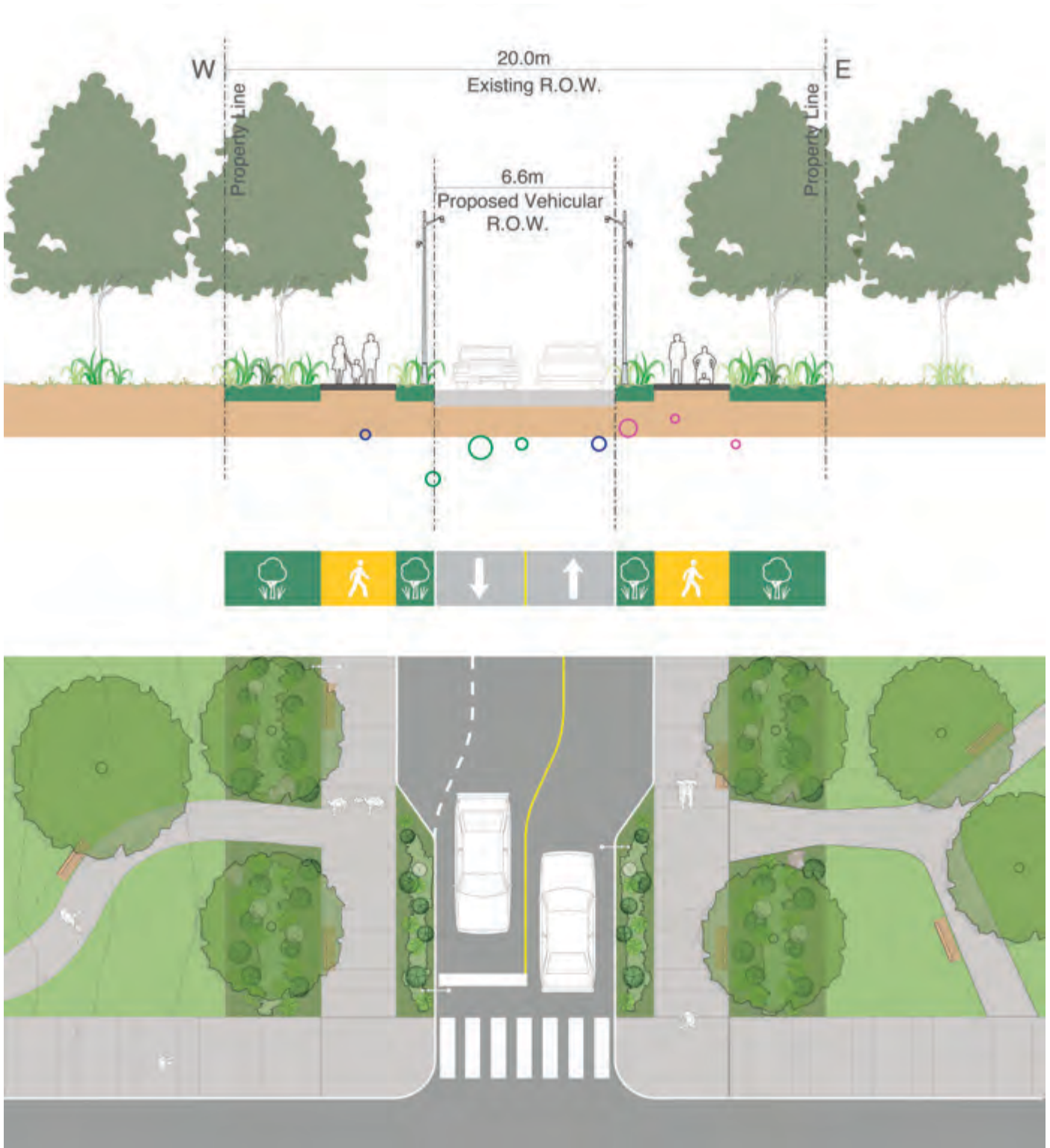


Figure 234 Section and Plan of Soudan Greenway - Recommended Final Condition

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.



SECTION 4.7

Figure 235 Section and Plan of general Greenways - Recommended Final Condition at major intersections

Implementation

Development-related Improvements

With a few development applications under review, public realm improvements will happen incrementally. As soft sites start to redevelop, the upgraded public realm will become more continuous.

New development will be responsible for providing a 5.0m building setback along all sides of all streets except for south side of Soudan Avenue, or maintain existing landscape setbacks with any new redevelopment that exceed the aforementioned setbacks.

Capital Works and Coordination

Opportunities for public realm improvements may arise through upcoming capital works projects led by Toronto Water and Transportation Services. Taking advantage of other planned works represents an opportunity to coordinate and implement public realm improvements in the near-term and avoids disruption from additional construction work in the future.

Note: The implementation timeline represents information available from divisional partners and data from T.O. Inview as of December 31st, 2021 and is subject to change.

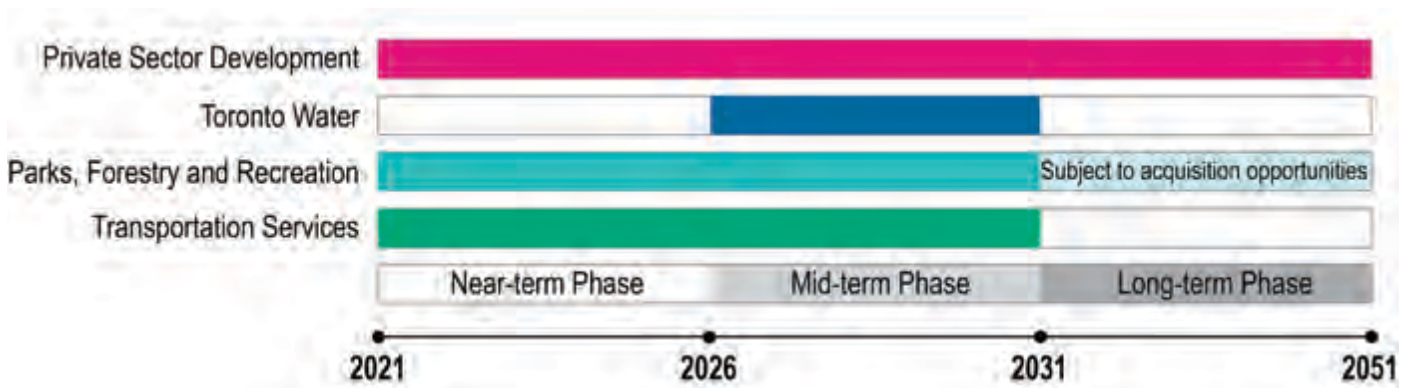


Figure 236 Projected Implementation Timeline for the Midtown Greenways



Figure 237 Checklist of Public Realm Elements to be implemented for the Midtown Greenways

4.8 Redpath Revisited

- Overview
- Background Analysis
- Final Condition Recommendations
- Implementation

Overview

Redpath Avenue is an important north-south route in Midtown. It is the only north-south street that crosses Eglinton Avenue between Yonge Street and Mount Pleasant Road, providing a key pedestrian connection for the area. Bookended by two great neighbourhood parks, Redpath Avenue will become an important local meeting place within Midtown to serve the everyday needs of a diverse and growing community.

Policy Direction

The Yonge-Eglinton Secondary Plan includes the following policies related to the Redpath Revisited Public Realm Move:

3.2.7. Redpath Revisited

- a. Redpath Avenue is an important north-south neighbourhood street that connects to Redpath Avenue Parkette to the north and the Church of the Transfiguration site to the south.
- b. On the block between Eglinton Avenue and Roehampton Avenue opportunities will be explored to reduce vehicular speed, improve pedestrian and cycling safety and include shared street elements. This block of Redpath Avenue will:
 - i. accommodate outdoor commercial uses, public art and opportunities for community activities; and
 - ii. incorporate a fine grain of active retail uses and universally-accessible building entrances at grade that promote a safe and animated pedestrian environment.

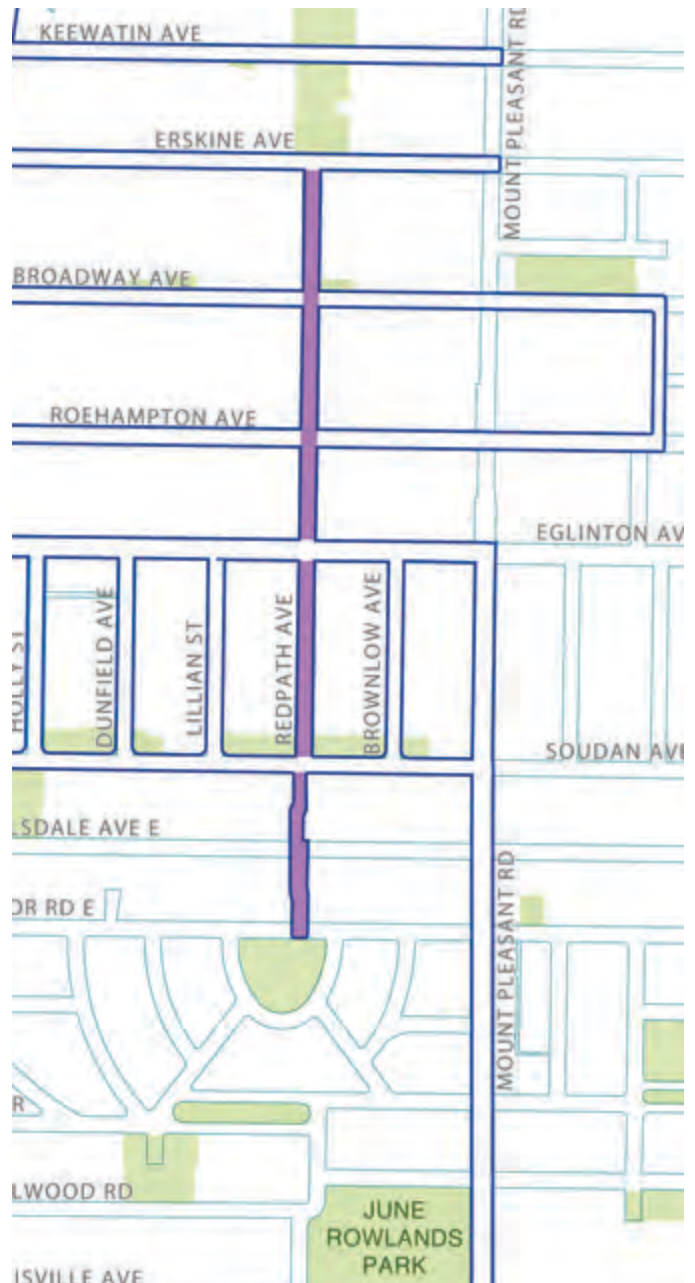
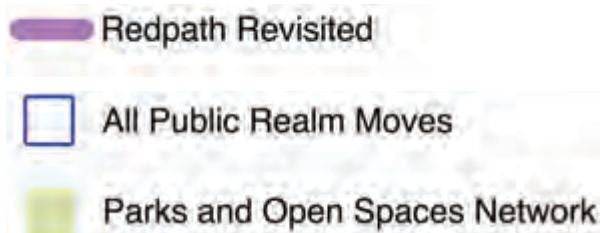


Figure 238 Map showing the location of the Redpath Revisited Public Realm Move

Background Analysis

Existing Street Conditions

- 20.0 m right-of-way (ROW)
- Existing trees planted in various conditions, such as planted boulevards and hardscape areas
- Various building setbacks
- 8.6m vehicular ROW, including 2 vehicular travel lanes and a parking lane
- Erskine Avenue to Broadway Avenue:
 - Two-way street with parking on the east side of the street
 - Substandard pedestrian clearway on narrow sidewalks on both sides of the street
 - Multiple driveways on the west side of the street
 - TTC Transit Stop
 - Pedestrian north-south and east-west crosswalks at the intersections with Erskine Avenue and Broadway Avenue



Figure 239 Existing condition at the intersection of Redpath and Roehampton Avenues



Figure 240 Existing condition on the west side of Redpath Avenue

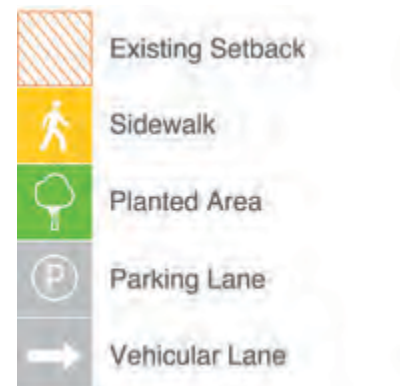


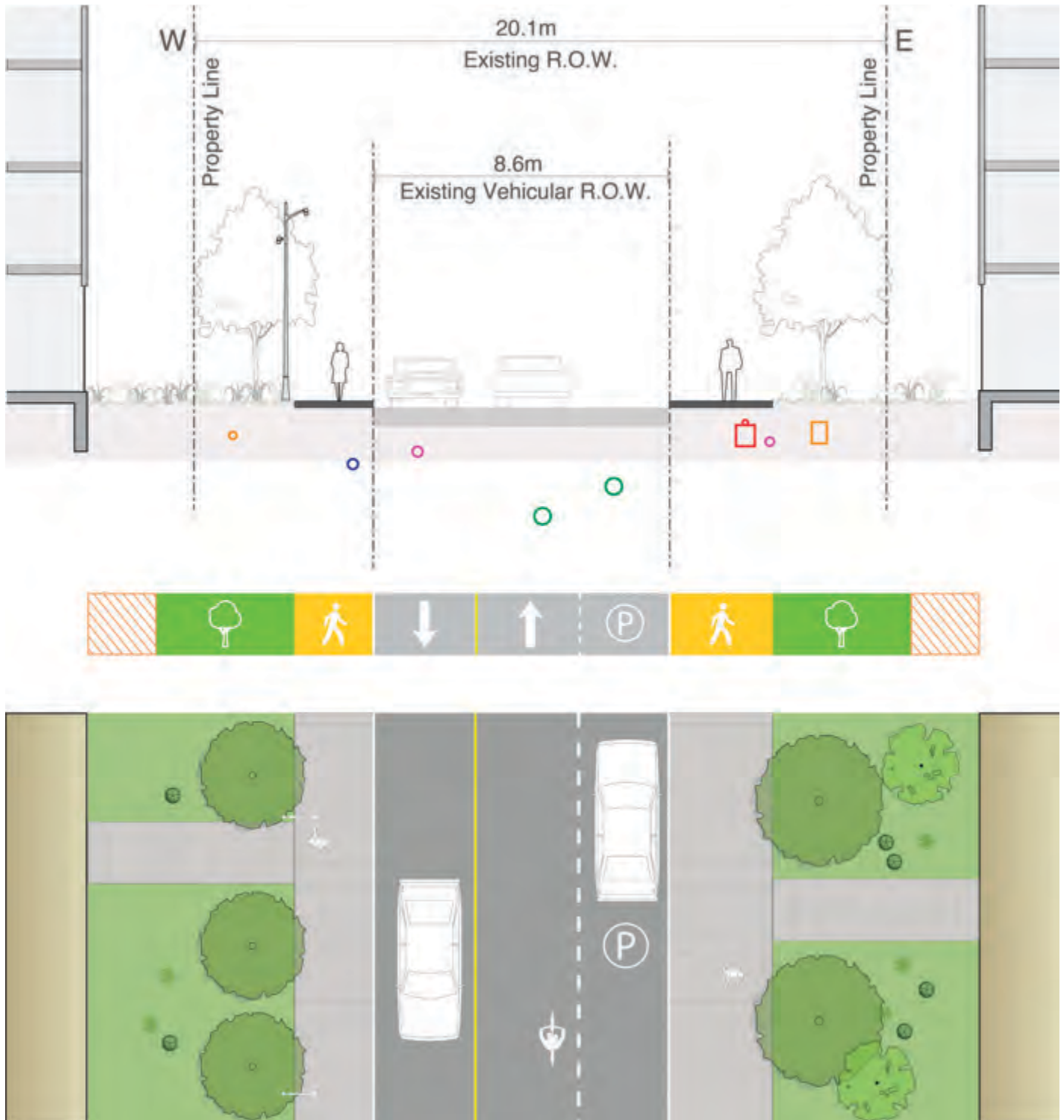
Figure 241 Existing condition on Redpath Avenue



Figure 242 Existing condition south of Soudan Avenue on Redpath Avenue

- **Broadway Avenue to Roehampton Avenue:**
 - Two-way street with only restricted parking on the east side of the street
 - Substandard pedestrian clearway on narrow sidewalks on both sides of the street
 - A few driveways on the west side of the street
 - Pedestrian north-south and east-west crosswalks at the intersection with Roehampton Avenue
- **Roehampton Avenue to Eglinton Avenue:**
 - Two-way street with no parking on either side of the street
 - Substandard pedestrian clearway with new unit paver sidewalks on both sides of the street closer to Roehampton Avenue
 - TTC Transit Stop
 - Signalized intersection with pedestrian north-south and east-west crosswalks at Eglinton Avenue
- **Eglinton Avenue to Soudan Avenue:**
 - Two-way street with some parking on the east side of the street
 - Substandard pedestrian clearway on narrow sidewalks on both sides of the street
- **Soudan Avenue to Manor Road:**
 - Two-way street with restricted parking on the east side of the street
 - No sidewalk on the west side of the street
 - Substandard pedestrian clearway on narrow sidewalk on the east side of the street





SECTION 4.8

Figure 243 General Section and Plan of the Existing Conditions along Redpath Avenue from Erskine Avenue to Soudan Avenue

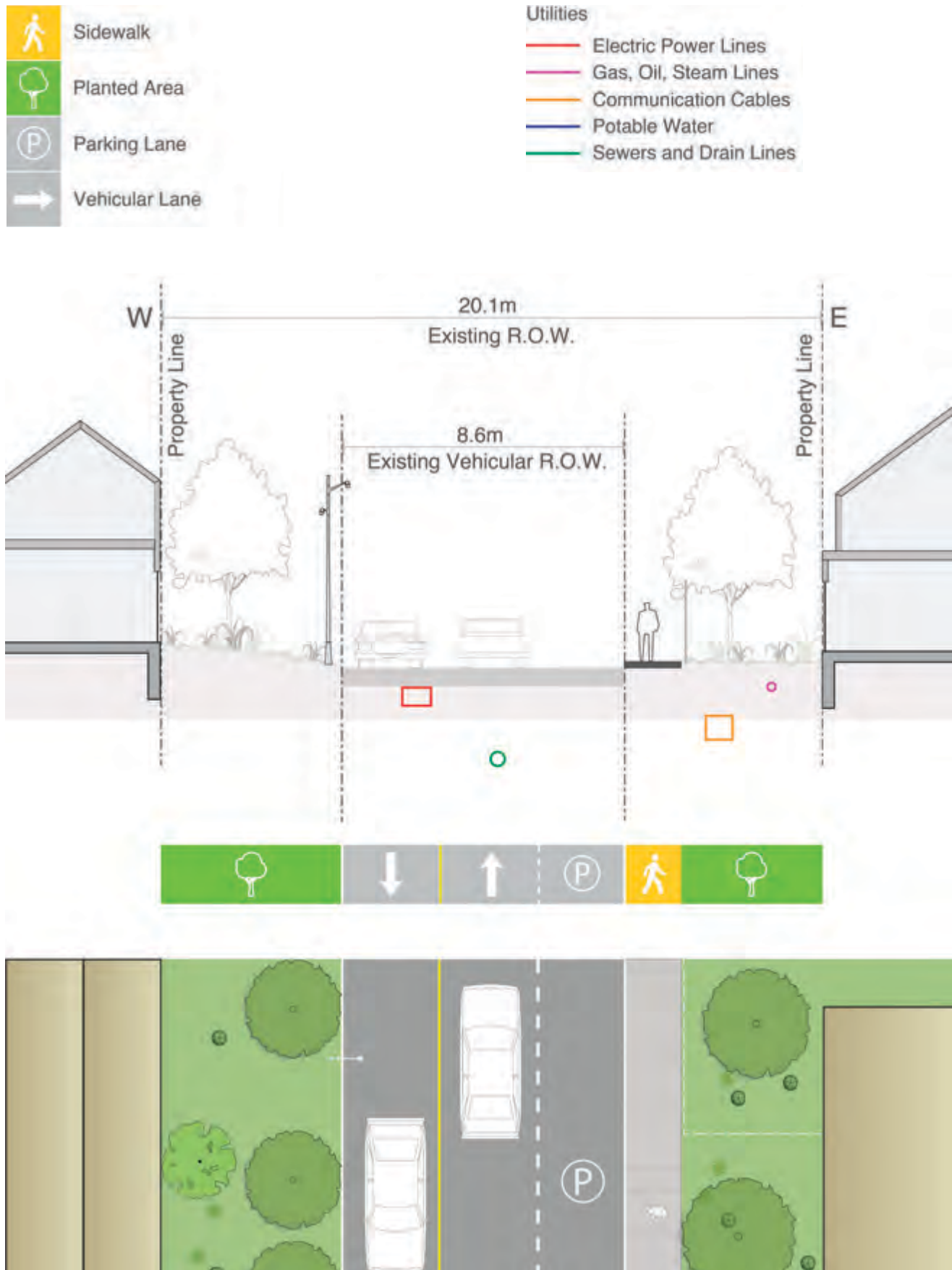


Figure 244 Section and Plan of the Existing Conditions along Redpath Avenue from Manor Road East to Soudan Avenue

Secured Section 37 Contributions

Recent development in the area has provided community benefits through Section 37 contributions to the City. There is one development application under construction in Ward 12, at 174-188 Soudan Avenue and 71-73 Redpath Avenue. With the approval of this application, the City secured \$940,854 towards streetscape and public realm improvements in the Yonge-Eglinton area.

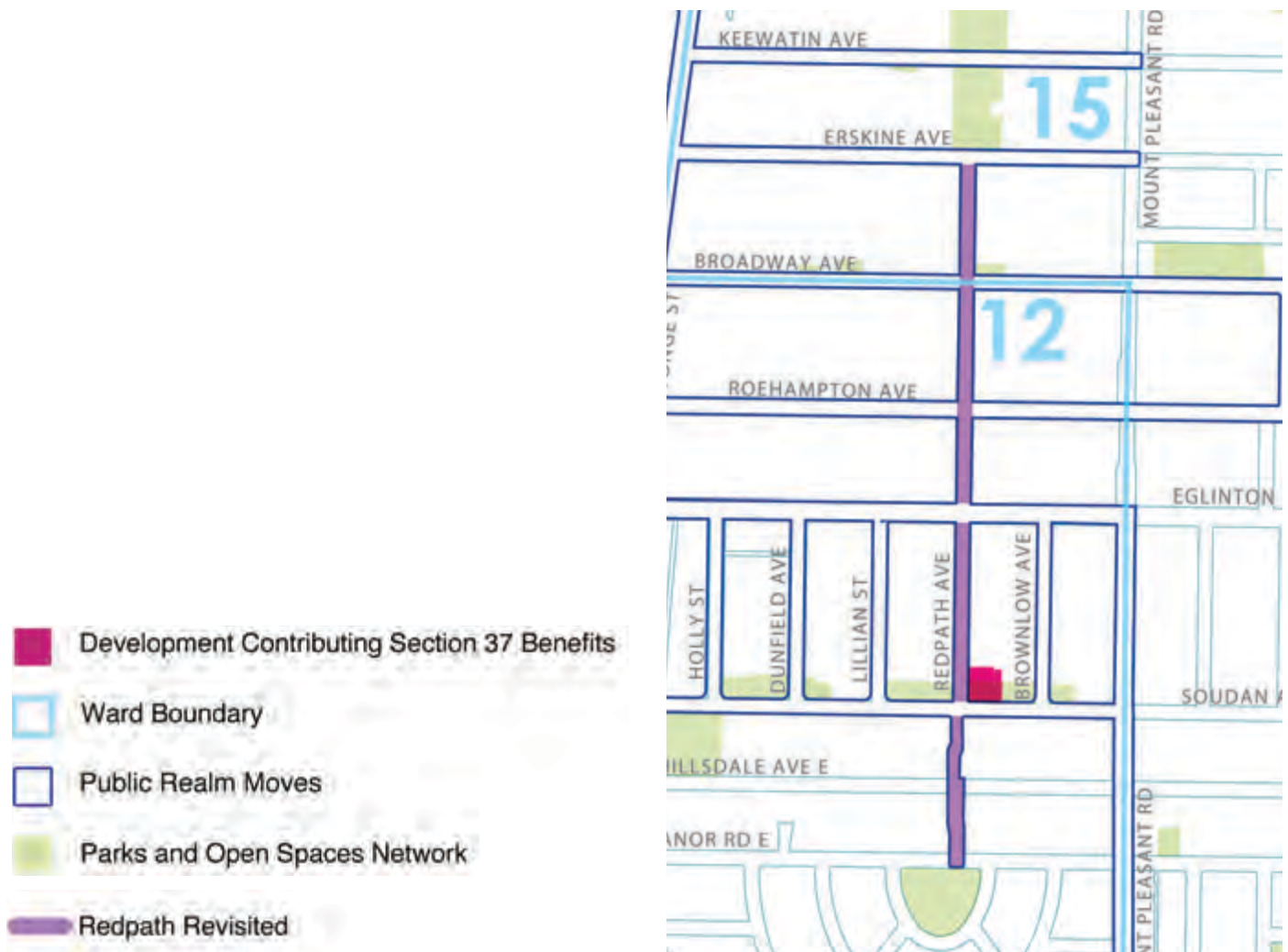


Figure 245 Map of the Section 37 secured contribution locations along Redpath Revisited

Summary of Opportunities and Constraints

Sites with Redevelopment Potential

Redpath Avenue has several approved development applications, which include implementing public realm improvements on their sites as part of the development process. Most of these sites are located north of Eglinton Avenue at intersections with other streets. For some of these sites, landscape setbacks were secured to the previous built form standards prior to the Yonge-Eglinton Secondary Plan. As well, there are development applications that are under review, where staff are working to secure a landscape setback and public realm improvements.

Many of the remaining lots in the area may also experience future development activity that would include public realm improvements on these sites. These lots are generally located on the west side of Redpath Avenue.

While there is no minimum setback requirement that generally applies as part of Redpath Revisited, there are some sites that are constrained with low development potential. There are various limiting reasons for each site, which include heritage designation, location on a planned or future park site, lack of feasibility due to site assembly challenges, or recent redevelopment. All sites between Soudan Avenue and Manor Road East are also constrained as they are designated *Neighbourhoods* in the Official Plan and do not permit large-scale intensification.

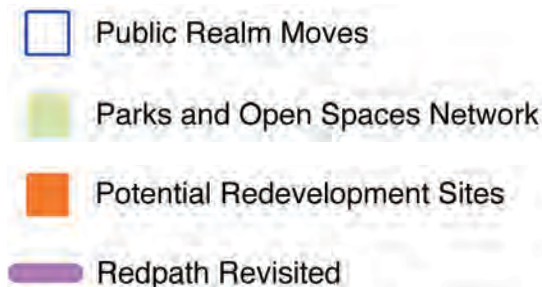


Figure 246 Map of potential redevelopment sites along Redpath Revisited



Figure 247 Diagram of opportunities and constraints at Redpath Avenue

Utility Locations and Key Issues

Similar to the rest of the Midtown Public Realm Moves, above-ground facilities such as hydro poles, fire hydrants and utility vaults are located immediately adjacent to the sidewalks and hinder sidewalk widening.

Where planted bump-outs are recommended, existing catch basins may be in conflict and will need to be relocated or incorporated into the bump-out design.

On the west side of the street, an existing 150mm watermain runs below the sidewalk. Proposed trees should be located to minimize conflict with the existing watermain.

Inconsistent public realm language along Redpath Avenue, plus numerous access driveways, private curbs, fences, railings, steps and planters, create challenges to implement a continuous, cohesive streetscape.

Missing sidewalk, mature trees, hydro poles and grading challenges on the west side of the street between Soudan Avenue and Manor Road E, create constraints for a future standard sidewalk, without narrowing vehicular portion of the ROW.

Opportunities exist in front of Redpath Avenue Parkette and the Church of the Transfiguration to create focal points and gateways through planted bump-outs, consistent planting, custom paving materials and street furniture.



Figure 248 Grading challenges south of Soudan Avenue on Redpath Avenue



Figure 249 Utility challenges along Redpath Avenue



Figure 250 Private fencing along Redpath Avenue



Figure 251 Missing sidewalk on the west side on Redpath Avenue, south of Soudan Avenue

Final Condition Recommendations:

- Maintain 8.6m vehicular ROW
- Provide planted bump-outs at future and existing parks fronting the street
- Create focal points and gateways through planted bump-outs, consistent planting, custom paving materials and street furniture in front of Redpath Avenue Parkette and the Church of the Transfiguration. Include custom furniture and custom paving where applicable for the entire length of Redpath Avenue to create a continuous streetscape palette
- Maintain current building setbacks. If there is no setback in the existing condition, provide building setbacks with the future development, where possible, to accommodate active retail uses and universally-accessible building entrances
- Provide a 5.0m building setback from the property line to the building face on both sides of the street between Eglinton Avenue and Soudan Avenue
- Provide a minimum 1.5m depth of planting soil on top of any underground structures in the setback to support new, healthy tree growth
- Provide above standard width sidewalk on both sides of the street
- Provide generous rain gardens on both sides of the street
- Roehampton Avenue to Eglinton Avenue:
 - Narrow the vehicular ROW from 8.6m to 6.6m
 - Create a shared street with continuous unit paving along the roadway, pedestrian promenade and setback
 - Provide 0.6m unit paver buffer between vehicular and pedestrian ROWs, with bollards where applicable and not in front of planters with trees
 - Provide a half curb between vehicular and pedestrian ROWs
 - Provide Green Boulevard with trees and incorporated seating elements
- Soudan Avenue to Manor Road East:
 - Narrow the vehicular ROW from 8.6m to 6.0m to provide for a new 2.1m sidewalk on the west side
 - Provide above standard width sidewalk on the east side of the street and a new sidewalk on the west side
 - Provide sidewalk transition zones to protect existing mature trees
 - Relocate existing on-street parking to accommodate new side walk on the west side of the street
 - Relocate existing light standards/ hydro poles outside of the new pedestrian clearway (or, if possible, relocate hydro underground)

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

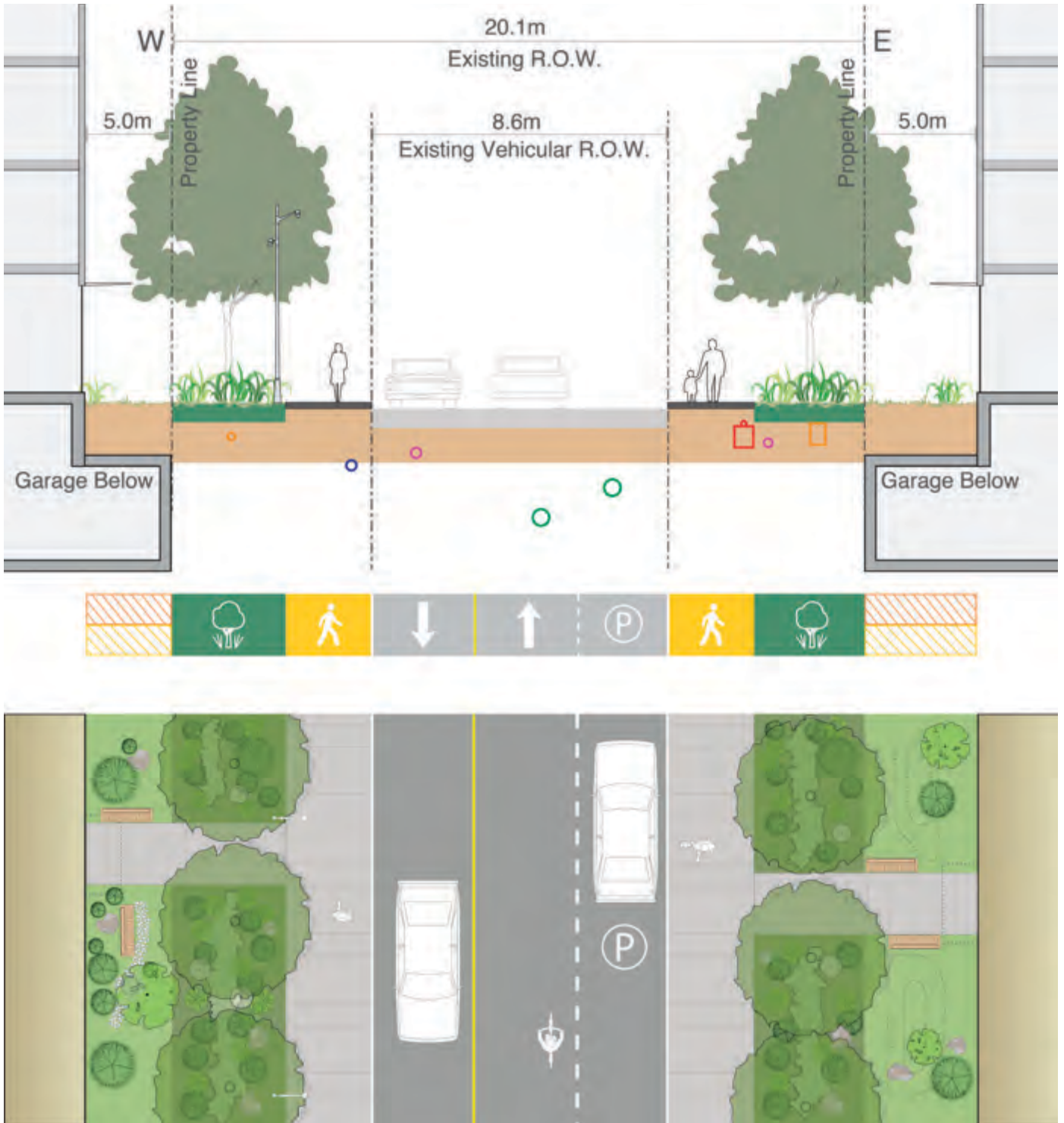
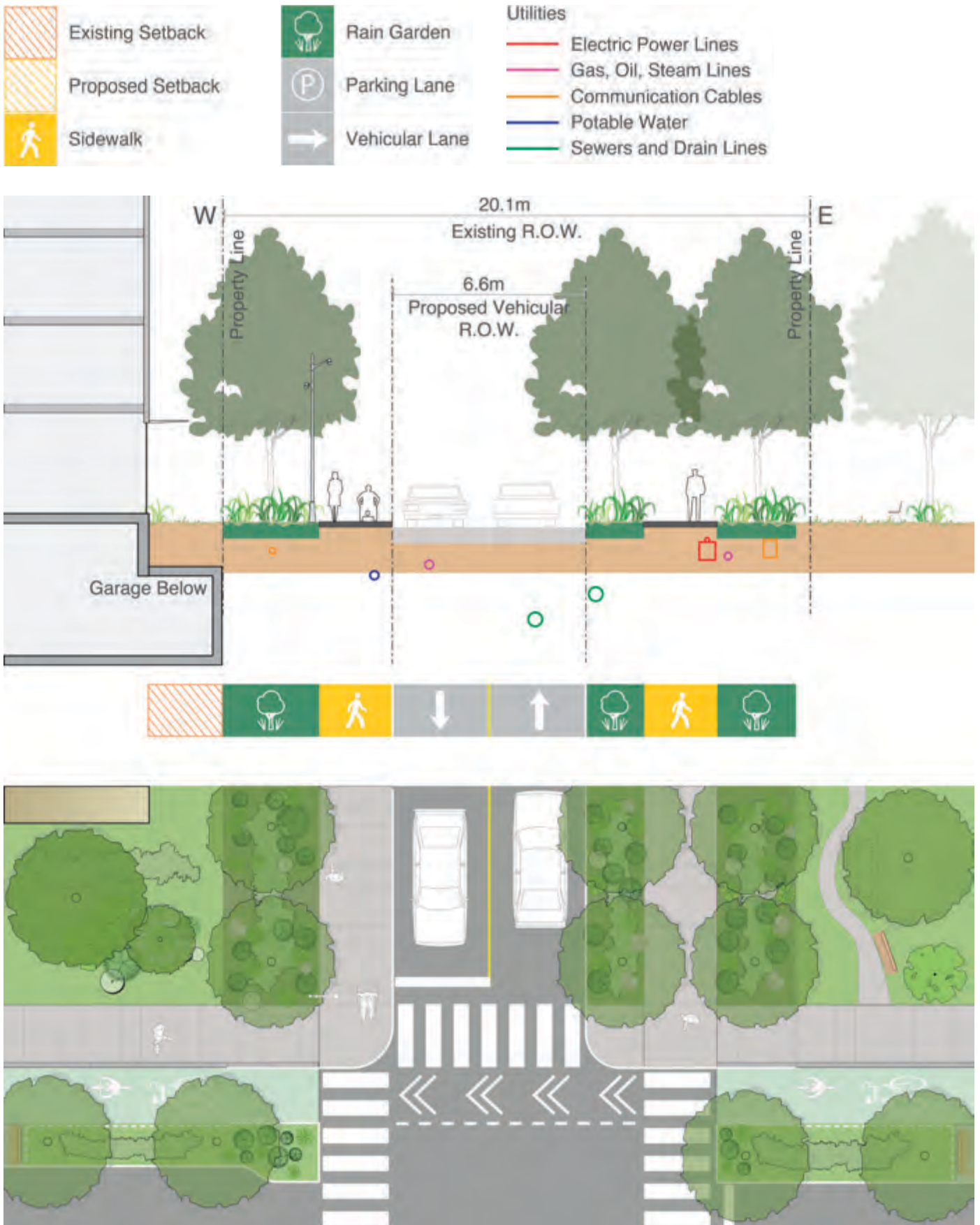


Figure 252 Section and Plan of Redpath Revisited - Recommended Final Condition from Erskine Avenue to Roehampton Avenue and from Eglinton Avenue to Soudan Avenue



SECTION 4.8

Figure 253 Section and Plan of Redpath Revisited - Recommended Final Condition of bump-out at intersection with Broadway Avenue

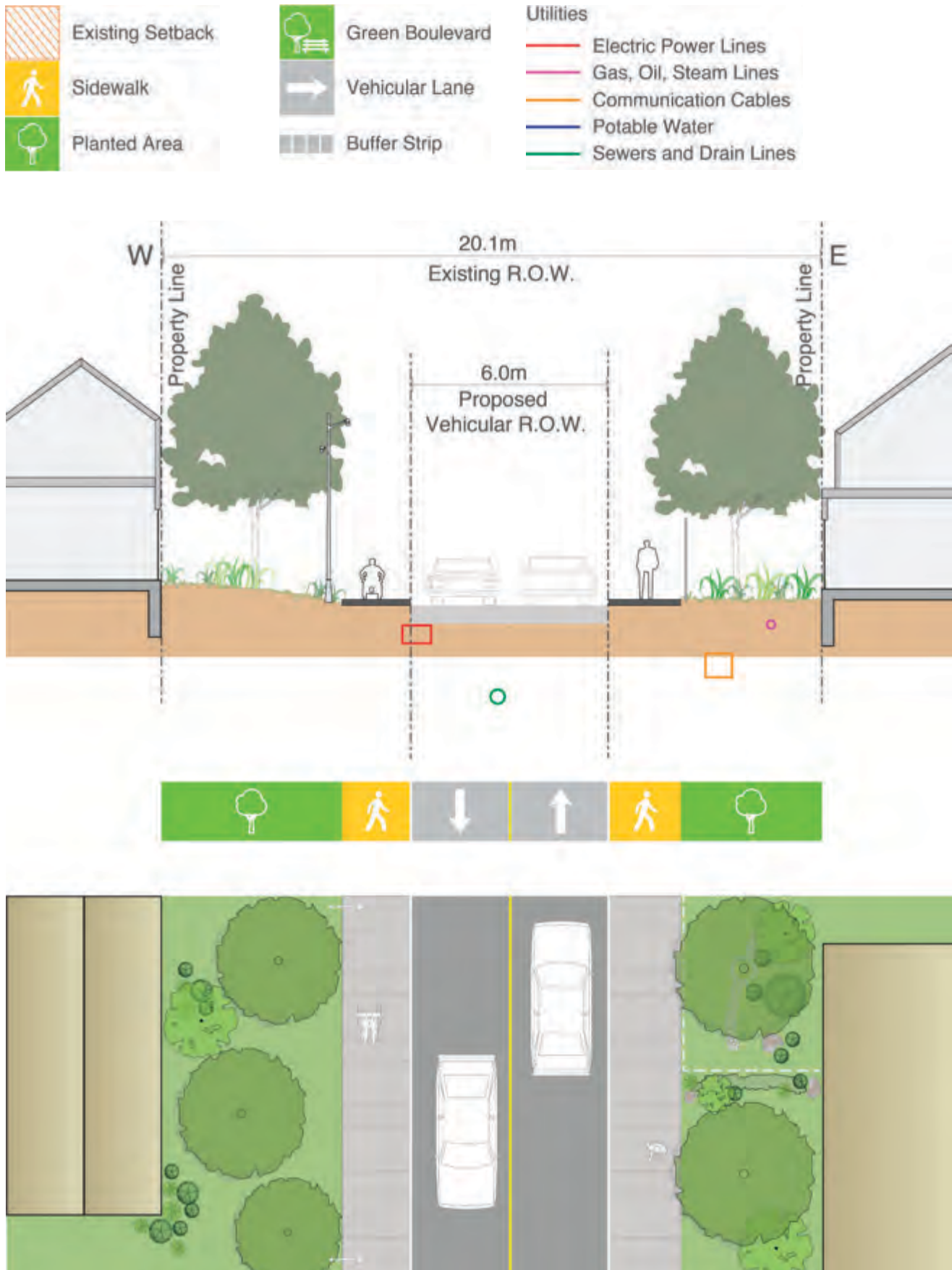
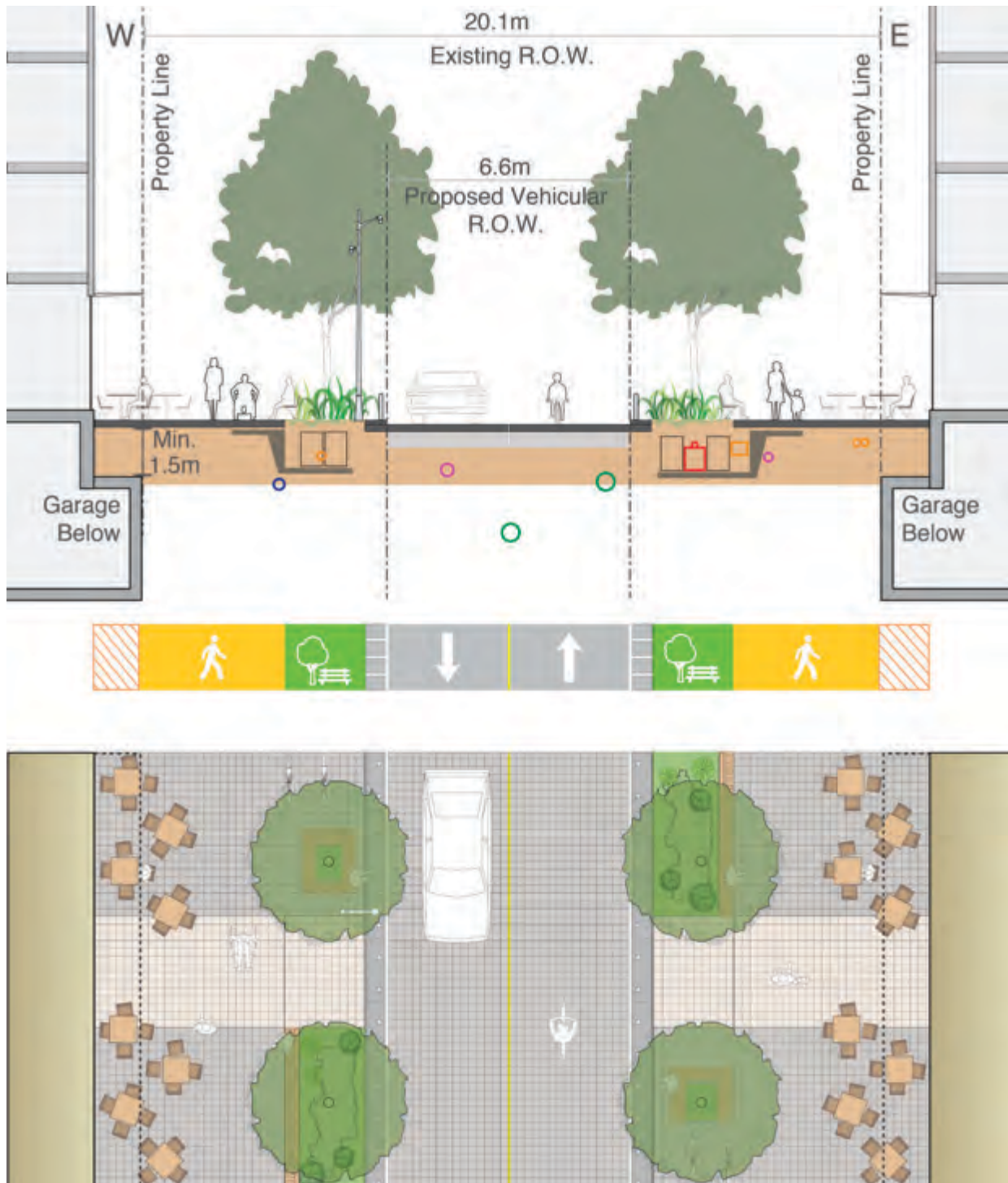


Figure 254 Section and Plan of Redpath Revisited - Recommended Final Condition from Soudan Avenue to Manor Road East

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.



SECTION 4.8

Figure 255 Section and Plan of Redpath Revisited - Recommended Final Condition from Roehampton Avenue to Eglinton Avenue

Implementation

Development-related Improvements

With a few recently completed developments and a number of developments under construction, public realm improvements will happen incrementally. As soft sites start to redevelop, the upgraded public realm will become more continuous.

New development will be responsible for providing a 5.0m building setback along both sides of the street between Eglinton Avenue and Soudan Avenue. Existing landscape setbacks should be maintained with any new redevelopment that exceeds the aforementioned setbacks.

Capital Works and Coordination

Opportunities for public realm improvements may arise through upcoming capital works projects led by Toronto Water and Transportation Services. Taking advantage of other planned works represents an opportunity to coordinate and implement public realm improvements in the near-term and avoids disruption from additional construction work in the future.

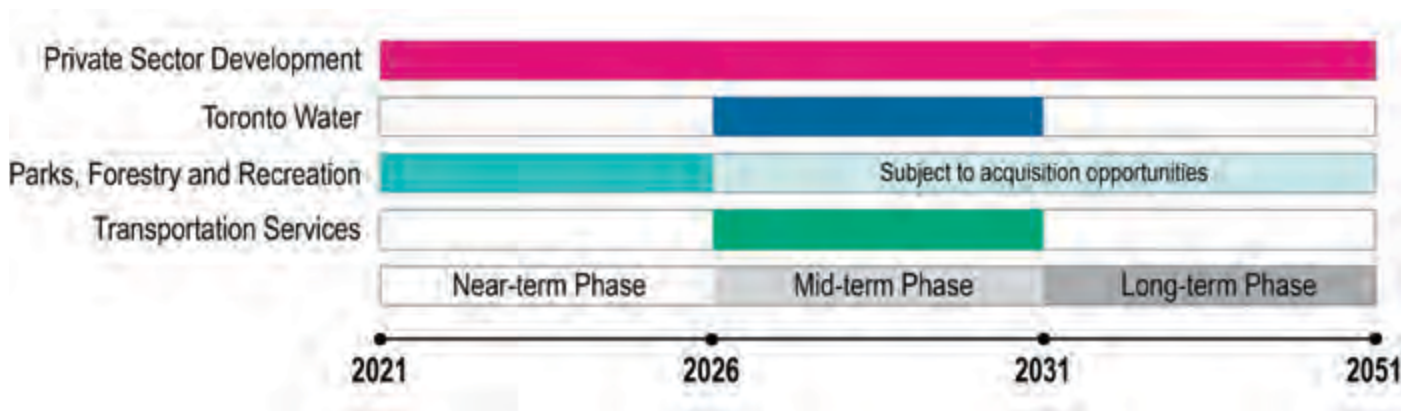


Figure 256 Projected Implementation Timeline for the Redpath Revisited

Note: The implementation timeline represents information available from divisional partners and data from T.O. Inview as of December 31st, 2021 and is subject to change.

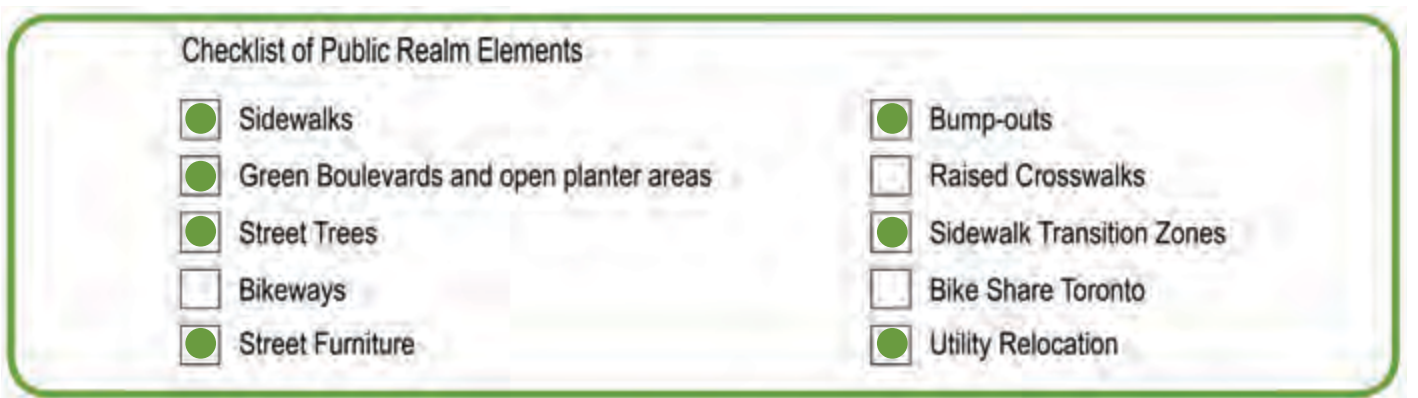


Figure 257 Checklist of Public Realm Elements to be implemented for the Redpath Revisited

4.9

Yonge Street Squares

Overview

Background Analysis

Final Condition Recommendations

Implementation

Overview

Yonge Street is the main north-south commercial and civic street in Midtown. Midtown Yonge Street's built form character is uniquely balanced with large scale commercial outlets clustered at the Yonge-Eglinton intersection and quaint, main street retail shops to the north and south, which are supported and enhanced by strategic public realm improvements.

Policy Direction

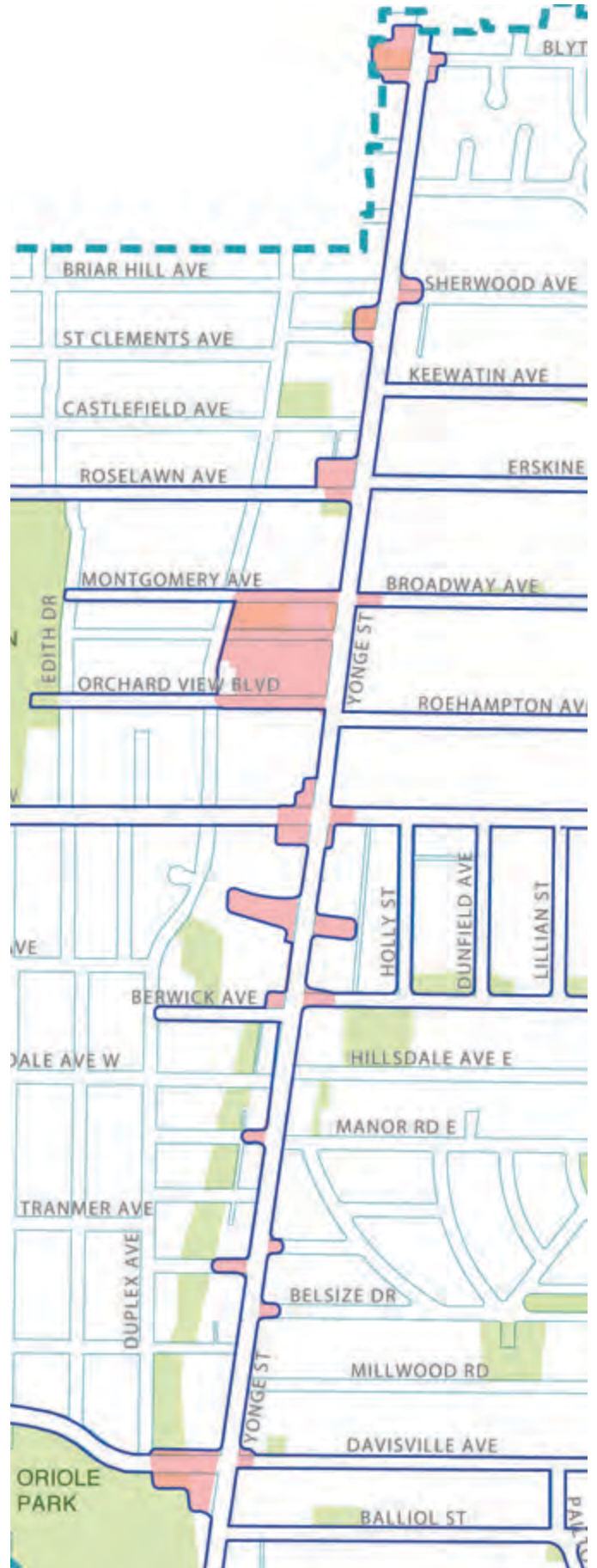
The Yonge-Eglinton Secondary Plan includes the following policies related to the Yonge Street Squares Public Realm Move:

3.2.3. Yonge Street Squares

- a. Yonge Street bisects the Secondary Plan area. Along Yonge Street, the pedestrian environment will be enhanced and a series of distinctive landscaped, publicly-accessible squares will be secured.
- b. Development adjacent to Yonge Street will accommodate and provide wide sidewalks and pedestrian clearways. All buildings will be reasonably set back at grade from the property line adjacent to Yonge Street. The setback requirement will be increased in order to realize the Yonge Street Squares. In areas of Yonge Street where the prevailing pattern of buildings consists of narrow frontage storefronts with little or no setbacks, the setback should ensure a continuous streetwall without amendment to this Plan.
- c. The landscaped squares, conceptually shown on Map 21-6, will be secured as part of development and City-initiated civic improvements.
- d. The squares, together with the Yonge Street streetscape, will be designed to be built incrementally and will have consistency in lighting, paving and street furniture to reinforce the interconnected character of Yonge Street and its series of squares.
- e. Streetscapes, parks and open spaces in the blocks between Montgomery Avenue, Yonge Street, Orchard View Boulevard and Duplex Avenue, known as Montgomery Square, will be developed to create a coordinated and cohesively designed precinct of public and publicly-accessible spaces and community facilities.
- f. At the Yonge and Eglinton Crossroads, a prominent site with city-wide significance, a large-scale publicly-accessible square will be created by providing squares on each of the four corners of the intersection and the connecting streetscapes on Yonge Street and Eglinton Avenue. The design of these open spaces will be coordinated in their design, configuration and layout to complement each other and together create a significant civic focal point. Each square will provide space for gathering and direct and comfortable access to transit. The southwest corner of Yonge Street and Eglinton Avenue will accommodate the largest of the four squares and will provide space for both community gathering and comfortable pedestrian movement, related to its function as the main entrance to the Yonge-Eglinton transit station.
- g. Three sets of squares will be created by realigning offset east-west streets that connect to Yonge Street, as indicated on Map 21-9. Realignment of the streets will be pursued as opportunities arise, including through the development process, to improve pedestrian movement and enable the creation of consolidated, cohesive open spaces and squares.
- h. A square will be created mid-block between Eglinton Avenue and Berwick Avenue on the west side of Yonge Street to provide an attractive publicly-accessible open space and a mid-block pedestrian connection.

- Yonge Street Squares
- Public Realm Moves
- Parks and Open Spaces Network
- Yonge-Eglinton Secondary Plan Area

Figure 258 Map showing the location of the Yonge Street Squares Public Realm Move



Background Analysis

The Yonge-Eglinton Secondary Plan identifies 16 squares along Yonge Street, associated with specific intersections. The Yonge-Eglinton Crossroads and Davisville Square span over the entire intersection capturing all four sides. Montgomery Square stretches between Montgomery Avenue, Yonge Street, Orchard View Boulevard and Duplex Avenue, including Helendale Avenue, Montgomery Square Parkette and the open spaces surrounding the Northern District Library.

The remaining squares are smaller in scale and generally are located at one corner of the intersection. The two exceptions are Glebe Road West and Manor Road West, with no through traffic.

In September 2016, the Midtown Yonge Business Improvement Area (BIA) prepared the *Midtown Meet in the Middle Streetscape Master Plan*. The plan outlines the goals, priorities and implementation strategy for the segment of Yonge Street between Soudan Avenue and Kay Gardner Beltline Park. Some of the ideas from the Streetscape Master plan are included in the Final Condition Recommendation section below.



Figure 259 Cover of Meet in the Middle Streetscape Master Plan



Figure 260 Streetscape on Belsize Drive at Yonge Street



Figure 261 POPS on Yonge Street



Figure 265 Yonge Street Square at Sherwood Avenue



Figure 262 Yonge and Davisville intersection



Figure 266 Yonge Street Square at Blythwood Road and Yonge Street



Figure 263 Midtown Yonge BIA street furniture



Figure 267 Yonge Street Square at Roselawn Avenue and Yonge Street



Figure 264 Manor Road West



Figure 268 Glebe Road West

Secured Section 37 Contributions

Recent development in the area has provided community benefits through Section 37 contributions to the City. There are four development sites along Yonge Street with secured Section 37 contributions and that include public realm improvements as a community benefit. With the approval of these applications, the City secured \$4,719,740, adjusted for interest to \$4,733,168 as of December 31st, 2021, towards streetscape and public realm improvements on Helendale Avenue and in the broader area. Of this amount, \$4.65 million is available from sites where the Yonge Street Squares are the only Public Realm Move that is adjacent or is closest to the site.

The four development sites are located in Wards 8 and 12. Section 37 funding would contribute \$1.17 million from Ward 8 and \$3.56 million from Ward 12 if the potential funding from these sites is entirely dedicated to the Yonge Street Squares.

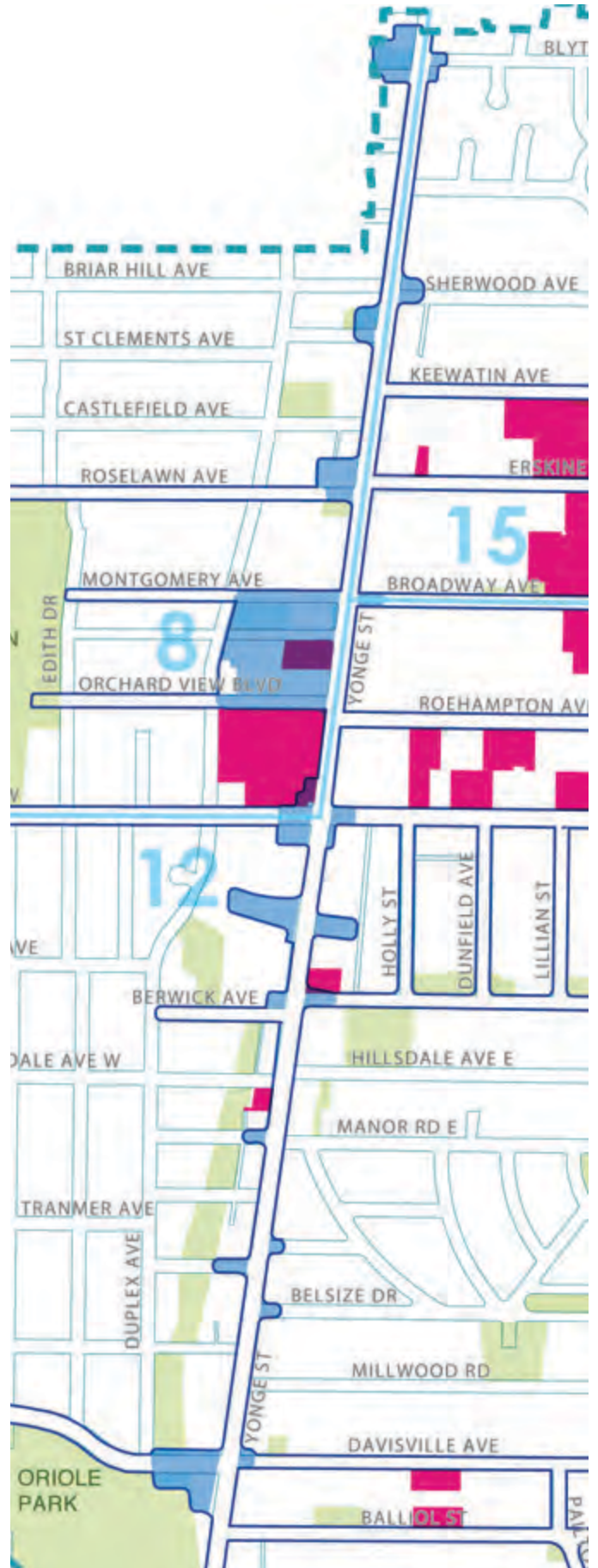
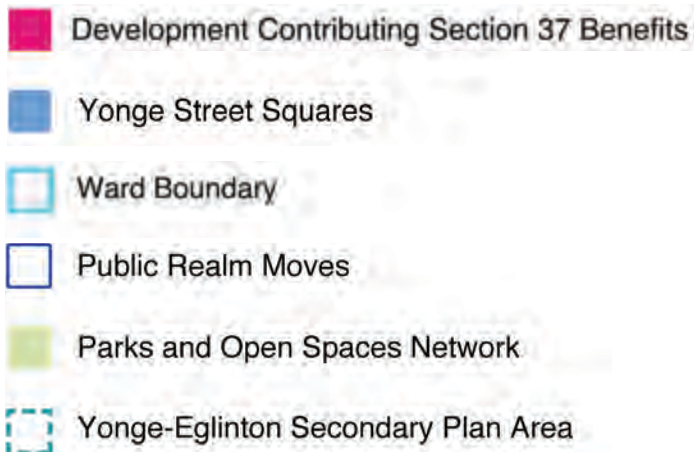


Figure 269 Map of the Section 37 secured contribution locations along the Yonge Street Squares

Summary of the Opportunities and Constraints

Sites with Redevelopment Potential

Due to the high amount of development activity along the Yonge Street corridor, there are several proposed applications adjacent to squares where staff are working to secure a landscape setback and public realm improvements. These are generally located between Eglinton Avenue and Davisville Avenue. A small number of recently redeveloped sites have also already completed the vision for parts of the Yonge Street Squares by implementing public realm improvements as part of their work.



-  Potential Redevelopment Sites
-  Yonge Street Squares
-  Public Realm Moves
-  Parks and Open Spaces Network
-  Yonge-Eglinton Secondary Plan Area

Figure 270 Map of potential redevelopment sites along the Yonge Street Squares

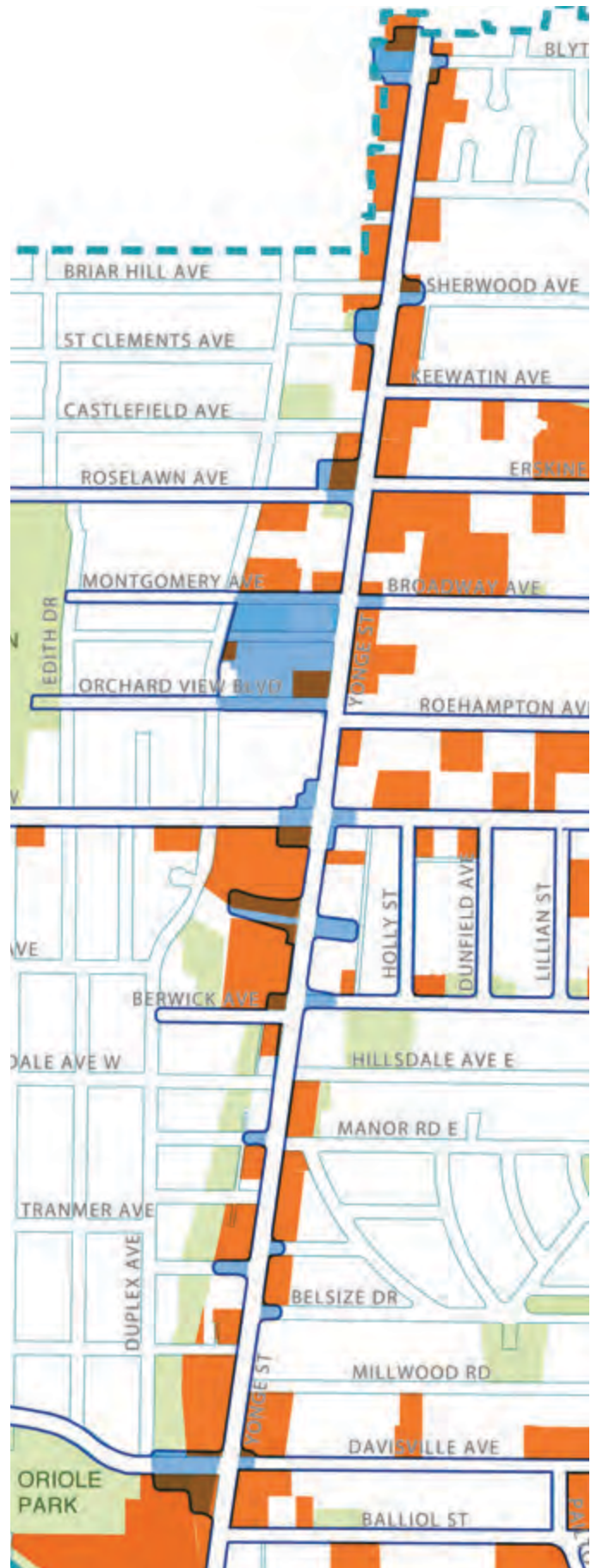




Figure 271 Diagram of opportunities and constraints at the intersection of Yonge Street and Blythwood Road and Lytton Boulevard

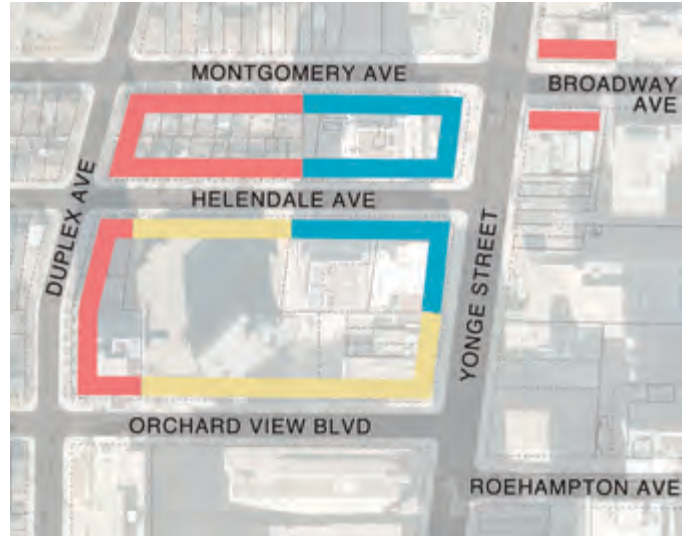


Figure 272 Diagram of opportunities and constraints along Yonge Street and Duplex Avenue, between Montgomery Avenue/ Broadway Avenue and Orchard View Boulevard



Figure 273 Diagram of opportunities and constraints along Yonge Street between Briar Hill Avenue and Roselawn Avenue



Figure 274 Diagram of opportunities and constraints along Yonge Street between Eglinton Avenue and Berwick Avenue



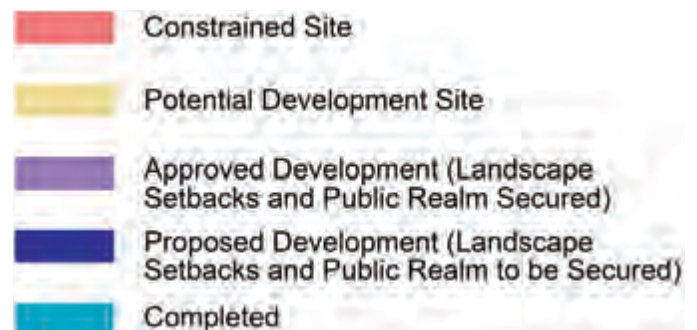
Figure 275 Diagram of opportunities and constraints at the intersections of Yonge Street and Manor Road, Glebe Road, and Belsize Drive

There are constrained sites around some of the squares, which have limited development potential. This are due to various reasons such as heritage designation, location on a future park site, lack of feasibility due to site assembly challenges, or recent redevelopment. While the southwest corner of Yonge Street and Chaplin Crescent/ Davisville Avenue is constrained due to heritage considerations, there is an existing plaza which will be maintained with the possibility of a redesign in the future.

There are some lots throughout the area, which may experience future development activity that would include public realm improvements on these sites.



Figure 276 Diagram of opportunities and constraints at the intersection of Yonge Street and Davisville Avenue



Utility Locations and Key Issues

Utilities are highly prominent in the squares due to their locations. Above-ground facilities such as hydro poles, fire hydrants and utility vaults are often located close to the intersections.

For Typical Squares (see p.198) where planted bump-outs are recommended, and for Shared Squares (see p.201) where roadway narrowing is recommended, existing catch basins may be in conflict and will need to be relocated.

In some squares, existing boulevard parking areas create an unsafe pedestrian condition and limit public realm experience.

At the intersection with Lytton Boulevard, the existing TTC structure limits opportunities for expansion, but allows for improvements of the retaining planter wall and a more prominent entrance to the Snider Parkette from Yonge Street.

The Yonge-Eglinton Secondary Plan identifies five intersections along Yonge Street for street realignment. Four of those intersections are associated with the Yonge Street Squares Public Realm Move. Implementation of the squares, including realignments, will occur through the development process as opportunities arise.

A heritage property at the south-east corner of Sherwood Avenue creates challenges for square expansion, however an opportunity exists to create a consistent public realm that can be updated in coordination with future north-east corner square. Additional challenges exist at other intersections with heritage properties.

In some squares, existing watermain below the sidewalk may create conflicts with tree planting. Proposed trees should be located to minimize conflict with the existing watermain.

There is inconsistent public realm language within existing Yonge Street Squares, including various types of bollards, planters and planting areas, seating, lighting, unit pavers and bike parking elements.



Figure 277 Boulevard Parking at Roselawn and Yonge Street



Figure 278 Utility challenges at Blythwood Road










Figure 279 Bike Share Toronto station located in the roadway on Glebe Road West



Figure 280 Heritage Building at Sherwood Avenue and Yonge Street

Final Condition Recommendations

Due to their location and programming, the squares are divided into three types: Special, Typical and Shared.

-  Special Squares
-  Typical Squares
-  Shared Squares
-  Yonge Street Squares
-  Public Realm Moves
-  Parks and Open Spaces Network
-  Yonge-Eglinton Secondary Plan Area

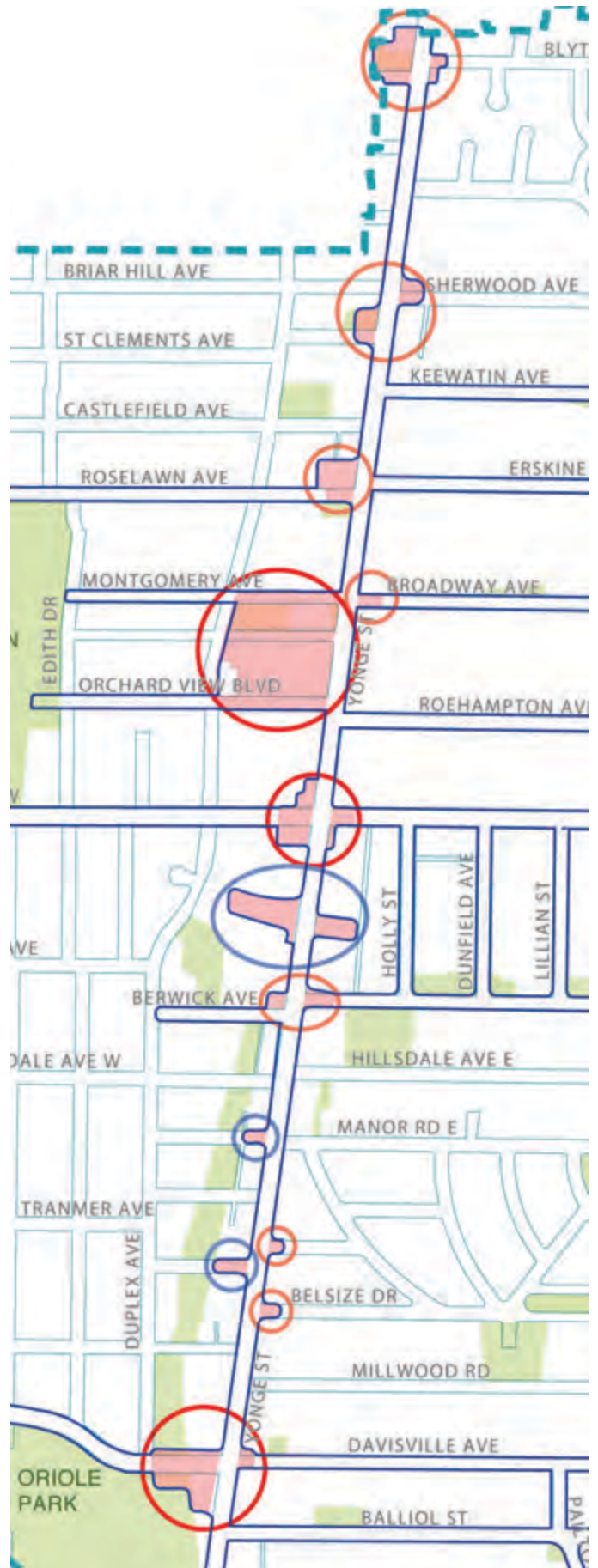


Figure 281 Map showing the Special, Typical and Shared Yonge Street Squares

Special Squares

Yonge-Eglinton Crossroads, Montgomery Square and Davisville Square are three Special Squares along Yonge Street. Special Squares will be designed as landmark destinations with high quality materials, including paving, seating, lighting and various landscape and streetscape elements. Public art should be incorporated into all Special Squares.

Montgomery Square

Montgomery Square is one of the largest Special Squares in Midtown. It is bound by Montgomery Avenue on the north, Yonge Street on the east, Orchard View Boulevard to the south and Duplex Avenue to the west. Helendale Avenue transects east-west through the square. Additional future parkland is also proposed on the west side of the block.

Recent constructed developments include 58 Orchard View Boulevard with townhouses at grade, 25 Montgomery Avenue with townhouses at grade, and 2384 Yonge Street with retail at grade. Existing on site are Stanley Knowles Co-op and Northern District Library.

- Building upon the existing Montgomery Square Park, provide high quality materials for the entire block. The same unit paving type, color and pattern should be used throughout, including mid-block connections, sidewalks, POPS and any pedestrian connections through the future park
- Provide planted bump-outs at future and existing parks fronting the street
- Design Helendale Avenue as a shared street between Duplex Avenue and Yonge Street with a low curb and a continuous unit paving to signal a shared and safe experience between cars, cyclists and pedestrians
- Continue to incorporate paving inlays to emphasize overall character and continuity throughout the site
- Provide a pedestrian crosswalk on Helendale Avenue to connect mid-block connections
- Relocate existing light standards/ hydro poles outside of the new pedestrian clearway (or, if possible, relocate hydro underground)



Figure 282 Streetscape along Montgomery Avenue



Figure 283 Montgomery Square Streetscape



Figure 284 Paving inlays at Montgomery Square

Yonge Eglinton Crossroads

A large-scale publicly-accessible square will be created by providing squares on each of the four corners of the intersection and the connecting streetscapes on Yonge Street and Eglinton Avenue. Each square will provide space for gathering, and direct and comfortable access to transit, while creating a significant civic focal point. The southwest corner of Yonge Street and Eglinton Avenue will accommodate the largest of the four squares and will provide space for both community gathering and comfortable, high volume pedestrian movement, related to its function as the main entrance to the Yonge-Eglinton transit station.

Davisville Square

The southwest corner of Yonge Street and Davisville Avenue will provide space for community related activities and adequate pedestrian circulation, acting as the main entrance to the Yonge-Davisville transit station with adequate bike parking facilities. Additional smaller squares are proposed at the northwest and northeast corners to provide space for seating, gathering and to accommodate flexible activities. The combination of the squares will create a welcoming threshold that connects Yonge Street to the Davisville Community Street, which in turn links with local parks and schools.

On the west side of Yonge Street, the square will be designed as gateway to an emerging park and open space network that includes the Kay Gardner Beltline Trail and a proposed signature park located over the Davisville Yard.



Figure 285 Yonge Street Square north-west corner at Yonge Street and Eglinton Avenue

Special Squares Materials Palette

Paving – Building on recently constructed paving palettes, design future squares with the plank type paver, running bond pattern, light or medium gray colour, with SRI value >29, to meet the *Toronto Green Standard*. Paving will continue to remain simple in colour and pattern with emphasis being put on seating elements and tree planting.

Public Art – Provide highly visible public art in all Special Squares to shape the identity and character of Midtown.

Seating – Provide a variety of seating elements to create focal points and allow for a unique character associated with the commercial use. Traditional type benches are discouraged. Locate seating walls and/or elements to face each other and create clusters for social interactions. Seating can be incorporated with planting areas. Free standing furniture is encouraged if space allows, but not to substitute for permanent seating.

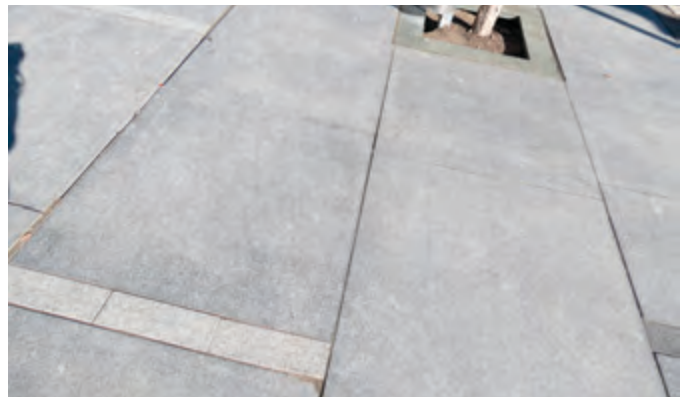


Figure 287 Yonge-Eglinton Crossroads north-east corner unit paving



Figure 288 Front Street Promenade



Figure 289 Montgomery Square Park seating elements



Figure 286 Liberty Park, Manhattan, New York



Figure 290 Yonge-Eglinton Crossroads north-east corner seating elements

Landscape Elements – Provide a tree grate for trees in paving. Raised planters are discouraged unless the tree is planted in a larger planting bed where proper soil volume can be met following the *Toronto Green Standard*. All tree planting must comply with the *Toronto Green Standard* and allow between 0.8m-1.6m of soil below grade to provide adequate soil volume. Raised planters should be designed with integrated seating. Incorporate standard City of Toronto information pillar and garbage receptacles, where possible. Locate bike parking rings away from the pedestrian circulation. Provide additional bike parking in squares associated with transit stations.



Figure 291 Tree Grate at Montgomery Square



Figure 292 Planters with seating along Bloor Street

Lighting – Provide pedestrian scale lighting to support safe pedestrian circulation in the evenings and at night. Provide column type lights to continue with the existing character of recently constructed squares.



Figure 293 Montgomery Square Park light column (Left)



Figure 294 Yonge-Eglinton Crossroads north-east corner light column (Right)

Typical Squares

Typical Squares are smaller scaled squares along Yonge Street, complementing the program and design of the Special Squares. Typical Squares include: Blythwood Road Square, Lytton Boulevard Square, Sherwood Avenue Square, St Clements Avenue Square, Roselawn Avenue Square, the west block between Yonge Street and Berwick Avenue, the east block between Yonge Street and Berwick Avenue, Berwick Avenue Square, Soudan Avenue Square and Belsize Road Square.

Typical Squares will be designed either as gathering places with active frontages that encourage grade-related retail uses to spill into these spaces or more intimate, green spaces for people to enjoy.

- Reduce vehicular ROW from 8.6m to 6.6m at intersections
- Provide a planted bump-out with street trees and integrated seating. South of Soudan Avenue, the design of planters should be consistent with Midtown Yonge BIA's street furniture
- Provide plank type pavers, running bond pattern, light or medium gray colour to match the existing streetscape palette of Special Squares
- Provide a minimum 3.5m pedestrian clearway between building edge and planting areas
- Provide an additional 1.5m setback, combined with a 5.0m pedestrian clearway between building face and planting areas towards intersections, excluding heritage buildings. Canopies can be extended into the 1.5m setback, while maintaining an unobstructed 3.5m clearway
- Remove all boulevard parking to create green and active squares
- Provide accessibility features such as tactile walking surface indicators and colour contrasts as squares meet Yonge Street sidewalk
- Relocate existing light standards/ hydro poles outside of the new pedestrian clearway (or, if possible, relocate hydro underground)
- For Yonge Street Squares on streets with setbacks, such as Berwick Avenue and Roselawn Avenue, design the setback as a part of the square. Provide a minimum 1.5m depth of planting soil on top of any underground structures in the setback to support new, healthy tree growth.

- For Yonge Street Squares with existing Bike Share Toronto stations such as Blythwood Road and Glebe Road West, incorporate stations into the square design while providing green spaces and seating opportunities.



Figure 295 Dundas Street West corner square



Figure 296 College Street corner square



Figure 297 Bloor Street and Major Street corner square

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

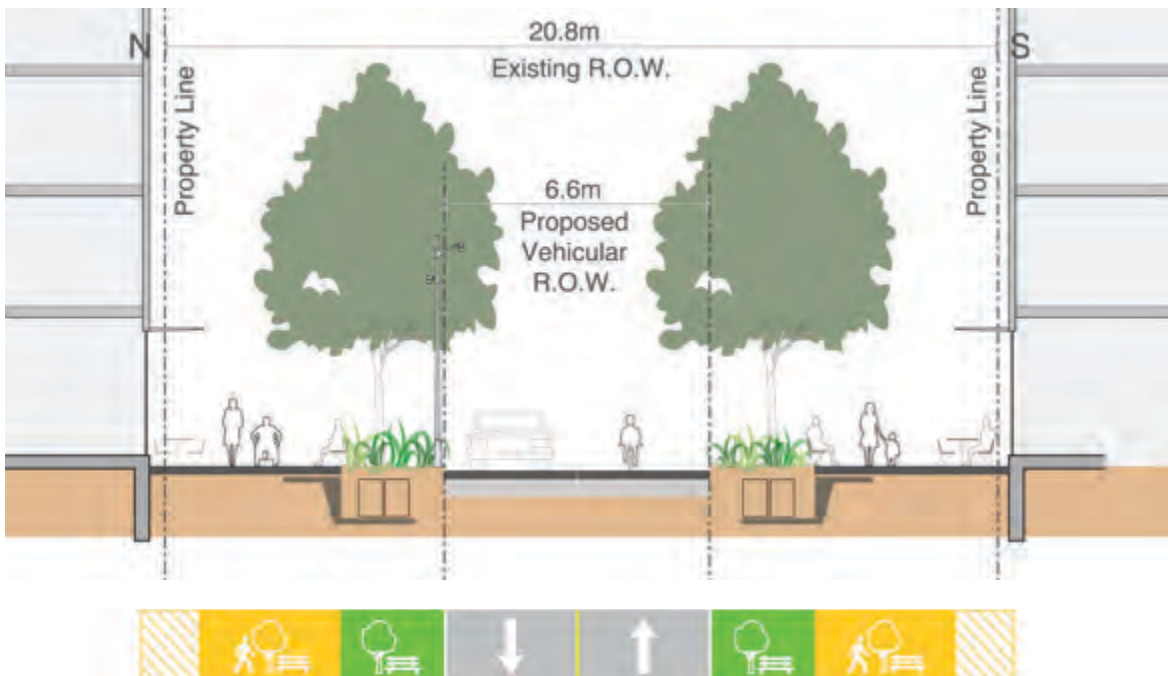


Figure 298 Section of Yonge Street Squares - Recommended Final Condition for Typical Squares



Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

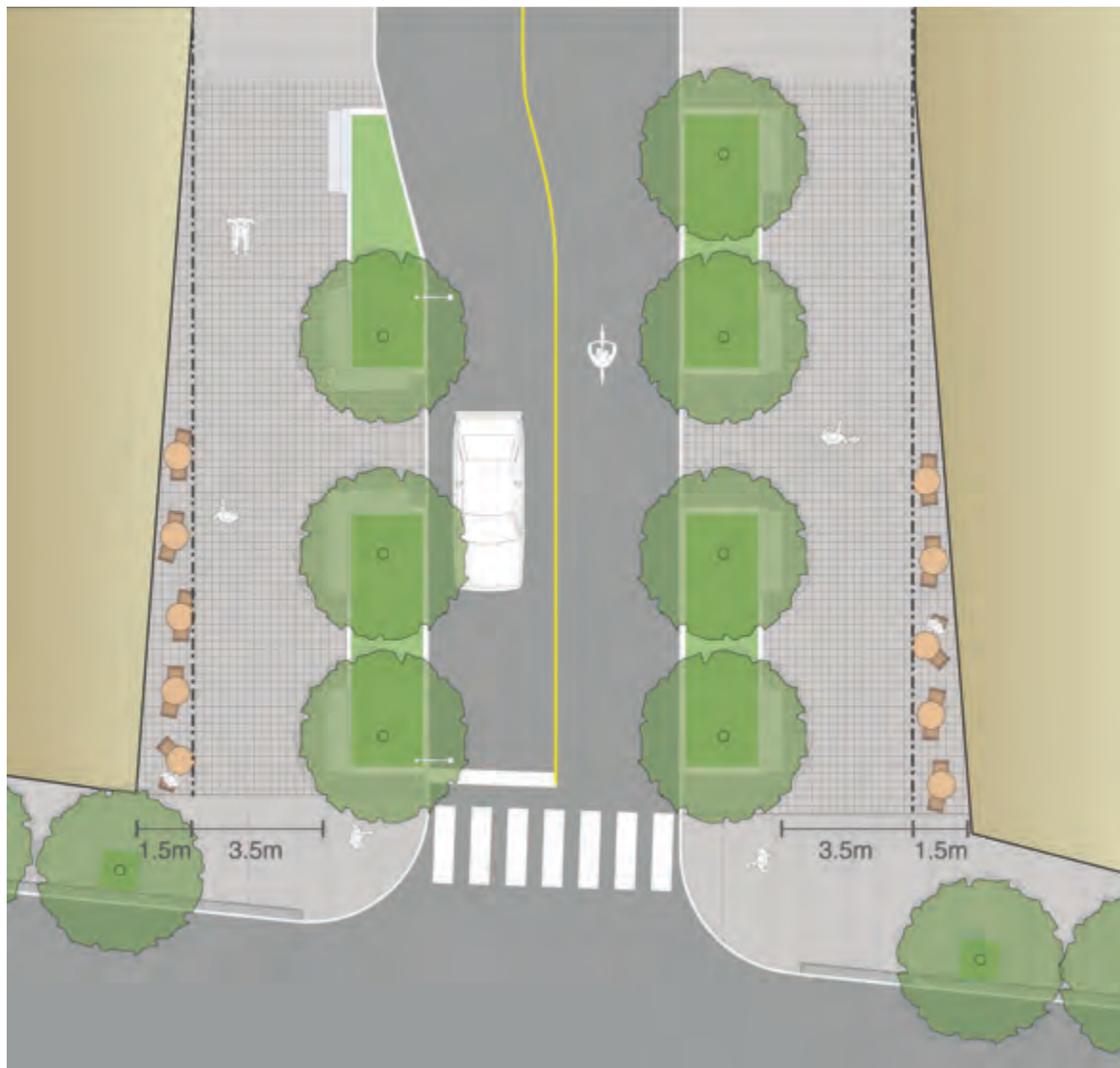


Figure 299 Plan of Yonge Street Squares - Recommended Final Condition for Typical Squares

Shared Squares

Both Manor Road West and Glebe Road West Squares are abutting laneways and a future new TTC trench park. They are to be designed as a Shared Street, prioritizing pedestrian movements, while allowing for vehicular access to the parking garages and laneways.

- Reduce vehicular ROW from 8.6m to 6.6m
- Provide a minimum 3.5m pedestrian clearway between building edge and planting areas
- Provide an additional 1.5m building setback, combined with a 5.0m pedestrian clearway between building face and planting areas towards intersections, excluding heritage buildings. Canopies can be extended into the 1.5m setback, while maintaining an unobstructed 3.5m clearway
- Provide unit pavers for the entire square including the vehicular portion
- Provide a wide paving strip delineating the pedestrian path of travel, separate from the vehicular roadway
- Locate low street planters at the edges of pedestrian paths of travel to ensure safety and separate from the vehicular roadway. If planters are not possible due to utility conflicts or existing mature trees, provide stainless steel bollards to discourage parking on the sidewalk
- Provide large planters with street trees and integrated seating. South of Soudan Avenue, design of planters should be consistent with the Midtown Yonge BIA's street furniture
- Remove all boulevard parking and on-street parking on Manor Road West and Glebe Road West
- Integrate Bike Share Toronto into the Glebe Road West square to minimize conflict with vehicular access
- Provide accessibility features such as tactile walking surface indicators and colour contrasts where squares meet the Yonge Street sidewalk
- Relocate existing light standards/ hydro poles outside of the new pedestrian clearway (or, if possible, relocate hydro underground)



Figure 300 Bellair Street



Figure 301 Scott Street



Figure 302 Market Street

Note: These conceptual sketches are intended to guide future design, and are subject to feasibility analysis, public consultation, detailed evaluation, and Council approval, using standards and best practices current at the time of implementation.

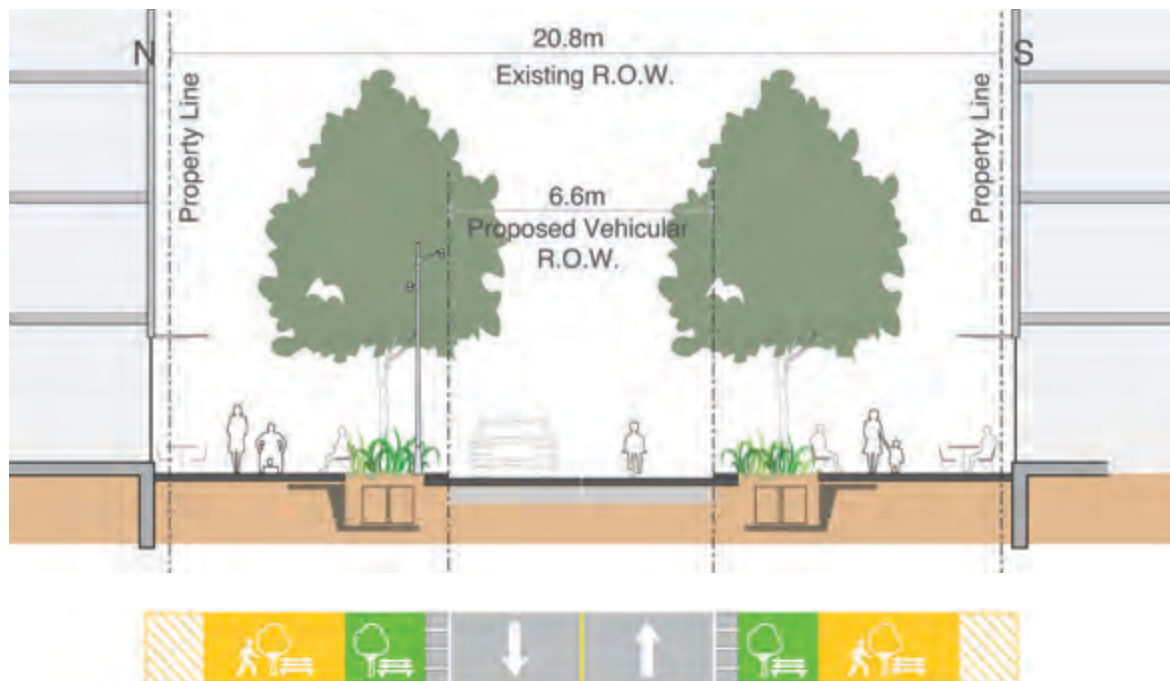
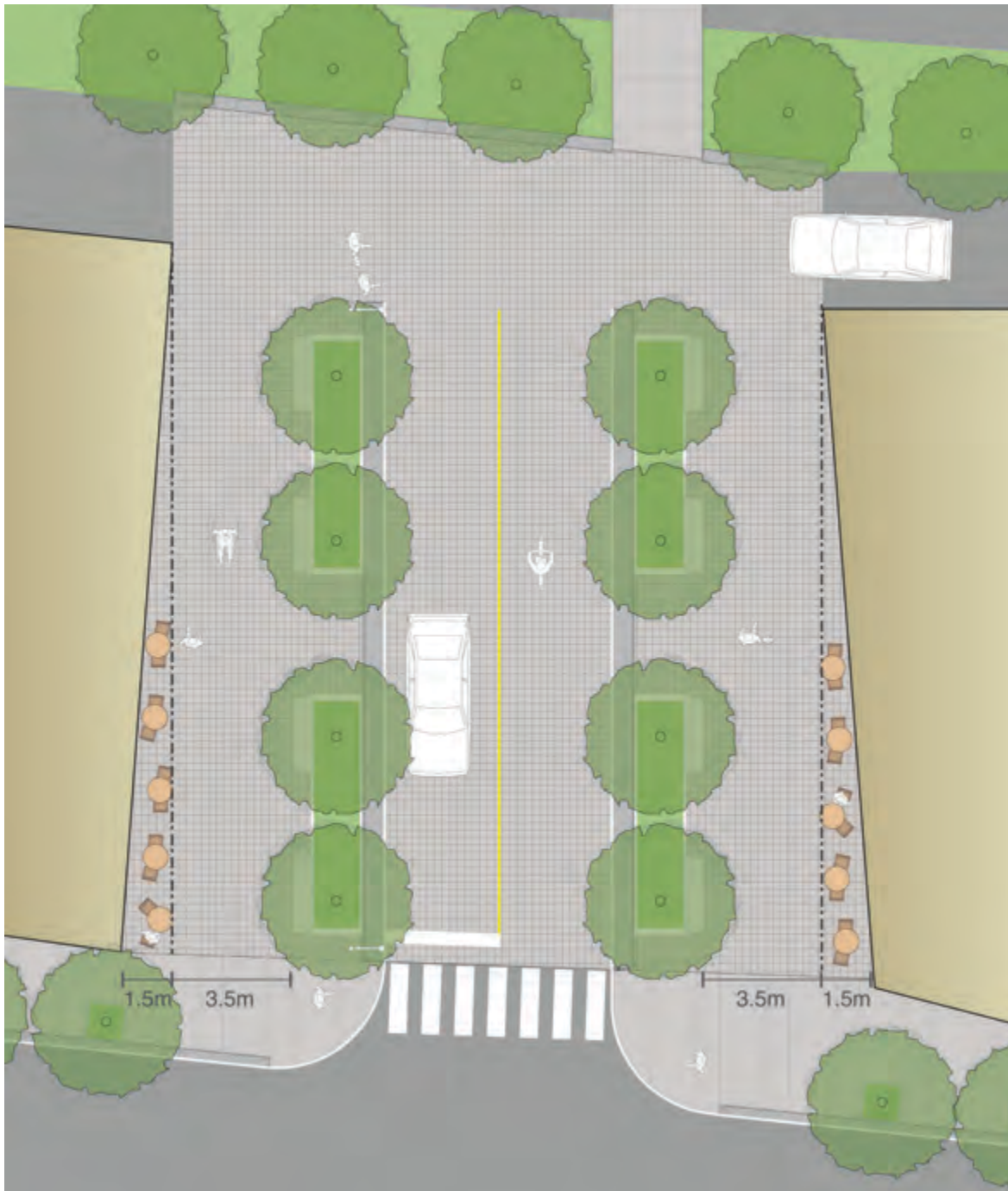


Figure 303 Section of Yonge Street Squares - Recommended Final Condition for Shared Squares



SECTION 4.9

Figure 304 Plan of Yonge Street Squares - Recommended Final Condition for Shared Squares

Implementation

Development-related Improvements

With a few recently completed developments and a number of developments under review, Yonge Street Squares' improvements will happen incrementally. As additional sites start to redevelop, the character and usability of the squares will become more consistent.

New development will be responsible for providing a 1.5m building setback on each side of the square at the intersection with Yonge Street, unless there is an existing heritage building.

Capital Works and Coordination

Opportunities for public realm improvements may arise through upcoming capital works projects led by Toronto Water and Transportation Services. Taking advantage of other planned works represents an opportunity to coordinate and implement public realm improvements in the near-term and avoids disruption from additional construction work in the future.

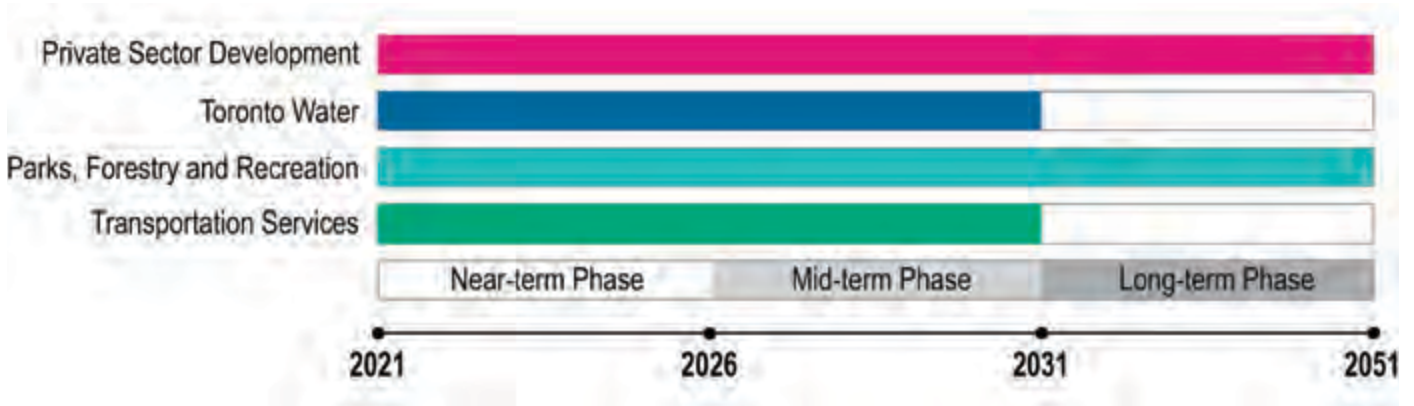


Figure 305 Projected Implementation Timeline for the Yonge Street Squares

Note: The implementation timeline represents information available from divisional partners and data from T.O. Inview as of December 31st, 2021 and is subject to change.

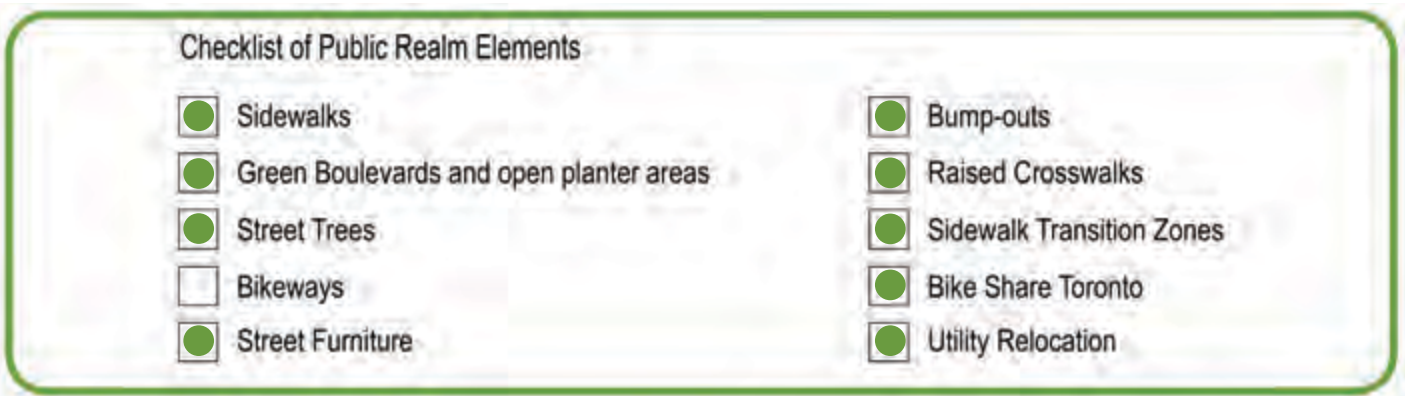


Figure 306 Checklist of Public Realm Elements to be implemented for the Yonge Street Squares

5.0

Parks and Open Spaces Summary

Summary

Intersection of Public Realm Moves with Parks and Other Open Spaces

Summary


A full description of the strategy to deliver new, expanded and improved parkland in Midtown is provided in the Midtown Infrastructure Implementation Strategy report, and summarized below.

There are currently 27 existing parks totaling 46.2 ha of parkland within the YESP area, including new parks established since the launch of the Midtown In Focus study. Most parks are small parkettes of 0.5 ha or less, while most parkland is concentrated in larger parks of 1.5 ha or greater in size. The need for parkland acquisition and improvement in Midtown has been articulated in the Midtown Parks and Public Realm Plan (2018) and the policies of the YESP, and confirmed in the City’s Parkland Strategy (2019). Portions of Midtown have been identified as Parkland Priority Areas based on local parkland provision rate, parkland supply, high growth, and low income population.

Based on updated growth estimates, it is anticipated that parkland provision per person in Midtown will decrease over the long term. This underscores the need to secure as much parkland as possible and make best use of existing parkland to serve the growing population, as opportunities arise.

Major capital initiatives that leverage existing City assets, such as exploring the feasibility of decking over the TTC Davisville Yard and open-cut subway trench west of Yonge Street, can lead to the creation of significant new parkland. City staff are also continually working to secure parkland both as part of the development review process and through City-initiated acquisitions. At the same time, City staff are undertaking improvements to existing parkland to provide enhanced programming, upgrade the condition and quality of existing facilities, add new amenities, improve accessibility and wayfinding, habitat restoration, tree planting, and interface with surrounding streets.

The implementation strategy for parks includes an inventory of all recommended Midtown parkland acquisition and improvement projects, together with a prioritization framework for their delivery in the near, mid and long term.

 Yonge-Eglinton Secondary Plan Area



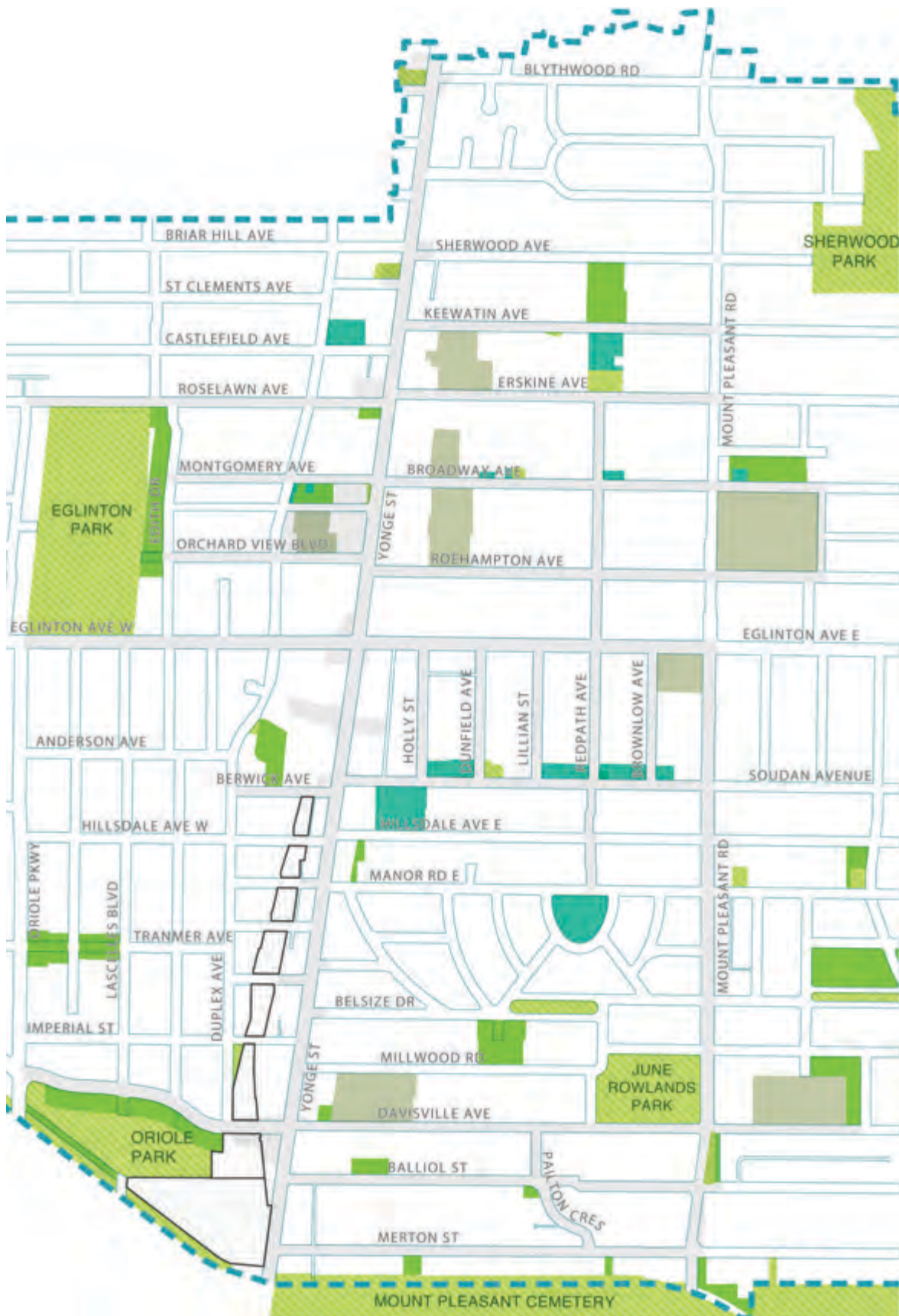
Intersection of Public Realm Moves with Parks and Other Open Spaces

The Public Realm Moves intersect with existing and planned future parks to create a cohesive green network for Midtown. The preliminary concept plans developed in this document are intended to connect and integrate with parks and open spaces as they are acquired or redesigned. Coordination with City Divisions including Transportation Services, City Planning and Toronto Water is needed to ensure alignment with Parks, Forestry and Recreation capital project scheduling, using the concept plans to provide guidance for the design of public realm improvements along the adjacent right-of-way. As a recent example of this coordination, the redesign of the Redpath Avenue Parkette, and the abutting streetscape to be incorporated along its Erskine Avenue frontage, will be informed by the concept plans developed for the Midtown Greenways, and implemented at the time of reconstruction.

The PPR Plan also highlights the importance of maximizing the use of other privately-owned public spaces to link the overall green network in Midtown. Similar to the approach described for the public realm interface with parks, the design of street frontages and connections to spaces such as school yards, church yards and cemeteries should take into consideration for the Public Realm Move concept plans outlined in this Strategy, to ensure a seamless integration between the public and private realm.

Parks and Open Spaces Network

-  Existing Parks and Ravines
-  Proposed Parks and Expansions
-  School Yards, Libraries and Sports Fields
-  Future and Planned Parks
-  Decking Studies
-  Public Realm Moves



SECTION 5.0

Figure 307 Map of Parks and Open Spaces Network relative to Midtown Public Realm Moves

6.0

Phasing and Implementation Recommendations

Summary

Public Realm Implementation Matrix

Summary

Improvements to the Public Realm Moves can be delivered through private development, City capital projects, or a joint public-private partnership. With numerous developments proposed along all Public Realm Moves, partial enhancements to the public realm, coordinated through the use of this document, can be realized within the next 5 years through private construction. Where possible, additional improvements should be undertaken and constructed by the developer for constrained sites immediately adjacent to development that are not anticipated to redevelop in the near future. Areas with the highest concentration of development activity will therefore see earlier implementation of associated Public Realm Moves, albeit in an incremental fashion.

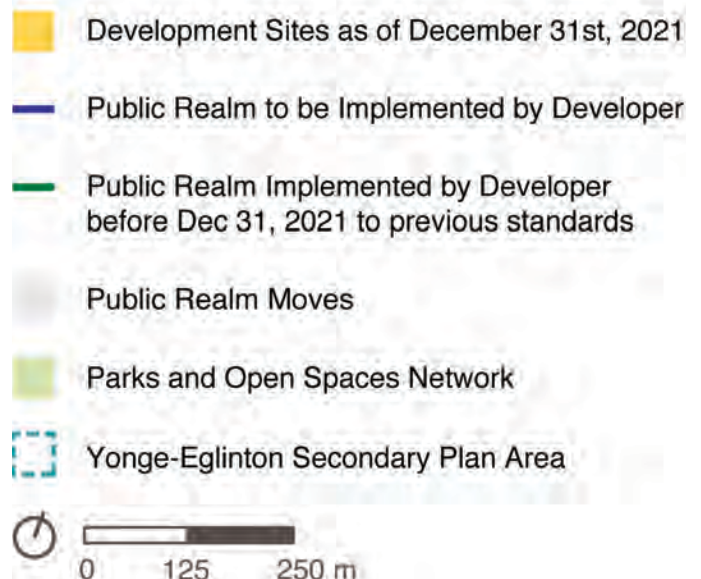
Where improvements are secured on a site through implementation tools described in the Midtown Infrastructure Implementation Strategy and summarized in this report, the developer will be responsible for the following:

- Preparing complete detailed designs and functional drawings, including sidewalk transition details
- Relocating utilities and obtaining associated approvals/ required easements
- Securing necessary permits/ approvals
- Constructing the improvements

The Public Realm Moves may also be delivered through opportunities to incorporate these elements into the design of other City capital projects, such as road reconstruction to maintain state of good repair, subject to evaluation by Transportation Services as part of their capital planning process. At the time streets captured by Public Realm Moves are programmed for works, they will be reviewed by capital program teams for safety issues, cycling connections, missing sidewalks, opportunities to widen sidewalks and green streets improvements.

Standalone projects or public realm elements may also be advanced independently by the City, where funding has been secured through Section 37 contributions, philanthropy and/ or local partnerships, such as with BIAs.

Developer-Implemented Improvements



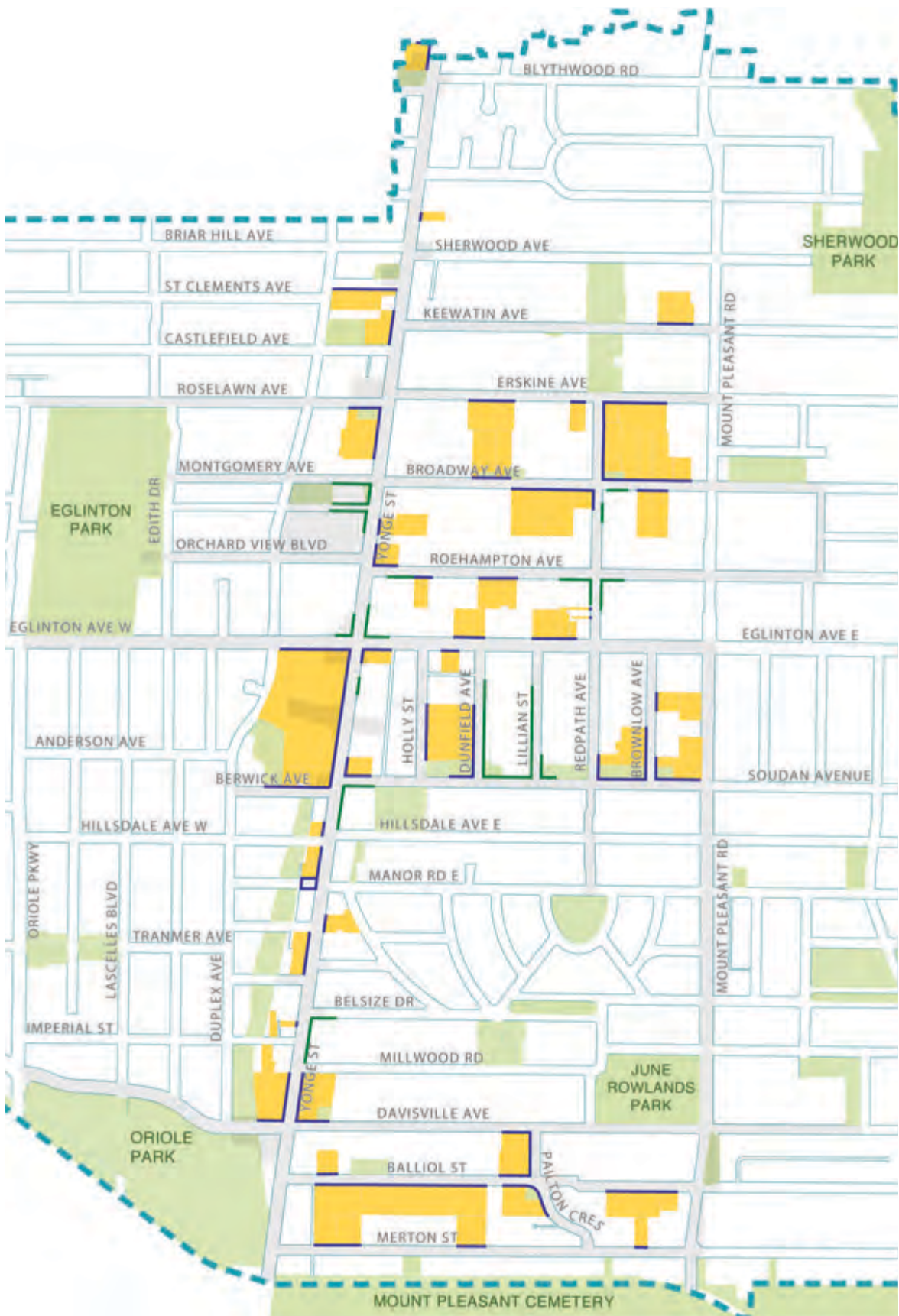


Figure 308 Map of Developer-Implemented Improvements

Public Realm Implementation Matrix

The following table outlines an action plan for moving the strategy forward, identifying key public realm moves, current stage gates and status, coordination with Parks and potential Division leads. The matrix also suggests the prioritization of projects within near-term, mid-term and long-term phases. This matrix should be used as a first step in developing a more detailed delivery schedule, which takes into consideration available budgets and resources at the time of implementation.

Public Realm Moves	Project or Location	Description	Lead	Stage Gate	Status
Yonge Street Squares	Davisville Special Square - north-west corner	Custom urban landscaped area including tree planting, seating, lighting, bike parking, potential utility relocation	CP	Project Planning	Underway
Yonge Street Squares	Davisville Special Square - north-east corner	Custom urban landscaped area including tree planting, seating, lighting, bike parking, potential utility relocation	CP	Project Planning	Underway
Yonge Street Squares	Davisville Special Square - south-west corner	Custom urban landscaped area including tree planting, seating, lighting, bike parking, potential utility relocation	CP	Project Planning	Underway
Yonge Street Squares	Yonge-Eglinton Crossroads Special Square north-west corner	Custom urban square framing the Yonge-Eglinton Crossroads including tree planting, street furniture, bike parking, potential utility relocation	CP	In Service	Complete
Yonge Street Squares	Yonge-Eglinton Crossroads Special Square north-east corner	Custom urban square framing the Yonge-Eglinton Crossroads including tree planting, street furniture, bike parking, potential utility relocation	CP	In Service	Complete
Yonge Street Squares	Yonge-Eglinton Crossroads Special Square south-west corner	Canada Square - Custom urban square framing the Yonge-Eglinton Crossroads including tree planting, street furniture, bike parking, potential utility relocation	CP	Detailed Design	Underway

Delivery Method	Funding Status	Funding Source (Potential or Secured)	Phasing	Alignment with Parks
Private - Development	No Funding Identified	None (requires Developer Pays)	2027-2031	
Private - Development	No Funding Identified	None (requires Developer Pays)	2027-2031	
Joint (City-Led (CP/TS) & Private)	No Funding Identified	None (requires City Capital (TS), Developer Pays)	Subject to Development/City Capital Programming	Davisville Yard Park
Private - Development	Funding Identified	Developer Pays	Completed	
Private - Development	Funding Identified	Developer Pays	Completed	
Private - Development	No Funding Identified	None (requires Developer Pays)	2027-2031	Canada Square Park

Public Realm Moves	Project or Location	Description	Lead	Stage Gate	Status
Yonge Street Squares	Yonge-Eglinton Crossroads Special Square - south-east corner	Custom urban square framing the Yonge-Eglinton Crossroads including tree planting, street furniture, bike parking, potential utility relocation	CP	Detailed Design	Underway
Yonge Street Squares	Montgomery Special Square	Neighbourhood civic space - south-west corner - 58 Orchard View Blvd	CP	In Service	Complete
Yonge Street Squares	Montgomery Special Square	Neighbourhood civic space - 25 Montgomery Avenue	CP	In Service	Complete
Yonge Street Squares	Montgomery Special Square	Neighbourhood civic space - south-west corner of Helendale Avenue and Yonge Street - 2384 Yonge Street	CP	In Service	Complete
Yonge Street Squares	Montgomery Special Square	Neighbourhood civic space encompassing Helendale shared street, Montgomery Square Parkette and open spaces surrounding library	CP/TS	Project Planning	Planned
Yonge Street Squares	Blythwood Road Typical Square	New square including bump-out, intersection realignment, tree planting, street furniture, potential relocation of utilities and/or parking	CP	Project Planning	Planned
Yonge Street Squares	Lytton Blvd Typical Square	New square including bump-out, intersection realignment, tree planting, street furniture, potential relocation of utilities and/or parking	CP	Project Planning	Planned
Yonge Street Squares	Sherwood Ave Typical Square	New square including bump-out, intersection realignment, tree planting, street furniture, potential relocation of utilities and/or parking	CP	Project Planning	Planned

Delivery Method	Funding Status	Funding Source (Potential or Secured)	Phasing	Alignment with Parks
Private - Development	No Funding Identified	None (requires Developer Pays)	2021-2026	
Private - Development	Funding Identified	Developer Pays	Completed	
Private - Development	Funding Identified	Developer Pays	Completed	
Private - Development	Funding Identified	Developer Pays	Completed	
Joint (City-Led (CP/TS) & Private)	No Funding Identified	None (requires City Capital (TS), Developer Pays)	Subject to Development/City Capital Programming	Montgomery/Helendale Parkette
Private - Development	No Funding Identified	None (requires Developer Pays)	Subject to Development	
Private - Development	No Funding Identified	None (requires Developer Pays)	Subject to Development	
Private - Development	No Funding Identified	None (requires Developer Pays)	Subject to Development	

Public Realm Moves	Project or Location	Description	Lead	Stage Gate	Status
Yonge Street Squares	St Clements Ave Typical Square	New square including bump-out, tree planting, street furniture, potential relocation of utilities and/or parking	CP	Detailed Design	Underway
Yonge Street Squares	Roselawn Ave Typical Square	New square including bump-out, intersection realignment, tree planting, street furniture, potential relocation of utilities and/or parking	CP	Project Planning	Underway
Yonge Street Squares	West block between Yonge Street and Berwick Ave	Part of Canada Square an urban open space providing mid-block connection and programming opportunities	CP	Detailed Design	Underway
Yonge Street Squares	East block between Yonge Street and Berwick Ave	Urban open space providing mid-block connection and programming opportunities	CP	Project Planning	Planned
Yonge Street Squares	Berwick Ave Typical Square	New Square including intersection realignment with bump-out, tree planting, street furniture, potential relocation of utilities and/or parking	CP	Detailed Design	Underway
Yonge Street Squares	Soudan Ave Typical Square - south-east corner	New Square including intersection realignment with bump-out, tree planting, street furniture, potential relocation of utilities and/or parking	CP	In Service	Complete
Yonge Street Squares	Soudan Ave Typical Square - north-east corner	New Square including intersection realignment with bump-out, tree planting, street furniture, potential relocation of utilities and/or parking	CP	Detailed Design	Underway
Yonge Street Squares	Glebe Road Typical Square	New Square including intersection realignment with bump-out, tree planting, street furniture, potential relocation of utilities and/or parking	CP	Project Planning	Planned

Delivery Method	Funding Status	Funding Source (Potential or Secured)	Phasing	Alignment with Parks
Private - Development	No Funding Identified	None (requires City Capital (TS), Developer Pays)	2021-2026	
Private - Development	No Funding Identified	None (requires Developer Pays)	2021-2026	Roselawn Avenue Park
Private - Development	No Funding Identified	None (requires Developer Pays)	2032-2051	Canada Square Park
Private - Development	No Funding Identified	None (requires City Capital (TS), Developer Pays)	Subject to Development	
Private - Development	No Funding Identified	None (requires Developer Pays)	2032-2051	
Private - Development	Funding Identified	Developer Pays	Completed	
Private - Development	No Funding Identified	None (requires Developer Pays)	2021-2026	
Private - Development	No Funding Identified	None (requires Developer Pays)	Subject to Development	

Public Realm Moves	Project or Location	Description	Lead	Stage Gate	Status
Yonge Street Squares	Manor Road Shared Square	Shared new pedestrian street, including bump-out, tree planting, street furniture, potential relocation of utilities and/or parking	CP	Detailed Design	Underway
Yonge Street Squares	Glebe Road Shared Square	Shared new pedestrian street, including bump-out, tree planting, street furniture, potential relocation of utilities and/or parking	CP	Project Planning	Underway
Yonge Street Squares	Belsize Road Typical Square-south-east corner	Urban square achieved through bump-out at intersection, tree planting, street furniture, potential relocation of utilities and/or parking	CP	In Service	Complete
Yonge Street Squares	Belsize Road Typical Square - north-east corner	Urban square achieved through bump-out at intersection, tree planting, street furniture, potential relocation of utilities and/or parking - north-east corner	CP	Project Planning	Planned
Eglinton Green Line	Eglinton Green Line	Landscaped promenade with animated outdoor spaces on the north side of the street, including extensive tree planting, seating, lighting and urban plazas	CP	Project Planning	Underway
Park Street Loop	Broadway Ave	Landscaped pedestrian promenade along the street, achieved through roadway and intersection narrowing, new bikeway, bump-outs at crosswalks, intersections and parks, tree planting, street furniture, potential relocation of utilities and/or parking	CP/TS	Project Planning	Planned

Delivery Method	Funding Status	Funding Source (Potential or Secured)	Phasing	Alignment with Parks
Private - Development	No Funding Identified	None (requires City Capital (TS), Developer Pays)	2021-2026	
Private - Development	No Funding Identified	None (requires City Capital (TS), Developer Pays)	2027-2031	
Private - Development	Funding Identified	Developer Pays	Completed	
Private - Development	No Funding Identified	None (requires Developer Pays)	Subject to Development	
Private - Development	Partial Funding Identified	Developer Pays	Subject to Development	North Toronto Memorial Community Centre Potential Expansion Eglinton Park Master Plan Eglinton Park Expansion
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	- Broadway Park - New parks associated with Park Street Loop Montgomery Square (2384 Yonge St)

Public Realm Moves	Project or Location	Description	Lead	Stage Gate	Status
Park Street Loop	Roehampton Ave	Landscaped pedestrian promenade along the street, achieved through bump-outs at crosswalks, intersections and parks, tree planting, street furniture, potential relocation of utilities and/or parking	CP/TS	Project Planning	Underway
Redpath Revisited	Redpath Revisited	Complete street achieved through partial roadway narrowing, bump-outs at intersections and parks, tree planting in rain gardens, sidewalk widening, street furniture, potential relocation of utilities and/or parking, new sidewalk and roadway narrowing between Soudan Avenue and Manor Road	CP/TS	Project Planning	Underway
Midtown Greenways	Keewatin Greenway	Complete street achieved through bump-outs at intersections, crosswalks and parks, tree planting in rain gardens, sidewalk widening, street furniture, potential relocation of utilities and/or parking	CP/TS	Project Planning	Underway
Midtown Greenways	Soudan Avenue	Complete street achieved through bump-outs at intersections and parks, tree planting in rain gardens, sidewalk widening, street furniture, potential relocation of utilities and/or parking	CP/TS	Project Planning	Underway
Midtown Greenways	Roselawn Greenway	Complete street achieved through bump-outs at intersections, tree planting in rain gardens, sidewalk widening, street furniture, potential relocation of utilities and/or parking	CP/TS	Project Planning	Underway

Delivery Method	Funding Status	Funding Source (Potential or Secured)	Phasing	Alignment with Parks
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	New parks associated with Park Street Loop
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	- New park at Church of Transfiguration (11 Manor Rd E) - Redpath Avenue Parkette Improvements
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	Redpath Revisited Local Park
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	- Hillsdale Local Park - Soudan Priority Park Area
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	

Public Realm Moves	Project or Location	Description	Lead	Stage Gate	Status
Midtown Greenways	Berwick Greenway	Complete street achieved through bump-outs at intersections, tree planting in rain gardens, sidewalk widening, street furniture, potential relocation of utilities and/or parking	CP/TS	Project Planning	Underway
Midtown Greenways	Erskine Greenway	Complete street achieved through bump-outs at intersections, crosswalks and parks, tree planting in rain gardens, sidewalk widening, street furniture, potential relocation of utilities and/or parking	CP/TS	Project Planning	Underway
Midtown Greenways	Holly Street	Complete street achieved through bump-outs at parks, tree planting in rain gardens, sidewalk widening, street furniture, potential relocation of utilities and/or parking	CP/TS	Project Planning	Underway
Midtown Greenways	Dunfield Avenue	Complete street achieved through bump-outs at parks, tree planting in rain gardens, sidewalk widening, street furniture, potential relocation of utilities and/or parking	CP/TS	Project Planning	Underway
Midtown Greenways	Lillian Street	Complete street achieved through bump-outs at parks, tree planting in rain gardens, sidewalk widening, street furniture, potential relocation of utilities and/or parking	CP/TS	Project Planning	Underway
Midtown Greenways	Brownlow Avenue	Complete street achieved through bump-outs at parks and intersections, tree planting in rain gardens, sidewalk widening, street furniture, potential relocation of utilities and/or parking	CP/TS	Project Planning	Underway

Delivery Method	Funding Status	Funding Source (Potential or Secured)	Phasing	Alignment with Parks
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	2032-2051	
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	- Redpath Avenue Parkette Improvements
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	Soudan Priority Park Area
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	Soudan Priority Park Area
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	Soudan Priority Park Area

Public Realm Moves	Project or Location	Description	Lead	Stage Gate	Status
Davisville Community Street	Davisville Community Street	Complete street achieved through roadway narrowing, new bikeway, sidewalk widening, bump-outs at mid-block connections and parks, tree planting, street furniture, potential relocation of utilities and/or parking	CP/TS	Project Planning	Underway
Merton Street Promenade	Merton Street Promenade	Complete street achieved through roadway narrowing, sidewalk widening, bump-outs, tree planting in rain gardens, street furniture, potential relocation of utilities and/or parking. Enhanced pedestrian connections to Beltline Trail and Mount Pleasant Cemetery	CP/TS	Project Planning	Underway
Balliol Green Street	Balliol Green Street	Complete street achieved through sidewalk widening, bump-outs, tree planting in rain gardens, street furniture, potential relocation of utilities and/or parking.	CP/TS	Project Planning	Underway
Pailton Crescent Connector	Pailton Crescent Connector	Complete street with urban plazas achieved through intersection narrowing, new and widened sidewalks, tree planting in rain gardens, street furniture, potential relocation of utilities and/or parking.	CP/TS	Project Planning	Underway

Delivery Method	Funding Status	Funding Source (Potential or Secured)	Phasing	Alignment with Parks
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	<ul style="list-style-type: none"> - Davisville Aquatic and Community Centre and new park associated with it - New parks associated with Davisville Community Street - Improvements to June Rowlands Park
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	<ul style="list-style-type: none"> - New park or mid-block connection (113, 115, 117 Merton) - New park or mid-block connection (267 Merton)
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	Balliol Parkette (60 Balliol)
Joint (City-Led (CP/TS) & Private)	Partial Funding Identified	City Capital (TS/CP) Developer Pays	Subject to Development/City Capital Programming	Pailton Square (185 Balliol St)

A

Appendix - List of Figures

List of Figures

Figure 1	Map of the Public Realm Moves	5
Figure 2	Map of Parks and Open Spaces Network relative to Midtown Public Realm Moves	7
Figure 3	Map of the Cycling Network Plan within the Public Realm Moves area	9
Figure 4	Map of transit network within the Public Realm Moves area	11
Figure 5	Scope of Public Realm Elements addressed in this report	13
Figure 6	Map of existing roadway widths along the Public Realm Moves	15
Figure 7	Map of existing sidewalk widths along the Public Realm Moves	17
Figure 8	Map of on-street and boulevard parking along the Public Realm Moves	19
Figure 9	Map of existing and future Bike Share Toronto stations along and in close proximity to the Public Realm Moves	21
Figure 10	Map of Developments contributing Section 37 benefits along the Public Realm Moves	25
Figure 11	Map of current development sites along the Public Realm Moves	29
Figure 12	Map of heritage properties and soft sites along the Public Realm Moves	31
Figure 13	Yonge Street	34
Figure 14	York Street and Lake Shore Boulevard West	34
Figure 15	Farquhars Lane and The Esplanade	34
Figure 16	Ossington Avenue	34
Figure 17	Montreal, QC	35
Figure 18	Raised cycle lanes on Bloor Street West	35
Figure 19	Yonge Street	35
Figure 20	Bump-out at Gerrard and Mutual Streets	35
Figure 21	Raised Crosswalk at St. George Street	36
Figure 22	Sidewalk Transition at Stewart Street	36
Figure 23	Bike Share Station at Yonge Street and Bloor Street	36
Figure 24	Utilities locations on Broadway Avenue	36
Figure 25	Duplex Avenue	37
Figure 26	Vent Screening, Davenport Road	37
Figure 27	Transformer Screening, Sorauren Avenue	37
Figure 28	Mid-block connection, Broadway Avenue	37
Figure 29	Map of landscape setbacks along the Public Realm Moves	39
Figure 30	Map of streetscape improvements along the Public Realm Moves	41
Figure 31	From Eglinton Green Line Landscape and Public Realm Standards	44
Figure 32	Planting arrangement diagram from Eglinton Green Line Landscape and Public Realm Standards	45
Figure 33	Conceptual section from Eglinton Green Line Landscape and Public Realm Standards	45
Figure 34	Map showing the location of the Park Street Loop Public Realm Move	48
Figure 35	Map of the Cycling Network Plan within the Park Street Loop area	49
Figure 36	Section of the Existing Conditions along Roehampton Avenue	50
Figure 37	Existing conditions along south side of Roehampton Avenue	50
Figure 38	Existing conditions along north side of Roehampton Avenue	50
Figure 39	Section of the Existing Conditions of Broadway Avenue between Yonge Street and Redpath Avenue	51
Figure 40	Existing conditions along Broadway Avenue	51
Figure 41	Existing conditions along Broadway Avenue	51
Figure 42	Section of the Existing Conditions of Broadway Avenue between Redpath and Mount Pleasant Road	52
Figure 43	Existing condition at the intersection of Yonge Street and Roehampton Avenue	52

Figure 44	Existing condition at the intersection of Broadway Avenue and Mount Pleasant Road	52
Figure 45	Existing condition at the intersection of Broadway Avenue and Redpath Avenue	52
Figure 46	Existing condition at the intersection of Broadway Avenue and Mount Pleasant Road	52
Figure 47	Section of the Existing Conditions of Broadway Avenue intersection with Yonge Street	53
Figure 48	Plan of the Existing Conditions of Broadway Avenue intersection with Yonge Street	53
Figure 49	Section of the Existing Conditions of Broadway Avenue intersection with Mount Pleasant Road	54
Figure 50	Plan of the Existing Conditions of Broadway Avenue intersection with Mount Pleasant Road	54
Figure 51	Map of the Section 37 secured contribution locations along the Park Street Loop Public Realm Move	55
Figure 52	Map of potential redevelopment sites along the Park Street Loop	56
Figure 53	Diagram of opportunities and constraints at Broadway Avenue	57
Figure 54	Diagram of opportunities and constraints at Roehampton Avenue	57
Figure 55	Utility Challenges along Roehampton Avenue	58
Figure 56	Utility Challenges along Broadway Avenue	58
Figure 57	Utility Challenges along Roehampton Avenue	58
Figure 58	Utility Challenges along Broadway Avenue	58
Figure 59	Constrained Condition at north west corner of Broadway and Redpath Avenues	58
Figure 60	Constrained Condition on Roehampton Avenue	58
Figure 61	Diagram of the Street Section showing Park Street Loop Zones	59
Figure 62	Section and Plan of Broadway Avenue - Recommended Interim Condition	61
Figure 63	Section and Plan of Broadway Avenue, between Yonge Street and Mount Pleasant Road - general Recommended Final Condition	63
Figure 64	Section and Plan of Broadway Avenue - Recommended Final Condition at intersection with Mount Pleasant Road	64
Figure 65	Section and Plan of Broadway Avenue - Recommended Final Condition with Bump-out at intersection with Yonge Street	65
Figure 66	Section and Plan of Roehampton Avenue - Recommended Final Condition	66
Figure 67	Section and Plan of Broadway Avenue - Recommended Final Condition of bump-out in front of parks	67
Figure 68	Section and Plan of Broadway Avenue - raised crossing Recommended Final Condition	68
Figure 69	Plan of existing constrained and sidewalk transition - Recommended Final Condition	69
Figure 70	Fort York Boulevard	70
Figure 71	Harbour Street	70
Figure 72	Queens Quay West	70
Figure 73	York Street	70
Figure 74	St. James Park	71
Figure 75	Duplex Avenue	71
Figure 76	Adelaide Place	71
Figure 77	Light Column at Montgomery Square Park	71
Figure 78	Yorkville Park	72
Figure 79	City Place	72
Figure 80	College Park	72
Figure 81	Hydro - Quebec Parc, Montreal	72
Figure 82	Transformer Screen at Adelaide Place	73
Figure 83	Mid-block connection at Broadway Avenue	73
Figure 84	Mid-block connection at Wellington Avenue	73

Figure 85	Adelaide Place	74
Figure 86	Niagara Street	74
Figure 87	St. James Park	74
Figure 88	Roehampton Avenue	74
Figure 89	Roehampton Avenue	74
Figure 90	Projected Implementation Timeline for the Park Street Loop	75
Figure 91	Checklist of Public Realm Elements to be implemented for the Park Street Loop	75
Figure 92	Map showing the location of the Davisville Community Street Public Realm Move	78
Figure 93	Map of the Cycling Network Plan within the Davisville Community Street area	79
Figure 94	Existing crosswalk along Davisville Avenue	80
Figure 95	Existing Bike Share Station	80
Figure 96	Existing conditions along Davisville Avenue	80
Figure 97	Existing TTC shelter on the south side	80
Figure 98	Section and Plan of the Existing Conditions along Davisville Avenue	81
Figure 99	Map of the Section 37 secured contribution locations along the Davisville Public Realm Move	82
Figure 100	Map of potential redevelopment sites along the Davisville Community Street	83
Figure 101	Diagram of opportunities and constraints at Davisville Avenue	83
Figure 102	Existing stairs at June Rowlands Park	84
Figure 103	Existing retaining walls at June Rowlands Park	84
Figure 104	Existing TTC Shelter at June Rowlands Park	84
Figure 105	Existing sidewalk transition at June Rowlands Park	84
Figure 106	Existing hydro poles along Davisville Avenue	85
Figure 107	Existing fire hydrants along Davisville Avenue	85
Figure 108	Existing utility conflicts along Davisville Avenue	85
Figure 109	Existing utilities along Davisville Avenue	85
Figure 110	TTC Shelter and raised Bikeway on Roncesvalles Avenue	86
Figure 111	Open planters with seating along Roncesvalles Avenue	86
Figure 112	Raised Bikeway on Bloor Street	86
Figure 113	Planted bump-out at Gerrard Street East and Pembroke Street	86
Figure 114	Section and Plan of Davisville Community Street - Recommended Final Condition	87
Figure 115	Section and Plan of Davisville Community Street - Recommended Final Condition at Yonge intersection	88
Figure 116	Section and Plan of Davisville Community Street - Recommended Final Condition at bump-out in front of June Rowlands Park	89
Figure 117	Section and Plan of Green Boulevard and Bike Share Station (North side), and Green Boulevard and TTC Shelter (South side) - Recommended Final Conditions	90
Figure 118	Section and Plan of Davisville Community Street - Recommended Interim Condition to be implemented by Developer	91
Figure 119	Projected Implementation Timeline for the Davisville Community Street	92
Figure 120	Checklist of Public Realm Elements to be implemented for the Davisville Community Street	92
Figure 121	Map showing the location of the Merton Street Promenade Public Realm Move	94
Figure 122	Existing bump-out at Merton Street and Pailton Crescent	95
Figure 123	Existing boulevard parking	95
Figure 124	Existing Toronto Bike Share Station	95

Figure 125	Existing crosswalk	95
Figure 126	Section and Plan of the Existing Conditions along Merton Street	96
Figure 127	Map of potential redevelopment sites along the Merton Street Promenade	97
Figure 128	Diagram of opportunities and constraints at Merton Street	98
Figure 129	Opportunities for connection with Kay Gardner Beltline Trail	98
Figure 130	Opportunities for pedestrian crossing at mid-block connections	98
Figure 131	Existing hydro poles along Merton Street	99
Figure 132	Existing utility challenges along Merton Street	99
Figure 133	Existing utilities along Merton Street	99
Figure 134	Existing utility conflicts along Merton Street	99
Figure 135	Section and Plan of Merton Street Promenade - Recommended Final Condition	100
Figure 136	Section and Plan of Merton Street Promenade - Recommended Final Condition at intersection with Yonge Street	102
Figure 137	Plan of Merton Street Promenade - Recommended Final Condition of bump-out and raised crossing at mid-block connection	103
Figure 138	Section and Plan of Merton Street Promenade - Recommended Interim Condition on the north side of the street to be implemented by development	104
Figure 139	Projected Implementation Timeline for the Merton Street Promenade	105
Figure 140	Checklist of Public Realm Elements to be implemented for the Merton Street Promenade	105
Figure 141	Map showing the location of the Pailton Crescent Connector Public Realm Move	108
Figure 142	Existing extra wide ROW at intersection with Balliol Street	109
Figure 143	Existing condition with no sidewalk along west side of Pailton Crescent	109
Figure 144	Existing condition between Balliol Street and Davisville Avenue	109
Figure 145	Existing condition along Pailton Crescent	109
Figure 146	Section and Plan of the Existing Conditions along Pailton Crescent between Davisville Avenue and Balliol Street	110
Figure 147	Section and Plan of the Existing Conditions along Pailton Crescent between Balliol and Merton Streets	111
Figure 148	Map of potential redevelopment sites along the Pailton Crescent Connector	112
Figure 149	Diagram of opportunities and constraints at Pailton Crescent	113
Figure 150	Existing mature trees along Pailton Crescent	113
Figure 151	Existing conflicts with hydro poles along Pailton Crescent	113
Figure 152	Missing sidewalk along Pailton Crescent south of Balliol Street	113
Figure 153	Missing sidewalk and boulevard parking along Pailton Crescent near the intersection with Merton Street	113
Figure 154	Section and Plan of Pailton Crescent Connector - Recommended Final Condition	114
Figure 155	Section and Plan of Pailton Crescent Connector - Recommended Final Condition of public square at intersection with Balliol Street	115
Figure 156	Section and Plan of Pailton Crescent Connector - Recommended Final Condition of public square at intersection with Merton Street	116
Figure 157	Projected Implementation Timeline for the Pailton Crescent Connector	117
Figure 158	Checklist of Public Realm Elements to be implemented for the Pailton Crescent Connector	117
Figure 159	Map showing the location of the Balliol Green Street Public Realm Move	119
Figure 160	Existing Bike Share Toronto station in front of Balliol Parkette	120
Figure 161	Painted bump-out at intersection with Yonge Street	120
Figure 162	Existing mid-block connection	120

Figure 163	Existing sidewalk condition on the south side	120
Figure 164	Section and Plan of the Existing Conditions along Balliol Street	121
Figure 165	Map of potential redevelopment sites along the Balliol Green Street	122
Figure 166	Diagram of opportunities and constraints at Balliol Street	122
Figure 167	Existing mature trees along Balliol Street	123
Figure 168	Existing grading challenges along Balliol Street	123
Figure 169	Existing utility conflicts along Balliol Street	123
Figure 170	Existing utility conflicts along Balliol Street	123
Figure 171	Section and Plan of Balliol Green Street - Recommended Final Condition	124
Figure 172	Plan of Balliol Green Street - Recommended Final Condition of bump-out of Bike Share station in front of Balliol Parkette	125
Figure 173	Plan of Balliol Green Street - Recommended Final Condition of bump-out and raised pedestrian crossing at mid-block connection	125
Figure 174	Tree Protection Zones (above) and Sidewalk Transition Zones (below) along Balliol Green Street	126
Figure 175	Projected Implementation Timeline for the Balliol Green Street	127
Figure 176	Checklist of Public Realm Elements to be implemented for the Balliol Green Street	127
Figure 177	Map showing the location of the Midtown Greenways Public Realm Moves	129
Figure 178	Existing condition along Keewatin Avenue	130
Figure 179	Existing condition along Erskine Avenue	130
Figure 180	Existing condition along Soudan Avenue	130
Figure 181	Existing condition along Dunfield Avenue	130
Figure 182	Existing sidewalk transition on the south side of Keewatin Avenue	131
Figure 183	Existing condition on the south side of Keewatin Avenue	131
Figure 184	Existing condition on the south side of Keewatin Avenue at sports field	131
Figure 185	Existing intersection at Yonge Street and Keewatin Avenue	131
Figure 186	Section and Plan of the Existing Conditions of Keewatin Avenue at sports field	132
Figure 187	Section and Plan of the Existing Conditions of Keewatin Avenue	133
Figure 188	Map of the Section 37 secured contribution locations along the Keewatin Greenway	135
Figure 189	Map of potential redevelopment sites along the Keewatin Greenway	135
Figure 190	Diagram of opportunities and constraints at Keewatin Avenue	136
Figure 191	Existing fencing along Keewatin Avenue	137
Figure 192	Existing grading challenges along Keewatin Avenue	137
Figure 193	Existing boulevard parking on the north side of Keewatin Avenue	137
Figure 194	Existing garden walls along north side of Keewatin Avenue	137
Figure 195	Existing utility conflicts along Keewatin Avenue	137
Figure 196	Existing mature trees along Keewatin Avenue	137
Figure 197	Section and Plan of Keewatin Greenway - Recommended Final Condition at sports field	139
Figure 198	Section and Plan of Keewatin Greenway - mid-block Recommended Final Condition	140
Figure 199	Section and Plan of Keewatin Greenway - Recommended Final Condition at major intersections with constrained sites	141
Figure 200	Section and Plan of Keewatin Greenway - Recommended Final Condition at mid-block raised crossing bump-out	142
Figure 201	Projected Implementation Timeline for the Keewatin Greenway	143
Figure 202	Checklist of Public Realm Elements to be implemented for the Keewatin Greenway	143
Figure 203	Existing Bike Share Toronto Station on the north side of Erskine Avenue	144

Figure 204	Existing condition at Redpath Avenue Parkette	144
Figure 205	Existing intersection at Redpath Avenue and Erskine Avenue	144
Figure 206	Existing intersection at Yonge Street and Erskine Avenue	144
Figure 207	Section and Plan of the Existing Conditions along Erskine Avenue	145
Figure 208	Map of potential redevelopment sites along the Erskine Greenway	146
Figure 209	Map of the Section 37 secured contribution locations along the Erskine Greenway	146
Figure 210	Diagram of opportunities and constraints at Erskine Avenue	147
Figure 211	Existing boulevard parking on the south side of Erskine Avenue	147
Figure 212	Existing utilities along Erskine Avenue	147
Figure 213	Section and Plan of Erskine Greenway - Recommended Final Condition	149
Figure 214	Section and Plan of Erskine Greenway - Recommended Final Condition at mid-block raised crossing bump-out	150
Figure 215	Section and Plan of Erskine Greenway - Recommended Final Condition at bump-out in front of parks	151
Figure 216	Section and Plan of Erskine Greenway - Recommended Final Condition at major intersections with constrained sites	152
Figure 217	Projected Implementation Timeline for the Erskine Greenway	153
Figure 218	Checklist of Public Realm Elements to be implemented for the Erskine Greenway	153
Figure 219	Existing conditions along Soudan Avenue	154
Figure 220	Existing conditions along Lillian Street	154
Figure 221	Plan of Existing Conditions along Holly Street, Dunfield Avenue, Lillian Street, and Brownlow Avenue	155
Figure 222	Plan of Existing Conditions along Soudan Avenue between Yonge Street and Lillian Street	155
Figure 223	Plan of Existing Conditions along Soudan Avenue between Lillian Street and Mount Pleasant Road	155
Figure 224	Map of the Section 37 secured contribution locations along the Midtown Greenways	156
Figure 225	Map of potential redevelopment sites along the Midtown Greenways	157
Figure 226	Diagram of opportunities and constraints at Holly Street, Dunfield Avenue, Lillian Street, Brownlow Avenue, and Soudan Avenue	158
Figure 227	Utility conflicts along Brownlow Avenue	159
Figure 228	Utility conflicts along Holly Street	159
Figure 229	Utility conflicts along Lillian Street	159
Figure 230	Utility and existing tree conflicts along Soudan Avenue	159
Figure 231	Section and Plan of Holly and Brownlow Greenways - Recommended Final Condition	161
Figure 232	Section and Plan of Lillian and Dunfield Greenways - Recommended Final Condition	162
Figure 233	Section and Plan of Soudan Greenway - bump-out in front of parks Recommended Final Condition	163
Figure 234	Section and Plan of Soudan Greenway - Recommended Final Condition	164
Figure 235	Section and Plan of general Greenways - Recommended Final Condition at major intersections	165
Figure 236	Projected Implementation Timeline for the Midtown Greenways	166
Figure 237	Checklist of Public Realm Elements to be implemented for the Midtown Greenways	166
Figure 238	Map showing the location of the Redpath Revisited Public Realm Move	168
Figure 239	Existing condition at the intersection of Redpath and Roehampton Avenues	169
Figure 240	Existing condition on the west side of Redpath Avenue	169
Figure 241	Existing condition on Redpath Avenue	169
Figure 242	Existing condition south of Soudan Avenue on Redpath Avenue	169
Figure 243	General Section and Plan of the Existing Conditions along Redpath Avenue from Erskine Avenue to Soudan Avenue	171

Figure 244	Section and Plan of the Existing Conditions along Redpath Avenue from Manor Road East to Soudan Avenue	172
Figure 245	Map of the Section 37 secured contribution locations along Redpath Revisited	173
Figure 246	Map of potential redevelopment sites along Redpath Revisited	174
Figure 247	Diagram of opportunities and constraints at Redpath Avenue	175
Figure 248	Grading challenges south of Soudan Avenue on Redpath Avenue	176
Figure 249	Utility challenges along Redpath Avenue	176
Figure 250	Private fencing along Redpath Avenue	176
Figure 251	Missing sidewalk on the west side on Redpath Avenue, south of Soudan Avenue	176
Figure 252	Section and Plan of Redpath Revisited - Recommended Final Condition from Erskine Avenue to Roehampton Avenue and from Eglinton Avenue to Soudan Avenue	178
Figure 253	Section and Plan of Redpath Revisited - Recommended Final Condition of bump-out at intersection with Broadway Avenue	179
Figure 254	Section and Plan of Redpath Revisited - Recommended Final Condition from Soudan Avenue to Manor Road East	180
Figure 255	Section and Plan of Redpath Revisited - Recommended Final Condition from Roehampton Avenue to Eglinton Avenue	181
Figure 256	Projected Implementation Timeline for the Redpath Revisited	182
Figure 257	Checklist of Public Realm Elements to be implemented for the Redpath Revisited	182
Figure 258	Map showing the location of the Yonge Street Squares Public Realm Move	185
Figure 259	Cover of Meet in the Middle Streetscape Master Plan	186
Figure 260	Streetscape on Belsize Drive at Yonge Street	186
Figure 261	POPS on Yonge Street	187
Figure 262	Yonge and Davisville intersection	187
Figure 263	Midtown Yonge BIA street furniture	187
Figure 264	Manor Road West	187
Figure 265	Yonge Street Square at Sherwood Avenue	187
Figure 266	Yonge Street Square at Blythwood Road and Yonge Street	187
Figure 267	Yonge Street Square at Roselawn Avenue and Yonge Street	187
Figure 268	Glebe Road West	187
Figure 269	Map of the Section 37 secured contribution locations along the Yonge Street Squares	188
Figure 270	Map of potential redevelopment sites along the Yonge Street Squares	189
Figure 271	Diagram of opportunities and constraints at the intersection of Yonge Street and Blythwood Road and Lytton Boulevard	190
Figure 272	Diagram of opportunities and constraints along Yonge Street between Briar Hill Avenue and Roselawn Avenue	190
Figure 273	Diagram of opportunities and constraints along Yonge Street and Duplex Avenue, between Montgomery Avenue/ Broadway Avenue and Orchard View Boulevard	190
Figure 274	Diagram of opportunities and constraints along Yonge Street between Eglinton Avenue and Berwick Avenue	190
Figure 275	Diagram of opportunities and constraints at the intersections of Yonge Street and Manor Road, Glebe Road, and Belsize Drive	191
Figure 276	Diagram of opportunities and constraints at the intersection of Yonge Street and Davisville Avenue	191
Figure 277	Boulevard Parking at Roselawn and Yonge Street	192
Figure 278	Utility challenges at Blythwood Road	192
Figure 279	Bike Share Toronto station located in the roadway on Glebe Road West	192

Figure 280	Heritage Building at Sherwood Avenue and Yonge Street	192
Figure 281	Map showing the Special, Typical and Shared Yonge Street Squares	193
Figure 282	Streetscape along Montgomery Avenue	194
Figure 283	Montgomery Square Streetscape	194
Figure 284	Paving inlays at Montgomery Square	194
Figure 285	Yonge Street Square north-west corner at Yonge Street and Eglinton Avenue	195
Figure 286	Liberty Park, Manhattan, New York	196
Figure 287	Yonge-Eglinton Crossroads north-east corner unit paving	196
Figure 288	Front Street Promenade	196
Figure 289	Montgomery Square Park seating elements	196
Figure 290	Yonge-Eglinton Crossroads north-east corner seating elements	196
Figure 291	Tree Grate at Montgomery Square	197
Figure 292	Planters with seating along Bloor Street	197
Figure 293	Montgomery Square Park light column (Left)	197
Figure 294	Yonge-Eglinton Crossroads north-east corner light column (Right)	197
Figure 295	Dundas Street West corner square	198
Figure 296	College Street corner square	198
Figure 297	Bloor Street and Major Street corner square	198
Figure 298	Section of Yonge Street Squares - Recommended Final Condition for Typical Squares	199
Figure 299	Plan of Yonge Street Squares - Recommended Final Condition for Typical Squares	200
Figure 300	Bellair Street	201
Figure 301	Scott Street	201
Figure 302	Market Street	201
Figure 303	Section of Yonge Street Squares - Recommended Final Condition for Shared Squares	202
Figure 304	Plan of Yonge Street Squares - Recommended Final Condition for Shared Squares	203
Figure 305	Projected Implementation Timeline for the Yonge Street Squares	204
Figure 306	Checklist of Public Realm Elements to be implemented for the Yonge Street Squares	204
Figure 307	Map of Parks and Open Spaces Network relative to Midtown Public Realm Moves	207
Figure 308	Map of Developer-Implemented Improvements	211

B

Appendix - Eglinton Green Line Landscape and Public Realm Standards

