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## HIGHLAND CREEK VILLAGE

Development in Highland Creek Village will be consistent with the following urban design guidelines. The guidelines provide a framework for maintaining the historic character of Highland Creek Village during future re-development. The guidelines correspond to Policy 1.4 of the Highland Creek Community Secondary Plan in Chapter 6 of the Official Plan. They are to be read in conjunction with the urban design policies of the Official Plan.

### LOCATION

The area bounded by Old Kingston Road, Military Trail, Kingston Road and Highway 2A.

### GUIDELINES

#### OBJECTIVES

The objectives of the Urban Design Guidelines are to:

- Encourage the development of an attractive living and working environment within Highland Creek Village
- Create a sense of community pride and provide options for business investment within the area
- Promote attractive streetscapes through the co-ordination of sites, buildings and landscape design, on and between individual sites
- Encourage all development to provide a safe, attractive and comfortable pedestrian environment, including, where appropriate, the following characteristics:
  - Frequent and convenient pedestrian connections between buildings, transit stops, parking lots and the public sidewalk
  - A range of active and passive pedestrian spaces with coordinated street furniture, paving and planting design.

## HIGHLAND CREEK VILLAGE

### HIGHLAND CREEK VILLAGE URBAN DESIGN GUIDELINES

Highland Creek Village has an innate charm evident to residents and visitors alike.

The guidelines will:

- Reinforce a “village” environment based on historical reference
- Provide development that supports street related activities
- Ensure that buildings help define the public realm
- Allow flexibility in building forms and open spaces and also promote physical benefits for the Village as a whole
- Enhance the pedestrian experience within the Village environs
- Promote the creation of gateways to the neighbourhood at the intersections of Military Trail & Old Kingston Road, Kingston Road & Old Kingston Road, and the convergence of Military Trail, Morrish Road, and Kingston Road at Highway 2A.

### HIGHLAND CREEK VILLAGE DISTRICT

#### Site Planning

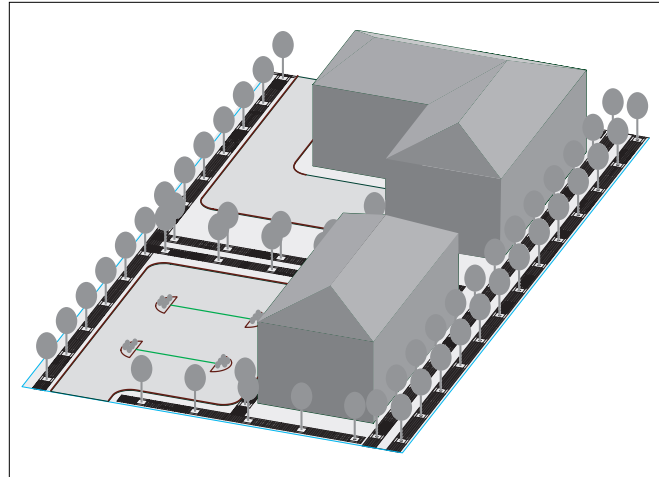
New development will be sited and organized to:

- Use existing streets for address and access
- Front onto public rather than private streets or lanes
- Provide cohesive building development along the street.

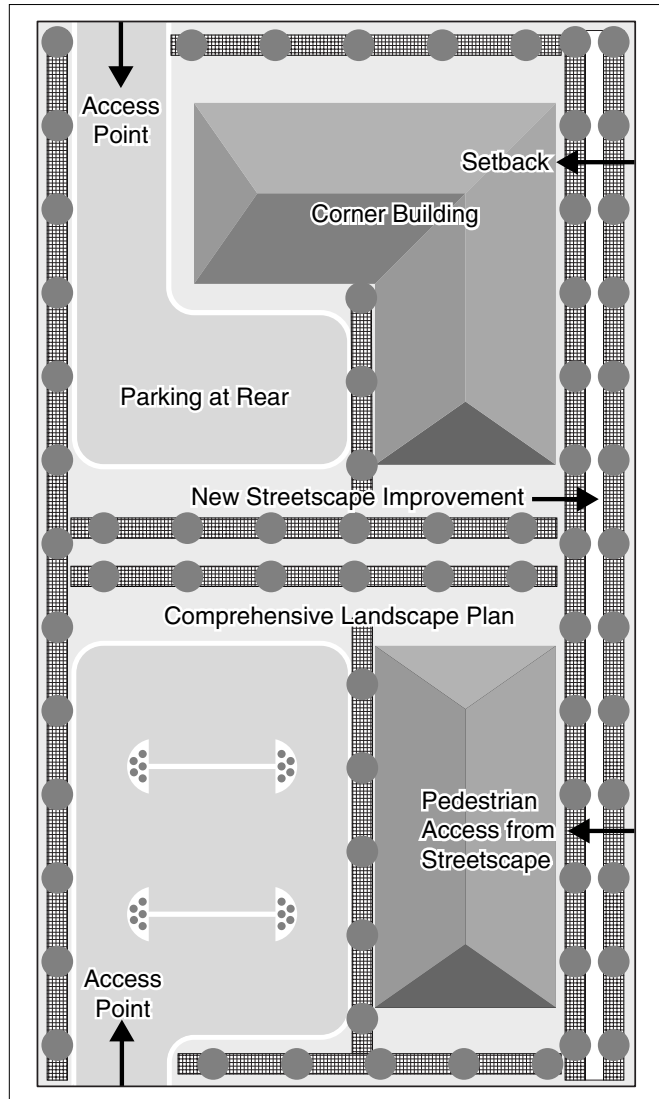
#### Access

New development will:

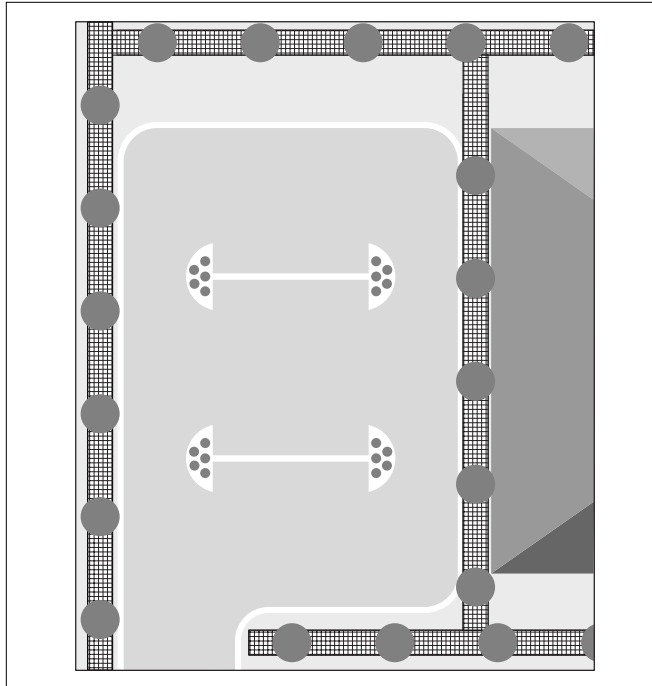
- Locate vehicular access points as far from intersections as possible
- Minimize the number of curb cuts along the City streets by promoting the sharing and linking of entrances and parking areas with adjacent properties
- Provide clear and safe pedestrian access from the curb to the new development.



Site Plan: Good Development



Site Plan: Streetscape & Servicing/Parking Configuration



Parking Lot Design

### Parking and Servicing

New development will provide:

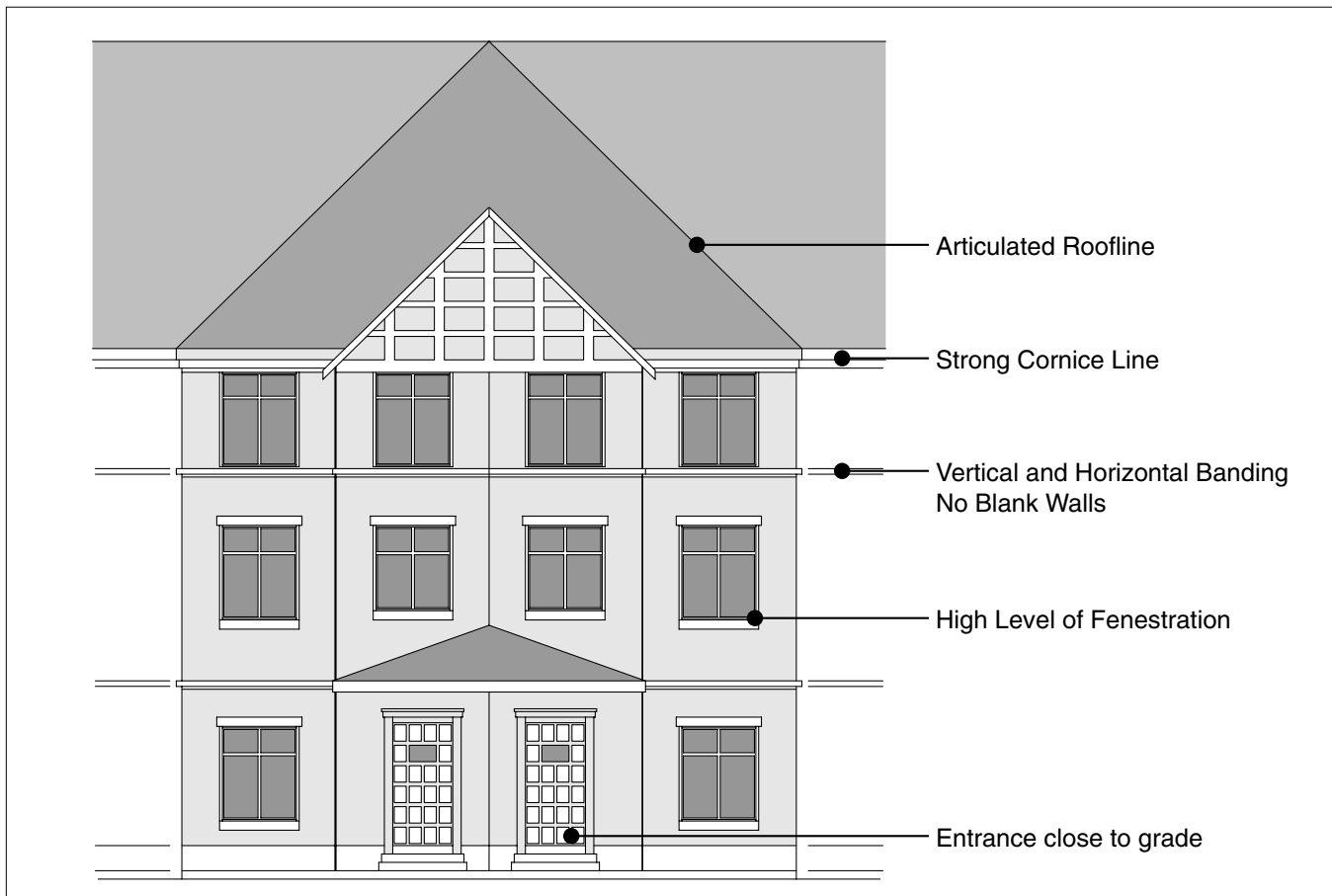
- Parking and servicing from the rear or side of the lot
- No parking facilities between the main façade of a building and the front property line; attractive screening, planting or low walls around all parking and servicing facilities
- Landscaped islands within parking facilities
- Signage that clearly distinguishes parking facilities from loading, delivery areas and driving lanes, and properly lit parking areas that do not impose on adjacent properties.

## HIGHLAND CREEK VILLAGE

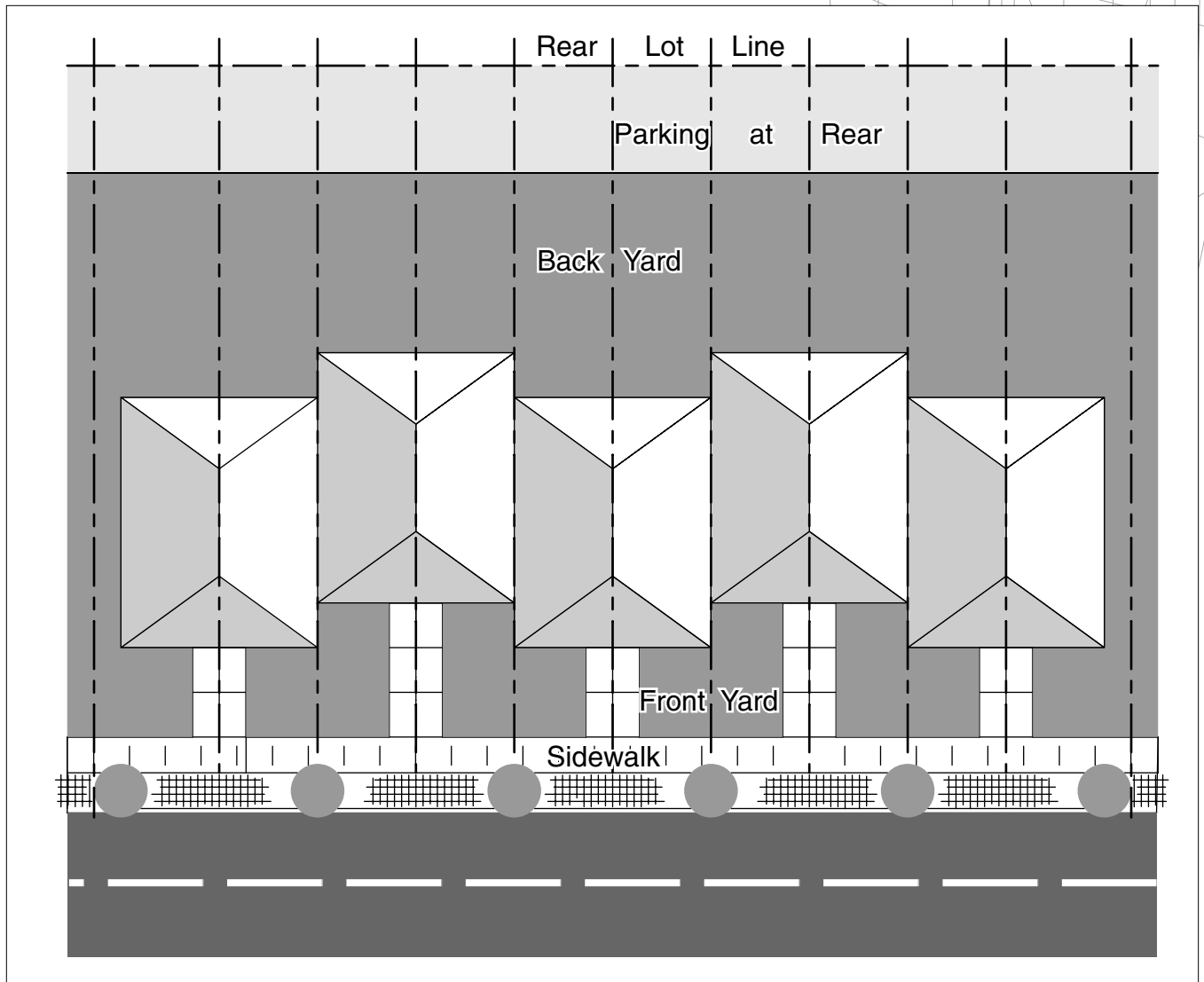
### Massing

New development will:

- Be architecturally interesting and consist of a clear base building, a strong cornice line and an articulated roof
- Contain banding or architectural treatment to add definition between storeys
- Consist of vertical and horizontal elements on the façade that repeat to give the street unity and coherence, have main entrances facing the street rather than from a parking facility
- Contain a proportionate solid wall-to-fenestration ratio
- Be located within a maximum distance of 15 m from the property line in order to create a consistent street presence.



Building Detail



Concept Site Plan

## Streetscape

The City encourages the landscaping of the public boulevards, in order to integrate private development with the design of the street and neighbouring properties.

New development will provide:

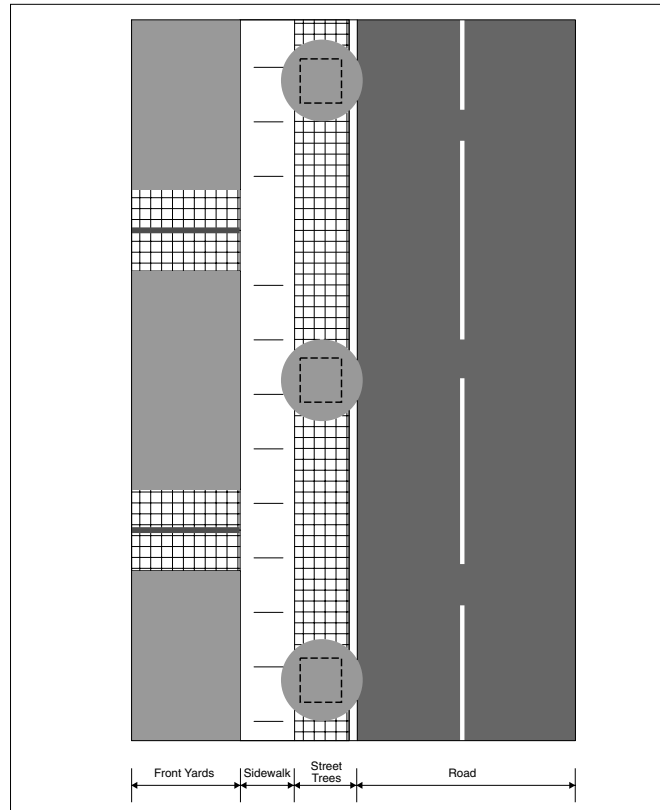
- New curbs and sidewalks in front of their sites
- Streetscape amenities such as paving materials, pedestrian scale lighting, street furniture, shelter and landscaping, which will be placed in attractive groupings, that allow for safe and unobstructed pedestrian movement
- Safe, convenient and well lit pedestrian walkways between building entrances, sidewalks and parking areas by means of minimal grade changes, curb cuts, ramps and railings, which will be well integrated into the overall design of the pedestrian area, and will protect pedestrians from vehicular intrusion by using landscaping and curbs or bollards.

## Open Space and Landscaping

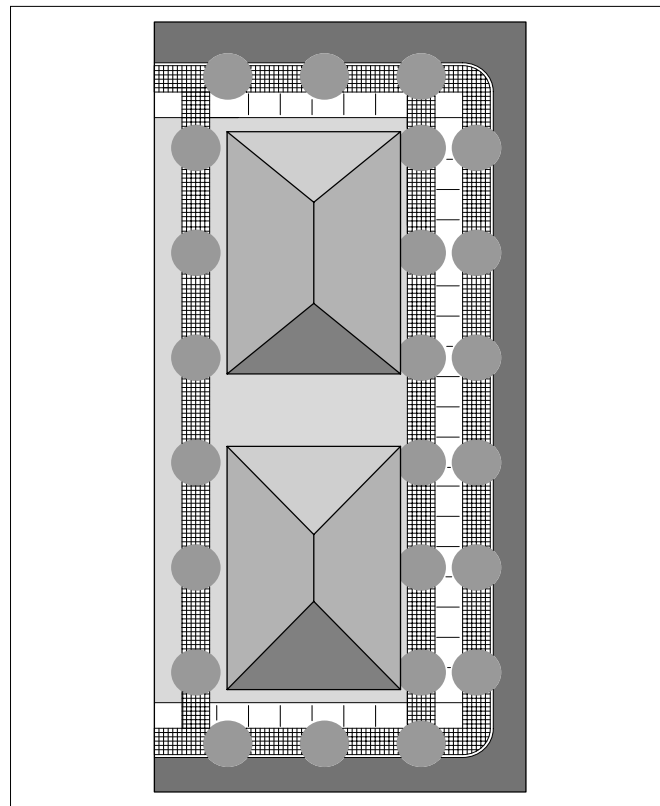
- Open space linkages will be strengthened.
- Opportunities to create gateways and public access routes will be encouraged and the public realm enhanced through appropriate landscaping, street furnishings, paving and public art.

New development will provide landscaping to:

- Add visual interest to open spaces and blank facades
- Soften building mass and promote a pedestrian scale
- Provide definition to public walkways and open areas
- Provide a consistent visual image between adjacent properties along the streetscape
- Screen unsightly areas.



Neighbourhood Streetscape



Block Development

## HIGHLAND CREEK VILLAGE

### HIGHLAND CREEK MAIN STREET AREA

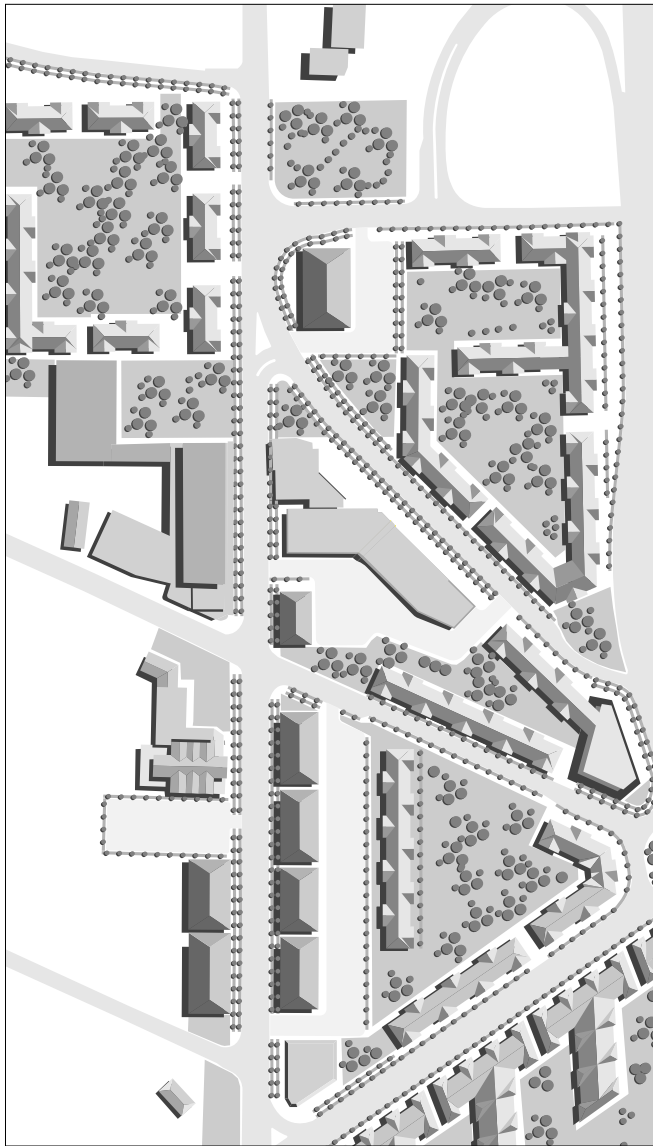
#### Enhance and Reinforce Character of the Village

The main street of Highland Creek Village is important to the neighbourhood.

- The continuation of commercial and retail uses will be encouraged and commercial amenities reinforced.
- New buildings will fit into the surrounding context with minimal negative impact.
- New buildings will be designed to create a strong “streetwall”.
- New building design will respect the existing two-storey base. Architectural materials, details and scale will respect the village context and historical reference.
- Any building higher than two storeys will maintain a strong base building condition, then establish a strong cornice line or setback of a minimum of 0.6 metres.
- Buildings will generally be no higher than 12 metres.
- “Main Street” development with a mix of residential and commercial uses is encouraged.

#### Building Elements

- New buildings will ensure a continuous retail edge, with retail uses at grade and residential above, directly accessible from the public sidewalk.
- Storefronts and entrances will be designed with particular regard for architectural details and materials, ratio of glazing to solid walls, lighting, signage type and location, canopies, types of doors and widths of retail units.
- The organization of elements such as structural bays, windows, doors and frames will be designed to provide visual interest to residents and pedestrians.
- Blank walls are not permitted in this area.
- Diagonal corner entrances of mixed use developments are encouraged in new buildings as long as they maintain the historic character of the area.



Main Street Plan

- One of the strong characteristics of this area is signage. As such, the main building entrances on new buildings will be demarcated with the appropriate signage, canopies and addresses.
- The renovation of any existing building or the erection of a new building will incorporate elements such as banners, signage, canopies, façade colours and materials that reflect the surrounding tone of the area.
- Special care will be taken to ensure that typical corporate images do not dominate or alter the existing character of the Village.
- All mechanical equipment will be screened from public view in order to enhance the pedestrian streetscape environment.

## Vehicular and Pedestrian Access

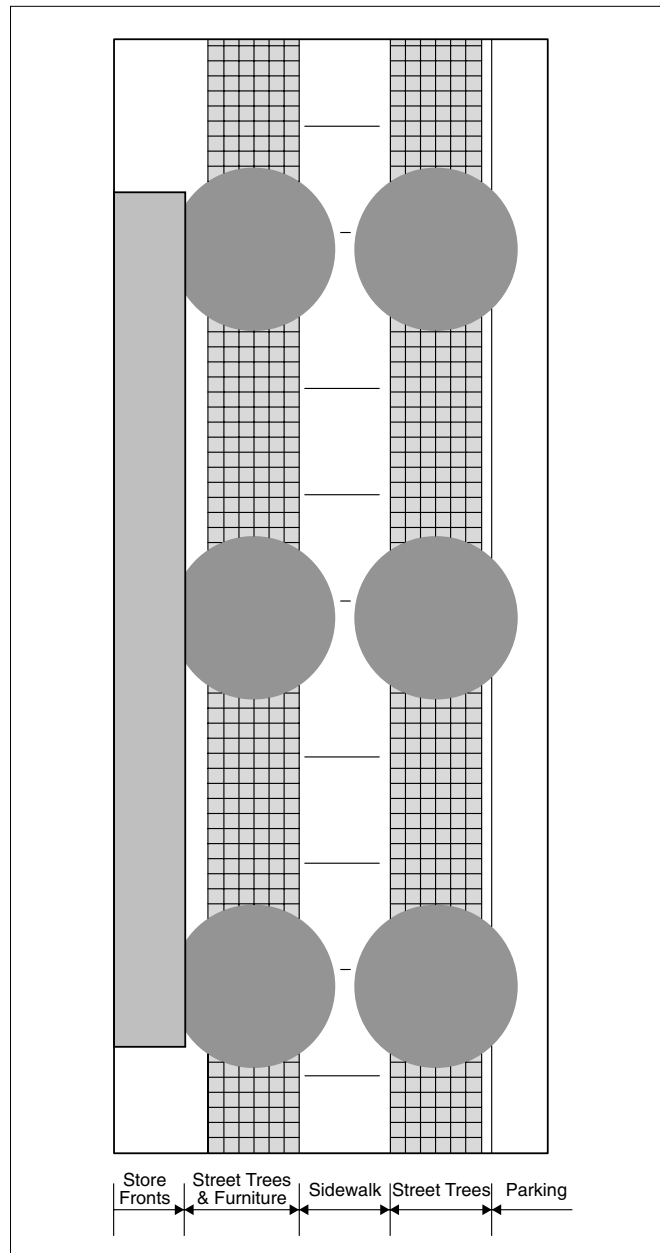
- Curb cuts in this area will be minimized. Vehicular entrances will be from the side or rear of the property in order to maintain a consistent unobstructed pedestrian streetscape.

## Streetscape and Public Amenities

- The design of building bases will integrate lighting, signage, planting, hanging elements, information boards and boxes, or other details which may add interest without clutter.
- Where on-street parking exists, new edge treatments are encouraged to separate the parking areas from the public sidewalk, (e.g., continuous planting strips, trees, street furniture, decorative sidewalk insets and curbs to clearly and safely define the public realm.)
- Gateway locations and unique features will be highlighted and enhanced with the incorporation of special landscape features such as decorative walls, fountains, planting, and local art or murals.

## Parking and Loading

- Parking and loading will be provided at the rear or side of the site.
- All buildings will be situated on the site to screen the public view of unsightly building elements such as shipping and loading areas, snow piling, transformers, meters and garbage areas.



Main Street Streetscape

## Enhancing Historic Buildings and Sites

- All buildings and landscaping adjacent to historic or heritage properties will be designed to complement and enhance the property's significant historical features. Landscape design will reflect the character of the site and the Village in a historical context. Architectural design will be complementary to the historical reference of existing buildings, without becoming imitative or false.



**LEGEND**

- MIXED USE
- EXISTING
- RESIDENTIAL

