
Section G.3:

Area A Heritage Impact Assessment, Dominion Public Building

Note: This appendix refers to Area A as Focus Area 1 and to Area B as Focus Area 2, a reflection of previous project nomenclature.



REP-059- WATERFRONT EAST LRT UNION STATION - QUEENS QUAY LINK

**Heritage Impact Assessment, Dominion Public
Building, 1 Front Street West,
City of Toronto, Ontario**
Project # OISO52004

Prepared for:

Toronto Transit Commission
1900 Yonge Street, Toronto, ON M4S 1Z2

OISO52004-TEM-001 R0
Waterfront East LRT Union Station -
Queens Quay Link Concept Design
Review Submission



Document Revision History

| Version | Date | Description |
|---------|---------------|--------------------------|
| R0 | July 23, 2021 | Released for TTC review. |
| | | |
| | | |
| | | |
| | | |
| | | |

| | | |
|--------------|---|---------------|
| Prepared by: | | Date: |
| | Chelsea Dickinson, BA, CAHP Cultural Heritage Specialist | July 26, 2021 |
| Reviewed by: | Heidy Schopf, MES, CAHP Cultural Heritage Team Lead | Date: |
| | | July 26, 2021 |
| | | |
| | | |



Executive Summary

The Toronto Transit Commission (TTC) is undertaking Preliminary Design and Engineering (PDE) to produce a Baseline Design (approximately 30% design completion) of a new TTC Waterfront East Light Rail Transit (WELRT) system (the Project). The WELRT, goes under Bay Street, from Front Street to Queens Quay West, and then heads easterly on Queens Quay West. A new portal on Queens Quay West between Bay Street and Yonge Street will be constructed to provide a transition from an underground Light Rail Transit (LRT) to an at-grade LRT, which will complement the existing west portal and at-grade LRT on Queens Quay West, west of Bay Street.

The WELRT will service Toronto's waterfront revitalization area by providing fast, reliable transit service in the East Bayfront (EBF) Area of the Waterfront. The expansion of the Union LRT and Queens Quay LRT Stations is required to accommodate the additional streetcar lines and passenger volume. This project is critical to the new waterfront transit plan in the EBF Precinct.

WSP E&I Canada (WSP), formerly Wood Environment and infrastructure was retained by TTC to complete a Heritage Impact Assessment (HIA) for the Dominion Public Building (1 Front Street West) in support of the WELRT. This HIA represents one deliverable to support program delivery. This HIA is being carried out under the Transit Project Assessment Process (TPAP) and was prepared in accordance with the MHSTCI guidance document titled *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties* (MCM 2017).

The purpose of this HIA is to establish the Statement of Cultural Heritage Value for the Dominion Public Building (1 Front Street West), assess the existing conditions of the property, describe the purpose of the proposed activity, complete an impact assessment and outline considered alternatives and mitigation measures, provide a summary of community engagement, and development recommendations for the conservation of the property

Based on the above, the following recommendations are made:

1) Avoidance:

- a. A new, below grade wall is proposed that will abut the southwest corner of the Dominion Public Building. While no direct alterations or demolition of the Dominion Public Building is anticipated, it is recommended that the project should be designed with a general avoidance approach in line with provincial and federal guidelines. Exploration of an alternative development approach to locate the new wall as far as possible from the southwest corner of the Dominion Public Building is recommended.

2) Protection Measures:

- a. The proposed work for the WELRT project is largely located below grade and involves open cut excavation, the demolition of the existing streetcar loop, and construction of the new loop/platform along Bay Street from



Front Street West to 40 Bay Street. Open cut construction will occur within the Dominion Public Building property and a new, below-grade wall will be constructed abutting the southeast corner of the building. Protection measures in the form of protective barriers and notes on construction drawings should be implemented during the construction phase of the project to protect this building. Protective fencing, film, or netting should be installed around the base of the Dominion Public Building in the vicinity of the proposed work to protect the exterior of the building from accidental damage during construction.

- b. The Dominion Public Building is designated under Part IV Ontario Heritage Act under By-Law 423-2017 and designated under Part V of the Ontario Heritage Act as part of the Union Station HCD through By-law 634-2006. The Dominion Public Building should be noted on project drawings as a “protected heritage property” to identify the heritage status of the building to project personnel. . The note should read:

“The Dominion Public Building is a significant heritage property with municipal protection under the *Ontario Heritage Act*. Use caution when conducting work in the vicinity of this building to avoid accidental damage”.
 - c. Construction work is proposed, within, and immediately adjacent to, the Dominion Public Building that introduces vibration-related risk. Vibration monitoring should be carried out by a qualified geotechnical engineer and should consist of a pre-construction survey, vibration monitoring during construction, and a post-construction survey.
- 3) This report should be submitted to the following agencies for review and comment:
- a. City of Toronto Heritage Preservation Services
 - b. MCM

The above recommendations were prepared using the best available information regarding potential impacts at the time of writing. Should the proposed work change, then the preliminary impact assessment should be revisited to confirm identified impacts and proposed mitigation measures.



Table of Contents

| | Page |
|---|-----------|
| Executive Summary | i |
| Table of Contents | iii |
| List of Appendices | iv |
| 1.0 Introduction..... | 1 |
| 1.1 Description and Location of the Property | 1 |
| 1.1.1 Heritage Recognition..... | 1 |
| 1.1.2 Property Ownership | 1 |
| 1.2 Proposed Undertaking and Purpose | 2 |
| 1.3 Potential Impacts to Cultural Heritage Resources | 6 |
| 1.4 Overview of Impacts and Mitigation Measures for the Dominion Public Building..... | 6 |
| 1.4.1 Summary of Potential Impacts | 6 |
| 1.4.2 Summary of Recommended Mitigation Measures..... | 7 |
| 2.0 Statement of Cultural Heritage Value | 11 |
| 2.1 Dominion Public Building (1 Front Street West)..... | 11 |
| 2.1.1 Description of Property..... | 11 |
| 2.1.2 Statement of Cultural Heritage Value..... | 11 |
| 2.1.3 Heritage Attributes | 12 |
| 2.2 Union Station Heritage Conservation District..... | 14 |
| 3.0 Assessment of Existing Conditions | 17 |
| 3.1 Property Context..... | 17 |
| 3.2 Dominion Public Building | 24 |
| 3.3 Area of Proposed Intervention | 33 |
| 4.0 Description and Purpose of Proposed Activity | 38 |
| 5.0 Impact Assessment..... | 41 |
| 5.1 Potential Impacts to Dominion Public Building..... | 41 |
| 5.2 Potential Impacts to Union Station HCD | 45 |
| 5.3 Discussion of Anticipated Impacts | 45 |
| 6.0 Considered Alternatives and Mitigation Measures..... | 47 |
| 6.1 Conservation Guidance | 49 |
| 6.2 Discussion of Mitigation Measures | 51 |
| 6.2.1 Avoidance | 51 |
| 6.2.2 Protection Measures | 51 |
| 7.0 Summary of Community Engagement..... | 53 |
| 8.0 Recommendations | 59 |
| 9.0 Assessor Qualifications..... | 61 |
| 10.0 Closure | 62 |
| 11.0 References | 64 |



List of Figures

| | |
|---|----|
| Figure 1: Location of the Study Area | 9 |
| Figure 2: Aerial Photograph Showing Location of the Study Area | 10 |
| Figure 3: Site Context of the Dominion Public Building, 1 Front Street West, City of Toronto, Ontario | 37 |

List of Tables

| | |
|---|----|
| Table 1: Assessment of Potential Direct Adverse Impacts | 41 |
| Table 2: Assessment of Potential Indirect Impacts | 44 |
| Table 3: Assessment of Potential Positive Impacts | 44 |
| Table 4: Mitigation Measures | 47 |
| Table 5: Summary of Community Engagement | 54 |

List of Appendices

| | |
|---|--|
| Appendix A: Designation By-law for the Dominion Public Building | |
| Appendix B: Proposed Work | |
| Appendix C: Conservation Guidelines | |
| Appendix D: Record of Community Engagement | |
| Appendix E: Assessor Qualifications | |
| Appendix F: Limitations | |



| List of Acronyms and Abbreviations | |
|------------------------------------|---|
| BHR | Built Heritage Resource |
| CHL | Cultural Heritage Landscape |
| CHR | Cultural Heritage Report: Existing Conditions and Impact Assessment |
| CHVI | Cultural Heritage Value or Interest |
| HCD | Heritage Conservation District |
| HIA | Heritage Impact Assessment |
| MCM | Ministry of Citizenship and Multiculturalism |
| O. Reg. | Ontario Regulation |
| PPS | Provincial Policy Statement |
| TTC | Toronto Transit Commission |
| WSP | WSP E&I Canada Limited |

| Glossary | |
|-------------------------|---|
| Adjacent lands | Those lands contiguous to a protected heritage property or as otherwise defined in the municipal official plan (Government of Ontario 2020). |
| Built Heritage Resource | Means a building, structure, monument, installation or any manufactured or constructed part or remnant that contributes to a property's cultural heritage value or interest as identified by a community, including an Indigenous community. Built heritage resources are located on property that may be designated under Parts IV or V of the <i>Ontario Heritage Act</i> , or that may be included on local, provincial, federal and/or international registers (Government of Ontario 2020). |
| Conserved | Means the identification, protection, management and use of built heritage resources, cultural heritage landscapes and archaeological resources in a manner that ensures their cultural heritage value or interest is retained. This may be achieved by the implementation of recommendations set out in a conservation plan, archaeological assessment, and/or heritage impact assessment that has been approved, accepted or adopted by the relevant planning authority and/or decision maker. Mitigative measures and/or alternative development approaches can be included in these plans and assessments (Government of Ontario 2020). |



| Glossary | |
|---|--|
| Cultural Heritage Landscape | Means a defined geographical area that may have been modified by human activity and is identified as having cultural heritage value or interest by a community, including an Indigenous community. The area may include features such as buildings, structures, spaces, views, archaeological sites or natural elements that are valued together for their interrelationship, meaning or association. Cultural heritage landscapes may be properties that have been determined to have cultural heritage value or interest under the <i>Ontario Heritage Act</i> , or have been included on federal and/or international registers, and/or protected through official plan, zoning by-law, or other land use planning mechanisms (Government of Ontario 2020). |
| Heritage Attributes | Means the principal features or elements that contribute to a protected heritage property's cultural heritage value or interest, and may include the property's built, constructed, or manufactured elements, as well as natural landforms, vegetation, water features, and its visual setting (e.g. significant views or vistas to or from a protected heritage property) (Government of Ontario 2020). |
| Protected Heritage Property | Means property designated under Parts IV, V or VI of the <i>Ontario Heritage Act</i> ; property subject to a heritage conservation easement under Parts II or IV of the <i>Ontario Heritage Act</i> ; property identified by the Province and prescribed public bodies as provincial heritage property under the Standards and Guidelines for Conservation of Provincial Heritage Properties; property protected under federal legislation, and UNESCO World Heritage Sites Government of Ontario 2020). |
| Provincial Heritage Property | Means real property, including buildings and structures on the property, that has cultural heritage value or interest and that is owned by the Crown in right of Ontario or by a prescribed body; or that is occupied by a ministry or prescribed body if the terms of the occupancy agreement are such that the ministry or public body is entitled to make the alterations to the property that may be required under these heritage standards and guidelines (Standards and Guidelines for Conservation of Provincial Heritage Properties 2020). |
| Provincial Heritage Property of Provincial Significance | Means provincial heritage property that has been evaluated using the criteria found in Ontario Heritage Act O. Reg. 10/06 and has been found to have cultural heritage value or interest of provincial significance (MCM 2017). |



| Glossary | |
|--------------------------------------|--|
| Significant | In regard to cultural heritage and archaeology, resources that have been determined to have cultural heritage value or interest. Processes and criteria for determining cultural heritage value or interest are established by the Province under the authority of the <i>Ontario Heritage Act</i> (Government of Ontario 2020). |
| Statement of Cultural Heritage Value | Means a concise statement explaining why a property is of heritage interest; this statement should reflect one or more of the criteria found in <i>Ontario Heritage Act</i> O. Regs. 9/06 and 10/06 (MCM 2017). |



1.0 Introduction

1.1 Description and Location of the Property

WSP Environment and Infrastructure (WSP) was retained by the Toronto Transit Commission (TTC) to conduct a Heritage Impact Assessment (HIA) for the Dominion Public Building (1 Front Street West) (the Study Area) as part of Preliminary Design and Engineering (PDE) to produce a Baseline Design (approximately 30% design completion) of a new TTC Waterfront East Light Rail Transit (WELRT) system (the Project). The Study Area is depicted in **Figure 1** and **Figure 2**.

Anchoring the southwest corner of Yonge Street and Front Street West, the Dominion Public Building is a large-scale former federal government building that was commissioned by the Government of Canada's Department of Public Works and originally served as Toronto's Custom's House. Completed in two phases in 1929-31 (centre and east pavilions) and 1934-35 (west pavilion), the north section of the building on Front Street West rises five stories, while the rear section extends six stories to address the change in grade.

As the third Customs House on the site, the Dominion Public Building occupies what was historically among the most sought-after locations in Toronto with its proximity to the city's financial district directly north, Union Station as its neighbor on the west, and the railway corridor and harbour to the south. The Dominion Public Building, with its office and warehouse components aligned to access Front, Yonge and Bay streets, forming an important precinct. The building has cultural heritage value on associative grounds, design grounds, and contextual grounds.

The Dominion Public Building is located within the Union Station Heritage Conservation District (HCD) and is identified as a contributing property within the HCD Plan.

1.1.1 Heritage Recognition

The Dominion Public Building has known Cultural Heritage Value or Interest (CHVI) and is designated at the municipal level:

- Municipal Recognition
 - Designated under Part V of the *Ontario Heritage Act* as a contributing building within the Union Station HCD under By-Law 634-2006
 - Individually Designated under Part IV of the *Ontario Heritage Act* under By-Law 423-2017

1.1.2 Property Ownership

The Dominion Public Building (1 Front Street West; PIN 213960116) is owned by 1 Front Street West Holdings Ltd. The property includes the building itself and parking at the rear. This property was formerly owned by Public Service and Procurement Canada (PSPC) but was disposed to Canada Lands Company in 2017.

1.2 Proposed Undertaking and Purpose

The TTC is undertaking Preliminary Design and Engineering (PDE) to produce a Baseline Design (approximately 30% design completion) of a new TTC Waterfront East Light Rail Transit (WELRT) system (the Project). The WELRT, goes under Bay Street, from Front Street to Queens Quay West, and then heads easterly on Queens Quay West. A new portal on Queens Quay West between Bay Street and Yonge Street will be constructed to provide a transition from an underground Light Rail Transit (LRT) to an at-grade LRT, which will complement the existing west portal and at-grade LRT on Queens Quay West, west of Bay Street.

The WELRT will service Toronto's waterfront revitalization area by providing fast, reliable transit service in the East Bayfront (EBF) Area of the Waterfront (**Plate 1**). The expansion of the Union LRT and Queens Quay LRT Stations is required to accommodate the additional streetcar lines and passenger volume. This project is critical to the new waterfront transit plan in the EBF Precinct.

The planning for the Project began in 2010 when the East Bayfront Transit Class Environmental Assessment (EA) Study was carried by Waterfront Toronto, who is the proponent for all redevelopment activities in the East Bayfront Area.



Plate 1: Overview of Waterfront Transit Network

Please reach out to the Project Team should you require alternative text for this image.

The Consultant, MRC, produced the draft Environmental Study Report in August 2009. The Engineering Department of TTC carried out the Conceptual 10% Design of the project, producing a final Conceptual Design Report in February 2010. The project has progressed to the current Preliminary Design and Engineering stage.



The overall scope of work to be completed for the Project includes, but is not limited to, Focus Area 1 and Focus Area 2. An overview of these Project Areas is provided in **Plate 2**.

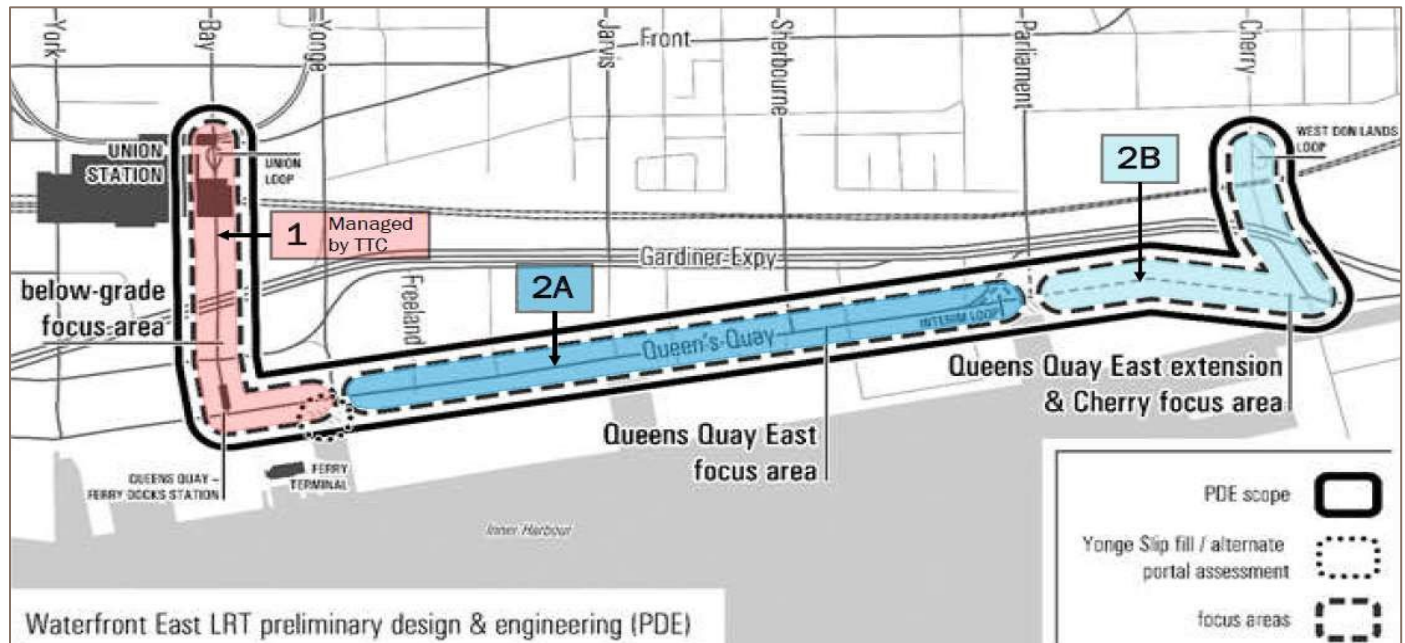


Plate 2: Focus Area 1 and Focus Area 2

Please reach out to the Project Team should you require alternative text for this image.

1. Focus Area 1 - Managed by TTC - Below Grade (Union Station Loop to future Portal east of Bay Street on Queens Quay), which includes:
 - a. Union LRT Station Expansion, including new crossover tracks; Queen Quay LRT Station Expansion;
 - b. New Streetcar tunnel and portal structures along Queens Quay between Bay Street and Yonge Street; and
 - c. Track works within the tunnel and portal structures.
2. Focus Area 2 - Managed by Waterfront Toronto:
 - a. 2A: Queens Quay East (Future Portal to Parliament vicinity ancillary Queens Quay surface/public realm between Bay & future portal).
 - b. 2B (Provisional): Queens Quay East Extension & Cherry (Parliament vicinity to West Don Lands Loop).

WSP Environment & Infrastructure Solutions, a division of WSP Canada Limited (WSP) scope of work pertains to Focus Area 1 only and includes a collaborative effort among the City of Toronto, the TTC, and Waterfront Toronto. WSP's overall scope of work includes PDE services to provide a baseline design (30% design), a level 3 cost estimate for the expansion of the existing Union LRT and Queens Quay LRT Stations,



and new running tunnel and portal as part of WELRT project. In particular, the main scope items include:

1. Union Station LRT Loop Expansion to accommodate up to four (4) new platforms, including new crossover tracks;
2. Queens Quay Station Expansion with up to two (2) extended platforms;
3. New streetcar tunnel and portal structures along Queens Quay between Bay Street and Yonge Street;
4. Track works within the tunnel and portal structures; and
5. Design interface and coordination with the work of Focus Area 2 and adjacent projects (public and private) along project limits.

Subject to further funding approval and a procurement options analysis, a contract amendment may be issued to extend the term of the contract and the consultant may be requested to carry out the detailed design and construction support services or develop Reference Concept Design (RCD) and Project Specific Output Specifications (PSOS) for this project.

The phases of WSP's overall scope of work are as follows:

1. Phase 1 – Work Plan (OISO52004-PLN-001 Phase 2 Work Plan);
2. Phase 2a - Concept Design Review Submission (CDRS) (approximately 15%); and,
3. Phase 2b - Baseline Design Review Submission (BDRS) (approximately 30%).

From the layouts indicated in Appendix B of the *City of Toronto, Union Station - Queens Quay Transit Link Study Final Report* (April 2019), further developments were made to progress the design to consider the latest requirements and current standards. A summary of the current station layouts are provided in **Plate 3** and **Plate 4**.

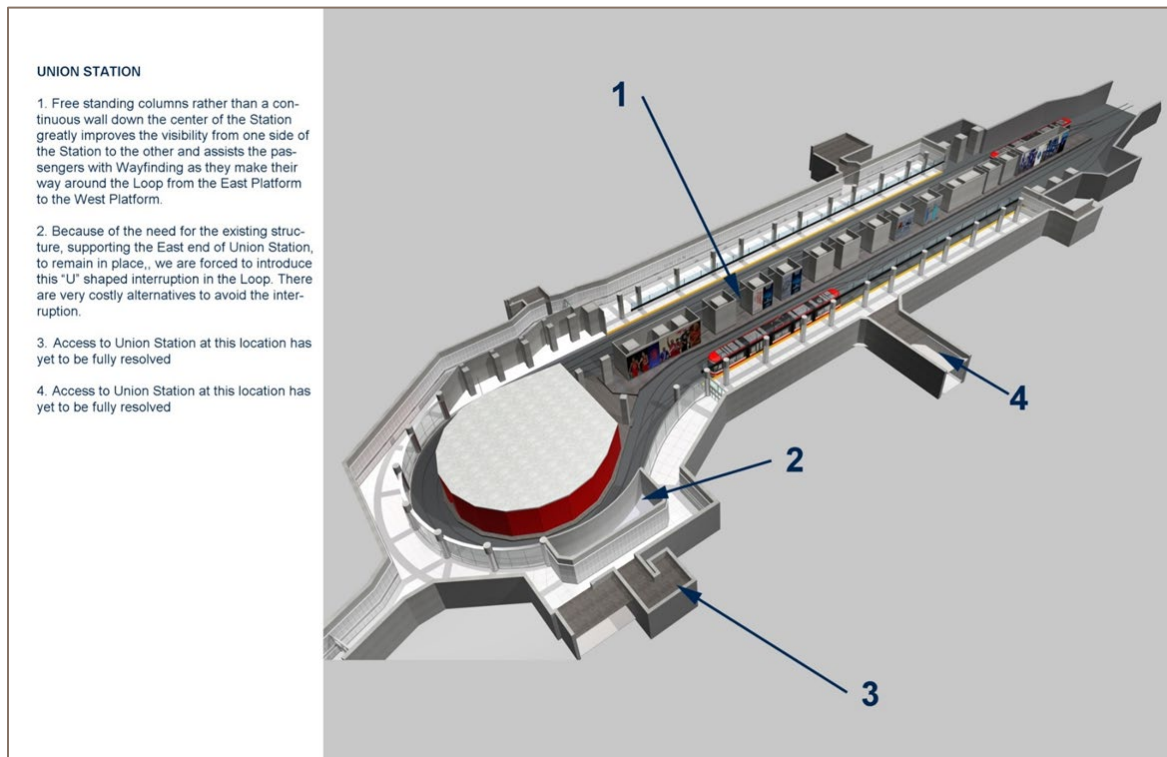


Plate 3: Union Station -CDRS Proposed Configuration

Please reach out to the Project Team should you require alternative text for this image.

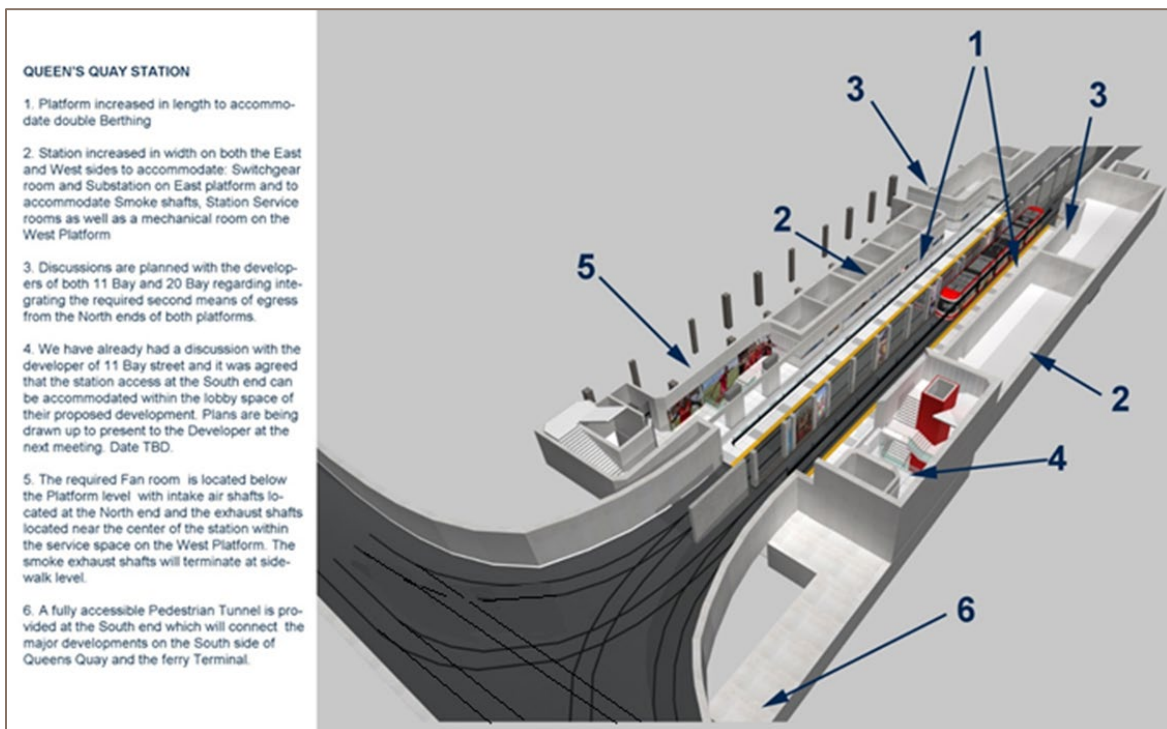


Plate 4: Queens Quay Station - CDRS Proposed Configuration

Please reach out to the Project Team should you require alternative text for this image.



1.3 Potential Impacts to Cultural Heritage Resources

As described above, the project consists of the design of a new TTC WELRT system that goes under Bay Street, from Front Street to Queens Quay West, and then heads easterly on Queens Quay West. A new portal on Queens Quay West between Bay Street and Yonge Street will be constructed to provide a transition from an underground LRT to an at-grade LRT, which will complement the existing west portal and at-grade LRT on Queens Quay West, west of Bay Street.

The proposed WELRT is located within the Union Station HCD and adjacent to, or in close proximity to, an additional 13 known and potential cultural heritage properties. A *Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment* (CHR) prepared for this project determined that direct impacts are anticipated to Union Station Complex (65-71 Front Street West), the Dominion Public Building (1 Front Street West), the former Postal Delivery Building (40 Bay Street), and the public realm of the Union Station HCD. Presently, Strategic Conservation Plans (SCP) are not available for these properties.

Given that there is proposed work within these heritage properties and SCPs are not available, HIAs are required. The purpose of the HIAs is to inform decisions that may affect the property while ensuring the conservation and protection of heritage attributes.

This report consists of the HIA for the Dominion Public Building (1 Front Street West). HIAs have been prepared for the Union Station Complex (65-71 Front Street West), the former Postal Delivery Building (40 Bay Street), and the Union Station HCD have been prepared under separate covers. All four HIAs has been prepared in accordance with *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties* (MCM 2017).

1.4 Overview of Impacts and Mitigation Measures for the Dominion Public Building

A detailed impact assessment was prepared as part of this HIA. The impact assessment is presented in Section 5.0 and mitigation measures are presented in Section 6.0. An overview of the findings is presented below. The detailed impact assessment determined that two potential direct adverse impacts and one potential indirect adverse impact are anticipated as part of the proposed work.

1.4.1 Summary of Potential Impacts

Construction drawings for the proposed work depict property acquisition, open cut construction, and a new wall for the WELRT streetcar loop abutting the southwest corner of the Dominion Public Building and the construction of a new curb within the parking lot at the rear of the Dominion Public Building (south elevation). This work will result in land disturbance, alterations to the property for access requirements, and the introduction of new elements below grade.

Construction activity that introduces vibration-related risk is planned within the property related to the construction of a new wall directly abutting the Dominion Public Building.



1.4.2 Summary of Recommended Mitigation Measures

Mitigation measures are proposed to address impacts resulting from the proposed work to the Dominion Public Building. These include:

1) Avoidance:

- a. A new, below grade wall is proposed that will abut the southwest corner of the Dominion Public Building. While no direct alterations or demolition of the Dominion Public Building is anticipated, it is recommended that the project should be designed with a general avoidance approach in line with provincial and federal guidelines. Exploration of an alternative development approach to locate the new wall as far as possible from the southwest corner of the Dominion Public Building is recommended.

2) Protection Measures:

- a. The proposed work for the WELRT project is largely located below grade and involves open cut excavation, the demolition of the existing streetcar loop, and construction of the new loop/platform along Bay Street from Front Street West to 40 Bay Street. Open cut construction will occur within the Dominion Public Building property and a new, below-grade wall will be constructed abutting the southeast corner of the building. Protection measures in the form of protective barriers and notes on construction drawings should be implemented during the construction phase of the project to protect this building. Protective fencing, film, or netting should be installed around the base of the Dominion Public Building in the vicinity of the proposed work to protect the exterior of the building from accidental damage during construction.
- b. The Dominion Public Building is designated under Part IV *Ontario Heritage Act* under By-Law 423-2017 and designated under Part V of the *Ontario Heritage Act* as part of the Union Station HCD through By-law 634-2006. This Dominion Public Building should be noted on project drawings as a “protected heritage property” to identify the heritage status of the building to project personnel. The note should read:

“The Dominion Public Building is a significant heritage property with municipal protection under the *Ontario Heritage Act*. Use caution when conducting work in the vicinity of this building to avoid accidental damage”.
- c. Construction work is proposed, within, and immediately adjacent to, the Dominion Public Building that introduces vibration-related risk. Vibration monitoring should be carried out by a qualified geotechnical engineer and should consist of a pre-construction survey, vibration monitoring during construction, and a post-construction survey.



- 3) This report should be submitted to the following agencies for review and comment:
 - a. City of Toronto Heritage Preservation Services
 - b. MCM

The Dominion Public Building is a contributing building in the Union Station HCD. At grade work is anticipated for the construction of a new curb at the rear of the building (south elevation). However, the proposed curb is directly tied to the public realm (i.e. sidewalk) along Bay Street. A standalone HIA for the Union Station HCD to address these impacts has been prepared by WSP under a separate cover.

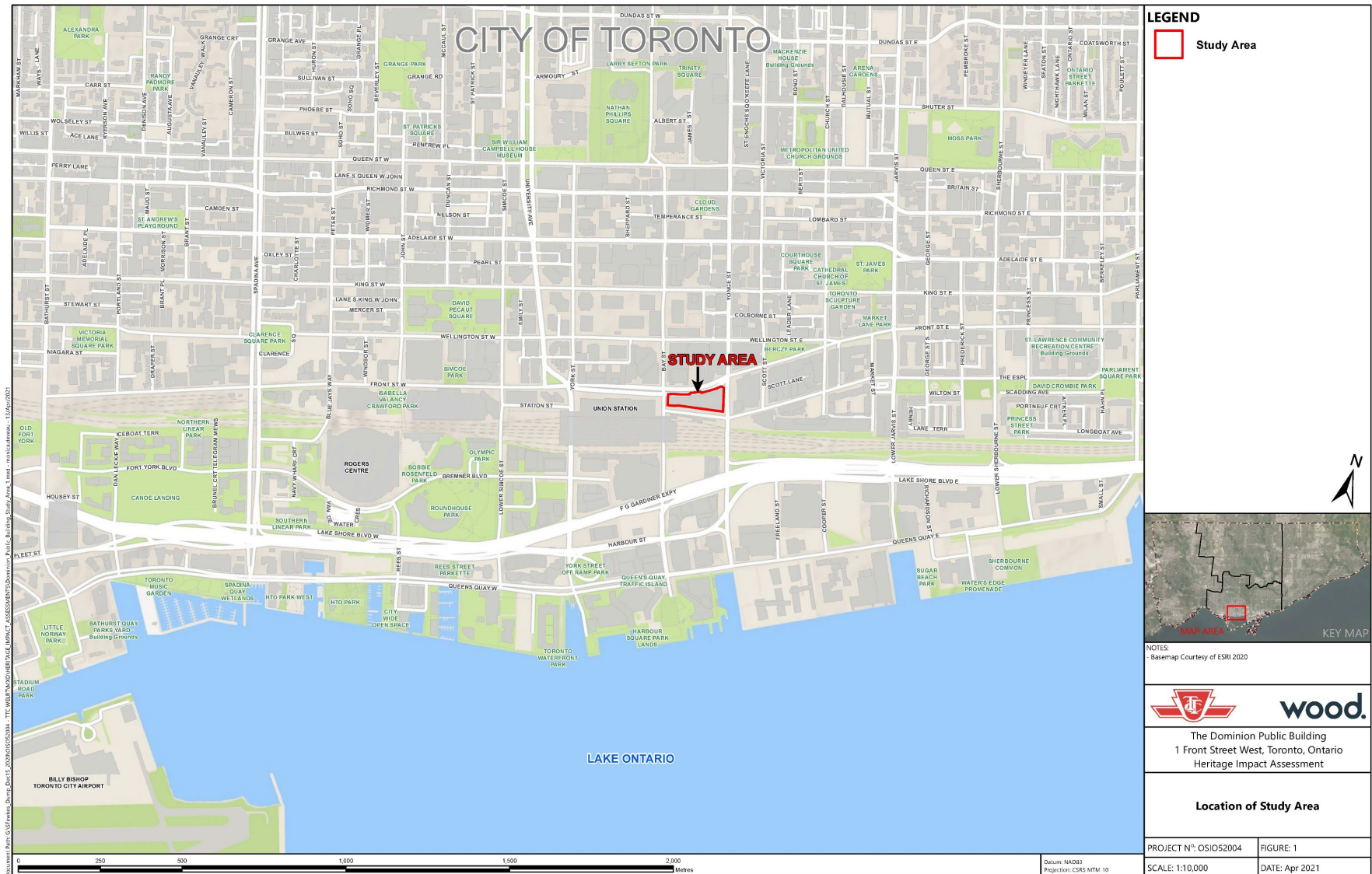


Figure 1: Location of the Study Area

Please reach out to the Project Team should you require alternative text for this image.



Figure 2: Aerial Photograph Showing Location of the Study Area
Please reach out to the Project Team should you require alternative text for this image.



2.0 Statement of Cultural Heritage Value

The Dominion Public Building (1 Front Street West) is designated under Part IV of the *Ontario Heritage Act* through By-law 423-2017 and under Part V of the *Ontario Heritage Act* as a 'contributing building' in the Union Station HCD under By-law 623-2006. The following Statement of Cultural Heritage Value and list of heritage attributes is taken from By-law 423-2017. A copy of the full By-law is provided in Appendix A.

2.1 Dominion Public Building (1 Front Street West)

2.1.1 Description of Property

Anchoring the southwest corner of Yonge Street and Front Street West, the Dominion Public Building is a large-scale former federal government building that was commissioned by the Government of Canada's Department of Public Works and originally served as Toronto's Custom's House. Completed in two phases in 1929-31 (centre and east pavilions) and 1934-35 (west pavilion), the north section of the building on Front Street West rises five stories, while the rear section extends six stories to address the change in grade.

2.1.2 Statement of Cultural Heritage Value

The Dominion Public Building has cultural heritage value for its role as the federal government's Toronto Customs House for the administration, taxation, inspection and storage of imported and exported goods. Conceived by the federal government as a Customs House only, the Dominion Public Building was completed as the third and largest Customs House in the city, as well as the first to incorporate the public offices and the examining warehouse in the same building. The construction of the Dominion Public Building during the Great Depression of the 1930s was a reflection of the significance of Toronto to the nation's economic status and recovery.

The associative value of the Dominion Public Building is also through its connection to T.W. Fuller, who served from 1927 to 1936 as the Chief Architect of the Federal Department of Public Works, which was responsible for the in-house design of nearly all public architecture in Canada in the late 19th and early 20th centuries. During the Great Depression, the Chief Architect oversaw monumental projects in the country's major centres, including Toronto where the Dominion Public Building remains Fuller's best-known work.

From a design standpoint, the Dominion Public Building is valued as a rare and exceptional example in Canada of Beaux-Arts Classicism, the international style popularized for monumental architecture in the late 18th and early 19th centuries. The Dominion Public Building has the hallmarks of the style with its imposing scale, the symmetrical organization of the facades, the hierarchy of spaces from grand to utilitarian, the decorative detailing inspired by precedents, and its placement in a highly visible and prominent setting. As a rare and early surviving example of a public building in Toronto constructed by the federal government in the early 20th century, the Dominion Public Building was among the first applications of Beaux Arts Classicism to a federal design. The Dominion Public Building stands as an important physical reminder



of the imposing public spaces created by the federal government, combining the monumentality and grandeur of the Front Street portion of the building (including the interior Long Room) with the practicality and accessibility of the warehouse component to the rear. Designed in two phases, over time the Dominion Public Building changed from a Customs House to a multi-use federal building, and the interior alterations dating to the 1980s and 1990s are part of the evolution of the building.

With the neighboring Union Station (which was officially opened in 1927), the Dominion Public Building establishes the character of the area along Front Street, west of Yonge Street. Following the Great Fire of 1904 that destroyed most of the existing buildings in this area, Toronto's Civic Improvement Committee commissioned a plan (1911) by architect John M. Lyle that was based on the principles of the City Beautiful Movement and that envisioned Front Street as a grand boulevard with expansive tracts reserved for monumental architecture that included a new Union Station and Customs House (the Dominion Public Building). According to the federal government, "The Dominion Public Building and Union Station together form probably the most imposing Beaux Arts streetscape in Canada."

Contextually, the Dominion Public Building is historically, visually, physically and functionally linked to its important setting on the south side of Front Street where it anchors the southwest corner of Yonge Street (Toronto's "main street") and extends across the entire city block to Bay Street. As the third Customs House on the site, the Dominion Public Building occupies what was historically among the most sought-after locations in Toronto with its proximity to the city's financial district directly north, Union Station as its neighbor on the west, and the railway corridor and harbour to the south. The Dominion Public Building, with its office and warehouse components aligned to access Front, Yonge and Bay streets, forms an important precinct.

As a monumental federal government building in a prominent location beside the complementing Union Station, the Dominion Building is a local landmark.

2.1.3 Heritage Attributes

The heritage attributes of the building known historically as the Dominion Public Building on the property at 1 Front Street West are:

- The placement, setback and orientation of the building on the south side of Front Street West where it extends from Yonge Street to Bay Street
- The scale, form and massing of the irregularly shaped plan that rises five stories along Front Street to the flat roofline and follows the curve of Front Street west of Yonge Street
- The partially raised stone base with window openings, which extended in height where the changes south of Front Street
- On the reinforced concrete structure and above the granite clad foundation, the limestone cladding that is channeled on the extended first (ground) floor and smoothly dressed in the stories above, with stone and metal detailing



- The tripartite organization of the north elevation on Front Street into the centre pavilion and the adjoining east and west pavilions, with the west pavilion rounded at the northwest corner and the east pavilion truncated at the northeast corner
- The horizontal division of the north elevation by the cornices above the extended first story and beneath the parapet
- The centre pavilion, with five-story projecting frontispiece composed of six freestanding Ionic columns supporting the entablature inscribed “Dominion Public Building A.D. MCMXXX”
- At the base of the centre frontispiece, the two-story main entrance where the three round-arched openings contain paired bronze doors beneath large transoms with metal mullions incorporating rope detailing and cast metal beavers (as symbols of Canada)
- The single secondary entrances on the north elevations of the east and west pavilions that repeat the detailing introduced on the central entrance. The east elevation on Yonge Street, which extends 12 bays and is divided into three parts with a recessed centre section
- The five-bay west elevation facing Bay Street
- The fenestration on the north, east and west elevations, which is arranged between Ionic pilasters and features double-height round arched openings with keystones in the first floor, and single, paired and triple flat-headed openings with spandrel panels in the remaining floors, and the metal window mullions with the rope detailing in the first and second floors
- The classical detailing on the elevations facing Front, Yonge and Bay streets, including the carved acanthus leaves, the dentils and the stone lion’s heads
- On the rear (south) elevation, the end bays (east and west) that continue the decorative detailing and fenestration from the east and west elevations
- The central utilitarian section of the south wall with fenestration and raised centre section
- On the interior, the organization and layout of the public spaces on the first floor, including the three marble-clad lobbies
- The detailing in the east vestibule and lobby, with the marble floors, dado and door and window surrounds, including the marble door pediment inscribed “Long Room”, the bronze window mullions between the lobby and the Long Room, the “enquiry” window in the lobby, and the classical detailing
- In the east pavilion, the two-story Long Room, with the marble floors, dado and door and window and surrounds, the marble counters with brass wickets, the double row of square columns with pilasters and the Corinthian capitals, the entrances with the bronze doors and classical detailing, the metal window mullions, the second-story



gallery with brass balustrade, and the coffered plaster ceiling with the dentils and mouldings.

2.2 Union Station Heritage Conservation District

The Dominion Public Building falls within the Union Station HCD and is identified as a 'contributing building' of the district. The Union Station Heritage Conservation District (HCD) was designated in 2006 through [By-law 634-2006](#) (City of Toronto 2006; Ontario Heritage Trust 2020). The Union Station HCD is located between Toronto's Financial District, Entertainment District, historical St. Lawrence Neighborhood, and the post-industrial waterfront. The Union Station HCD Plan is available [online](#) and includes an in depth analysis of the history, heritage character, district policies, municipal policies, implementation guidance, and design guidelines (ERA Architects Inc 2006).

The boundaries of the Union Station HCD coincide with Wellington Street West to the north, Yonge Street to the east, and Lakeshore Boulevard West/Harbour Street to the south. The east boundary of the HCD is defined by Simcoe Street north of the rail corridor and Reese Street south of the rail corridor. The Union Station HCD consists of an assemblage of buildings, open spaces and streets that have a collective interdependent history. Intersecting development and planning initiatives associated with Toronto's railway lands, waterfront and central business district, historic and monumental architecture; as well as physical patterns of interrelated function are the key heritage attributes that comprise the heritage character of the HCD (ERA Architects Inc. 2006).

The Union Station HCD Plan contains design guidelines for contributing buildings, non-contributing buildings, new construction, adjacent properties, and the public realm (ERA Architects Inc. 2006: 57-65).

The HCD Plan describes contributing buildings as properties that contribute to the character of the district and/or are historically, architecturally or culturally significant as identified in the heritage evaluation or determined by further evaluation. Non-contributing buildings are defined as properties that do not contribute to the character of the district and/or are not historically, architecturally or culturally significant as identified in the heritage evaluation or determined by further evaluation (ERA Architects Inc. 2006: 58).

This proposed project scope includes anticipated alteration to the Dominion Public building property and adjacent public realm. Applicable sections of the HCD Plan are provided below.

2.2.1.1 Union Station HCD Guidelines for Contributing Buildings

Design guidelines for contributing heritage buildings in the Union station HCD are as follows:

Additions and Alterations: Additions and alterations may be approved, depending on their impact within the district. Additions that are not prominently visible – especially from Union Station, will generally be



approved. Those that are visible will be evaluated on a case-by-case basis. Additions will be evaluated by the following criteria:

- The new structure respects the general size, shape and scale of features associated with the property of district.
- The site plan respects the general site characteristics associated with the property or district.
- The design respects the general historic and architectural characteristics associated with the property or district.
- The material choice respects the existing character of the property and district as a whole. Material choice not directly emulating what exists will be contextual and appropriate.
- Any addition is to be connected to the property in a way that does not alter, change, obscure, damage, or destroy any significant building features.
- Additions, renovations and alterations that enhance the character of the district, and are compatible with the overall planning goals of the district will be encouraged, yet subject to thorough review.

Demolition: The demolition involving any contributing building in the district will only be approved after thorough review in accordance with the *Ontario Heritage Act*.

(ERA Architects Inc. 2006: 60)

2.2.1.2 Guidelines for the Public Realm

Section 8.3.5 of the Union Station HCD Plan provides direction for the development of the public realm within the district. As outlined in the HCD Plan, the public realm provides a stage for the daily life of the city, comprising gathering spaces such as parks, public squares, streets, path networks, and the interior of malls. The design and functional aspects of the public realm, such as sidewalks, streetscapes, and boulevards, provide public spaces with both form and a sense of place (ERA Architects Inc. 2006: 64-65). The HCD Plan notes that enhancing connectivity and maintaining open spaces through public realm can promote the heritage character of the HCD. Similarly, the consolidation of and simplification of streetscape elements in the district, including paving, curbs, tree grates, signage, base plants, vertical elements, and lighting has a strong impact on the general quality and understanding of the public realm.

The design guidelines for the public realm in the Union Station HCD are as follows:

Promote Heritage Character: All aspects of the public realm need to recognize the heritage character of the district. Lighting should be used to emphasize building forms at night, in a manner representative of the grandeur of the architecture. Historical precedents in planning around Union Station, such as the John Lyle Plan of 1911, should be used to inform the importance of establishing connections to the south of the district. Historical connections, both visual and physical, should be maintained and enhanced,



such as the view of the Royal York Hotel and the physical connections between the John Street Roundhouse and Union Station. The moats, teamways, and bridges of Union Station should be better utilized as important linkages between areas of the district.

Aspire to Highest Standards of Design Excellence: Streetscape elements should be of high-quality design and enduring materials that are appropriate to the district's historic character. The approach to design and materials used should reflect good contemporary design to emphasize the district's evolving character. Designs using inauthentic historical pastiche, for example mock-Victorian, should be avoided because they look backwards, and when executed using modern techniques do not have the quality of craft of the original.

Streetscape furniture should fit into the landscape rather than be its focus: Street furniture should have high standards of functionality, durability, environmental performance, and visual attractiveness. Materials such as stainless steel should be used because they wear well and do not require continual maintenance. Pedestrian comfort should be encouraged by considering and supporting pedestrian flow, needs of elderly, visually impaired, etc.

Maintain open spaces: Open spaces, such as Roundhouse Park, serve critical functions by helping to maintain the environmental quality of the district, in addition to providing a calm gathering space. Open spaces should be properly protected and maintained.

Reduce clutter: In order to reduce clutter on the streetscape, the size and number of objects like waste and recycling receptacles and newspaper boxes should be reduced. For example, the three-unit garbage receptacle should be reconsidered as three separate components. Consolidated newspaper boxes should be used to replace the banks of 10-20 boxes chained together.

Coordinate Design Implementation and Maintenance: The overall design of the public realm should be carried out in a consistent and well-coordinated manner to ensure that design measures complement each other and work towards enhancing the district's identity.

(ERA Architects Inc. 2006: 64-65)



3.0 Assessment of Existing Conditions

3.1 Property Context

Field reviews were undertaken by Cultural Heritage Specialists Luke Fischer and Chelsea Dickinson, on May 21, 2021 and by Heidi Schopf, Cultural Heritage Team Lead on July 16, 2021. Conditions were clear and sunny on both field dates. The field reviews confirmed that the Dominion Public Building property is located on the southwest corner of Front Street West and Yonge Street. The property is generally bounded by Front Street to the north, Yonge Street to the east, the rail line to the south and Bay Street to the west (**Plate 5 to Plate 9**). The Dominion Public Building is set within the Union Station HCD and is identified as a 'contributing building' of the district. In the HCD Plan it is described as a property that contributes to the character of the district and/or is historically, architecturally or culturally significant as identified in the heritage evaluation or determined by further evaluation.

The Dominion Public building is located directly adjacent to eight heritage properties including:

- 1) Brookfield Place (161-181 Bay Street) located directly north of the Dominion Public Building (Plate 10 to Plate 11). Brookfield place is designated under Part V of the *Ontario Heritage Act* as part of the Union Station HCD through By-law 634-2006 as a contributing property.
- 2) The Bank of Montreal/Hockey Hall of Fame (10 Front Street West) is located directly north of the Dominion Public Building (Plate 12-Plate 13). The Bank of Montreal/Hockey Hall of Fame is designated under Part IV of the *Ontario Heritage Act* through By-law 256-76.
- 3) The Gowans Kent (20 Front Street West) building is located directly to the north of the Dominion Public Building (Plate 14). The Gowans Kent Building is designated under Part IV of the *Ontario Heritage Act* through By-law 108-83 and under Part V of the *Ontario Heritage Act* as a 'Contributing Building' in the Union Station HCD through By-law 634-2006
- 4) The St. Lawrence Neighborhood HCD is located directly east of the Dominion Public Building. The St. Lawrence Neighborhood HCD is designated under Part V of the *Ontario Heritage Act* under By-law 1328-2015 (City of Toronto 2015). The St. Lawrence Neighbourhood HCD is currently under appeal to the Local Planning Appeal Tribunal (City of Toronto 2021).
- 5) O'Keefe Centre/Meridian Hall (6 Scott Street) is located directly east of the Dominion Public Building (Plate 15-Plate 17). The O'Keefe Center (now Meridian Hall) is designated under Part IV (By-law 1156-2008) and Part V (1328-2015) of the *Ontario Heritage Act* under the St. Lawrence Neighbourhood HCD.
- 6) The Yonge Street Railway Bridge is located southeast of the Dominion Public building (Plate 18-Plate 19). The Yonge Street Railway Bridge is identified as a Provincial Heritage Property (of local significance) and as such is protected under section 34.5 of the *Ontario Heritage Act*

- 7) The Union Station Complex (65-71 Front Street West) is located west of the Dominion Public Building (Plate 20). Union Station is designated under Part IV (By-Law 948-2005) and Part V (By-law 634-2006) of the *Ontario Heritage Act*. The Union Station Complex is listed as a 'Contributing Building' in the Union Station HCD and is also a National Historic Site under the *Historic Sites and Monuments Act*. The Union Station Complex is also a Provincial Heritage Property of Provincial Significance.
- 8) The Royal Bank Plaza (200 Bay Street) which is located directly northwest of the Dominion Public Building (Plate 21). The Royal Bank Plaza is designated under Part V of the *Ontario Heritage Act* as part of the Union Station HCD through By-law 634-2006 as a contributing property.

Figure 3 depicts the aforementioned eight existing heritage properties in relation to the Dominion Public Building.



Plate 5: View facing east towards the Dominion Public Building from the intersection of Front Street West and University Avenue. This photo illustrates the spatial relationship between the Dominion Public Building and Union Station and how these properties form a cohesive Beaux-Arts streetscape on the south side of Front Street West.



Plate 6: View of the southeast corner of the Dominion Public Building from the intersection of Front Street West and Bay Street. Note the Union Station Trainshed over Bay Street and several high rise buildings in the background



Plate 7: View of Front Street West from the west side of the Dominion Public Building (facing east). On the north side of Front Street is Brookfield Place.

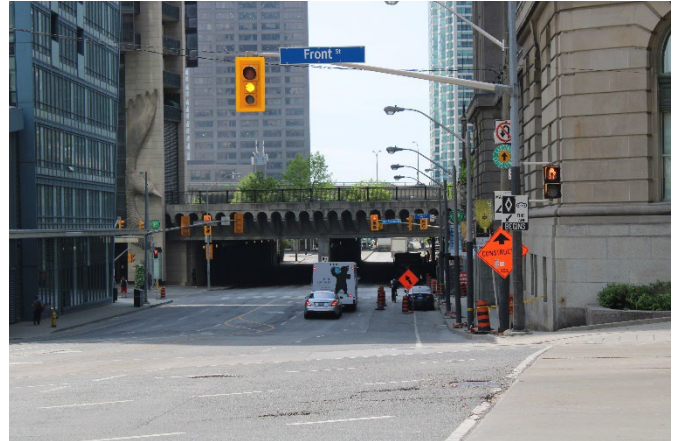


Plate 8: View of the north and east elevations of the Dominion Public Building from the intersection of Front Street West and Yonge Street, facing south. Located further south is the Yonge Street Bridge and the Esplanade.



Plate 9: Oblique view of the Dominion Public Building taken from the corner of Bay Street and Front Street West, facing southeast. Shows the length of the north and west elevations of the Dominion Public Building.



Plate 10: View of Brookfield Place from the corner of Bay and Front Street facing northeast.



Plate 11: South elevation of Brookfield Place

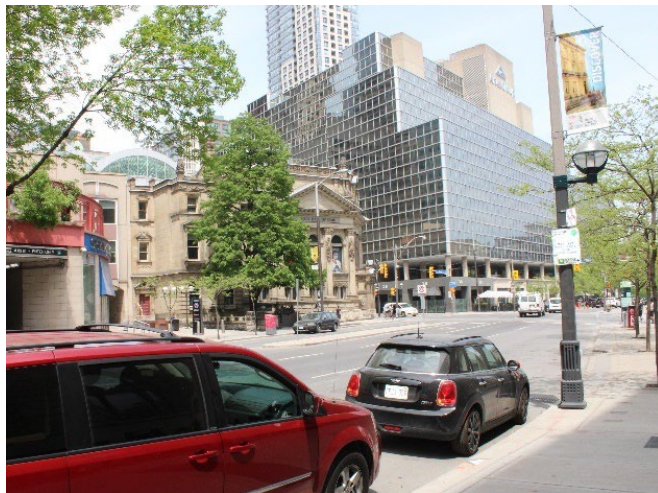


Plate 12: View of the Bank of Montreal/Hockey Hall of Fame taken from the Dominion Public Building, facing northeast



Plate 13: Oblique view of Bank of Montreal/Hockey Hall of Fame (10 Front Street West)



Plate 14: South elevation of Gowans Kent Building



Plate 15: View of Meridian Hall (1 Front Street East) in relation to the Dominion Public Building, facing southwest

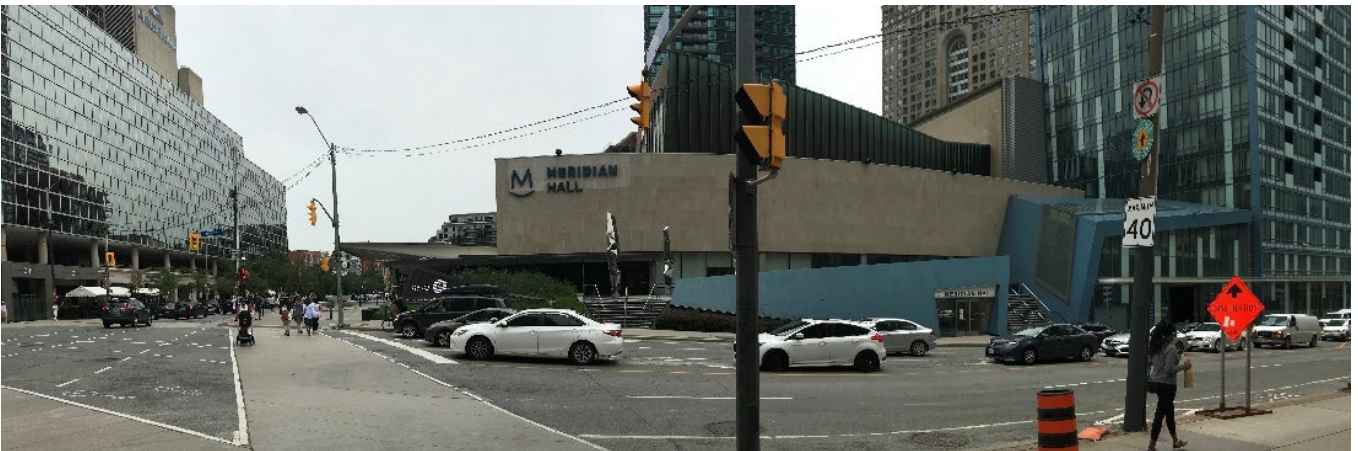


Plate 16: View of Meridian Hall (1 Front Street East) taken from the corner of Yonge Street and Front Street East

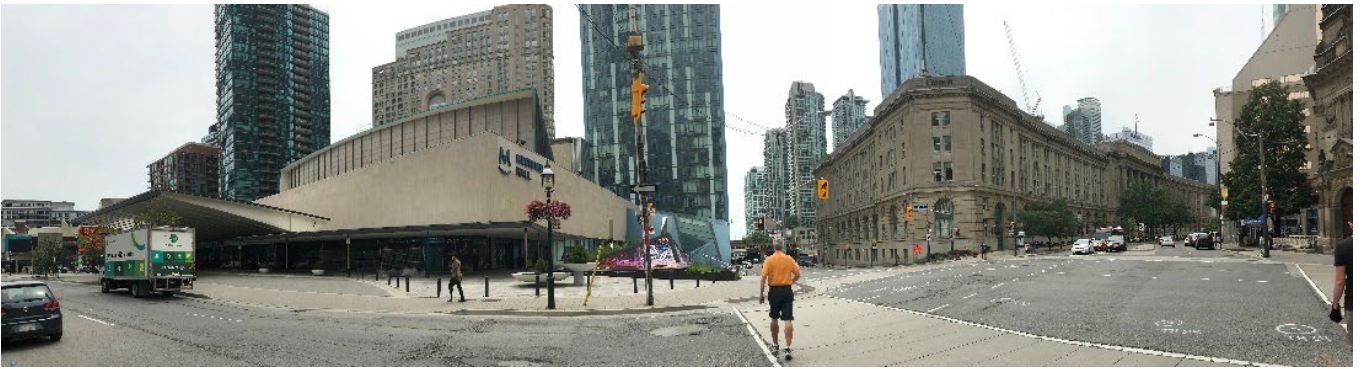


Plate 17: View of Meridian Hall (1 Front Street East) in relation to the Dominion Public Building at the intersection of Yonge Street and Front Street

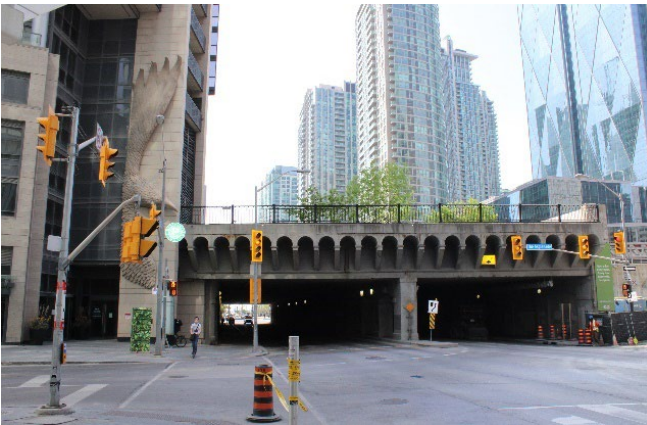


Plate 18: North elevation of the Yonge Street Railway Bridge



Plate 19: View of the projecting beams with decorative semi-circular arches at the beam ends on the north elevation of the Yonge Street Railway Bridge



Plate 20: View facing south-southwest towards the Dominion Public Building and the Union Station Complex from Brookfield Place. This photo illustrates the spatial context between the Dominion Public Building and the Union Station Complex and how these properties form a cohesive Beaux-Arts streetscape on the south side of Front Street West.



Plate 21: South elevation of Royal Bank Plaza



3.2 Dominion Public Building

The Dominion Public Building is a large-scale former federal government building which was completed in two phases in 1929-31 (centre and east pavilions) and 1934-35 (west pavilion). As previously stated in Section 2.1, the Dominion Public Building was commissioned by the Government of Canada's Department of Public Works and originally served as Toronto's Custom's House. The northern section of the building on Front Street West rises five stories, while the rear section extends six stories to address the change in grade. The building features an irregular plan and a flat roof and is clad in concrete with a granite clad foundation and is valued as a rare and exceptional example in Canada of Beaux-Arts Classicism (**Plate 22-Plate 29**).

The north elevation features a tripartite organization façade. The central pavilion features a projecting extension that includes an extended first story with cornices and six freestanding Ionic columns supporting the entablature inscribed "Dominion Public Building A.D. MCMXXX". The front façade (north elevation) features three separate entrance areas, the first is located within the centre pavilion and includes three first story entrances with round arch openings with paired bronze doors beneath large transoms with metal mullions incorporating rope detailing and cast metal beavers. These doors are replicated on the far eastern and western sides of the north elevation for a total of five distinct entryways. The first story windows located on the north elevation are visible on the complementary eastern and western wings of the building. These windows are reflective of the entryways and include round arch opened windows with metal mullions incorporating rope detailing. Some additional classical detailing on the central pavilion and eastern/western wings, include carved acanthus leaves, the dentils, the engaged ionic columns and the stone lion's heads.

The east and west elevations feature complementary exterior architectural features to the north elevation and include first-storey windows with round arch opened windows with metal mullions incorporating rope detailing (**Plate 22-Plate 55**).

The south elevation is distinct from the other elevations and features a tripartite organization façade including the eastern and western wings which feature the complementary exterior architectural features to the northern western and eastern elevations. The central portion is distinct and features a utilitarian façade with a raised centre and rows paired one-over-one sash windows with stone lug sills which continue along the entire upper and lower stories of the south elevation, excluding the eastern and western wings as previously stated (**Plate 27-Plate 29**).



Plate 22: Oblique view of the Dominion Public Building from the intersection of Front Street West and Bay Street showing the north and west elevations



Plate 23: Oblique view of the Dominion Public Building from the intersection of Front Street and Yonge Street showing the north and east elevations



Plate 24: Oblique view of the Dominion Public Building from the intersection of Front Street West and Bay Street showing the west elevation



Plate 25: Close-up oblique view of the Dominion Public Building showing the north and west elevations of the building



Plate 26: East elevation of the Dominion Public Building , facing southeast



Plate 27: West elevation of the Dominion Public Building,, facing east



Plate 28: South elevation of the Dominion Public Building, facing northwest. Note ongoing construction at 141 Bay Street (left)



Plate 29: South elevation of the Dominion Public Building, facing northeast. Note ongoing construction at 141 Bay Street (right)



Plate 30: North elevation, east side entryway, facing south



Plate 31: First story entrance with round arch openings with paired bronze doors



Plate 32: Cast metal beaver located above the first-storey entrance



Plate 33: Metal mullions with rope detailing located above the first-storey entrance



Plate 34: First level and upper level windows of the north elevation



Plate 35: Engaged ionic columns on the north elevation



Plate 36: Upper cornices with the dentils, engaged ionic columns and stone lion's heads.



Plate 37: Central pavillion including the projecting extension, freestanding ionic columns and inscribed entablature



Plate 38: Central pavillion freestanding ionic columns and inscribed entablature



Plate 39: Close-up of inscribed entablature "Dominion Public Building A.D. MCMXXX".



Plate 40: Close-up view of freestanding ionic columns and keystone on the north elevation



Plate 41: Close-up view of freestanding ionic column and dentils on the north elevation



Plate 42: Close-up view of Dentil trim located on the north elevation



Plate 43 : Close-up view of central pavillion front entrances



Plate 44: Close-up view of central pavillion front entrances and 'Dominion Public Building' plaques

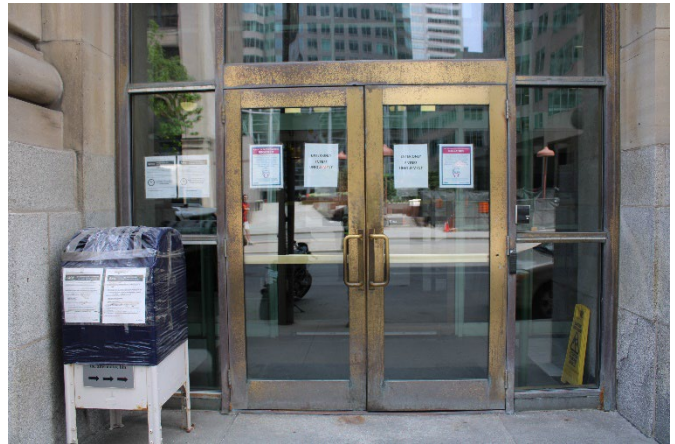


Plate 45: Close-up view of central pavillion front entrance paired bronze doors



Plate 46: North elevation as seen from Front Street West, facing east



Plate 47: North elevation as seen from Front Street West, facing west

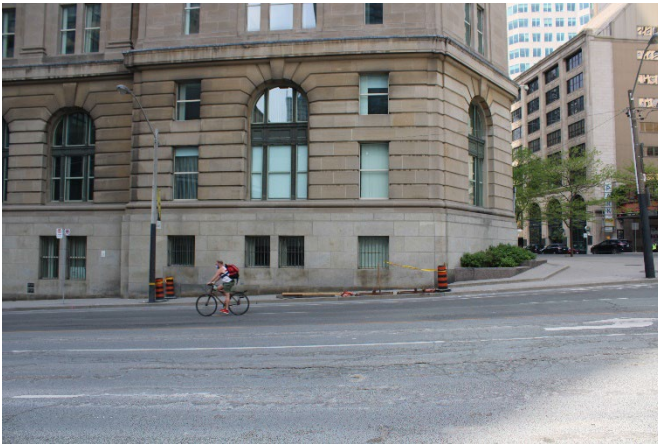


Plate 48: East elevation (northeast corner) of the Dominion Public Building



Plate 49: Example of a first-storey window on the east elevation featuring metal mullions incorporating rope detailing



Plate 50: Example of a first-storey window on the west elevation featuring metal mullions incorporating rope detailing



Plate 51: Example of a first-storey window on the west elevation featuring metal mullions incorporating rope detailing



Plate 52: Example of a first-storey window on the west elevation featuring metal mullions incorporating rope detailing



Plate 53: Dominion plaque located on the eastern half of the north elevation

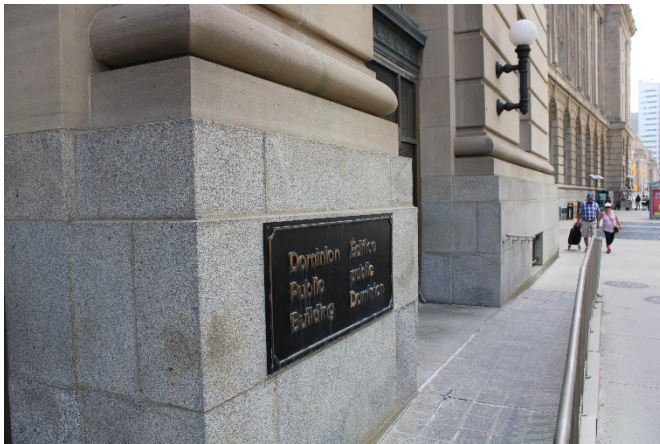


Plate 54: Dominion plaque located on the western half of the north elevation



Plate 55: Upper Cornice and dentil detailing with 'Canada' sign located on the northeast corner of the building

3.3 Area of Proposed Intervention

The proposed work at the Dominion Public Building is located at the southwest corner of the building. Proposed work in this location includes property acquisition of a small section of the parking lot at the rear of the Dominion Public Building, open cut construction, and construction of a new, below-grade wall for the WELRT streetcar loop that abuts the southwest corner of the building.

The area of the proposed work is generally bounded by Bay Street to the west and the rail line to the south. This area features a concrete sidewalk that abuts Dominion Public Building and features a rounded corner with vertical clean-cut detailing (**Plate 56 to Plate 62**).

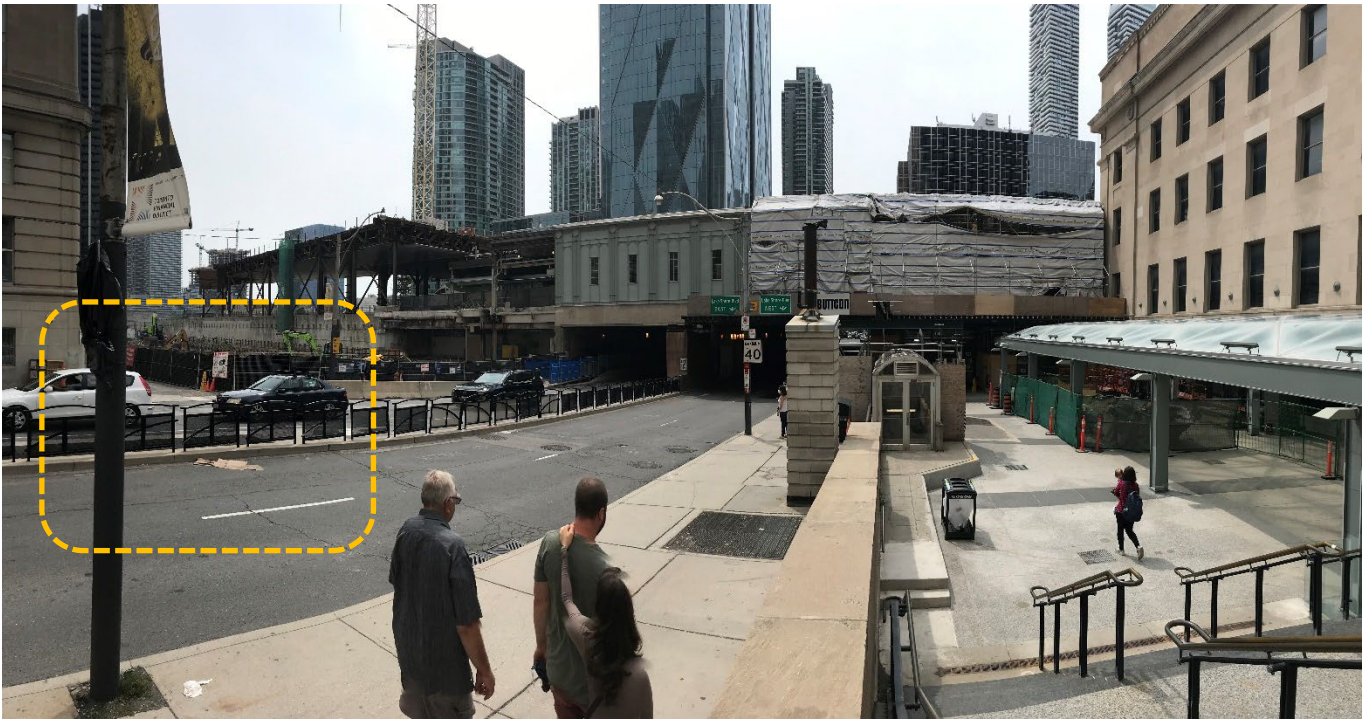


Plate 56: Approximate location of proposed intervention adjacent to the southwest corner of the Dominion Public Building. Note the ongoing construction at 141 Bay Street between the Dominion Public Building (left) and the Union Station Trainshed (centre)



Plate 57: Southwest corner of the Dominion Public Building, facing north. Orange hatching utilized to indicate approximate area of proposed intervention in relation to the property



Plate 58: Southwest corner of the Dominion Public Building, facing west



Plate 59: Southwest corner of the Dominion Public Building, facing northwest



Plate 60: Southwest corner of the Dominion Public Building, facing north



Plate 61: Southwest corner of the Dominion Public Building at curb connection with a rounded corner with vertical clean cut detail. A new, below-grade wall will be constructed that will abut this corner of the building.



Plate 62: Area of proposed intervention in relation to the southwest corner of the Dominion Public Building

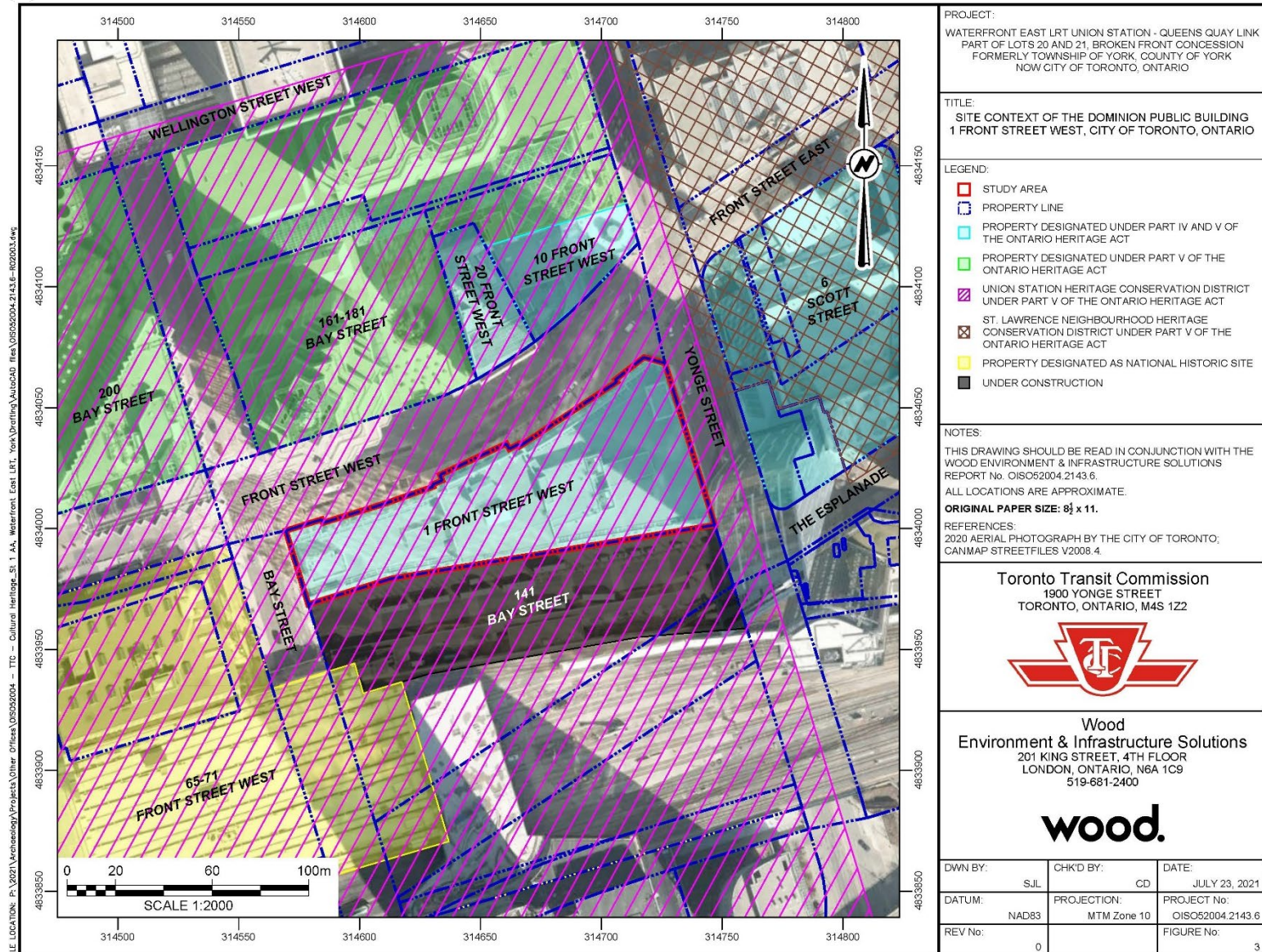


Figure 3: Site Context of the Dominion Public Building, 1 Front Street West, City of Toronto, Ontario

Please reach out to the Project Team should you require alternative text for this image.



4.0 Description and Purpose of Proposed Activity

The proposed work at the Dominion Public Building (1 Front Street West) consists of property acquisition, open cut construction within the property, and construction of a new, below-grade wall for the WELRT streetcar loop abutting the southwest corner of the Dominion Public Building. Details of the proposed work are provided below and drawings showing the proposed architectural plan and construction phasing plans are provided in **Appendix B**.

High-level description of potential impacts to the Dominion Public Building (1 Front Street West):

- Open cut excavation to install the WELRT streetcar loop along Bay Street and Front Street West involving demolition of existing infrastructure, property acquisition, and construction of new loop, including the associated platform and connections to existing transit infrastructure.
 - Permanent Impacts to Dominion Public Building:
 - Property acquisition at the southwest corner of the building to accommodate excavation area required for new subway loop.
 - Street level demolition/open cut construction within the southwest corner of the property
 - Construction of a below-grade wall abutting the southwest corner of the Dominion Public Building
 - Construction of new curb within the parking lot at the rear of the Dominion Public Building (south elevation)
 - Temporary Impacts:
 - Construction staging areas along Bay Street and within private property
 - Vibration related impacts due to open cut excavation, demolition activities, and construction of new WELRT streetcar loop at platform

Review of drawings of proposed work:

- Union Station Aerial View Renders, RE35-1-AA011 to RE35-1-AA013 (July 7, 2021)
 - Overview of the proposed work shown as 3D renders
- Union Station Platform Level Demolition Plan, RE35-1-AAD001 (July 7, 2021)
 - Demolition of Union Station Streetcar Loop at platform level
 - Basement work proposed at 141 Bay Street
 - Excavation area within 1 Front Street West and immediately adjacent to southwest corner of heritage building
 - Excavation within, and adjacent to, Union Station Complex



- Union Station Street Level Demolition Plan, RE35-1-AD002 (July 7, 2021)
 - Street level proposed demolition area depicted
 - Demolition activities shown with Union Station Complex (65-71 Front Street West), Dominion Public Building (1 Front Street West), Postal Delivery Building (40 Bay Street), and the public realm of the Union Station HCD along Bay Street
- Union Station Streetcar Platform Level Demolition Reflected Ceiling Plan, RE35-1-AD003 (July 7, 2021)
 - Street level demolitions depicted within Union Station (65-71 Front Street West), Postal Delivery Building (40 Bay Street), Dominion Public Building (1 Front Street West), and public realm of Union Station HCD
- Union Station Platform Level Staging (Plans 1 and 2), RE35-1-AS001 and RE35-1-AS002 (July 7, 2021)
 - Footprint of proposed work showing including alterations to the corridor/stairs in the 'USEP' Corridor, platform level mechanical rooms (northbound and southbound platforms), connections/alterations to stairs within Union Station (65-71 Front Street West), connection to existing TTC Union Station Subway Station, stairs and elevator within 141 Bay Street)
 - Retention of columns supporting the Union Station Trainshed
- Union Station Site Plan, RE35-1-A1001 (July 7, 2021)
 - Site plan depicting the location of new construction shown
 - New construction shown within Union Station Complex (65-71 Front Street West) and 141 Bay Street, Dominion Public Building (1 Front Street West), and within public realm of Union Station HCD
 - New stairs showing within 'Existing Arcade' of Union Station and northwest portion of building
 - Vent shafts to street level shown
 - Existing columns supporting the Union Station Trainshed to be retained
- Union Station Platform Level Plan, RE35-1-A2001 (July 7, 2021)
 - New track arrangement shown
 - Union Station Arcade: New Stair M, Elevator E3, Presto TVM/FSVM machines shown
 - New openings shown between existing columns
 - New West Platform shown
 - New East Platform shown
 - 141 Bay Street: Stair Q, Elevator E4, new corridor
 - Pedestrian Bridge Columns to remain
 - Existing Union Station Elevator 13 to remain
 - Stair N proposed within east block of Union Station Headhouse (65-71 Front Street West)
 - Existing doors to TTC Union Station Subway to remain



- New streetcar loop depicted including new handrails, new glazed wall, new structural walls and columns, and fan room
 - Existing columns supporting the Union Station Trainshed to remain
- Union Station Street Level Plan, RE35-1-A2002 (July 7, 2021)
 - Union Station Arcade: Stair M and new Elevator E3
 - East block of Union Station Headhouse: Existing elevator 13 to remain and New Stair N
 - Proposed vent shafts on west side of Bay Street, south of Front Street and within Bay Street right-of-way
 - New curbs adjacent to Dominion Public Building (1 Front Street West)
 - 141 Bay Street: Stair Q and new Elevator E4 shown
 - Bay Street: Bay Street underpass (northbound and southbound shown) and associated new bike lanes shown



5.0 Impact Assessment

The Ministry of Citizenship and Multiculturalism' (MCM) *Standards & Guidelines for Conservation of Provincial Heritage Properties – Information Bulletin 3: Heritage Impact Assessment for Provincial Heritage Properties* (MCM 2017) gives guidance on how to complete HIAs for public bodies prescribed under the *Ontario Heritage Act* (Government of Ontario 2014). The purpose of the HIA is to identify and assess the proposed activity to determine impacts (positive or negative, direct or indirect) that the proposed activity may have on the property's cultural heritage value or interest. For the purpose of this HIA, the following definitions of direct, indirect, and positive impacts are used:

- **Direct Adverse Impact:** A permanent or irreversible negative affect on the cultural heritage value or interest of a property or result in the loss of a heritage attribute on all or part of the provincial heritage property.
- **Indirect Adverse Impact:** An impact that is the result of an activity on or near the property that may adversely affect its cultural heritage value or interest and/or heritage attributes.
- **Positive Impact:** An impact that may positively affect a property by conserving or enhancing its cultural heritage value or interest and/or heritage attributes.

(Government of Ontario 2017)

A detailed assessment of potential impacts resulting from the proposed TTC WELRT work is provided in Sections 5.1 and 5.2. A discussion of the potential impacts resulting from the proposed work is provided in Section 5.3

5.1 Potential Impacts to Dominion Public Building

As outlined in Section 4.0, the proposed work includes property acquisition, open cut construction within the property, and construction of a new, below-grade wall for the WELRT streetcar loop abutting the southwest corner of the Dominion Public Building. A detailed assessment of the potential impacts resulting from the proposed work is provided in **Table 1** to **Table 3**.

Table 1: Assessment of Potential Direct Adverse Impacts

| Potential Direct Adverse Impact | Y/N | Discussion |
|---|-----|---|
| Removal or demolition of all or part of any heritage attribute. | N | No heritage attributes will be removed or demolished. |
| Removal or demolition of any building or structure on the provincial heritage property whether or not it contributes to the cultural heritage value or interest of the property (i.e. non-contributing properties). | N | No buildings or structures will be removed or demolished. |



| Potential Direct Adverse Impact | Y/N | Discussion |
|---|-----|--|
| Any land disturbance, such as change in grade and/or drainage patterns that may adversely affect a provincial heritage property, including archaeological resources. | Y | <p>The proposed work at the Dominion Public Building includes street level demolition/open cut construction within the property abutting the southwest corner of the Dominion Public Building (1 Front Street West) to accommodate the excavation area required for new subway loop. A new, below-grade wall will be constructed in this location abutting the southwest corner of the Dominion Public Building.</p> <p>The majority of the proposed work does not pose a risk to the heritage attributes of the property. However, land disturbance will take place within the property and directly adjacent to the building. Accordingly, mitigation measures must be prepared to avoid potential impacts.</p> |
| Alterations to a property in a manner that is not sympathetic, or is incompatible, with cultural heritage value or interest of the property. This may include necessary alterations, such as new systems or materials to address health and safety requirements, energy-saving upgrades, building performance upgrades, security upgrades or servicing needs. | N | No alterations to the Dominion Public Building are anticipated. |
| Alterations for access requirements or limitations to address factors as accessibility, emergency egress, public access, or security. | Y | <p>The proposed work at 1 Front Street West includes the construction of a new curb within the parking lot at the rear of the Dominion Public Building (south elevation).</p> <p>The majority of the proposed work does not pose a risk to the heritage attributes of the property. However, alterations for access requirements will take place within the property and directly adjacent</p> |



| Potential Direct Adverse Impact | Y/N | Discussion |
|--|-----|--|
| | | to the building. Accordingly, mitigation measures must be prepared to avoid potential impacts. |
| Introduction of new elements that diminish the integrity of the property, such as a new building, structure or addition, parking expansion or addition, access or circulation roads, or landscape features | Y | <p>The proposed work at 1 Front Street West includes the construction of a new curb within the parking lot at the rear of the Dominion Public Building (south elevation). In addition, a new, below-grade wall will be constructed that abuts the southwest corner of the building,</p> <p>The majority of the proposed work does not pose a risk to the heritage attributes of the property due to the below-grade nature of the project. However, work will take place within the property and directly adjacent to the building. Accordingly, mitigation measures must be prepared to avoid potential impacts.</p> |
| Changing the character of the property through removal or planting trees or other natural features, such as a garden, or that may result in the obstruction of significant views or vistas within, from, or of built and natural features. | N | No changes to the character of the property are proposed. |
| Change in use for the provincial heritage property that could result in permanent, irreversible damage or negates the property's cultural heritage value or interest. | N | No changes in land use are planned. |
| Continuation or intensification of a use of the provincial heritage property without conservation of heritage attributes. | N | No intensification of use is planned. |



Table 2: Assessment of Potential Indirect Impacts

| Potential Indirect Adverse Impact | Y/N | Discussion |
|---|-----|--|
| Shadows that alter the appearance of a heritage attribute or change the visibility of an associated natural feature or plantings, such as a tree row, hedge, or garden. | N | No shadow-related impacts are anticipated. |
| Isolation of a heritage attribute from its surrounding environment, context, or a significant relationship | N | No isolation-related impacts are anticipated. |
| Vibration damage to a structure due to construction or activities on adjacent to the property | Y | Construction activity that introduces vibration-related risk planned on the property as a result of the construction directly abutting the Dominion Public Building could result in vibration damage. Accordingly, mitigation measures must be prepared to avoid potential impacts. |
| Alteration or obstruction of a significant view of or from the provincial heritage property from a key advantage point | N | No impacts to significant views are anticipated since the proposed work is largely located below grade. |

Table 3: Assessment of Potential Positive Impacts

| Potential Positive Impact | Y/N | Discussion |
|---|-----|---|
| Changes or alterations that are consistent with accepted conservation principles, such as those articulated in MCM' <i>Eight Guiding Principles in the Conservation of Historic Properties</i> , <i>Heritage Conservation Principles for Land Use Planning</i> , Parks Canada's <i>Standards and Guidelines for the Conservation of Historic Places in Canada</i> | N | No alterations of the property are planned. |



| Potential Positive Impact | Y/N | Discussion |
|--|-----|--|
| Adaptive re-use of a property – alteration of a provincial heritage property to fit new uses or circumstances of the property in a manner that retains its cultural heritage value or interest | N | No new use or re-use is planned as part of the proposed work. |
| Public interpretation or commemoration of the provincial heritage property | N | No public interpretation or commemoration is planned as part of the proposed work. |

5.2 Potential Impacts to Union Station HCD

The Dominion Public Building is a contributing building in the Union Station HCD. However, work associated with the WELRT is below grade and no exterior impacts to the Dominion Public Building are anticipated. The HCD provides guidelines for contributing buildings to address additions, alterations, or demolition of properties with heritage significance within the district (ERA Architects Inc. 2006: 60). Given that no exterior demolition, additions, or alterations of the Dominion Public Building are proposed, the HCD guidelines for contributing buildings are not applicable. The at-grade work within the Dominion Public Building property includes the construction of a new curb at the rear of the building (south elevation). The proposed curb is directly tied to the public realm (i.e. sidewalk) along Bay Street.

The impacts to the Union Station HCD are related to the public realm of the district since the WELRT project will result in open cut excavation in the vicinity of Front Street West and Bay Street to accommodate the demolition of the existing streetcar loop and construction of the new streetcar loop and platform. Post-construction landscaping will be carried out to rehabilitate the sidewalk and disturbed sections of the public realm. A standalone HIA for the Union Station HCD to address impacts to the public realm has been prepared by WSP under a separate cover.

5.3 Discussion of Anticipated Impacts

The detailed impact assessment determined that three potential direct adverse impacts and one potential indirect adverse impact are anticipated as part of the proposed work.

5.3.1.1 Summary of Potential Direct Adverse Impacts

Construction drawings for the proposed work depict property acquisition, open cut excavation, demolition activities, construction within the property and construction of a new, below-grade wall for the WELRT streetcar loop abutting the southwest corner of the Dominion Public Building and the construction of a new curb within the parking lot at the rear of the building. This proposed work will result in land disturbance, alterations for



access requirements, and the introduction of new elements (below-grade). Alternatives and mitigation measures to avoid potential adverse impacts to the building exterior are provided in Section 6.0.

5.3.1.2 Summary of Potential indirect Adverse Impacts

Construction activity that introduces vibration-related risk planned on the property as a result of the construction directly abutting the Dominion Public Building which could result in vibration damage. Alternatives and mitigation measures to avoid potential adverse impacts to the building exterior are provided in Section 6.0.

5.3.1.3 Summary of Potential Positive Impacts

No potential positive impacts were identified as part of the proposed work. Accordingly, no mitigation measures are required.



6.0 Considered Alternatives and Mitigation Measures

Alternative approaches and mitigation measures are required when impacts are anticipated to a property with CHVI. In the case of the Dominion Public Building (1 Front Street West), three direct adverse impacts and one indirect adverse impact were identified due to open cut construction within the property and construction of a new wall for the WELRT streetcar loop abutting the southwest corner of the Dominion Public Building which includes the construction of a new curb within the parking lot at the rear of the building. To address these impacts, the mitigation measures contained in MCM InfoSheet#5 were considered (Government of Ontario 2005). An evaluation of applicable mitigation measures is presented in Table 4.

Table 4: Mitigation Measures

| Mitigation Measures | Relevance |
|--|--|
| Alternative development approaches | A new, below grade wall is proposed that will abut the southwest corner of the Dominion Public Building. While no direct alterations or demolition of the Dominion Public Building is anticipated, it is recommended that the project should be designed with a general avoidance approach in line with provincial and federal guidelines. Exploration of an alternative development approach to locate the new wall as far as possible from the southwest corner of the Dominion Public Building is recommended. |
| Isolating development and site alteration from significant built and natural features and vistas | N/A |
| Design guidelines that harmonize mass, setback, setting, and materials | The proposed work will result in the construction of a new curb at the rear of the Dominion Public Building. The curb should be designed as part of a larger post-construction landscaping plan that follows the Union Station HCD guidelines for the public realm. An HIA that addresses the impacts of the proposed work on the public realm of the Union Station HCD has been prepared under a separate cover. Design guidelines related to the proposed curb and public realm are covered in the HIA for the Union Station |



| Mitigation Measures | Relevance |
|--|--|
| | HCD. Per the HCD guidelines regarding new construction the materials for the new curb should be considered in context with those of adjacent contributing properties and with the district as a whole. |
| Limiting height and density | N/A |
| Allowing only compatible infill and additions | N/A |
| Reversible alterations | N/A |
| Buffer zones, protection measures, and other planning mechanisms | <p>The proposed work within this property involves the open cut excavation, demolition of the existing streetcar loop, and below-grade construction of the new WELRT streetcar loop and platform. A new, below-grade wall will be constructed that directly abuts the southwest corner of the Dominion Public Building. A new curb within the parking lot at the rear of the Dominion Public Building (south elevation) will be installed as part of the post-constructing landscaping plan..</p> <p><i>Protective Barriers</i></p> <p>Protective fencing, film, or netting should be installed around the base of the Dominion Public Building in the vicinity of the proposed work to protect the exterior of the building from accidental damage during construction</p> <p><i>Protection Measures</i></p> <p>The heritage status of the Dominion Public Building should be noted on project drawings to communicate the status of this property to project personnel. The note should read:</p> <p>“The Dominion Public Building is a significant heritage property with municipal protection under the <i>Ontario Heritage Act</i>. Use caution when conducting work in</p> |



| Mitigation Measures | Relevance |
|---------------------|--|
| | <p>the vicinity of this building to avoid accidental damage”.</p> <p><i>Vibration Monitoring</i></p> <p>Work is proposed within the Dominion Public Building property, including the installation of a new below-grade wall that directly abuts the southwest corner of the building. Vibration monitoring should be carried out by a qualified geotechnical engineer. Vibration monitoring should consist of pre-construction survey, vibration monitoring during construction, and post-construction survey.</p> |

6.1 Conservation Guidance

The *Standards and Guidelines for the Conservation of Historic Places in Canada* (Standards and Guidelines) were reviewed to develop a conservation strategy for the Dominion Public Building (1 Front Street West). The Standards and Guidelines lays out the framework for the conservation of historic places, can be defined as “all actions or processed aimed at safeguarding the character-defining elements [heritage attributes] of an historic place to retain is heritage value and extend its physical life” (Parks Canada 2011: 15). Per the Standards and Guidelines, there are three primary conservation treatments:

- **Preservation:** Involves protecting, maintaining, and stabilizing the existing form, material, and integrity of an historic place or individual component, while protecting its heritage value. Preservation should be considered as the primary treatment when, a) materials, features and spaces of the historic place are essentially intact and convey the historical significance without extensive repair or replacement, b) depiction during a particular period in its history is not appropriate, and, c) continuation or new use does not require extensive alterations or additions.
- **Rehabilitation:** Involves the sensitive adaptation of an historic place or individual component for a continuing or compatible contemporary use, while protecting its heritage value. Rehabilitation should be considered as the primary treatment when, a) repair or replacement of deteriorated features is necessary, b) alterations or additions to the historic place or planned for a new or continued use, and c) depiction during a particular period in its history is not appropriate.
- **Restoration:** Involves accurately revealing, recovering or representing the state of an historic place or individual component as it appeared at a particular period in its history, while protecting its heritage value. Restoration should be



considered as the primary treatment when, a) an historic place's significance during a particular period in its history significantly outweighs the potential loss of existing, non-character defining materials, features and spaces from other periods, b) substantial physical and documentary or oral evidence exists to accurately carry out the work, and c) contemporary additions or alterations and are not planned.

(Parks Canada 2011: 17)

Given that construction is planned within the immediate vicinity of the Dominion Public Building that could result in both direct and indirect adverse impacts, it is recommended that *preservation* be considered the primary conservation treatment for the Dominion Public Building. Accordingly, it is recommended that the 'General Standards' contained in Section 3 of the Standards and Guidelines are used when developing plans for the proposed work that will impact the Dominion Public Building. Accordingly, it is recommended that the 'General Standards' and guidelines for Exterior Walls (Section 4.3.4), are used to guide the design and implementation of the WELRT project in the vicinity of the Dominion Public Building. Full copies of these standards and guidelines are provided in Appendix C. Standards and guidelines of note are included below.

General Standards for Preservation, Rehabilitation, and Restoration (Parks Canada 2011: 22-23)

1. Conserve the heritage value of an historic place. Do not remove, replace or substantially alter its intact or repairable character defining elements. Do not move a part of an historic place if its current location is a character-defining element.
3. Conserve heritage value by adopting an approach calling for minimal intervention.

Section 4.3.4 *Exterior Walls* of the Standards and Guidelines should be used to guide work proposed within the Dominion Public Building property. Guidelines of note from this section include:

Guidelines for Exterior Walls (Section 4.3.4) (Parks Canada 2011: 146-152)

1. Understanding the exterior walls and how they contribute to the heritage value of the historic building
4. Assessing the condition of wall assemblies and their materials early in the planning process so that the scope of work is based on current conditions.
8. Stabilizing deteriorated exterior walls by using structural reinforcement, weather protection, or correcting unsafe conditions, as required, until repair work is undertaken



10. Protecting adjacent character-defining elements from accidental damage or exposure to damaging materials during maintenance or repair work.

6.2 Discussion of Mitigation Measures

The evaluation of mitigation measures contained in InfoSheet#5 determined that alternative development approaches and protection measures are appropriate mitigation measure to protect the cultural heritage value of the Dominion Public Building.

6.2.1 Avoidance

A new, below grade wall is proposed that will abut the southwest corner of the Dominion Public Building. While no direct alterations or demolition of the Dominion Public Building is anticipated, it is recommended that the project should be designed with a general avoidance approach in line with provincial and federal guidelines. Exploration of an alternative development approach to locate the new wall as far as possible from the southwest corner of the Dominion Public Building is recommended.

6.2.2 Protection Measures

The proposed work for the WELRT project is largely located below grade and involves open cut excavation, the demolition of the existing streetcar loop, and construction of the new loop/platform along Bay Street from Front Street West to 40 Bay Street. Open cut construction will occur within the Dominion Public Building property and a new, below-grade wall will be constructed abutting the southeast corner of the building. Protection measures in the form of protective barriers and notes on construction drawings should be implemented during the construction phase of the project to protect this building.

6.2.2.1 Protective Barriers

Protective fencing, film, or netting should be installed around the base of the Dominion Public Building in the vicinity of the proposed work to protect the exterior of the building from accidental damage during construction

6.2.2.2 Protection Measures

The Dominion Public Building is designated under Part IV *Ontario Heritage Act* under By-Law 423-2017 and designated under Part V of the *Ontario Heritage Act* as part of the Union Station HCD through By-law 634-2006. This Dominion Public Building should be noted on project drawings as a “protected heritage property” to identify the heritage status of the building to project personnel. The note should read:

“The Dominion Public Building is a significant heritage property with municipal protection under the *Ontario Heritage Act*. Use caution when conducting work in the vicinity of this building to avoid accidental damage”.



6.2.2.3 Vibration Monitoring

Construction work is proposed, within, and immediately adjacent to, the Dominion Public Building that introduces vibration-related risk. Vibration monitoring should be carried out by a qualified geotechnical engineer and should consist of a pre-construction survey, vibration monitoring during construction, and a post-construction survey.



7.0 Summary of Community Engagement

In order to identify the existing heritage protections and requirements for the Dominion Public Building (1 Front Street West), the MCM, Ontario Heritage Trust, the City of Toronto, Metrolinx, and Parks Canada were consulted. A summary of community engagement results is provided in the following table and copies of community engagement correspondence are provided in **Appendix D**.



Table 5: Summary of Community Engagement

| Individuals/Groups Engaged | Method | Results | Incorporation into HIA |
|---|---|---|--|
| Yasmina Shamji, Urban Design/Heritage Planning, City of Toronto | -Email sent on December 16, 2020 -Response received on January 7, 2021 | <p>Yasmina Shamji from Urban Design and Heritage Planning at the City of Toronto was initially contacted as part of the CHR prepared for the TTC WELRT project. Ms. Shamji reported that there are a number of known/protected municipal heritage properties within the CHR Study Area. These included:</p> <ul style="list-style-type: none">-40 Bay Street (Designated under Part IV and V of the <i>Ontario Heritage Act</i>)-71 Front Street West (Union Station) (Designated under Part IV and V of the <i>Ontario Heritage Act</i>)-145 Queens Quay West (Designated under Part IV of the <i>Ontario Heritage Act</i>)-2 Cooper Street (Intention to Designate)-1 Front Street West (Designated under Part IV and V of the <i>Ontario Heritage Act</i>)-61 Front Street West (Listed Heritage Property) <p>Ms. Shamji noted that property information can be found on the City of Toronto's Heritage Register Map and By-law information is available on the By-laws and Municipal Codes webpage.</p> <p>Information was requested on additional properties within the Union Station HCD. Ms. Shamji provided the contact information for Guy Zimmerman, Heritage Planner at the City of Toronto, and recommended that WSP contact Mr. Zimmerman regarding information related to the Union Station HCD.</p> | Levels of heritage protection for the Dominion Public Building (1 Front Street West) and nearby provincial heritage properties noted in the HIA. Guy Zimmerman contacted to obtain direction on the Union Station HCD. |



| | | | |
|---|--|--|---|
| <p>Guy Zimmerman, Heritage Planner, City of Toronto</p> | <p>-Series of email correspondence between March 26, 2021 and July 7, 2021</p> <p>-Online meeting on Tuesday, July 7, 2021 regarding the Gardiner Expressway, PATH system, and Bay Street Bridge</p> | <p>Based on the recommendation of Yasmina Shamji, Guy Zimmerman, Heritage Planner with the City of Toronto, was contacted regarding properties within the Union Station HCD that are located within the TTC WELRT Study Area. Guy Zimmerman is noted as being the Heritage Planner for the Union Station HCD. Mr. Zimmerman provided HCD inventory sheets for a number of individual HCD properties/landscape features within the TTC WELRT Study Area. Subsequent conversations with Mr. Zimmerman determined that a demolition permit was issued for the Workmen's Compensation Board Building (90 Harbour Street) in 2011 and that this building is no longer standing.</p> <p>An online meeting with Guy Zimmerman (City of Toronto), Brent Fairbairn (City of Toronto), and Heidy Schopf (WSP) was carried out on Tuesday, July 7, 2021 to discuss the role of the Gardiner Expressway, PATH System, and Bay Street Bridge in the Union Station HCD. Mr. Zimmerman discussed how the Gardiner Expressway and PATH System are contributing landscape elements of the HCD and that the public realm contributions of these resources should be considered. Ms. Schopf asked if the Bay Street Bridge (supporting the Union Station Trainshed over Bay Street) had heritage significance in its own right since this substructure was not noted as a heritage resource on the municipal register or by the MCM and OHT. Mr. Zimmerman expressed that Union Station is run by Parks Canada and that this federal agency will have direction regarding the heritage status of individual components of the Union Station</p> | <p>Inventory sheets provided by Guy Zimmerman used as background information in the preparation of the CHR and this HIA. Information on the role of Parks Canada and Metrolinx used to expand the community engagement strategy for this HIA.</p> |
|---|--|--|---|



| | | | |
|--|--|---|--|
| | | complex. Mr. Zimmerman suggested that Parks Canada and Metrolinx have previous cultural heritage studies that may be of relevance to the current Study Area. | |
| Kevin DeMille, Natural Heritage Coordinator, Ontario Heritage Trust | -Emails sent on December 16, 2020, March 25, 2021, April 8, 2021, and July 8, 2021 -Responses received on March 26, 2021, April 14, 2021, and July 13, 2021 | <p>Kevin DeMille, Natural Heritage Coordinator with the Ontario Heritage Trust was initially contacted as part of the CHR prepared for the TTC WELRT project. Mr. DeMille reported that the Ontario Heritage Trust does not have any conservation easements or Trust-owned properties within the WELRT Study Area. Mr. DeMille recommended reviewing the Ontario Heritage Trust's Plaques Database and Ontario Heritage Act register to obtain information on municipal/local heritage properties in the Study Area. Mr. DeMille further recommended direct communication with the City of Toronto regarding listed and designated municipal heritage properties.</p> <p>Kevin DeMille was subsequently contacted on April 8, 2021 and July 8, 2021 regarding three properties within the Study Area, including Union Station (65-71 Front Street West), the Dominion Public Building (1 Front Street West), and the Postal Delivery Building (40 Bay Street). Mr. DeMille provided the designation By-laws for these properties and also noted that Union Station (65-71 Front Street West) is a National Historic Site. Mr. DeMille recommended consultation with Parks Canada given the federal status of this property. Mr. DeMille subsequently provided the contact information for Kirushanth Gnanachandran with Parks Canada at the request of WSP.</p> | Heritage protections noted by Kevin DeMille were incorporated into the CHR and this HIA. By-laws provided by Mr. DeMille were reviewed. Contact information for Kirushanth Gnanachandran at Parks Canada was used to consult this agency as part of the community engagement completed for this HIA. |



| | | | |
|--|---|--|--|
| Karla Barboza, (A) Team Lead, MCM | -Email sent on December 16, 2020 -Email response received on December 17, 2020 -Email sent on April 8, 2021 | <p>Karla Barboza, Acting Team Lead for Heritage at the MCM was initially contacted as part of the CHR prepared for the TTC WELRT project. Karla reported that there were no properties designated by the Minister in the TTC WELRT Study Area. Ms. Barboza did identify that Union Station is a Provincial Heritage Property of Provincial Significance. She also noted that there are two nearby provincial heritage properties including the Union Station Rail Corridor (USRC) Interlocking Tower – Scott Street (Provincial Heritage Property of Provincial Significance) and Yonge Street Railway Bridge (Provincial Heritage Property – Local Significance). Ms. Barboza also noted that Strategic Conservation Plans (SCP) have not been prepared for Union Station (65-71 Front Street), the URSC Interlocking Tower-Scott Street, or the Yonge Street Bridge. Ms. Barboza noted that lands owned or controlled by an Ontario Ministry or Prescribed Public Body on behalf of the Crown may have responsibilities under the Standards and Guidelines for the Conservation of Provincial Heritage Properties (MTC 2010).</p> <p>Communication with Karla Barboza continued in April 2021 when WSP contacted her to request input on the HIAs being prepared for this project. Ms. Barboza requested to review the CHR before providing input on the HIAs.</p> | Levels of heritage protection for the Dominion Public Building (1 Front Street West) and nearby provincial heritage properties noted in the HIA. Direction regarding obligations of prescribed public bodies under the <i>Standards and Guidelines for the Conservation of Provincial Heritage Properties</i> noted and used to guide the preparation of HIAs. |
| Laura Hatcher, Heritage Planner, MCM | -Response received on | As part of the ongoing communication with Karla Barboza outlined above, Laura Hatcher, Heritage Planner, at the MCM provided the Statements of | Heritage protections regarding the Yonge |



| | | | |
|--|---|---|---|
| | December 18, 2020 | Cultural Heritage Value (SCHV) for Union Station (65-71 Front Street), the URSC Scott Street Interlocking Tower, and the Yonge Street Bridge. All three SCHV had been approved by the Metrolinx Heritage Committee. | Street Bridge were incorporated into this HIA. |
| Kirushanth Gnanachandran Project Coordinator, Rouge National Urban Park Parks Canada/Government of Canada | -Email sent on July 13, 2021 -Response received on July 14, 2021 | An email was sent to Kirushanth Gnanachandran from Parks Canada on July 13, 2021 to request background information on Union Station (65-71 Front Street West) and the Dominion Public Building (1 Front Street West). A response was received on July 14, 2021 to notify WSP that the information gathering request had been passed on to others at FHBRO and the Cultural Resource Management group. To date, responses to fulfill the information gathering request have not been received. | n/a |
| Blair Philpott Acting Cultural Resource Management Advisor, Parks Canada Agency | -Email received on July 26, 2021 | Blair Philpott reported that the Dominion Public Building is no longer a Federal Heritage Building. Public Service and Procurement Canada (PSPC) disposed of the property to Canada Lands Company in 2017. In addition, Blair Philpott identified that Shelley Bruce could advice on the Union Station National Historic Site. WSP send a subsequent email to Shelley Bruce on July 26, 2021. | Heritage status of the Dominion Public Building updated to note that it is no longer a Federal Heritage building under FHBRO. |
| Dan Beare, Metrolinx | -Email sent on July 13, 2021 | To date, a response from Dan Beare at Metrolinx has not been received. | n/a |
| Tamkin Naghshbandi, Metrolinx | -Email sent on July 13, 2021 | Tamkin Naghshbandi from Metrolinx responded and provided detailed information about the Union Station Complex. No information related to the Dominion Public Building was provided by Metrolinx. | n/a |



8.0 Recommendations

The TTC is undertaking PDE to produce a Baseline Design (approximately 30% design completion) of a new TTC WELRT system (the Project). The WELRT, goes under Bay Street, from Front Street to Queens Quay West, and then heads easterly on Queens Quay West. A new portal on Queens Quay West between Bay Street and Yonge Street will be constructed to provide a transition from an underground LRT to an at-grade LRT, which will complement the existing west portal and at-grade LRT on Queens Quay West, west of Bay Street.

The WELRT will service Toronto's waterfront revitalization area by providing fast, reliable transit service in the East Bayfront (EBF) Area of the Waterfront. The expansion of the Union LRT and Queens Quay LRT Stations is required to accommodate the additional streetcar lines and passenger volume. This project is critical to the new waterfront transit plan in the EBF Precinct.

WSP was retained by TTC to complete an HIA for the Dominion Public Building (1 Front Street West) in support of the WELRT. This HIA represents one deliverable to support program delivery. This HIA is being carried out under the Transit Project Assessment Process (TPAP) and was prepared in accordance with the MCM guidance document titled *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties* (MCM 2017).

The purpose of this HIA is to establish the Statement of Cultural Heritage Value for the Dominion Public Building, assess the existing conditions of the property, describe the purpose of the proposed activity, complete an impact assessment and outline considered alternatives and mitigation measures, provide a summary of community engagement, and development recommendations for the conservation of the property.

Based on the above, the following recommendations are made:

1) Avoidance:

- a. A new, below grade wall is proposed that will abut the southwest corner of the Dominion Public Building. While no direct alterations or demolition of the Dominion Public Building is anticipated, it is recommended that the project should be designed with a general avoidance approach in line with provincial and federal guidelines. Exploration of an alternative development approach to locate the new wall as far as possible from the southwest corner of the Dominion Public Building is recommended.

2) Protection Measures:

- a. The proposed work for the WELRT project is largely located below grade and involves open cut excavation, the demolition of the existing streetcar loop, and construction of the new loop/platform along Bay Street from Front Street West to 40 Bay Street. Open cut construction will occur within the Dominion Public Building property and a new, below-grade wall will be constructed abutting the southeast corner of the building. Protection



measures in the form of protective barriers and notes on construction drawings should be implemented during the construction phase of the project to protect this building. Protective fencing, film, or netting should be installed around the base of the Dominion Public Building in the vicinity of the proposed work to protect the exterior of the building from accidental damage during construction.

- b. The Dominion Public Building is designated under Part IV Ontario Heritage Act under By-Law 423-2017 and designated under Part V of the Ontario Heritage Act as part of the Union Station HCD through By-law 634-2006. This Dominion Public Building should be noted on project drawings as a “protected heritage property” to identify the heritage status of the building to project personnel. The note should read:

“The Dominion Public Building is a significant heritage property with municipal protection under the *Ontario Heritage Act*. Use caution when conducting work in the vicinity of this building to avoid accidental damage”.
 - c. Construction work is proposed, within, and immediately adjacent to, the Dominion Public Building that introduces vibration-related risk. Vibration monitoring should be carried out by a qualified geotechnical engineer and should consist of a pre-construction survey, vibration monitoring during construction, and a post-construction survey.
- 3) This report should be submitted to the following agencies for review and comment:
- a. City of Toronto Heritage Preservation Services
 - b. MCM

The Dominion Public Building is a contributing building in the Union Station HCD. At grade work is anticipated for the construction of a new curb at the rear of the building (south elevation). However, the proposed curb is directly tied to the public realm (i.e. sidewalk) along Bay Street. A standalone HIA for the Union Station HCD to address these impacts has been prepared by WSP under a separate cover.

The above recommendations were prepared using the best available information regarding potential impacts at the time of writing. Should the proposed work change, then the preliminary impact assessment should be revisited to confirm identified impacts and proposed mitigation measures.



9.0 Assessor Qualifications

This report was prepared and reviewed by the undersigned, employees of WSP. WSP is one of North America's leading engineering firms, with more than 50 years of experience in the earth and environmental consulting industry. The qualifications of the assessors involved in the preparation of this report are provided in **Appendix E**.



10.0 Closure

This report was prepared for the exclusive use of the TTC and is intended to provide a HIA for the Dominion Public Building (1 Front Street West) (the Study Area). The Study Area the property located at 1 Front Street West (bounded by Front Street West on the north, Yonge Street on the east, 141 Bay Street on the south, and Bay Street on the west).

Any use which a third party makes of this report, or any reliance on or decisions to be made based on it, are the responsibility of the third party. Should additional parties require reliance on this report, written authorization from WSP will be required. With respect to third parties, WSP has no liability or responsibility for losses of any kind whatsoever, including direct or consequential financial effects on transactions or property values, or requirements for follow-up actions and costs.

The report is based on data and information collected during the cultural heritage assessment conducted by WSP. It is based solely a review of historical information, a property reconnaissance conducted in April to July 2021 and data obtained by WSP as described in this report. Except as otherwise maybe specified, WSP disclaims any obligation to update this report for events taking place, or with respect to information that becomes available to WSP after the time during which WSP conducted the cultural heritage assessment. In evaluating the Study Area, WSP has relied in good faith on information provided by other individuals noted in this report. WSP has assumed that the information provided is factual and accurate. In addition, the findings in this report are based, to a large degree, upon information provided by the current owner/occupant. WSP accepts no responsibility for any deficiency, misstatement or inaccuracy contained in this report as a result of omissions, misinterpretations or fraudulent acts of persons interviewed or contacted.

WSP makes no other representations whatsoever, including those concerning the legal significance of its findings, or as to other legal matters touched on in this report, including, but not limited to, ownership of any property, or the application of any law to the facts set forth herein. With respect to regulatory compliance issues, regulatory statutes are subject to interpretation and change. Such interpretations and regulatory changes should be reviewed with legal counsel.

We trust that the information presented in this report meets your current requirements. Should you have any questions, or concerns, please do not hesitate to contact the undersigned.



Respectfully Submitted,

**WSP Environment & Infrastructure,
a Division of WSP Canada Limited**

Prepared by:

Reviewed by:

Chelsea Dickinson, BA, CAHP
Cultural Heritage Specialist

Heidy Schopf, MES, CAHP
Cultural Heritage Team Lead



11.0 References

City of Toronto

- 2006 *Union Station Heritage Conservation District Plan, Appendix 4*. Available Online: https://www.toronto.ca/wp-content/uploads/2018/01/8dfc-CityPlanning_Union-Station-HCD.pdf. Retrieved 31 December 2020
- 2015 *St. Lawrence Neighbourhood Heritage Conservation District Plan*. Available Online: <https://www.toronto.ca/legdocs/mmis/2015/te/bgrd/backgroundfile-84943.pdf>
- 2021 *Heritage Conservation Districts & Studies*. Available Online: <https://www.toronto.ca/city-government/planning-development/heritage-preservation/heritage-conservation-districts-planning-studies/>

ERA Architects Inc., Et Al

- 2006 *Union Station Heritage Conservation District Plan*. Document on file at WSP.

Government of Ontario

- 1990 *Ontario Heritage Act, R.S.O. 1990, Chapter O.18*. Last amendment: July 1, 2019. Electronic document: <https://www.ontario.ca/laws/statute/90o18>. Last accessed July 15, 2021
- 2005 *InfoSheet #5, Heritage Resources in the Land Use Planning Process*. Available online: http://www.mtc.gov.on.ca/en/publications/Heritage_Tool_Kit_Heritage_PPS_info_Sheet.pdf. Last accessed July 15, 2021.
- 2006a *Ontario Regulation 9/06: Criteria for Determining Cultural Heritage Value or Interest*. Available online: <https://www.ontario.ca/laws/regulation/060009>. Last accessed July 15, 2021.
- 2006b *Ontario Regulation 10/06: Criteria for Determining Cultural Heritage Value or Interest of Provincial Significance*. Available online: <https://www.ontario.ca/laws/regulation/060010>. Last accessed July 15, 2021.
- 2010 *Standards & Guidelines for Conservation of Provincial Heritage Properties*. Available online: http://www.mtc.gov.on.ca/en/publications/Standards_Conservation.pdf. Last accessed July 15, 2021.
- 2014 *Ontario Regulation 157/10: Public Bodies – Part III.1 of the Act*. Available online: <https://www.ontario.ca/laws/regulation/100157>. Last accessed July 15, 2021.
- 2019a *Environmental Assessment Act, R.S.O. 1990, c. E.18*. Available online: <https://www.ontario.ca/laws/statute/90e18>. Last accessed July 15, 2021.
- 2020 *Provincial Policy Statement, Section 2.6 Cultural heritage and archaeology*. Electronic document: <https://files.ontario.ca/mmah-provincial-policy-statement-2020-accessible-final-en-2020-02-14.pdf>. Last accessed July 15, 2021.



Ministry of Citizenship and Multiculturalism (MCM)

2005 *Heritage Resources in the Land Use Planning Process, InfoSheet#5*. Available online:

http://www.mtc.gov.on.ca/en/publications/Heritage_Tool_Kit_Heritage_PPS_info_Sheet.pdf. Last accessed July 22, 2021.

2007 *Eight Guiding Principles in the Conservation of Built Heritage Properties*. Available online:

http://www.mtc.gov.on.ca/en/publications/InfoSheet_8%20Guiding_Principles.pdf. Last accessed July 15, 2020.

2017 *Standards and Guidelines for Conservation of Provincial Heritage Properties – Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties*. Ontario Ministry of Tourism, Culture and Sport, Toronto.

Parks Canada

2010 *Standards and Guidelines for the Conservation of Historic Places in Canada*.

Available online: <https://www.historicplaces.ca/media/18072/81468-parks-s+g-eng-web2.pdf>. Last accessed July 22, 2021.

Appendix A: Designation By-law for the Dominion Public Building



An agency of the Government of Ontario



Un organisme du gouvernement de l'Ontario

This document was retrieved from the Ontario Heritage Act Register, which is accessible through the website of the Ontario Heritage Trust at **www.heritagetrust.on.ca**.

Ce document est tiré du registre aux fins de la *Loi sur le patrimoine de l'Ontario*, accessible à partir du site Web de la Fiducie du patrimoine ontarien sur **www.heritagetrust.on.ca**.



City Clerk's Office

Secretariat

Ellen Devlin
Toronto and East York Community Council
City Hall, 2nd Floor, West
100 Queen Street West
Toronto, Ontario M5H 2N2

Ulli S. Watkiss
City Clerk

Tel: 416-392-7033

Fax: 416-397-0111

e-mail: teycc@toronto.ca

Web: www.toronto.ca

ONTARIO HERITAGE TRUST

MAY 11 2017

**IN THE MATTER OF THE ONTARIO HERITAGE ACT
R.S.O. 1990 CHAPTER 0.18 AND
CITY OF TORONTO, PROVINCE OF ONTARIO
1 FRONT STREET WEST**

NOTICE OF PASSING OF BY-LAW

Ontario Heritage Trust
10 Adelaide Street East
Toronto, Ontario
M5C 1J3

Take notice that the Council of the City of Toronto has passed By-law No. 423-2017 to designate 1 Front Street West (Dominion Public Building) (Toronto Centre-Rosedale Ward 28) as being of cultural heritage value or interest.

Dated at Toronto this 10th day of May, 2017.

Ulli S. Watkiss
City Clerk

Authority: Toronto and East York Community Council Item TE13.18, as adopted by City of Toronto Council on February 3 and 4, 2016

CITY OF TORONTO

BY-LAW 423-2017

To designate the property at 1 Front Street West (Dominion Public Building) as being of cultural heritage value or interest.

Whereas the *Ontario Heritage Act* authorizes the Council of a municipality to enact by-laws to designate real property, including all buildings and structures thereon, to be of cultural heritage value or interest; and

Whereas authority was granted by Council to designate the property at 1 Front Street West (the Dominion Public Building) as being of cultural heritage value or interest; and

Whereas the Council of the City of Toronto caused to be served upon the owners of the land and premises known as 1 Front Street West and upon the Ontario Heritage Trust, Notice of Intention to designate the property and caused the Notice of Intention to be posted on the City's web site for a period of 30 days in accordance with Municipal Code Chapter 162, Notice, Public, Article II, § 162-4.1. Notice requirements under the *Ontario Heritage Act*; and

Whereas the reasons for designation are set out in Schedule "A" to this by-law; and

Whereas no notice of objection was served upon the Clerk of the municipality;

The Council of the City of Toronto enacts:

1. The property at 1 Front Street West, more particularly described in Schedule "B" attached to this by-law, is designated as being of cultural heritage value or interest.
2. The City Solicitor is authorized to cause a copy of this by-law to be registered against the property described in Schedule "B" to this by-law in the proper Land Registry Office.
3. The City Clerk is authorized to cause a copy of this by-law to be served upon the owners of the property at 1 Front Street West and upon the Ontario Heritage Trust and to cause notice of this by-law to be posted on the City's web site for a period of 30 days in accordance with Municipal Code Chapter 162, Notice, Public, Article II, § 162-4.1. Notice requirements under the *Ontario Heritage Act*.

Enacted and passed on April 28, 2017.

Frances Nunziata,
Speaker

Ulli S. Watkiss,
City Clerk

(Seal of the City)

SCHEDULE "A"
REASONS FOR DESIGNATION
(STATEMENT OF SIGNIFICANCE)

The property at 1 Front Street West is worthy of designation under Part IV, Section 29 of the Ontario Heritage Act for its cultural heritage value, and meets the criteria for municipal designation prescribed by the Province of Ontario under the three categories of design, associative and contextual values. Listed on the inaugural City of Toronto Inventory of Heritage Properties (now the City of Toronto's Heritage Register) in 1973, the Dominion Public Building is a key component of the Union Station Heritage Conservation District, which was designated under Part V of the Ontario Heritage Act in 2006. Parks Canada's Federal Heritage Buildings Review Office (FHBRO) designated the Dominion Public Building as a Classified Federal Heritage Building in 1983, and it was listed on the Canadian Register of Historic Places in 2011.

Description

Anchoring the southwest corner of Yonge Street and Front Street West, the Dominion Public Building is a large-scale federal government building that was commissioned by the Government of Canada's Department of Public Works and originally served as Toronto's Customs House. Completed in two phases in 1929-31 (centre and east pavilions) and 1934-35 (west pavilion), the north section of the building on Front Street West rises five stories, while the rear section extends six stories to address the change in grade.

Statement of Cultural Heritage Value

The Dominion Public Building has cultural heritage value for its historic role as the federal government's Toronto Customs House for the administration, taxation, inspection and storage of imported and exported goods. Conceived by the federal government as a Customs House only, the Dominion Public Building was completed as the third and largest Customs House in the city, as well as the first to incorporate the public offices and the examining warehouse in the same building. The construction of the Dominion Public Building during the Great Depression of the 1930s was a reflection of the significance of Toronto to the nation's economic status and recovery.

The associative value of the Dominion Public Building is also through its connection to T. W. Fuller, who served from 1927 to 1936 as the Chief Architect of the Federal Department of Public Works, which was responsible for the in-house design of nearly all public architecture in Canada in the late 19th and early 20th centuries. During the Great Depression, the Chief Architect oversaw monumental projects in the country's major centres, including Toronto where the Dominion Public Building remains Fuller's best known work.

From a design standpoint, the Dominion Public Building is valued as a rare and exceptional example in Canada of Beaux-Arts Classicism, the international style popularized for monumental public architecture in the late 18th and early 19th centuries. The Dominion Public Building has the hallmarks of the style with its imposing scale, the symmetrical organization of the facades, the hierarchy of spaces from grand to utilitarian, the decorative detailing inspired by classical precedents, and its placement in a highly visible and prominent setting. As a rare and early surviving example of a public building in Toronto constructed by the federal government in the

early 20th century, the Dominion Public Building was among the first applications of Beaux Arts Classicism to a federal design. The Dominion Public Building stands as an important physical reminder of the imposing public spaces created by the federal government, combining the monumentality and grandeur of the Front Street portion of the building (including the interior Long Room) with the practicality and accessibility of the warehouse component to the rear. Designed in two phases, over time the Dominion Public Building changed from a Customs House to a multi-use federal building, and the interior alterations dating to the 1980s and 1990s are part of the evolution of the building.

With the neighbouring Union Station (which was officially opened in 1927), the Dominion Public Building establishes the character of the area along Front Street, west of Yonge Street. Following the Great Fire of 1904 that destroyed most of the existing buildings in this area, Toronto's Civic Improvement Committee commissioned a plan (1911) by architect John M. Lyle that was based on the principles of the City Beautiful Movement and that envisioned Front Street as a grand boulevard with expansive tracts reserved for monumental architecture that included a new Union Station and Customs House (the Dominion Public Building). According to the federal government, "The Dominion Public Building and Union Station together form probably the most imposing Beaux Arts streetscape in Canada."

Contextually, the Dominion Public Building is historically, visually, physically and functionally linked to its important setting on the south side of Front Street where it anchors the southwest corner of Yonge Street (Toronto's "main street") and extends across the entire city block to Bay Street. As the third Customs House on the site, the Dominion Public Building occupies what was historically among the most sought-after locations in Toronto with its proximity to the city's financial district directly north, Union Station as its neighbour on the west, and the railway corridor and harbour to the south. The Dominion Public Building, with its office and warehouse components aligned to access Front, Yonge and Bay Streets, forms an important precinct.

As a monumental federal government building in a prominent location beside and complementing Union Station, the Dominion Public Building is a local landmark in Toronto.

Heritage Attributes

The heritage attributes of the building known historically as the Dominion Public Building on the property at 1 Front Street West are:

- The placement, setback and orientation of the building on the south side of Front Street West where it extends from Yonge Street to Bay Street
- The scale, form and massing of the irregularly-shaped plan that rises five stories along Front Street to the flat roofline and follows the curve of Front Street west of Yonge
- The partially raised stone base with window openings, which is extended in height where the grade changes south of Front Street West
- On the reinforced concrete structure and above the granite clad foundation, the limestone cladding that is channelled on the extended first (ground) floor and smoothly dressed in the stories above, with the stone and metal detailing
- The tripartite organization of the north elevation on Front Street into the centre pavilion and the adjoining east and west pavilions, with the west pavilion rounded at the northwest corner and the east pavilion truncated at the northeast corner

- The horizontal division of the north elevation by the cornices above the extended first storey and beneath the parapet
- The centre pavilion, with the five-storey projecting frontispiece composed of six freestanding Ionic columns supporting the entablature inscribed "Dominion Public Building A.D. MCMXXX"
- At the base of the centre frontispiece, the two-storey main entrance where the three round-arched openings contain paired bronze doors beneath large transoms with metal mullions incorporating rope detailing and cast metal beavers (as symbols of Canada)
- The single secondary entrances on the north elevations of the east and west pavilions that repeat the detailing introduced on the central entrance The east elevation on Yonge Street, which extends 12 bays and is divided into three parts with a recessed centre section
- The five-bay west elevation facing Bay Street
- The fenestration on the north, east and west elevations, which is arranged between Ionic pilasters and features double-height round arched openings with keystones in the first floor, and single, paired and triple flat-headed openings with spandrel panels in the remaining floors, and the metal window mullions with the rope detailing in the first and second floors
- The classical detailing on the elevations facing Front, Yonge and Bay Streets, including the carved acanthus leaves, the dentils and the stone lion's heads
- On the rear (south) elevation, the end bays (east and west) that continue the decorative detailing and fenestration from the east and west elevations
- The central utilitarian section of the south wall with the fenestration and raised centre section
- On the interior, the organization and layout of the public spaces on the first-floor, including the three marble-clad lobbies
- The detailing in the east vestibule and lobby, with the marble floors, dado and door and window surrounds, including the marble door pediment inscribed "Long Room", the bronze window mullions between the lobby and the Long Room, the "enquiry" window in the lobby, and the classical detailing
- In the east pavilion, the two-storey Long Room, with the marble floors, dado and door and window surrounds, the marble counters with the brass wickets, the double row of square columns with the pilasters and the Corinthian capitals, the entrances with the bronze doors and classical detailing, the metal window mullions, the second-storey gallery with the brass balustrade, and the coffered plaster ceiling with the dentils and mouldings.

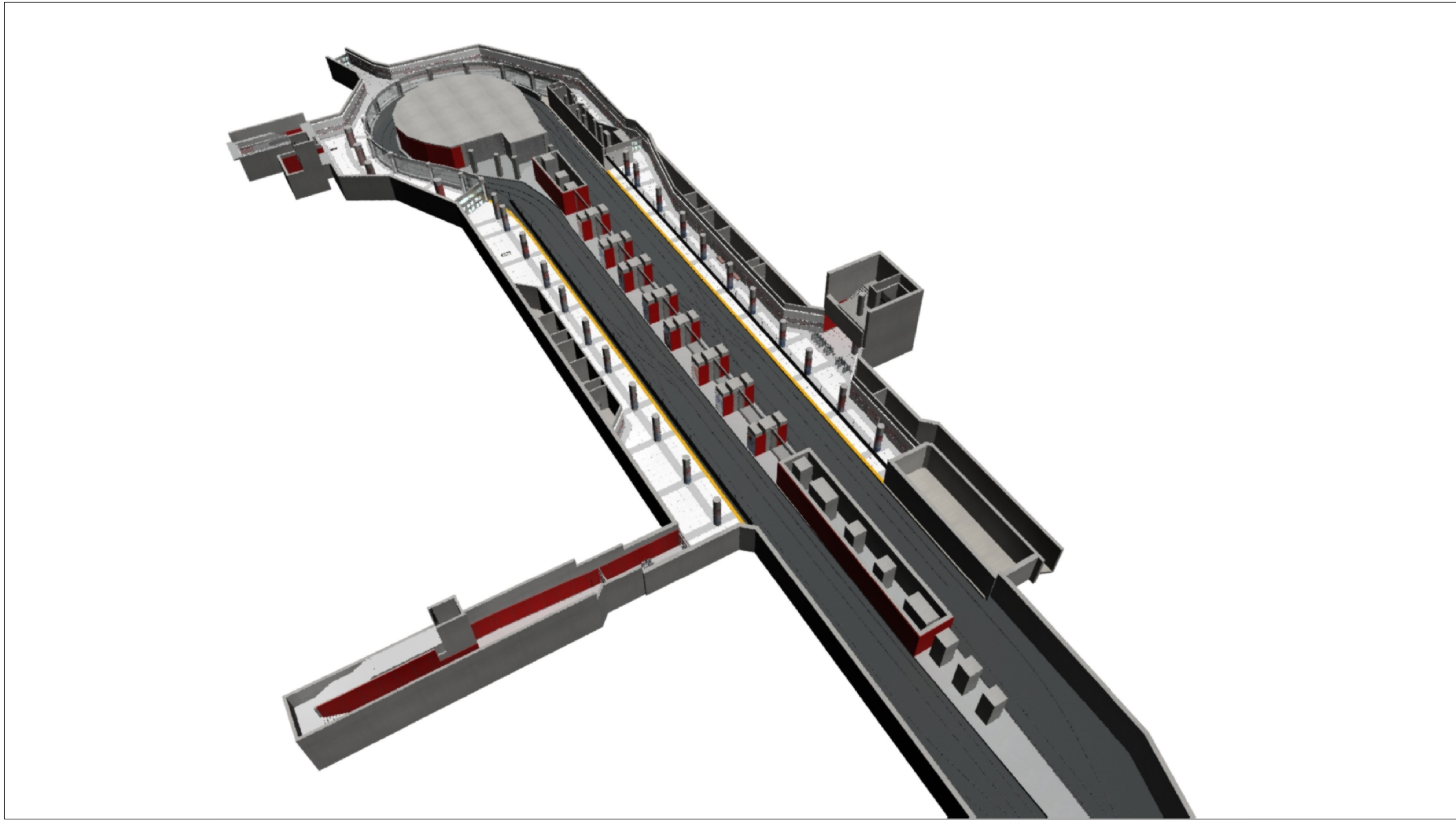
SCHEDULE "B"
LEGAL DESCRIPTION

PIN 21396-0116(LT)

LT 23 PL 12164; PTS 1, 2, PL 63R - 3854

City of Toronto, Province of Ontario
Land Titles Division of the Toronto Registry Office (No. 66)

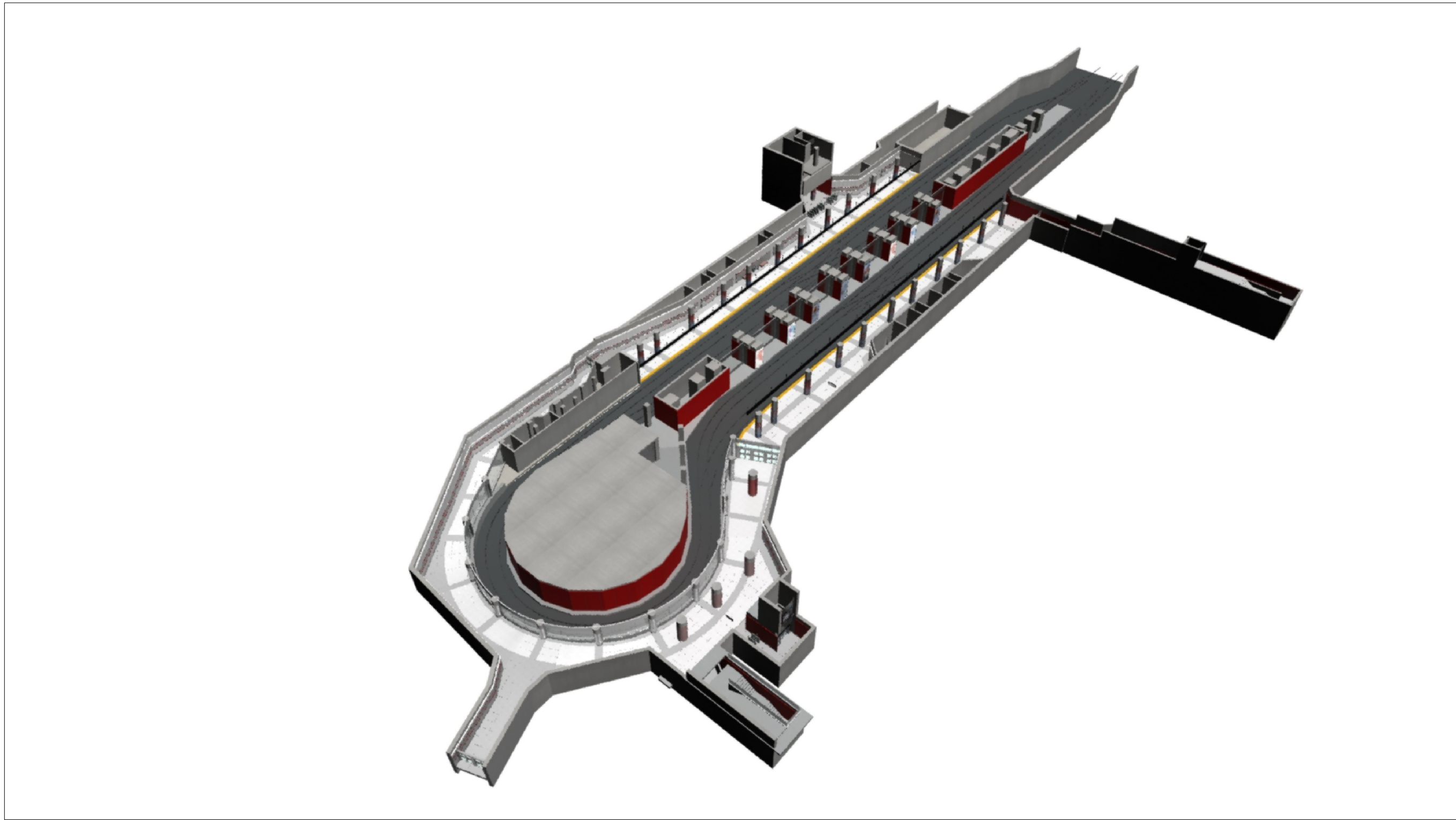
Appendix B: Proposed Work



DETAIL

3D RENDER

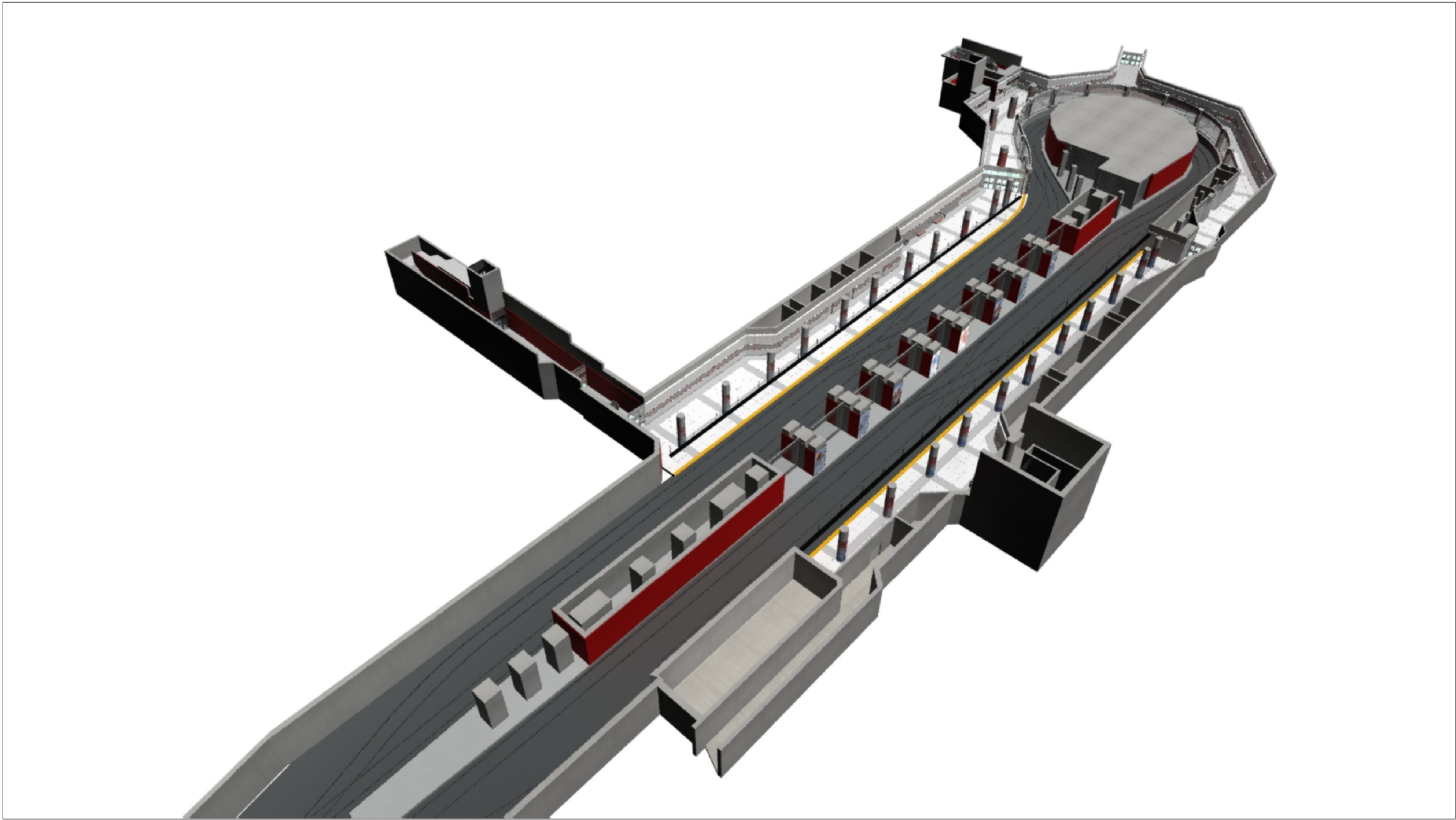
3
AA011



DETAIL

3D RENDER

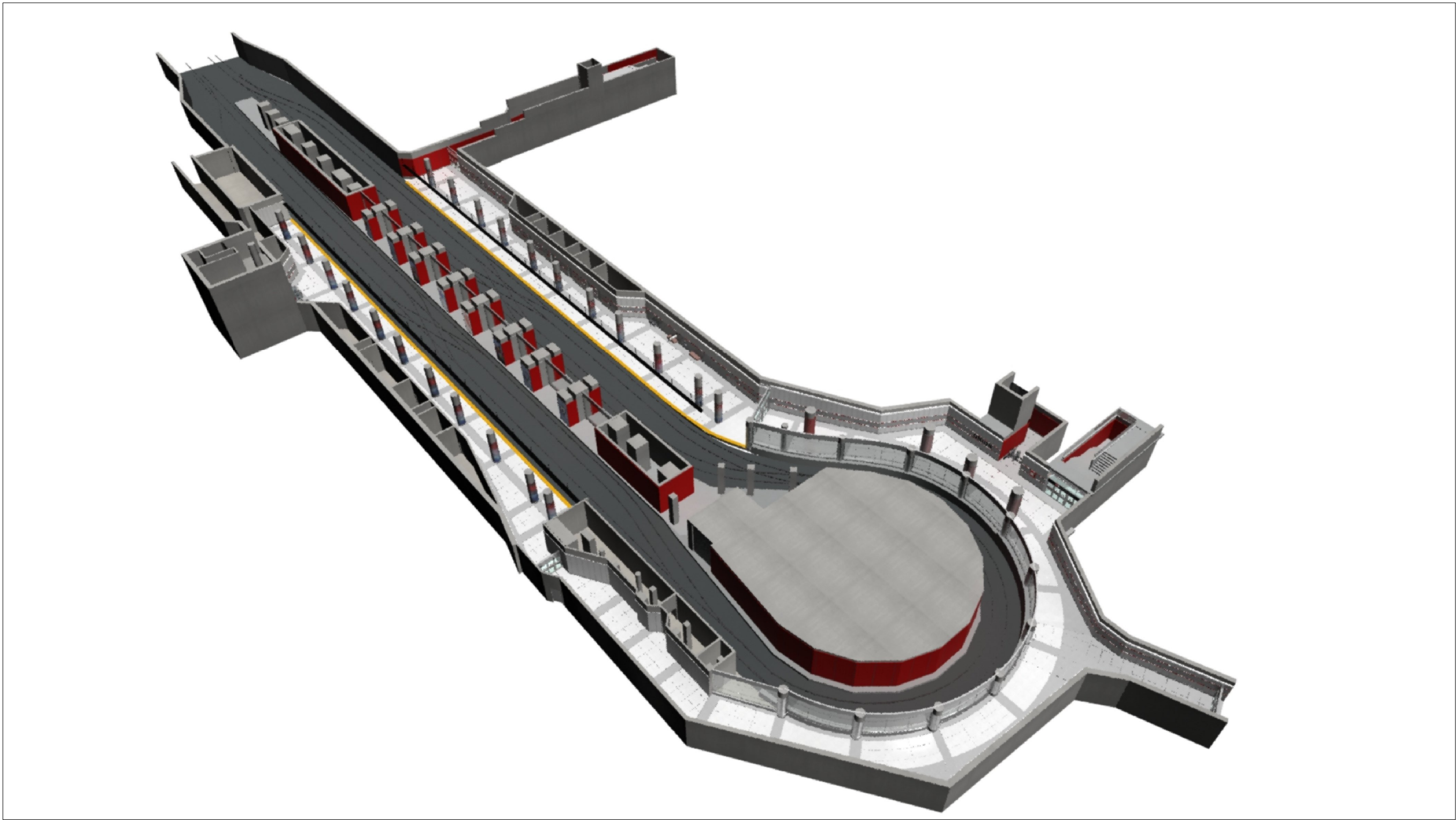
4
AA011



DETAIL

3D RENDER

1
AA011



DETAIL

3D RENDER

2
AA011

| REVISIONS | REVISIONS |
|-----------|-----------|
| △ | △ |
| △ | △ |
| △ | △ |

30% REVIEW
DRAWING
NOT FOR CONSTRUCTION

wood.


WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS
3400 HARVESTER ROAD, SUITE 100
BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 336 2263

| | | |
|---------|-------------|------------|
| DRAWN | E. ENGLAND | YYYY/MM/DD |
| CHECKED | O. PHELAN | 2020/10/23 |
| CORRECT | J. STRASMAN | |
| SCALE | N.T.S. | |

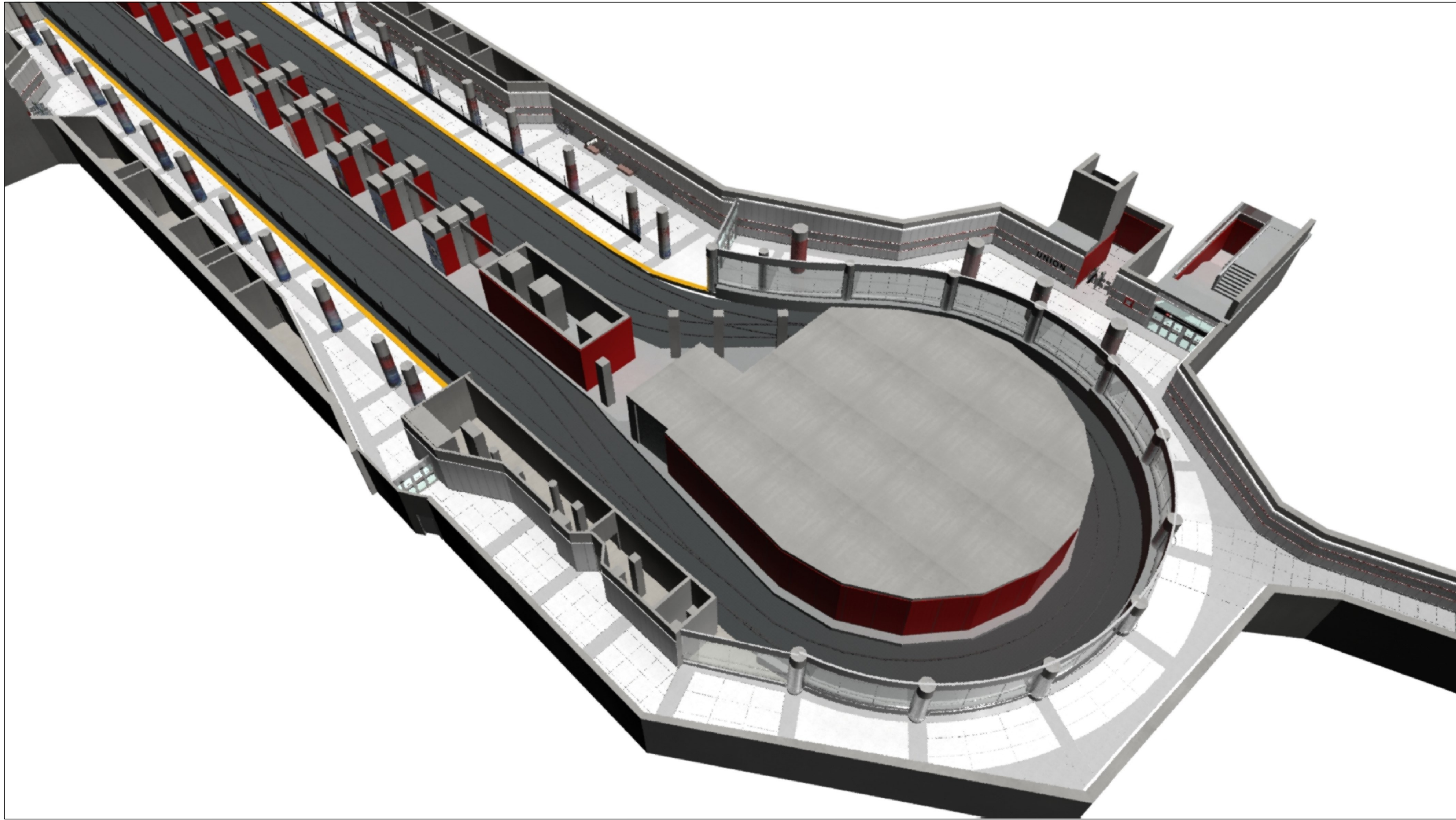
WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK

UNION STATION

AERIAL VIEW RENDERS

| | | |
|---|----------|-----------|
| Contract: RE35-1 | | |
|  | | |
| TORONTO TRANSIT COMMISSION ENGINEERING DEPARTMENT | | |
| Dwg. No. RE35-1-AA011 | Rev. No. | Sheet No. |

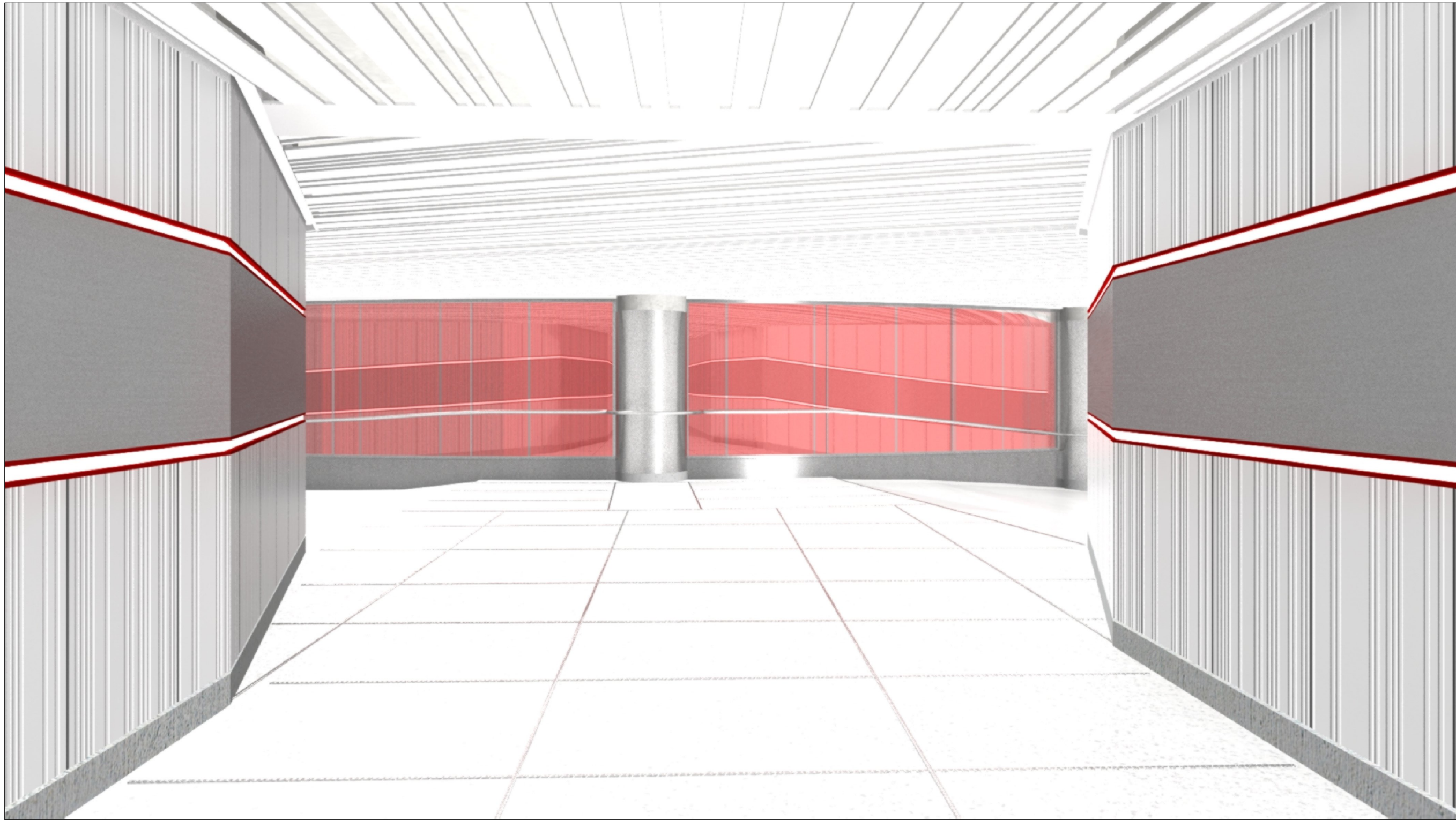
Plot Date: 7-JUL-2021



DETAIL

3D RENDER

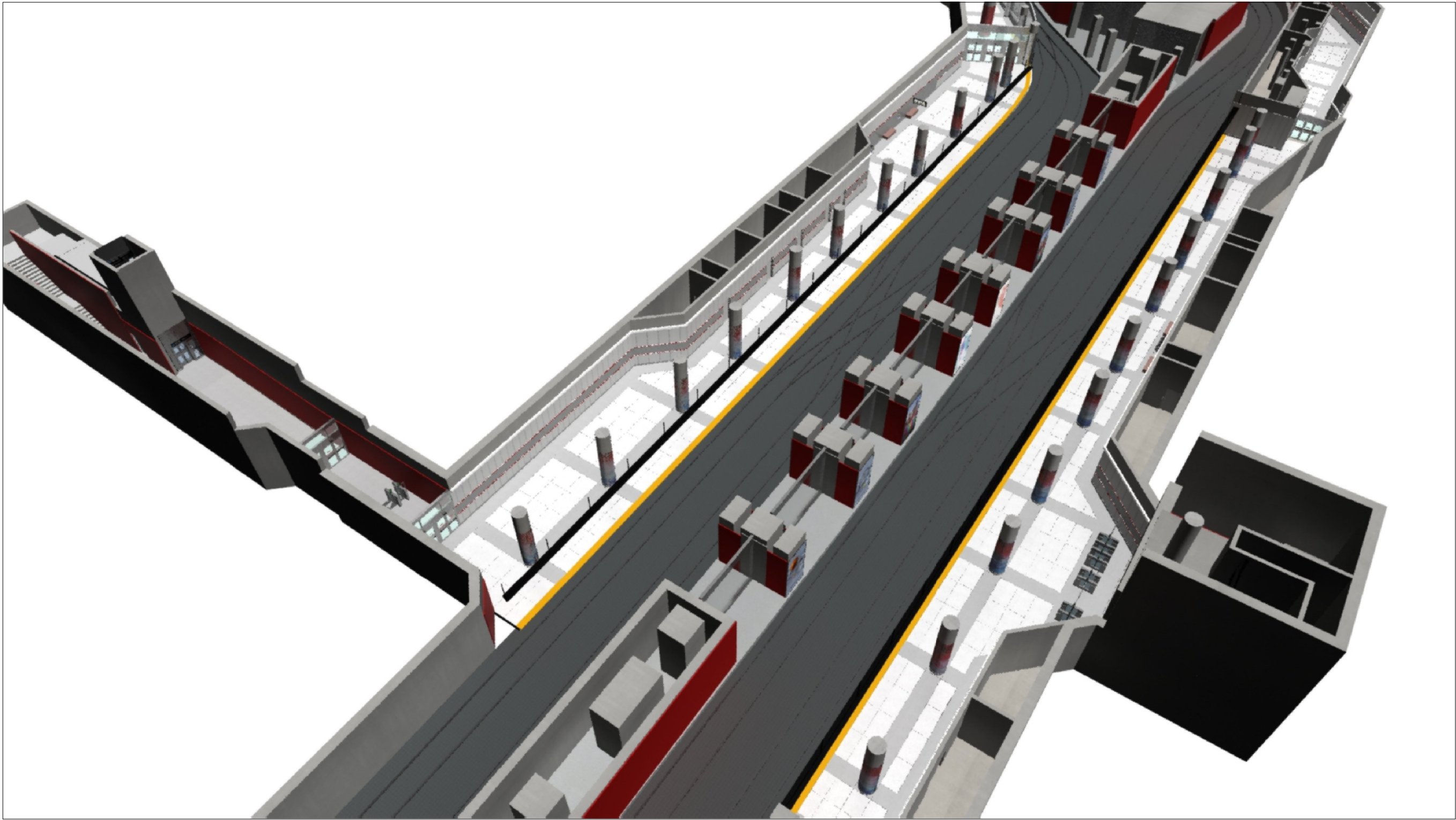
3
AA012



DETAIL

3D RENDER

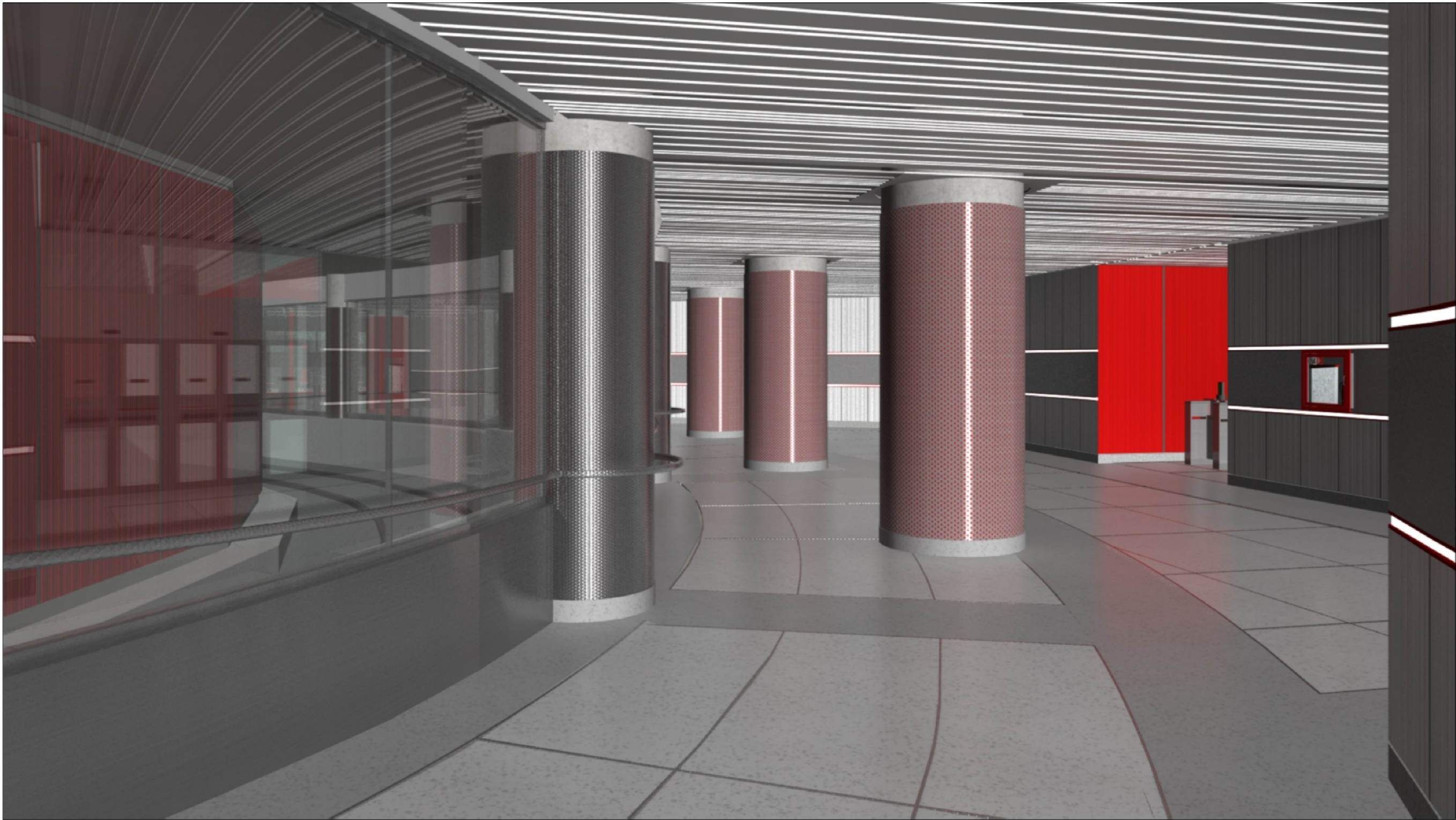
4
AA012



DETAIL

3D RENDER

1
AA012



DETAIL

3D RENDER

2
AA012

| REVISIONS | REVISIONS |
|-----------|-----------|
| △ | △ |
| △ | △ |
| △ | △ |

30% REVIEW
DRAWING
NOT FOR CONSTRUCTION

wood.

WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS
3450 HARVESTER ROAD, SUITE 100
BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 336 2553

| | |
|--------------------------------|--------------------------|
| DRAWN E.ENGLAND | YYYY/MM/DD 2020/10/23 |
| CHECKED O.PHELAN | |
| CORRECT J.STRASMAN | |
| SCALE | N.T.S. |

WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK

UNION STATION

PLATFORM LEVEL VIEWS
3D AXONOMETRIC RENDERERS

| | | |
|---|----------|-----------|
| Contract: RE35-1 | | |
|  | | |
| TORONTO TRANSIT COMMISSION ENGINEERING DEPARTMENT | | |
| Dwg. No. RE35-1-AA012 | Rev. No. | Sheet No. |
| Plot Date: 7-JUL-2021 | | |

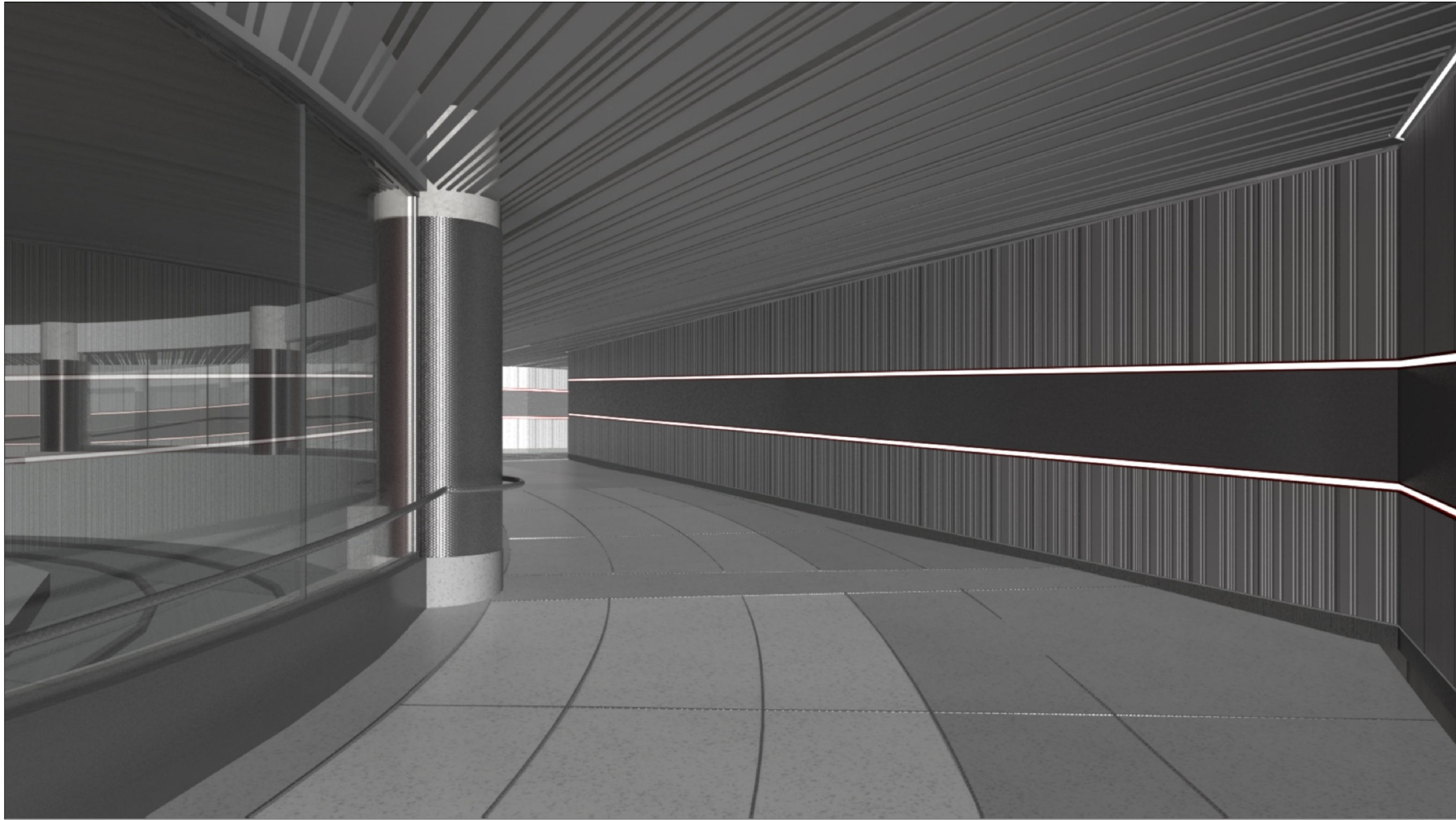
engbord3.dgn (BORDER) FEBRUARY 2, 2020

BLDG. REF. No.

FILENAME:

SHEET No.

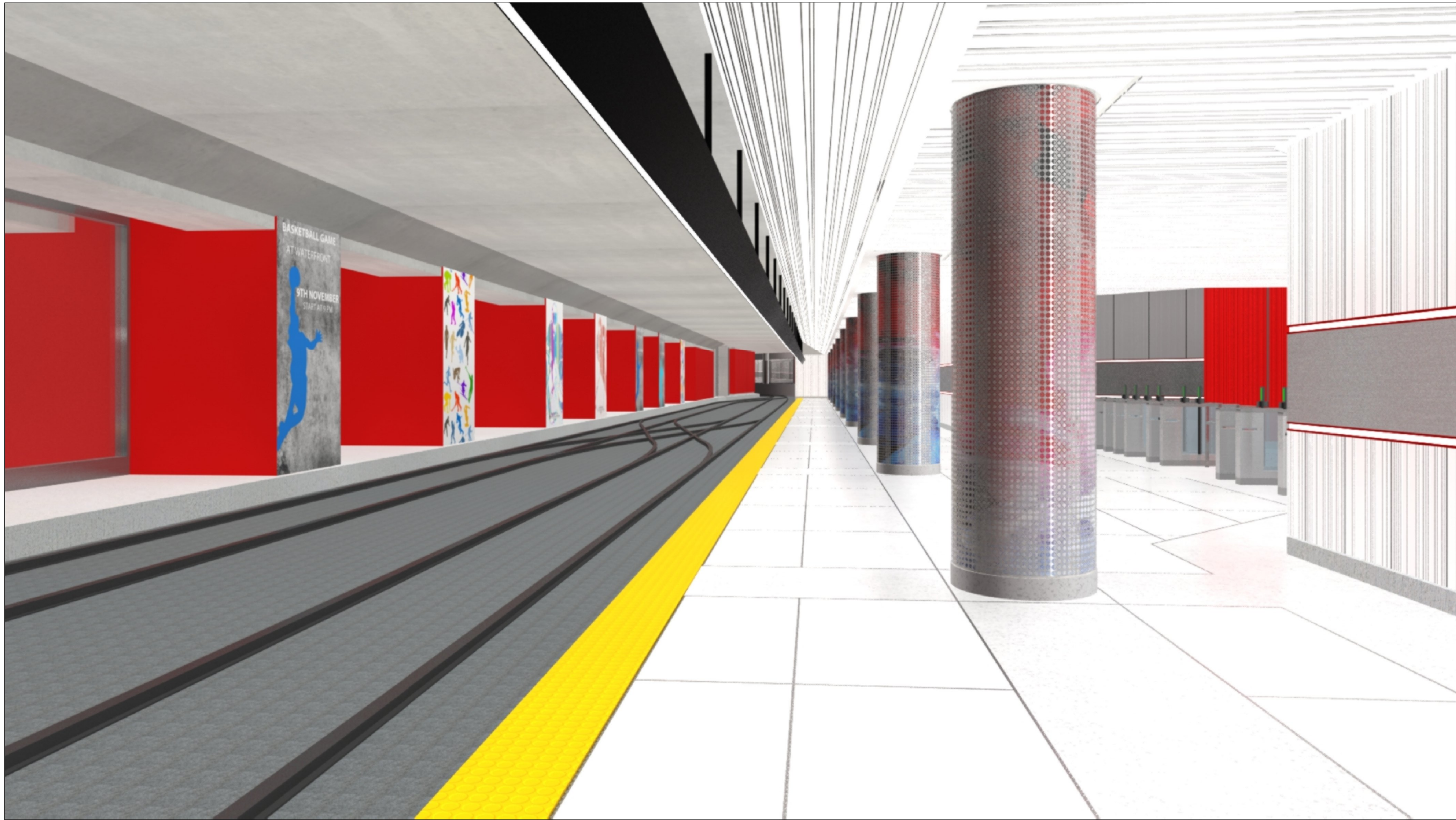
DRAWING No.



DETAIL

3D RENDER

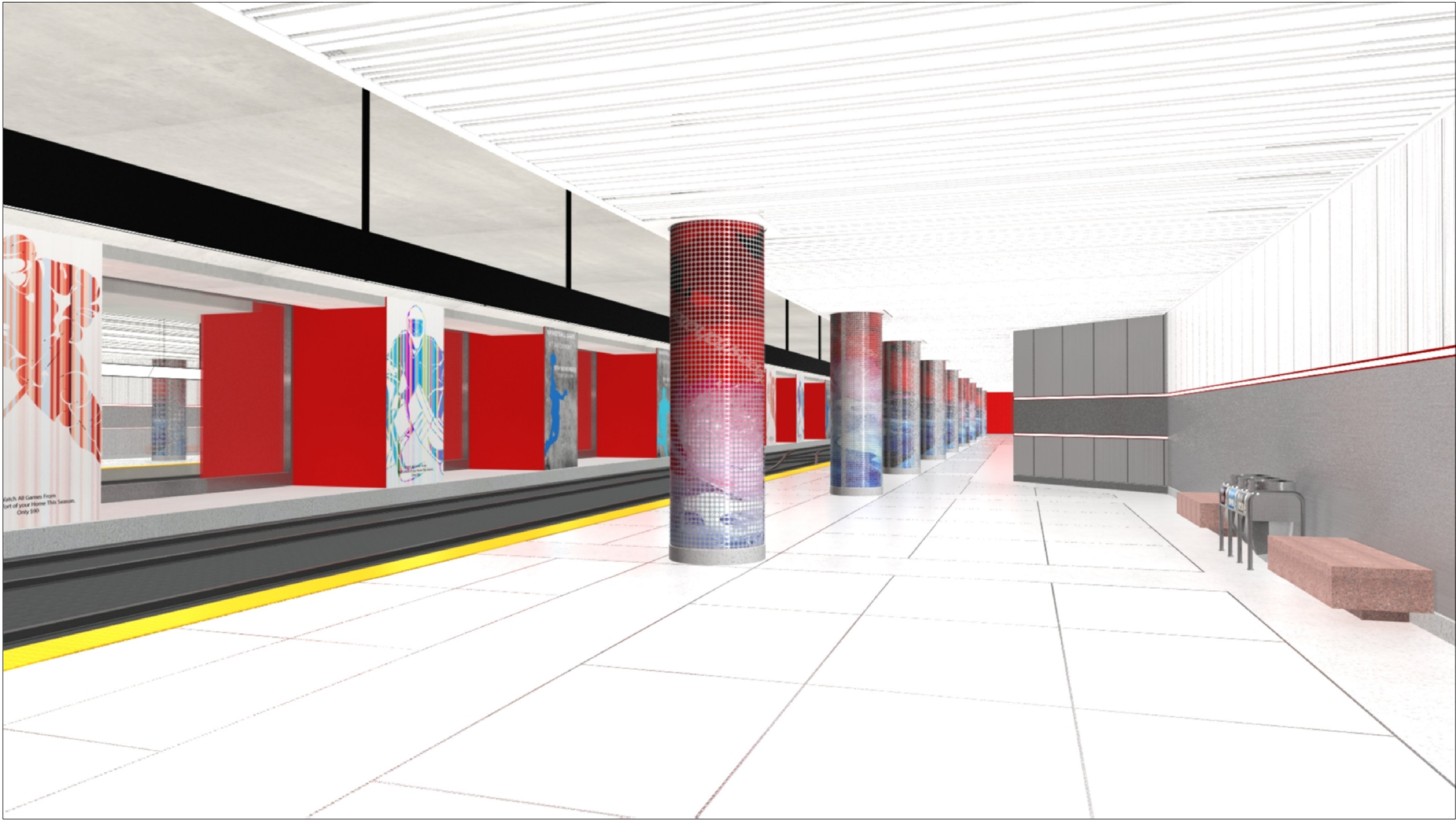
3
AA013



DETAIL

3D RENDER

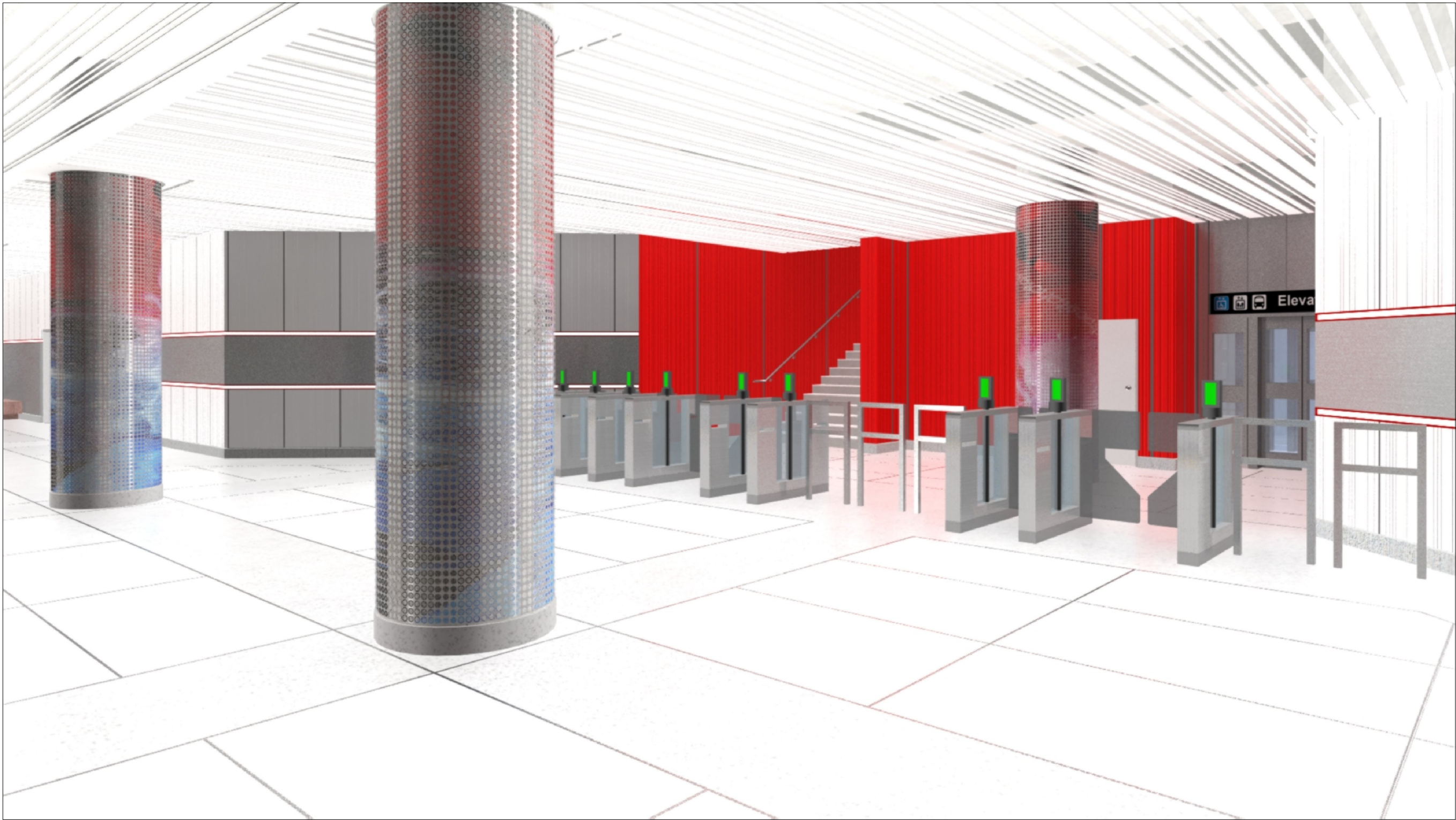
4
AA013



DETAIL

3D RENDER

1
AA013



DETAIL

3D RENDER

2
AA013

| REVISIONS | REVISIONS |
|-----------|-----------|
| △ | △ |
| △ | △ |
| △ | △ |

30% REVIEW
DRAWING
NOT FOR CONSTRUCTION

wood.

WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS
3450 HARVESTER ROAD, SUITE 100
BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 336 2263

| | | |
|---------|------------|------------|
| DRAWN | E.ENGLAND | YYYY/MM/DD |
| CHECKED | O.PHELAN | 2020/10/23 |
| CORRECT | J.STRASMAN | |
| SCALE | N.T.S. | |

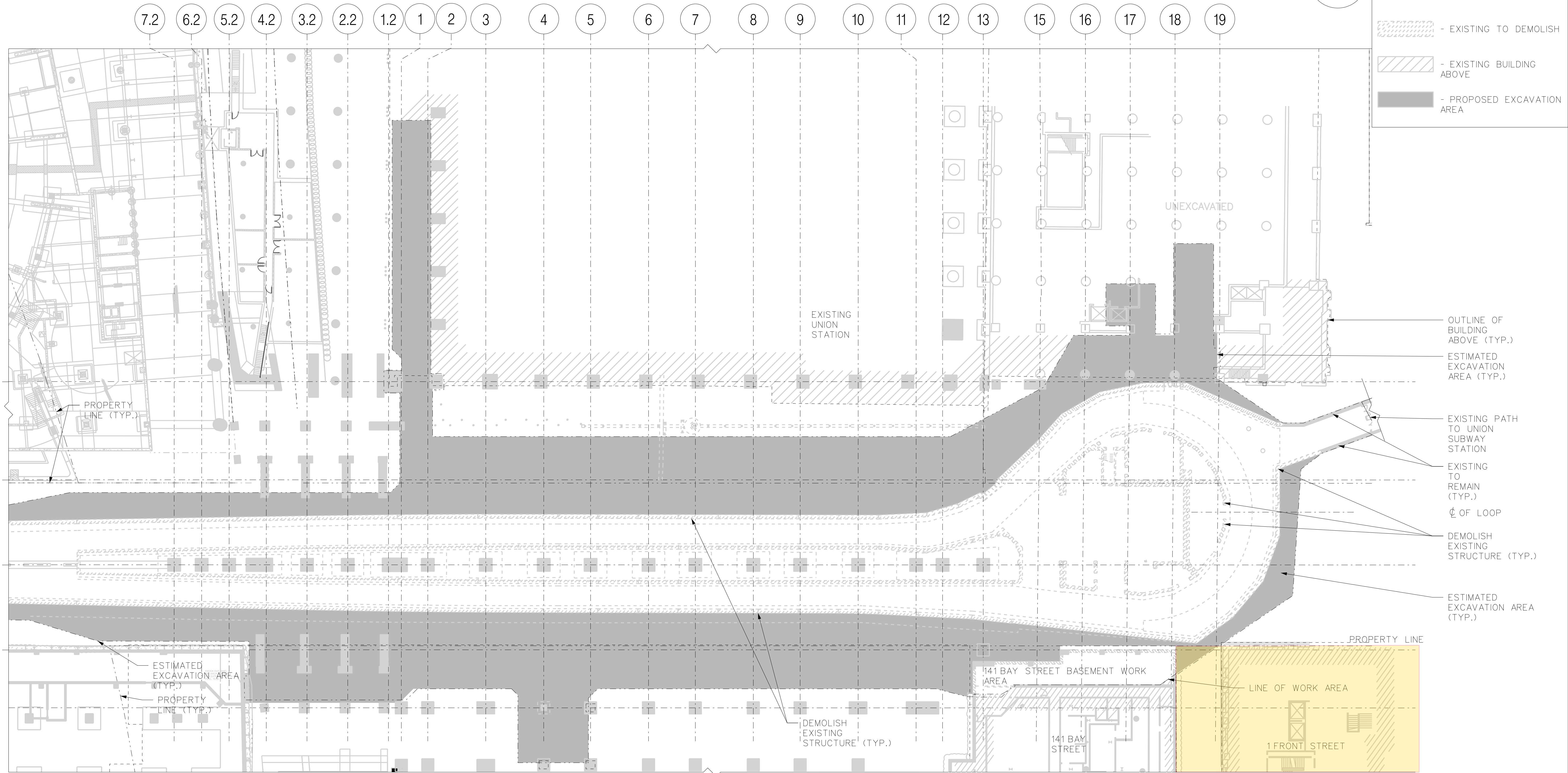
WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK

UNION STATION

PLATFORM LEVEL VIEWS
3D AXONOMETRIC RENDERERS

| | | |
|---|----------|-----------|
| Contract: RE35-1 | | |
|  | | |
| TORONTO TRANSIT COMMISSION ENGINEERING DEPARTMENT | | |
| Dwg. No. RE35-1-AA013 | Rev. No. | Sheet No. |
| Plot Date: 7-JUL-2021 | | |

CADD FILE NAME: SAI_2s_demo_scplatt_01.dgn
engbord3.dgn (BORDER) FEBRUARY 2, 2020
BLDG. REF. No.
FILENAME:
SHEET No.
DRAWING No.



PLAN
UNION STATION STREETCAR LOOP PLATFORM LEVEL
DEMOLITION PLAN
1
AD001

| REVISIONS | REVISIONS |
|-----------|-----------|
| △ | △ |
| △ | △ |
| △ | △ |

30% REVIEW
DRAWING
NOT FOR CONSTRUCTION

wood.

WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS
3450 HARVESTER ROAD, SUITE 100
BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 336 2263


DRAWN ... E. ENGLAND ...
CHECKED ... O. PHELAN ...
CORRECT ... J. STRASMAN ...
SCALE
0 2 4 6 8 10 12m

WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK

UNION STATION

PLATFORM LEVEL
DEMOLITION PLAN

Contract: RE35-1

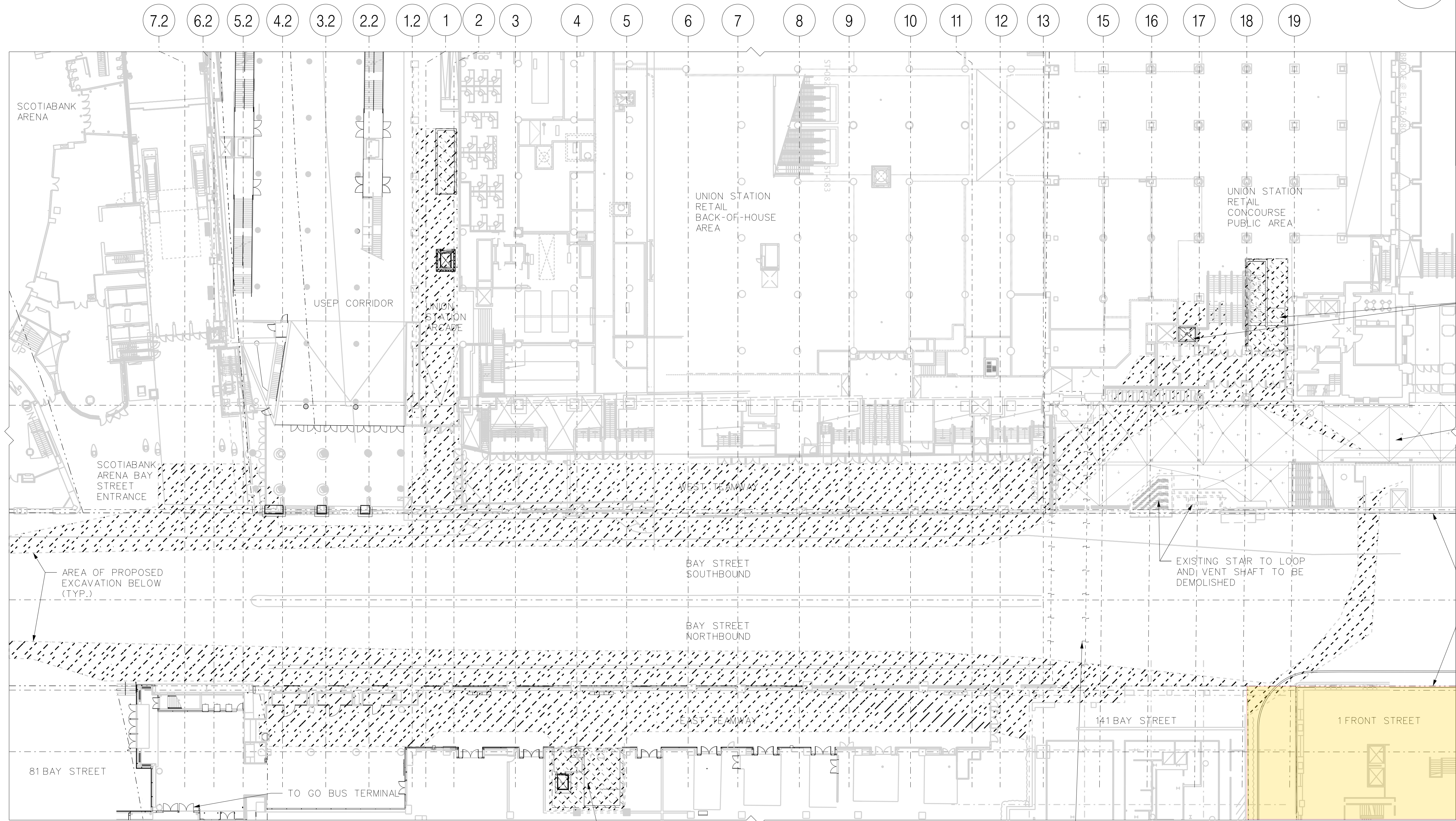


TORONTO TRANSIT COMMISSION
ENGINEERING DEPARTMENT

Dwg. No. RE35-1-AD001
Rev. No.
Sheet No.

Plot Date: 7-JUL-2021

CADD FILE NAME: SAI_2s_demo_st_01.dgn
engbord3.dgn (BORDER) FEBRUARY 2, 2020
BLDG. REF. No.
FILENAME:
SHEET No.
DRAWING No.



PLAN

UNION STATION STREETCAR LOOP STREET LEVEL
DEMOLITION PLAN

1
AD002

LEGEND

- EXISTING TO REMAIN

- EXISTING TO DEMOLISH

- EXISTING BUILDING ABOVE

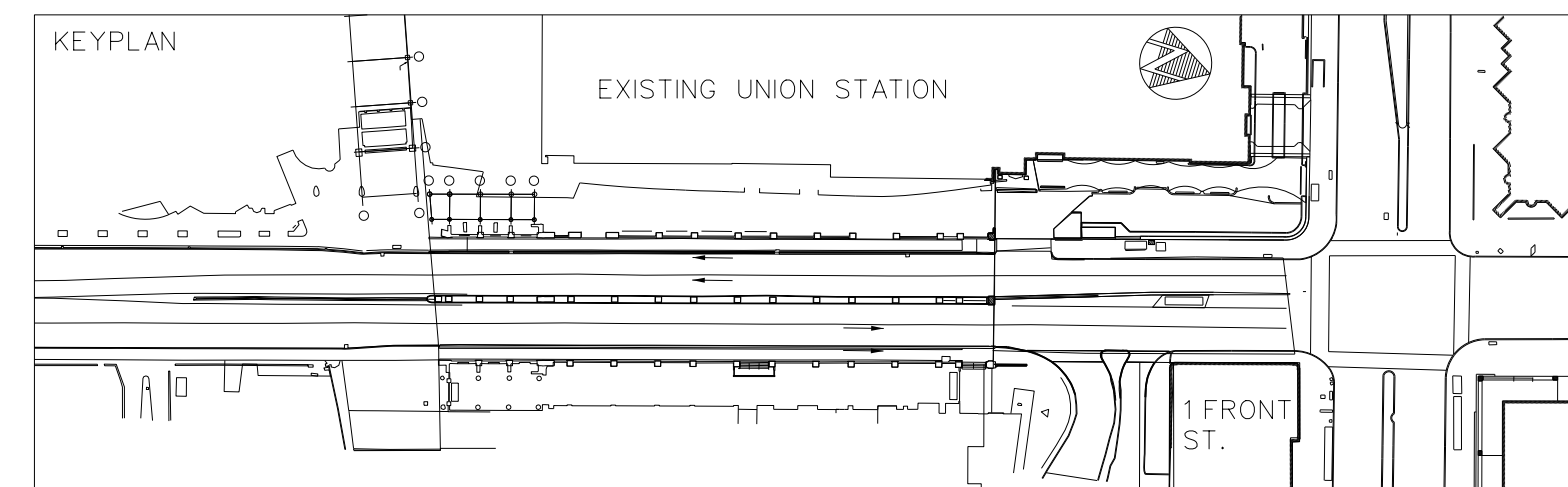
- EXTENT OF EXCAVATION AREA BELOW

- STREET LEV. PROPOSED DEMOLITION AREA

PROPOSED EXTENT OF
DEMOLITION AT STREET LEVEL
(TYP.)

BAY STREET MOAT

PROPERTY LINE (TYP.)



| REVISIONS | REVISIONS |
|-----------|-----------|
| | |
| | |
| | |

30% REVIEW
DRAWING
NOT FOR CONSTRUCTION

wood.

WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS
3400 HARVESTER ROAD, SUITE 100
BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 336 2583

DRAWN E.ENGLAND
CHECKED O.PHELAN
CORRECT J.STRASMAN
SCALE
0 2 4 6 8 10 12m

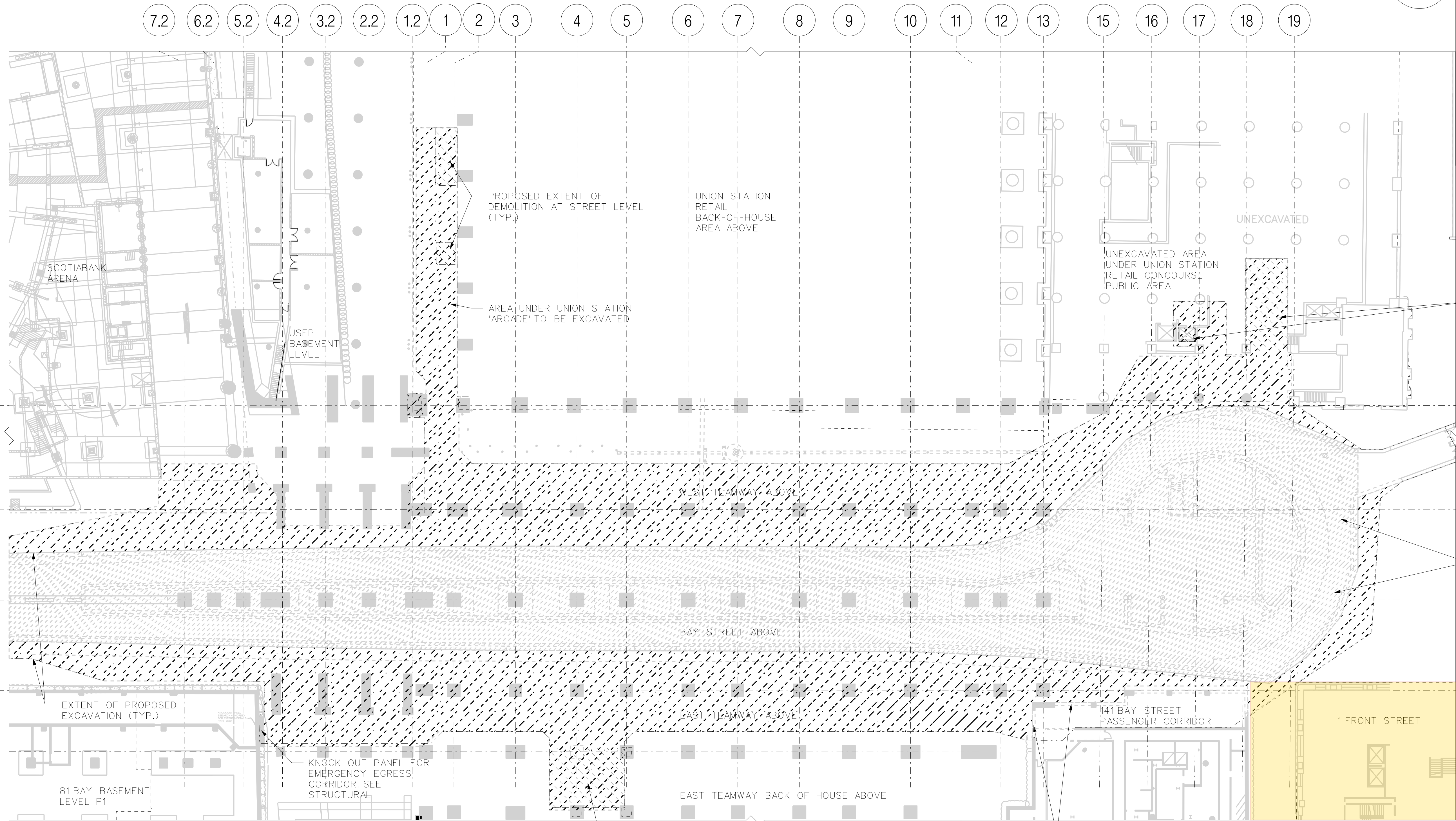
YYYY/MM/DD
2021/05/05

WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK

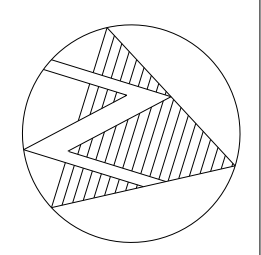
UNION STATION
STREET LEVEL DEMOLITION
PLAN

Contract: RE35-1
TORONTO TRANSIT COMMISSION
ENGINEERING DEPARTMENT
Dwg. No. RE35-1-AD002
Rev. No. Sheet No.
Plot Date: 7-JUL-2021

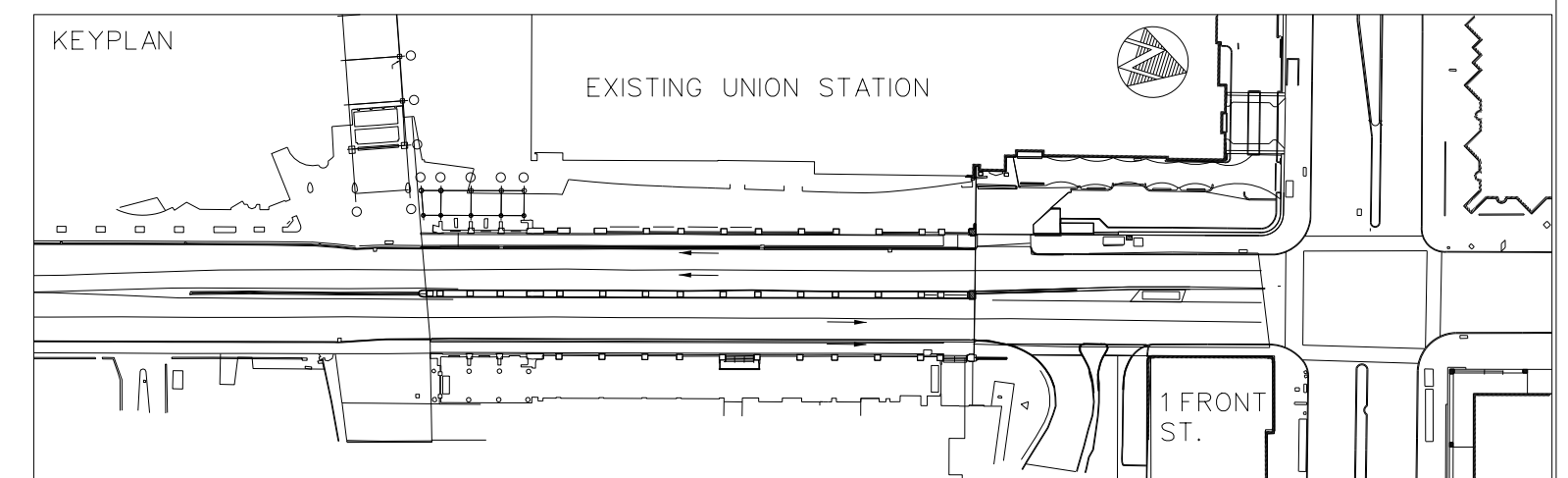
CADD FILE NAME: SAI_2s_demo_rep_01.dgn
engbord3.dgn (BORDER) FEBRUARY 2, 2020
BLDG. REF. No.
FILENAME:
SHEET No.
DRAWING No.



PLAN
UNION STATION STREETCAR LOOP STREET LEVEL
DEMOLITION REFLECTED CEILING PLAN



- LEGEND**
- EXISTING TO REMAIN
 - EXISTING TO DEMOLISH
 - EXISTING BUILDING ABOVE
 - EXTENT OF EXCAVATION AREA BELOW
 - STREET LVL PROPOSED DEMOLITION AREA



| REVISIONS | REVISIONS |
|-----------|-----------|
| △ | △ |
| △ | △ |
| △ | △ |

**30% REVIEW
DRAWING**
NOT FOR CONSTRUCTION



WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS
3400 HARVESTER ROAD, SUITE 100
BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 336 2553

DRAWN E. ENGLAND
CHECKED O. PHELAN
CORRECT J. STRASMAN
SCALE
0 2 4 6 8 10 12m

**WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK**

UNION STATION
STREETCAR PLATFORM LEVEL
DEMOLITION REFLECTED CEILING PLAN

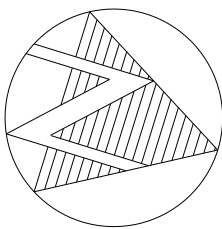
Contract: RE35-1

TORONTO TRANSIT COMMISSION
ENGINEERING DEPARTMENT

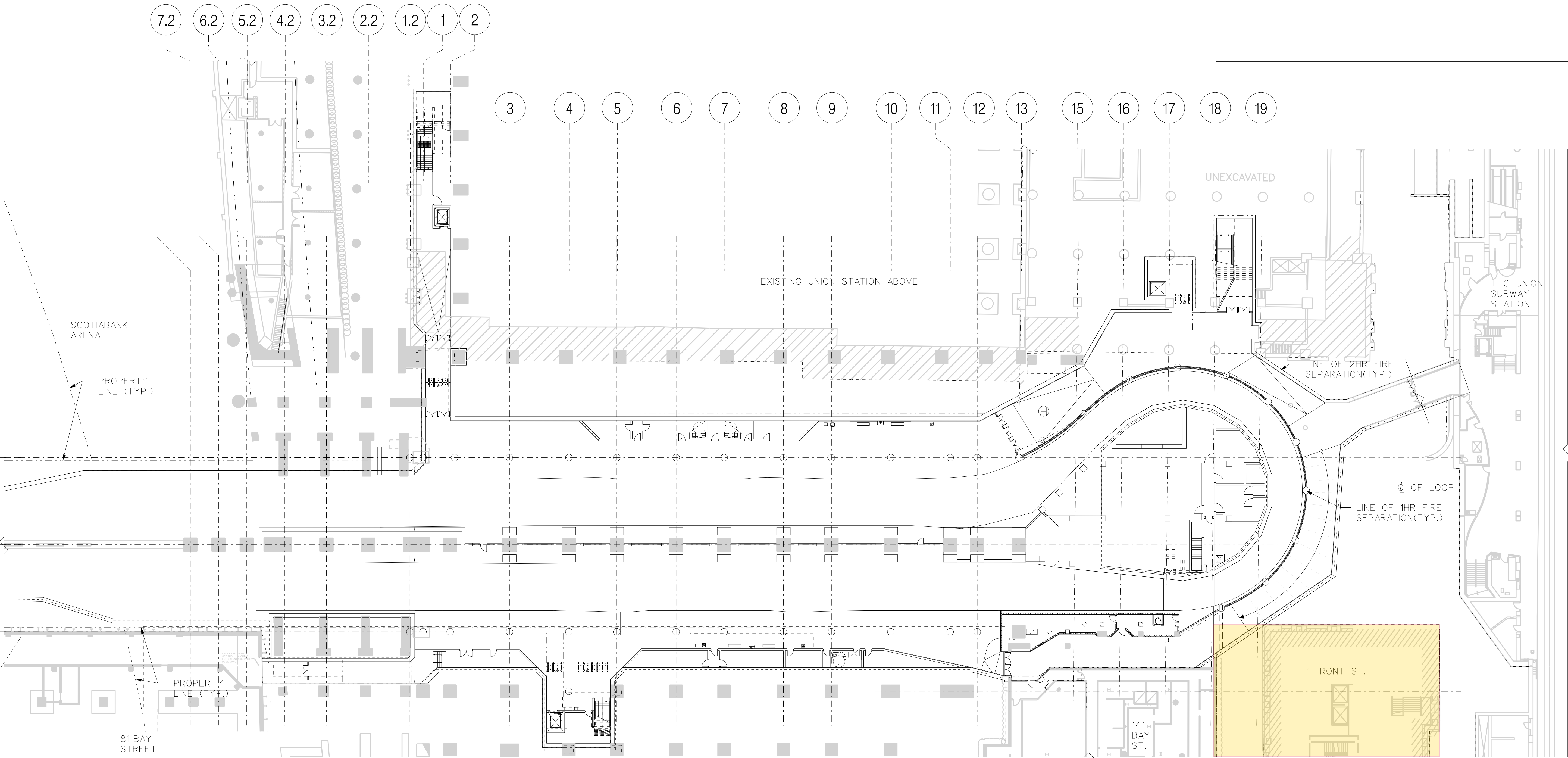
Dwg. No. **RE35-1-AD003** Rev. No. Sheet No.

Plot Date: 7-JUL-2021

CADD FILE NAME: SAI_2s_pln_unst_stg_01.dgn
engbord3.dgn (BORDER) FEBRUARY 2, 2020
BLDG. REF. No.
FILENAME:
SHEET No.
DRAWING No.

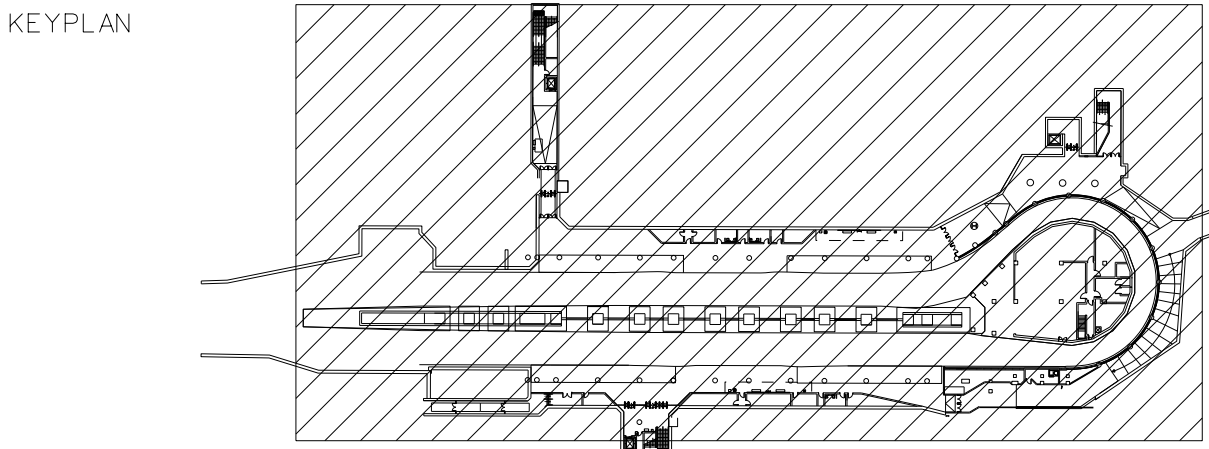


GENERAL NOTES



PLAN
UNION STATION STREETCAR LOOP - PLATFORM STAGING
PLAN
1 AS001

NP-A NP-B NP-C NP-D



| REVISIONS | REVISIONS |
|-----------|-----------|
| △ | △ |
| △ | △ |
| △ | △ |

30% REVIEW
DRAWING
NOT FOR CONSTRUCTION

wood.

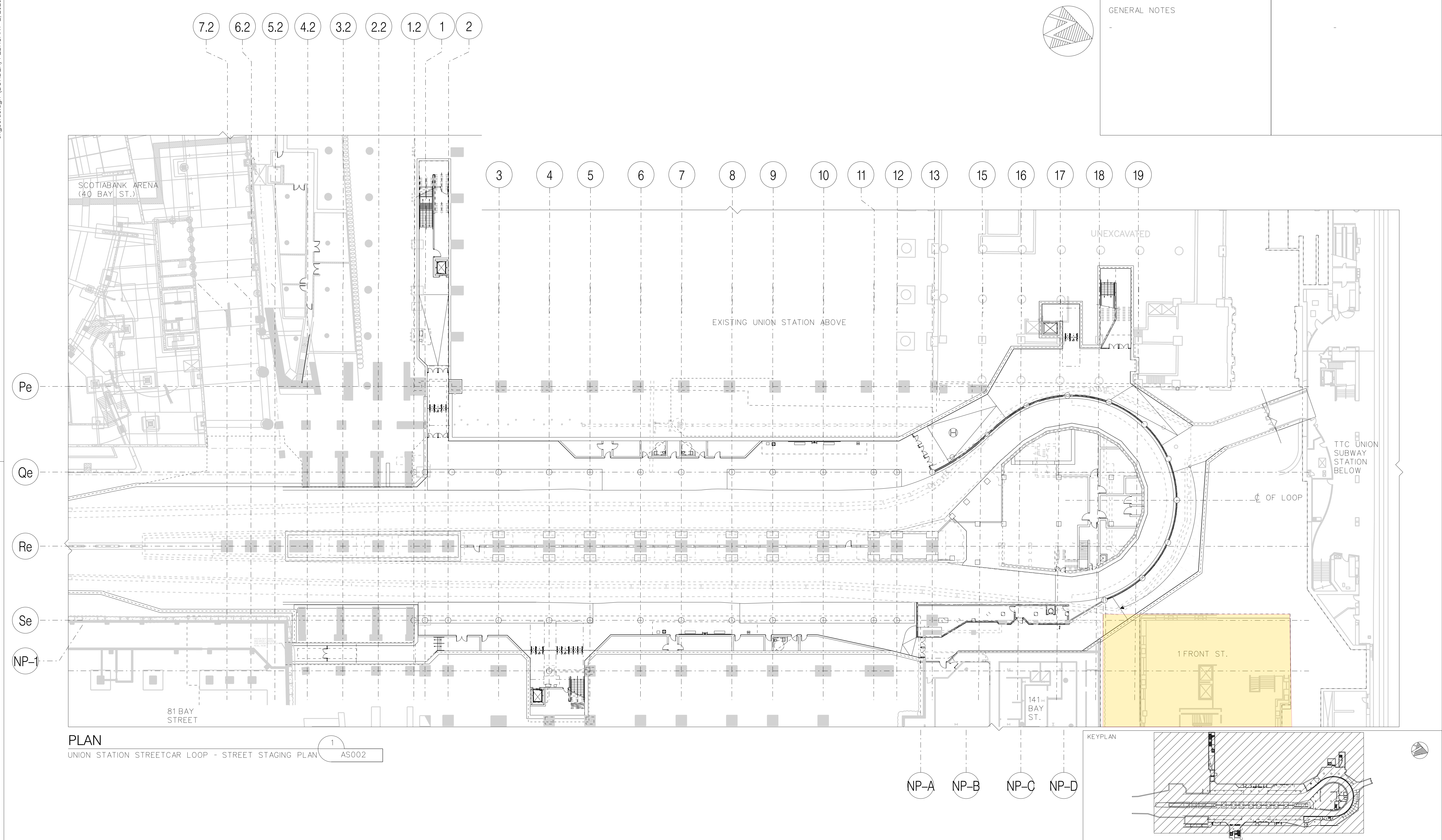
WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS
3450 HARVESTER ROAD, SUITE 100
BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 336 2263

DRAWN E. ENGLAND
CHECKED O. PHELAN
CORRECT J. STRASMAN
SCALE
0 2 4 6 8 10 12m

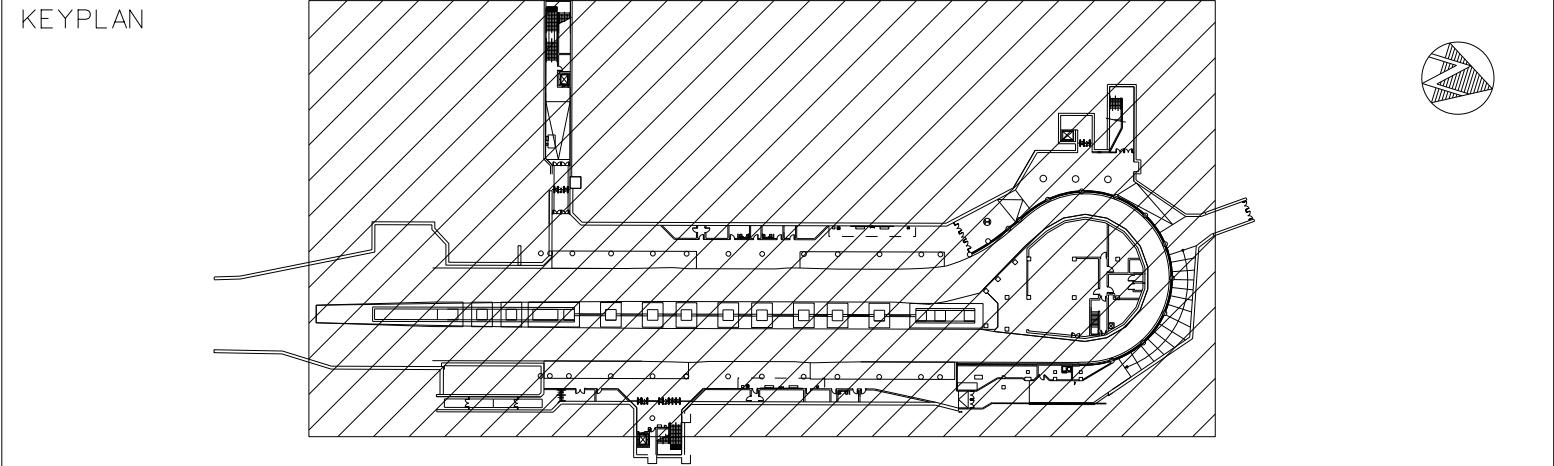
WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK
UNION STATION
PLATFORM LEVEL
STAGING PLAN 1 OF 2

Contract: RE35-1
TORONTO TRANSIT COMMISSION
ENGINEERING DEPARTMENT
Dwg. No. RE35-1-AS001
Rev. No.
Sheet No.
Plot Date: 7-JUL-2021

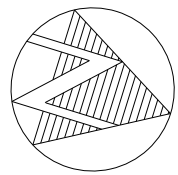
CADD FILE NAME: SAI_2s_pln_unst_stg_02.dgn
engbord3.dgn (BORDER) FEBRUARY 2, 2020
BLDG. REF. No.
FILENAME:
SHEET No.
DRAWING No.



PLAN
UNION STATION STREETCAR LOOP - STREET STAGING PLAN
1 AS002



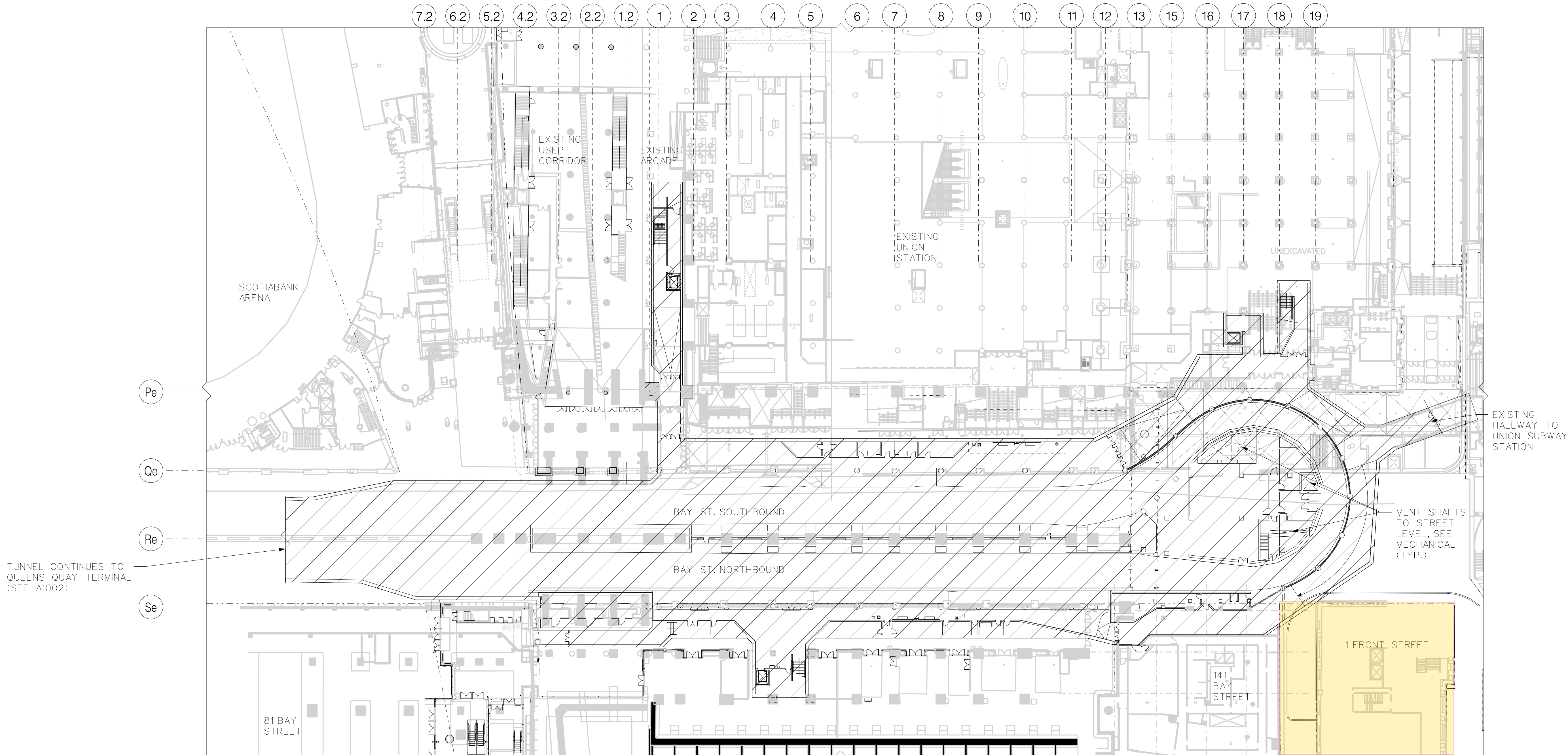
| | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|--|---|--|---|--|---|--|--|--|---|--|---|--|---|--|---|--|--|--|---|--|--|--|---|--|
| <div>REVISIONS</div> <table><tr><td>△</td><td></td></tr><tr><td>△</td><td></td></tr><tr><td>△</td><td></td></tr></table> | | △ | | △ | | △ | | <div>REVISIONS</div> <table><tr><td>△</td><td></td></tr><tr><td>△</td><td></td></tr><tr><td>△</td><td></td></tr></table> | | △ | | △ | | △ | | <div>30% REVIEW DRAWING</div> <div>NOT FOR CONSTRUCTION</div> | | <div>wood.</div> <div>WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS 3450 HARVESTER ROAD, SUITE 100 BURLINGTON, ONTARIO, CANADA, L7N 3W5 TEL: +1 905 336 2263</div> | | <div>DRAWN E. ENGLAND CHECKED O. PHELAN CORRECT J. STRASMAN</div> <div>SCALE 0 2 4 6 8 10 12m</div> | | <div>WATERFRONT EAST LRT UNION STATION QUEENS QUAY LINK</div> <div>UNION STATION</div> <div>PLATFORM LEVEL STAGING PLAN 2 OF 2</div> | | <div>Contract: RE35-1</div> <div>TORONTO TRANSIT COMMISSION ENGINEERING DEPARTMENT</div> <div>Dwg. No. RE35-1-AS002 Rev. No. Sheet No.</div> <div>Plot Date: 7-JUL-2021</div> | |
| △ | | | | | | | | | | | | | | | | | | | | | | | | | |
| △ | | | | | | | | | | | | | | | | | | | | | | | | | |
| △ | | | | | | | | | | | | | | | | | | | | | | | | | |
| △ | | | | | | | | | | | | | | | | | | | | | | | | | |
| △ | | | | | | | | | | | | | | | | | | | | | | | | | |
| △ | | | | | | | | | | | | | | | | | | | | | | | | | |



LEGEND

- EXISTING TO REMAIN

- NEW CONSTRUCTION



TUNNEL CONTINUES TO QUEENS QUAY TERMINAL (SEE A1002)

EXISTING HALLWAY TO UNION SUBWAY STATION

VENT SHAFTS TO STREET LEVEL, SEE MECHANICAL (TYP.)

1 FRONT STREET

141 BAY STREET

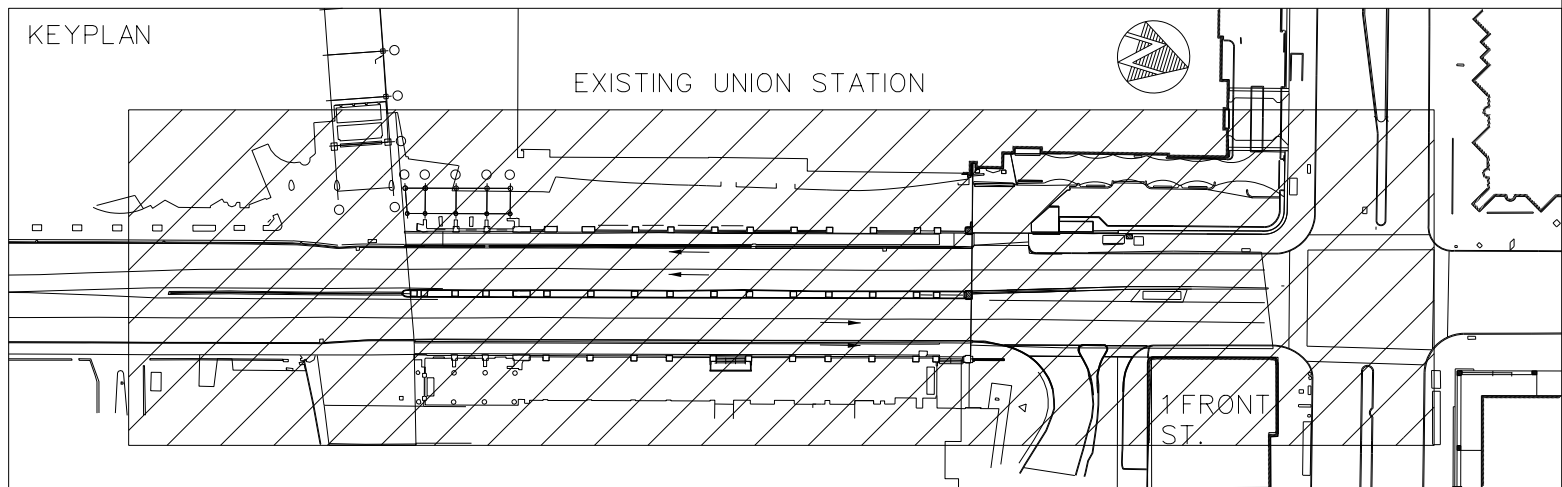
81 BAY STREET

PLAN

UNION STATION STREETCAR LOOP - STREET LEVEL

1
A1001

NP-A NP-B NP-C NP-D



| REVISIONS | REVISIONS |
|-----------|-----------|
| △ | △ |
| △ | △ |
| △ | △ |

30% REVIEW
DRAWING
NOT FOR CONSTRUCTION

wood.

WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS
3450 HARVESTER ROAD, SUITE 100
BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 336 2263

DRAWN E. ENGLAND
CHECKED O. PHELAN
CORRECT J. STRASMAN

SCALE
0 2 4 6 8 10 15m

WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK

UNION STATION

SITE PLAN

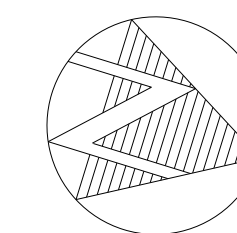
Contract: RE35-1



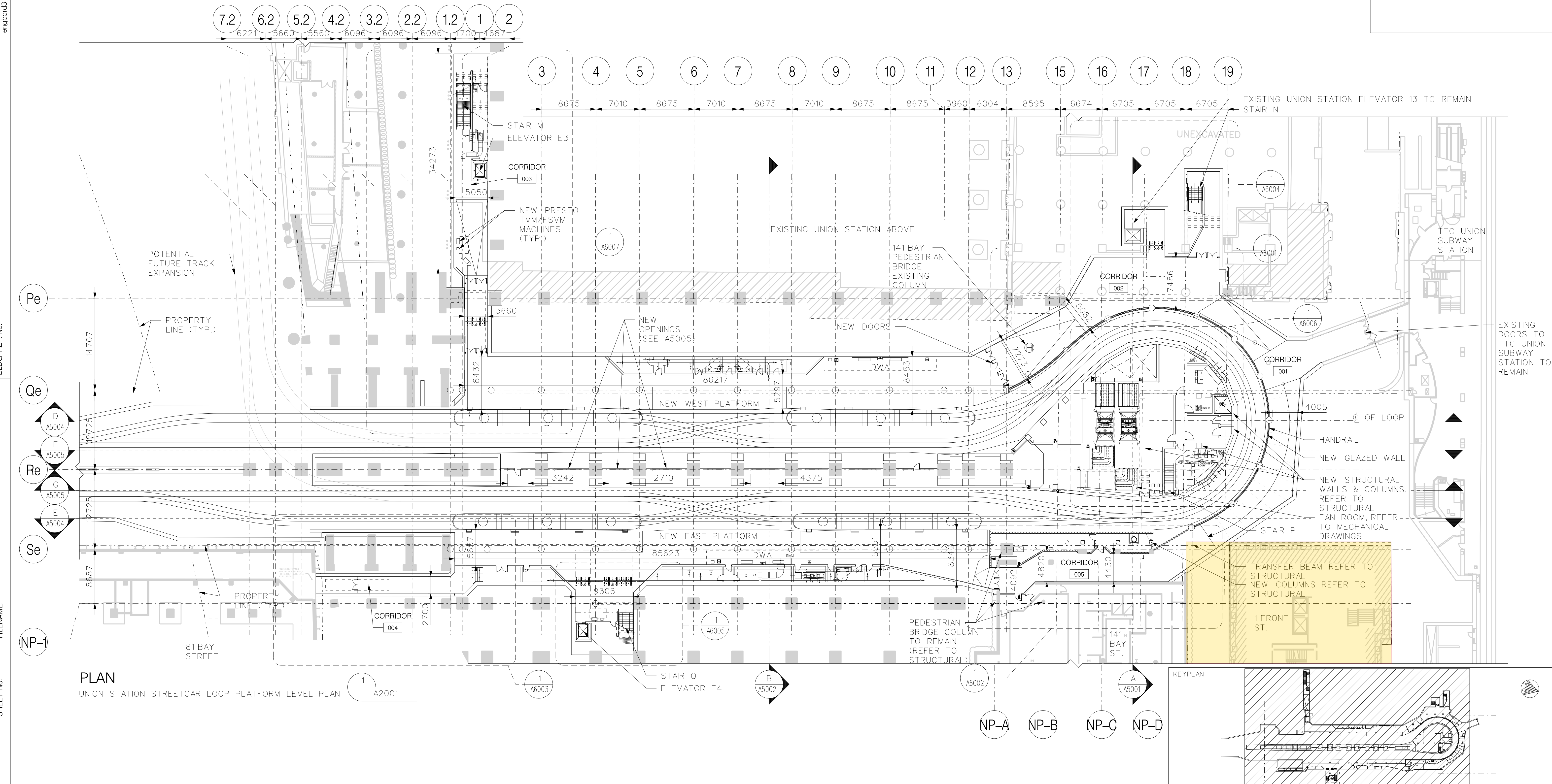
TORONTO TRANSIT COMMISSION
ENGINEERING DEPARTMENT




Dwg. No. RE35-1-A1001
Rev. No. Sheet No.

Plot Date: 7-JUL-2021



 - EXISTING TO REMAIN
 - NEW CONCRETE
 - PROPERTY LINE



| | | | | | | | |
|-------------|-----------|-----------|---|--|---|---|--|
| DRAWING NO. | REVISIONS | REVISIONS | <div>30% REVIEW DRAWING</div> <div>NOT FOR CONSTRUCTION</div> |  <div>WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS 3450 HARVESTER ROAD, SUITE 100 BURLINGTON, ONTARIO, CANADA L7N 3W5 TEL: +1 905 335 2263</div> | <div> <div> DRAWN N.MANABAT CHECKED O.PHELAN CORRECT J.STRASMAN </div> <div> SCALE  </div> </div> | <div> <div> WATERFRONT EAST LRT UNION STATION QUEENS QUAY LINK </div> <div> UNION STATION PLATFORM LEVEL PLAN </div> </div> | <div> Contract: RE35-1  TORONTO TRANSIT COMMISSION ENGINEERING DEPARTMENT Dwg. No. RE35-1-A2001 Rev. No. Sheet No. </div> |
| | △ | △ | | | | | |
| | △ | △ | | | | | |
| | △ | △ | | | | | |



| REVISIONS | REVISIONS |
|-----------|-----------|
| △ | △ |
| △ | △ |
| △ | △ |

30% REVIEW
DRAWING
NOT FOR CONSTRUCTION

wood.

WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS
3450 HARVESTER ROAD, SUITE 100
BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 335 2353

DRAWN E.ENGLAND
 CHECKED O.PHELAN
 CORRECT J.STRASMAN

SCALE



WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK

UNION STATION

STREET LEVEL PLAN

Contract: RE35-1

TORONTO TRANSIT COMMISSION
ENGINEERING DEPARTMENT

| | | |
|--------------|----------|--|
| Dwg. No. | Rev. No. | |
| RE35-1-A2002 | | |

Plot Date: 7-JUL-2021

Appendix C: Conservation Guidelines

THE STANDARDS

The Standards are not presented in a hierarchical order. All standards for any given type of treatment must be considered, and applied where appropriate, to any conservation project.

General Standards for Preservation, Rehabilitation and Restoration

1. Conserve the *heritage value* of an *historic place*. Do not remove, replace or substantially alter its intact or repairable *character-defining elements*. Do not move a part of an historic place if its current location is a character-defining element.
2. Conserve changes to an *historic place* that, over time, have become *character-defining elements* in their own right.
3. Conserve *heritage value* by adopting an approach calling for *minimal intervention*.
4. Recognize each *historic place* as a physical record of its time, place and use. Do not create a false sense of historical development by adding elements from other historic places or other properties, or by combining features of the same property that never coexisted.
5. Find a use for an *historic place* that requires minimal or no change to its *character-defining elements*.
6. Protect and, if necessary, stabilize an *historic place* until any subsequent *intervention* is undertaken. Protect and preserve archaeological resources in place. Where there is potential for disturbing archaeological resources, take mitigation measures to limit damage and loss of information.
7. Evaluate the existing condition of *character-defining elements* to determine the appropriate *intervention* needed. Use the gentlest means possible for any intervention. Respect *heritage value* when undertaking an intervention.
8. Maintain *character-defining elements* on an ongoing basis. Repair character-defining elements by reinforcing their materials using recognized conservation methods. Replace in kind any extensively deteriorated or missing parts of character-defining elements, where there are surviving *prototypes*.
9. Make any *intervention* needed to preserve *character-defining elements* physically and visually compatible with the *historic place* and identifiable on close inspection. Document any intervention for future reference.

Additional Standards Relating to Rehabilitation

- 10.** Repair rather than replace *character-defining elements*. Where character-defining elements are too severely deteriorated to repair, and where sufficient physical evidence exists, replace them with new elements that match the forms, materials and detailing of sound versions of the same elements. Where there is insufficient physical evidence, make the form, material and detailing of the new elements compatible with the character of the *historic place*.
- 11.** Conserve the *heritage value* and *character-defining elements* when creating any new additions to an *historic place* or any related new construction. Make the new work physically and visually compatible with, subordinate to and distinguishable from the historic place.
- 12.** Create any new additions or related new construction so that the essential form and integrity of an *historic place* will not be impaired if the new work is removed in the future.

Additional Standards Relating to Restoration

- 13.** Repair rather than replace *character-defining elements* from the *restoration* period. Where character-defining elements are too severely deteriorated to repair and where sufficient physical evidence exists, replace them with new elements that match the forms, materials and detailing of sound versions of the same elements.
- 14.** Replace missing features from the *restoration* period with new features whose forms, materials and detailing are based on sufficient physical, documentary and/or oral evidence.

4.3.4 EXTERIOR WALLS

These guidelines provide direction when exterior walls and their elements are identified as character-defining elements of an historic place. Exterior walls include foundation walls, structural masonry or log walls, and wood, concrete or steel framing with an exterior cladding, such as *curtain-wall systems*. The guidelines also provide direction on how to minimize the negative impact of additions or alterations on exterior walls.



Because they are largely below the surface, foundations are often dismissed as not contributing to a building's heritage character. However, it is important to remember that heritage value is not only found in what can be seen. Construction methods, materials and techniques can all be character defining, as is the case for the Grange Alexander-Salomon-Wallbridge in Quebec.

Log, stone and concrete foundations are visually and functionally very different. The decision to repair, replace in kind or rehabilitate a foundation should be determined by both the condition of the foundation and its compatibility with the heritage values of the place.

Exterior walls perform many functions, including those of structure, weatherproofing, thermal protection, daylight control and ventilation. Traditional load-bearing walls, such as log or masonry walls, perform all of these functions in a single composition. Later, the development of frame-based structural systems led to the separation of these functions. In modern buildings, components, such as cladding, air barriers and insulation, are combined to create a complex exterior wall assembly. These components act both independently and as a whole; consequently, there may be specific conservation issues associated with the materials concealed in the core or cavity of the assembly.



A modern building envelope includes multiple materials, such as a concrete structure, metal curtain wall mullions and glazed panels. Each of these materials ages differently and reacts in its own way to stresses, heat and cold. For a face-sealed curtain wall like this one, it is important to select sealants that make the assembly weathertight and that are compatible with all the materials with which they come into contact. Sealant materials generally have a shorter service life than adjacent materials and will eventually fail due to exposure to weather, stress and age. Regularly replacing appropriate sealants is an important part of building maintenance.

When frame-based cladding and curtain walls were developed, air and vapour barriers were introduced, as well as mechanical heating, ventilating and air conditioning (HVAC) systems that changed the thermal and moisture conditions in the walls. Changes to existing HVAC systems, or introducing mechanical systems in buildings where the walls were designed without insulation or air barriers, often led to the deterioration of exterior wall assemblies. It is, therefore, important to understand how the exterior wall relates to the building systems.

Curtain wall systems present a range of new conservation challenges, because they were the result of an era of experimentation in structures and materials, and predate higher standards for energy efficiency. Their conservation should be examined on a case-by-case basis, taking into account the heritage value of the design and the actual conditions and causes of deterioration, while planning for extended or improved performance.

These guidelines provide general recommendations appropriate to all types of exterior walls. For recommendations on associated issues related to walls, refer to the individual guidelines for Exterior Form; Windows, Doors and Storefronts; and Structural Systems. For specific materials that make up exterior walls, refer to the Guidelines for Materials.



New signs (top right) should be compatible with the building in terms of size, scale, material, style and colour. They should also not obscure, damage or destroy character-defining elements. In some cases, as Mel's Tea Room in Sackville, NB (left), signs added later become character-defining elements in their own right. Character defining signage should also be maintained despite a change in use, such as this painted sign for the Hartt Boot and She Factory in Fredericton (bottom right), which has been converted into apartments.



Responding to the dangers of arctic exploration and the loss of the Franklin Expedition, Kellett's Storehouse in Nunuvut was built in 1853 for sailors in distress. Constructed from local stone, it was filled with enough provisions to maintain a large group for several months. Conserving building remains or above-ground archaeological resources pose unique challenges. In this case, the walls and foundations were stabilized and an insulated floor was installed allowing remaining soil layers and associated artifacts to be left in place while being protected from theft and further exposure to the elements.



The impact of adding insulation to an exterior wall assembly that was not historically insulated should be carefully considered, including by measurement of the current performance of the wall and energy modelling the performance of different approaches. Adding insulation on the exterior or interior of solid masonry or log walls may impact on heritage character and lead to deterioration of the wall, if not based on a thorough understanding of the wall's physical characteristics and context, including its exposure to air, water and vapour pressures. Any changes to an exterior wall should be based on the building envelope science for the type of historic assembly.

GENERAL GUIDELINES FOR PRESERVATION, REHABILITATION AND RESTORATION

| | Recommended | Not Recommended |
|----|---|--|
| 1 | Understanding the exterior walls and how they contribute to the heritage value of the historic building. | |
| 2 | Understanding the properties and characteristics of the exterior walls as well as changes and previous maintenance practices. | Failing to consider the impact of previous changes to the exterior wall assembly, such as the addition of insulation and vapour barriers, or new heating or cooling systems. |
| 3 | Documenting the composition, form, materials, details, dimensions and condition of exterior wall assemblies before undertaking an intervention. This includes geometry, scale, proportions, openings, form and supporting frames or structures. | Undertaking an intervention that affects exterior wall assemblies without first documenting their existing character and condition. |
| 4 | Assessing the condition of wall assemblies and their materials early in the planning process so that the scope of work is based on current conditions. | |
| 5 | Determining the cause of distress, damage or deterioration of exterior walls through investigation, monitoring and minimally invasive or non-destructive testing techniques. | |
| 6 | Protecting and maintaining exterior walls by cleaning and repairing damaged materials, and checking exterior wall assemblies for moisture penetration and insect infestation, taking corrective action, as necessary and as soon as possible. | Failing to maintain exterior walls on a cyclical basis. Failing to correct causes of deterioration of the exterior wall assembly, such as failed sealants. |
| 7 | Retaining sound or deteriorated exterior wall assemblies that can be repaired. | |
| 8 | Stabilizing deteriorated exterior walls by using structural reinforcement, weather protection, or correcting unsafe conditions, as required, until repair work is undertaken. | Removing deteriorated exterior wall elements that could be stabilized or repaired. |
| 9 | Repairing parts of exterior walls by patching, piecing-in, consolidating, or otherwise reinforcing, using recognized conservation methods. Repair may also include the limited replacement in kind, or with a compatible substitute material, of extensively deteriorated or missing parts of the exterior wall assembly. Repairs should match the existing work as closely as possible, both physically and visually. | |
| 10 | Protecting adjacent character-defining elements from accidental damage or exposure to damaging materials during maintenance or repair work. | |

GENERAL GUIDELINES FOR PRESERVATION, REHABILITATION AND RESTORATION

| | Recommended | Not Recommended |
|----|---|---|
| 11 | Replacing in kind extensively deteriorated or missing parts of exterior wall assemblies where there are surviving prototypes. | Replacing an entire exterior wall assembly when only limited replacement of deteriorated and missing parts is possible. Using a substitute material for the replacement part that neither conveys the same appearance as the surviving parts of the element, nor is physically or visually compatible. |
| 12 | Testing proposed interventions to establish appropriate replacement materials, quality of workmanship and methodology. This can include reviewing samples, testing products, methods or assemblies, or creating a mock-up. Testing should be carried out under the same conditions as the proposed intervention. | |
| 13 | Documenting all interventions that affect the exterior walls, and ensuring that the documentation is available to those responsible for future interventions. | |

ADDITIONAL GUIDELINES FOR REHABILITATION PROJECTS

| | Recommended | Not Recommended |
|----|---|---|
| 14 | Repairing an exterior wall assembly, including its functional and decorative elements, by using a minimal intervention approach. Such repairs might include the limited replacement in kind, or replacement using an appropriate substitute material of irreparable or missing elements, based on documentary or physical evidence. Repairs might also include dismantling and rebuilding a masonry or wood wall, if an evaluation of its overall condition determines that more than limited repair or replacement in kind is required. | Over-cladding a deteriorated or poorly insulated exterior wall with a new material or assembly, without considering the impact on heritage value or the condition of underlying materials. Replacing an entire exterior wall assembly when the repair and limited replacement of deteriorated or missing elements is feasible. Failing to reuse intact cladding when only the internal parts of the wall assembly need replacement. |
| 15 | Improving the drying ability of exterior wall assemblies through suitable heating and/or ventilation measures. | Damaging the masonry of an exterior wall by drilling drainage holes into the masonry units or into the joints, with a drill bit wider than the mortar joints. Introducing a vapour barrier in an exterior wall that was constructed to be permeable or breathable. |

ADDITIONAL GUIDELINES FOR REHABILITATION PROJECTS

| | Recommended | Not Recommended |
|----|--|---|
| 16 | Accommodating the thermal expansion and contraction of masonry, concrete and curtain wall assemblies, by introducing expansion or control joints, and incorporating those joints into existing crack patterns, where feasible, to minimize impact on character-defining elements. | Filling moving cracks or expansion joints in exterior wall assemblies with materials that inhibit or prevent thermal expansion and contraction. |
| 17 | Replacing in kind an irreparable exterior wall assembly, based on documentary and physical evidence. If using the same kind of material is not environmentally sound, or technically or economically feasible, then a compatible substitute material may be considered. | Removing an irreparable exterior wall assembly, such as a cornice or <i>brise-soleil</i> , and not replacing it, or replacing it with a new element that does not convey the same appearance or serve the same function. Replacing deteriorated elements and materials in curtain wall assemblies that are no longer available, with physically and visually incompatible substitutes. |
| 18 | Replacing missing historic features by designing and constructing a new portion of the exterior wall assembly, based on physical and documentary evidence, or one that is compatible in size, scale, material, style and colour. | Creating a false historical appearance, because the replicated feature is incompatible or based on insufficient physical and documentary evidence. |

ADDITIONS OR ALTERATIONS TO EXTERIOR WALLS

| | | |
|----|--|--|
| 19 | Modifying exterior walls to accommodate an expanded program, a new use, or applicable codes and regulations, in a manner that respects the building's heritage value. | |
| 20 | Designing a new addition in a manner that preserves the character-defining exterior walls of the historic building. | Constructing an addition that requires the removal of character-defining exterior walls. |

HEALTH, SAFETY AND SECURITY CONSIDERATIONS

| | | |
|----|---|--|
| 21 | Complying with health, safety and security requirements in a manner that conserves the heritage value of the exterior wall assembly and minimizes impact on its character-defining elements. | Damaging or destroying elements while making modifications to comply with health, safety or security requirements. |
| 22 | Working with code specialists to determine the most appropriate solution to health, safety and security requirements with the least impact on the character-defining elements and overall heritage value of the historic building. | Making changes to exterior walls, without first exploring equivalent systems, methods or devices that may be less damaging to character-defining elements and the heritage value of the historic building. |
| 23 | Removing or encapsulating toxic materials, using the least-invasive abatement methods possible, and only after thorough testing has been conducted. | |
| 24 | Protecting exterior walls against loss or damage by identifying and assessing specific risks, and by implementing an appropriate fire-protection and blast protection strategy that addresses those risks. | Covering flammable character-defining walls with fire-resistant sheathing or coatings that alter their appearance. |

ADDITIONAL GUIDELINES FOR REHABILITATION PROJECTS

| | Recommended | Not Recommended |
|--------------------------------------|---|---|
| SUSTAINABILITY CONSIDERATIONS | | |
| 25 | Complying with energy efficiency objectives in upgrades to exterior wall assemblies in a manner that respects the building's character-defining elements, and considers the energy efficiency of the building envelope and systems as a whole. | <p>Changing the composition or materials of the exterior wall assembly in a manner that compromises the building's character-defining elements and the durability of its materials.</p> <p>Replacing single pane glazing with sealed thermal units, without considering the impact on interrelated elements, such as curtain wall connections.</p> |
| 26 | Assessing the potential impacts of adding insulation to the building envelope, such as displacing the <i>dew point</i> and creating <i>thermal bridges</i> . | <p>Inserting thermal insulation in exterior wall cavities, in attics, and in unheated cellars and crawl spaces, that might adversely affect the building's envelope and character-defining elements.</p> <p>Installing insulation on the inside of exterior walls without considering the effect on character-defining interior mouldings or detailing.</p> |
| 27 | Working with energy efficiency specialists to determine the most appropriate solution to energy efficiency requirements with the least impact on the character-defining elements and overall heritage value of the historic building. | Making changes to the exterior walls, without first exploring alternative energy efficiency solutions that may be less damaging to the character-defining elements and overall heritage value of the historic building. |

ADDITIONAL GUIDELINES FOR RESTORATION PROJECTS

| | Recommended | Not Recommended |
|----|--|---|
| 28 | Repairing an exterior wall assembly from the restoration period by reinforcing its materials; for example, using heavier gauge metal to reinforce a mullion in a curtain wall. | Replacing an entire exterior wall assembly from the restoration period when the repair of materials and limited replacement of deteriorated or missing parts is possible. |
| 29 | Replacing in kind an entire exterior wall assembly from the restoration period that is too deteriorated to repair, using the physical evidence as a model to reproduce the assembly. The new work should be well documented and unobtrusively dated to guide future research and treatment. | Removing an irreparable exterior wall assembly from the restoration period and not replacing it, or replacing it with an inappropriate exterior wall assembly. Reinstating an exterior wall detail that is damaging to adjacent character-defining elements. |

REMOVING EXISTING FEATURES FROM OTHER PERIODS

| | | |
|----|---|---|
| 30 | Removing or altering a non character-defining exterior wall assembly or element from a period other than the restoration period. | Failing to remove a non character-defining exterior wall assembly or element from another period that confuses the depiction of the building's chosen restoration period. |
| 31 | Retaining alterations to exterior wall assemblies that address problems with the original design, if those alterations do not have a negative impact on the building's heritage value. | Removing an exterior wall assembly or element from a later period that serves an important function in the building's ongoing use. |

RECREATING MISSING FEATURES FROM THE RESTORATION PERIOD

| | | |
|----|--|---|
| 32 | Recreating a missing exterior wall assembly from the restoration period, based on physical or documentary evidence. | Constructing an exterior wall assembly that was part of the building's original design, but was never actually built, or constructing a feature thought to have existed during the restoration period, but for which there is insufficient documentation. |
|----|--|---|

Appendix D: Record of Community Engagement

From: [Yasmina Shamji](#)
To: [Schopf, Heidy](#)
Subject: FW: Information Gathering Request
Date: Thursday, March 25, 2021 11:37:01 AM
Attachments: [image001.png](#)
[image006.png](#)
[image007.png](#)
[OSIO52004 TTC Waterfront East LRT Information Gathering Request 20201216....pdf](#)

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy

Sorry, I didn't realize I hadn't copied you on the email. Here is my response to Luke.

Thank you

Yasmina Shamji
Urban Design | Heritage Planning
Toronto City Hall | 100 Queen Street West, 17th Floor East Tower
Toronto, ON M5H 2N2
416-392-1975
[City of Toronto](#)



From: Yasmina Shamji
Sent: January 7, 2021 4:27 PM
To: 'Fischer, Luke' <luke.fischer@woodplc.com>
Cc: Guy Zimmerman <Guy.Zimmerman@toronto.ca>; Joe Muller <Joe.Muller@toronto.ca>
Subject: RE: Information Gathering Request

Luke

Happy New Year!

I have reviewed your report and compiled the information as requested. I don't have information for the properties noted in red below – it's possible details for these properties doesn't exist. In that case, I am deferring your request to Guy Zimmerman (also copied on this email), who is the Heritage Planner for the Union Station Heritage Conservation District.

According to our [Heritage Register Map](#), the following properties fall within the area study areas indicated in your report:

- Bay Street from Front Street West to Queens Quay East
- Queens Quay East from York Street to Cooper Street

(I am attaching the link to the property details from our online register as well as the by-laws wherever applicable)

40 Bay St: Part IV Designation (Part IV By-law 360-90) as well as Union Station HCD (By-law 634-2006)
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433226&propertyRsn=206956>
<https://www.toronto.ca/legdocs/pre1998bylaws/toronto%20-%20former%20city%20of/1990-0360.pdf>

71 Front St W (Union Station) aka 140 & 142 Bay St & 55 Front St W: Part IV Designation (Part IV By-law 948-2005) as well as Union Station HCD
And 61 Front St W - <http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2440090&propertyRsn=206955>
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2435414&propertyRsn=705027>
<https://www.toronto.ca/legdocs/bylaws/2005/law0948.pdf>

145 Queens Quay W aka 77 Harbour Sq: Part IV Designation (By-law 1249-2007)
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2439065&propertyRsn=206198>
<https://www.toronto.ca/legdocs/bylaws/2007/law1249.pdf>

2 Cooper St (entrance address for 55 Lake Shore Blvd E) – Under Intention to Designate
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2438226&propertyRsn=719130>
<http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2018.TE30.17>

Further to the above, you also requested information sheets for the following properties:

Gardiner Expressway – no information available – deferred to Guy

1 Front St W
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2435336&propertyRsn=578987>
<https://www.toronto.ca/legdocs/bylaws/2017/law0423.pdf>

61 Front St W – Listed – no additional information is available
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2440090&propertyRsn=206955>

65 Front St W - refer to details provide above

18 Yonge St – no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2437048&propertyRsn=615674>

33 Bay St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433225&propertyRsn=751123>

40 Bay St – refer to details provide above

161 Bay St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=3246951&propertyRsn=206967>

200 Bay St - no property info sheets available – deferred to Guy

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433240&propertyRsn=206973>

60 Harbour St - no property info sheets available – deferred to Guy

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2434664&propertyRsn=206938>

88-100 Harbour St - no property info sheets available – deferred to Guy

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=3607011&propertyRsn=826634>

15-19 York St - no property info sheets available – deferred to Guy

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2436569&propertyRsn=750799>

Also, please note that most of the information provided above can be found on our website via the links below:

<https://www.toronto.ca/city-government/planning-development/heritage-preservation/heritage-register/> - using both the Heritage Register Map as well as the Heritage Search Tool.

By-law information can be found here: <https://www.toronto.ca/legdocs/bylaws/lawhome.htm>

Thank you

Yasmina Shamji

Urban Design | Heritage Planning

Toronto City Hall | 100 Queen Street West, 17th Floor East Tower

Toronto, ON M5H 2N2

[City of Toronto](http://www.toronto.ca)

416-392-1975



From: Fischer, Luke [<mailto:luke.fischer@woodplc.com>]

Sent: December 16, 2020 3:13 PM

To: Yasmina Shamji <Yasmina.Shamji@toronto.ca>

Cc: Schopf, Heidi <heidy.schopf@woodplc.com>

Subject: Information Gathering Request

Hello Yasmina,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,

Luke

Luke Fischer, M.A., CAHP

Senior Archaeologist, Built Heritage Analyst

Direct: 1 (519) 681-2400

Mobile: 1 (226) 376-2968

luke.fischer@woodplc.com

www.woodplc.com



This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: unsubscribe@woodplc.com and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.

Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at <https://www.woodplc.com/policies/privacy-notice>

From: [Guy Zimmerman](#)
To: [Schopf, Heidy](#)
Cc: [Fischer, Luke](#); [Brent Fairbairn](#)
Subject: RE: DRAFT Response from HP with contact names and document references
Date: Wednesday, July 7, 2021 11:55:10 AM
Attachments: [image020.png](#)
[image021.png](#)
[image022.png](#)
[image002.png](#)
[image005.png](#)
[image006.png](#)

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy,

I was glad to be able to share my experience with you and Brent regarding my stewardship of the Union Station HCD and provide that contact information to source background regarding the Bay Street Bridge (that you apparently may not have been familiar with)

Regarding any additional feedback that you were fishing for... beyond that please let me know if there is missing information that I can source for you as my expectation was that I was providing pretty specific insight into the Gardiner Expressway and Path System (relative to the Union Station HCD).

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning /Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca
416.338.1094

From: Schopf, Heidy [mailto:heidy.schopf@woodplc.com]
Sent: July 6, 2021 3:51 PM
To: Guy Zimmerman <Guy.Zimmerman@toronto.ca>
Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>
Subject: RE: DRAFT Response from HP with contact names and document references

Thank you both once again for your time today. Your insight on the Gardiner, PATH, and Bay Street bridge is appreciated!

Sincerely,
-Heidy

From: Guy Zimmerman <Guy.Zimmerman@toronto.ca>
Sent: Tuesday, July 6, 2021 2:59 PM
To: Schopf, Heidi <heidy.schopf@woodplc.com>
Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>
Subject: DRAFT Response from HP with contact names and document references

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy et al,

Please disregard my DRAFT RESPONSE that I had been preparing if you have that random assortment of materials and contacts names. I had included Dima's contact information at the bottom for your reference should there be any confusion with the consultant indicated on the image of the cover of the report that is included as to who would be the first point of contact.

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning /Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca
416.338.1094

From: Guy Zimmerman
Sent: July 6, 2021 7:59 AM
To: 'Schopf, Heidi' <heidy.schopf@woodplc.com>
Cc: 'Fischer, Luke' <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>
Subject: RE: Union Station HCD Background Request Cultural Heritage Report for the Waterfront East LRT - Outstanding question RE:Bridge supporting the Union Station Trainshed Inquiry

Good morning Heidy,

I thought that it was funny when you first asked about this that you wouldn't have access to this material through Metrolinx.

I won't bother completing the material that I was assembling below as we can discuss it during our 3:00 Union Station/TTC WELRT Discussion (catchy name!)

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning /Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca
416.338.1094

DRAFT RESPONSE 7/6/2021

I dredged up this report from the work associated with the Bay Street Railway Bridge.

I don't know if I am at liberty to share this material but I think you would do better to deal directly with the consultants as they may have additional research that you would benefit from.

I'd be surprised if you have not dealt with Dima Cook in one capacity or another over the years.

Dima has been involved in heritage related work in Toronto for likely 15 years.

ERA was also involved and I don't know who has what... Dima will.

Bay Street PATH Connection (North)

1.1 Project Overview

The proposed development of 45 (81) & 141 Bay Street is part of a contemporary large-scale city building initiative incorporating transportation and is located within the Union Station Heritage Conservation District (HCD) Plan and adjacent to a number of prominent Beaux-Arts buildings associated with Toronto's development.

CHER

Bay Street railway Bridge

Union Station rail Corridor

Project # 16-118-01

Prepared by GS / JN (ERA)

Cultural Heritage Evaluation Report - Part 2

PREPARED FOR:

Metrolinx

20 Bay Street, Suite 600

Toronto Ontario M5J2W3

Rodyney Yee, Project Coordinator, GO Transit

rodney.yee@gotransit.com

416-202-4516

PREPARED BY:

ERA Architects Inc.

10 St. Mary Street, Suite 801

Toronto, Ontario M4Y 1P9

416-963-4497



STAGE TWO
HERITAGE IMPACT ASSESSMENT

**Bay Street PATH
Connection (North)**

IVANHOÉ CAMBRIDGE C/O HINES

PROJECT NO. EVOQ: 9019-13

ISSUE DATE: NOVEMBER 2019



Dima Cook

OAQ OAA AAA AANB RAIC LEED AP CAHP

ARCHITECT, PRINCIPAL

NEW OFFICE ADDRESS

75 SHERBOURNE STREET, SUITE 503

TORONTO, ON M5A 2P9

D./C. 416. 873.2018

T. 647.723.2030 / 1001

EVOQ ARCHITECTURE

@evoqarchitecture

Instagram / Facebook / LinkedIn

From: Guy Zimmerman

Sent: July 5, 2021 4:23 PM

To: 'Schopf, Heidy' <heidy.schopf@woodplc.com>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>

Subject: RE: Union Station HCD Background Request Cultural Heritage Report for the Waterfront East LRT - Outstanding question RE: Bridge supporting the Union Station Trainshed Inquiry

RE: Bridge supporting the Union Station Train shed Inquiry

Sorry Heidy... "I dropped the ball" on that aspect of my response/your request.

I don't see a problem sourcing the material that I previously referenced in time for our call (although there is nothing really to discuss in that regard... as it is really just providing the consultant's background associated with the integration of the new passage forward of the metal elevation)

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning/Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca
416.338.1094

From: Schopf, Heidy [<mailto:heidy.schopf@woodplc.com>]

Sent: July 5, 2021 3:06 PM

To: Guy Zimmerman <Guy.Zimmerman@toronto.ca>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>

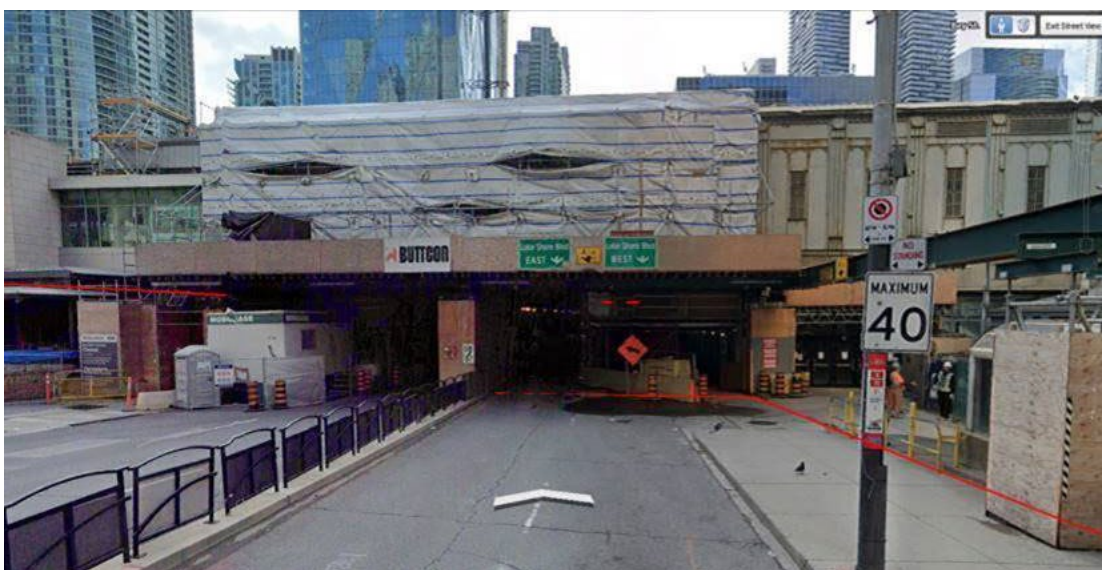
Subject: RE: Union Station HCD Background Request Cultural Heritage Report for the Waterfront East LRT - Outstanding question RE: Gardiner and Path

Thanks Guy!

The only outstanding item is the heritage status of the bridge associated with the Union Station trainshed (see pictures below).

To date, we have assumed that this bridge is part of the Union Station complex. However, I would like to double check with you if this structure has any heritage status independent of Union Station. The bridge itself did not come up on our searches of the municipal register or in our information gathering exercises.

-Heidy



From: Guy Zimmerman <Guy.Zimmerman@toronto.ca>

Sent: Monday, July 5, 2021 9:30 AM

To: Schopf, Heidi <heidy.schopf@woodplc.com>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>

Subject: RE: Union Station HCD Background Request Cultural Heritage Report for the Waterfront East LRT - Outstanding question RE: Gardiner and Path

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy et al,

Please confirm that the only outstanding information that you are seeking (to include in

your Cultural Heritage Report for the Waterfront East LRT) is clarification of the HCD direction regarding the PATH system and Gardiner Expressway.

(Between the material that I had provide and the additional information that Yasmina referenced I believe all other aspects of your request have been satisfied)

I have copied below excerpts from the 2006 Union Station HCD Appendix that can underpin our Teams discussion tomorrow at 3:00.

I had mentioned that I would send you a summary of what I intend to share regarding these two subjects and I believe that these excerpts serve that purpose.



Gardiner Expressway

Era: Post-War
 Style: Modern Infrastructure
 Contributing: Yes

The development of this expressway was part of Fred Gardiner's vision of a modern Toronto. As Metropolitan Toronto's first Chairman, he was instrumental in the development of the Gardiner Expressway beginning in 1955. Built in an industrial zone at the foot of the city, the expressway modernized access to the centre of the city and from the city to its suburbs during a period of rapid growth. It was a nascent project in the process of creating regional infrastructure for the newly created Metropolitan region. The expressway was planned in conjunction with the rapid transit network as part of the comprehensive transportation planning of the region. The Gardiner was the first of several highways planned for the Toronto area, most of which were never realized.

The Skywalk was completed in 1989 to coincide with the completion of the Rogers Centre. Designed in the arcade style, the Skywalk consists of a large glazed passageway enclosed by a semi-circular arched roof reminiscent of early century European pedestrian areas. The Skywalk begins at the western most side of Union Station, passes through the second and third stories of the Canadian National Express building and crosses the Toronto rail viaduct at Simcoe Street. South of the railway the Skywalk traverses through the south end of the Metro Convention Centre and terminates in the plaza adjacent to the CN Tower, Roundhouse and Rogers Centre. The skywalk is a piece of urban infrastructure of growing importance; connecting the city to the increasingly developed rail lands. Furthermore, the skywalk offers unique views of Toronto's existing rail activity and provides a physical connection between to the District's two national historic sites: the Roundhouse and Union Station.



PATH System

Era: Post-War
Style: Various
Contributing: Yes

The PATH system is the subgrade pedestrian network which connects 6 subway stations and over 4 million square feet of retail space through 27 km of passages. It is currently the largest network of this type in the world.

The system developed as a result municipal policy of the 1960's encouraging sub-grade connections between new large office developments to one another and to the subway. The system has continued to grow incrementally, as new developments make connections with adjacent buildings. An underground tunnel of 1929 between the Fairmont Royal York Hotel and Union Station has been incorporated into the system.

Heritage Conservation District Plan Appendix 1

141

Currently the city has a contentious relationship with the Gardiner as a result of a change in use of the central waterfront. As new solutions are realized in managing transportation in an increasingly post-industrial mixed-use waterfront and urban core, the Gardiner remains as an important piece of the City's early modern heritage and contributing element to the function and character of the Union Station Heritage Conservation District.

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning/Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca

416.338.1094

From: Guy Zimmerman

Sent: June 18, 2021 11:20 AM

To: 'Schopf, Heidy' <heidy.schopf@woodplc.com>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>;

Yasmina Shamji <Yasmina.Shamji@toronto.ca>

Subject: RE: Union Station HCD Background Request - HP Response re 90 Harbour Street

Heidy,

Here is the low hanging fruit:

1) Workmen's Compensation Board Building

i) Withdrawal of Notice of Intention to Designate, Part IV, Section 29, Ontario Heritage Act - 90 Harbour Street

<http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2008.TE12.15>

ii) Demolition permit issued February 14 2011 (11 110475)

2) Gardiner Expressway or the PATH System

I need to have "a think" about this one and articulate their role in the HCD... and get back to you.

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning/Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca
416.338.1094

From: Schopf, Heidy [<mailto:heidyschopf@woodplc.com>]

Sent: June 18, 2021 10:21 AM

To: Guy Zimmerman <Guy.Zimmerman@toronto.ca>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>;
Yasmina Shamji <Yasmina.Shamji@toronto.ca>

Subject: RE: Union Station HCD Background Request

Hi Guy,

Following up on this. Do you have any direction on the two items below?

-Heidy

From: Schopf, Heidy

Sent: Friday, April 9, 2021 9:54 AM

To: Guy Zimmerman <Guy.Zimmerman@toronto.ca>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>; Yasmina Shamji <Yasmina.Shamji@toronto.ca>

Subject: RE: Union Station HCD Background Request

Good morning Guy,

We received the HCD inventory information earlier this week. Thank you! The information was very helpful.

I have a couple questions:

- 1) The Workmen's Compenstaion Board Building (90 Harbour Street) appears to have been demolished between 2009 and 2012 based on Google Earth imagery. Can you confirm?
- 2) The Union Station HCD Plan does not depict the Gardiner Expressway or the PATH System as contributing buildings in the HCD (see below). Yet, the inventory sheets note that these resources are "contributing". What is the heritage status of these two resources? Our approach has been to address them as elements of the HCD in general but not as individual resources. It would be great to get some direction here to confirm if the City views the Gardiner and the PATH System as standalone heritage resources (or not).

Thanks,
Heidy



From: Guy Zimmerman <Guy.Zimmerman@toronto.ca>

Sent: Friday, March 26, 2021 9:00 AM

To: Schopf, Heidy <heidy.schopf@woodplc.com>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>;

Yasmina Shamji <Yasmina.Shamji@toronto.ca>

Subject: Union Station HCD Background Request

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy/Luke,

I will be working on this today.

The HCD material that you are referencing dates back to 2006.

Brent Fairbairn has made an inquiry as well and I will determine what additional material we have relative to your requests... beyond what Yasmina has already provided.

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning/Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca
416.338.1094

From:

From: Schopf, Heidy [<mailto:heidy.schopf@woodplc.com>]
Sent: March 26, 2021 6:53 AM
To: Guy Zimmerman <Guy.Zimmerman@toronto.ca>
Cc: Fischer, Luke <luke.fischer@woodplc.com>
Subject: FW: Information Gathering Request

Hi Guy,

I am following up on an information request submitted by Luke this past January. Apologies if you already replied to him!

We are completing a Cultural Heritage Report for the Waterfront East LRT and part of the Study Area falls within the Union Station HCD. Any additional information you can provide on the heritage attributes of the district and property sheets for addresses in the Study Area would be hugely helpful! Happy to have a call sometime if that works better for you.

Thank you,
Heidy


From: Yasmina Shamji <Yasmina.Shamji@toronto.ca>
Sent: Thursday, March 25, 2021 11:36 AM
To: Schopf, Heidy <heidy.schopf@woodplc.com>
Subject: FW: Information Gathering Request

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy

Sorry, I didn't realize I hadn't copied you on the email. Here is my response to Luke.

Thank you

Yasmina Shamji
Urban Design | Heritage Planning
Toronto City Hall | 100 Queen Street West, 17th Floor East Tower
Toronto, ON M5H 2N2
416-392-1975
[City of Toronto](#)


From: Yasmina Shamji

Sent: January 7, 2021 4:27 PM

To: 'Fischer, Luke' <luke.fischer@woodplc.com>

Cc: Guy Zimmerman <Guy.Zimmerman@toronto.ca>; Joe Muller <Joe.Muller@toronto.ca>

Subject: RE: Information Gathering Request

Luke

Happy New Year!

I have reviewed your report and compiled the information as requested. I don't have information for the properties noted in red below – it's possible details for these properties doesn't exist. In that case, I am deferring your request to Guy Zimmerman (also copied on this email), who is the Heritage Planner for the Union Station Heritage Conservation District.

According to our [Heritage Register Map](#), the following properties fall within the area study areas indicated in your report:

- Bay Street from Front Street West to Queens Quay East
- Queens Quay East from York Street to Cooper Street

(I am attaching the link to the property details from our online register as well as the by-laws wherever applicable)

40 Bay St: Part IV Designation (Part IV By-law 360-90) as well as Union Station HCD (By-law 634-2006)
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433226&propertyRsn=206956>
<https://www.toronto.ca/legdocs/pre1998bylaws/toronto%20-%20former%20city%20of/1990-0360.pdf>

71 Front St W (Union Station) aka 140 & 142 Bay St & 55 Front St W: Part IV Designation (Part IV By-law 948-2005) as well as Union Station HCD

And 61 Front St W - <http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2440090&propertyRsn=206955>

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2435414&propertyRsn=705027>
<https://www.toronto.ca/legdocs/bylaws/2005/law0948.pdf>

145 Queens Quay W aka 77 Harbour Sq: Part IV Designation (By-law 1249-2007)
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2439065&propertyRsn=206198>
<https://www.toronto.ca/legdocs/bylaws/2007/law1249.pdf>

2 Cooper St (entrance address for 55 Lake Shore Blvd E) – Under Intention to Designate
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2438226&propertyRsn=719130>
<http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2018.TE30.17>

Further to the above, you also requested information sheets for the following properties:

Gardiner Expressway – no information available – deferred to Guy

1 Front St W
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2435336&propertyRsn=578987>
<https://www.toronto.ca/legdocs/bylaws/2017/law0423.pdf>

61 Front St W – Listed – no additional information is available
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2440090&propertyRsn=206955>

65 Front St W - refer to details provide above

18 Yonge St – no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2437048&propertyRsn=615674>

33 Bay St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433225&propertyRsn=751123>

40 Bay St – refer to details provide above

161 Bay St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=3246951&propertyRsn=206967>

200 Bay St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433240&propertyRsn=206973>

60 Harbour St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2434664&propertyRsn=206938>

88-100 Harbour St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=3607011&propertyRsn=826634>

15-19 York St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2436569&propertyRsn=750799>

Also, please note that most of the information provided above can be found on our website via the links below:

<https://www.toronto.ca/city-government/planning-development/heritage-preservation/heritage-register/> -

using both the Heritage Register Map as well as the Heritage Search Tool.

By-law information can be found here: <https://www.toronto.ca/legdocs/bylaws/lawhome.htm>

Thank you

Yasmina Shamji
Urban Design | Heritage Planning
Toronto City Hall | 100 Queen Street West, 17th Floor East Tower
Toronto, ON M5H 2N2
[City of Toronto](#)

416-392-1975



From: Fischer, Luke [<mailto:luke.fischer@woodplc.com>]
Sent: December 16, 2020 3:13 PM
To: Yasmina Shamji <Yasmina.Shamji@toronto.ca>
Cc: Schopf, Heidy <heidy.schopf@woodplc.com>
Subject: Information Gathering Request

Hello Yasmina,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,
Luke

Luke Fischer, M.A., CAHP
Senior Archaeologist, Built Heritage Analyst
Direct: 1 (519) 681-2400
Mobile: 1 (226) 376-2968
luke.fischer@woodplc.com
www.woodplc.com



This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: unsubscribe@woodplc.com and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.

Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at <https://www.woodplc.com/policies/privacy-notice>

From: [Kevin DeMille](#)
To: [Schopf, Heidy](#)
Subject: Re: Information Gathering Request
Date: Tuesday, July 13, 2021 8:07:01 AM
Attachments: [image001.png](#)

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Good morning Heidi,

Please feel free to contact:

Kirushanth Gnanachandran
Project Coordinator, Rouge National Urban Park
Parks Canada / Government of Canada
kirushanth.gnanachandran@canada.ca /Tel: 647-205-6150

They were notified and should be able to help you or forward you on to the correct person.

Kind regards,

Kevin De Mille

Kevin De Mille
Natural Heritage Coordinator
Telephone: 437-246-5854*NEW



From: Schopf, Heidy <heidy.schopf@woodplc.com>
Sent: July 8, 2021 10:50 AM
To: Kevin DeMille <Kevin.DeMille@heritagetrust.on.ca>; Fischer, Luke <luke.fischer@woodplc.com>
Subject: RE: Information Gathering Request

CAUTION: External mail. Do not click on links or open attachments unless you recognize the sender and know the content.

Hi Kevin,

It's been a while but following up on this. Is there a contact at Parks Canada that you could recommend? We are trying to track down the correct person/department to contact regarding Union Station.

Thanks,
Heidy

From: Kevin DeMille <Kevin.DeMille@heritagetrust.on.ca>
Sent: Wednesday, April 14, 2021 11:15 AM
To: Fischer, Luke <luke.fischer@woodplc.com>
Cc: Schopf, Heidy <heidy.schopf@woodplc.com>
Subject: Re: Information Gathering Request

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Good morning Luke,

You may have these already but the Registrar provided the following information:

- 1 Front Street West: <https://www.heritagetrust.on.ca/en/oha/details?id=3781&backlinkslug=search-results&fields%5Baddress%5D=1+Front+Street>
- Union Station (note I have it as 71 Front Street West) and includes HCD info: https://www.heritagetrust.on.ca/en/oha/details?id=2024&backlinkslug=search-results&fields%5Bproperty_name%5D=Union+Station
- 40 Bay Street: <https://www.heritagetrust.on.ca/en/oha/details?id=2522&backlinkslug=search-results&fields%5Baddress%5D=40+bay+street>

There is a National Historic Site, so we recommend you reach out to Parks Canada.

Kind regards,

Kevin De Mille

Kevin De Mille
Natural Heritage Coordinator
Telephone: 437-246-5854*NEW



From: Fischer, Luke <luke.fischer@woodplc.com>
Sent: April 8, 2021 11:04 AM
To: Kevin DeMille <Kevin.DeMille@heritagetrust.on.ca>
Cc: Schopf, Heidy <heidy.schopf@woodplc.com>
Subject: RE: Information Gathering Request

CAUTION: External mail. Do not click on links or open attachments unless you recognize the sender and know the content.

Hi Kevin,

Thank you for your feedback for the TTC Waterfront East LRT CHR. We have completed a draft report for this project. For the next stage of our work, we will prepare four HIAs for the following properties:

- CHR 1: Union Station Heritage Conservation District → designated under Part V of the OHA
- CHR 2: Union Station, 65 Front Street → designated under Parts IV and V of the OHA, National Historic Site, Provincial Property of Provincial Significance
- CHR 3: Dominion Public Building, 1 Front Street → designated under Parts IV and V of the OHA, Classified Federal Heritage Building (FHBRO)
- CHR 4: Postal Delivery Building, 40 Bay Street → designated under Parts IV and V of the OHA

Do you have any further information or guidance regarding these properties? We plan to prepare the HIAs using *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties* (2017) but can use other guidance documents if you recommend a different approach.

In addition, we are carrying out community engagement for these HIAs via email and telephone. We have ongoing dialogue with MHSTCI and the City of Toronto but also plan to reach out to the Toronto Preservation Board, GO Transit, Metrolinx, and the property owners (i.e. Canada Revenue and Maple Leaf Sports). Do you have any further groups that you feel we should contact? We are committed to providing an early and ongoing dialogue regarding the potential impacts of the project to these properties so that we can incorporate feedback into our reports.

Thanks ,
Luke

From: Schopf, Heidy <heidy.schopf@woodplc.com>
Sent: Wednesday, April 7, 2021 9:44 AM
To: Fischer, Luke <luke.fischer@woodplc.com>
Subject: FW: Information Gathering Request

From: Kevin DeMille <Kevin.DeMille@heritagetrust.on.ca>

Sent: Friday, March 26, 2021 3:46 PM
To: Schopf, Heidi <heidy.schopf@woodplc.com>
Subject: Re: Information Gathering Request

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Good afternoon Heidi,

I'm sorry for the confusion. I was away in December and January and it seems your inquiry was received during my absence and unfortunately the response was not sent.

Thank you for your information request related to a Cultural Heritage Evaluation Report relating to the TTC Waterfront East LRT study area including Bay Street from Front Street West to Queens Quay East, and Queens Quay East from York Street to Cooper Street in Toronto. Your request to verify the presence of OHT heritage properties or easements within or adjacent to the study area has been processed. I've reviewed the study area against our database of OHT easements and properties. We can confirm that the OHT does not have any conservation easements or Trust-owned properties within or adjacent to the study area provided in your map.

If you have not already done so, I recommend you check the Trust's Plaque Database (available online) <https://www.heritagetrust.on.ca/en/online-plaque-guide> to verify the presence of plaques. Additionally, I recommend you check the Trust's register (available online) <http://www.heritagetrust.on.ca/en/index.php/pages/tools/ontario-heritage-act-register> and contact the local municipality to verify no local heritage properties are present within the identified study area.

As described in Section 23 of the Ontario Heritage Act, the Trust holds and maintains the provincial Ontario Heritage Act Register of properties that have been designated by municipalities under sections 29 and 41 of the Act as well as properties designated under the Act by the Minister. We rely on municipalities to send us information and it is advisable to check with the clerk's office to verify information.

Under Section 27 of the Act (OHA) the clerk of a municipality is required to maintain a local register of all designated properties. Section 27 also states that municipalities may keep a register of property that has not been designated, but that the municipality has determined to be of cultural heritage value or interest. These are often referred to as "listed" properties. These non-designated heritage properties are not reflected in the OHA Register.

Kind regards,

Kevin De Mille

Kevin De Mille
Natural Heritage Coordinator
Telephone: 437-246-5854*NEW



From: Schopf, Heidy <heidyschopf@woodplc.com>

Sent: March 25, 2021 10:02 AM

To: registrar <registrar@heritagetrust.on.ca>

Cc: Fischer, Luke <luke.fischer@woodplc.com>

Subject: FW: Information Gathering Request

CAUTION: External mail. Do not click on links or open attachments unless you recognize the sender and know the content.

Good morning,

Following up on this request. Do you have any information regarding trust-owned properties or heritage easements in this Study Area? Apologies if this information was already sent. I can find a record in my email.

Sincerely,
Heidy

From: Fischer, Luke <luke.fischer@woodplc.com>

Sent: Wednesday, December 16, 2020 3:10 PM

To: registrar@heritagetrust.on.ca

Cc: Schopf, Heidy <heidyschopf@woodplc.com>

Subject: Information Gathering Request

Hello,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,
Luke

Luke Fischer, M.A., CAHP

Senior Archaeologist, Built Heritage Analyst

Direct: 1 (519) 681-2400

Mobile: 1 (226) 376-2968
luke.fischer@woodplc.com
www.woodplc.com



This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: unsubscribe@woodplc.com and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.

Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at <https://www.woodplc.com/policies/privacy-notice>

From: [Barboza, Karla \(MHSTCI\)](#)
To: [Fischer, Luke](#)
Cc: [Hatcher, Laura \(MHSTCI\)](#); [Registrar \(MHSTCI\)](#); [Schopf, Heidy](#)
Subject: RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)
Date: Thursday, April 8, 2021 4:27:49 PM
Attachments: [image001.png](#)

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Hi Luke,

I hope this email finds you well.

It may be prudent for us to review the draft Cultural Heritage Report before we provide any advice on the approach for the Heritage Impact Assessment. Would you be able to share the draft report for our review?

Regards,
Karla

From: Fischer, Luke <luke.fischer@woodplc.com>
Sent: April-08-21 9:11 AM
To: Hatcher, Laura (MHSTCI) <Laura.E.Hatcher@ontario.ca>; Schopf, Heidy <heidy.schopf@woodplc.com>; Registrar (MHSTCI) <Registrar@ontario.ca>
Subject: RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hi Laura,

Thank you for your feedback for the TTC Waterfront East LRT CHR. We have completed a draft report for this project and we will circulate this to you for review shortly. For the next stage of our work, we will prepare four HIAs for the following properties:

- CHR 1: Union Station Heritage Conservation District → designated under Part V of the OHA
- CHR 2: Union Station, 65 Front Street → designated under Parts IV and V of the OHA, National Historic Site, Provincial Property of Provincial Significance
- CHR 3: Dominion Public Building, 1 Front Street → designated under Parts IV and V of the OHA, Classified Federal Heritage Building (FHBRO)
- CHR 4: Postal Delivery Building, 40 Bay Street → designated under Parts IV and V of the OHA

Do you have any further information or guidance regarding these properties? We plan to prepare the HIAs using *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties*

(2017) but can use other guidance documents if you recommend a different approach.

In addition, we are carrying out community engagement for these HIAs via email and telephone. We have ongoing dialogue with the City of Toronto but also plan to reach out to the Toronto Preservation Board, GO Transit, Metrolinx, and the property owners (i.e. Canada Revenue and Maple Leaf Sports). Do you have any further groups that you feel we should contact? We are committed to providing an early and ongoing dialogue regarding the potential impacts of the project to these properties so that we can incorporate feedback into our reports.

Thanks,
Luke

From: Hatcher, Laura (MHSTCI) <Laura.E.Hatcher@ontario.ca>
Sent: Friday, December 18, 2020 12:56 PM
To: Schopf, Heidi <heidyschopf@woodplc.com>; Registrar (MHSTCI) <Registrar@ontario.ca>; Fischer, Luke <luke.fischer@woodplc.com>
Subject: RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Hi Heidi and Luke,

Karla is away today so I am following up with the statements of Cultural Heritage Value for the three properties. Please let us know if you need anything else.

Sincerely,
Laura

Laura Hatcher, MCIP, RPP

Heritage Planner

Heritage Planning Unit | Programs and Services Branch | Heritage, Tourism and Culture Division

Ministry of Heritage, Sport, Tourism and Culture Industries

401 Bay Street Suite 1700 Toronto ON M7A 0A7

Tel. 437-239-3404 **New** | email: laura.e.hatcher@ontario.ca

From: Schopf, Heidi <heidyschopf@woodplc.com>
Sent: Friday, December 18, 2020 7:24 AM
To: Registrar (MHSTCI) <Registrar@ontario.ca>; Fischer, Luke <luke.fischer@woodplc.com>
Cc: Hatcher, Laura (MHSTCI) <Laura.E.Hatcher@ontario.ca>
Subject: RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hi Karla,

Thank you so much for the quick reply. Yes, please send along the 'Statements of Cultural Heritage Value' for all three properties. That would be much appreciated!

I will double check on the process as well and get back to you.

Happy Holidays

-Heidy

From: Barboza, Karla (MHSTCI) <Karla.Barboza@ontario.ca> **On Behalf Of** Registrar (MHSTCI)
Sent: Thursday, December 17, 2020 5:02 PM
To: Fischer, Luke <luke.fischer@woodplc.com>
Cc: Registrar (MHSTCI) <Registrar@ontario.ca>; Hatcher, Laura (MHSTCI) <Laura.E.Hatcher@ontario.ca>; Schopf, Heidi <heidyschopf@woodplc.com>
Subject: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

File 0013552 – TTC Waterfront East Light Rail Transit (LRT)

Hi Luke,

I hope this email finds you well.

As you may know, the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI) developed screening checklists to assist property owners, developers, consultants and others to identify known and potential cultural heritage resources:

- [Criteria for Evaluating Archaeological Potential](#)
- [Criteria for Evaluating Marine Archaeological Potential](#)
- [Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes](#)

I have used the document above (Built Heritage and Cultural Heritage Landscapes) in order to respond to your question:

- Question 3a. i. Is the property (or project area) identified, designated or otherwise protected under the Ontario Heritage Act as being of cultural heritage value e.g. a property that is designated by order of the Minister of Heritage, Sport, Tourism and Culture Industries as being of cultural heritage value or interest of provincial significance [s.34.5]?

MHSTCI Response: To date, no properties have been designated by the Minister.

- Question 3a.v. Is the property (or project area) identified, designated or otherwise protected under the Ontario Heritage Act as being of cultural heritage value included in the Ministry of Heritage, Sport, Tourism and Culture Industries' list of provincial heritage properties?

MHSTCI Response: The following provincial heritage properties are within or adjacent to the study area:

- Union Station – provincial heritage property of provincial significance
- Union Station Rail Corridor (USRC) Interlocking Tower - Scott Street - provincial heritage property of provincial significance
- Yonge Street Railway Bridge – provincial heritage property (of local significance)

Let me know if you would like a copy of the Statement of Cultural Heritage Value for any of the properties above. None of these properties have a Strategic Conservation Plan in place. Please note that if the subject lands or parts of the subject lands are owned or controlled by an Ontario Ministry or Prescribed Public Body (PPB) on behalf of the Crown (the list of PPBs is available as O. Reg. 157/10), a Ministry or PPB may have responsibilities under the [Standards and Guidelines for Conservation of Provincial Heritage Properties](#).

Regarding your request about other protected heritage properties within or adjacent to the study area, you should contact the Ontario Heritage Trust, Provincial Heritage Registrar at registrar@heritagetrust.on.ca or 416-212-7104 and the City of Toronto Heritage Preservation Services.

Please let us know which process will this project fall under (e.g. Ontario Regulation 231/08 – Transit Project Assessment Process) as we may have additional advice.

I hope this helps. Let me know if you have any questions.

Regards,
Karla

Karla Barboza MCIP, RPP, CAHP | (A) Team Lead, Heritage
Ministry of Heritage, Sport, Tourism and Culture Industries
Heritage, Tourism and Culture Division | Programs and Services Branch | Heritage Planning Unit
T. 416.314.7120 | Email: karla.barboza@ontario.ca

From: Fischer, Luke <luke.fischer@woodplc.com>

Sent: December-16-20 3:08 PM

To: Registrar (MHSTCI) <Registrar@ontario.ca>

Cc: Schopf, Heidy <heidy.schopf@woodplc.com>

Subject: Information Gathering Request

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hello,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,
Luke

Luke Fischer, M.A., CAHP

Senior Archaeologist, Built Heritage Analyst

Direct: 1 (519) 681-2400

Mobile: 1 (226) 376-2968

luke.fischer@woodplc.com

www.woodplc.com



This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: unsubscribe@woodplc.com and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.

Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at

<https://www.woodplc.com/policies/privacy-notice>

From: [Hatcher, Laura \(MHSTCI\)](#)
To: [Schopf, Heidy](#); [Registrar \(MHSTCI\)](#); [Fischer, Luke](#)
Subject: RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)
Date: Friday, December 18, 2020 12:56:19 PM
Attachments: [image001.png](#)
[MHC SCHV - Union Station FINAL.pdf](#)
[4. USRC Scott Street Interlocking Tower - MHC Decision Form & SCHV \(PHPPS\).pdf](#)
[Metrolinx Heritage Committee - Yonge Street Bridge.pdf](#)

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Hi Heidy and Luke,

Karla is away today so I am following up with the statements of Cultural Heritage Value for the three properties. Please let us know if you need anything else.

Sincerely,
Laura

Laura Hatcher, MCIP, RPP

Heritage Planner

Heritage Planning Unit | Programs and Services Branch | Heritage, Tourism and Culture Division

Ministry of Heritage, Sport, Tourism and Culture Industries

401 Bay Street Suite 1700 Toronto ON M7A 0A7

Tel. 437-239-3404 [New](#) | email: laura.e.hatcher@ontario.ca

From: Schopf, Heidy <heidy.schopf@woodplc.com>
Sent: Friday, December 18, 2020 7:24 AM
To: Registrar (MHSTCI) <Registrar@ontario.ca>; Fischer, Luke <luke.fischer@woodplc.com>
Cc: Hatcher, Laura (MHSTCI) <Laura.E.Hatcher@ontario.ca>
Subject: RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hi Karla,

Thank you so much for the quick reply. Yes, please send along the 'Statements of Cultural Heritage Value' for all three properties. That would be much appreciated!

I will double check on the process as well and get back to you.

Happy Holidays

-Heidy

From: Barboza, Karla (MHSTCI) <Karla.Barboza@ontario.ca> **On Behalf Of** Registrar (MHSTCI)
Sent: Thursday, December 17, 2020 5:02 PM
To: Fischer, Luke <luke.fischer@woodplc.com>
Cc: Registrar (MHSTCI) <Registrar@ontario.ca>; Hatcher, Laura (MHSTCI) <Laura.E.Hatcher@ontario.ca>; Schopf, Heidy <heidyschopf@woodplc.com>
Subject: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

File 0013552 – TTC Waterfront East Light Rail Transit (LRT)

Hi Luke,

I hope this email finds you well.

As you may know, the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI) developed screening checklists to assist property owners, developers, consultants and others to identify known and potential cultural heritage resources:

- [Criteria for Evaluating Archaeological Potential](#)
- [Criteria for Evaluating Marine Archaeological Potential](#)
- [Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes](#)

I have used the document above (Built Heritage and Cultural Heritage Landscapes) in order to respond to your question:

- Question 3a. i. Is the property (or project area) identified, designated or otherwise protected under the Ontario Heritage Act as being of cultural heritage value e.g. a property that is designated by order of the Minister of Heritage, Sport, Tourism and Culture Industries as being of cultural heritage value or interest of provincial significance [s.34.5]?
MHSTCI Response: To date, no properties have been designated by the Minister.
- Question 3a.v. Is the property (or project area) identified, designated or otherwise protected under the Ontario Heritage Act as being of cultural heritage value included in the Ministry of Heritage, Sport, Tourism and Culture Industries' list of provincial heritage properties?
MHSTCI Response: The following provincial heritage properties are within or adjacent to the study area:
 - Union Station – provincial heritage property of provincial significance
 - Union Station Rail Corridor (USRC) Interlocking Tower - Scott Street - provincial heritage property of provincial significance
 - Yonge Street Railway Bridge – provincial heritage property (of local significance)Let me know if you would like a copy of the Statement of Cultural Heritage Value for any of the properties above. None of these properties have a Strategic Conservation Plan in place.

Please note that if the subject lands or parts of the subject lands are owned or controlled by an Ontario Ministry or Prescribed Public Body (PPB) on behalf of the Crown (the list of PPBs is available as O. Reg. 157/10), a Ministry or PPB may have responsibilities under the [Standards and Guidelines for Conservation of Provincial Heritage Properties](#).

Regarding your request about other protected heritage properties within or adjacent to the study area, you should contact the Ontario Heritage Trust, Provincial Heritage Registrar at registrar@heritagetrust.on.ca or 416-212-7104 and the City of Toronto Heritage Preservation Services.

Please let us know which process will this project fall under (e.g. Ontario Regulation 231/08 – Transit Project Assessment Process) as we may have additional advice.

I hope this helps. Let me know if you have any questions.

Regards,
Karla

Karla Barboza MCIP, RPP, CAHP | (A) Team Lead, Heritage
Ministry of Heritage, Sport, Tourism and Culture Industries
Heritage, Tourism and Culture Division | Programs and Services Branch | Heritage Planning Unit
T. 416.314.7120 | Email: karla.barboza@ontario.ca

From: Fischer, Luke <luke.fischer@woodplc.com>
Sent: December-16-20 3:08 PM
To: Registrar (MHSTCI) <Registrar@ontario.ca>
Cc: Schopf, Heidy <heidy.schopf@woodplc.com>
Subject: Information Gathering Request

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hello,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,
Luke

Luke Fischer, M.A., CAHP
Senior Archaeologist, Built Heritage Analyst
Direct: 1 (519) 681-2400
Mobile: 1 (226) 376-2968

luke.fischer@woodplc.com
www.woodplc.com



This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: unsubscribe@woodplc.com and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.

Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at <https://www.woodplc.com/policies/privacy-notice>

From: [Gnanachandran, Kirushanth \(PC\)](#)
To: [Schopf, Heidy](#)
Cc: [Fischer, Luke](#); [Dickinson, Chelsea](#)
Subject: RE: Information Request: Union Station (65-71 Front St W) and Dominion Public Building (1 Front St W)
Date: Tuesday, July 13, 2021 5:46:06 PM
Attachments: [image002.png](#)

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Hello Heidy,

Thank you for your email. I have forwarded on your information request to a colleague at FHBRO, and another colleague that is with Cultural Resource Management. They may be reaching out to you to help you with your request.

If you don't hear back from them soon, please let me know.

Thanks and have a great day,

Kirushanth Gnanachandran

Project Coordinator, Rouge National Urban Park
Parks Canada / Government of Canada
kirushanth.gnanachandran@canada.ca / Tel: 647-205-6150

Coordonnateur de projet, Parc urbain national de la Rouge
Parcs Canada / Gouvernement du Canada
kirushanth.gnanachandran@canada.ca / Tél: 647-205-6150

Parks Canada - 450 000 km² of memories / Parcs Canada - 450 000 km² de souvenirs

From: Schopf, Heidy <heidy.schopf@woodplc.com>
Sent: July 13, 2021 12:04 PM
To: Gnanachandran, Kirushanth (PC) <kirushanth.gnanachandran@canada.ca>
Cc: Fischer, Luke <luke.fischer@woodplc.com>; Dickinson, Chelsea <chelsea.dickinson@woodplc.com>
Subject: Information Request: Union Station (65-71 Front St W) and Dominion Public Building (1 Front St W)

Good afternoon Kirushanth,

Kevin DeMille from the Ontario Heritage Trust passed your contact information to me.

Wood has been retained by the Toronto Transit Commission (TTC) to complete a Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment (CHR) and subsequent Heritage Impact Assessments (HIAs) to support the Waterfront East Light Rail Transit (WELRT) project in the City of Toronto, Ontario. To date, our work has identified that there are two properties with federal protection within the Study Area. These include:

- Union Station (65-71 Front Street West): Designated as a National Historic Site of Canada under the Historic Sites and Monuments Act by Parks Canada on 1975-11-28 (R.S.C., 1985, c. H-4)
- Dominion Public Building (1 Front Street West): Classified as a Federal Heritage Building by Parks Canada's Federal Heritage Review Office (FHBRO) in 1983

Wood has completed information gathering with the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI), the Ontario Heritage Trust, and the City of Toronto. These groups identified that Parks Canada should be consulted as part of this work given the presence of federal heritage properties within the Study Area. Accordingly, Wood is seeking background information on both Union Station (65-71 Front Street West) and the Dominion Public Building (1 Front Street West) and direction on any permits or approvals required for these properties.

Any information you can provide would be greatly appreciated.

Sincerely,

Heidy Schopf, MES, CAHP (she/her)

Built and Landscape Heritage Team Lead

Mobile: +1 (416) 518 0145

www.woodplc.com

wood.

This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us

immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: unsubscribe@woodplc.com and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.

Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at <https://www.woodplc.com/policies/privacy-notice>

From: [Tamkin Naghshbandi](#)
To: [Schopf, Heidy](#); [Dan Beare](#)
Cc: [Fischer, Luke](#); [Dickinson, Chelsea](#)
Subject: RE: Information Request: Union Station (65-71 Front Street West), Provincial Heritage Property of Provincial Significance
Date: Friday, July 16, 2021 4:27:32 PM
Attachments: [image001.png](#)

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Glad to be of help! Sounds good, thanks Heidy. Have a great weekend.

Kindly,

Tamkin Naghshbandi

T: 416.202.1804 | M: 416.271.1546

From: Schopf, Heidy <heidy.schopf@woodplc.com>
Sent: Friday, July 16, 2021 4:21 PM
To: Tamkin Naghshbandi <Tamkin.Naghshbandi@metrolinx.com>; Dan Beare <Dan.Beare@metrolinx.com>
Cc: Fischer, Luke <luke.fischer@woodplc.com>; Dickinson, Chelsea <chelsea.dickinson@woodplc.com>
Subject: RE: Information Request: Union Station (65-71 Front Street West), Provincial Heritage Property of Provincial Significance

EXTERNAL SENDER: Do not click any links or open any attachments unless you trust the sender and know the content is safe.
EXPÉDITEUR EXTERNE: Ne cliquez sur aucun lien et n'ouvrez aucune pièce jointe à moins qu'ils ne proviennent d'un expéditeur fiable, ou que vous ayez l'assurance que le contenu provient d'une source sûre.

Thank you so much Tamkin – This is hugely helpful!

I will review the documents and may be in touch again next week if I have questions.

Have a wonderful weekend,
Heidy

From: Tamkin Naghshbandi <Tamkin.Naghshbandi@metrolinx.com>
Sent: Friday, July 16, 2021 1:51 PM
To: Schopf, Heidy <heidy.schopf@woodplc.com>; Dan Beare <Dan.Beare@metrolinx.com>
Cc: Fischer, Luke <luke.fischer@woodplc.com>; Dickinson, Chelsea <chelsea.dickinson@woodplc.com>
Subject: RE: Information Request: Union Station (65-71 Front Street West), Provincial Heritage Property of Provincial Significance

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Hi Heidy,

Thanks for reaching out and providing the background on this ask. We've put pulled some documents we hope you will find of assistance. The reference documents listed below are available at the following link: <https://we.tl/t-wZ0zwDr5aI>. Please note this link will expire in 2 weeks.

- SCHV for Union Station
- Heritage Statement Report, Union Station Complex, Toronto, 2016
- Union Station Electrification HIA, 2017
- Heritage Easement Agreement, 2000
- Collateral Agreement, 2006
- Standards and Guidelines for Conservation of Provincial Heritage Properties (Ontario Heritage Act), 2010

For details related to direction on permits or approvals required for work proposed at Union Station, please see below:

- It's important to note that Union Station is owned by a number of parties.
- In 2016, Union Station Complex was identified by Metrolinx as a Provincial Heritage Property of Provincial Significance. Prescribed public bodies prescribed under Ontario Regulation 157/10, must comply with the Standards and Guidelines for Conservation of Provincial Heritage Properties (Ontario Heritage Act). Ministries and prescribed public bodies are required to use best efforts to mitigate loss of cultural heritage value or interest. The Standards and Guidelines, established by MHSTCI, set out the criteria and process for identifying provincial heritage properties and set the standards for their protection, maintenance, use and disposal. The Standards and Guidelines require all provincial ministries and prescribed public bodies to identify, protect and care for provincial heritage properties they own and manage.
- The Metrolinx owned heritage areas of the Union Station Complex are subject to a Heritage Easement Agreement dated June 30, 2000 between The Toronto Terminals Railway Company Limited and the City of Toronto and a Collateral Agreement dated May 1, 2006 between Parks Canada (Approval Authority), City of Toronto and Greater Toronto Transit Authority (GO Transit now Metrolinx) which establishes the approval process for certain alterations.
 - The Collateral Agreement notes, "the City, GO Transit and the Approval Authority acknowledge that Union Station is an operating multi-modal transportation centre and, as such, the Station Complex is subjected to intensive use by a number of parties who require a clear understanding of the approval process within the Station Complex; and ... wish to agree on a process for how the approvals required under the Easement Agreement will be carried out when there is a request for approval to undertake or permit any demolition, construction, reconstruction, alteration, remodelling or any other thing or act that would materially affect the appearance or construction of the Heritage Elements so that the Approval Authority can effectively deal with requests for approvals" (2006, p.2).
- There are additional heritage recognitions and agreements that apply to varying extents, including:

- under the Heritage Railway Stations Protection Act a heritage railway station designation in 1989 with a heritage character statement in 1992, and statement of significance in 2007;
- under the Historic Sites and Monuments Act a national historic site designation in 1975 with a commemorative integrity statement in 2002, and statement of significance in 2006;
- under the Ontario Heritage Act a Part IV designation in 2005 with reasons for designation; and
- under the Ontario Heritage Act a Part V designation in 2006 with a heritage conservation district plan.

I hope this helps and feel free to let me know if you have any questions.

Kindly,

Tamkin Naghshbandi

T: 416.202.1804 | M: 416.271.1546

From: Schopf, Heidy <heidy.schopf@woodplc.com>

Sent: Tuesday, July 13, 2021 4:32 PM

To: Dan Beare <Dan.Beare@metrolinx.com>; Tamkin Naghshbandi <Tamkin.Naghshbandi@metrolinx.com>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Dickinson, Chelsea <chelsea.dickinson@woodplc.com>

Subject: Information Request: Union Station (65-71 Front Street West), Provincial Heritage Property of Provincial Significance

EXTERNAL SENDER: Do not click any links or open any attachments unless you trust the sender and know the content is safe.
EXPÉDITEUR EXTERNE: Ne cliquez sur aucun lien et n'ouvrez aucune pièce jointe à moins qu'ils ne proviennent d'un expéditeur fiable, ou que vous ayez l'assurance que le contenu provient d'une source sûre.

Good afternoon,

Wood has been retained by the Toronto Transit Commission (TTC) to complete a Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment (CHR) and subsequent Heritage Impact Assessments (HIAs) to support the Waterfront East Light Rail Transit (WELRT) project in the City of Toronto, Ontario. To date, our work has identified that the Study Area contains a Metrolinx Provincial Heritage Property of Provincial Significance (Union Station, 65-71 Front Street West).

Wood has completed information gathering with the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI), the Ontario Heritage Trust, and the City of Toronto. Wood has also contacted Parks Canada to gather information on heritage properties within the Study Area that have federal heritage protection.

Presently, Wood is seeking background information from Metrolinx on Union Station (65-71 Front Street West). Wood respectfully requests that Metrolinx consider sharing the following background

documents with Wood:

- Statement of Cultural Heritage Value (SCHV) for Union Station (65-71 Front Street West) as approved by the Metrolinx Heritage Committee
- Previously completed cultural heritage reports related to the Union Station Complex. Reports that are of relevance to the TTC WELRT work include:
 - CHR, CHERs, or HIAs related to the Bay Street Bridge/Union Station Trainshed
 - CHR, CHERs, or HIAs related to work on the east elevation/Bay Street side of the Union Station building
- Direction on permits or approvals required for work proposed at Union Station (65-71 Front Street West)

Any information you can provide would be greatly appreciated.

Sincerely,

Heidy Schopf, MES, CAHP (she/her)

Built and Landscape Heritage Team Lead

Mobile: +1 (416) 518 0145

www.woodplc.com

The logo for Wood, featuring the word "wood." in a bold, dark blue, sans-serif font. The period is a small dot.

This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: unsubscribe@woodplc.com and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.

Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at <https://www.woodplc.com/policies/privacy-notice>

This e-mail is intended only for the person or entity to which it is addressed. If you received this in error, please contact the sender and delete all copies of the e-mail together with any attachments. This e-mail is intended only for the person or entity to which it is addressed. If you received this in error, please contact the sender and delete all copies of the e-mail together with any attachments.

From: [Philpott, Blair \(PC\)](#)
To: [Schopf, Heidy](#); [Dickinson, Chelsea](#); [Fischer, Luke](#)
Cc: [Gnanachandran, Kirushanth \(PC\)](#); [Miller, Deborah \(PC\)](#); [Bruce, Shelley \(PC\)](#)
Subject: Information Request: Union Station (65-71 Front St W) and
Date: Monday, July 26, 2021 8:36:28 AM

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Hello,

This request for information was passed along to me.

The Dominion Public Building (1 Front St W) is no longer a Federal Heritage Building. Its custodian department, Public Service and Procurement Canada (PSPC) disposed it to the Canada Lands Company in 2017.

My colleague, Shelley Bruce is involved with Union Station NHS. She has been supporting the site and can put you in touch with the management.

Her contact information is:

Shelley Bruce
Built Heritage Advisor, Indigenous Affairs and Cultural Heritage Directorate
Parks Canada / Government of Canada
300 - 300 West Georgia St, Vancouver, BC V6B 6B4
shelley.bruce@canada.ca / Tel: 604.505.6245

Regards,
Blair

L. Blair Philpott
A/Cultural Resource Management Advisor / Conseillère de la gestion des ressources culturelles
Parks Canada Agency / L'Agence Parcs Canada
145 McDermot Avenue / 145, avenue McDermot
WINNIPEG MB R3B 0R9
blair.philpott@canada.ca
Telephone / Téléphone 204 984 - 1298
Cell / 204 583 – 3094 - **NEW**
Government of Canada / Gouvernement du Canada

Appendix E: Assessor Qualifications

Heidy Schopf, MES, CAHP – Cultural Heritage Team Lead

Heidy Schopf is the Cultural Heritage Team Lead for WSP Canada Inc. She is a Senior Cultural Heritage Specialist and has worked in the field of cultural resource management since 2007. Ms. Schopf is a Professional Member of the Canadian Association of Heritage Professionals (CAHP).

Ms. Schopf has worked on hundreds of cultural heritage projects in Ontario, including Cultural Heritage Reports, Cultural Heritage Evaluation Reports (CHERs), Heritage Impact Assessments (HIAs), Strategic Conservation Plans (SCP), heritage documentation (photography, photogrammetry, and LiDAR), Heritage Conservation District (HCD) Studies and Plans, and heritage peer review. She regularly provides cultural heritage conservation guidance to public and private sector clients. Heidy is a Senior Project Manager and has managed and delivered cultural heritage work under a variety of processes, including: Environmental Assessment Act, Planning Act, Transit Project Assessment Process (TPAP), and the Ontario Heritage Act. She has extensive and applied knowledge of Ministry of Citizenship and Multiculturalism (MCM) guidance documents for heritage properties.

Ms. Schopf has had the privilege of working with Indigenous Nations on several projects to gather Indigenous perspectives on cultural heritage and integrate this shared learning into WSP's heritage work.

Chelsea Dickinson, BA, CAHP - Cultural Heritage Specialist

Ms. Dickinson is a Cultural Heritage Specialist for WSP Canada Inc. and has worked in the field of cultural resource management since 2015. Ms. Dickinson holds an Honours B.A. Degree in Near Eastern and Classical Archaeology from Wilfrid Laurier University, a Post-Graduate Certificate in Geographical Information Systems from Fanshawe College and is currently pursuing her Master of Arts in Planning at the University of Waterloo. She has worked on a wide variety of projects throughout Ontario including: cultural heritage resource assessments and screenings, cultural heritage evaluations, cultural heritage impact assessments, conservation plans, Stage 1-4 archaeological assessments and documentation reports. She is experienced in applying local, Provincial, and Federal heritage guidelines and regulations to evaluate protected and potential cultural heritage properties and specializes in historical background research. She has managed and produced deliverables on cultural heritage projects under a variety of processes, including: the Aggregates Act, Environmental Assessment Act, Ontario Heritage Act, Planning Act, and the Transit Project Assessment Process. Ms. Dickinson is a Professional Member of the Canadian Association of Heritage Professionals (CAHP) and holds an Applied Research License (R1194) issued by the Ministry of Citizenship and Multiculturalism.

Appendix F: Limitations

Limitations

1. The work performed in the preparation of the Heritage Impact Assessment (HIA) and the conclusions presented are subject to the following:
 - (a) The Standard Terms and Conditions which form a part of our Professional Services Contract;
 - (b) The Scope of Services;
 - (c) Time and Budgetary limitations as described in our Contract; and,
 - (d) The Limitations stated herein.
2. No other warranties or representations, either expressed or implied, are made as to the professional services provided under the terms of our Contract, or the conclusions presented.
3. The conclusions presented in the report are based, in part, on visual observations of the Study Area. Our conclusions cannot and are not extended to include those portions of the Study Area which were not reasonably available, in WSP Environment & Infrastructure's opinion, for direct observation.
4. The potential and protected cultural heritage resources encountered at the Study Area were assessed, within the limitations set out above, having due regard for applicable heritage regulations as of the date of the inspection.
5. Services including a background study and fieldwork were performed. WSP Environment & Infrastructure's work, including archival studies and fieldwork, were completed in a professional manner and in accordance with the Ministry of Citizenship and Multiculturalism's guidelines. It is possible that Cultural heritage resources not visible from the public realm may be within, or adjacent to the Study Area.
6. The utilization of WSP Environment & Infrastructure's services during the implementation of any further cultural heritage work recommended will allow WSP Environment & Infrastructure to observe compliance with the conclusions and recommendations contained the HIA. WSP Environment & Infrastructure's involvement will also allow for changes to be made as necessary to suit field conditions as they are encountered.
7. This report is for the sole use of the parties to whom it is addressed unless expressly stated otherwise in the report or contract. Any use which any third party makes of the report, in whole or in part, or any reliance thereon, or decisions made based on any information of conclusions in the report, is the sole responsibility of such third party. WSP Environment & Infrastructure accepts no responsibility whatsoever for damages or loss of any nature or kind suffered by any such third party as a result of actions taken or not taken or decisions made in reliance on the report or anything set out therein.
8. This report is not to be given over to any third-party other than a governmental entity, for any purpose whatsoever without the written permission of WSP Environment & Infrastructure, which shall not be unreasonably withheld.