

## City-Initiated Highland Creek Village Area Study – Final Report

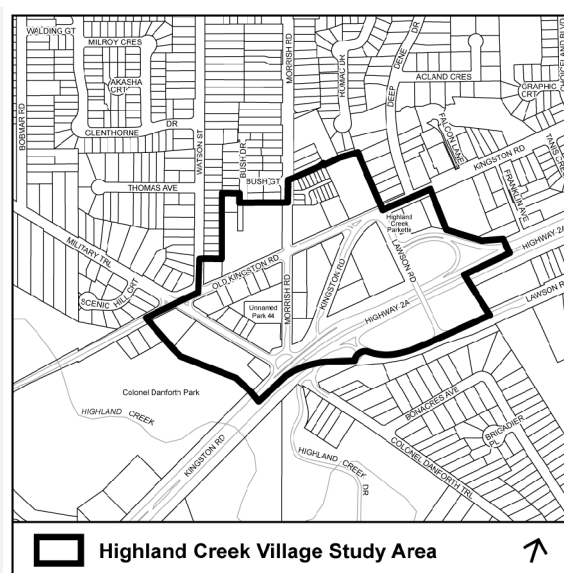
<b>Date:</b>	October 19, 2012
<b>To:</b>	Scarborough Community Council
<b>From:</b>	Director, Community Planning, Scarborough District
<b>Ward:</b>	Ward 44 – Scarborough East
<b>Reference Number:</b>	10 303518 EPS 44 TM

### SUMMARY

These City-initiated official plan and zoning by-law amendments are subject to the new provisions of the Planning Act and the City of Toronto Act, 2006.

This report summarizes and contains recommendations to implement the Highland Creek Village Area Study for the lands generally located between Old Kingston Road, Highway 2A, Lawson Road and Military Trail.

City staff, the Ward Councillor, and members of the local community have worked together to develop a vision for new development and improvement to the Highland Creek Village while respecting the unique physical and contextual attributes of the area. To implement this area study, the report recommends approval of amendments to the Official Plan and the Highland Creek Community Zoning By-law as shown on Attachments 6 and 7, as well as adoption of the revised Highland Creek Village Design Guidelines shown on Attachment 8. The report also outlines implementation strategies to attract investment to the community, improve the streetscape, and ensure that any new development is consistent with the established vision.



## RECOMMENDATIONS

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### **The City Planning Division recommends that:**

1. City Council amend the official plan for the lands known as 255 Old Kingston Road, 1625 Military Trail, 6000, 6115, 6143-6147, 6149, 6096, and 6098 Kingston Road, all lands on the north side of Old Kingston Road and Kingston Road between Lawson Road and Watson Street, all lands south of Old Kingston Road and north of Kingston Road and Highway 2A between Lawson Road and Military Trail, and all lands comprising the Highway 2A right-of-way bordered by Lawson Road to the south, Military Trail to the west, and Franklin Avenue and 56 Lawson Road to the east, substantially in accordance with the draft official plan amendment attached as Attachment 6 to the report (October 19, 2012) from the Director, Community Planning, Scarborough District.
2. City Council amend the Highland Creek Community Zoning By-law No. 10827, as amended, for the lands known as 255 Old Kingston Road, 1625 Military Trail, 6000, 6115, 6143-6147, 6149, 6096, and 6098 Kingston Road, all lands on the north side of Old Kingston Road and Kingston Road between Lawson Road and Watson Street, and all lands south of Old Kingston Road and north of Kingston Road and Highway 2A between Lawson Road and Military Trail, substantially in accordance with the draft zoning by-law amendment attached as Attachment 7 to the report (October 19, 2012) from the Director, Community Planning, Scarborough District.
3. City Council authorize the City Solicitor to make such stylistic and technical changes to the draft official plan and zoning by-law amendments as may be required.
4. City Council adopt the revised Highland Creek Village Urban Design Guidelines as generally shown on Attachment 8 to the report (October 19, 2012) from the Director, Community Planning, Scarborough District.
5. City Council direct the General Manager of the Transportation Services Division in collaboration with the Chief Planner and Executive Director of the City Planning Division, and in consultation with the Ward Councillor to undertake a Transportation Environmental Assessment for the purpose of determining the feasibility of transportation network changes to the Highland Creek Village Area, including those noted below and identified on the concept plan shown on Attachment 11 to the report (October 19, 2012) from the Director, Community Planning, Scarborough District, beginning no later than 2013:
  - a. reconfiguration of the Old Kingston Road and Kingston Road intersection;

- b. elimination of access to Highway 2A from Kingston Road and Morrish Road;
  - c. addition of a westbound access to Highway 2A from Lawson Road;
  - d. provision of a new road connection linking Meadowvale Road with Highway 2A; and,
  - e. installation of a 'T' intersection at Highway 2A and Military Trail.
- 6. City Council direct Transportation Services staff to consider the Highland Creek Village study area as a priority area for the placement of street furniture under the City's Co-ordinated Street Furniture Program.
- 7. City Council direct Transportation Services staff to study the existing angled parking spaces on the north side of Old Kingston Road between Morrish Road and Lawson Road.
- 8. City Council direct Transportation Services, Public Realm Management and Overhead staff to consider the Highland Creek Village study area as a priority area for the design and placement of branded street signage in consultation with the Ward Councillor.
- 9. City Council direct Parks, Forestry and Recreation staff to direct the alternative parks levy above the first 5%, arising from developments in the Highland Creek Village study area towards parks, other recreational purposes, in the Highland Creek Village Community within, or in close proximity to the study area, in consultation with the Ward Councillor.
- 10. City Council direct Real Estate Services staff to initiate the transfer of operational management of the vacant city-owned lands on the west side of Morrish Road, south of Old Kingston Road, and north of Kingston Road to the Parks, Forestry and Recreation Division.
- 11. City Council direct Parks, Forestry and Recreation staff, in consultation with the Ward Councillor, to report to Community Council on naming the vacant city-owned lands on the west side of Morrish Road, south of Old Kingston Road, and north of Kingston Road, as a park.

### **Financial Impact**

Planning staff are recommending a Transportation Environmental Assessment (EA) be undertaken as part of the recommendations contained in this report.

A transportation EA is necessary to examine the conceptual road network changes detailed in this report (see Road Network Improvements section later in this report and Attachment 11) and provide Council with recommendations on the implementation, alternatives, cost, and effectiveness of any road network changes. In order to complete the EA, the City must hire a consultant specializing in transportation engineering, though

Transportation Infrastructure Management and Infrastructure Planning staff will be responsible for project managing the EA.

The estimated cost of the transportation EA consultant assignment is between \$200,000 and \$300,000 and will require 12 to 18 months to complete. City Planning Division will contribute \$50,000 to the cost of the EA, to which funds have been allocated in the Division's 2012 capital budget. The remaining funds to undertake the EA will be allocated to the project by Transportation Services in their 2013 capital budget.

The Deputy City Manager and Chief Financial Officer have reviewed this report and agree with the financial impact information.

## **BACKGROUND**

### **Decision History**

At its meeting on February 16, 2011, Scarborough Community Council received a report (Agenda Item No. SC4.22) advising of City Planning staff's intention to initiate a study of the area known as the Highland Creek Village, as shown on Attachment 1. This report can be found through the following website link:

<http://www.toronto.ca/legdocs/mmis/2011/sc/bgrd/backgroundfile-35428.pdf>

A webpage has been created for this study. It is found at the following website link:

<http://www.toronto.ca/planning/highlandcreekvillage.htm>

### **Recent Development Proposals and Approvals**

There are currently five development applications in various stages of the review and approval process (locations shown on Attachment 4).

#### **1625 Military Trail**

Applications were submitted in April 2010 to permit a 4-storey, 160-unit retirement residence that could house approximately 175 to 200 residents. This application is currently on hold pending the outcome of the area study.

#### **280 Old Kingston Road**

A site plan control application was submitted in March 2010 for a parking lot containing 75 spaces on the vacant lot at 280 Old Kingston Road to be used for additional parking by St. Joseph's Parish. This application was approved on June 15, 2010.

#### **6039 Kingston Road**

Applications were submitted in December 2009 to permit a 4-storey apartment building containing 30 units including live/work units. On December 1, 2010 the official plan and zoning by-law applications were approved by the Ontario Municipal Board (OMB) for a 30-unit stacked townhouse development with surface parking.

### **363 Old Kingston Road**

Applications were submitted in June 2010 to permit a 3-storey, 39-unit residential development with ground floor commercial. The official plan and zoning amendment applications were approved on October 4, 2011.

### **1624 Military Trail**

Applications were submitted in 2006 to permit a 42 residential unit development with 9 live/work units in a 4-storey stacked townhouse format. The applications are currently on hold pending the outcome of the area study.

## **Area History**

In the 1850's, Highland Creek was the largest residential and business centre in the former Township of Scarborough. Highland Creek was officially recognized as a community in 1852 when it was granted its own post office. Highland Creek also contained Presbyterian, Catholic, and Methodist churches, a school and a number of general stores, blacksmith shops and hotels that catered to those travelling along Kingston Road.

Highland Creek remained a rural farming community until the 1950's, when the majority of the present day neighbourhood was developed. Since this time, Highland Creek Village has seen little development and contains mostly one and two-storey commercial uses and a large number of automotive uses, such as paint and body shops and automobile sales businesses.

While the Village has not retained much of its built heritage, the Morrish Store, which forms part of the Morrish Plaza, was constructed in 1891 and is listed as a significant heritage building. It is located on the northeast corner of Old Kingston Road and Morrish Road.

## **Area Characteristics**

The study area consists of 52 properties located generally between Kingston Road, Old Kingston Road, Highway 2A, Military Trail, and Lawson Road, over a relatively flat area of 9.5 hectares (23.5 acres), as shown on Attachment 1. The study area is bordered on the west by the Highland Creek ravine.

The Village generally contains one and two-storey commercial buildings. Some older house form buildings on Morrish Road and Military Trail have also been converted to commercial, office, or automotive sales and service related uses.

The streetscape is poorly defined and offers little to promote pedestrian activity. Some mature street trees line the western part of the study area, but the majority of the area lacks landscaping within the public realm.

Lands to the north, east and south of the study area consist of existing stable neighbourhoods composed predominantly of large-lot single detached dwellings.

Lots within the study area span a broad range of sizes and configurations. The unique road pattern results in a number of irregular shaped lots.

## **Land Use**

The primary land uses within the study area consist of automotive sales and automotive repair and service, and retail uses. Automotive uses are situated throughout the Village, while the majority of retail uses are located along Old Kingston Road.

A townhouse development was recently constructed north of the plaza at the northeast corner of Old Kingston Road and Morrish Road and represents the only residential development within the study area, with the exception of a few dwelling units situated above existing commercial plazas.

Two places of worship, the Baitul Afiyat Mosque (255 Old Kingston Road), and St. Joseph's Parish (200 Morrish Road), are situated within the study area along Old Kingston Road.

There are a number of vacant parcels and vacant buildings within the Village.

## **Transportation and Parking**

The study area is intersected east-west by two main roads: Old Kingston Road/Kingston Road, upon which the majority of existing development is situated; and Highway 2A, a four-lane arterial road.

The Toronto Transit Commission (TTC) currently serves the Village with two bus routes. GO Bus drop-off service is available at Highway 2A and Military Trail. The nearest GO Transit station is Rouge Hill station, located east of the study area at East Avenue and Lawrence Avenue East.

Parking within the Village is provided on-street in front of the commercial buildings on Kingston Road and Old Kingston Road. Angled parking is provided in front of the Morrish Plaza on the north side of Kingston Road, east of Morrish Road. Street parking is also permitted on Morrish Road and Military Trail.

Though not officially a public parking area, the St. Joseph's Parish parking lot is often used by visitors to the Village when the church is not holding service.

## **Built Environment and Heritage**

The Morrish Plaza, situated at the northeast corner of Morrish Road and Old Kingston Road, St. Joseph's Cemetery (not including the church) and the Methodist Cemetery, are properties listed on the heritage inventory.

There are currently no buildings within the Village designated under the Ontario Heritage Act, RSO 1990.

## **Parks and Community Space**

Highland Creek Parkette, a small park on the southeast corner of Kingston Road and Lawson Road provides for a small sitting area within the Village, while a City-owned parcel of land located on the west side of Morrish Road, just south of Old Kingston Road, is occasionally used for local events. This parcel is currently under the operational management of the Real Estate Services Division.

At fifty-four hectares, Colonel Danforth Park is the largest park in the vicinity and is situated just west of the study area. West of Colonel Danforth Park lies a portion of the University of Toronto Scarborough Campus, which contains baseball and soccer fields, as well as tennis courts. A trail system runs through the park from Lawrence Avenue East to Lake Ontario.

Several city-owned community centres, including the Port Union Recreation Centre, West Rouge Community Centre, Heron Park Community Centre, Tall Pines Community Centre and Curran Hall Community Centre, offer a range of recreational programs to serve the Highland Creek area, though none of these centres are located within the Village itself. Heron Park Recreation Centre is the closest facility, located at Manse Road and Lawrence Avenue East and is well used by local residents for recreational sports and programs. The facility features an outdoor pool and two indoor ice pads. The Highland Creek Branch Library is located on Ellesmere Road, just north of the Village.

In November 2009, the City of Toronto won the bid to host the 2015 Pan Am Games. A new, approximately 37,000 square metre (400,000 square feet) competitive aquatic centre and multi-sport field facility, will be built on University of Toronto Scarborough Campus and City of Toronto lands located northwest of the study area at Military Trail and Morningside Avenue. The proposed facility will include two 50-metre swimming pools, a dive tank, gymnasiums and running and sprint track facilities, minor recreational facilities as well as offices, and the headquarters of the Canadian Sports Institute of Ontario (CSIO) for training and development of high performance athletes.

## **Schools**

While the study area itself does not contain any primary or secondary schools, Highland Creek Public School, West Hill Collegiate Institute, Sir Oliver Mowat Collegiate Institute, Cardinal Leger Catholic School, St. Malachy Catholic School and Pope John Paul II Catholic Secondary School, are all within the vicinity of the study area.

The Toronto District School Board (TDSB) has stated that Highland Creek Public School (Junior and Senior) and West Hill Collegiate Institute will provide capacity for new students generated from the proposed residential developments within the study area.

The Toronto Catholic District School Board (TCDSB) has stated that Cardinal Leger and St. Malachy (Elementary) and Pope John Paul II (Secondary) schools will serve any new student population.

## **Policy Statement and Provincial Plans**

### **The Provincial Policy Statement**

The Provincial Policy Statement (PPS) was approved by the Lieutenant Governor in Council and came into effect in 2005. The PPS provides policy direction on matters of provincial interest related to land use planning and development. The Planning Act requires that all decisions affecting land use planning matters "shall be consistent with" the Provincial Policy Statement.

The PPS encourages land use and development planning to support strong, liveable and healthy communities by heavily endorsing intensification. It provides clear policy direction on land use planning to promote strong communities, a clean and healthy environment and a strong economy. It includes policies on key issues, such as, the efficient use and management of land and infrastructure, protection of the environment, and ensuring appropriate opportunities for employment and residential development, including support for a mix of uses.

### **The Growth Plan for the Greater Golden Horseshoe**

The Growth Plan for the Greater Golden Horseshoe was released by the Province of Ontario in 2006 under the Places to Grow Act (2005). It provides a framework for managing growth in the Greater Golden Horseshoe including: directions for where and how to grow; the provision of infrastructure to support growth; and protecting natural systems and cultivating a culture of conservation.

City Council's planning decisions are required by the Planning Act to conform, or not conflict, with the Growth Plan for the Greater Golden Horseshoe.

### **Official Plan**

The Official Plan designates the majority of the lands within the study area as 'Mixed Use Areas' (Attachment 2). 'Mixed Use Areas' are made up of a broad range of commercial, residential and institutional uses, as well as parks and open spaces. These areas will absorb, over time, a portion of Toronto's expected growth. 'Mixed Use Areas' will vary in scale and intensity subject to a site's locational attributes and context.

The Official Plan contains a number of policies to guide the development of 'Mixed Use Areas', including: the creation of a balance of uses that reduces automobile dependence and meets the needs of the community; the provision of new jobs and homes on underutilized lands; the location and massing of buildings to frame streets, minimize shadow impacts and provide areas of transition toward lower scale 'Neighbourhoods'; and the provision of an attractive, safe and comfortable pedestrian environment.

A small area of City-owned lands at the southeast part of the study area is designated 'Neighbourhoods'. 'Neighbourhoods' are typically composed predominantly of low rise residential uses. The lands within the study area subject to this designation are vacant, and situated adjacent to an overpass, and currently undevelopable.



The core of the Highland Creek Village Community is located in the Highland Creek Community Secondary Plan and is designated 'Mixed Use Areas' permitting a variety of uses, including residential uses.

The study area is contained within the Highland Creek Community Secondary Plan. The policies contained within the Secondary Plan require that any development in the Highland Creek Village area not exceed a density of 37 units per hectare and be designed in accordance with the urban design guidelines of the area.

## **Zoning**

The lands within the study area are subject to the provisions of the Highland Creek Community Zoning By-law No. 10827, as amended, and the Centennial Community Zoning By-law No. 12077, as amended. Within the Highland Creek Community Zoning By-law, lands are zoned predominantly as Highway Commercial (HC) and Place of Worship (PW), with other lands being zoned Neighbourhood Commercial (NC), and Community Commercial (CC), and Highway (HWY) (Attachment 3). These zones provide for a mix of commercial and office uses, and places of worship in the case of the PW zone, but do not provide for significant residential permissions. Four properties within the study area are zoned Institutional (I), which permits a range of office and community uses. Within the Centennial Community Zoning By-law, lands are zoned and Highway (HWY) and Single-Family Residential (S) (Attachment 3). The Single-Family Residential (S) zone permits single-detached dwellings and group homes while the Highway (HWY) zone permits highways.

In order to implement the vision and concept plan, amendments to the Highland Creek Community Zoning By-law are required. The proposed zoning by-law amendment is attached as Attachment 7.

## **Site Plan Control**

All lands within the study area are subject to the City's Site Plan Control By-law No. 774-2012.

## **Urban Design Guidelines**

In 2002, City staff, in consultation with the Ward Councillor and the residents of Highland Creek, undertook a review of the built characteristics of the Highland Creek area. The result of this exercise was the preparation of the current Highland Creek Village Design Guidelines.

The Guidelines contain a number of provisions to regulate the built form and site planning within the Village area, including:

- orienting buildings towards the street;
- prohibiting parking in front of buildings (except street parking);
- providing vehicular access from side streets where available;
- massing buildings to create a strong street wall;
- providing pedestrian access from the street;

- landscaping parking lot islands; and,
- limiting buildings to a maximum building height of 12 metres (approximately 3½ storeys) along Old Kingston Road.

## **Study Process**

The Highland Creek Village Area Study was initiated by City Planning staff in December 2010.

An information report detailing the intent of the area study was received by Scarborough Community Council on February 16, 2011 (Item Number SC4.22). A notice was then published in the area's local paper, the 'Highland Creek Villager' and mailed to 1000 plus property owners within and surrounding the study area. The notice included an opportunity for those interested to join a working group to assist City Staff with the study.

The working group, which consisted of 22 area residents, landowners, and business owners, was established to comment on options, and provide feedback to staff throughout the study process. The working group met on five occasions between March 2011 and February 2012 to discuss matters such as land use, vehicular movement and parking, community services, and urban design, and to provide input on preliminary conclusions.

Prior to the first working group meeting, a web page was created to enable those interested to obtain information, learn about the study and follow its progress. The web page provides links to the various presentation materials prepared by city staff and other related materials from the City's website. It can be viewed at the following address: <http://www.toronto.ca/planning/highlandcreekvillage.htm>.

A business community information meeting for business owners in Highland Creek Village was held in May 2011. Invitations to the business community information meeting were sent to each property owner within the study area and hand delivered to each individual business. A total of 12 people attended the meeting.

A design charrette was held on July 16, 2011, to further develop a vision for the future of Highland Creek Village through creative exercises and discussions. Notice for the design charrette was circulated to approximately 1500 property owners, in addition to those who had requested further notification through the course of the study. Approximately 50 residents, business owners, and working group members attended the charrette. Charrette attendees were organized into five groups and lead through discussions on land use, transportation, and built form by facilitators at each table. Using images, trace paper, and modelling foam, each group was responsible for creating a model illustrating a vision for the Village.

On April 18, 2012, a community information meeting was held to inform residents about the area study conclusions and seek their comments. The meeting was attended by approximately 300 people.

On June 12, 2012, a meeting was held with Councillor Moeser and representatives from the community associations in and around the area. This meeting was arranged to allow City staff an opportunity to provide an update on the study and discuss the changes made to the recommendations to address comments and concerns raised at the April 18 community information meeting.

On October 9, 2012, a second community information meeting was held to inform residents about amendments to the study conclusions in response to comments and concerns raised at the April 18, 2012 meeting. The meeting was attended by approximately 200 people.

## **COMMENTS**

The recommendations presented in this report have been developed in consultation with the appropriate agencies and City divisions.

## **The Vision**

The goal of this study is to create a vision for the Highland Creek Village (HCV) along with an implementation strategy to provide clear direction and guidance for future public and private initiatives to achieve the vision.

Community engagement has been instrumental in developing the vision. Through the public consultation process, the community has identified the following key values associated with the future development of the Village:

- respect for the village character;
- respect for the surrounding established neighbourhoods;
- the development of attractive mixed use buildings of appropriate heights;
- attractive, accessible and safe streets and public spaces;
- increased opportunities for visitors to park in and around the HCV; and
- a transition away from the prevalence of automotive uses.

A Vision Statement was established to reflect these values:

Over time, the Highland Creek Village will become a vibrant, mixed-use, community-focused, pedestrian-friendly destination, where attractive building design references the area's heritage, respects the Village character and the surrounding established neighbourhoods, and attracts new businesses and residents to live and work in the community.

The study envisions that the Village will consist of three general areas: 1) the Main Street, 2) the South Village, and 3) the East Village. These areas are further defined in the revised Urban Design Guidelines (See Attachment 8).

### **The Main Street**

Old Kingston Road, the Village's 'Main Street', represents an excellent opportunity for a lively, pedestrian-oriented, low-rise, mixed-use destination. Ground level commercial and personal services will provide residents and visitors a place to eat and shop, while the above-grade office uses will provide additional jobs and services. New residential units above the retail uses will attract a mix of residents to the area, and provide a variety of housing options to existing area residents.

### **The South Village**

The South Village includes properties south of the 'Main Street' area and north of Highway 2A within the study area. While commercial at grade is permitted, this area is anticipated to be mostly a "green" residential area along local streets lined with double rows of trees and buildings with pedestrian friendly scale. Residents in the area will have views and access to the green spaces in the Village, including the Village Green and the Highland Creek Ravine. The residential developments in the South Village will provide much needed support for the businesses on the Main Street.

### **The East Village**

The East Village includes lands generally around the Highland Creek overpass. It is anticipated to be a future mix-use or residential area which will further support the overall vitality of the Village. Developments in this area will take a mid-rise form similar to those in the South Village, to respect and enhance the Village character. Developments in this area will largely depend on the demolition of the overpass in the future.

The study promotes the enhancement of the community identity and the Village character. A traditional main street character will be established along Old Kingston Road. Branded street signage, decorative street and pedestrian lights, as well as public art installations that reflect the Village's history, are encouraged throughout the Village. Improvements to the existing historic landmark sites in the Village are encouraged.

The study recommends substantial public investments and improvements in the Village. These include potential road reconfigurations to calm the traffic and to enhance Village character, introduction of a public lane to promote a more pedestrian friendly Main Street, creation of the Village Green as a focal point and gathering place, and streetscape improvements, including street tree planting, decorative lighting, and new street furniture.

The study recommends mid-rise developments of 4 to 6 storeys as the predominant built form for the Village, as well as a gradual transition in heights from the Main Street to the edges of the Village, where buildings are contemplated to reach between 6 and 8 storeys. The study recognizes that, while the Village presents many opportunities for redevelopment with its large vacant or underutilized lots, redevelopment must take a form which respects the characteristics and qualities residents have come to identify as being unique to the Village. The proposed built form approach will allow the Village to grow and attract investment needed to revitalize the Village, while respecting its history and character.

The transformation of HCV will occur incrementally. Over time, it will allow people to live, shop, work, raise children, and age in place within the Village, and enhance the quality of life for existing and new residents. The recommendations will provide strong guidance to the future initiatives and developments in the Village to achieve the common Vision.

## **Guiding Principles**

City Planning staff, in consultation with the working group, developed the following five principles to inform the direction and conclusions of the study. Further details of these principles can be found in Attachment 5. The principles can be summarized as follows:

### **Transformation**

Create a vibrant community that attracts new businesses. Promote key sites that will enable positive change and drive re-investment in the area.

### **Community Identity**

Reflect and refine the character of the Village.

### **Movement**

Promote the use of transit, cycling and walking as viable modes of transportation to access and explore the Village area. Encourage new on-street and off-street parking opportunities.

### **Safety and Accessibility**

Foster comfort and safety and accommodate people of all abilities and ages within the physical environment.

### **Development as a Good Neighbour**

Further define Highland Creek Village through compatible development and protection of the adjacent, stable, low-rise residential areas.

The following sections summarize comments received to date on the study vision; detail the proposed official plan and zoning by-law amendments; and, summarize other key strategies to implement the study's conclusions.

## **Community Consultation**

The community has raised a number of concerns and comments throughout the course of the study process related to land use, transportation, urban design and others. These comments are identified generally in the following chart:

	<b>Public Comment</b>
<b>Land Use</b>	Support for residential uses in the Village.
	Townhouses should be permitted in the Village. Some suggest that they should be the dominant form of residential use, while others note that mid-rise residential buildings are more appropriate.
	Many residents are not in support of any purpose-built student housing being located in the Village.
	Small scale retail stores are preferred over large format stores.
	Automotive sales and service uses do not contribute to the character of the Village and should be removed over time.
<b>Parks</b>	The park space on Morrish Road should be programmed and improved as a community gathering space with easier access from Old Kingston Road.
<b>Parking and Roads</b>	There is a lack of convenient parking in the Village. The City should acquire lands for a free parking lot within the Village.
	Opportunity for access to Highway 2A from Meadowvale Road should be explored. Residents living on Meadowvale are strongly opposed to a connection at Meadowvale and Highway 2A.
	A road connection should not be made between Colonel Danforth and Military Trail.
	The Highland Creek Overpass should be removed at the end of its lifespan, and either a signalized intersection or a roundabout should be installed at the Lawson Road / Highway 2A intersection. Some residents are not in favour of removal of the overpass.
	Some would prefer more frequent bus transit in the Village while others would prefer light rail transit options.
	While there were strong opinions presented regarding the possibility for connections at Military Trail and Colonel Danforth Trail and an intersection at Meadowvale Road and Highway 2A, a number of respondents were supportive of road network changes within the Village.
<b>Urban Design</b>	Crime Prevention Through Environmental Design (CPTED) principles should be implemented throughout the Village.
	A more pedestrian-friendly streetscape with patios, hanging baskets, banners and consistent light fixtures are key to the implementation of the vision for the main street area of the Village.
	The City needs to be able to secure appropriate building design for any new development in the Village.

<b>Built Heritage</b>	Highland Creek's significant heritage should be preserved through building design.
	A Heritage District should be considered.
<b>UofT Scarborough Campus</b>	Connections to the University of Toronto, Scarborough Campus should be strengthened to encourage students to visit Village shops.
<b>Building Height</b>	<p>Throughout the course of the study, increased building height in the Village was met with a mix of comments. Some study participants are quite content with the perceived current height limit of approximately 3 storeys throughout the Village while others support taller buildings, including 6 and 8 storey buildings where appropriate.</p> <p>While many residents noted they were opposed to buildings of 6 storeys and higher because of the potential for increased traffic and detriment to the heritage character of the area, most noted that bringing more people into the Village would be positive for area businesses.</p> <p>Lands which were not adjacent to, or near, existing single family dwellings were considered more appropriate locations for mid-rise development, while lands along Old Kingston Road were to maintain the small town feel, which typically features 2, 3 and 4 storey buildings.</p> <p>In areas closer to Highway 2A, where a number of vacant lots and automotive uses currently exist, maximum heights of 6 storeys were generally considered acceptable. The lands on the west side of Military Trail were also considered to be suitable for buildings with a maximum height between 6 and 8 storeys.</p> <p>Buildings of 11 storeys or greater were not considered appropriate by the majority of residents.</p>

## DISCUSSION AND IMPLEMENTATION

In order to implement the vision established through this study, the proposed official plan amendment (Attachment 6) contains a number of policies that:

1. address built form objectives of the study;
2. recognize existing parks and other open space areas;
3. adjust the boundary of the Highland Creek Community Secondary Plan to include lands within the Centennial Community;
4. amend transportation rights-of-way; and,
5. outline a development framework to guide growth within the Village.

## Official Plan Amendment

### Streets and Lanes

The official plan amendment proposes to increase the right-of-way width of Old Kingston Road, between Kingston Road and Military Trail, from 20 metres to 23 metres to provide sufficient space for pedestrian movement, increased opportunities for landscaping, light fixtures, and street furniture, as well as other streetscape improvements. As the existing

road is situated close to the south limit of the right-of-way, the additional 3 metres in width is only required from the properties on the south side of Old Kingston Road.

In order to reduce the number of vehicular entrances along Old Kingston Road between Morrish Road and Military Trail, enhancing the pedestrian environment and allowing for more on-street parking opportunities, a new 7.5 metre public laneway is being proposed along the rear of the properties fronting the south side of Old Kingston Road between Morrish Road and Military Trail. This laneway is shown on the Highland Creek Community Secondary Plan – Highland Creek Village Plan and Tertiary Plan maps (see Attachment 6 - Schedules 4 and 5). Lands necessary to construct this laneway would be required to be conveyed to the City as a condition of redevelopment

### **Land Use**

The official plan amendment proposes to redesignate the lands at the southeast corner of Kingston Road and Lawson Road and the vacant lands located on the west side of Morrish Road, between Old Kingston Road and Kingston Road from 'Mixed Use Areas', to the 'Parks and Open Space Areas - Parks', in order to provide convenient and accessible park space within the Village in accordance with the Vision (Attachment 6 - Schedule 1).

The lands on the north side of Old Kingston Road immediately east-adjacent to the Morrish plaza, are currently used as a cemetery. These lands are proposed to be redesignated from 'Mixed Use Areas' to 'Parks and Open Space Areas – Other Open Space Areas' which permit specialized open space uses, such as cemeteries.

The lands surrounding the Highland Creek overpass can potentially be redeveloped at such time as the overpass is removed and Lawson Road / Highway 2A are connected at-grade. Should these lands be available for development in the future, it is important that a planning framework be in place to allow for future development. It is proposed that these lands be redesignated from 'Neighbourhoods' to 'Mixed Use Areas' (Attachment 6 - Schedule 1).

### **Highland Creek Village Development Framework**

In order to maintain and enhance the characteristics of the Village's main street (Old Kingston Road), allow for appropriate redevelopment of the vacant and underutilized lands south of Old Kingston Road, and provide direction on future development on the lands surrounding the Highland Creek overpass, the official plan amendment proposes a development framework specific to each of these three areas. A broader policy framework is also included to provide general development direction for the Village as a whole (Attachment 6 - Schedule 5 for area boundaries).

The Main Street Area policies establish low-rise building massing to create a comfortable, mixed-use pedestrian environment with ground floor commercial uses along Old Kingston Road. Grade related residential uses such as townhouses, are generally discouraged along Old Kingston Road so as to better develop a cohesive pedestrian environment. Enhanced landscaping, outdoor furnishings, decorative paving and



pedestrian scale lighting are encouraged in order to achieve a thematically consistent, high quality main street environment.

The South Village is intended to be a predominantly residential area. Policies in this area aim to provide pedestrian connections to the green space on Morrish Road and protect for views into the Highland Creek Ravine. A mix of building types, from townhouses to mid-rise buildings, will be encouraged. These buildings will transition in height towards the low-rise buildings along Old Kingston Road, and be stepped back or terraced to maximize sunlight into the green space on Morrish Road and the Highland Creek ravine.

The East Village is anticipated to be a future mix-use or residential area which will further support the overall vitality of the Village. It is contingent on the removal of the Highland Creek overpass. Developments in this area will take a mid-rise form similar to those in the South Village; however, given the proximity of these lands to Highway 2A, grade related units will be discouraged in the East Village. The policies directing development in the East Village will be expanded at such time as the Highland Creek overpass is contemplated for removal. The entirety of the lands within the East Village are currently owned by the City of Toronto.

The general development framework for the Village includes policies to promote pedestrian activity by encouraging publicly accessible walkways and courtyards throughout the Village. New automotive uses will not be permitted within the Village. Buildings will frame the streets by requiring parking and loading areas to be located either underground, or in the rear yard. Development will generally occur in accordance with the Highland Creek Village Urban Design Guidelines (Attachment 8).

### **Density and Building Height**

Providing additional residential use permissions within the Village is a key component to implementing the Vision. At this time, the Highland Creek Secondary Plan requires that any new residential development not exceed 37 units per hectare. This policy requires that even modest residential developments apply for an amendment to the official plan. This is inconsistent with the intent of the 'Mixed Use Areas' designation, as these areas are intended to accommodate new residential units as the City grows. It is recommended that this policy be deleted, and that it be replaced with a development framework to better guide new growth within the Village area.

In response to concerns raised by the community, the proposed maximum building heights are generally less than what could be achieved given the land area of these parcels, adjacent right-of-way widths, and adjacent land uses.

Site and Area Specific Policies have been added to certain parcels of land within the secondary plan to allow an increase in building height by way of a zoning by-law amendment application.

## **Parks**

The lands on the west side of Morrish Road between Old Kingston Road and Kingston Road are currently used occasionally for community functions, such as the Heritage Day celebrations. The vision requires that these lands continue to be maintained for outdoor use, and improved.

In order to achieve this, a Site and Area Specific Policy is proposed to ensure that a minimum of 2800 square metres, roughly 90% of the current area of land is retained by the City for the sole purpose of community amenity space. The remaining portion of the lands could be incorporated with adjacent lands to create more developable parcels. Planning staff propose that a minimum of 2800 square metres of land should remain within City ownership, and that operational management of these lands is transferred to the Parks, Forestry and Recreation Division. While they will no longer be responsible for operational management, Real Estate Services will facilitate any partial sale of the lands to an adjacent property owner.

Further, the policy recommends that the area beneath these lands may be used for underground parking and related structures, provided that all vehicular and pedestrian access is provided from adjacent lands and that no part of the parking structure is visible at grade.

## **Zoning by-law Amendment**

### **Land Use**

The proposed zoning by-law amendment (Attachment 7) contains detailed performance standards to regulate development within the study area.

The majority of the study area is currently zoned to permit only commercial, office and place of worship uses. In order to provide for mixed use development that is consistent with the vision and the 'Mixed Use Areas' policies of the official plan, staff recommend deleting the existing zoning in the Highland Creek Community Zoning By-law and replacing it with a Commercial-Residential (CR) Zone.

The proposed CR zone permits a range of commercial, institutional and residential uses, but will prohibit new automobile sales and service related uses, and low-density residential uses such as single-detached dwellings, semi-detached dwellings, and duplexes.

### **Commercial and Retail Uses**

In order to reinforce and encourage commercial uses at-grade along Old Kingston Road, commercial uses will be required to be provided at-grade on lands generally fronting Old Kingston Road, between Lawson Road and Military Trail, excepting those lands on the north side of Old Kingston Road between Military Trail and Morrish Road.

Automobile sales and service uses existing at the time of the passing of the proposed by-law amendment will remain as legal non-conforming uses. No new uses of this nature will be permitted within the study area. This would not prohibit the City from considering

a site-specific rezoning application to permit new automobile sales and service related uses, but simply prohibit such uses as-of-right, enabling the City to assess the site-specific proposal.

### **Residential Uses**

Townhouse dwellings, multiple-family dwellings, and apartment type dwelling units are expected to be the dominant form of development throughout the study area. Townhouse and multiple-family dwelling units will be permitted on all lands within the study area with exception to those lands within 20 metres (66 feet) of the Old Kingston Road and Highway 2A street lines. This will ensure that development along Old Kingston Road contributes to the pedestrian-oriented commercial environment detailed in the vision, and that new grade related residential uses are not constructed adjacent to Highway 2A.

Development of new single-detached dwellings, two-family dwellings, duplexes, and split-level dwellings is not permitted within the study area.

### **Places of Worship and Cemetery Lands**

The lands occupied by St. Joseph's Parish will continue to be zoned 'Institutional' (I), which contains a site specific exception to permit a place of worship.

The lands occupied by the Baitul Afiyat Mosque will be rezoned from Highway Commercial/Place of Worship (HC / PW) Zone to a Commercial-Residential (CR) Zone, but the use of a place of worship will continue to be permitted through a site specific exception.

The cemetery on the north side of Old Kingston Road between Lawson Road and Morrish Road will retain its current Institutional (I) zoning.

There are very few properties in the Village which could accommodate the parking required for a new place of worship. New places of worship will not be permitted on lands zoned Commercial-Residential (CR), with the exception of the Baitul Afiyat Mosque, as noted above. This restriction would not prohibit the City from considering a site-specific rezoning application to permit a place of worship but simply prohibits such a use as-of-right, enabling the City to analyze the impact of a site-specific proposal.

### **Parks**

Highland Creek Parkette, at the southeast corner of Kingston Road and Lawson Road is currently zoned Place of Worship (PW) and the City-owned parcel on the west side of Morrish Road, south of Old Kingston Road, is currently zoned Institutional Uses (I). To implement the Vision, both of these parcels will be zoned Park (P).

### **Building Height**

In order to implement the vision, minimum and maximum building heights are recommended which will establish a predominantly low scale mid-rise built form through the Village, with transition in heights from the Main Street (Old Kingston Road) to the edges of the Village while allowing for residential intensification opportunities.

The proposed zoning by-law amendment (Attachment 7) recommends a minimum building height of two storeys (7.5 metres) for all lands fronting onto Old Kingston Road, and a minimum building height of three storeys (10.5 metres) for all other lands within the study area.

In order to allow Old Kingston Road, the Main Street, to develop into a pedestrian oriented street, analogous of a small-town Ontario main street, the properties that front onto Old Kingston Road will range in height from three (3) storeys (10.5 metres) to four (4) storeys (13.5 metres). To promote transition to the established low rise residential areas to the north, the lands fronting onto the north side of the street will have a maximum height of three storeys (10.5 metres) and the properties on the south side of Old Kingston Road will generally have a maximum building height of four (4) storeys (13.5 metres).

In order to transition building height away from adjacent neighbourhoods and Old Kingston Road, maximum building height is proposed to increase incrementally towards the south and to the edges of the Village. These lands were identified by staff, the working group, and some members of the community, as lands which could accommodate more residential density, as they were at a distance from the Main Street area and the adjacent neighbourhood areas. Lands to the south of those properties fronting onto Old Kingston Road will have a maximum building height ranging from four (4) storeys (13.5 metres) to six (6) storeys (20 metres), whereas lands in the south eastern and south western most corner of the study area, abutting Kingston Road / Highway 2A will have a maximum building height of eight (8) storeys (27 metres).

While many residents are opposed to increased height and density, they are supportive of the removal of the automotive sales and service uses and would support streetscape improvements throughout the Village. If development at a realistic scale is not permitted within the Village, the lots upon which automobile sales and service uses currently exist may never redevelop, nor will any significant streetscape improvements occur in the near future. Not everyone may agree with increases in height and density, but allowing Highland Creek Village to maintain its current land use planning framework will greatly diminish the opportunity for streetscape, park, and other improvements identified in the Vision.

One of the goals of this study is to promote change and reinvestment within the Village, while maintaining its core values. In order to ensure that development occurs in a harmonious manner, over time, and according to the Vision established through this study, the heights being proposed are appropriate.

### **Density**

Currently, the Highland Creek Community Secondary Plan does not allow development to exceed 37 residential units per net hectare within the Village area. The proposed official plan amendment (Attachment 6) modifies Chapter 6, Subsection 1.4, to delete the maximum density of 37 units per hectare.

In order to maintain control of the massing and density of new developments, while allowing the Village to develop in a way which supports the Vision and implements the City's official plan, the proposed zoning by-law amendment contains density provisions. The level of density permitted on a site will be contingent on lot area, and will be generally consistent with the proposed maximum building heights. A maximum density of 2.0 times the lot area will be permitted along Old Kingston Road while a maximum density of 2.5 times the lot area will be permitted on lots closer to Highway 2A. In areas where there are larger and/or irregularly shaped lots and lower building heights, a gross floor area of 1.5 times the lot area will be permitted.

The level of density on a site within the Village will be contingent on lot area, lot configuration, the proposed maximum building heights, and the building layout and design. Maximum densities contained in the zoning bylaw are established as a general guide and should not be used as targets for maximizing developments on a particular site.

### **Minimum and Maximum Building Frontage**

A continuous wall of buildings defines a main street as an urban space. It was noted during the consultation with the working group that Old Kingston Road streetscape should reference design elements and massing typical of small town Ontario main streets. A strong street wall composed of 2, 3 and 4-storey mixed-use buildings is characteristic of these type of streets. In order to achieve these streetscape objectives within the Village, staff recommends that the main wall of the first 10.5 metres (3 storeys) of buildings abutting Kingston Road, Old Kingston Road, Highway 2A, Morrish Road, Military Trail, and/or Lawson Road, will have a minimum width of 70% of the lot at that street line for lots having frontage or flankage of more than 30 metres in width and a minimum of 60% of the lot at the street line having frontage or flankage of 30 metres or less.

For the two larger sites in the Village (1625 Military Trail and 6075-6109 Kingston Road) staff recommends that the main wall of the buildings abutting Military Trail and Lawson Road will have a minimum width of 60% and a maximum width of 70% of the lots at the street lines. This will limit the total building lengths and ensure that extra landscape breaks will be created in addition to the side yard setbacks.

### **Building Setbacks and Stepbacks**

Setbacks of existing buildings vary greatly within Highland Creek Village. The inconsistency of front yard setbacks throughout the Village results in a disjointed streetscape that does little to promote pedestrian activity or create usable front yard space. In order to create new storefronts in the Main Street Area, and enhance the pedestrian realm, minimum and maximum setbacks are proposed along all streets in the study area.

Minimum building setbacks of 0.0 metres and maximum building setbacks of 3.0 metres are generally proposed along Old Kingston Road to reinforce the pedestrian environment as new development occurs.

While non-residential uses are required at-grade along most of Old Kingston Road between Lawson Road and Military Trail, in specific areas where residential uses are permitted to be located at-grade, minimum building setbacks of 3.0 metres and maximum building setbacks of 5.0 metres are proposed to provide landscaping and a landscaped transition area from the public realm, but not allow for individual driveway access for units fronting onto Old Kingston Road.

Within the setback area, elements such as porches and steps, landscaping, decorative paving, and low decorative fences are encouraged to create a pedestrian friendly street. Further streetscape features will be reviewed and evaluated through the site plan approval process.

Buildings above the third storey (10.5 metres) will be required to step back a minimum of an additional 1.5 metres from the main front wall. Buildings with heights between 6 and 8 storeys will be required to stepback an additional 1.5 metres at either the seventh or eighth storey (24 and 27 metres).

Balconies will not be permitted to encroach into building setbacks at, or below, the 3<sup>rd</sup> storey and continuous balconies will be prohibited.

To mitigate potential privacy impacts on adjacent 'Neighbourhoods' and provide guidance on overall building heights, the application of a 45-degree angular plane taken from the property lines which abut Single-Family Residential (S) or Multiple-Family Residential (M) zones is recommended.

Blank side walls will not be permitted above the fourth storey of any new buildings.

Mechanical penthouses will be setback a minimum of 5.0 metres in addition to the required setbacks from all street yards. The mechanical penthouse shall cover no more than 30% of the roof area and extend no more than 5.0 metres above the roof deck.

A minimum 7.5 metre (25 feet) setback is proposed for all properties where they abut Single-Family Residential (S) or Multiple-Family Residential (M) zones.

Due to the unique configuration of lots within the study area, rear yard setbacks on lots which do not abut Single-Family Residential (S) and Multiple-Family (M) Zones will be subject to lesser setbacks. In some cases, requiring rear yard setbacks of 7.5 metres or greater would render some sites undevelopable.

In order to promote a continuous street wall, no side yard setback will be required for buildings up to four (4) storeys (13.5 metres) in height which front onto Old Kingston Road.

To promote a more residential character and encourage landscaped breaks along Military Trail, Morrish Road, Kingston Road, Lawson Road, and Highway 2A, side yard setbacks

ranging from 0 metres to 7.5 metres are required, depending on the locations and configurations of the lots.

The City may seek midblock pedestrian connections along the south side of Old Kingston Road and along the east side of Military Trail to enhance the pedestrian environment within the Village, provide convenient access to the park space on Morrish Road, and further break up the street wall. A 7.5 metre side yard setback is being proposed along 1624X Military Trail, abutting 1622 Military Trail, to allow for a walkway into the green space fronting Morrish Road. Any pedestrian connections from the south side of Old Kingston Road to the green space on Morrish Road will be determined through subsequent development applications.

### **Separation Distances Between Buildings on the Same Lot**

The study area contains two large parcels of land, 1625 Military Trail, and 6075-6109 Kingston Road which possess significant development potential. While these parcels are large, installation of new public streets is unlikely. As no new public streets are likely, sufficient separation distances between buildings developed on these parcels will help to create a pedestrian friendly environment within the sites. Adequate separation distances will allow for driveways to be designed as private streets that look and function like public streets. They will also allow for features such as courtyards and walkways/passageways with good sun exposure, adequate landscaping, and pedestrian amenities. In the case of 1625 Military Trail this separation distance would also allow for views into the ravine.

Separation distances between buildings where principal windows face each other, the minimum above grade distance between the walls of each building must be 20 metres (66 feet) and where a building has windows facing a wall which does not have a window the above grade distance between the buildings must be a minimum of 11 metres (36 feet).

### **Holding Provision**

Lands within the study area which may be required to implement the road network changes detailed further in this report will be subject to an 'H' holding provision. This holding provision requires landowners to dedicate the needed lands to the City for the design, construction and financing of public lanes/streets prior to any new development proceeding.

### **Minimum Ground Floor Height**

In order to help reinforce the main street character along Old Kingston Road and provide the minimum industry standard for new commercial units, a minimum ground floor-to-floor height of 4.5 metres (15 feet) is recommended for that portion of all buildings located within 20 metres (66 feet) of the Old Kingston Road street line.

### **Section 37 Benefits**

Section 37 of the Planning Act allows for community benefits in exchange for the ability of a development to be built higher, or have a greater density, than the maximum height and/or density permitted in the zoning by-law. As part of this study, staff recommend

that where buildings are proposed to have a height greater than 4 storeys or 13.5 metres, the landowner will be required to enter into a Section 37 agreement with the City prior to obtaining an increase in height and/or density.

The provision of community benefits in conjunction with these larger developments will assist in addressing the additional demand on local community resources, maintaining the quality of life in the larger area, and implementing official plan objectives related to balanced and well-managed growth.

Should a development application be submitted for a building greater than 4 storeys in height within the study area, staff may seek to secure one or more of the following benefits:

- public art;
- dedicated and accessible community service space for non-profit service agencies to deliver programs/services for various user groups, particularly for youth and seniors;
- high quality and accessible licensed child care facilities, particularly for infants and toddlers;
- additional satellite recreation space including large flexible multi-purpose rooms to run programs for all age groups, particularly for youth and seniors;
- other non-profit arts, cultural, community or institutional facilities;
- streetscape improvements not abutting the subject property/site;
- purpose-built rental housing with mid-range or affordable rents, land for affordable housing, or, at the discretion of the owner, cash-in-lieu of affordable rental units or land;
- park improvements for lands within the Village;
- municipally-owned and operated parking lots; and
- funding for approved capital projects in the vicinity of the study area.

## **Urban Design Guidelines**

The Highland Creek Village Urban Design Guidelines and Concept Plan (Attachment 8) are intended to provide guidance on the design of the built environment within the Village.

In 2002, City staff, in consultation with the Ward Councillor and the residents of Highland Creek, undertook a review of the built characteristics of the Highland Creek Village Area. The intent of the exercise was, not unlike this study, to direct the built form and design characteristics of new developments within the Village. The result of this exercise was the 2002 Highland Creek Village Design Guidelines.

While the existing urban design guidelines contain useful direction and concepts for the future development of the Village, they have been amended to better conform to the Vision developed through this area study and provide a more comprehensive direction for new development within the Village.



The updated guidelines provide detailed guidance to achieve key urban design objectives of the study including the following:

- maintain the historic character of the Village by encouraging improvements to the existing historic landmarks in the Village, and by promoting compatible developments;
- create a vibrant Main Street by enhancing the traditional main street character along Old Kingston Road;
- create a green residential area, behind the Main Street, centred on the Village Green, and with access and views towards the Highland Creek Ravine;
- provide general directions on the potential future mid-rise developments in the East Village;
- transform the City-owned open space on Morrish Road to create the Highland Creek Village Green, the focal point of the Village and gathering place for the residents;
- recommend public art installations that reflect the Village's history and identify potential public art locations;
- establish a well connected and fine grained pedestrian network throughout the Village;
- promote extensive landscaping in private developments;
- promote pedestrian scale developments through measures such as step backs, adequate separation distances, side yard setbacks, and multiple individual entrances at grade where appropriate; and
- promote high quality of design; in particular, recommend building exterior materials that reflect the Village's built heritage.

## **Parks and Open Space**

The Highland Creek Village is currently served by the Highland Creek Parkette, a small park on the southeast corner of Kingston Road and Lawson Road, and a City-owned parcel of land situated on the west side of Morrish Road, south of Old Kingston Road, which serves as a green space for community events. Colonel Danforth Park is the largest park in the vicinity and is situated just west of the study area.

The Official Plan Map 8B (Local Parkland Provision Levels), indicates that the Highland Creek Community is generally well served by parkland, with 1.57 – 2.99 hectares of parkland per 1,000 people. This falls within the second highest quintile of local parkland provision.

## **Morrish Road Green Space**

During the course of the study, participants identified a desire for a centrally located park within the Village that could be used as a community gathering space and accommodate other types of passive outdoor recreation activities. The City-owned parcel on the west side of Morrish Road, south of Old Kingston Road, could address the community's need for such park space. These lands are, however, currently under the operational management of Toronto Real Estate Services.

In order to ensure that adequate open space is retained in the Village, staff are recommending that this parcel of land be redesignated 'Parks and Open Space Areas - Parks' in the official plan. In addition, a Site and Area Specific Policy has been added that would allow for the sale of part of the lands for amalgamation with an adjacent property, provided a minimum of 2800 square metres of the land are retained by the City for park purposes. This would allow the City to provide a suitable size park for area residents while allowing the neighbouring owners to create more developable parcels of land, thereby ensuring intensification can still occur.

To ensure the maintenance and improvement of this park space, it is recommended that the Parks, Forestry and Recreation Division assume operational management of these lands and explore funding opportunities to improve the space. To facilitate the sale of part of the lands, if necessary, Real Estate Services would assist with this process.

There are a number of City documents which refer to this park space by several different names, most commonly, 'Unnamed Park #44' or 'Morrish Road Park'. It is recommended that Parks, Forestry and Recreation staff, in consultation with the Ward Councillor, report back to Community Council to name these lands.

### **Parkland Contributions**

As the study area is designated 'Mixed Use Areas' in the official plan with potential for residential growth, properties within this area are subject to the alternative parkland dedication policy. A larger dedication of land (greater than 5% of land, or cash-in-lieu) for parks purposes will be required for new residential development or the residential portion of mixed use development. The alternative parks levy above the first 5%, arising from developments in the Highland Creek Village study area should be directed towards parks or other recreational purposes in the Highland Creek Community within, or in close proximity to, the study area.

### **Servicing**

Technical Services is not aware of sewer capacity issues within the study area; however, applicants will be required to submit functional servicing reports for storm, sanitary and water to demonstrate that there is sufficient capacity to service any proposed developments. New developments will also be required to meet the Wet Weather Flow Management Guidelines.

### **Road Network Improvements**

#### **Public Transportation**

The Village area is served by TTC bus transit. The Number 38 bus connects the Village area to the Rouge Hill Go Transit station to the east, and Scarborough Civic Centre, to the West, while the Numbers 86 A / B buses run along Kingston Road to the southwest and north along Meadowvale Road northeast.

The nearest GO Transit station is the Rouge Hill station at Port Union Road and Lawrence Avenue East.

Permanent transit infrastructure (eg. Light Rail Transit, Dedicated Bus Lanes) is not contemplated within the study area by the City or the Province at this time, however the TTC is circulated on applications for development within the area and will adjust service as the population in the area increases.

### **Pedestrian Environment**

Promoting and enhancing the pedestrian realm can greatly, and positively, transform the character and usability of a public space. An attractive, multi-purpose, public realm creates pride in identity and a sense of stewardship within the community.

The pedestrian environment in Highland Creek Village has not developed over time in a manner that best provides for pedestrian activity. Several streets lack sidewalks, while others have dead end walkways. The frequency of vacant or underutilized parcels and automotive uses further detracts from the pedestrian environment in the Village.

Study participants noted that the Village is in need of streetscape improvements, especially along Morrish Road and Old Kingston Road, the intersection of which was noted as the 'Centre of the Village'. It is around this intersection that the majority of pedestrian activity is anticipated. To improve the pedestrian environment, both aesthetically and functionally, the following initiatives are recommended:

- acquiring 3.0 metres of land as development proceeds through road-widenings along the south side of Old Kingston Road to allow for adequate space to install sidewalks, streetlights, planting and pedestrian furniture;
- converting the existing angled parking on the north side of Old Kingston Road, east of Morrish Road, to standard parallel parking spaces which will allow for a safer, and wider pedestrian environment along the north side of Old Kingston Road;
- installing new planting and street trees along all streets within the study area to visually enhance the area, and separate pedestrians from traffic;
- providing appropriately lined parallel parking on both sides of Morrish Road will further buffer pedestrians from vehicular traffic;
- installing curb cuts at appropriate locations;
- requiring minimum 1.7 metre (5.5 feet) walkways on all public and private lands throughout the Village;
- assessing opportunities for mid-block pedestrian connections as development occurs;
- installing decorative street lights, especially along Old Kingston Road; and,
- installing new street furniture, including benches, way-finding signs, street signs and waste receptacles.

### **Parking**

Generally, new parking will be provided within below-grade structures or in the rear yard of new developments. On-street parking is recommended to be provided on all proposed realigned public streets where feasible.

Parking requirements for new residential and commercial uses will be generally consistent with the parking regulations contained in former City of Toronto Zoning By-

law No. 1156-2010. Bicycle parking will also be required in accordance with the official plan policies that promote bicycle use and the Toronto Green Standards.

A lack of convenient parking was noted as a concern during the public consultation of this study. The Toronto Parking Authority (TPA) completed a parking study of the Village area in 2002 and again, in response to this study, in 2011. In both instances, no notable shortage of parking was identified.

At this time, the TPA is not anticipating acquiring lands for a pay parking lot, or installing parking meters in the vicinity of the Village. The TPA may re-evaluate their position as the Village develops.

In order to address the parking concerns of the residents, the potential road changes noted above would provide for additional on-street parking opportunities within the Village.

### **Bicycle Lanes**

Cycling is an environmentally responsible way to travel around and experience the City while maintaining personal fitness. The official plan and the City of Toronto Bike Plan both contain directions to promote the use of bicycles as a mode of personal transport throughout the City, as well as practices to make cycling safer for both cyclists and motorists sharing the roadways.

The City of Toronto Bike Plan identifies Old Kingston Road and Lawson Road as part of the City's on-street bicycle network, however, staff recommends that the location and configuration of any bicycle lanes be further considered as part of the Transportation EA.

### **Road Network**

The roads within the study area do not follow a grid pattern common to most of the rest of the City of Toronto. This results in a number of irregularly shaped parcels and awkward intersections which are not ideal for safe vehicular or pedestrian movements. The road network has been subject to two previous transportation studies in 1986 and 1995.

In 1985, the City of Toronto retained Marshal Macklin Monaghan (MMM) Limited to complete a Study of Transportation in the Highland Creek area. The study was completed in May 1986. The purpose of the study was to identify the needs of the transportation system within Highland Creek. The study commented on the connection of Meadowvale Road to the 401, easterly improvements to Highway 401, and the road network within Highland Creek Village.

Among recommendations related to access at Meadowvale Road and Highway 401, the study recommended the installation of a full-movement, signalized intersection at Meadowvale Road and Highway 2A. The study also recommended the replacement of the Highland Creek overpass at Highway 2A and Lawson Road with an at-grade intersection, and suggested other minor improvements to the roads within the Village.

Scarborough City Council received the MMM study final report in 1986 and referred all recommended improvements to Technical Services for further review.

In 1995, Totten Sims Hubicki (TSH) Associates was retained by the City to update the 1986 MMM study, assess the current and future traffic demands for the Village area, assess options to improve traffic circulation, and provide an implementation strategy for any necessary improvements.

The TSH study noted traffic circulation problems within the Village in their final report and recommended a number of options to address these and other traffic concerns, including:

- reconfiguration of the Old Kingston Road and Kingston Road intersection;
- elimination of access to Highway 2A from Kingston Road and Morrish Road;
- addition of a westbound access to Highway 2A from Lawson Road;
- provision of access at Meadowvale Road and Highway 2A; and,
- installation of a 'T' intersection at Highway 2A and Military Trail.

Through this study, staff identified a number of improvements to the road network which should enhance the pedestrian realm, allow for more on-street parking opportunities, and improve traffic flow in, and around, the Village. Prior to any of the road improvements being undertaken, staff are recommending that a Transportation Environmental Assessment (EA) be initiated. The transportation EA would examine the proposed changes identified below, and others, to determine the most appropriate solutions to improve traffic patterns in and around the Village. The potential for a realigned road network will result in more developable parcels of land and may result in increased opportunities for on-street parking. The EA process includes a public comment component which would include community consultation meetings. Upon completion of the EA, staff will report to Community Council on the findings of the study and make recommendations regarding the road network.

The following changes to the road network, among others, are recommended to be studied through a subsequent transportation EA:

- connect Meadowvale Road to Highway 2A with, at a minimum, a westbound right-in access for vehicles travelling south. Option for a full movement, signalized intersection should also be considered;
- close access to Highway 2A from Kingston Road and Morrish Road and connect both roads in a loop;
- install a T-intersection at Kingston Road and Old Kingston Road;
- install a T-intersection at Military Trail and Kingston Road;
- remove the Lawson overpass and install a signalized intersection at Highway 2A / Lawson Road. Remove on/off ramps associated with the overpass. Reduce right-of-way (ROW) to be consistent with an Avenue ROW further to the west; and,
- explore the opportunity for bicycle lanes within the Village.

Potential property impacts associated with these road improvements are not yet known, but will be identified through the EA study process.

In order to improve the pedestrian realm on the south side of Old Kingston Road, an increase in ROW is being proposed. Typically, a right-of-way increase or decrease would require a supporting transportation rationale, however, the basis for this increased ROW is only to accommodate pedestrian improvements. No changes to the pavement width or road configuration itself are contemplated by this increase. Planning staff, Transportation Services, and Transportation Planning staff are recommending this right-of-way increase through the proposed official plan amendment (Attachment 6).

In order to secure for the orderly development of parcels along the south side of Old Kingston Road between Military Trail and Morrish Road, a 7.5 metre (24 feet) public laneway is being proposed at the rear of these lands. This laneway would provide access for vehicles, and ensure that delivery and garbage collection do not occur on Old Kingston Road.

The proposed land use changes detailed in this report can be accommodated by the existing road network. It should be noted that applicants for development within the village may be required to submit a transportation impact study and/or traffic operations assessment in conjunction with their application. The need for localized road and access improvements may be identified through the review of such studies/analyses.

## **Community Services and Facilities**

Community services and facilities (CS&F) reviews are undertaken as part of the building of healthy communities to ensure that local residents have access to a full range of locally-based services and facilities. These reviews are also undertaken in areas that are experiencing major or incremental growth or within established neighbourhoods that are under served (i.e. priority neighbourhoods) and/or going through significant demographic changes. The City's official plan provides a framework to ensure that when considering new growth, the overall quality of city living must be assessed. One area that the Plan requires review and consideration is the adequacy of the CS&F infrastructure.

A CS&F report is attached to this report (Attachment 5) and provides a review of community services and facilities that are available to residents living in and around the Highland Creek Village area. The purpose of the review is to identify the range and adequacy of available CS&F in order to address existing gaps and emerging priorities when planning for new growth. Key resources include publicly funded schools, child care facilities, libraries, community centres, swimming pools, arenas and parks and open space. The review also covers human service agencies (e.g. seniors' support, food banks) that offer support locally.

The CS&F Review completed for the Highland Creek Village area highlights a mature and aging population with a large proportion of older youth and seniors that make up the community. Given increased development proposed within the commercial village area,

it is timely that a new vision for this area incorporates an appropriate planning framework to guide such development. The anticipated development will result in a new resident population which will increase demands for additional community services and facilities. A number of key findings have been identified as part of the CS&F review for the Highland Creek Village Study. These findings highlight the needs of the residential population that currently lives in the study area. Based on an analysis of these findings, CS&F priorities have been developed for the area as follows:

- Community Agency Space - dedicated and accessible community service space for non-profit service agencies to deliver programs/services for various user groups, particularly for youth and seniors (minimum 5,000 square feet);
- Child Care - high quality and accessible licensed child care facilities, particularly for infants and toddlers (62-72 space child care); and,
- Community Recreation – additional satellite recreation space including large flexible multi-purpose rooms to run programs for all age groups, particularly for youth and seniors.

Findings from the Highland Creek Village CS&F Review will assist in ensuring that key elements including identification of appropriate Section 37 community benefits will be considered as part of future development proposed for this area.

### **Toronto Green Development Standard**

New development within the study area will be encouraged to incorporate sustainable features in accordance with the Toronto Green Standard adopted by Council in July 2006.

### **CONCLUSION**

Highland Creek Village presents many opportunities for redevelopment and investment. It is important that this redevelopment occur in a form which respects the characteristics and qualities residents have come to identify as the being unique to the Village, while still allowing it to grow and contribute to the revitalization of the Village area and implement Provincial and city policy to accommodate growth.

The recommendations of this study have been developed in response to community input and having regard for the existing provincial and city policy framework, and sound planning principles.

Cohesive policy direction is crucial to the successful implementation of the vision for Highland Creek Village. The proposed official plan amendment (Attachment 6), zoning by-law amendment (Attachment 7) and revised urban design guidelines (Attachment 8) provide a comprehensive and consistent development framework in Highland Creek Village.

## **CONTACT**

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Fax No. 416-392-1330

E-mail: [guens@toronto.ca](mailto:guens@toronto.ca)

## **SIGNATURE**

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Raymond David, Director  
Community Planning, Scarborough District

## **ATTACHMENTS**

Attachment 1: Study Area

Attachment 2: Current Official Plan Map 22 and 23 Excerpt

Attachment 3: Current Zoning By-law Schedule

Attachment 4: Development Applications in the Study Area

Attachment 5: Guiding Principles

Attachment 6: Draft Official Plan Amendment

Attachment 7: Draft Highland Creek Community Zoning By-law Amendment

Attachment 8: Draft Highland Creek Village Urban Design Guideline Changes

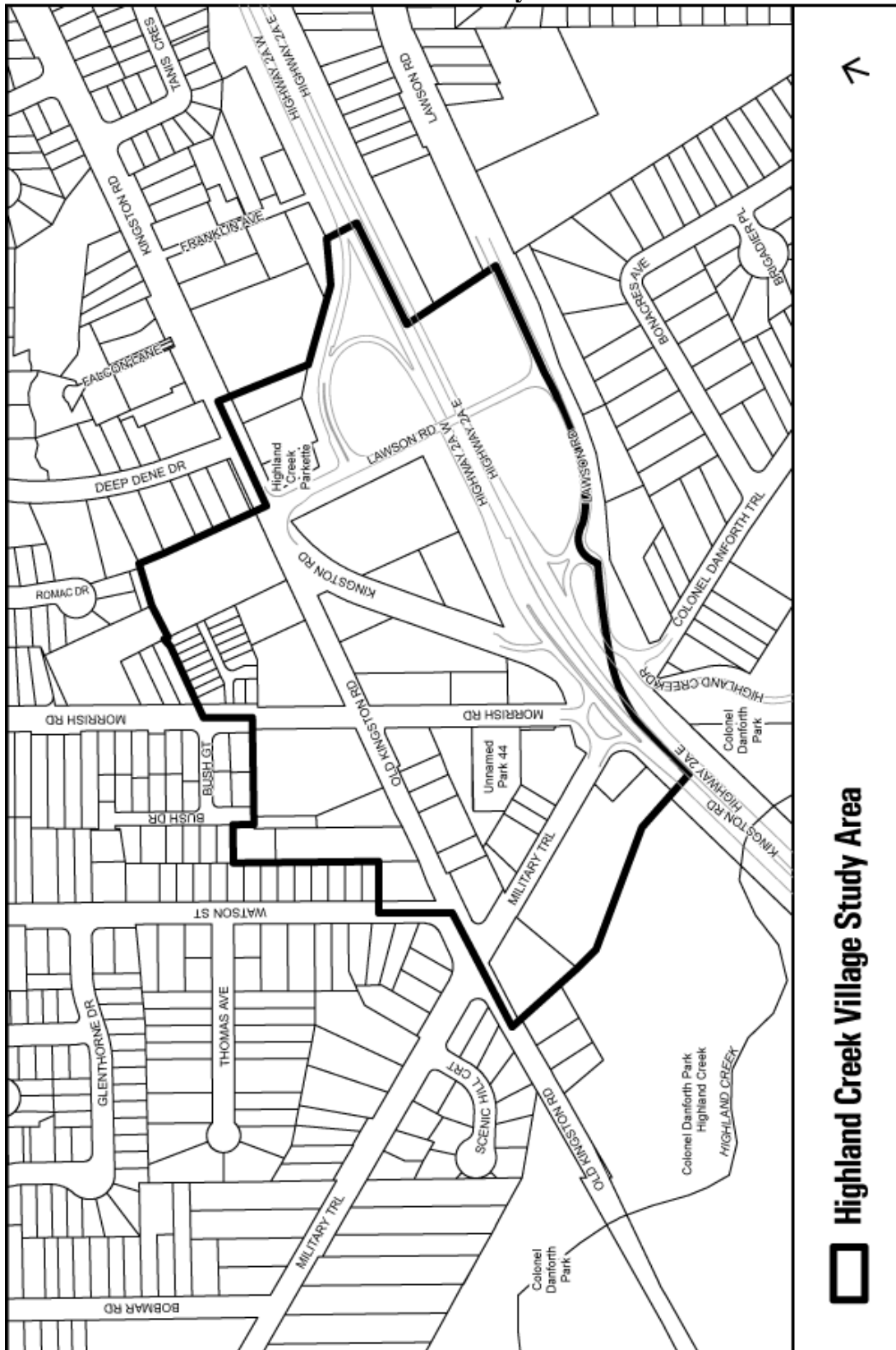
Attachment 9: Community Services and Facilities Review

Attachment 10: Minimum and Maximum Building Height Map

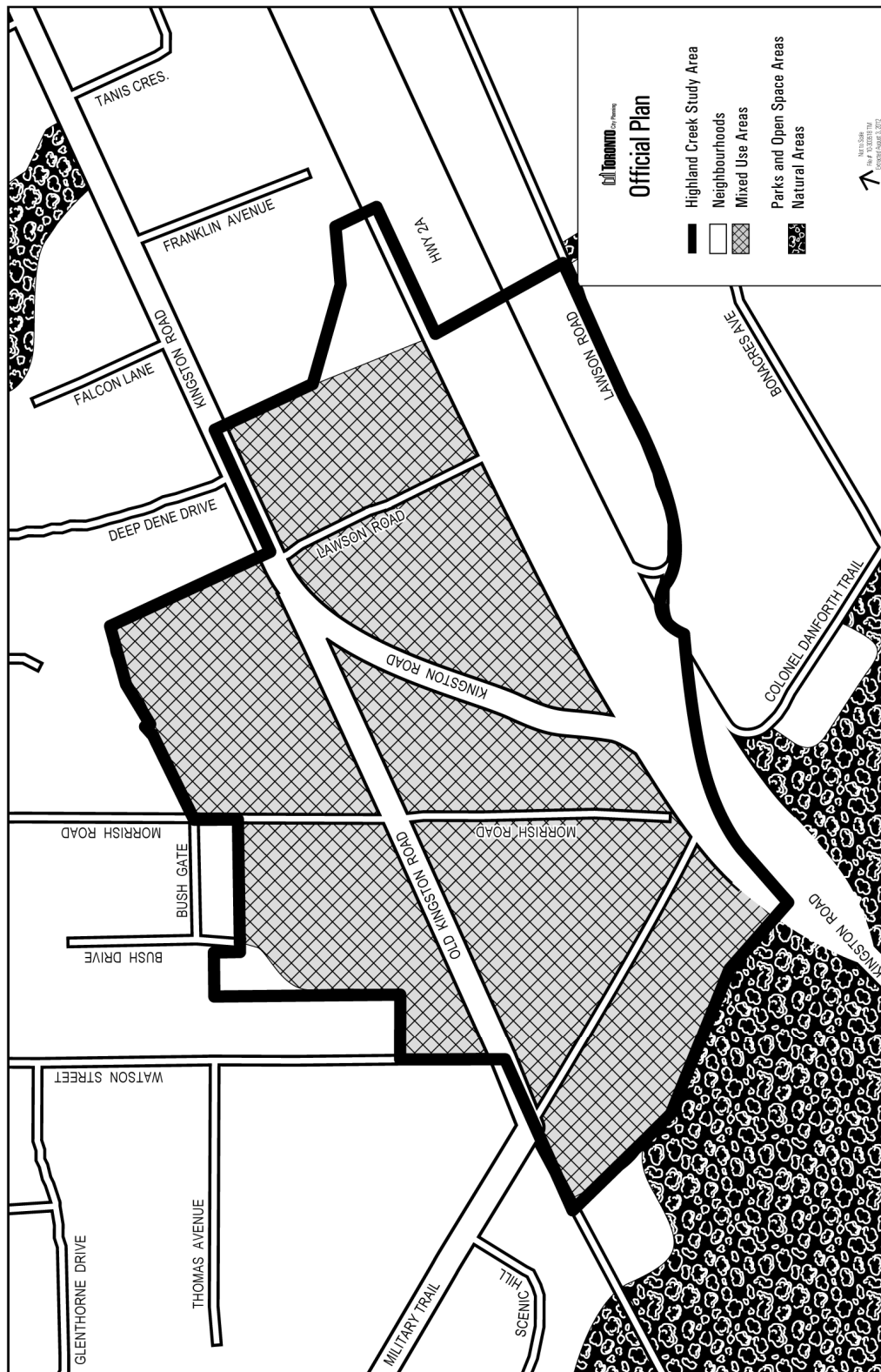
Attachment 11: Proposed Concept Road Changes



## Attachment 1: Study Area



## Attachment 2: Current Official Plan Map 22 and 23 Excerpt

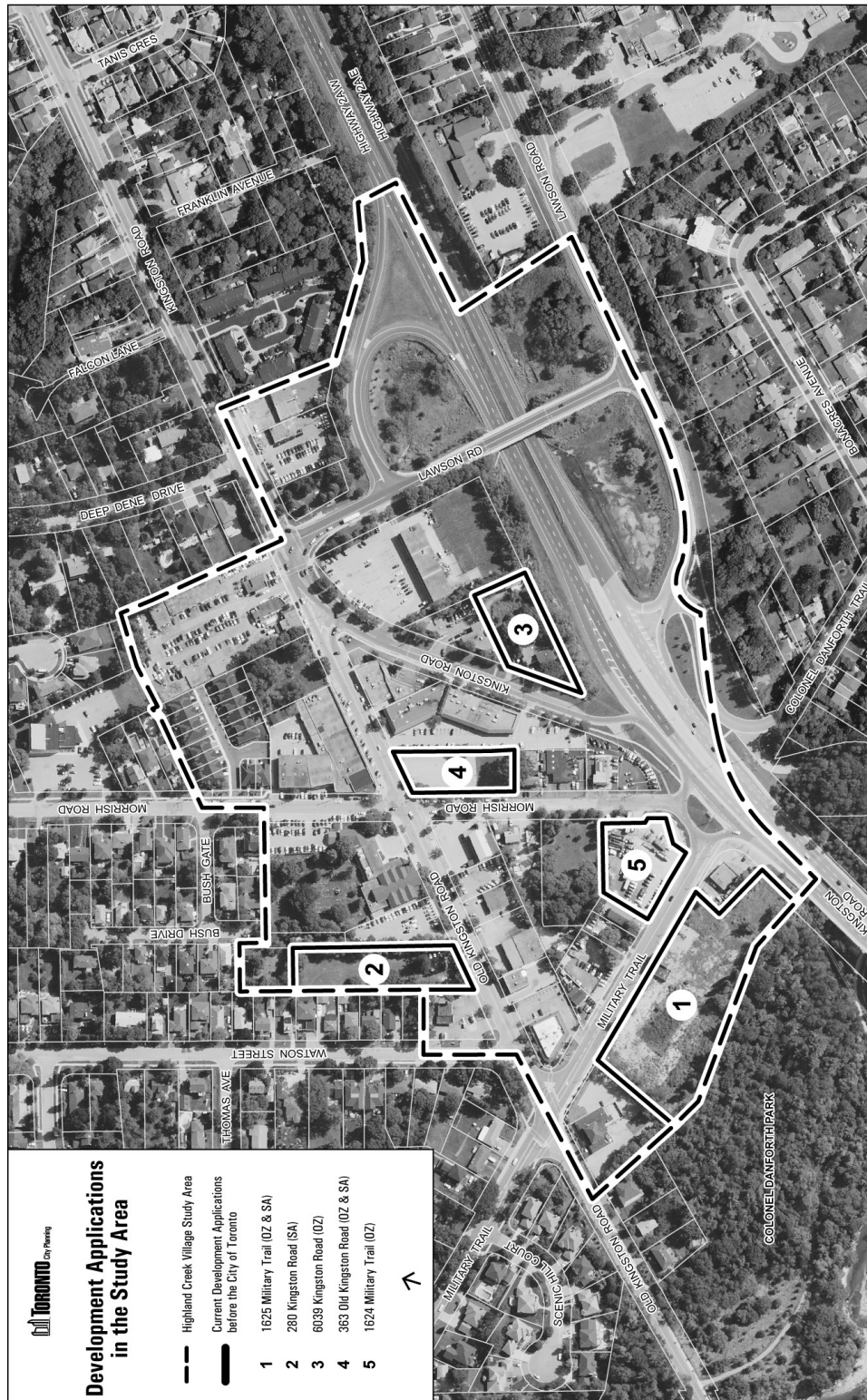


**Toronto**

Highland Creek Study Area

- S Single Family Residential
- M Multiple Family Residential
- I Institutional
- PW Place(s) of Worship
- CC Community Commercial
- O Major Open Spaces
- HC Highway Commercial
- AG Agricultural Uses
- Hwy Highway

## Attachment 4: Development Applications in the Study Area



## **Attachment 5: Guiding Principles**

City Planning Staff, in consultation with the Working Group, developed the following set of five principles to inform the direction and conclusions of the Study.

- **Transformation**

**Create a vibrant community that attracts new businesses. Promote key sites that will enable positive change and drive re-investment in the area.**

The Highland Creek Village identity will be promoted and refined with high quality urban design throughout the Village area that reinforces the character of the Village and promotes a mix of uses that expands population and creates diverse employment opportunities. Possible reconfigurations of the road network and way-finding elements shall assist in establishing stronger relationships and connections within the built environment. Automotive uses will be phased out, and transform over time into more compatible uses that promote and enhance the Village community.

- **Community Identity**

**Reflect and refine the character of the Village.**

The distinct Village character will be promoted by gateway features, public art, common light fixtures, and common architectural elements, while borrowing from past historical built elements to pay tribute of the Village's contextual heritage value. Opportunities will be explored to enhance connectivity to the open space network to the west of the Village and improve or reconfigure the existing green spaces in the village.

- **Movement**

**Promote the use of transit, cycling and walking as viable modes of transportation to access and explore the Village area. Encourage new on-street and off-street parking opportunities.**

Movement and connectivity will be enhanced within the Village area and into adjacent key areas, such as Highway 401 and the University campus to draw more visitors into the Village. Streetscape design will focus on an improved pedestrian and cycling environment, safe parking options, and traffic calming measures.

- **Safety and Accessibility**  
**Foster comfort and safety and accommodate people of all abilities and ages within the physical environment.**

By implementing Crime Prevention Through Environmental Design (CPTED) principles, through the construction and installation of appropriate lighting, signage, and accessible design elements, safety and accessibility will be improved and enhanced within the Village and surrounding area.

- **Development as a Good Neighbour**  
**Further define Highland Creek Village through compatible development and protection of the adjacent, stable, low-rise residential areas.**

New buildings will be appropriately massed and scaled, responding to the community's desire to create a high quality pedestrian environment while allowing for densities and heights that enable attractive developments that strengthen the community character. New development will provide for a range of housing options within the Village area, ensuring that detached homes continue to define the surrounding low-density area.

## **Attachment 6: Draft Official Plan Amendment**

Authority: Scarborough Community Council Item ~ [or Report No.~, Clause No.~] as adopted by City Council on ~, 20~

Enacted by Council: ~, 2012

### **CITY OF TORONTO**

#### **BY-LAW No.**

**To adopt Amendment No.187 to the Official Plan for the City of Toronto with respect to 255 Old Kingston Road, 1625 Military Trail, 6000, 6115, 6143-6147, 6149, 6096, and 6098 Kingston Road, all lands on the north side of Old Kingston Road and Kingston Road between Lawson Road and Watson Street, all lands south of Old Kingston Road and north of Kingston Road and Highway 2A between Lawson Road and Military Trail, and all lands comprising the Highway 2A right-of-way bordered by Lawson Road to the south, Military Trail to the west, and Franklin Avenue and 56 Lawson Road to the east.**

WHEREAS authority is given to Council under the *Planning Act*, R.S.O. 1990, c.P. 13, as amended, to pass this By-law; and

WHEREAS Council of the City of Toronto has provided adequate information to the public and has held at least one public meeting in accordance with the *Planning Act*;

The Council of the City of Toronto HEREBY ENACTS as follows:

1. The attached Amendment No. 187 to the Official Plan is hereby adopted pursuant to the *Planning Act*, R.S.O. 1990, c.P. 13, as amended.

ENACTED AND PASSED this   th day of   , A.D. 2012.

FRANCES NUNZIATA,  
Speaker

ULLI WATKISS  
City Clerk

**AMENDMENT No. 187 TO THE OFFICIAL PLAN  
OF THE CITY OF TORONTO**

The following text and schedule constitute Amendment No. 187 to the Official Plan for the City of Toronto.

**OFFICIAL PLAN AMENDMENT**

The Official Plan of the City of Toronto is amended as follows:

1. Map 3, Right-of-way Widths Associated with Existing Major Streets, is amended by re-designating Old Kingston Road between Military Trail and Kingston Road from a 20 metre right-of-way to a 23 metre right-of-way as shown on the attached Schedule 2.
2. Maps 22 and 23, Land Use Plan, are amended by re-designating the lands from 'Mixed Use Areas' to 'Parks and Open Space Areas – Other Open Space Areas' and 'Parks and Open Space Areas – Parks', and by re-designating the lands from 'Neighbourhoods' to 'Mixed Use Areas' as shown on the attached Schedule 1.
3. Map 35 - Secondary Plan Key Map is amended by adding lands to the Highland Creek Community Secondary Plan on the south side of Highway 2A as shown on the attached Schedule 6.
4. Chapter 6, Section 2, Highland Creek Community Secondary Plan, is amended by deleting sub-section 1.1 and replacing it with the following:
  - 1.1 The Highland Creek Community encompasses residential areas characterized by detached dwellings on spacious, treed lots. The preservation of this character within the residential areas of the Community will be the principal criterion in evaluating development proposals. Residential infill and redevelopment will be compatible with the existing residential character. The Highland Creek Community also contains Highland Creek Village, which will become a vibrant, mixed-use, community-focused, pedestrian-friendly area, where attractive building design references the area's heritage, respects the Village character and that of the surrounding established neighbourhoods, and a range of building types will be encouraged to attract new businesses and residents to live and work in the community.
5. Chapter 6, Section 2, Highland Creek Community Secondary Plan, is amended by deleting sub-section 1.4 and replacing it with the following:



- 1.4 Highland Creek Village, the core area of the Highland Creek Community, is located in the vicinity of Old Kingston Road, Military Trail, Kingston Road, and Lawson Road shown as Highland Creek Village on Map 2-2.

1.4.1 General Policies for Highland Creek Village

The major objectives for the Highland Creek Village are as follows:

- (a) building design and massing will create a comfortable pedestrian environment and transition to protect adjacent neighbourhoods;
- (b) building design and public realm elements, such as enhanced landscaping, public art, decorative paving, and attractive lighting, will strengthen the community identity;
- (c) building stepbacks and setbacks will be used to mitigate the impact of building height and massing and to maintain appropriate relationships of buildings to the street and public open spaces;
- (d) the reurbanization of the Village will be achieved by providing a variety of commercial and residential uses while excluding any new automobile sales and service uses;
- (e) a variety of housing opportunities will be encouraged to accommodate a range of household sizes and incomes, excluding house forms such as single detached, semi detached and duplex dwellings;
- (f) construction of new buildings will be strongly encouraged to incorporate elements of sustainability and environmental responsibility regarding building materials, internal infrastructure, outdoor spaces, green roofs, and opportunities for renewable energy.
- (g) opportunities for on-street parking will be provided wherever appropriate to provide a buffer between vehicular traffic and pedestrian activity;
- (h) the pedestrian environment will be further enhanced by locating parking, vehicular access and loading areas in rear or side yards where possible;

- (i) consolidation of lands will be encouraged to facilitate more comprehensive developments on larger parcels;
- (j) publicly accessible private courtyards and walkway connections will provide attractive and accessible pedestrian connections throughout the Village;
- (k) the character of the Village will be further enhanced through the preservation of viable mature trees, and enhanced landscaping on both public and private lands; and
- (l) built form and design of new development, green spaces, and pedestrian connections will occur generally in accordance with the Highland Creek Village Urban Design Guidelines.

1.4.2 Highland Creek Village is further separated into three distinct areas. Each area will develop in a way that addresses the needs of the community over time, provides transition from the adjacent low-rise residential neighbourhoods, and contributes to the form of the Village as a whole. Development in these areas, as shown on Map 2-2, will proceed in accordance with the following:

(1) The Main Street

Old Kingston Road, the Village's 'Main Street', will develop as a lively, pedestrian oriented, low-rise, mixed-use area based on the following criteria:

- (i) buildings will range between 2 storeys and 4 storeys on properties with frontage on the north side of Old Kingston Road, and between 3 and 4 storeys on properties with frontage on the south side of Old Kingston Road;
- (ii) a vibrant, pedestrian oriented Main Street will be developed with, commercial and institutional uses required on the ground floor of developments on the south side of Old Kingston Road between Lawson Road and Military Trail, and on the north side of Old Kingston Road between Morrish Road and Lawson Road;
- (iii) landscaping, outdoor furnishings, decorative paving and pedestrian scale lighting will be coordinated in order to achieve a consistent character and high quality environment; and,

- (iv) to increase opportunity for on-street parking on Old Kingston Road, vehicular accesses to properties will be minimized, located on side streets, or accessed via a public laneway where possible.

(2) The South Village

The South Village will be a predominantly residential area. Residents in the area will have views and accesses to the green spaces in the Village, including the Village Green on Morrish Road, the Highland Creek Parkette and the Highland Creek Ravine. Lands in the South Village will develop based on the following criteria:

- (i) development occurring on the west side of Military Trail will provide adequate view corridors into the ravine;
- (ii) to allow for variation in design and transition in scale, maximum building heights will vary in the South Village between 4 and 8 storeys, as defined on a site-by-site basis in the Zoning By-law. Minimum building heights will be 3 storeys

(3) The East Village

The East Village is anticipated to be a future mix-use or residential area which will further support the overall vitality of the Village. Developments in this area will take a mid-rise form similar to those in the South Village, to respect the Village character. Development of these lands is contingent on the removal of the Highland Creek Overpass. Development in the East Village will occur based on the following criteria:

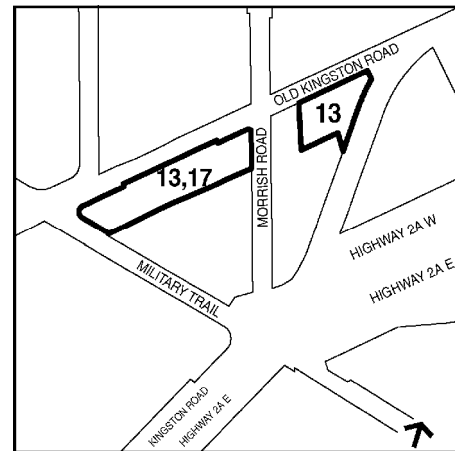
- (i) buildings will have a minimum height of 3 storeys.
- (ii) the character of Highway 2A is likely to be similar to that of an Avenue at such time as the Highland Creek Overpass is removed, and in order to encourage development consistent with that located on other Avenues within the City, commercial and institutional uses will be required on the ground floor of developments facing Highway 2A in the East Village.

6. Chapter 6, Section 2, Highland Creek Secondary Plan, is amended by adding to Subsection 3, Site and Area Specific Policies, the following Site and Area Specific Policies:

**13. 271, 275, 277A, 285, 287, 291, 297, 371, 385 Old Kingston Road**

For the lands shown as 13 on Map 2-1:

- (a) To allow adequate space for the pedestrian environment, as a condition of development approval, an additional 3.0 metres of land on the south side of Old Kingston Road, will be secured and required to be dedicated to the City.

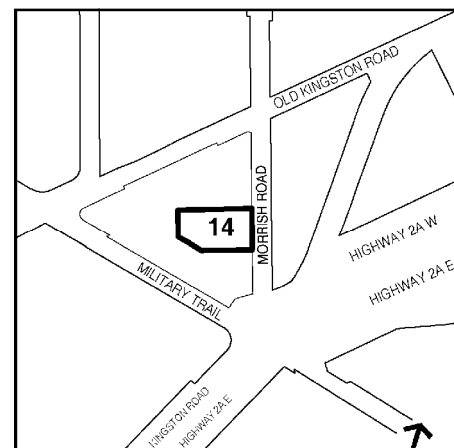


**14. Lands on the west side of Morrish Road South of Old Kingston Road**

For the lands shown as 14 on Map 2-1:

- (a) Policies of this Plan prohibiting the disposal of City owned land in the *Green Space System* or *Parks and Open Space Areas* do not apply, provided that a minimum of 2,800 square metres of land is retained for the sole purpose of providing city owned parkland.

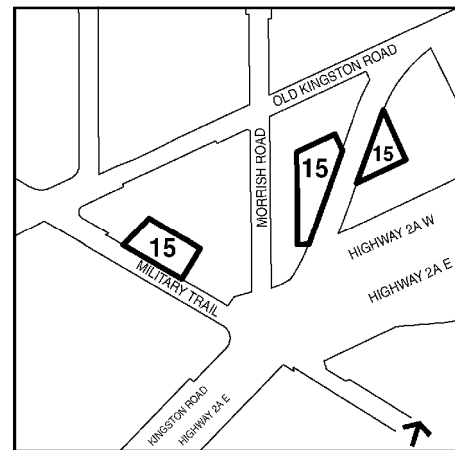
- (b) Public or private underground parking is permitted on the lands provided that no part of a parking structure or related structure or equipment is visible at grade.



**15. 1614, 1620, and 1622 Military Trail, and 6049X and 6067 Kingston Road**

For the lands shown as 15 on Map 2-1:

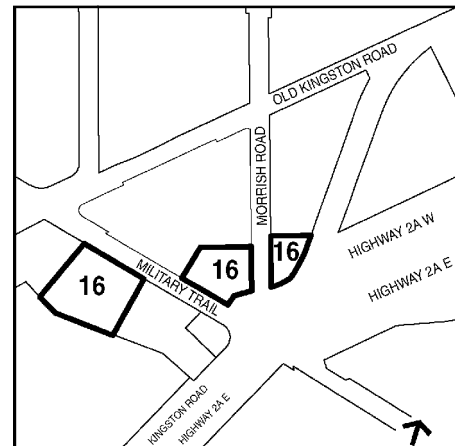
- (a) Building heights of up to 6 storeys (20 metres), not including mechanical penthouses, may be considered, by way of a Zoning By-law Amendment.



**16. 1624X and 1625 Military Trail, 28X Morrish Road, and 6030, and 6032 Kingston Road**

For the lands shown as 16 on Map 2-1:

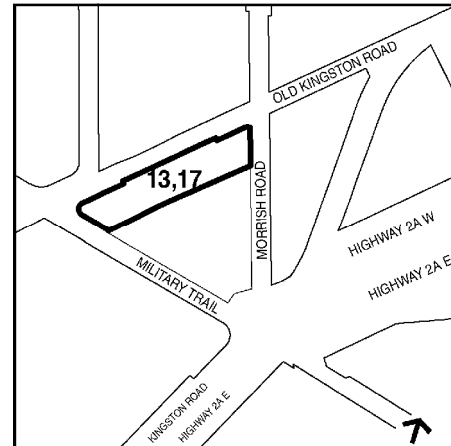
- (a) Building heights of up to 8 storeys (27 metres), not including mechanical penthouses, may be considered, by way of a Zoning By-law Amendment.



**17. 271, 275, 277A, 285, 287, 291, and 297 Old Kingston Road**

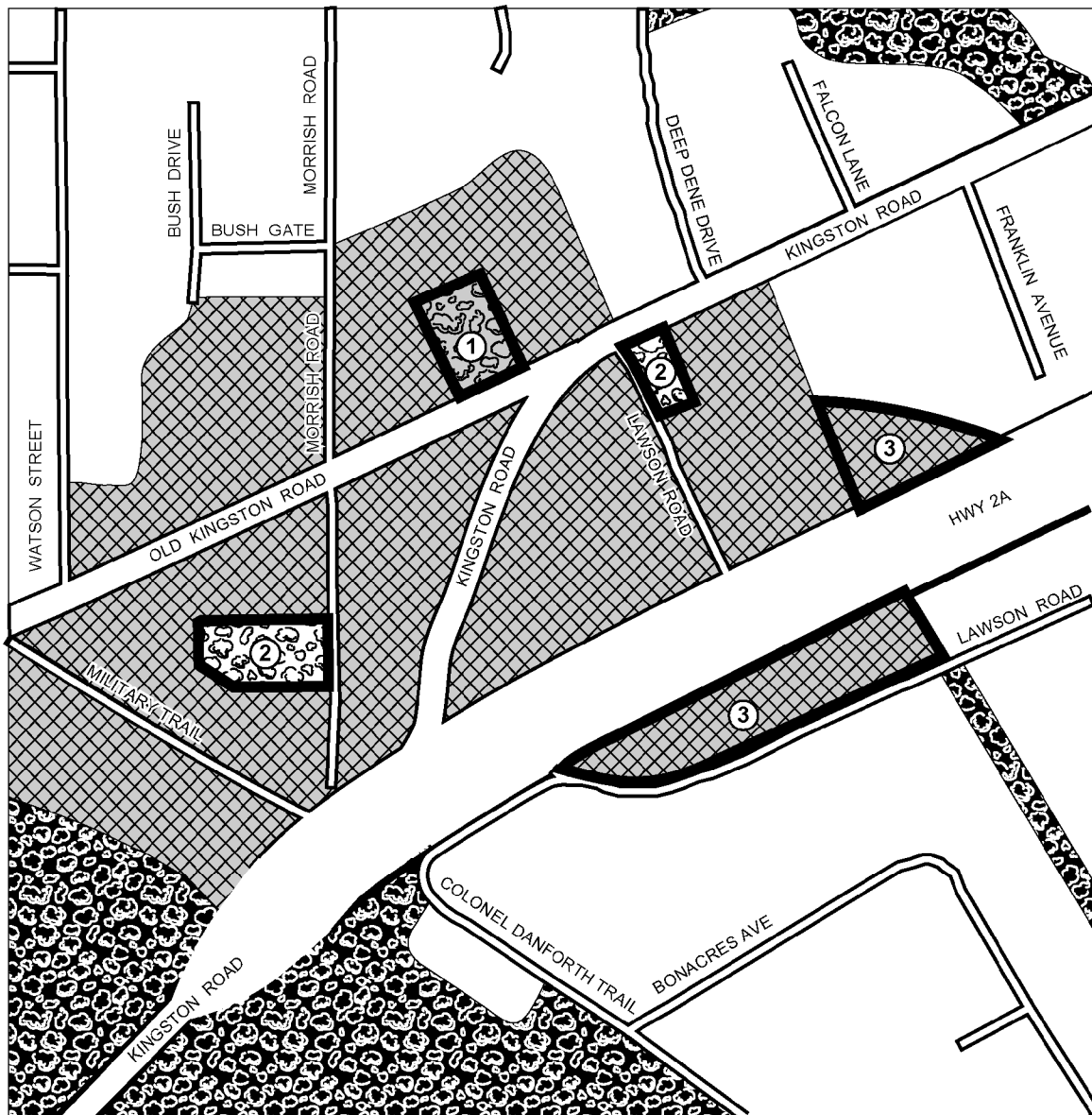
For the lands shown as 17 on Map 2-1:

- (a) To serve and provide vehicular access to the properties on the south side of Old Kingston Road between Lawson Road and Morrish Road, as a condition of development, a 7.5 metre public lane, including a 1.5 metre landscaped strip, will be secured and required to be dedicated to the City.



7. Chapter 6, Section 2, Highland Creek Secondary Plan, Map 2-1, Urban Structure Plan, is amended by adding Site and Area Specific Policies 13, 14, 15, 16, and 17 as shown on the attached Schedule 3.
8. Chapter 6, Section 2, Highland Creek Secondary Plan, is amended by renumbering Map 2-2, Tertiary Plan to Map 2-3, Tertiary Plan, and by adding a future public lane as shown on the attached Schedule 4.
9. Chapter 6, Section 2, Highland Creek Secondary Plan, is amended by adding Map 2-2, Highland Creek Village Plan, as shown on the attached Schedule 5.

## Schedule 1



### Official Plan Amendment # 187

Revisions to Land Use Map 23

Highland Creek Community

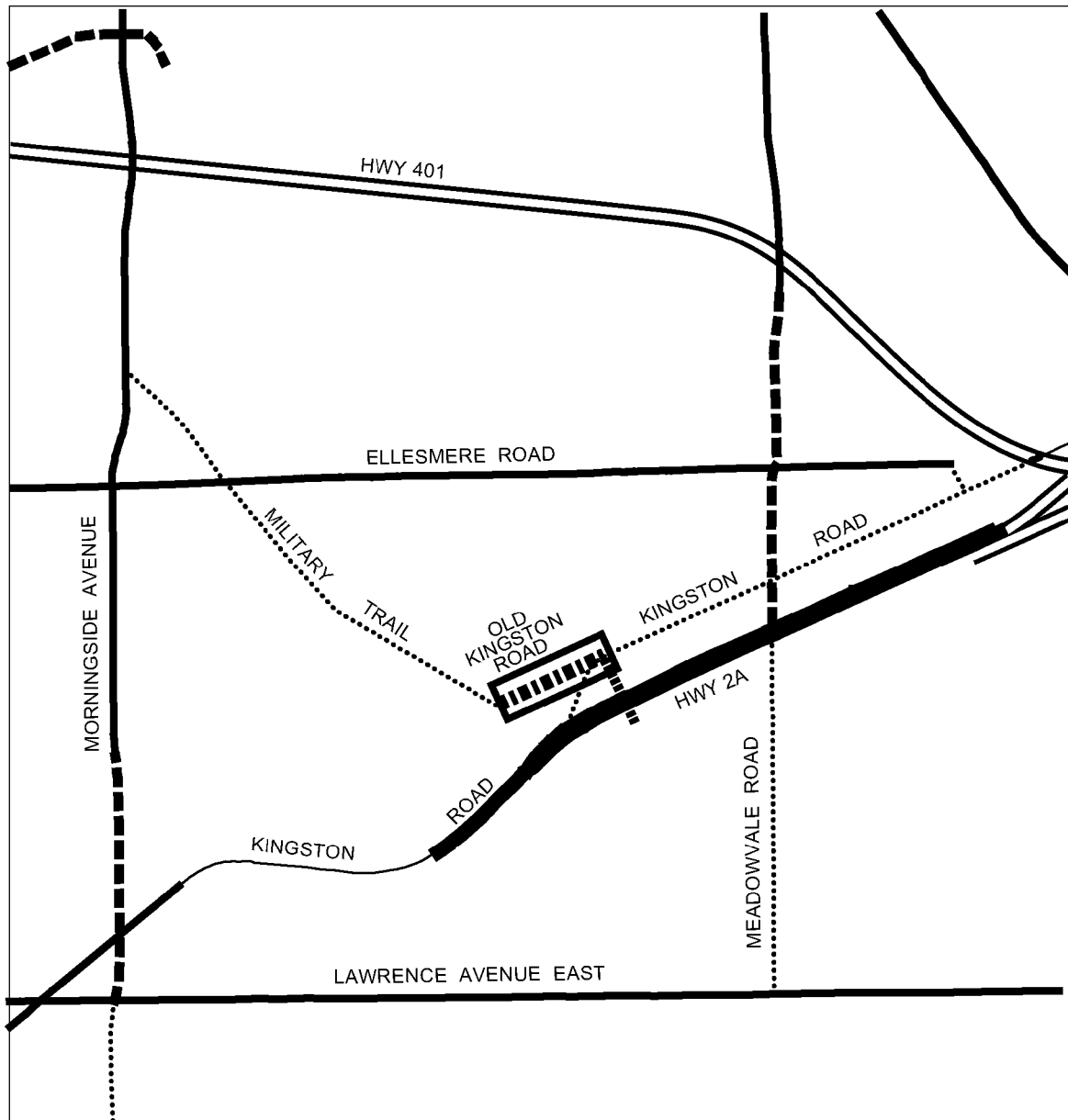
File # 10 303518

- 1** Revisions to Land Use Map 23 to redesignate lands from Mixed Use Areas to Parks and Open Space Areas - Other Open Space Areas
- 2** Revisions to Land Use Map 23 to redesignate lands from Mixed Use Areas to Parks and Open Space Areas - Parks
- 3** Revisions to Land Use Map 23 to redesignate lands from Neighbourhoods to Mixed Use Areas

	Neighbourhoods		Parks & Open Spaces Areas		Parks		Other Open Space Areas
	Mixed Use Areas		Natural Areas				

↑  
Not to Scale  
04/16/12

## Schedule 2



### Official Plan Amendment # 187

Highland Creek Community

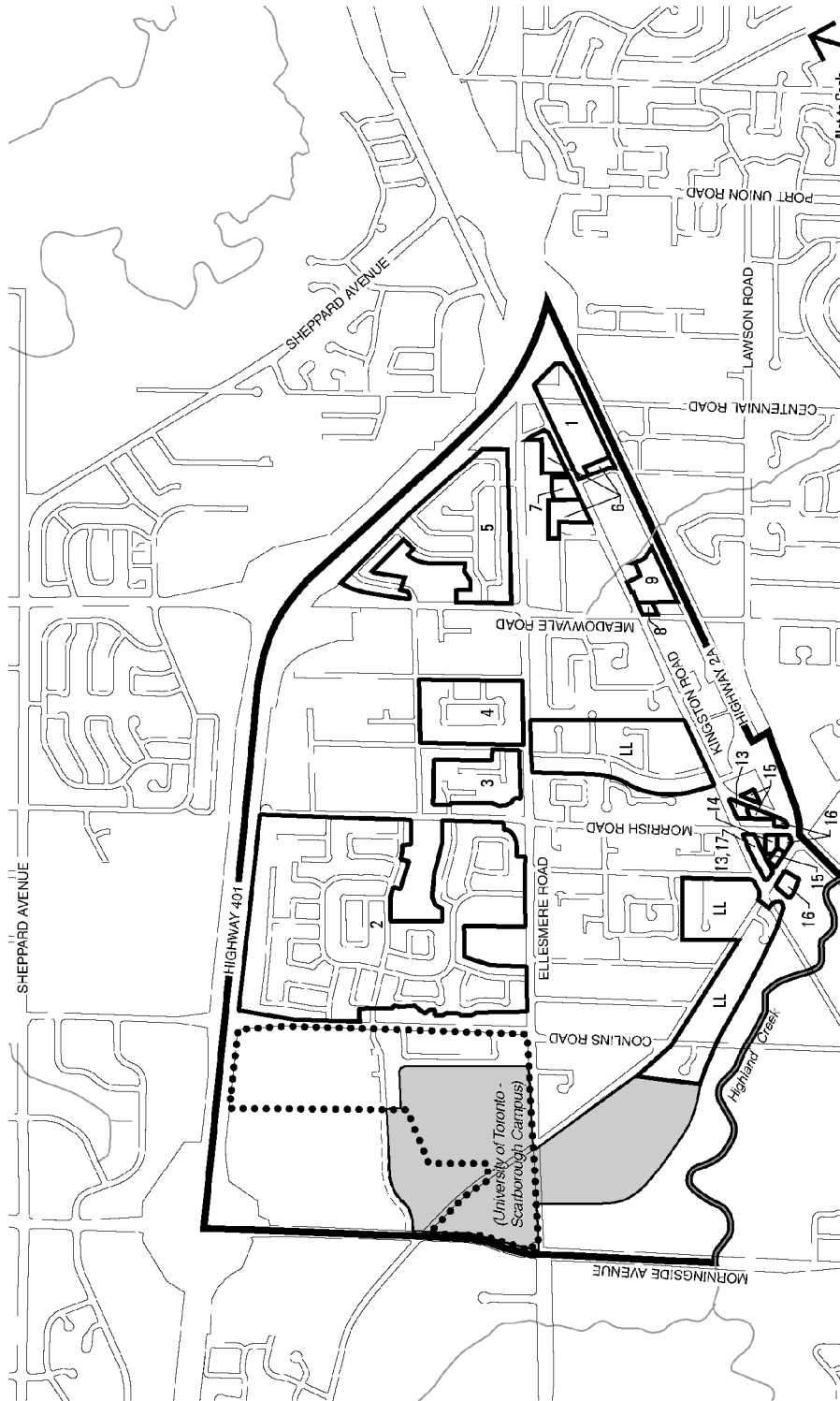
File # 10 303 518

Revisions to Right-of-Way Widths Associated with Existing Major Streets Map 3 to Redesignate 20m Right of Way Width to 23m Right of Way Width





# Schedule 3



## Highland Creek Secondary Plan

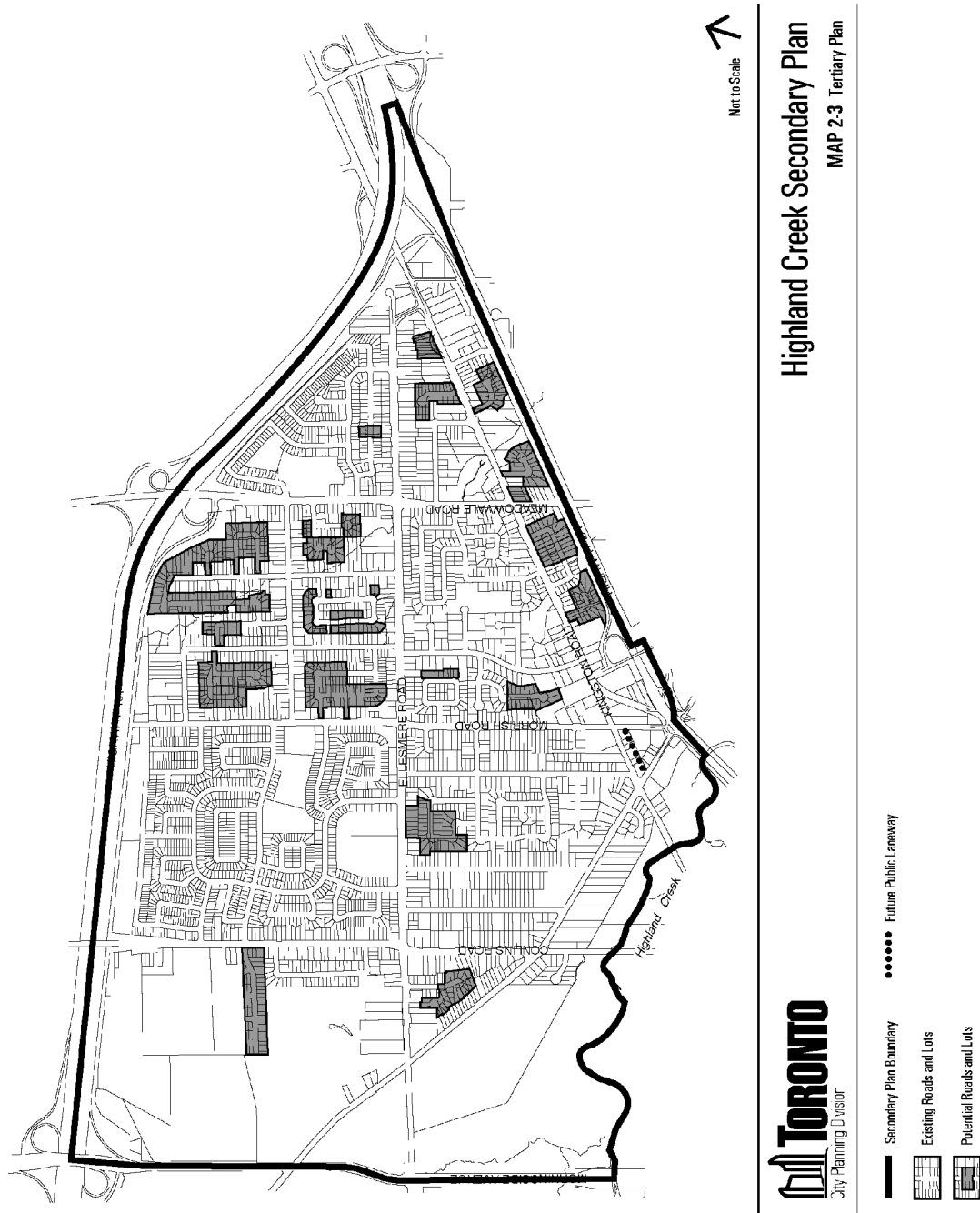
MAP 2-1 Urban Structure Plan

- 1 Site and Area Specific Policies
- LL Large Lot Areas

- Secondary Plan Boundary
- Proposed Road
- Area 'A'

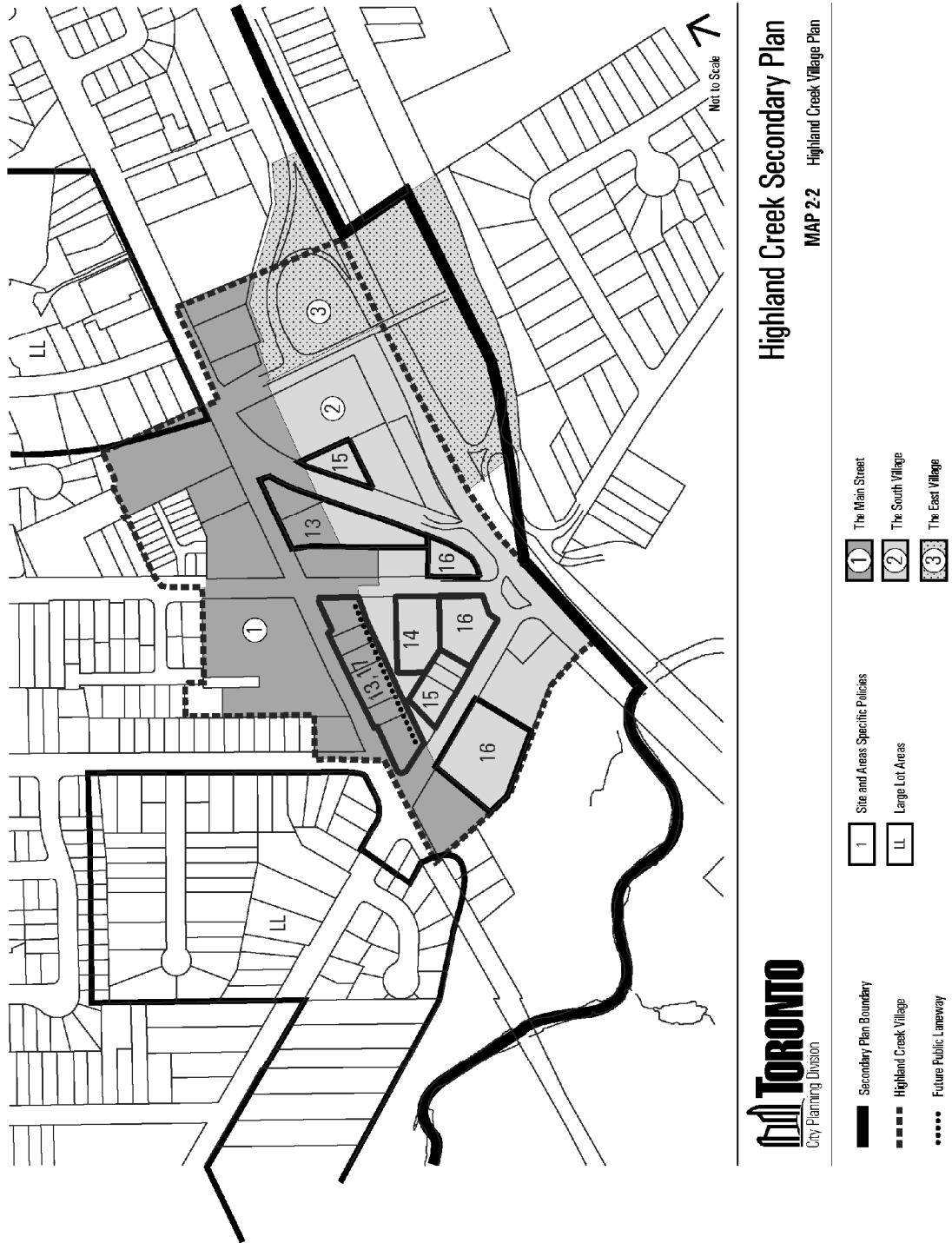
October 2012

## Schedule 4



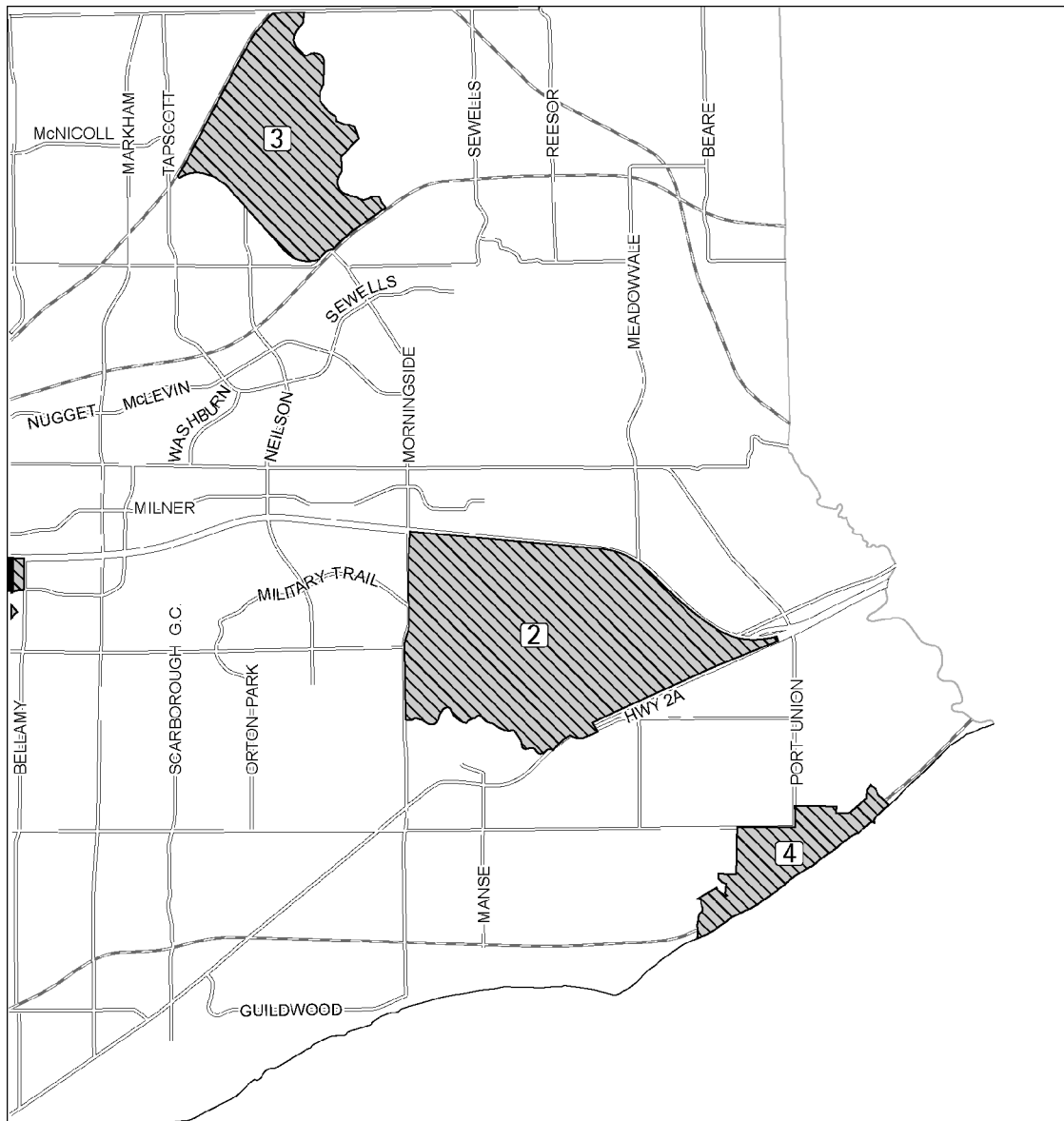
October 2012

# Schedule 5



October 2012

## Schedule 6



City Planning

### Official Plan Amendment # 187

Revisions to Secondary Plan Key Map 35 - - Boundary Adjustment

Highland Creek Community

File # 10 303518



Secondary Plan Areas

#### Secondary Plan Areas

- 2 Highland Creek
- 3 Morningside Heights
- 4 Port Union Village Community



Not to Scale  
October 2012

## Attachment 7: Draft Highland Creek Community Zoning By-law Amendment

Authority: Scarborough Community Council Item No. , as adopted by City of  
Toronto Council on , 2012  
Enacted by Council: , 2012

### CITY OF TORONTO

#### BY-LAW No.

To amend Highland Creek Community Zoning By-law No. 10827, as amended, with respect to lands known as 255 Old Kingston Road, 1625 Military Trail, 6000, 6115, 6143-6147, 6149, 6096, and 6098 Kingston Road, all lands on the north side of Old Kingston Road and Kingston Road between Lawson Road and Watson Street, and all lands south of Old Kingston Road and north of Kingston Road and Highway 2A between Lawson Road and Military Trail as outlined on Schedule 1, attached to and forming part of this By-law.

WHEREAS authority is given to Council by Section 34 and 36 of the *Planning Act*, R.S.O. 1990, c. P. 13, as amended, to pass this By-law; and

WHEREAS Council of the City of Toronto has provided adequate information to the public and has held at least one public meeting in accordance with the *Planning Act*;

The Council of the City of Toronto HEREBY ENACTS as follows:

1. **CLAUSE V – INTERPRETATION**, is amended by adding the following definitions to sub-section (f), **Definitions**:

#### **Ancillary**

means naturally and normally incidental, subordinate in purpose or **floor area**, and exclusively devoted to a permitted use, building or structure.

#### **Angular Plane**

means an imaginary flat surface projecting over a lot, at an inclined angle measured up from the horizontal.

#### **Established Grade**

means the average elevation of the ground measured at the two points where the projection of the required minimum **front yard setback** line is 0.01 metres past each **side lot line**.

#### **First Floor**

means the floor of the building, other than a floor used for a **parking space**, that:

- i) is closest in elevation to the elevation of **established grade**;
- ii) has an interior **floor space** in excess of 10 square metres; and

- iii) is closest to the required **front yard setback**.

**Landscaping, Soft**

means landscaping and excludes hard-surfaced areas such as decorative stonework, retaining walls, walkways, or other hard surface landscaped-architectural elements.

**Outdoor Patio**

means an outdoor patron area **ancillary** to a non-residential use.

2. **CLAUSE VI – PROVISIONS FOR ALL ZONES**, Sub-Clause 15, **Lands Not Covered by Buildings** is amended by adding the following paragraph to the end of this Sub-Clause:

In areas designated in Schedule ‘A’ as “CR”, the lands not covered by permitted buildings, structures and **parking spaces**, shall be used only for **landscaping**, 50% of which shall be **soft landscaping**. **Outdoor patio** areas **ancillary** to a permitted non-residential use in the “CR” Zone shall not be considered **landscaping** and shall be permitted, provided the **outdoor patio**:

- a) is located between the front wall of the building or structure housing the principal uses and the **front lot line** of the lot; or
- b) is located between the wall facing the **side lot line** of the building or structure housing the principal uses and the **side lot line** of the lot, to a maximum of 50% of the depth of the building or structure housing the principal use, measured from the front wall; and
- c) has a maximum area the greater of:
  - 1. 30 square metres; or
  - 2. 30% of the interior floor area of the premises it is associated with;
- d) is not used to provide entertainment uses such as music and dancing, whether as an **ancillary** use or associated with the principal use; and
- e) is at least 10 metres from a lot in a **Single-Family Residential (S), Two-Family Residential (T) or Multiple Family Residential (M)** zone.
- f) despite e) above, if located on a roof, or on a platform that is higher than 0.6 m above average grade, must be at least 40.0 m, measured horizontally, from a lot in a **Single-Family Residential (S), Two-Family Residential (T) or Multiple Family Residential (M)** zone; and
- g) if on a lot which abuts a lot in a **Single-Family Residential (S), Two-Family Residential (T) or Multiple Family Residential (M)** zone, or a lot is within 10.0 m of a lot in a **Single-Family Residential (S), Two-Family Residential (T) or Multiple Family Residential (M)** zone, an **outdoor patio** located in the rear yard of the lot must have a fence installed along the portion of the **outdoor patio** parallel to the **rear lot line**."

3. **CLAUSE VIII - ZONE PROVISIONS** is amended by adding the following to Sub-Section 16 **Commercial-Residential (CR) Zone**:

(c) Supplementary Regulations

- i. **Street townhouse dwellings and multiple-family dwellings** are permitted in all locations with exception to those lands within 20 m of the Old Kingston Road or Highway 2A **street** lines.

4. **Schedule “A”** is amended by deleting the zoning for the lands outlined on Schedule '1' by adding the following zoning to the lands outlined on Schedule '1':

CR-214-215-216-217-265-270-275-503-511-513-516-518-519-542-552-556-557-571-573-574-577

CR-214-215-216-217-265-270-275-503-506-516-517-518-519-520-522-541-555-556-570-573-574-575-577

CR-214-215-216-217-265-270-275-502-506-511-515-516-518-519-521-540-551-556-557-571-572-573-574-577

CR-214-215-216-217-265-270-275-503-505-510-516-518-519-540-551-556-571-573-574-575-577

CR-214-215-216-217-265-270-275-503-507-510-516-518-519-540-551-556-571-573-574-575-577

CR(H)-214-215-216-217-265-270-275-501-504-505-509-516-518-519-541-551-556-571-573-574-576-577

CR-214-215-216-217-265-270-275-501-504-505-511-518-519-521-540-550-556-557-571-572-574-577

CR-214-215-216-217-265-270-275-503-505-515-516-518-519-540-551-556-571-573-574-575-577

CR-214-215-216-217-265-270-275-500-504-505-510-518-519-540-550-556-557-571-572-574-576-577

CR-214-215-216-217-265-270-275-500-511-518-519-540-550-556-557-571-572-574-575-576-577

CR(H)-214-215-216-217-265-270-275-501-504-505-516-518-519-541-551-556-571-573-574-576-577

CR-214-215-216-217-265-270-275-502-505-511-518-519-540-550-556-557-571-572-574-575-577

CR-214-215-216-217-265-270-275-503-512-514-515-516-517-518-519-522-542-553-554-556-571-573-574-575-576-577

CR-214-215-216-217-265-270-275-503-508-510-516-518-519-540-551-556-571-574-577

CR(H)-214-215-216-217-265-270-275-503-504-514-516-518-519-541-551-556-571-573-574-576-575-577

CR(H)-214-215-216-265-270-275-217-503-506-510-516-518-519-540-551-556-571-574-575-577

CR(H)-214-215-216-217-265-270-275-503-506-516-518-519-542-551-556-571-573-574-577

P

5. **Schedule “B”, PERFORMANCE STANDARD CHART**, is amended by adding the following Performance Standards as follows:

**PARKING**

215. Notwithstanding the provisions of **CLAUSE VII - GENERAL PARKING REQUIREMENTS FOR ALL ZONES**, any buildings or structures constructed after January 1, 2013, the following minimum parking rates will apply to uses permitted within the **Commercial Residential (CR) Zone**:

<b>Day Nurseries</b>	-	1.0 space per 100 square metres of <b>gross floor area</b>
<b>Dwelling Units</b>	-	Parking will be provided at a minimum rate of:
	-	0.8 spaces for each bachelor <b>dwelling unit</b> ;
	-	0.9 spaces for each one-bedroom <b>dwelling unit</b> ;
	-	1.0 space for each two-bedroom <b>dwelling unit</b> ;
	-	1.2 spaces for each three or more bedroom <b>dwelling unit</b> ; and,
	-	visitors parking will be provided at 0.15 spaces per <b>dwelling unit</b> .



Educational and Training Facility Uses	-	3.0 spaces per 100 square metres of <b>gross floor area</b>
Financial Institutions	-	3.0 spaces per 100 square metres of <b>gross floor area</b>
Hotels	-	1.0 space per guest room
Medical Centres	-	3.0 spaces per 100 square metres of <b>gross floor area</b>
Nursing Homes	-	0.3 spaces for each <b>dwelling unit</b> and bed sitting room
Offices	-	1.5 spaces per 100 square metres of <b>gross floor area</b>
<b>Personal Service Shops</b>	-	1.5 spaces per 100 square metres of <b>gross floor area</b>
<b>Places of Entertainment</b>	-	10 spaces per 100 square metres of <b>gross floor area</b>
<b>Place of Worship</b>	-	If there is permanent or fixed seating in a <b>place of worship</b> , then a minimum of 23.0 <b>parking spaces</b> must be provided for each 100 square metres of worship area, and;
	-	If there is no seating or variable seating in a <b>place of worship</b> , then a minimum of 34.0 <b>parking spaces</b> must be provided for each 100 square metres of worship area.
<b>Recreational Uses</b>	-	3.0 spaces per 100 square metres of <b>gross floor area.</b>
Restaurants	-	3.0 spaces per 100 square metres of <b>gross floor area</b> , or part thereof, between 0 and 500 square metres of <b>gross floor area</b> , and 5.0 spaces per 100 square metres of <b>gross</b>

		<b>floor area</b> , or part thereof, in excess of 500 square metres of <b>gross floor area</b>
Retail Stores	-	3.0 spaces per 100 square metres of <b>gross floor area</b>
Retirement Homes	-	0.3 spaces for each unit or bed-sitting room.
<b>Multiple Family Dwellings</b>	-	1.0 space per <b>dwelling unit</b> , and, visitors parking will be provided at 0.15 spaces per <b>dwelling unit</b> .
<b>Street Townhouse Dwelling</b>	-	1.0 spaces per <b>dwelling unit</b> .

216. For any **dwelling units** constructed after January 1, 2013, **bicycle parking spaces** shall be provided at a rate of:

- (a) 0.7 long term **bicycles parking spaces** for each **dwelling unit**;
- (b) 0.1 short term **bicycle parking spaces** for each **dwelling unit**; and where:
  - i) Long-term bicycle parking are **bicycle parking spaces** for use by the occupants or tenants of a building and must be located in a secure, weather protected and enclosed bicycle parking area within 30.0 m from a pedestrian entrance to the principal building on the lot; and
  - ii) Short-term bicycle parking are **bicycle parking spaces** for use by visitors to a building and shall be located in bicycle parking area at grade within 30.0 m from a pedestrian entrance to the principal building on the lot.

**Bicycle parking space** for a **dwelling unit** shall not be located:

- (a) in a **dwelling unit**; or
- (b) on a balcony; or
- (c) in a storage locker; or
- (d) an area used for **ancillary** commercial space.

217. Minimum 0.13 occupant **bicycle parking spaces** per 100 m<sup>2</sup> of **gross floor area** for all uses excluding **dwelling units** and the greater of 0.15 visitor **bicycle parking spaces** per 100 m<sup>2</sup> of **gross floor area** or 6 **bicycle parking spaces** for all uses excluding **dwelling units**.

## **SETBACKS**

- 500. Minimum building **setback** of 0.0 metres and a maximum building **setback** of 3.0 metres from a **street** line for that portion of a building measured from **established grade** to a building **height** of 13.5 metres.
- 501. Minimum building **setback** of 0.0 metres and a maximum building **setback** of 3.0 metres from the **street** line abutting Old Kingston Road and a minimum building **setback** of 1.5 metres and a maximum building **setback** of 3.0 m from any other **street** line for that portion of a building measured from **established grade** to a building **height** of 13.5 metres.
- 502. Minimum building **setback** of 1.5 metres and a maximum building **setback** of 3.0 metres from any **street** line for that portion of a building measured from **established grade** to a building **height** of 13.5 metres.
- 503. Minimum building **setback** of 0.0 metres and a maximum building **setback** of 3.0 metres for all uses permitted from a **street** line abutting Old Kingston Road and a minimum building **setback** of 3.0 metres and a maximum building **setback** of 5.0 metres from the **street** line abutting Kingston Road, Military Trail, Lawson Road, Morrish Road, and/or Highway 2A for that portion of a building measured from **established grade** to a building **height** of 13.5 metres.
- 504. The lot line abutting Old Kingston Road shall be deemed to be the **front lot line**.
- 505. Minimum building **setback** of 0.0 metres from a **side yard**.
- 506. Minimum building **setback** of 3.0 metres from a **side yard**.
- 507. Minimum **side yard** building **setback** of 3.0 metres on one side and 0.0 metres on the other side.
- 508. Minimum **side yard** building **setback** of 7.5 metres on one side and 3.0 metres on the other side.
- 509. Minimum building **setback** of 6.0 metres from a **rear yard**.
- 510. Minimum building **setback** of 7.5 metres from a **rear yard**.
- 511. Minimum building **setback** of 7.5 metres from any lot line abutting an 'S', 'T', 'M' 'PW' or 'P' zone.
- 512. Minimum building **setback** of 7.5 metres from the west lot line.

513. Minimum building **setback** of 7.5 metres from the lot line abutting the lands known as 1622 Military Trail at the time of passing of this By-law.
514. For any portion of a building within 30.0 metres of the Old Kingston Road frontage, the minimum **side yard setback** will be 0.0 metres and for any portion of a building beyond 30.0 metres of the Old Kingston Road frontage, the minimum **side yard setback** will be 7.5 metres.
515. Minimum building **setback** of 6.0 metres from the south lot line if grade related entrances to **dwelling units** are constructed adjacent to the south lot line, and a minimum building **setback** of 3.0 metres if no grade related entrances to **dwelling units** are constructed adjacent to the south lot line.
516. For that portion of buildings above 10.5 metres in **height**, buildings shall be setback at a minimum an additional 1.5 metres from that portion of the building closest to all **street** lines.

For the purposes of this Performance Standard the wall below 10.5 m in **height** shall be no less than 75% in length of the overall length of the wall facing all **street** lines.

517. For that portion of buildings above 20.0 metres in **height**, buildings shall be setback at a minimum an additional 1.5 metres from that portion of the building closest to all **street** lines.
518. No building step backs will be provided at, or below, 2 **storeys** or 7.5 metres in building **height**.
519. Setbacks that apply to the “**CR**” Zone shall not apply to underground structures.
520. The lot line abutting Military Trail shall be deemed to be the **front lot line**.
521. Minimum building **setback** of 3.0 metres and a maximum building **setback** of 5.0 metres from the **street** line for all residential uses for that portion of a building measured from **established grade** to a building **height** of 13.5 metres.
522. For that portion of a building that has a **height** equal to or less than the width of the right-of-way of the **street** it abuts, the following provisions apply:
  - i) if a wall of the building has principal windows and a line projected at a right angle from one of these walls intercepts another such wall on the same lot, the minimum above ground distance between the walls must be 20.0 m; or

- ii) if a wall of the building has windows facing a wall which does not have a window and a line projected at a right angle from one of these walls intercepts the other wall on the same lot, the minimum above ground distance between them must be 11.0 m.

### **INTENSITY OF USE**

- 540. Maximum **gross floor area** 1.5 times the area of the lot or parcel.
- 541. Maximum **gross floor area** 2.0 times the area of the lot or parcel.
- 542. Maximum **gross floor area** 2.5 times the area of the lot or parcel.

### **BUILDING HEIGHT**

- 550. Minimum building **height** of 7.5 metres and a maximum building **height** of 10.5 metres, excluding rooftop mechanical penthouses.
- 551. Minimum building **height** of 10.5 metres and a maximum building **height** of 13.5 metres, excluding rooftop mechanical penthouses.
- 552. Minimum building **height** of 10.5 metres and a maximum building **height** of 20.0 metres, excluding rooftop mechanical penthouses.
- 553. Minimum building **height** of 10.5 metres and a maximum building **height** of 13.5 metres, excluding rooftop mechanical penthouses for any building or structure, or portion thereof, located within 30.0 metres of the Old Kingston Road **street** line.
- 554. Minimum building **height** of 10.5 metres and a maximum building **height** of 20.0 metres, excluding rooftop mechanical penthouses for any building or structure, or portion thereof, situated greater than 30.0 metres from the Old Kingston Road **street** line and greater than 50.0 metres from the Highway 2A **street** line; and a minimum building **height** of 10.5 metres, and a maximum building **height** of 27.0 metres, excluding rooftop mechanical penthouses for any building or structure, or portion thereof, situated within 50.0 metres from the Highway 2A **street** line.
- 555. Minimum building **height** of 10.5 metres and a maximum building **height** of 20.0 metres, excluding rooftop mechanical penthouses for any building or structure, or portion thereof, situated within 70.0 metres of the **lot line** abutting the property municipally known as 255 Old Kingston Road at the time of the passing of this By-law, and a minimum building **height** of 10.5 metres and a maximum building **height** of 27.0 metres, excluding rooftop mechanical penthouses for any building or structure, or portion thereof,

situated greater than 70.0 metres from the **lot line** abutting the property municipally known as 255 Old Kingston Road at the time of the passing of this By-law.

- 556. Mechanical penthouse shall be **setback** a minimum of 5.0 metres in addition to the required **setbacks** from all **street yards**. The mechanical penthouse shall cover no more than 30% of the roof area and extend no more than 5.0 metres above the roof deck.
- 557. Buildings shall not penetrate into a **45 degree angular plane** from the lot line abutting “S”, “T”, “O”, “P” and “M” Zones.

### **MISCELLANEOUS**

- 570. The **main wall** shall have a minimum width of 60% and a maximum width of 70% of the lot at the Military Trail or Lawson Road **street line**.
- 571. The **main wall** of the first 10.5 metres of building **height** shall have a minimum width of 60% for any lot with 30.0 metres of frontage or less, and a minimum width of 70% for any lot with greater than 30.0 metres of frontage.
- 572. Minimum 1.5 metre strip of land immediately abutting lots zoned “S”, “T”, and “M” shall be used for **soft landscaping** purposes only.
- 573. For that portion of a building above 13.5 metres in building **height**, buildings shall be set back a minimum of 5.5 metres from that portion of a building below 13.5 metres in building **height** adjacent to a **side lot line**.
- 574. Balconies and unenclosed porches located above the 1<sup>st</sup> **storey** shall not project beyond that portion of a building below 10.5 meters in building **height** adjacent to a **street line**.
- 575. Additional provisions for **street townhouse dwellings** and **multiple-family dwellings** are as follows:
  - i. Minimum separation between the side walls of adjacent **street townhouse dwelling** or **multiple-family dwelling** blocks shall be 1.2 metres, and 0.0 metres along the common wall between **dwelling units**; and,
  - ii. Minimum rear yard **setback** of 7.5 metres
- 576. For any building or structures constructed after January 1, 2013, the minimum floor to floor **height** of the **first floor** shall be 4.5 metres for that

portion of a building located within 20.0 metres of the Old Kingston Road **street** line.

577. In addition to developing the buildings or structures in accordance with the Performance Standards provided herein, the applicant of any building or structures being 13.5 metres in building **height** or greater, shall enter into an agreement satisfactory to the City of Toronto pursuant to Section 37 of the *Planning Act*, as amended, authorizing the increase in height and density in exchange for one or more of the following benefits:
- Public art;
  - dedicated and accessible community service space for non-profit service agencies to deliver programs/services for various user groups, particularly for youth and seniors.
  - high quality and accessible licensed child care facilities, particularly for infants and toddlers;
  - additional satellite recreation space including large flexible multi-purpose rooms to run programs for all age groups, particularly for youth and seniors;
  - Other non-profit arts, cultural, community or institutional facilities;
  - Streetscape improvements not abutting the site;
  - Purpose built rental housing with mid-range or affordable rents, land for affordable housing, or, at the discretion of the owner, cash-in-lieu of affordable rental units or land;
  - Park improvements to parkland within the Village Area; and
  - Municipally owned and operated parking lots.
7. **Schedule “C”, EXCEPTION MAP** is amended by deleting Exception Number 1 from the lands outlined on Schedule '2'.
8. **Schedule "C", EXCEPTION MAP and EXCEPTION LIST** is amended by deleting Exception Numbers 23, 30, 37 and 39.
9. **Schedule “C”, EXCEPTION MAP and EXCEPTION LIST** is amended by adding Exception Number 7 to the lands outlined on Schedule '2' so that it reads as follows:
7. On those lands identified as Exception 7 on the accompanying Schedule "C" map, the following provisions shall apply:
- (a) Permitted uses prior to the removal of the Holding Provision (H) from the zoning shall be restricted to all uses permitted in the **Highway-Commercial (HC)** zone and **Place of Worship (PW)** zone with the exception of the following uses which are not permitted: **Automobile Service Stations**, **Automobile Sales Uses**, **Mechanical or Automatic Car Washes** and **Public Garages**.

- (b) Prior to the removal of the Holding Provision (H) the following performance standards will apply:

HC-25-30-34-53-76 & PW-30-47-77

- (c) The Holding Provision (H) used in conjunction with the **Commercial Residential (CR) Zone** shall be removed by amending By-law when:

Council is satisfied that land dedicated to the City for the design, construction and financing of new public lanes or streets, in accordance with the recommendations of the Highland Creek Village Area Study and subsequent transportation environmental assessment, has been satisfactorily reviewed by the Chief Planner and Executive Director of City Planning in consultation with the Executive Director of Technical Services and that any recommended improvements have been implemented, or financially secured through appropriate agreements.

- 10. **Schedule “C”, EXCEPTION MAP and EXCEPTION LIST** is amended by deleting Exception Number 20 and replacing it on the lands outlined on Schedule '2', so that it reads as follows:

- 20. On those lands identified as Exception 20 on the accompanying Schedule 'C' map, the following provisions shall apply:

- (a) Additional permitted uses:
  - **Street Townhouse Dwellings**
  - **Multiple-Family Dwellings**

- 11. **Schedule “C”, EXCEPTION MAP and EXCEPTION LIST** is amended by creating Exception Number 55 to the lands outlined on Schedule '2', so that it reads as follows:

- 55. On those lands identified as Exception 56 on the accompanying Schedule 'C' map, the following provisions shall apply:

- (a) Additional permitted uses:
  - **Place of Worship**

- 12. **Schedule “C”, EXCEPTION MAP and EXCEPTION LIST** is amended by adding Exception Number 56, to the lands outlined on Schedule '2', so that it reads as follows:



56. On those lands identified as Exception 57 on the accompanying Schedule 'C' map, the following provisions shall apply:

- (a) No residential uses shall be permitted on the **first floor** of a building within 20 metres of the Old Kingston Road **street line**.

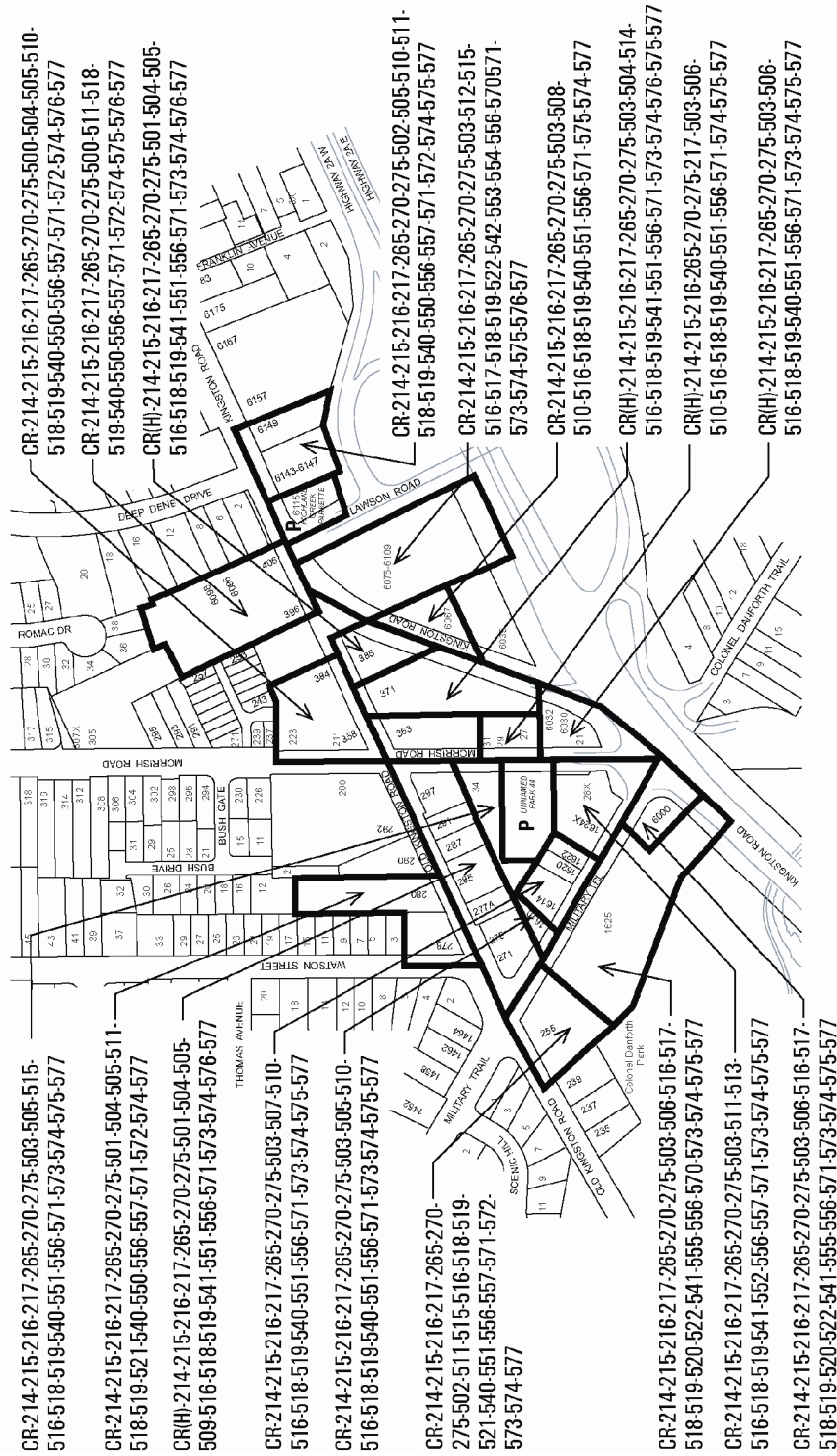
ENACTED AND PASSED this     day of     , A.D. 2012.

FRANCES NUNZIATA,  
Speaker

ULLI S. WATKISS  
City Clerk

(Corporate Seal)

# Schedule '1'



## Zoning By-Law Amendment

## Highland Creek Village Study

File # 10 303518 EPS 44 TM

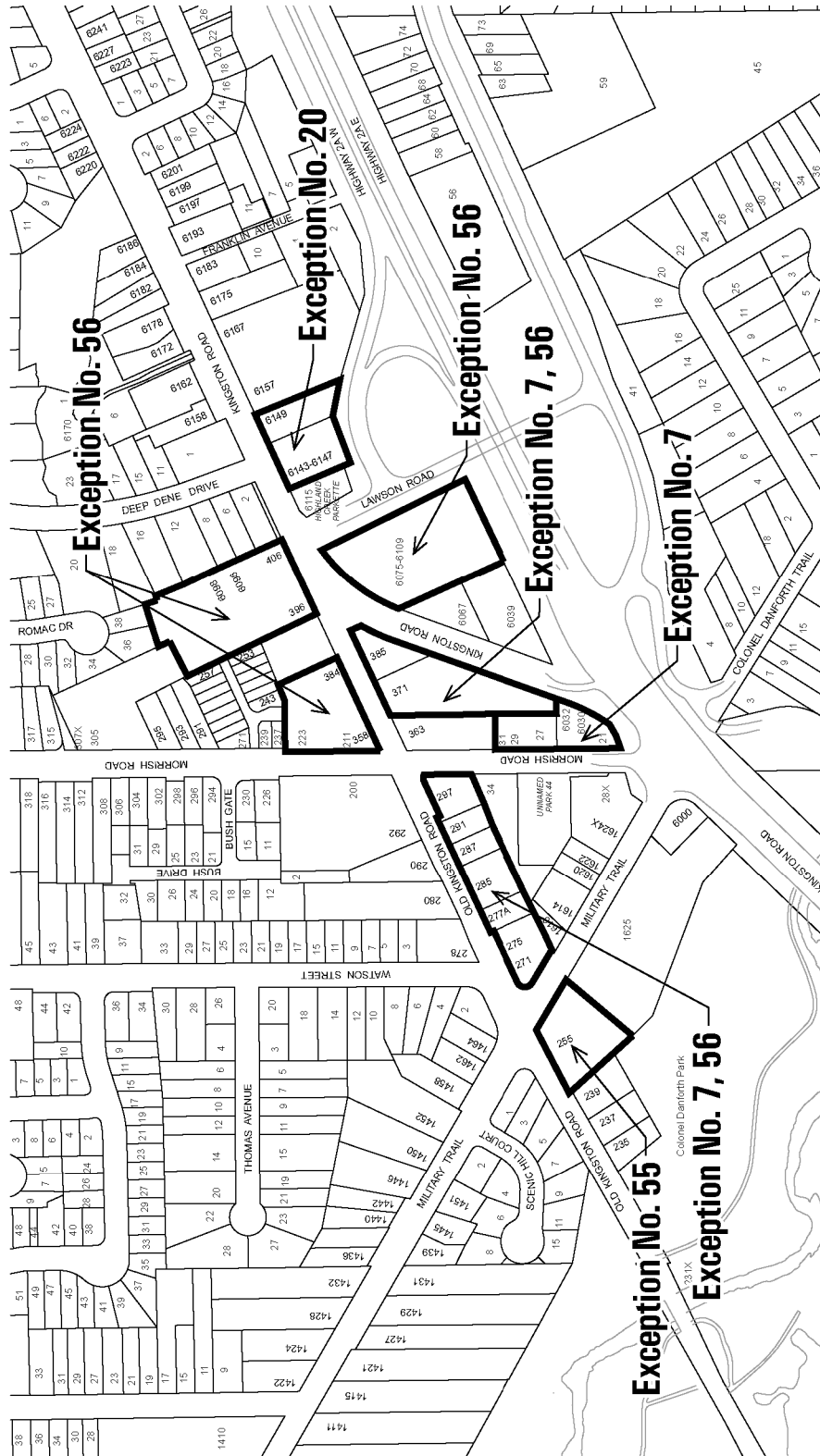


Area Affected By This By-Law

Highland Creek Community By-law  
Not to Scale  
10/19/12



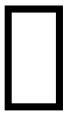
## Schedule '2'



## Zoning By-Law Amendment

## Highland Creek Village Study

File # 10 303518 EPS 44 TM



Area Affected By This By-Law

Highland Creek Community By-law  
Not to Scale  
8/10/12

**Attachment 8: Draft Updated Highland Creek Village Urban Design Guidelines**

**SEE SEPARATE WORD FILE**

**Attachment 9: Community Services and Facilities Review**

**COMMUNITY SERVICES AND FACILITIES REVIEW  
HIGHLAND CREEK VILLAGE AREA STUDY**

**Policy & Research Section  
City Planning Division  
April 2012**

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**April 2012**

## **COMMUNITY SERVICES AND FACILITIES REVIEW HIGHLAND CREEK VILLAGE AREA STUDY**

### **1.0 Introduction**

Community services and facilities (CS&F) reviews are undertaken as part of the building of healthy communities to ensure that local residents have access to a full range of locally-based services and facilities. These reviews are also undertaken in areas that are experiencing major or incremental growth or within established neighbourhoods that are under served (i.e. priority neighbourhoods) and/or going through significant demographic changes. The City's Official Plan provides a framework that ensures that when considering new growth the overall quality of city living must be considered. One area that the Plan requires review and consideration of is the adequacy of the CS&F infrastructure.

This report provides a review of community services and facilities that are available to residents living in and around the Highland Creek Village area. The purpose of the review is to identify the range and adequacy of available CS&F in order to address existing gaps and emerging priorities when planning for new growth. Key resources include publicly funded schools, child care facilities, libraries, community centres, swimming pools, arenas and parks and open space. The review also covers human service agencies (e.g. seniors' support, food banks) that offer support locally.

The CS&F findings identified from this review will assist in recommending appropriate Section 37 community benefits which may be secured to support future residential development proposed for lands located within the Highland Creek Village planning study area. Findings will also help to inform potential Section 37 matters related to the evaluation of four development applications for sites located within the study area.

Land use change is expected to occur over the long term as some sites in and around the study area have the potential to accommodate more growth. The Highland Creek Village currently contains a number of underutilized sites comprised largely of small scale commercial buildings, parking areas and vacant lots. The City of Toronto's Official Plan designates the Highland Village commercial area "Mixed Use Areas" which encourage a broad range of residential, commercial and institutional uses, in single use or mixed use buildings. As such, there are opportunities for intensification and/or revitalization of sites that are located in and around the study area. This type of development would result in an increased residential population which has the potential to place further demands on the existing community services and facilities.

### **2.0 Background**

The Highland Creek Village Area Study was initiated by City Planning in February 2011 to develop a planning framework for future development in the Highland Creek Village area, including four development proposals currently under review by City Planning staff. Given this increased growth projected for the area, it is timely that a future vision

be developed as part of the proposed framework. Community input has been obtained through a working group process that was initiated in February 2011.

Two proposals are for residential development totalling 74 units were approved in Highland Creek Village. These proposals are described below.

- 363 Kingston Road - 3 storey, 30 unit multiple family residential building
- 6039 Kingston Road – 4-storey, 35 unit multiple family residential building, including 3 live/work units

The purpose of the planning framework is to provide guidance for the overall development and vision for the area, including appropriate heights and densities, locations of parks, public parking, social infrastructure needs along with coordination of public realm improvements.

### **3.0 Study Area**

The Highland Creek Village Planning Study Area is located within the broader CS&F study area (See Map A). The planning study area is designated "Mixed Use Areas" in the City of Toronto Official Plan and generally bounded by Old Kingston Road, Highway 2A, Lawson Road and Military Road.

The CS&F study area is bounded by Ellesmere Road to the north, Lawrence Avenue East to the south, Morningside Avenue to the east and Centennial Road to the west. The CS&F Review examined a broader study area in order to capture the range of existing CS&F that serve the local population as well as recognition that the boundaries of agency service areas vary given the nature of their services/programs (e.g. home care services, youth drop-in programs).

### **4.0 Purpose**

The objectives of the Highland Creek Village CS&F Review are:

- To examine the existing characteristics of the local area population;
- To profile the current inventory of CS&F that serves the local population;
- To identify existing unmet needs and service gaps for people living in the study area;
- To determine what CS&F improvements may be necessary to enhance the quality of life for current and future area residents; and,
- To recommend CS&F priorities (e.g. Section 37 community benefits) as part of the evaluation of future development proposed within the Highland Creek Village area.

### **5.0 CS&F Priorities**

A number of key findings have been identified as part of the CS&F review for the Highland Creek Village Study. These findings highlight the needs of a largely mature



and aging residential population that currently lives in the study area. Based on an analysis of these findings, CS&F priorities have been developed for the area as follows:

- *Community Agency Space* - Dedicated and accessible community service space for non-profit service agencies to deliver programs/services for various user groups, particularly for youth and seniors (Minimum 5,000 square feet).
- *Child Care* - High quality and accessible licensed child care facilities, particularly for infants and toddlers (62-72 space child care)
- *Community Recreation* –Additional satellite recreation space including large flexible multi-purpose rooms to run programs for all age groups, particularly for youth and seniors.

## **6.0 Methodology**

The CS&F Review work included preparation and analysis of:

- A demographic profile of residents living within the study area, using population data from the 2006 Census data;
- An inventory of existing CS&F located in and around the study area;
- Consultation with City operating divisions/boards (e.g. Parks, Forestry and Recreation, Children's Services, Toronto Public Library and School Boards) and key service providers (e.g. Salvation Army, Scarborough Centre For Healthy Communities);
- Results from agency questionnaire; and,
- Background information of key service sectors (e.g. local schools, libraries, child care centres, community recreation centres, and human services).

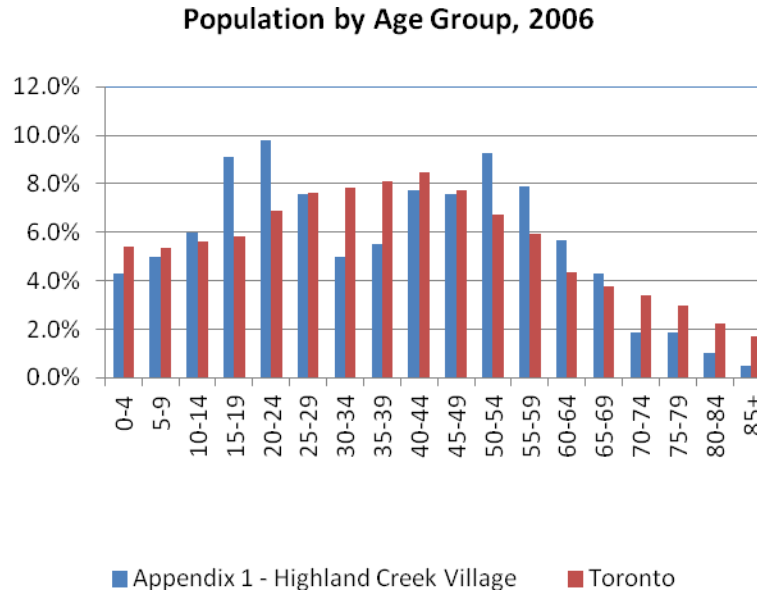
## **7.0 Demographic Highlights**

The demographic profile was based on census data for 2001 and 2006 for the CS&F Study Area. The profile provides a snapshot of the area residents in order to gain a better understanding of their particular needs. A summary of the demographic characteristics is attached as Appendix 1. Appendix 2 provides the 2006 Census data to support the summary.

The Highland Creek Village demographic study area runs east-west from Kingston Road to Ellesmere Road and north-south from Military Trail and Bobmar Road to Centennial Road. The Highland Creek Village is a family oriented neighbourhood. This is reflected in the high proportion of one-family households.

As of the 2006 Census, the area has 2,908 residents, 860 households (total number of dwelling units) and 780 families (1 person, 2 person, etc.). The population growth rate between 2001-2006 fell by 13% (435 residents). The study area has a higher proportion of both children (10-24 years) living at home and younger seniors (50-69 years) than the City averages.

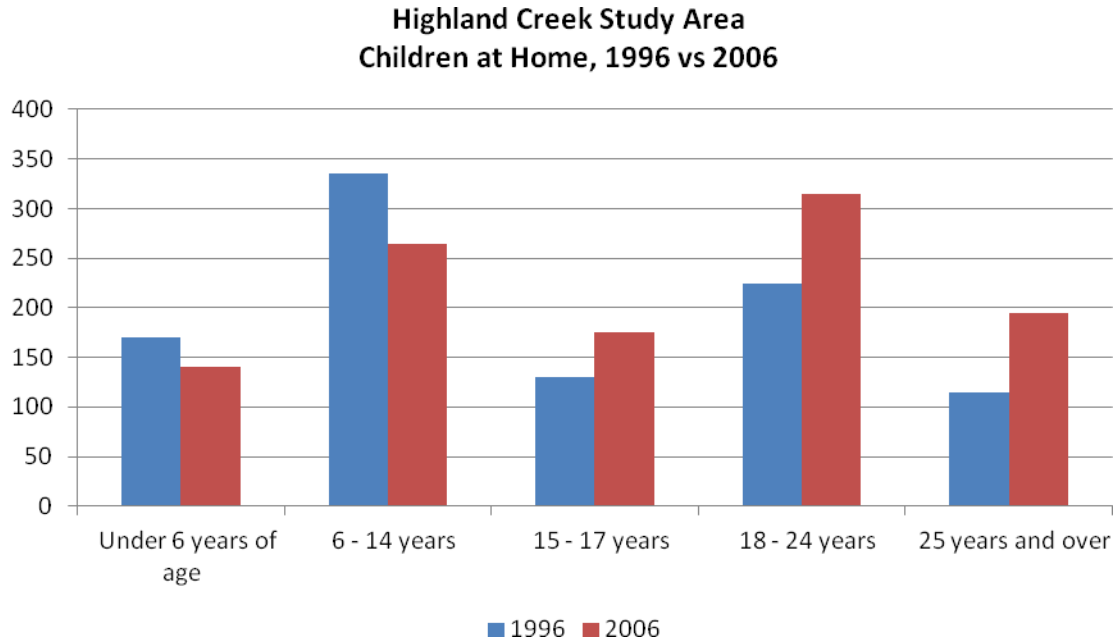
**Figure 1: Population Age Profile**



Below are the key highlights of residents living in the Highland Creek Village area:

- 90% are couples while 10% are lone-parents with children present in 71% of all family households
- Large non-immigrant population (65%) where 81% of residents reported "English" as their home language
- South Asians comprise 31.6% of the recent immigration population
- Large household sizes (4-5 person families) comprise 36.6% compared to City at 20.5%
- Higher number of owner occupied dwellings (92%) compared to City average of 54.5%, as reflected by the large number of single-detached dwellings (76.3%)
- Aging in place community given the lower number of residents moving out of the area, as reflected by the non-movers outnumbering the movers
- Slightly lower proportion of Highland Creek Village residents hold university certificates, diplomas or degrees compared to the City averages
- Higher labour force participation rate, employment-population ratio and a lower unemployment rate compared to the City overall; this is reflected in income earnings
- Average household income of Highland Creek residents (\$102,561) is higher than the City's average (\$80,343)

**Figure 2: Age Profile of Children At Home**



## **8.0 Projected Population**

Given the increased number of planning applications for residential development (as noted in Section 2.0 above), there is potential for growth over a longer time period. As such, the overall planning study will result in a comprehensive planning framework to provide guidance for the overall development of the Highland Creek Village area. Based on the 2 residential development applications at 363 Kingston Road and 6039 Kingston Road, there will be a total of 65 new residential units within the study area. These proposals would potentially generate a new population of approximately 175 people. This projected growth assumption has been prepared by City Planning Research staff based on an average of 2.64 persons per new residential unit.

## **9.0 Agency Questionnaire**

In September 2011, a questionnaire was mailed out to non-profit service agencies serving the Highland Creek Village area. The purpose of the questionnaire was to inquire about each agency's programs and/or services, organizational characteristics, space and funding needs and priorities. Based on survey feedback, the top issue identified by service agencies is the need for affordable and accessible community space in the study area, particularly for youth and seniors. These needs are in addition to those identified by the City's operating divisions (e.g. PF&R, Children's Services) which are discussed in Section 10.0 below.

## **10.0 Community Services and Facilities Inventory**

An inventory and map of CS&F that serve the study area residents is attached as Appendix 3 and Map A. Data was collected from May to September 2011. The purpose of the inventory is to identify the range of available CS&F, to review the capacity of key services (e.g. school enrolments, child care waiting lists), and to identify current service gaps. An analysis of key CS&F was undertaken including publicly funded schools, child care facilities, community recreation centres, libraries, and human services. The inventory also includes places of worship and parks and open space. Places of worship are considered in CS&F reviews when they play a key role in providing community space for local groups to run meetings and programs (e.g. bridge, girl guides) as well as to hold special events (e.g. banquets, weddings). The area has several places of worship that serve this role.

The CS&F inventory for the study area includes:

- 4 TDSB elementary schools
- 2 TDSB secondary schools
- 4 TCDSB elementary school
- 1 TCDSB secondary school
- 8 child care centres
- 3 public libraries
- 5 community recreation centres
- 12 places of worship
- 1 arena
- 2 indoor pools
- 1 outdoor pool
- 6 human service agencies
- Over 66 hectares of parks and open space

### **10.1 Schools**

#### **10.1.1 Toronto District School Board (TDSB)**

The Toronto District School Board (TDSB) has 4 elementary schools and 2 secondary schools that serve students living in the CS&F study area. A description of the school capacities and enrolments is contained in Appendix 3, Table 1a. Two out of the 4 schools are currently operating over capacity – Centennial Road Jr. P.S. at 93.6% and Heron Park Jr. P.S. at 91.5%. An elementary school with a utilization rate of 80% is considered to be operating optimally by the TDSB.

TDSB advise that based on the attendance boundaries for the area, the proposed developments fall within the Highland Creek P.S. and West Hill C.I.'s attendance boundaries. Highland Creek P.S. is currently operating well below capacity at 54.6%.

Based on the proposed residential developments (363 Old Kingston Road and 6039 Kingston Road), TDSB have determined pupil yield factors to assess the new student population. They have determined that the proposed developments will generate student projections of 16 elementary students and 10 secondary students. As such, given these low student yields, there would be sufficient capacity within both elementary and secondary school panels to accommodate the new students generated from these developments.

The 2 secondary schools – West Hill C.I. and Sir Oliver Mowat C.I. are situated just outside the study area. Sir Oliver Mowat is currently operating at capacity (100.6%) while West Hill C.I. is operating close to capacity at 87.9%. A secondary school with a utilization rate of 90% is considered to be operating "at capacity". TDSB staff note that given potential accommodation issues associated with the secondary school panel, this has prompted warning clauses. This increased secondary student enrolment reflects the area as a maturing community with a higher proportion of children between the ages of 10-24 years, compared to the City averages.

#### **10.1.2 Toronto Catholic District School Board (TCDSB)**

The Toronto Catholic District School Board (TCDSB) has four elementary schools and one secondary school serving the study area. Three of the four elementary schools are operating well over capacity. Pope John Paul II located at 685 Military Trail is the closest secondary school serving this area. It is currently operating over capacity at 129% and has 13 on-site portables to address school accommodation issues for this area.

TCDSB staff advise that any new elementary students generated from the proposed developments would be directed to Cardinal Leger, currently operating at 93% or St. Malachy at 165%. Also, new secondary students would be directed to Pope John Paul II. TCDSB has addressed accommodation issues for both elementary and secondary school panels by providing a number of on-site portables at these locations. A description of the school facilities, capacities and enrolments and no. of portables is contained in Appendix 3, Table 1b.

#### **10.2 Child Care Centres**

There are a total of 8 licensed child care programs located within the study area. These child care programs are situated in Wards 43 and 44. Four are non-profit and offer subsidized spaces while the other 4 are commercial operators and do not offer any subsidies. Most of the child care facilities are currently operating with waiting lists for subsidized spaces in each age group. A description of the child care facilities is contained in Appendix 3, Table 2.

There are a total of 500 licensed child care spaces within the study area across all age groups with the pre-school age group comprising 65% (325 out of 500) of the overall number of licensed child care spaces. Five out of the 8 licensed child care programs are located in private buildings representing over 62% of the spaces within the study area.

The remaining 3 child care programs are located in an elementary school, place of worship and a university campus building (U of T-Scarborough Campus).

The breakdown is as follows:

Infants (0 to 18 months)	30
Toddlers (18 months to 2 ½ years)	75
Preschool (2 ½ to 5 years)	325
School Age (6 to 12 years)	<u>70</u>
<b>TOTAL</b>	<b>500</b>

In September 2009, the Province of Ontario announced a 5 year plan to implement a Full-Day Early Learning Kindergarten (FDELK) program for children aged 4 and 5 years. This program will have implications for full day learning. As implementation of full-day kindergarten continues, more children will transition out of child care into the education system. As of 2010, two schools (Highland Creek PS, St. Martin de Porres) have started the Full Day Kindergarten program. In 2012, there will be another two schools (Centennial Rd. Jr., West Hill PS). St. Malachy's start date will be September 2013 and St. Brendan's in September 2014. The start date for Heron Park PS has not yet been determined. As of 2014, all publicly funded schools will offer Full Day Kindergarten. Kindergarten aged children (4-6 years) will no longer be served in licensed child care programs. The pre-school group will change to include children 2 ½ years of age until they start Jr. Kindergarten. This group already includes children 2 ½ years of age until they start Grade 1.

Children's Service's staff advise that currently Ward 44 has only 2 centres that offer infant care with fee subsidy (N'Sheemaehn and Friendly Little Campus 2) and 1 centre that has infant care but no fee subsidy (Sunshine Beechgrove). There is considerable demand for infant and toddler subsidized child care in the study area. Should there be an opportunity to secure a new child care for this area, Children's Services recommend a 72 space child care facility model to serve 20 infants, 20 toddlers and 32 pre-school children aged 2.5 to 3.8 years.

### **10.3 Public Libraries**

There are 3 public libraries serving the study area – Highland Creek, Morningside and Cedarbrae. The Highland Creek and Morningside Neighbourhood Branches are located in the study area. The Cedarbrae District Branch, located just outside the study area at 545 Markham Road, supports these two neighbourhood branches. Cedarbrae was re-opened in December 2010 after a major renovation. There is also an existing bookmobile stop located at 20 Waldock Street that serves the nearby Kingston-Galloway area.

In accordance with the Toronto Public Library's Service Delivery Model, the role of the District Library is to provide extensive recreational and informational collections, reference and a broad range of services to diverse populations by offering more in-depth

resources than neighbourhood libraries. The Service Delivery Model defines District libraries to have a minimum size of 25,000 square feet and to serve a population of a minimum 100,000 within a 2.5 km. radius.

Neighbourhood library branches provide general informational and recreational collections and services for adults and children in areas with populations ranging from 20,000 to 50,000. The Service Delivery Model defines the primary service area of neighbourhood branches to be within a 1.6 km radius and to have a minimum size of 10,000-15,000 square feet. Both Highland Creek and Morningside libraries fall below the minimum facilities size for a neighbourhood branch. Toronto Public Library staff advise that there are no future expansion plans or significant changes to service these libraries.

Both neighbourhood libraries offer a range of programs/services and resource materials such as large print and French and multi-lingual collections, audio books on CD, computer training, reading support programs, youth leadership programs, Leading to Reading programs and access to computers and wireless service. Located within these facilities are an auditorium and large meeting room for community groups and service agencies to access. These meeting spaces accommodate from 25 to 45 people and are equipped with flip charts, overhead projectors, chairs, DVD player. The services offered in each of the library facilities are contained in Appendix 3, Table 3.

The table below illustrates the catchment populations and sizes of each branch.

Branch	Highland Creek	Morningside	Cedarbrae
Location	3550 Ellesmere Rd.	4279 Lawrence Ave. E.	545 Markham Rd.
2006 Census Population	24,020	25,632	46,580
Branch Size	7,000 s.f. (651 sq.m.)	7,000 s.f. (651 sq.m.)	31,475 s.f. (2,927sq.m.)

#### **10.4 Community and Recreation Facilities**

Community recreation plays an important role in maintaining a healthy lifestyle. Parks, Recreation and Forestry Division (PF&R) is committed to the healthy development of residents by providing safe, accessible, quality programs and services that engage the public in a welcoming environment.

PF&R's Policy and Development staff advise that there are 5 City-owned community centres that offer a full range of recreational programs and services to the Highland Creek Village residents. Three are located within the study area – Heron Park Recreation Centre, Tall Pines Community Centre and Port Union Recreation Centre while the other 2 are situated just outside the study area - West Rouge C.C. and Curran Hall C.C. Appendix 3, Table 4a provides description of these recreational facilities along with the various programs that are offered within them.

Heron Park R.C. located at 292 Manse Road is the closest recreational facility serving the Highland Park Village area residents. It is also the largest facility and well used by local

residents for a wide range of recreational programs including indoor soccer, indoor basketball, ball hockey and fitness and wellness programs. General interest programs together with arts and crafts programs are also offered for children, youth and adults. Its popular pre-school and summer camp programs draw users from beyond the study area. The community centre features a double pad indoor ice arena which offers recreational skating and learn-to-skate programs. The arena is also used by the West Hill Minor Hockey Association on weekday evenings and on weekends through a permit system. The Centre also features an outdoor swimming pool which offers aquafit programs for adults and swimming lessons for children.

Port Union R.C. located at 5450 Lawrence Avenue East, just east of Heron Park R.C., features a large fitness/weight room and 4 multi-purpose rooms. Programs offered include arts and heritage, fitness and wellness, and pre-school. According to PF&R's records (2010), Port Union R.C. serves a large seniors client base as it had the majority of Older Adult registrations from all community recreation centres serving the study area.

West Rouge C.C. located at 270 Rouge Hills Drive features 4 multi-purpose rooms. The centre offers programs for all age groups including pre-school, children's camps, and fitness and wellness for adult and older adult. Based on PF&R records (2010), West Hill C.C. had the highest number of pre-school registrations from all community recreation centres in the area.

Tall Pines and Curran Hall Community Centres are smaller facilities with one or two multi-purpose rooms. They offer limited programs in fitness and wellness, arts and heritage, pre-school and camps.

Based on PF&R records, other notable registration results show that indoor swimming is a popular program offered at the local area high schools – Sir Oliver Mowat C.I. and West Hill C.I. These locations had the highest program registrations. Also, records indicate that some area residents are willing to travel beyond PF&R's 3.2 km service radius to area facilities such as Centennial R.C., Agincourt R.C., and Sir Wilfred Laurier C.I. in order to register for high demand programs such as swimming, skating, pre-school and camps.

The absence of a nearby City-owned indoor pool and the limited amount of programming space are common concerns identified by PF&R Recreation staff. In November 2009, the City of Toronto won the bid to host the 2015 Pan Am Games. As part of Toronto's partnership/participation with University of Toronto, a new 37,000 s.m. (400,000 s.f.) competitive aquatic centre and multi-sport field will be built on the University of Toronto Scarborough Campus (UTSC) lands, immediately located northwest of the study area. The proposed facility will include two 50-metre swimming pools, a dive tank, gymnasiums and running and sprint track facilities, minor recreational facilities as well as offices, and the headquarters of the Canadian Sports Institute of Ontario (CSIO) for training and development of high performance athletes.



The City's Official Plan contains policies which encourage the shared use of multi-service facilities as well as adequate and equitable access to such facilities. The proposed facility will be shared between the UTSC students and faculty and the City of Toronto PF&R Division. It will also provide much needed recreational and community centre space to the neighbourhood and world class aquatics facilities to the region.

## **10.5 Parks and Open Spaces**

Parks are an important component of the City's system of natural areas and the City's open space network and public realm. Parks and trails provide benefits to individuals on a physical and psychological basis and help to define the character of neighbourhoods across the City.

The Highland Creek area is generally well served with a variety of local parks. Appendix 3, Table 4b includes an inventory of parks and open space found within the study area. Colonel Danforth Park is the largest park in the area. This 54 hectare natural wooded park contains paved walking and hiking trails offering residents opportunities to cycle or hike and explore. It stretches along Highland Creek from the University of Toronto Scarborough Campus to south of Lawrence Avenue East connecting to the Port Union waterfront. There are numerous active parks in the vicinity. Megan Park and Wanita Park offer opportunities for baseball and soccer play along with a playground for children. Lawn bowling is featured at Highland Creek Bowling Green. Highland Creek Parkette, a small sitting park, is the only park found in the immediate village area. According to PF&R's 2012-2021 Capital Budget, Council has authorized new outdoor basketball courts in Heron Park for 2013.

Based on the City's Official Plan, Map 8B, local parkland provision levels in the study area are all within the second highest quintile of 1.57 ha to 2.99 ha per 1,000 people. Future residential and/or mixed use development proposed within the Highland Creek Study Area will be subject to the application of the Alternative Parkland Dedication Rate given that it is identified as a Secondary Plan Area and designated Mixed Use within the City's Official Plan.

## **10.6 Places of Worship**

The study area has 12 places of worship which serve the community by providing accessible and affordable space for child care (e.g. Montessori Preschool) along with meeting and programming space for community groups (e.g. Senior's Club, Youth Group). Appendix 3, Table 5 outlines the various community activities offered within the places of worship. These activities vary in type and target diverse populations from youth (e.g. drop-in programs) to seniors (+50 social club). Several places of worship also offer space for child care programs.

## **10.7 Human Service Agencies**

Human service agencies provide a broad range of community-based services that contribute to the quality of life and well-being of the city. Such services cover a broad range of human services sectors to assist residents including immigrant and settlement services, employment and job training, legal aid, home support services, family resource programs, food bank programs, health services, counselling services and others. The CS&F inventory has identified 6 key service agencies located in and around the study area. Locations of these agencies are identified on Map B. Appendix 3, Table 6 provides a description of these agencies.

### *Seniors' Services*

Scarborough Centre for Healthy Communities (formerly West Hill Community Services) and Salvation Army are two key non-profit service agencies that provide a range of programs and services to the area's growing seniors' population. Scarborough Centre for Healthy Communities is located at 4637 Kingston Road. They occupy two units within an existing commercial building and run a wide range of seniors' support services/programs, including Adult Day Program, Frail Elderly Program, Home Maintenance Program, Bereavement Group, Caregiver Support and Transportation Service. Staff from Scarborough Centre for Healthy Communities advise that they are currently operating at capacity to meet the increased demand for seniors' services/programs. They would like to expand their services to meet the needs of a growing seniors' population. As such, there is a need for additional community space to run expanded seniors services/programs in the area. The Salvation Army is located in a place of worship at 305 Morrish Road. They run a popular food and clothing bank and offer seniors' recreation programs. Salvation Army staff advise that the food and clothing bank is currently operating at capacity.

### *Residential Care Services*

There are two residential care facilities located in the study area that target specific populations. Tony Stacey Centre is a long term care facility located at 59 Lawson Road that serves veterans and their spouses. The Ehatare Retirement and Nursing Home located at 40 Old Kingston Road comprises of a 32 bed nursing home along with a retirement home (76 private rooms and 12 double rooms). This facility provides accommodation for Estonian seniors.

### *Services for People With Developmental Disabilities*

There are two service providers that provide services and programs for people with disabilities. The Meta Centre is an innovative and highly successful non-profit organization that provides day programs for adults and children with developmental disabilities. Day services offered include recreation and leisure, work experience (ie Ontario Disability Support Program), community integration and centre-based activities (ie life skills, computer training). DLC Residential Services is a non-profit agency that operates 4 group homes in Scarborough, including one within the study area located at 344A Morningside Avenue. This program, with a capacity of 36, is geared for children,

youth and transitional aged youth (6-25 years) with developmental disabilities, autism and dual diagnosis. The program's focus is on communication, behaviour analysis, life and social skills training, community integration and family inclusion.

## **11.0 Conclusions**

The CS&F Review completed for the Highland Creek Village area highlights a mature and aging population with a large proportion of older youth and seniors that make up the community. Given increased development proposed within the commercial village area, it is timely that a new vision for this area incorporate an appropriate planning framework to guide such development. The anticipated development will result in a new resident population which will increase demands for additional community services and facilities.

Findings from the CS&F Review have identified existing service gaps for various user groups. In particular, the need for additional large and flexible multi-purpose space, particularly for seniors and youth has been identified as the top CS&F priority. The building of healthy and liveable communities requires area residents to have equitable access to a range of locally-based CS&F. Findings from the Highland Creek Village CS&F Review will assist in ensuring that key elements including identification of appropriate Section 37 community benefits will be considered as part of future development proposed for this area.

## APPENDIX 1

### Highland Creek Village 2006 Demographic Summary

Highland Creek Demographic Study area runs east-west from Kingston Road to Ellesmere and north-south from Military Trail and Bobmar Road to Centennial Road.

As of the 2006 Census, there were 2,908 residents, 860 households and 780 families living in the study area. The population of Highland Creek fell by 13% or by 435 residents between 2001 and 2006.

Compared to City averages, the study area has a higher proportion of children between the ages of 10 and 24, 50 to 69 years, and a lower proportion of pre-school aged children 0 to 4 years, adults 30 to 49 years and older adults 70 years of age and older.

Of the 780 families living in the Highland Creek boundary, 90% are couples and 10% are lone-parents. Children are present in 71% of all family households. The relative proportion of one-person households is below the City's average, while those with 4 or 5 persons exceeds it. Thus, Highland Creek is a family neighbourhood. This is reflected in the high proportion of one-family households.

Owner-occupied single-detached houses characterize the urban landscape. The majority of dwellings were built between 1981 and 2000.

Two-thirds of Highland Creek residents are non-immigrants. This is reflected in the home language statistics where 81% of residents reported 'English' as their home language.

In terms of the percentage of residents who moved into and out of the study area between 2001 and 2006, non-movers outnumbered movers; therefore, to a certain extent the residents of Highland Creek are aging in place.

Education statistics show that a slightly lower proportion of Highland Creek residents hold university certificates, diplomas or degrees compared to City averages.

The main occupations of Highland Creek residents are in 2 areas: Business, Finance and Administration, and in Sales and Service. The main industries are in Manufacturing, Retail Trade, Finance and Insurance.

The area has a higher labour force participation rate, employment-population ratio and a lower unemployment rate compared to Toronto overall. This is reflected in income earnings. The average household income of Highland Creek residents is higher than the City's average. The percentage of owners who are paying more than 30% of their household income on shelter costs also exceed the City's average (29% Highland Creek vs. 15% City).

## **Data Note**

### **Population Data – The Undercount**

Although Statistics Canada makes a great effort to count every person, some people are missed in each Census. Statistics Canada takes this into account and estimates an 'undercoverage' rate for the urban region (Census Metropolitan Area) every Census, but not for the City. The CMA undercount in 2006 was 4.94%.

The 2006 Census counted an unexpectedly high number of unoccupied dwelling units. This leads City staff to suspect that the Census may have missed more households than usual.

## **Census Definitions**

### **Households**

Households refer to '**private**' households. It does not refer to people living in collective dwellings such as rooming houses, religious institutions, group homes, hospitals, jails and long term care facilities.

There are 2 types of households:

1. Census Family Households
2. Non-Family Households

### **Census Family Households (Family Households)**

Family Households includes

- married couples (same or opposite sex), with and without children
- common-law couples (same or opposite sex), with and without children
- Lone parents, with at least one child living in the same dwelling.

Note: the term, 'couple families' refers to married couples and common-law couples.

### **Non-Family Households**

Non-Family Households refers to either one person living alone in a private dwelling or to a group of two or more people who share a private dwelling, but who do not constitute a family.

**Data Source:** Statistics Canada, 2006 Census

**Prepared by:** City of Toronto, City Planning Division, Research and Information, March, 2011

## Appendix 2 - Highland Creek Village 2006 Census Data

### A word of caution about the 2006 Census Data

#### Population Data - The Undercount

Although Statistics Canada makes a great effort to count every person, some people are missed in each Census. Statistics Canada takes this into account and estimates an 'undercoverage' rate for the urban region (CMA) every Census, but not for the City. The CMA undercount in 2006 was 4.94%. The 2006 Census counted an unexpectedly high number of unoccupied dwelling units. This leads City staff to suspect that the Census may have missed more households than usual.

#### Structure Type - Probable Data Problem

The 2006 data shows large changes in various dwelling structures. For example, the number of single and semi-detached dwellings fell by 55,600, whereas the number of duplexes and apartment units in buildings under 5 storeys increased by 66,400. Please note that this data may not reflect actual construction and demolition patterns, thus the data must be analyzed with caution.

Demographic Variable	Highland Creek	%	Toronto	%
<b>Population</b>				
Total 2001 Population	3,343		2,481,494	
Total 2006 Population	2,908		2,503,281	
2001-2006 Change	-435	-13.0%	21,787	0.9%
<b>Age</b>				
Total Population by Age Group	2,910		2,503,280	
0-4	125	4.3%	134,980	5.4%
5-9	145	5.0%	133,595	5.3%
10-14	175	6.0%	141,045	5.6%
15-19	265	9.1%	146,205	5.8%
20-24	285	9.8%	172,450	6.9%
25-29	220	7.6%	190,255	7.6%
30-34	145	5.0%	195,670	7.8%
35-39	160	5.5%	203,025	8.1%
40-44	225	7.7%	212,595	8.5%
45-49	220	7.6%	193,980	7.7%
50-54	270	9.3%	168,440	6.7%
55-59	230	7.9%	148,120	5.9%
60-64	165	5.7%	109,465	4.4%
65-69	125	4.3%	93,830	3.7%
70-74	55	1.9%	85,165	3.4%
75-79	55	1.9%	74,905	3.0%
80-84	30	1.0%	56,450	2.3%
85+	15	0.5%	43,105	1.7%
<b>Characteristics of Families and Households</b>				
<b>Families</b>				
Total number of families in private households	780		670,090	
Without children	225	28.8%	219,345	32.7%
With children	555	71.2%	450,745	67.3%

Total couple families by family structure	700	89.7%	533,955	79.7%
Total married couples	650	92.9%	472,410	88.5%
Total common-law couples	50	7.1%	61,545	11.5%
Total couple families by family structure	700	89.7%	533,955	79.7%
Total couples without children at home	225	32.1%	219,345	41.1%
Total couples with children at home	475	67.9%	314,610	58.9%
1 child	160	22.9%	129,275	24.2%
2 children	240	34.3%	130,045	24.4%
3 or more children	90	12.9%	55,290	10.4%
Total lone-parent families by Number of Children at Home	80	10.3%	136,135	20.3%
1 child	30	37.5%	83,560	61.4%
2 children	10	12.5%	37,430	27.5%
3 or more children	40	50.0%	15,145	11.1%
Breakdown of children at home by age group (20% sample data)	1,090		783,465	
Under 6 years of age	140	12.8%	159,605	20.4%
6 - 14 years	265	24.3%	245,895	31.4%
15 - 17 years	175	16.1%	83,030	10.6%
18 - 24 years	315	28.9%	163,225	20.8%
25 years and over	195	17.9%	131,710	16.8%
Average number of children at home per census family	1.4		1.2	
Total number of persons 65 years and over (20% sample data)	240		333,725	
Number of non-family persons 65 years and over	85	35.4%	121,435	36.4%
Living with relatives	40	16.7%	24,890	7.5%
Living with non-relatives only	0	0.0%	6,755	2.0%
Living alone	45	18.8%	89,790	26.9%
Number of family persons 65 years and over	155	64.6%	212,290	63.6%
<b>Households</b>				
Total no. of private households by household size	860		979,325	
1 person	110	12.8%	295,825	30.2%
2 persons	205	23.8%	282,685	28.9%
3 persons	150	17.4%	161,440	16.5%
4 - 5 persons	315	36.6%	200,735	20.5%
6 or more persons	80	9.3%	38,640	3.9%
Number of persons in private households	2,895		2,467,000	
Average number of persons in private households	3.4		2.5	
Total no. of private households by household type	870		979,440	
One-family households	615	70.7%	602,235	61.5%
Multiple-family households	85	9.8%	32,640	3.3%
Non-family households	170	19.5%	344,565	35.2%
Total no. of persons in private households	2,880		2,467,165	
Number of non-family persons	310	19.4%	479,625	19.4%

Number of family persons	2,570	80.6%	1,987,540	80.6%
Average number of persons per census family	3.3		3.0	

### Housing

Total no. of occupied private dwellings by structural type	865		979,310	
Single-detached house	660	76.3%	266,880	27.3%
Semi-detached house	0	0.0%	69,465	7.1%
Row house	35	4.0%	54,685	5.6%
Apartment, detached duplex	150	17.3%	44,100	4.5%
Apartment building, five or more storeys	0	0.0%	379,695	38.8%
Apartment building, less than five storeys	15	1.7%	162,980	16.6%
Other single attached house	5	0.6%	1,345	0.1%
Movable dwelling	0	0.0%	160	0.0%

Total no. of occupied private dwellings by period of construction	865		979,450	
Before 1946	35	4.0%	180,790	18.5%
1946-1960	120	13.9%	203,495	20.8%
1961-1970	75	8.7%	185,315	18.9%
1971-1980	15	1.7%	161,750	16.5%
1981-1990	360	41.6%	115,490	11.8%
1991-2000	185	21.4%	72,220	7.4%
2001-2005	75	8.7%	60,390	6.2%

Total number of occupied private dwellings by tenure	870		979,445	
Owned	800	92.0%	532,590	54.4%
Rented	70	8.0%	446,855	45.6%

### Immigrants

Total Population by Immigrant status	4,455		2,476,540	
Non-immigrant population	2,890	64.9%	1,184,230	47.8%
Immigrant population	1,555	34.9%	1,237,700	50.0%
Non-permanent residents	10	0.2%	54,610	2.2%

Top 5 immigrant groups by region of birth	1,320		1,237,720	
1st	India	17.0	Eastern Asia	17.3%
2nd	Southern Europe	13.3	Southern Asia	14.9%
3rd	South America	12.1	Southern Europe	13.5%
4th	Caribbean and Bermuda	10.6	Southeast Asia	10.2%
5th	Africa	4.5	Eastern Europe	8.5%

Top 5 recent immigrant groups by region of birth (2001-2006)	95		267,855	
1st	Southern Asia	31.6%	Southern Asia	26.0%
2nd	India	21.1%	Eastern Asia	22.2%
3rd	Eastern Africa	15.8%	W. Central Asia & the Middle East	10.7%
4th	Northern Europe	15.8%	Southeast Asia	9.6%



5th	Republic of China	10.5%	Eastern Europe	9.0%
<b>Total immigrant population by period of immigration</b>				
Before 1961	1,330	45.7%	1,237,715	49.4%
1961 to 1970	95	7.1%	105,965	8.6%
1971 to 1980	225	16.9%	118,575	9.6%
1981 to 1990	315	23.7%	162,340	13.1%
1991 to 2000	240	18.0%	205,600	16.6%
2001 to 2005	360	27.1%	377,380	30.5%
2006 to 2010	95	7.1%	267,855	21.6%
<b>Total population 15 years and over by generation status</b>				
1st generation	2,490		2,067,445	
2nd generation	1,335	53.6%	1,221,020	59.1%
3rd generation and over	850	34.1%	417,455	20.2%
4th generation and over	315	12.7%	428,970	20.7%
<b>Home Language</b>				
Top 5 single responses	2,775		2,378,835	
1st	English	80.9	English	67.0%
2nd	Hindi	3.2	Chinese <sup>1</sup>	3.5%
3rd	Cantonese	2.9	Cantonese	3.2%
4th	Italian	1.3	Tamil	2.1%
5th	Tamil	1.3	Italian	1.9%
<sup>1</sup> n.o.s. = "not otherwise stated" (all Chinese other than Mandarin, Cantonese and Hakka)				
<b>Education</b>				
Total population 25 to 64 years	1,705		1,413,815	
No certificate, diploma or degree	155	9.1%	175,690	12.4%
Certificate, diploma or degree	1,550	90.9%	1,238,125	87.6%
High school certificate or equivalent	475	27.9%	298,670	21.1%
Apprenticeship or trades certificate or diploma	140	8.2%	87,390	6.2%
College, CEGEP or other non-university certificate or diploma	275	16.1%	231,770	16.4%
University certificate, diploma or degree	660	38.7%	620,295	43.9%
University certificate or diploma below bachelor level	110	6.5%	92,070	6.5%
University certificate or degree	550	32.3%	528,225	37.4%
<b>Mobility</b>				
Total by mobility status 5 years ago	2,785		2,340,835	
Non-movers	2,015	72.4%	1,283,715	54.8%
Movers	770	27.6%	1,057,120	45.2%
Non-migrants	595	21.4%	661,240	28.2%
Migrants	175	6.3%	395,880	16.9%
Internal migrants	65	2.3%	144,435	6.2%
Intraprovincial migrants	55	2.0%	110,930	4.7%
Interprovincial migrants	10	0.4%	33,505	1.4%
External migrants	110	3.9%	251,445	10.7%
<b>Income<sup>2</sup></b>				
Total private households	865		979,310	

Average 2005 household income \$	102,561		80,343	
Total population in private households	2,900		2,465,500	
Low income before tax in 2005 %		12.7		24.5
Low income after tax in 2005 %		10.9		19.4
Total households (30%+ of Hhld income on shelter costs)	285	32.9	355,530	36.3
Tenant households (30%+ of Hhld income on shelter costs)	35	4.0	208,260	21.3
Owner households (30%+ of Hhld income on shelter costs)	250	28.9	147,270	15.0
Total number of dwellings	865		979,310	

<sup>2</sup> Average income data should be treated as estimated within the study area. These numbers are calculated from aggregate data.

#### Labour Force

Total population 15 years and over by labour force activity	2,490		2,067,445	
In the labour force	1,840		1,344,825	
Employed	1,725		1,242,215	
Unemployed	115		102,610	
Not in the labour force	650		722,620	
Participation rate		73.9%		65.0%
Employment-population ratio		69.3%		60.1%
Unemployment rate		6.3%		7.6%

#### Occupations<sup>3</sup>

All occupations	1,795		1,311,665	
Management occupations	195	10.9%	139,465	10.6%
Business, finance and administration occupations	450	25.1%	271,815	20.7%
Natural and applied sciences and related occupations	160	8.9%	105,840	8.1%
Health occupations	60	3.3%	64,650	4.9%
Occupations in social science, education, government service and religion	135	7.5%	125,295	9.6%
Occupations in art, culture, recreation and sport	40	2.2%	70,270	5.4%
Sales and service occupations	445	24.8%	299,155	22.8%
Trades, transport and equipment operators and related occupations	205	11.4%	134,175	10.2%
Occupations unique to primary industry	10	0.6%	7,815	0.6%
Occupations unique to processing, manufacturing and utilities	95	5.3%	93,185	7.1%

<sup>3</sup> Based on 2001 National Occupational Classification for Statistics.

#### Industries<sup>4</sup>

All industries	1,800		1,311,660	
11 Agriculture, forestry, fishing and hunting	0	0.0%	2,140	0.2%
21 Mining and oil and gas extraction	0	0.0%	2,020	0.2%
22 Utilities	35	1.9%	6,370	0.5%
23 Construction	90	5.0%	63,225	4.8%
31-33 Manufacturing	230	12.8%	153,700	11.7%
41 Wholesale trade	105	5.8%	61,535	4.7%
44-45 Retail trade	210	11.7%	130,405	9.9%
48-49 Transportation and warehousing	75	4.2%	51,325	3.9%
51 Information and cultural industries	45	2.5%	60,705	4.6%

52 Finance and insurance	210	11.7%	98,785	7.5%
53 Real estate and rental and leasing	35	1.9%	33,795	2.6%
54 Professional, scientific and technical services	90	5.0%	141,480	10.8%
55 Management of companies and enterprises	0	0.0%	2,750	0.2%
56 Administrative and support, waste management and remediation services	100	5.6%	74,005	5.6%
61 Educational services	165	9.2%	88,330	6.7%
62 Health care and social assistance	115	6.4%	116,975	8.9%
71 Arts, entertainment and recreation	30	1.7%	30,305	2.3%
72 Accommodation and food services	75	4.2%	83,380	6.4%
81 Other services (except public administration)	105	5.8%	65,595	5.0%
91 Public administration	85	4.7%	44,835	3.4%

<sup>4</sup> Based on 1997 North American Industry Classification System.

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Source: Statistics Canada, 2006 Census of Canada

**APPENDIX 3**  
**Highland Creek CS&F Inventory**

**1. Schools**

**Table 1a - Toronto District School Board**

Map Code	Name	Address	Capacity	FTE Enrolment (Oct. 31/10)	Utiliz. Rate	Portables
<b>Elementary</b>						
*PE 1	Centennial Road Jr. P.S.	271 Centennial Rd.	233	218	93.6%	2
*PE 2	Heron Park Jr. P.S.	280 Manse Rd.	305	279	91.5%	0
PE 3	West Hill P.S.	4550 Kingston Rd.	279	210	75.4%	0
PE4	Highland Creek P.S.	1410 Military Tr.	282	154	54.6%	0
<b>Elementary Totals:</b>			<b>1099</b>	<b>861</b>	<b>78.3%</b>	<b>2</b>
<b>Secondary</b>						
*PS 1	West Hill C.I.	350 Morningside Ave.	1413	1241.89	87.9%	0
*PS 2	Sir Oliver Mowat C.I.	5400 Lawrence Ave. East	1158	1165.26	100.6%	0
<b>Secondary Totals:</b>			<b>2571</b>	<b>2407.15</b>	<b>93.6%</b>	<b>0</b>

**Table 1b - Toronto Catholic District School Board**

Map Code	Name	Address	Capacity	Enrolment (2010-2011 ADE)	Utiliz. Rate	Portables
<b>Elementary</b>						
CE 1	St. Brendan	186 Centennial Rd.	432	562	130%	3
*CE2	St. Malachy	80 Bennet Rd.	191	315	165%	5
*CE3	St. Martin de Porres	230 Morningside Ave.	288	323	112%	4
*CE4	Cardinal Leger	600 Morrish Rd.	489	486	93%	3
<b>Elementary Totals:</b>			<b>1400</b>	<b>1686</b>	<b>120%</b>	<b>15</b>
<b>Secondary</b>						
*CS1	Pope John Paul II	685 Military Tr.	1074	1394	129%	13
<b>Secondary Totals:</b>			<b>1074</b>	<b>1394</b>	<b>129%</b>	<b>13</b>

\*School Facilities Located Outside Study Area, Serves Area Residents

## APPENDIX 3

### Highland Creek CS&F Inventory

**Table 2 – Child Care**

Map Code	Name/Location	Capacity	Details
C1	Sunshine Beechgrove Child Care 4679 Kingston Rd. – Ward 44	10 Infant 15 Toddler 76 Preschool 70 School Age Total 171 spaces	Non-Profit Licensed No Fee Subsidy Located in Commercial Plaza Waiting List
C2	Friendly Little Campus 2 63 Orchard Park Dr. – Ward 44	10 Infant 30 Toddler 64 Preschool Total 104 spaces	Commercial Licensed Fee Subsidy Available Located in Private Building Waiting List
C3	Absorbent Minds Montessori School 16 Old Kingston Rd. – Ward 44	10 Toddler 41 Preschool Total 51 spaces	Commercial Licensed No Fee Subsidy Located in Private Building
C4	Morningside Day Care Centre 292 Morningside Ave. – Ward 43	10 Toddler 32 Preschool Total 42 spaces	Commercial Licensed Fee Subsidy Available Located in Private Building
C5	East Scarborough – St. Martin De Porres 230 Morningside Ave. – Ward 43	36 Preschool Total 36 spaces	Non-Profit Licensed Fee Subsidy Available Located in St. Martin De Porres Catholic School
C6	Highland Creek Montessori Pre-School 226 Morrish Rd. – Ward 44	24 Preschool Total 24 spaces	Non-Profit Licensed No Fee Subsidy Located in Private Building
C7	King of Kings Montessori 4260 Lawrence Ave. E. – Ward 44	18 Preschool Total 18 spaces	Commercial Licensed No Fee Subsidy Located in Heron Park Baptist Church
C8	N'Sheemaehn 1255 Military Trail – Ward 44	10 Infant 10 Toddler 34 Preschool Total 54 spaces	Non-Profit Licensed Fee Subsidy Available Located in University of Toronto Scarborough Campus Waiting List

#### Total No. Licensed Spaces

Infant	-	30
Toddler	-	75
Preschool	-	325
School Age	-	70
<b>Total</b>	<b>-</b>	<b>500</b>

### APPENDIX 3

#### Highland Creek CS&F Inventory

**Table 3 – Public Libraries**

Map Code	Library/Description	Services/Programs	Type of Facilities
L1	<p>Highland Creek Neighbourhood Branch 3550 Ellesmere Road Ward 44</p> <p>Population Served: 24,020</p> <p>Facility Description: One storey, stand-alone, 7,000 s.f. (651 sq.m.)</p>	<p>Circulation (2010): 231,673 Collection Size: 43,744</p> <p><b>Collections:</b> -Audiobooks on CD -Large Print Collection</p> <p><b>Services:</b> -Book Drop – After Hours -Internet/Microsoft Office Workstations (7) -Seating (44) Equipment For People With Disabilities: -Computer With Screen Magnification Software (Zoomtext) -Magnifiers – Hand held or Stand</p> <p><b>Programs:</b> -Leading to Reading -Computer Training</p>	<p>Auditorium – 500 s.f. -Seating (24) -Lecture Style (50)</p> <p>Equipment – Chairs, Easel, Flip Chart Stand, Lectern Screen (Wall Mounted) and Tables</p> <p>Fire Code Occupancy – 60</p> <p>Wheelchair Access</p>
L2	<p>Morningside Neighbourhood Branch 4279 Lawrence Ave. East Ward 44</p> <p>Population Served: 25,632</p> <p>Facility Description: One storey, stand-alone, 7,000 s.f. (651 sq.m.)</p>	<p>Circulation (2010): 261,744 Collection Size: 43,647</p> <p><b>Collections:</b> Recreational and Children's French and Multi-lingual</p> <p><b>Services:</b> -Book Drop – After Hours -Internet/Microsoft Office Workstations (10) -Pedometer Lending -Quiet Study Room -Seating (70) -Service Ontario in Libraries -Computer with Screen Magnification Software (Zoomtext) -Reference and Readers' Services</p> <p><b>Programs:</b> -Book Clubs -Computer Training -Library Settlement Partnership -Reading Support Programs -Book Clubs -Youth Leadership</p>	<p>Art Gallery Space Kitchenette Meeting Room – 600 s.f. -Seating (45) -Classroom (24)</p> <p>Equipment- Chairs, Screen, Tables, DVD Player, Flip Chart Stand, LCD Projector</p>

*	<p>Cedarbrae District Branch 545 Markham Road Ward 43</p> <p>Population Served:46,580</p> <p>Facility Description: Two storey, stand-alone, 31,475 s.f. (2,927 sq. m.)</p>	<p>Circulation (2010): 699,048 Collection Size: 106,234</p> <p><b>Collections:</b> -Childrens' + Adult Multi-lingual -Childrens' + Adult French -Adult Literacy Materials -Audiobooks on CD -Large Print Collection</p> <p><b>Services:</b> -Book Drop – After Hours -CD Listening Centre -Digital Signage -Express Check-out and Check-in -Flat Panel TVs (2) -Internet Microsoft Office Workstations (66 – Includes Le@rning Centre) -Kidstop Early Literacy Centre -Outdoor Reading Area -Pedometer Lending -Proctoring -Public Phone (Inside) -Quiet Study Rooms (4) -Refreshment/Snack Vending Machine -Seating (75) -Teen Zone</p> <p><b>Equipment For People With Disabilities:</b> -Computer with Screen Magnification Software (Zoomtext) -Magnifiers – Hand held or Stand -Large Print Keyboard -Reading Light (Verilux)</p> <p><b>Programs:</b> -Book Club -Computer Training -English Can Be Fun -English as a Second Language -Kids @ Computers -Legal Aid Clinic -Reading Support Programs -Settlement and Education Partnerships in Toronto (SEPT) -Youth Advisory Group</p>	<p>Auditorium – 1,750 s.f. + Kitchen -Seating (40) -Lecture Style (60)</p> <p>Fire Code Occupancy (100)</p> <p>Equipment – Chairs, Tables, DVD Player, LCD Projector, Wireless Microphones, Alcove with Sink</p>

**\*Located Outside Study Area, Serves Area Residents**

**Table 4a: Community Centres, Arenas and Pools**

Map Code	Name/Address	Facilities	Programs
<b>Community Centres</b>			
**CR1	Heron Park Recreation Centre 292 Manse Road Ward 44	Double Gym Kitchen Multi-Purpose Room 1 Pre-school Room 1 Youth Lounge Staff Office 1 Snack Bar	Preschool Programs Children's Programs Youth Programs Adult Programs Seniors' Programs Summer Camps
**CR2	Tall Pines Community Centre 64 Rylander Blvd. East Ward 44	1 Meeting Room 1 Games Room 1 Staff Office	Preschool Programs Children's Programs Fitness Classes
**CR3	Port Union Recreation Centre 5450 Lawrence Ave. East Ward 44	Fitness Centre/Weight Room Saunas Gymnasium Senior's Lounge Meeting Room Staff Office Library	Seniors' Programs Preschool Programs Children's Programs Adult Programs Summer Camps Fitness Classes
*	West Rouge Community Centre 270 Rouge Hills Drive Ward 44	4 Multi-Purpose Rooms	Preschool Programs Summer Camps Fitness and Wellness Seniors' Programs
*	Curran Hall Community Centre 277 Orton Park Ward 43	1 Multi-Purpose Room 1 Small Kitchen 1 Staff Office	Preschool Programs Summer Camps Fitness and Wellness Arts and Heritage
<b>Arenas</b>			
**A1	Heron Park Arena 292 Manse Road	Double Ice Pad Indoor Arena/Dry Pad 8 Change Rooms	Hockey League (West Hill) Learn to Skate Recreation Skates
<b>Pools</b>			
**IP1	West Hill Indoor Pool 350 Morningside Ave.	25 metre Indoor Pool Located in West Hill CI	Swim Lessons Leisure Swims
**IP2	Mowat Indoor Pool 5400 Lawrence Ave. East	25 metre Indoor Pool Located in Sir Oliver Mowat CI	Swim Lessons Leisure + Lane Swims Aqua Fitness
**OP1	Heron Park Outdoor Pool 292 Manse Road	Outdoor Pool in Park	Leisure + Lane Swims Swim Lessons Aqua Fitness, Camp Swim

**\*Not Shown on Map, Located Outside Study Area, Serves Residents**

**\*\*Located Outside Study Area, Serves Residents**



**Table 4b: Parks Forestry & Recreation Catchment Analysis – Parks and Facilities**

<b>Park Name</b>	<b>Address</b>	<b>Site (ha)</b>	<b>Amenities</b>
Asterfield/Plumrose Blvd. Parkette	75 Asterfield Drive	0.01	
Bramber Woods Park	15 Bramber Road	2.25 ha	Playground
Centennial Creek		3.14	
Colonel Danforth Park	73 Colonel Danforth Trail	54.77	Washrooms
Conlins Park	75 Conlins Road	0.08	
Highland Creek Bowling	4670 Kingston Road	0.21	Lawn Bowling Greens
Highland Creek Parkette	6115 Kingston Road	0.12	
Megan Park	4667 Kingston Road	2.81	Baseball Diamond, Playground
Tanis Crescent Woodlot	30 Tanis Crescent	0.19	
Wanita Park	5 Meadowvale Road	2.89	2 Soccer Fields, Playground

### APPENDIX 3

#### Highland Creek CS&F Inventory

**Table 5 – Places of Worship**

Map Code	Name/Address	Facilities/Community Activities	Availability of Space/Room to Local Community
PW1	EPIPHANY LUTHERAN CHURCH 20 Old Kingston Rd.	Blood Donor Clinics Women's Group Seniors' Club Craft Circle	
PW2	MELVILLE PRESBYTERIAN CHURCH 70 Old Kingston Rd.	Vacation Bible School Day and Summer Camps Ladies' Fitness	
PW3	MASJID OMAR 4640 Kingston Rd.	No Information Available	
PW4	TORONTO MAHAVIHARA 4698 Kingston Rd.	Education Mediation Counseling Children's Programs	Program Space for Community Groups Hostel Accommodation for Out of Town Guests
PW5	HERON PARK BAPTIST CHURCH 4260 Lawrence Ave. East	Montessori Preschool Summer Camp Youth Drop-in Program	
PW6	WEST HILL UNITED CHURCH 62 Orchard Park Dr.	Book Study Group	
PW7	WORD OF LIFE CHURCH OF GOD 4637 Kingston Rd.	No Information Available	
PW8	ST DUNSTAN OF CANTEBURY 56 Lawson Rd.	No Information Available	Concert Venue (240 Seating Capacity) Banquet Facility (175 Seating Capacity)
PW9	BAITUL' AFIYAT MOSQUE 255 Old Kingston Rd.	No Information Available	
PW10	ST JOSEPH'S CHURCH 292 Old Kingston Rd.	Early Years Program (0-4 years) Meals on Wheels Seniors 50+ Club Youth Group	
PW11	SALVATION ARMY – WEST HILL 305 Morrish Rd.	Community Services Food and Clothing Distribution Centre	
PW12	CENTENNIAL ROUGE UNITED 6540 Kingston Rd.	No Information Available	

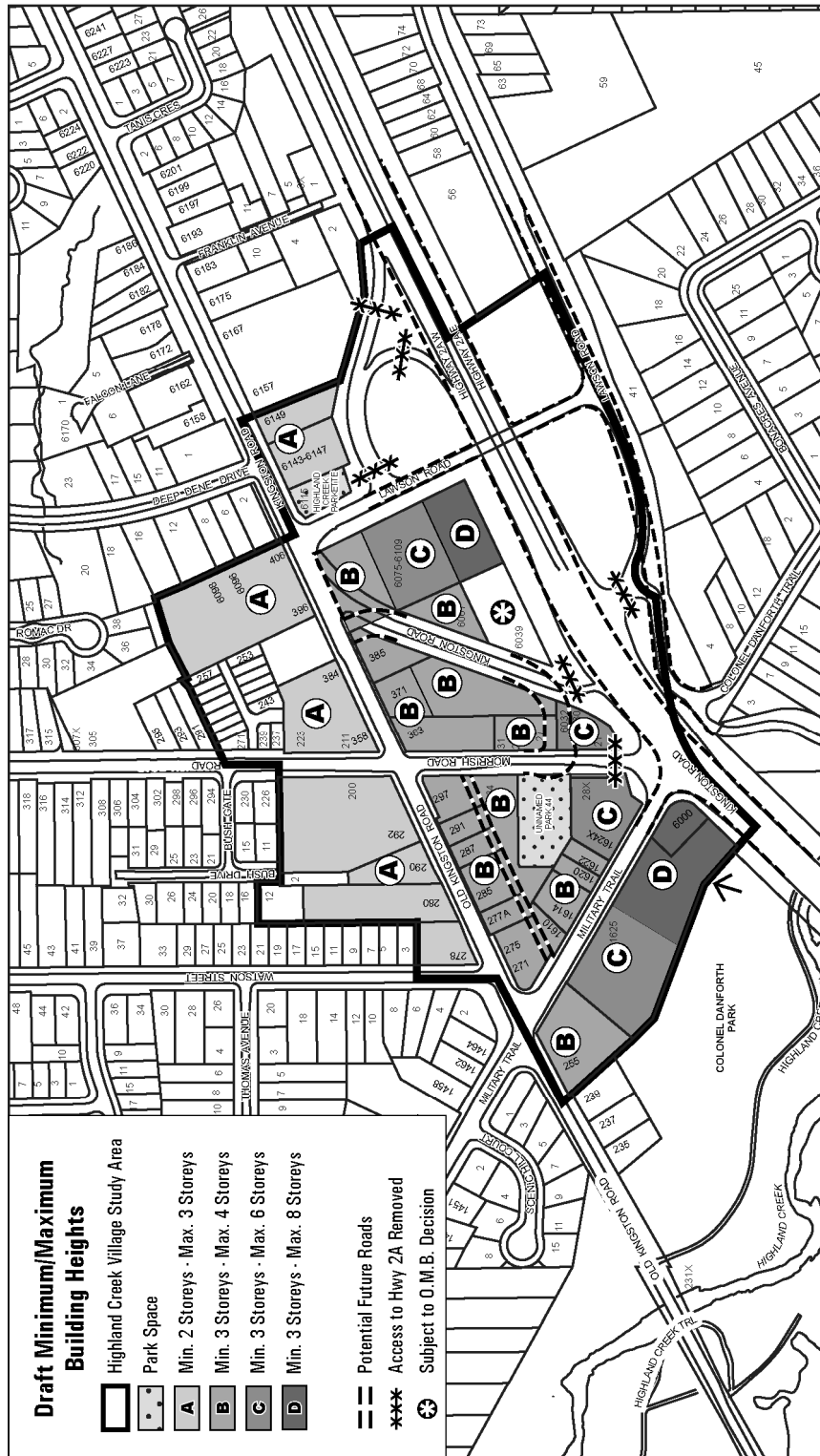
### APPENDIX 3

#### Highland Creek CS&F Inventory

**Table 6 – Human Services**

Map Code	Service Provider	Type of Service	User Groups
1	Scarborough Centre For Healthy Communities 4637 Kingston Road, Units 3 + 4 Contact: Nancy North (416) 847-4131	Adult Day Program Frail Elderly Program Home Maintenance Program Bereavement Group Caregiver Support and Information Sessions	Seniors with Alzheimer's Disabled Adults Adult Caregivers
2	Tony Stacey Centre for Veterans Care 59 Lawson Road (416) 284-9235	Long Term Care Facility	Veterans + Spouses
3	Ehatare Retirement and Nursing Home 40 Old Kingston Road Contact: Ruth McFarlane (416) 284-0828	Nursing Home (32 Bed Facility)  Retirement Home (76 Private rooms + 12 Double Rooms)	Estonian Seniors
4	Salvation Army 305 Morrish Road Contact: Linda Blake (416) 282-5333	Seniors Services Seniors Recreation Programs	Seniors
5	Meta Centre 371 Old Kingston Road (416) 736-0199	Day Services Including: Recreation + Leisure Work Experience Community Integration Centre-Based Activities	Adults and Children With Developmental Disabilities
6	DLC Residential Services 344A Morningside Avenue Contact: Sherry Izzett (416) 281-1147	Group Home (Total Capacity 36) Focus on communication, behavior analysis, life and social skills, training, community integration and family inclusion	Children, Youth and Transitional Aged Youth 6-25 years, with developmental disabilities, autism, and dual diagnosis, must be ambulatory
7	Salvation Army 305 Morrish Road Contact: Linda Blake (416) 282-2987	Food and Clothing Bank	All

## Attachment 10: Minimum and Maximum Building Height Map



## Attachment 11: Proposed Concept Road Changes

