

Fig. 8. Orfus Road Streetscape - Typical Section

### 3.3 EXISTING COLLECTOR STREET (ORFUS ROAD)

Orfus Road is a higher volume pedestrian street in the Dufferin Street Secondary Plan Area and is identified as a Major On Street Bicycle Route in the Lawrence Allen Secondary Plan (Map 32-10 Bicycle Plan). The existing buildings beyond the secondary plan boundary are setback between 5.0 to 7.0m from the public right-of-way. New buildings in the Secondary Plan Area should relate to and provide a suitable transition towards Dufferin Street. The streetscape should support existing and future development, and encourage greater pedestrian and cycling activity.

#### Orfus Road Guidelines

- R17** Orfus Road should have a consistent 20.0m public right-of-way.
- R18** Provide a continuous 3.7m pedestrian boulevard on each side on the street.
- R19** Provide a dedicated and marked bike lane on each side of the street.

- R20** Street trees should be planted at the back of sidewalk with either a trench or concrete bridge detail.
- R21** Coordinate below grade utilities to ensure proper tree growth and reduce visual clutter.
- R22** Introduce new street lighting and furnishings.
- R23** Commercial boulevard parking is discouraged.

#### Development Guidelines

- R24** All new buildings should be set back 5.0m from the public right-of-way, with either a paved retail frontage or a residential landscape frontage.
- R25** Setbacks will be landscaped to compliment and coordinate with the public streetscape, enhance pedestrian amenity and provide a setting for commercial and residential ground floor uses.

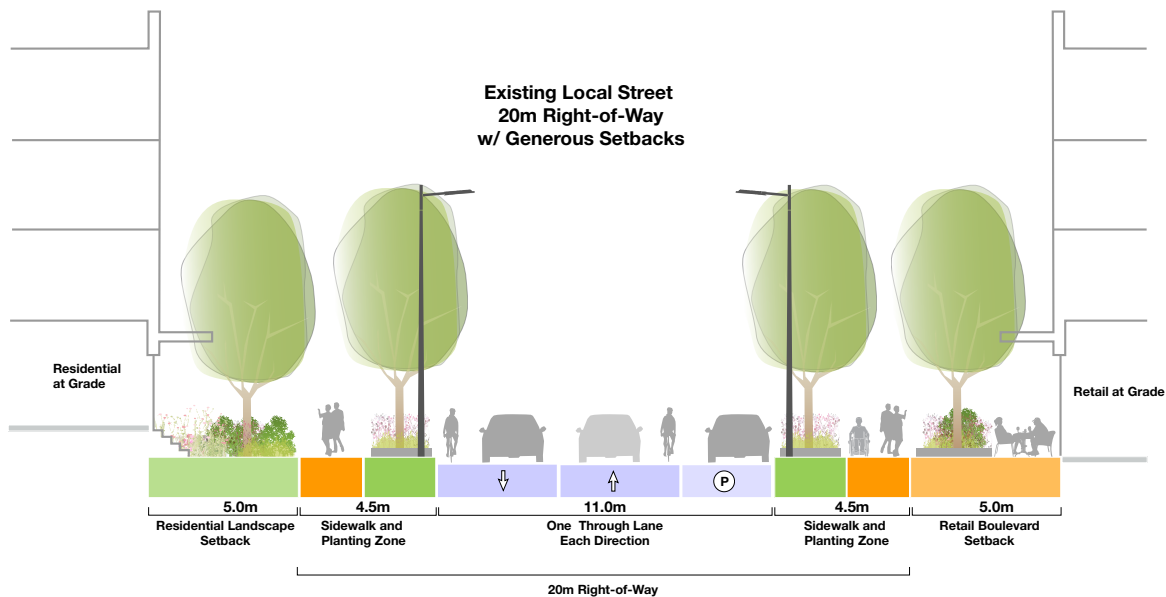


Fig. 9. Existing Local Street Streetscape - Typical Section  
New Local Street Streetscape - Typical Section (Option 1)

### 3.4 EXISTING LOCAL STREETS (SAMOR AND APEX ROADS) AND NEW LOCAL STREETS (OPTION 1)

Both Samor Road and Apex Road are local streets that have significant segments within the Dufferin Street Secondary Plan Area. Both streets have minimum sidewalks and wide vehicular travelways, little planting, and buildings broadly set back from the public right-of-way. These existing streets should maintain the broad setbacks to better relate new development with existing, and rearrange the space available in the public right-of-way to provide greening opportunities and improve pedestrian amenities.

#### Local Street Guidelines

- R26** The existing Samor Road and Apex Road, and all new local streets should have a consistent 20.0m public right of way.
- R27** Provide a continuous 4.5m boulevard on each side of the street.

- R28** Street trees should be planted with open planter details.
- R29** Coordinate below grade utilities to ensure proper tree growth and reduce visual clutter.
- R30** Introduce new street lighting and furnishings.
- R31** Commercial boulevard parking is discouraged.

#### Development Guidelines

- R32** On Samor and Apex Roads, all new buildings should be set back 5.0m from the public right-of-way. For new local streets, all new buildings should set back 3.0m.
- R33** Setbacks will be landscaped to compliment and coordinate with the public streetscape, enhance pedestrian amenity and provide a setting for commercial and residential ground floor uses.

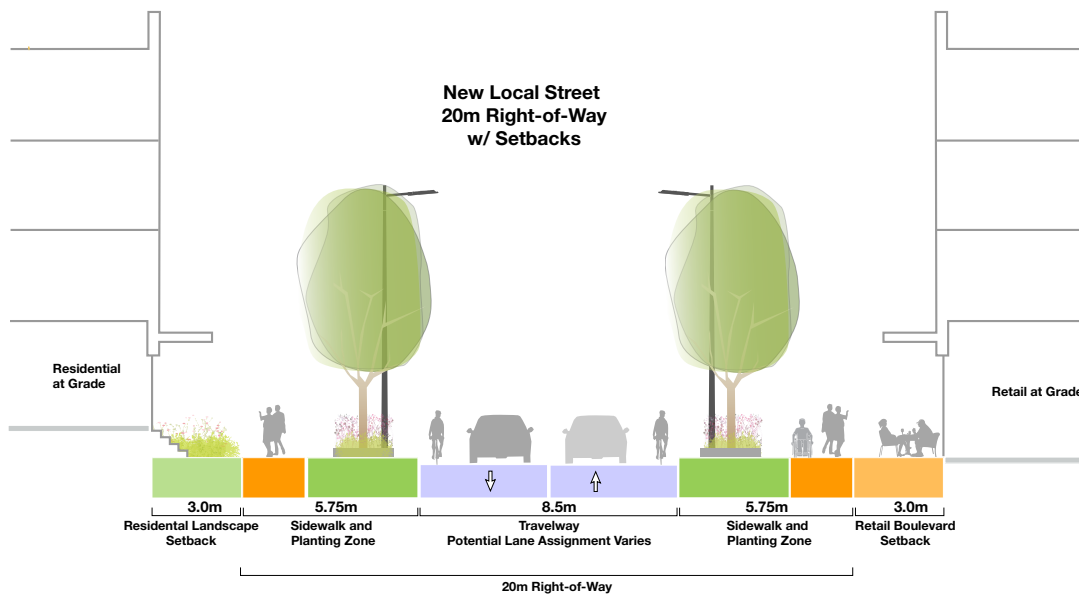


Fig. 10. New Local Street Streetscape - Typical Section (Option 2)

### 3.5 NEW LOCAL STREETS (OPTION 2)

A second option for new local streets in the large blocks is to have a more narrow travelway and assign additional space to the sidewalk and planting zone. This would also require a smaller building setback on both sides of the street, creating a more contained streetscape.

#### Local Street Guidelines

- R34** All new local streets should have a consistent 20.0m public right of way.
- R35** Provide a continuous 5.75m boulevard on each side of the street.
- R36** Street trees should be planted with open planter details.

Note: Street sections subject to further refinement through the Block Master Plan and Functional Design process.

- R37** Coordinate below grade utilities to ensure proper tree growth and reduce visual clutter.
- R38** Introduce new street lighting and furnishings.
- R39** Commercial boulevard parking is discouraged.

#### Development Guidelines

- R40** All new buildings should be set back 3.0m from the public right-of-way, with either a paved retail frontage or a residential landscape frontage.
- R41** Setbacks will be landscaped to compliment and coordinate with the public streetscape, enhance pedestrian amenity and provide a setting for commercial and residential ground floor uses.

# 4.0 PARKS AND OPEN SPACES

## 4.1 PARKS AND OPEN SPACES

There are currently no parks or landscaped open spaces for the public within the Dufferin Street Secondary Plan Area. Redevelopment and intensification presents the opportunity to contribute to a range of high-quality parks and public spaces that provide a setting for civic and community life by enhancing the identity and character of an area.

New public parks, promenades, streetscapes, and other publicly accessible open spaces should be designed to form a coherent and accessible green space system. These spaces should have individual character and functions but contribute to the whole open space system. New public spaces should be high quality environments that support a wide range of roles, allow for a variety of pedestrian uses, and are distinct yet visually connected through the use of contemporary materials and details.

The greening strategy and parks and open space framework includes the following key components:

**1) Parks.** The entire Dufferin Street Secondary Plan Area falls into a Parkland Acquisition Priority Area, as identified in the Official Plan, in which a greater parkland dedication requirement (15%) is in place for higher density residential and mixed-use redevelopments. Parks are required in each of the larger blocks.

**2) Privately Owned Publicly Accessible Spaces (POPS).** Privately owned publicly accessible spaces, or “PoPs” for short, are a specific type of open space which the public are invited to use, but remain privately owned and maintained. They are intended to compliment the public parks and open space as part of the public realm network. PoPs such as urban plazas, courtyard and mid-block connections are recommended within the Dufferin Street Secondary Plan Area.

**2a) Urban Plazas.** Urban Plazas are small, privately owned, publicly accessible areas with predominantly hardscape character. Urban Plazas are to be located at key nodes where pedestrian and retail activity is anticipated to be most intense as shown on the District Structure Plan. Urban Plazas are required in each of the large blocks.

**2b) Courtyards.** Feedback received by the public and local stakeholders through the Dufferin Street Avenue Study and input from the City of Toronto Design Review Panel encouraged a different arrangement of built form and open space. Courtyard buildings, where a u-shaped building is open to a street, are recommended as a means to introduce more greening and provide relief along Dufferin Street from the typical “street-wall urbanism” promoted along the corridor. The courtyards would be privately owned and in most cases semi-publicly accessible. Courtyard buildings are encouraged as part of new development, particularly in the large blocks.

**3a) Streetscape Greening.** Tree planting is recommended to line both sides of all existing and new proposed streets to enhance the character of the public realm and Dufferin Street Promenade. More details regarding streetscape improvements are illustrated and described from page 20 to page 28 of the Dufferin Street Urban Design Guidelines.

**3b) Landscape Gateway.** Dufferin Street is one of the major exits from Highway 401, and is the first urban experience of the City that many motorists have as they head toward Yorkdale Shopping Centre or downtown destinations. As such, it should be considered as a ‘higher order’ gateway and given an appropriate landscape treatment in recognition of this. The landscape gateway will occupy the lands between the ramps and required setbacks for the blocks at the northern end of the Dufferin Street Secondary Plan Area. The gateway could include special planted features, landmark signage or large public art installations.

**4) Public Art.** Public Art is encouraged within the Dufferin Street Secondary Plan Area to contribute to local identity and enhance the character of the Dufferin Street public realm. Public art contribution is recommended for development proposals within the larger blocks. Public art is encouraged for landscaped open space and higher order street boulevards such as the Dufferin Street Promenade.



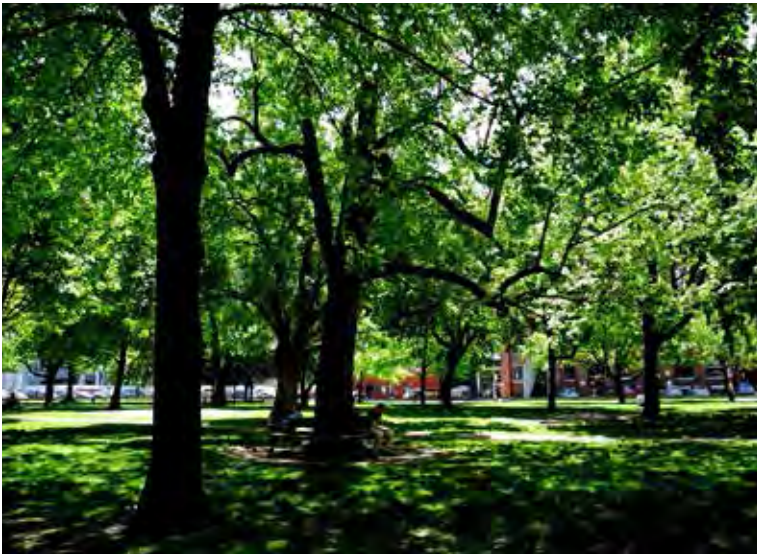
Landscape Gateway



Tree Lined Promenade



Urban Plazas



Parks and Parkettes



Urban Plazas



Courtyards



Mid Block Connections

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## 4.2 PARKS GUIDELINES

- R42** Prior to the redevelopment of any parcel on a large block (1,6,7,8,14) the proponent should submit a conceptual Block Plan for the comprehensive redevelopment of the entire block. Proposed parkland provided through dedication requirements (with exception of block 14) should be located at the mutual property line, in order to permit its eventual expansion to the second abutting parcel.
- R43** Parks should be located on each of the larger blocks (with the exception of Yorkdale Shopping Centre), and on the present site of the McAdam turning loop.
- R44** All parks will front onto public streets (Official Plan 3.1.1 (18) ).
- R45** Parks should be visible and accessible from adjacent public streets, and be of a usable shape, topography and size that reflect their intended use (Official Plan 3.2.3 (8 b-c)).
- R46** Public parks and open spaces should be considered as a network, provide appropriate space for recreational needs and ensure good visibility, access and safety.
- R47** Parks should provide a high quality design, be sustainable and provide a sense of place for residents.
- R48** On smaller lots, where the size or configuration of the required parkland conveyance would prove unusable or undesirable, developments should provide cash-in-lieu funds, as directed by the Toronto Municipal Code - Development of Land, Chapter 415, Article IV, to contribute toward the development of parks in the Secondary Plan Area.
- R49** New buildings should be positioned to positively define the shape and function of the public park.
- R50** Ambiguous, unprogrammed or residual orphaned parkland spaces should be avoided in Park location and design.

## 4.3A POPS GUIDELINES - URBAN PLAZAS

- R51** Urban plazas are recommended at the following locations: Dufferin Street and Orfus Road, southwest corner (Block 6); Dufferin Street and Lawrence Avenue West, northwest corner (Block 8); and Yorkdale Mall (Block 14).
- R52** Urban plazas and publicly accessible privately owned public spaces are subject to the City of Toronto's guidelines for Privately Owned Publicly Accessible Space.
- R53** New buildings should be positioned to positively define the shape and function of urban plazas.
- R54** A range of uses and amenities is encouraged for the urban plazas.

## 4.3B POPS GUIDELINES - COURTYARDS

- R55** Courtyard building arrangements facing Dufferin Street are encouraged to enhance the Dufferin Street Promenade.
- R56** The development of publicly accessible open spaces are encouraged at ground level to add to the public realm network. Where feasible, design these open spaces in relation to local serving retail uses such as cafes and to the public open space network.

## 4.3C POPS GUIDELINES - MID BLOCK CONNECTIONS

- R57** Safe and comfortable mid block connections are encouraged through development blocks, through active fronting uses, building setbacks and pedestrian scaled lighting.

## 4.4 STREETSCAPE GUIDELINES

- R58** The Dufferin Landscape Gateway will establish a green vegetated entrance to the City from Highway 401 and as one approaches from the north. The gateway will occupy the lands between the Highway 401 ramps, the minimum mandatory 14.0m setback from the Dufferin off-ramp, and the minimum 7.0m setback for the portions of Blocks 1 and 14 as identified.
- R59** A 5.0m boulevard setback is recommended for larger blocks on the west side of Dufferin Street to allow for the Dufferin Street Promenade as illustrated in the Dufferin Street Future Sectional Perspective. The promenade consists of a generous public walkway, double row tree planting to allow for a continuous tree canopy and space for outdoor spill out retail uses to animate the street edge. No below grade uses or structures are recommended within the setback to ensure adequate space and soil volumes for tree growth.
- R60** Street trees should be planted on both sides of all streets in the Dufferin Street Secondary Plan Area.

## 4.5 PUBLIC ART GUIDELINES

- R61** Public art is encouraged for parks, private open spaces and street boulevards.
- R62** The provision of public art will be carried out in compliance with the City of Toronto Percent for Public Art Program Guidelines. Implementation and placement finding should be coordinated through a Public Art Plan for the redevelopment block.

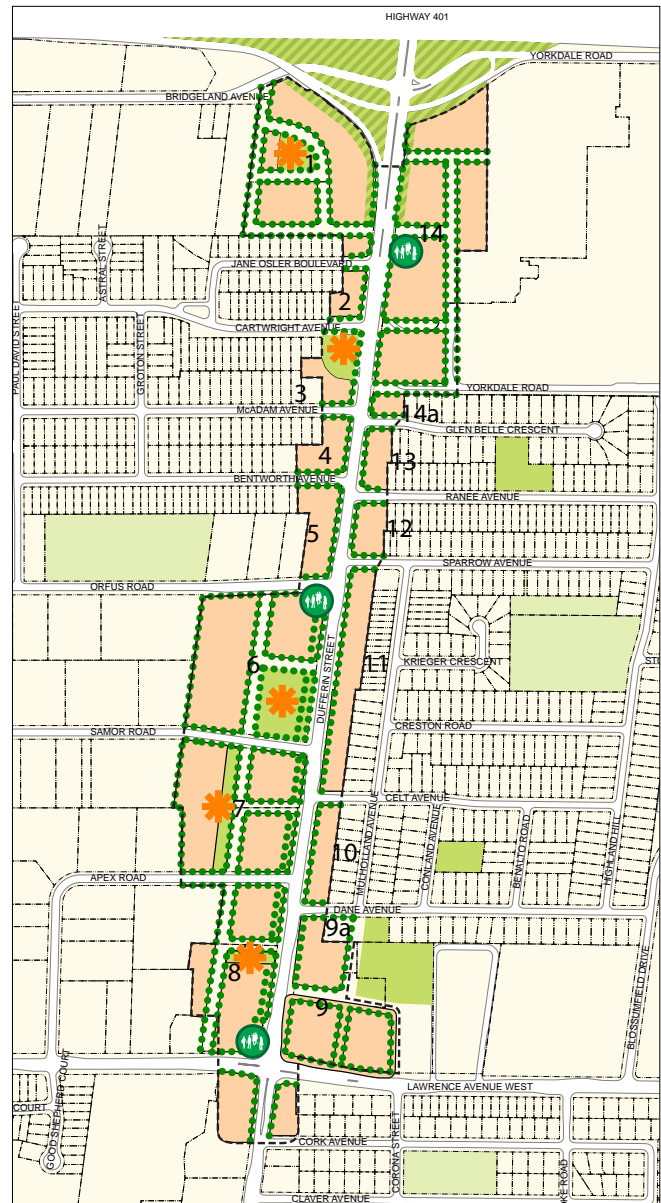


Fig. 11. Parks and Open Space Framework Plan



# 5.0 BUILT FORM - GENERAL

## 5.1 GRADE RELATED USES

The Official Plan encourages pedestrian-oriented retail uses at grade along *Avenues* (see Section 3.5.3). The Dufferin Street Secondary Plan supports this policy, but also acknowledges that Dufferin Street presently lacks a pedestrian-oriented retail character and is adjacent to an existing regional shopping centre, which may generate competition and challenge the health of retail along Dufferin Street. For these reasons, retail at grade is recommended only at key nodes.

Auto-oriented and large format retail uses are discouraged, in keeping with the Official Plan policy for *Avenues* and with consideration for mitigating negative transportation impacts.

### Guidelines and Standards

- R63** Mandatory retail at grade is recommended to be applied only to selective sites within the Dufferin Street Secondary Plan Area. At all other sites, retail will be encouraged at grade, but not required.
- R64** In areas where retail is not secured, provide other ground floor uses with transparent windows and doors to provide outlook and animation onto the street edge.

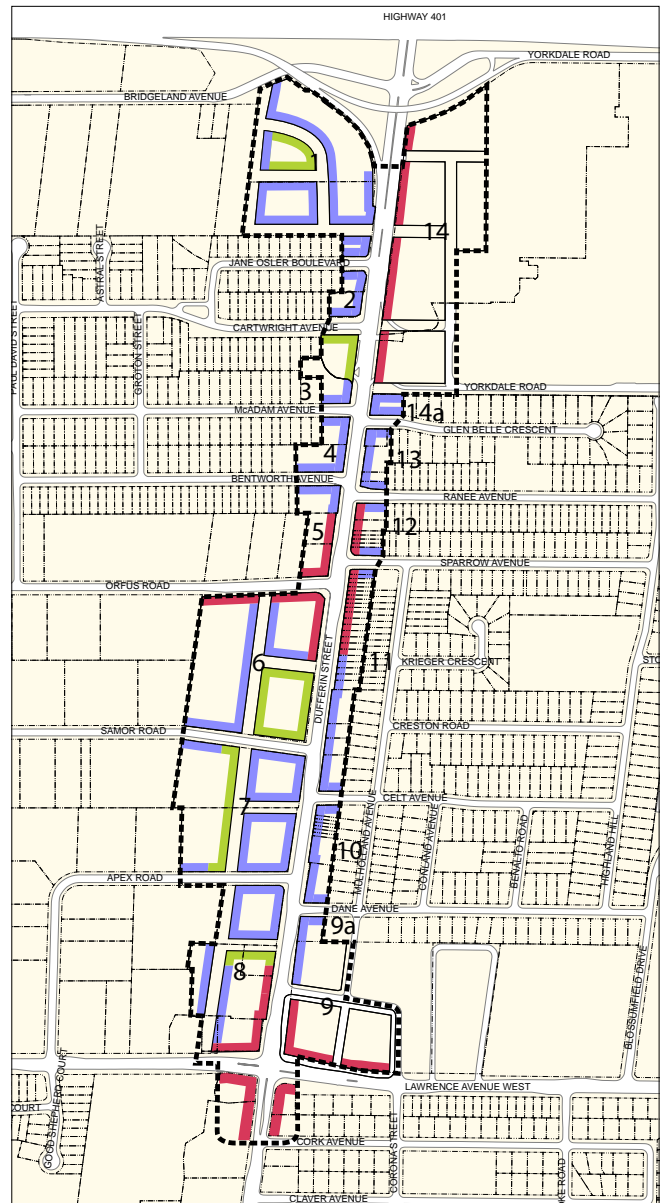


Fig. 12. Grade Related Uses

- Retail at Grade: Mandatory
- Retail at Grade: Encouraged but not Required
- Park Frontage
- Secondary Plan Boundary



## 5.2 SETBACKS

Setbacks improve the civic and pedestrian experience along street and open space edges. They also reinforce the goals of the Transportation Master Plan, which encourages walking and cycling. Landscaped setbacks will enhance retail uses, allowing shops and patio space to flourish. Landscape setbacks will balance the buffering of residential uses at grade, offering privacy and separation from the street, while still supporting the street edge.

Where grade-related retail is developed, setbacks will take on a hardscape character, extending the sidewalk, adding a second row of trees and allowing room for cafe patios and other retail amenity space. Where residential uses occur at grade, their character will be landscaped in nature. Building projections, such as small balconies, awnings, canopies and overhangs may extend into the setback area.

### Guidelines and Standards

- R67** Provide a minimum 3.0m setback for all shallow blocks fronting on Dufferin Street.
- R68** Provide a minimum 3.0m setback for all new development on local streets, existing or proposed.
- R69** Provide a 5.0m setback on all larger blocks that front on Dufferin Street between Lawrence Avenue West and Orfus Road and all existing east-west local streets (Orfus Road, Samor Road and Apex Road).
- R70** In Block 1, provide a minimum 14.0m setback adjacent to the Dufferin Street off-ramp (as required by the Ministry of Transportation).
- R71** Provide a minimum 7.0m setback to implement the Dufferin Landscape Gateway on Blocks 1 and 14 at the northern end of the Secondary Plan Area.

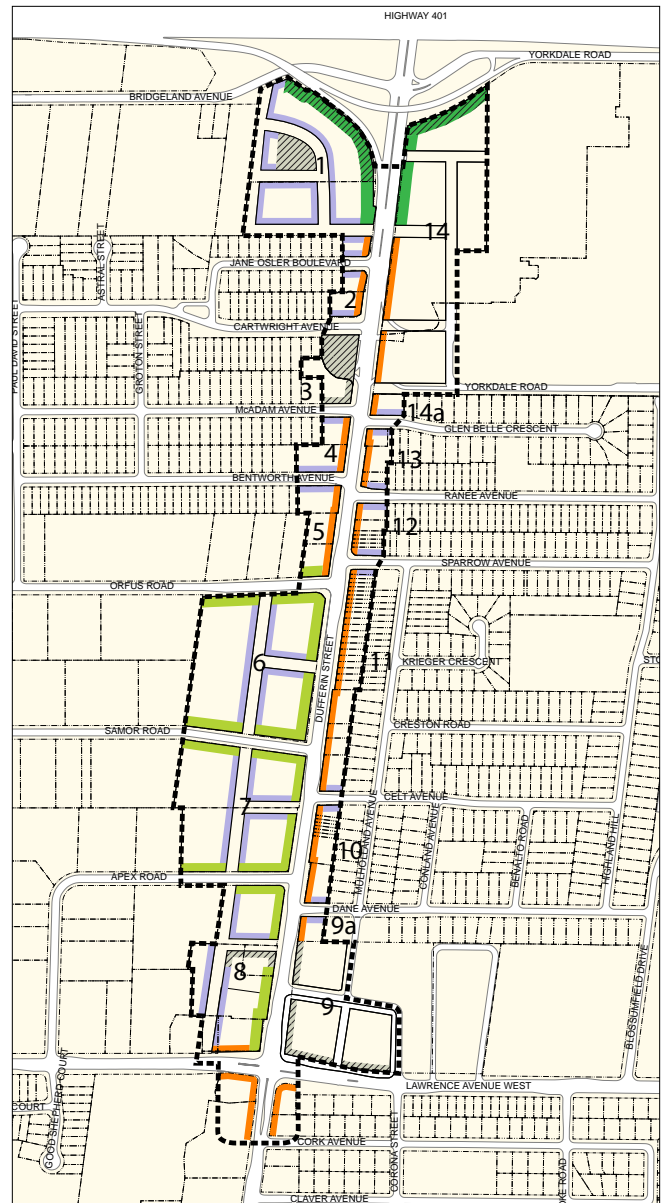
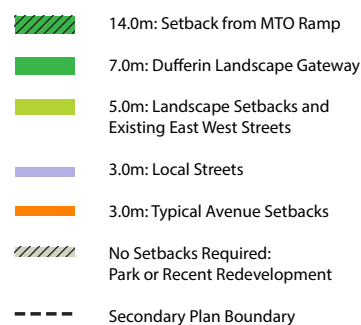


Fig. 13. Recommended Mandatory Minimum Setbacks



\* Not to Scale: setbacks illustrated are conceptual only

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## 5.3 BUILDING TYPES

Good urban places are composed of many buildings, varied in type and size.

Built form analyses conducted through the Dufferin Street Avenue Study conclude that the policy context, guiding principles, and direction received from the public consultation activities can be satisfied if:

- mid-rise development is the primary form of intensification,
- tall buildings are sited strategically at the northern and southern extents of the Dufferin Street Secondary Plan Area, and
- new development is encouraged to be concentrated towards Dufferin Street, transitioning to lower development closer to the adjacent *Neighbourhoods and Employment Areas*.

This pattern of built form is entirely consistent with the overall study objectives of having substantial built up edges and mixed uses facing Dufferin Street to give shape and a sense of enclosure as well as reinforcing the pedestrian realm of the main street.

### Guidelines and Standards

- R72** Mid-rise buildings will define the Dufferin Street frontages of the redevelopment blocks and provide a transition towards the new low-rise buildings adjacent to the existing residential neighbourhoods.
- R73** The differing lot depths on Dufferin Street will result in midrise buildings of asymmetrical height—taller ones on the west and shorter ones on the east. However, they will be unified by a common streetwall height of 20.0m maximum.

**R74** Courtyard buildings of mid-rise height—those that have an open end towards a public street or network—are encouraged along Dufferin Street in places where ground level retail is not required to further the landscape open space character along the corridor.

**R75** Tall buildings will be limited to the northern and southern extents of the Dufferin Street Secondary Plan Area, acting as gateways and separated a considerable distance from the existing low-rise buildings.

**R76** Townhouses or lowrise apartments are encouraged to create a finer residential scale within the larger blocks and provide transition from the taller, more intense tall and midrise building types that are massed towards Dufferin Street.

**R77** All new mid-rise buildings will be informed by the City of Toronto Mid Rise Building Performance Standards.

**R78** All new tall buildings will be informed by the City of Toronto Tall Building Design Guidelines.

**R79** All new townhouse development will be informed by the City of Toronto Infill Townhouse Urban Design Guidelines.



Examples of Tall Buildings



Examples of Mid-Rise Buildings along Avenues



Example of Mid-Rise Buildings along Avenues

Example of Townhouse Buildings



Examples of Townhouse Buildings

Examples of Courtyard Building

## 5.4 HEIGHTS

The City of Toronto's Avenues and Mid-rise Buildings Study defines mid-rise buildings as being between three storeys and a height equivalent to the width of the right-of-way on which the building fronts. In the Dufferin Street Secondary Plan Area, this means minimum heights of 10.5m and maximum heights of 20.0m, 27.0m, and 30.0m depending on the width of the street adjacent to the site.

Tall buildings are only permitted at specific sites at the north and south ends of the Dufferin Street Secondary Plan Area. This:

- Reinforces these sites as nodes, at the intersection of major east-west movement corridors (Highway 401 and Lawrence Avenue West), as well as their closer proximity to subway stations; and
- Recognizes the complexity of redeveloping Block 1, due to required setbacks, roads and parkland conveyances, a density that could be achieved with mid-rise buildings alone on the other large blocks cannot be achieved without tall buildings on this block.

Building heights are restricted to 45.72m (Block 1) and 60.96m (Block 8) pursuant to Schedule 'D' of North York Bylaw 7625, which relates to the flight paths of Downsview Airport. In the event that airport operations change such that those flight paths may be altered, the maximum heights of tall buildings in the Avenue will be reviewed.

Similarly, the 15.0m maximum height for Block 14 reflects the 15.24m restriction for that site contained within Schedule 'D' of North York Bylaw 7625. It is also roughly consistent with the height of the existing Yorkdale shopping center.



Tall Buildings are recommended for the special gateways



Mid-Rise Buildings are recommended for the majority of the Dufferin Street Secondary Plan Area



Townhouses are appropriate for transition towards the *Neighbourhoods and Employment Areas*

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## Guidelines and Standards

**R80** Buildings with greater heights are recommended closer to the Dufferin Street frontage. Buildings of lower height are recommended for transition to the rear of the larger and deeper blocks.

**R81** Mid-rise buildings are appropriate for the entire Dufferin Street Secondary Plan Area, with the exception of Block 14. The maximum height of mid-rise buildings will vary depending on the width of the adjacent right-of-way. They should be:

- 30.0m (approximately nine storeys) on Dufferin Street north of Lawrence Avenue West and 27.0m (approximately 8 storeys) south of Lawrence Avenue West. This maximum height will extend around the corners, to a distance of 30.0m (the length of a typical midrise block) where the building will step down to meet the 6 storey midrise height of the local side street.
- 20.0m (approximately 6 storeys) for buildings fronting onto side streets, existing and proposed, which have a 20.0m right-of-way.

**R82** The minimum height for all buildings will be 10.5m (approximately 3 storeys).

**R83** A minimum 4.5m ground floor height should be provided to facilitate retail uses at grade and provide sufficient clearance for loading areas.

**R84** Tall buildings are only recommended for the northern and southern ends of the Dufferin Street Secondary Plan, where they act as gateways to the Dufferin Street Secondary Plan Area. The location for Tall Building Placement is illustrated in the Locations for Tall Buildings Figure 14.

**R85** Where Tall Buildings are permitted, Tall buildings should transition down in height toward lower scale neighborhood and employments areas.

**R86** In all cases, the bases of tall buildings will be no greater than 20.0m (approximately 6 storeys) in height and no less than 10.5m (3 storeys).

**R87** Tall buildings will be evaluated against the City of Toronto's Tall Building Design Guidelines.