




Fig. 14. Locations for Tall Buildings

-  Existing Tall Buildings
-  Area within which New Tall Buildings are permitted
-  Secondary Plan Boundary

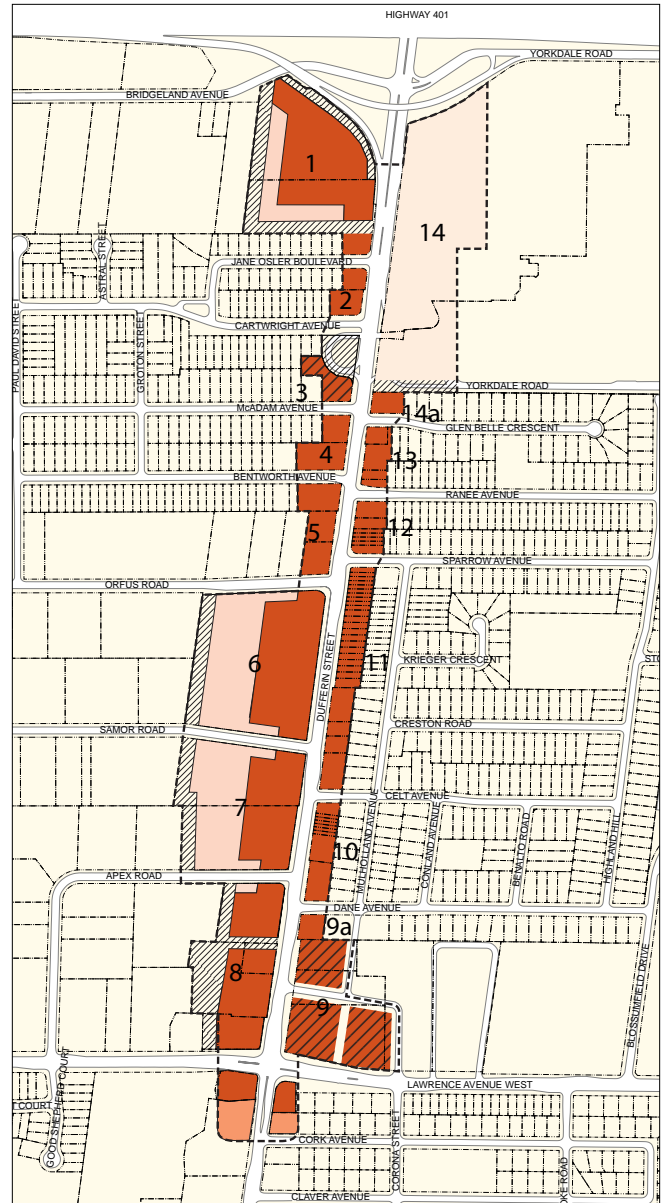









Fig. 15. Maximum Building Height (excluding Tall Buildings)

-  Existing Mid-Rise or Base Buildings: 30m
-  30m (9 storeys)
-  27m (8 storeys)
-  20m (6 storeys)
-  15m (3 storeys)
-  Defined No Build Zones: i.e., Buffers, Future Streets or Parks
-  Secondary Plan Boundary

---

## 5.5 STREET PROPORTION

Official Plan policies on Built Form (sec 3.1.2) state that new development will be massed to fit harmoniously into its planned context and limit its impact on adjacent streets and open spaces through:

- massing of new buildings to define the edge of the street and open spaces at good proportion and,
- limiting shadowing of and uncomfortable wind conditions on adjacent streets and open spaces to allow streets and open spaces to be comfortable pedestrian routes.

Adherence to front angular planes, defined in the Avenues and Mid-Rise Buildings Performance Standards, result in building envelopes that ensure a street wall height is in proportion with the right of way, allow for 5 hours of sunlight access on the opposite sidewalk and skyview from the sidewalk.

A streetwall height of 6 stories is recommended for Dufferin Street to allow for a consistent street wall height along the corridor in defining the character of the Dufferin Street promenade.

### Front Angular Plane Guidelines

- R88** All new buildings in the Dufferin Street Secondary Plan Area will be subject to front angular plane performance standards. These planes will apply to both mid-rise buildings and the bases of tall buildings, as well as to establish the height and location of tall buildings.
- R89** Front angular planes are to be extended at a 45 degree angle from the front property line, beginning at a height 80 percent of the width of the adjacent right-of-way.
- R90** Development facing Dufferin Street should adhere to front angular planes and a street wall height of 6 storeys in maintaining good street proportion, sunlight access, skyview and a consistent streetwall character on Dufferin Street.
- R91** Developments facing existing collector and local 20m streets should adhere to front angular planes and a street wall height of 4 storeys in maintaining good street proportion, sunlight access, skyview and a consistent streetwall character on local street.

Fig. 16. Street Proportion for Dufferin Street, illustrating the maximum streetwall height and setbacks to allow for sunlight and skyview on Dufferin Street

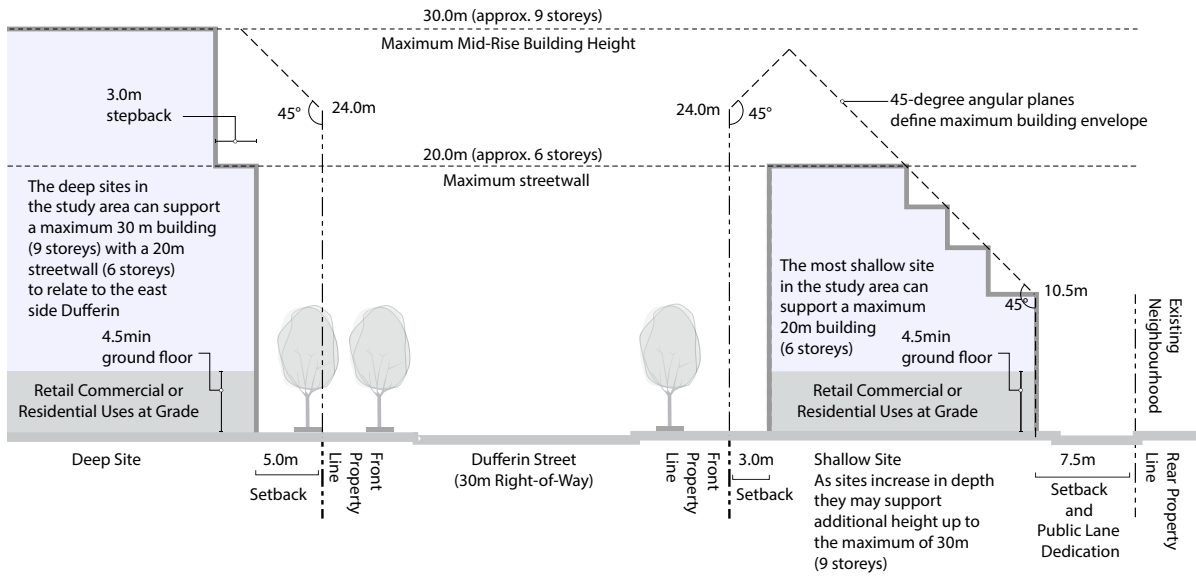
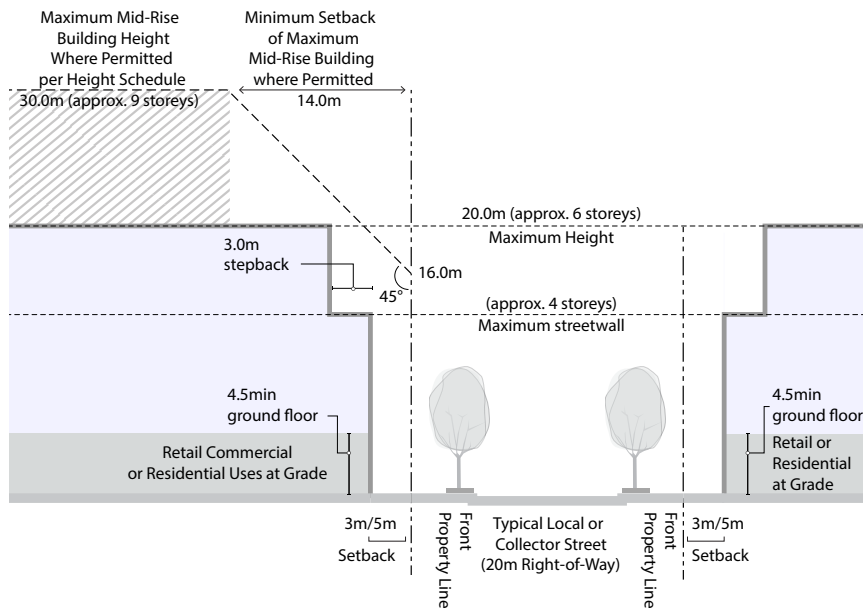


Fig. 17. Street Proportion for Blocks 6, 7 and 8, along existing and new streets



## 5.6 STEPBACKS

Good urban streets and places require buildings along their edges of sufficient height and continuity to provide a sense of enclosure, but not be so tall as to over-power or over-shadow them. Stepbacks are portions of buildings that are recessed from the primary street wall. Stepbacks help to relate the scale of larger buildings to the scale of the pedestrian and allow newer buildings to fit with an older building fabric.

To ensure the appearance of harmony along the street, the recommended maximum streetwall height is 20.0m for all sites and new development that address Dufferin Street. While building height may vary, a consistent streetwall at 20.0m will do much to visually tie both sides of Dufferin Street together.

On smaller 20.0m rights-of-way, a stepback should be provided at 16.0m (80 per cent of the right-of-way width), as recommended by the Avenues and Mid-Rise Buildings Performance Standards.

### Guidelines and Standards

**R92** All mid-rise buildings that front Dufferin Street should provide a stepback of 3.0m at a height of no more than 20.0m (approximately six storeys). This step back applies to both the 30.0m and 27.0m right-of-way segments of the Dufferin Street Secondary Plan Area.

**R93** All mid-rise buildings that front 20.0m rights-of-way should provide a step back of 3.0m at a height of no more than 16.0m (4 to 5 storeys), and no less than 10.5m (approximately three storeys).



Mid-rise development with streetwall stepbacks and facade articulation to reduce the perception of building mass to the pedestrian



6 storey street wall with stepback to tall building

---

## 5.7 TRANSITIONS IN SCALE

Official Plan policies (section 4.5) state that buildings will be located and massed to provide transition between areas of different development intensities and scale. Transition will be provided through setbacks and stepping down of heights, particularly towards lower scale *Neighbourhoods*. Buildings will also be located and massed to adequately limit shadow impacts on *Neighbourhoods*.

Where new development backs onto existing *Neighbourhoods*, the rear angular planes and setbacks outlined in the Avenues and MidRise Building Performance Standards will serve as the required transitions in scale to achieve adequate light and privacy to the *Neighbourhoods*.

In some of the larger blocks (Blocks 1), rear angular planes are also used to determine the location of tall buildings (taller than the height suggested by the street right-of-way width), and mid-rise buildings. The intent of this approach is to avoid overwhelming the narrower rights-of-way and adjacent *Neighbourhoods* with buildings of substantial height.

### Rear Angular Plane Guidelines

- R94** All new buildings in the Dufferin Street Secondary Plan Area will be subject to rear angular plane performance standards as defined in the Avenues and Mid-Rise Building Performance Standards. These planes will apply to both mid-rise buildings and the bases of tall buildings, as well as to establish the height and location of tall buildings.
- R95** Rear angular planes for deep properties (over 44.6m in depth) backing onto *Neighbourhoods* will extend at a 45 degree angle from the rear property line. A 7.5m rear setback from the residential zone is also required.
- R96** Rear angular planes for shallow properties backing onto *Neighbourhoods*, will extend at a 45 degree angle, beginning 10.5m above the ground, 7.5m from the rear property line.

Fig. 18. Transition in scale to adjacent *Neighbourhood* for shallow sites on Dufferin Street

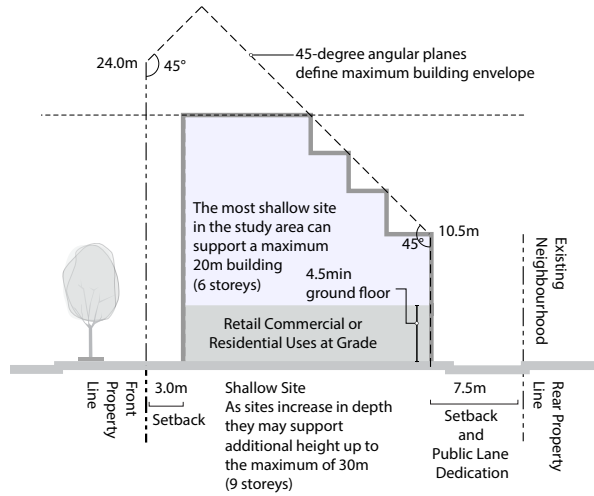
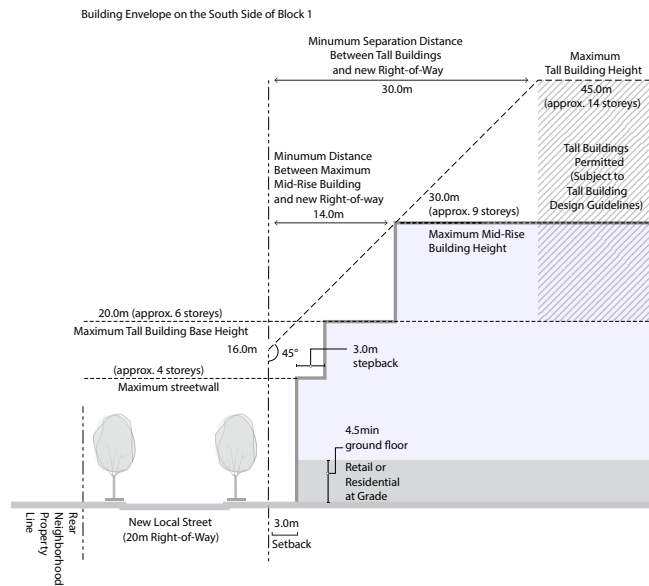


Fig. 19. Transition in scale to adjacent *Neighbourhood* using rear Angular Planes in Block 1



## 5.8 EMPLOYMENT AREAS

Light manufacturing is permitted in the zoning of the adjacent *Employment Areas*. Provincial guidelines require a separation distance between industrial and residential uses to limit resident exposure to noxious conditions including noise, odors and other contaminants and to avoid conflicts that may affect industrial operations.

In addition to setbacks, a stepping down in height from proposed residential development to *Employment Areas* is recommended to complement the lower scale employment buildings in the *Employment Areas*.

### Guidelines and Standards

**R97** For properties backing onto *Employment Areas*, a minimum separation distance of 20.0m is required to ensure that the proposed development of sensitive uses does not

compromise the *Employment Area* operations. The 20.0m transition area may include: a street (required in Blocks 1 and 8); a laneway; landscaped space, either private or publicly accessible; or surface parking, as a least preferred alternative.

**R98** Between *Mixed Use* and *Employment Areas* designations, building heights should step down in scale to complement the lower scale employment buildings, in particular in large blocks (Blocks 1 and 8) where taller buildings are permitted.

Fig. 20. Setback from Employment Areas, Typical

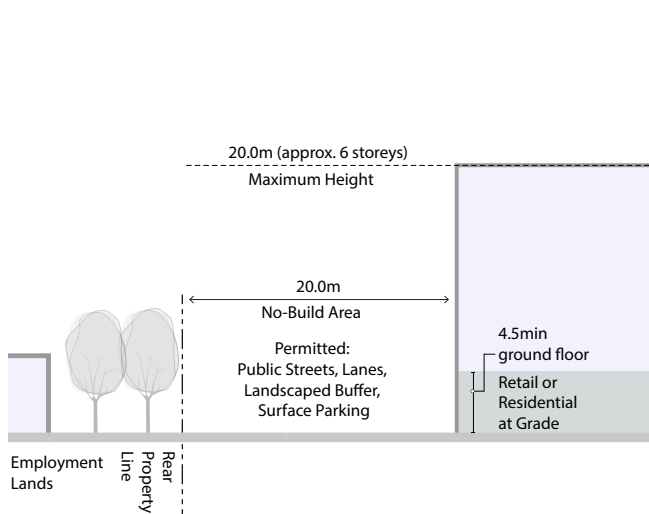
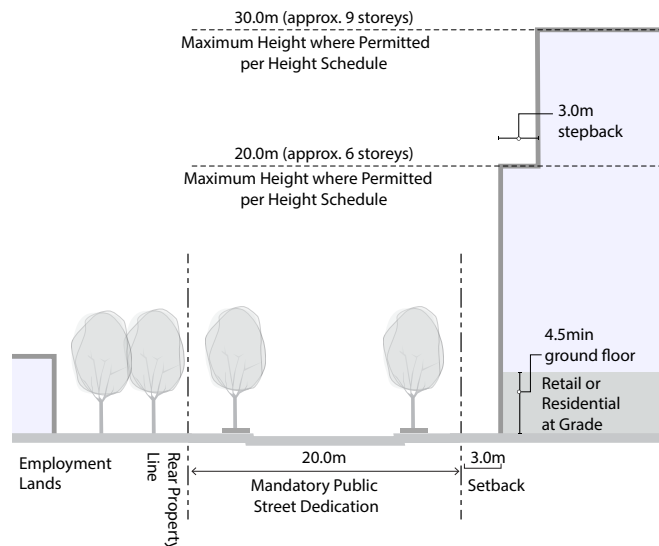


Fig. 21. Setback and Transition in height from Mixed Use Area to Employment Area, Blocks 1 and 8



# 6.0 BUILT FORM - SPECIFIC BLOCKS

## 6.1 MIDRISE BLOCKS

Most of the parcels on the east side of Dufferin Street and on the west between Orfus Road and Jane Osler Boulevard are of a typical size and depth consistent with other *Avenues* in Toronto. The midrise blocks are composed of numerous individual parcels with different individual owners. As a result of these characteristics, the redevelopment of these blocks will likely take longer and be more difficult than the larger blocks with fewer parcels and owners. Together the midrise sites represent only 15% of the total potential redevelopment area within the study boundaries, but a significant portion of the overall street frontage.

These blocks can accommodate mid-rise buildings between 20.0m to 30.0m (6 to 9 storeys), although not every development will be able to achieve the maximum allowable height due to shallow lot depth and geometry of transition at the rear. With redevelopment, vehicular access will reorganize to the rear of buildings or from side streets to assist with the Dufferin Street streetscape improvements.

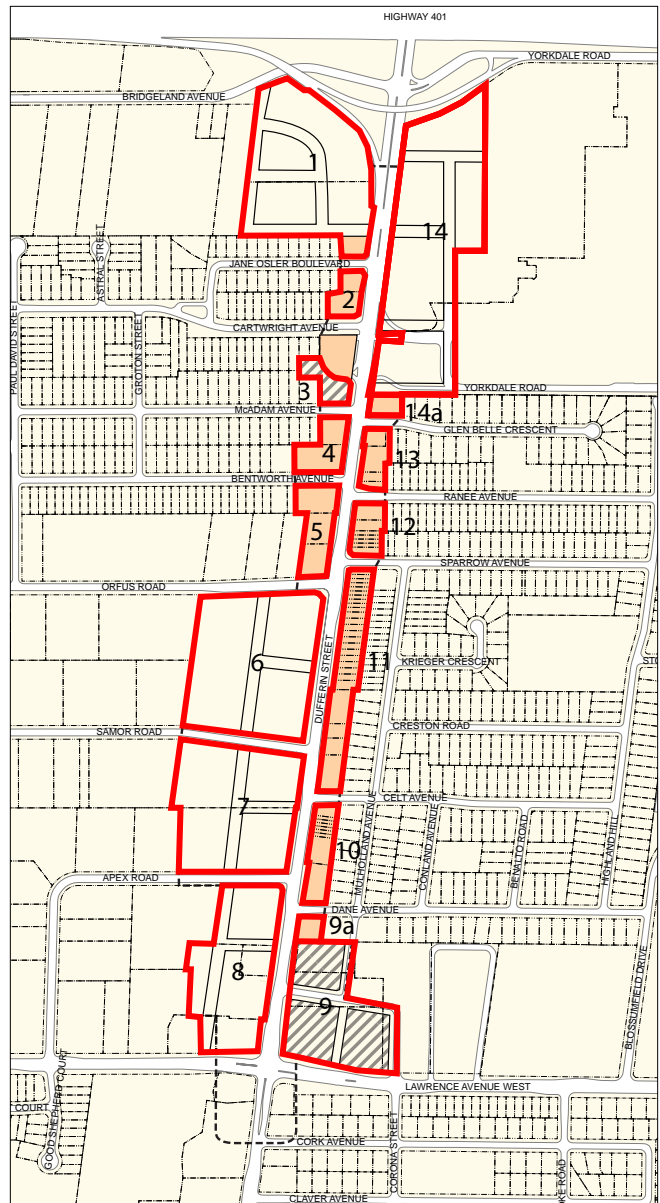


Fig. 22. Midrise Blocks in the Dufferin Street Secondary Plan Area (in orange)



## Guidelines and Standards

### Streets and Blocks

**R99** No new streets are recommended through the midrise blocks.

**R100** Public rear laneways or shared private driveways are encouraged for some of the midrise blocks (as shown in the Streets and Blocks framework plan) to provide better servicing and parking access and to minimize vehicular entrances and impact on the Dufferin Street streetscape. The laneways should occupy the required setback for all new development from the rear property line.

**R101** For Block 11, two east-west laneway or shared private driveway connections are recommended from Dufferin Street to the rear

laneway. This block between Celt Avenue and Sparrow Avenue is much longer than all of the other typical midrise blocks. The two east-west laneways or shared private driveways should align with Krieger Crescent and Creston Road to protect for possible future mid-block connections.

**R102** Servicing and parking access for all new development should be from rear laneways or from side streets. Vehicular access from Dufferin Street is highly discouraged.

### Parks and Open Spaces

**R103** No public parks are recommended for midrise blocks given their more constrained dimensions.



Fig. 23. Midrise Blocks - Demonstration Model View



Queensway (30.0m ROW)



Sheppard Avenue East (36.0m ROW)

Examples of midrise development projects on Toronto's Avenues

**R104** In Block 3, this study recommends that the City decommission the McAdam vehicular turning loop and replace with a new public park.

#### Land Use

**R105** Retail is recommended at grade on parts of the four blocks around the intersection of Orfus Road and Dufferin Street. This responds to Orfus Road's current character as a retail destination, and Dufferin Street's potential role as a gateway to that area. Retail is encouraged but not mandatory for all other midrise blocks.

#### Built Form and Height

**R106** All new development should set back 3.0m from the Dufferin Street right-of way to enhance streetscape and retail uses. Below grade uses or structures are discouraged within the setback to accommodate streetscape improvements.

**R107** New development will be mid-rise buildings. All new mid-rise buildings should follow the City of Toronto Mid Rise Building Performance Standards.

**R108** The maximum height for mid-rise buildings on the Dufferin Street frontage is 30.0m (9 storeys) from grade. Stepbacks of 3.0m are required at 20.0m height (approximately 6 storeys). Due to the shallow lot depth and geometry of transition at the rear, most development sites will not be able to achieve a 9 storey midrise.

**R109** A rear 45 degree angular plane taken at the rear property line is to apply to all Midrise development backing onto *Neighbourhoods* to provide transition in height and scale. Where midrise block properties are shallow (under 44.6m in depth) a 45 degree angular plane taken at 10.5m above the ground and 7.5m from the rear property line should be applied. See Fig 19.

## 6.2 LARGE BLOCKS

The Large Blocks (1, 6, 7, 8 and 14) offer several possible arrangements of streets, buildings and open space. They can accommodate a combination of mid-rise and townhouse buildings with taller buildings at the northern and southern extents (Fig. 14) to reinforce the identity of the gateways to the Dufferin Street Promenade.

Together, the Large Blocks represent 76% of the total potential redevelopment area and have the greatest opportunity to influence the character and quality of Dufferin Street.

To best understand the overall guidelines and standards, refer to the overall framework recommendations alongside the specific recommendations for each of the larger blocks.

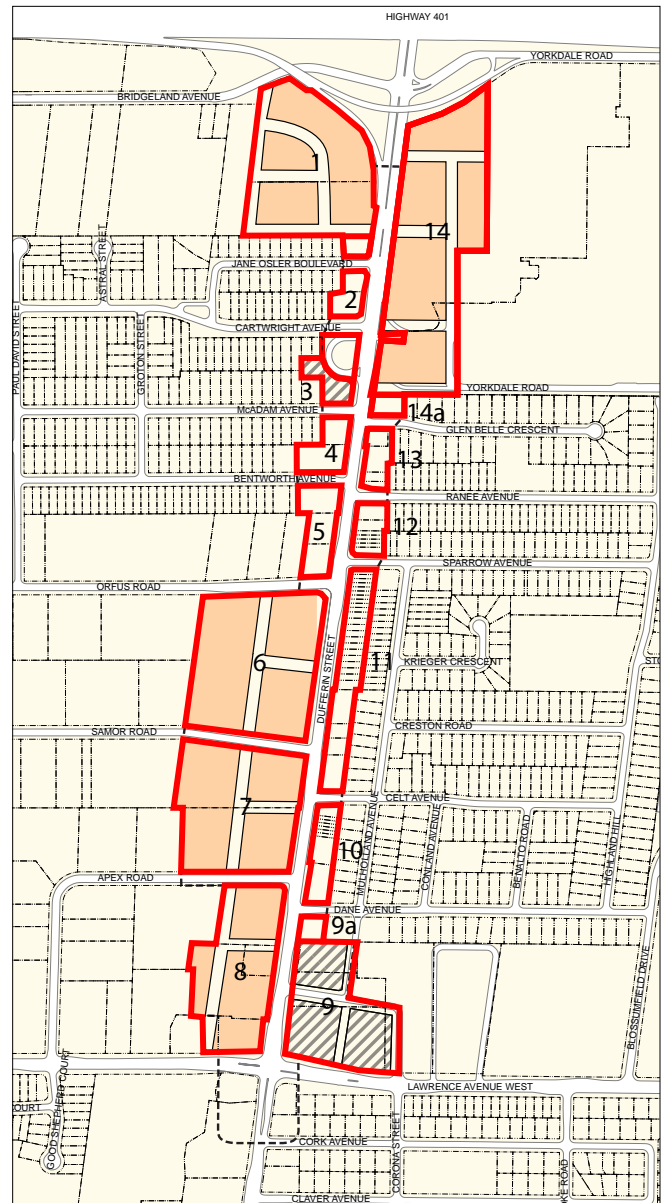


Fig. 24. Larger Blocks in the Dufferin Street Secondary Plan Area (in orange)

## 6.2.1 BLOCK 1

This Large Block is at the northwest end of the Dufferin Street Secondary Plan Area and is bordered by a *Neighbourhood* to the south, *Employment Areas* to the west, and Highway 401 and Bridgeland Avenue to the north.

The planning and design intent is for a mixed use block with new public streets and a new park space. The Dufferin Landscape Gateway will form the northeastern boundary of the block as it relates to the Dufferin Eastbound off ramp from Highway 401.

New development will largely be focused internally to the block given its location. Block 1 is one of the two locations in the Secondary Plan Area where tall buildings are permitted.

It is recommended the two landowners prepare a conceptual block master plan to coordinate their redevelopment efforts.

### Guidelines and Standards

#### Streets and Blocks

**R110** Introduce a perimeter 'L' shaped public street from Bridgeland Avenue to Dufferin Street. The location illustrated is recommended in connecting to the existing street network and assisting with the transition to adjacent land uses. The east west street segment may be extended west in the future to improve connectivity within the *Employment Areas*.



Fig. 25. Block 1 Structure Plan



Fig. 26. Block 1 - Demonstration Model View looking northwest

**R111** Introduce additional internal local streets to divide the larger site into smaller development blocks for a connected grid of streets and to provide access and address to the public park and the remainder of the buildings within the block. Private streets are to be designed to function and appear as public streets, integrating into the public realm and meeting design objectives of public streets.

#### *Parks and Open Spaces*

**R112** Introduce a new public park as a central focal point of the block. The new park should have at least two frontages on a public right-of-way.

**R113** Setback development fronting on a public park 5m from the park to allow for landscaping, amenity and walkways to enhance and animate the park edge without limiting park function.

**R114** Introduce privately owned publicly accessible open spaces such as mid-block walkways to encourage pedestrian activity and access through the site and to the park.

**R115** Public Art is encouraged for landscaped open spaces and street boulevards. Implementation, placement and funding should be coordinated through a Public Art Program for the block.

**R116** Implement the western portion of the Dufferin Street Landscape Gateway. This defining open space feature will occupy the minimum 14.0m setback from the property line and Dufferin Street off-ramp from Highway 401 as required by the Ministry of Transportation. Immediately south of the point where the ramp meets Dufferin Street, the setback can be reduced to 7.0m and continue to form part of the Landscape Gateway. A lighted pedestrian pathway is recommended within this space, linking the Dufferin Street sidewalk with the Bridgeland Avenue sidewalk.

#### *Built Form and Height*

**R117** All new buildings should be appropriately set back to transition towards the adjacent *Neighbourhood* and *Employment Area*.

**R118** All new buildings that address new public streets within the block shall be set back a minimum of 3.0m from the public right-of-way. Below grade uses or structures are discouraged within the setback to accommodate landscape improvements.

**R119** A standard front angular plane will be applied to buildings fronting on the proposed new east-west street in Block 1. Buildings directly fronting on the new street will be limited to 20.0m in height, but the angular plane will permit 30.0m mid-rise buildings beginning at a distance of 14.0m from the new right-of-way (34.0m from the existing *Neighbourhood* to the south - Fig. 19).

**R120** New development should be primarily of mid-rise buildings. All new mid-rise buildings should follow the City of Toronto Mid Rise Building Performance Standards.

**R121** The maximum height for all mid-rise buildings is 30.0m (9 storeys) from grade, except for those fronting on the new side streets where it is recommended that height be limited to 20.0m (6 storeys) in height. This maximum 9 storey height will extend around the corners of the new public road, to a distance of 30m (the length of a typical midrise block) where the building will step down to meet the 20.0m (6 stories) midrise height of the public street.

**R122** Tall buildings are permitted in parts of Block 1 given that all setbacks, transitions, and guidelines are met. All new tall buildings should follow the City of Toronto Tall Building Design Guidelines.

**R123** The maximum height for all tall buildings is 45.0m (14 to 15 storeys) from grade. The maximum height for the base building is 6 storeys.

**R124** Tall buildings should be at least 50.0m from the adjacent *Neighbourhoods* to provide an appropriate built form transition.

## 6.2.2 BLOCK 6

This Large Block is currently in a single consolidated ownership, and is framed by Dufferin Street, Orfus Road and Samor Road, bordered by the *Employment Area* to the west. This is a very deep block –approximately four times the depth of a typical midrise site.

The planning and design intent is for a mixed use block with new public streets and a new park space. New development will be street related to both the existing and future new public streets, and create several new development blocks. The landowner is encouraged to prepare a conceptual block master plan to comprehensively coordinate redevelopment efforts for the entire block.

## Guidelines and Standards

### Streets and Blocks

- R125** Introduce a network of new public streets through the block. A minimum of one new north-south street and one east-west street is recommended for the block. The location of new streets is flexible and will be determined through the block master plan process. Alignment of north south streets with block 7 to the south should be considered.
- R126** Servicing and parking access to these blocks should occur via these new public streets and not from the existing Dufferin Street frontage or the existing collector streets.



Fig. 28. Block 6 Structure Plan



Fig. 27. Block 6 - Demonstration Model View looking northwest, illustrating mid-rise development throughout

---

## Land Use

**R127** Commercial uses are recommended at grade near the Dufferin Street and Orfus Road intersection.

## Parks and Open Spaces

**R128** Introduce a new public park as a focal point of the block. The new park should have at least two frontages on a public right-of-way and be no less than 15 per cent of the total block area, excluding public roads and conveyances. Park address on Dufferin Street is recommended for the Block 6 park. The exact orientation and design will be determined through the development approval process.

**R129** Setback development fronting on a public park 5m from the park to allow for landscaping, amenity and walkways to enhance and animate the park edge with out limiting park function.

**R130** Introduce an urban plaza at the southwest corner of Dufferin Street and Orfus Road. The orientation and design of this privately owned publicly-accessible open space will be determined through the development approval process. Retail uses are recommended to frame and animate the urban plaza.

**R131** Introduce privately owned publicly accessible open spaces such as mid-block walkways to encourage pedestrian activity.

**R132** Public Art is encouraged for landscaped open spaces and street boulevards. Implementation, placement and funding should be coordinated through a Public Art Program for the block.

## Built Form and Height

**R133** All new buildings on the Dufferin Street, Orfus Road and Samor Road frontage shall be set back a minimum of 5.0m from the public right-of-way. This will provide sufficient dimension to implement the recommended Dufferin Street streetscape improvements and landscape

improvements to Orfus and Samor Road. Below grade uses or structures are discouraged within the setback to allow for streetscape improvements.

**R134** All new buildings that front new public streets within the block shall be set back a minimum of 3.0m from the public right-of-way. Below grade uses or structures are discouraged within the setback to accommodate landscape improvements.

**R135** From the *Employment Area*, a minimum 20.0m setback and transition is required from the western property boundary (subject to Ministry of the Environment regulations).

**R136** New development should be primarily of mid-rise buildings. All new mid-rise buildings should follow the City of Toronto Mid Rise Building Performance Standards.

**R137** The maximum height for mid-rise buildings on the Dufferin Street frontage is 30.0m (9 storeys) from grade. Stepbacks of 3.0m are required at 20.0m height. This maximum height will extend around the corners, to a distance of 30.0m (the length of a typical midrise block) where the building will step down to meet the 20.0m (6 storeys) midrise height of the local side street.

**R138** Portions of buildings directly fronting on the existing collectors and new public streets will be limited to 20.0m in height (6 storeys), but front angular planes will permit 30.0m mid-rise portions internal to the block at a distance of 14.0m from the public right-of-way.

**R139** Courtyard building forms are encouraged along Dufferin Street to create a more open and green streetscape character.

**R140** Low-rise forms, such as townhouses, are encouraged for the rear and interior of these blocks to transition in height and scale. All new townhouse development should follow the City of Toronto Infill Townhouse Urban Design Guidelines.

## 6.2.3 BLOCK 7

Block 7 consists of two parcels and is framed by Dufferin Street, Samor Road and Apex Road and is bordered by the *Employment Area* to the west. The planning and design intent is for a mixed use block with new public streets and new park space. New development will be street related to both the existing and future new public streets, and create several new development blocks. The landowners are encouraged to prepare a conceptual block master plan to comprehensively coordinate redevelopment efforts for the entire block.

### Guidelines and Standards

#### Streets and Blocks

**R141** Introduce a network of new public streets through the block. A minimum of one new north-south street and one east-west street is recommended for the block. The location of new streets is flexible and will be determined through the block master plan process. Alignment of north south streets with Block 6 to the north and Block 8 to the south should be considered.

**R142** Servicing and parking access to these blocks should occur via these new public streets and not from the existing Dufferin Street frontage or the existing collector streets.

#### Land Use

**R143** Retail commercial uses along the Dufferin Street frontages is encouraged.

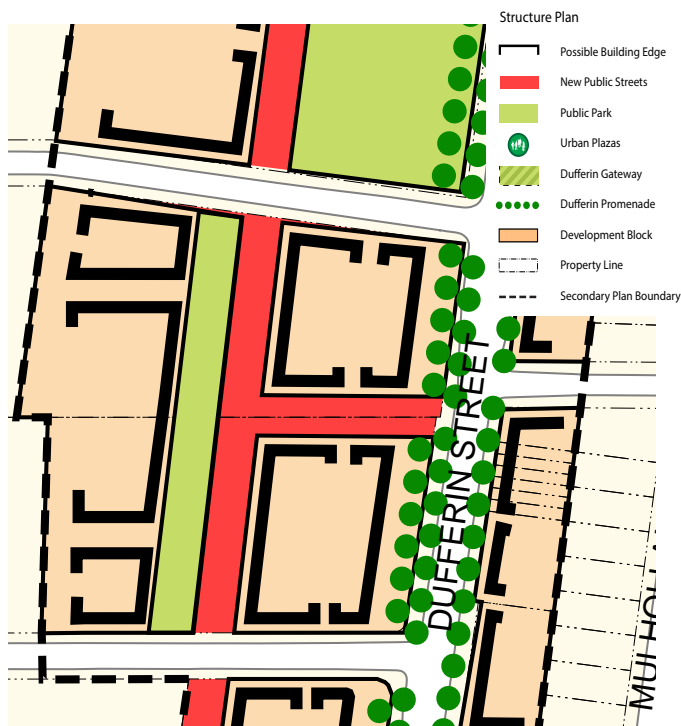


Fig. 30. Block 7 Structure Plan



Fig. 29. Block 7 - Demonstration Model View looking northwest, illustrating mid-rise towards Dufferin Street and townhouse buildings to the west of the block



---

### *Parks and Open Space*

**R144** Introduce a new public park (or parks) in the block. The new park(s) should have at least two frontages on a public right-of-way and be no less than 15% of the total block area, excluding public roads and conveyances. The exact orientation and design will be determined through the development approval process.

**R145** Setback development fronting on a public park 5m from the park to allow for landscaping, amenity and walkways to enhance and animate the park edge without limiting park function.

**R146** Residential courtyards at grade open to the public street are encouraged to enhance the landscaped open space network. Courtyard buildings are encouraged along Dufferin Street to create a more open and landscaped streetscape character.

**R147** Introduce privately owned publicly accessible open spaces such as mid-block walkways to encourage pedestrian activity.

**R148** Public Art is encouraged for landscaped open spaces and street boulevards. Implementation, placement and funding should be coordinated through a Public Art Program for the block.

### *Built Form and Height*

**R149** All new buildings on the Dufferin Street, Samor Road and Apex Road frontage shall be setback a minimum of 5.0m from the public right-of-way. This will provide sufficient dimension to implement the recommended Dufferin Street streetscape improvements and landscape improvements to Samor and Apex Road. Below grade uses or structures are discouraged within the setback to accommodate streetscape improvements.

**R150** All new buildings that front public streets within the block—shall be set back a minimum of 3.0m from the public right-of-way. Below grade uses or structures are discouraged within the setback to accommodate landscape improvements.

**R151** From the *Employment Area*, a minimum 20.0m setback and transition is required from the western property boundary (subject to Ministry of the Environment regulations).

**R152** New development should be primarily of mid-rise buildings. All new mid-rise buildings should follow the City of Toronto Mid Rise Building Performance Standards.

**R153** The maximum height for all mid-rise buildings on the Dufferin Street frontage is 30.0m (9 storeys) from grade. Stepbacks of 3.0m are required at 20.0m height. This maximum height will extend around the corners, to a distance of 30.0m (the length of a midrise block) where the building will step down to meet the 20.0m midrise height of the local side street.

**R154** Portions of buildings directly fronting on the existing collectors and new public streets will be limited to 20.0m in height (6 storeys), but front angular planes will permit 30.0m mid-rise portions internal to the block at a distance of 14.0m from the public right-of-way.

**R155** Low-rise forms, such as townhouses, are encouraged for the rear and interior of these blocks to transition in height and scale. All new townhouse development should follow the City of Toronto Infill Townhouse Urban Design Guidelines.

## 6.2.4 BLOCK 8

With four individual properties, Block 8 is framed by Dufferin Street, Lawrence Avenue West, Apex Road and is bordered by the *Employment Areas* to the west.

The planning and design intent is for a mixed use block with new public streets and new park space. The corner of Dufferin Street and Lawrence Avenue West is where two *Avenues* meet, and will serve as a gateway to the Dufferin Street Secondary Plan, with greater building height at the corner transitioning to lower height moving north along Dufferin Street with the new east-west street defining the limit of buildings greater than 9 storeys. This new east-west street and the height transition correspond with the same transition condition found on the east side of Dufferin Street.

New development will be street related to both the existing and future new public streets, and create several new development blocks.

Block 8 will require an additional setback from Dufferin Street to create a generous green promenade. The multiple landowners are encouraged to prepare a conceptual block master plan to comprehensively coordinate redevelopment efforts for the entire block.

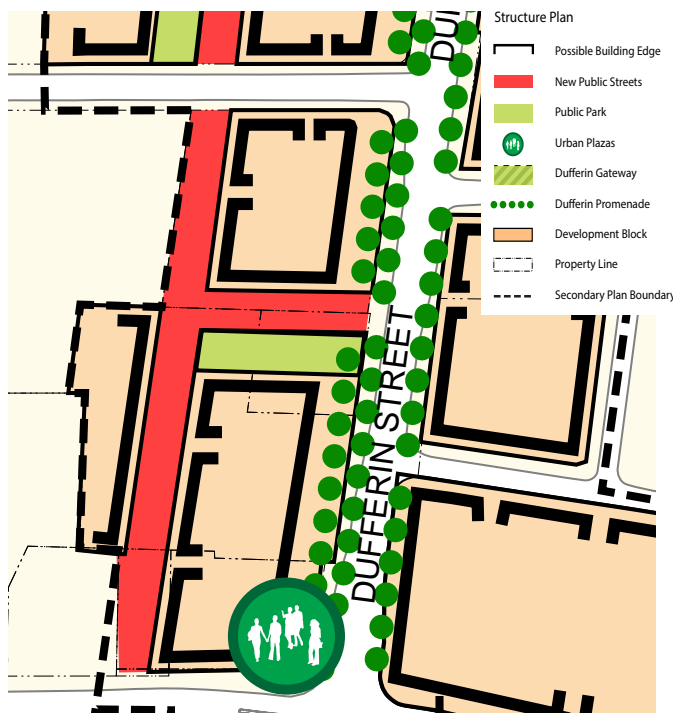


Fig. 32. Block 8 Structure Plan



Fig. 31. Block 8 - Demonstration Model View

---

## Guidelines and Standards

### *Streets and Blocks*

**R156** Introduce a network of new public streets through the block. A minimum of one new north-south street and one east-west street is recommended for the block. The location of the north-south street is recommended along the western boundary given the block is less deep than the other large blocks. Alignment of the north south street with block 7 to the north should be considered. The location of the east-west street is also recommended along a mutual property line, so the right-of-way conveyance is shared between both properties. The east-west street serves as the transition from tall building to mid-rise building form, and reflects the similar transition relationship as on the east side of the street.

**R157** Servicing and parking access to these blocks should occur via these new public streets and not from the existing Dufferin Street and Lawrence Avenue West frontages, or the existing collector streets.

### *Parks and Open Spaces*

**R158** Introduce a new public park in the block. The new park should have three frontages on a public right-of-way and be no less than 15% of the total block area, excluding public roads and conveyances. Three recommended public frontages for the Block 8 Park include Dufferin Street and new north-south and east-west public street. The park will serve as part of the transition from the gateway corner to the mid-rise segments of the Dufferin Street Avenue. The exact orientation and design will be determined through the development approval process.

**R159** Setback development fronting on a public park 5m from the park to allow for landscaping, amenity and walkways to enhance and animate the park edge without limiting park function.

**R160** Introduce an urban plaza at the northwest corner of Dufferin Street and Lawrence Avenue West. The orientation and design of this privately-owned publicly accessible open space will be determined through the development approval process. Retail uses are recommended to frame and animate the urban plaza.

**R161** Introduce other privately owned publicly accessible open spaces such as mid-block walkways to encourage pedestrian activity.

**R162** Public Art is encouraged for landscaped open spaces and street boulevards. Implementation, placement and funding should be coordinated through a Public Art Program for the blocks.

### *Built Form and Height*

**R163** All new buildings on the Dufferin Street and Apex Road frontage shall be setback a minimum of 5.0m from the public right-of-way. This will provide sufficient dimension to implement the recommended Dufferin Street streetscape improvements and landscape improvement along Apex Road. Below grade uses or structures are discouraged within the setback to accommodate streetscape improvements.

**R164** All new buildings that front Lawrence Avenue West or new public streets within the block— shall be set back a minimum of 3.0m from the public right-of-way. Below grade uses or structures are discouraged within the setback to accommodated streetscape and landscape improvements.

**R165** From the *Employment Areas*, a minimum 20.0m setback and transition is required from the western property boundary (subject to Ministry of the Environment regulations). An additional 3.0m setback is required from the new public right-of-way.

---

**R166** New development should be mid-rise buildings (except where tall buildings are permitted, south of the park). All new mid-rise buildings should adhere to the City of Toronto Mid Rise Building Performance Standards.

**R167** The maximum height for all mid-rise buildings is 30.0m (9 storeys) from grade. Step backs of 3.0m are required at 20.0m height. This maximum height will extend around the corners, to a distance of 30.0m (the length of a midrise block) where the building will step down to meet the 20.0m midrise height of the local side street.

**R168** Portions of buildings directly fronting on Apex Road will be limited to 20.0m in height (6 storeys), but front angular planes will permit 30.0m mid-rise portions internal to the block at a distance of 14.0m from the public right-of-way.

**R169** Tall buildings are permitted only in the portion of Block 8 south of the new park, and require that all setbacks, transitions, and guidelines are met. This reflects the gateway status of this site, which sits directly across from the Treviso complex. All new tall buildings should follow the City of Toronto Tall Building Design Guidelines.

**R170** The maximum height for tall buildings in the permitted zone is 65.0m (20 storeys) from grade.

**R171** The maximum height for the base portion of tall buildings is 20.0m (6 storeys), to best relate to the maximum street wall height for Dufferin Street.

## 6.2.5 BLOCK 14

This Large Block forms the northeastern boundary in the Dufferin Street Secondary Plan Area, and is the Dufferin Street frontage of the much larger Yorkdale Shopping Centre property. With Block 1 and the lands around the Highway 401 interchange these blocks together form the Dufferin Gateway.

The planning and design intent is for a landmark destination retail commercial focused block that positively addresses and animates Dufferin Street, with new public and publicly accessible streets, pedestrian routes and an urban plaza. The Dufferin Landscape Gateway will form the western boundary of the block. New development will address Dufferin Street and the internal streets. The landowner is encouraged to prepare a conceptual block master plan to comprehensively coordinate redevelopment efforts for the entire block.

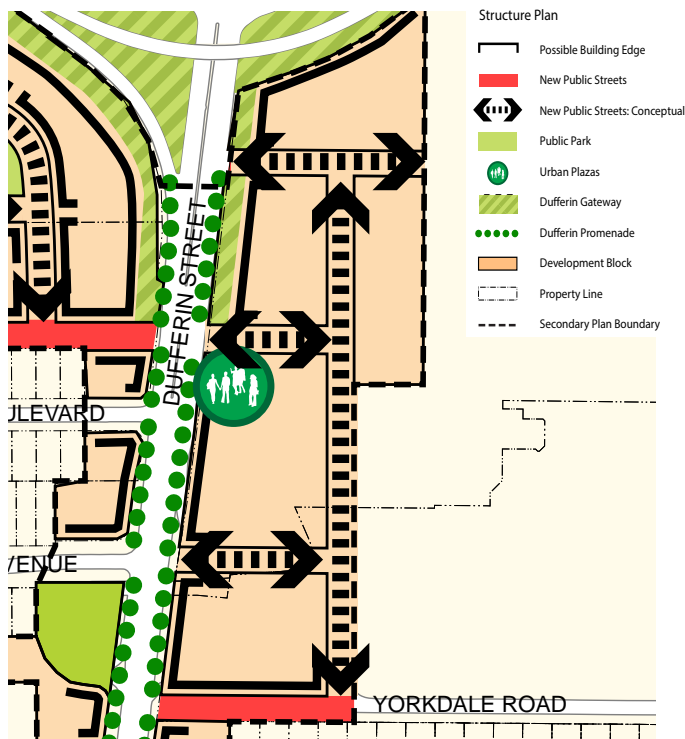


Fig. 34. Block 14 Structure Plan

## Guidelines and Standards

### Streets and Blocks

**R172** Dedicate Yorkdale Road, currently a private road, as a public street, with a rearranged full move intersection. This will facilitate better public access to Allen Road and Highway 401.

**R173** Introduce a new east-west street with a full intersection to align with the proposed public street in Block 1. This will allow for a new street front entrance to Yorkdale Shopping Centre.

**R174** Introduce additional internal east-west and north-south streets to divide the larger site into smaller development blocks for a connected grid of streets and pedestrian routes and to provide access and address to new retail development. Private streets will be designed



Fig. 33. Block 14 - Demonstration Model View

---

to function and appear as public streets, integrating into the public realm and meeting design objectives of public streets.

- R175** In the case of future mixed use residential development, to be further studied, public streets are recommended to provide address and access to residential buildings.

#### *Parks and Open Spaces*

- R176** Introduce an urban plaza related to the new east-west street. This open space feature should be located at Yorkdale Shopping Centre's primary Dufferin Street entrance. This will form an attractive gateway and enhance the pedestrian experience of the shopping center. The orientation and design of this privately owned publicly accessible open space will be determined through the development approval process. Retail uses are recommended to frame and animate the urban plaza.
- R177** Implement the eastern portion of the Dufferin Landscape Gateway. This defining open space feature will occupy the minimum 14.0m setback from the property line to Highway 401 along the north edge of the block as required by the Ministry of Transportation. Along the Dufferin Street frontage the setback can be reduced to 7.0m and continue to form part of the Landscape Gateway. This defining open space feature will occupy the 7.0m setback along the western boundary of the block down to the new east-west street that aligns with Block 1. Below grade uses or structures are discouraged within the setback to allow for streetscape improvements.
- R178** Public Art is encouraged for landscaped open spaces and street boulevards. Implementation, placement and funding should be coordinated through a Public Art Program for the blocks.

#### *Land Use*

- R179** Retail is recommended at grade along the entire Dufferin Street frontage of Yorkdale Shopping Centre. This reflects the intention that any westward expansion of the mall be 'urban' in character, providing a streetwall edge oriented toward Dufferin Street and easily

#### *Built Form and Height*

- R180** All new buildings on the Dufferin Street frontage shall be setback a minimum of 3.0m from the public right-of-way south of the new east west street that aligns with Block 1. This setback will enhance streetscape and retail uses at grade. Below grade uses or structures are discouraged within the setback to accommodate for streetscape improvements.
- R181** New buildings should reflect an urban character and scale appropriate to create a pedestrian friendly environment along Dufferin Street and within the larger block. Continuous blank walls that sterilize the public realm are discouraged.
- R182** Grade related entrances to retail commercial units are highly encouraged.
- R183** Maximum building height is limited to 15.0m (approximately 2-3 commercial storeys).

---

## 6.3 BLOCKS SOUTH OF LAWRENCE AVENUE WEST

South of the Dufferin Street and Lawrence Avenue West intersection are an additional two blocks that are partially within the Dufferin Street Secondary Plan Area. The Dufferin Street public right-of-way narrows from 30.0m to 27.0m in this segment. On the southeast side of the intersection are two properties typical of *Avenue* midrise sites elsewhere in the Secondary Plan Area. On the southwest side is a church and apartment building that are part of the much larger Villa Charities lands, which are under single ownership.

The planning and design intent is for mixed use mid-rise redevelopment consistent with the typical *Avenue* midrise blocks. For the eastern sites, the maximum mid-rise building heights will be defined by the width of the right-of-way and constrained by angular planes. For the western block, it is recommended that the landowners prepare a conceptual block master plan to comprehensively coordinate redevelopment efforts and to deliver the recommendations of these guidelines and for the portions of their block that are within the Dufferin Street Secondary Plan Area.

### Guidelines and Standards

#### *Streets and Blocks*

**R184** Introduce a shared laneway or driveway for the southeast block. Parking and servicing access should be from this shared facility.

**R185** The block south of Lawrence Avenue West and west of Dufferin Street, will likely undergo comprehensive redevelopment in future. Such development should enhance connectivity with the broader planned street network, with new north-south streets linking south to Playfair Avenue, north to Lawrence Avenue West and further north to the new street network proposed in the Secondary Plan Area. New east-west streets should align to Cork and Claver Avenues, on the east side of Dufferin Street.

#### *Parks and Open Spaces*

**R186** Introduction of a public park as part of the comprehensive redevelopment of the western block south of Lawrence Avenue West fronting new streets is recommended.

#### *Land Use*

**R187** Retail is recommended at grade for the Lawrence Avenue West and Dufferin Street frontages. This reflects the intersection's status as a node where two *Avenues* meet.

---

*Built Form and Height*

**R188** Set back all buildings a minimum of 3.0m from the public right-of-way. No below grade uses or structures are permitted within the setback.

**R189** New development should be primarily of mid-rise buildings. All new mid-rise buildings should follow the City of Toronto Mid Rise Building Performance Standards.

**R190** All new buildings should appropriately transition towards adjacent *Neighbourhoods*.

**R191** For the portion of blocks that address the Dufferin Street and Lawrence Avenue West intersection, the maximum building height shall be 30.0m (9 storeys). Stepbacks of 3.0m are required at 20.0m height.

**R192** For the portion of blocks south of Lawrence Avenue West, the maximum building height is 27.0m (8 storeys). This is due to the narrower right-of-way width. Stepbacks of 3.0m are required at 20.0m height.



# APPENDICES

## A.1 PEDESTRIAN STRATEGY

Improvements to the streetscape along the Dufferin Street Promenade will help develop a comfortable and interesting space for pedestrians. Encouraging walking as a sustainable, vital mode of transportation as proposed in the City of Toronto Pedestrian Charter (2002) will provide a safe and attractive environment on the street that will complement mixed use development and increase retail activity. High density development with mixed uses in close proximity will help improve the walking mode share, and reduce auto trips.

Development of a fine-grained local street network grid will also promote walking and help realize health benefits by improving pedestrian access between buildings and blocks. In addition, an improved east-west pedestrian crossing environment across Dufferin Street will enhance safety and promote walking connections between the *Employment Areas*, new residential developments, Yorkdale Shopping Centre, the Lawrence Heights residential district, and the subway along Allen Road.

### Guidelines and Standards

- R193** Implement new local streets in the larger blocks to provide a high level of permeability for public circulation.
- R194** Implement streetscape improvements with broad pedestrian boulevards and landscape setbacks on all existing and new streets.
- R195** Introduce where possible additional signaled pedestrian crossings at intersections to improve east-west pedestrian movement.

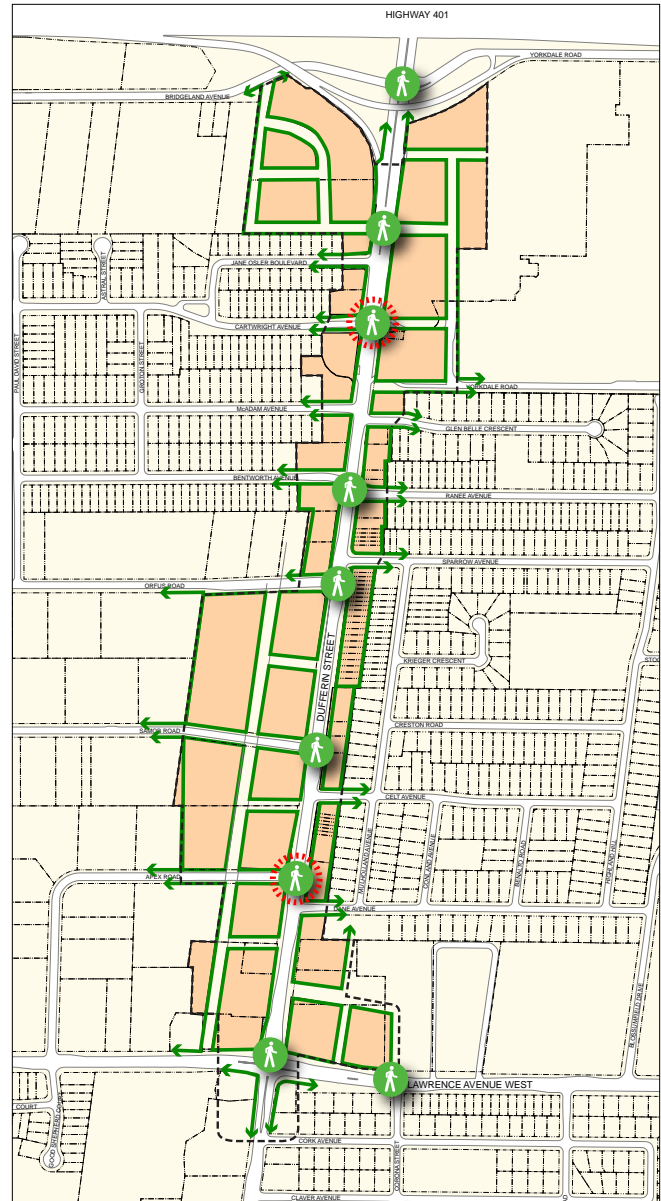
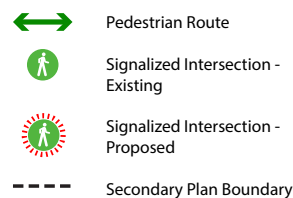


Fig. 35. Recommended Pedestrian Strategy



## A.2 CYCLING STRATEGY

Improvements to the cycling infrastructure will promote recreational cycling and bicycle commuting. Implementation of cycling routes recommended in the City of Toronto Bike Plan (2001) and the Lawrence-Allen Secondary Plan (2011) will help improve cycling mode share by providing a safer environment as shorter trips in the local area are increasingly made using bicycles.

### Guidelines and Standards

- R196** Implement the cycling infrastructure improvements recommended in the Streetscape section, including a grade separated cycle track on Dufferin Street, bike lanes on Orfus Road, and bike friendly lanes on all other existing and new local streets.
- R197** Introduce bike parking facilities on all public streets and private development setbacks to encourage local cycling activity.

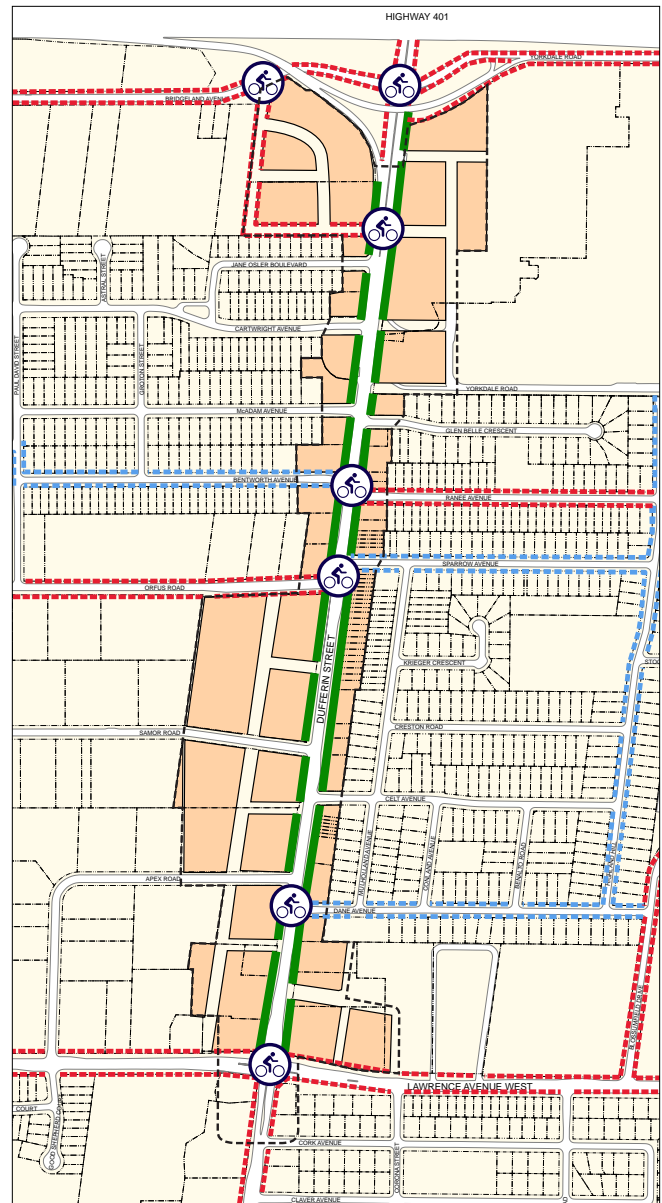


Fig. 36. Recommended Cycling Strategy

- Proposed Cycle Track
- - - Proposed Bike Lane
- - - Proposed Signed Route
- Proposed Cycling Interchange
- - - Secondary Plan Boundary

---

# PROJECT CREDITS

## CITY OF TORONTO

### **Urban Design**

Leo Desorcy, Sasha Terry, Dawn Hamilton

### **Planning**

Al Rezoski, Andria Sallese, Colin Ramdial

### **Transportation**

Victoria Witowski, Andrew Au, Dewan Karim

### **Strategic Initiatives & Planning Policy**

Mario Giambattista

### **Parks, Forestry and Recreation**

Lora Mazzocca

### **Toronto Water**

Brian Worsley

## CONSULTANTS

### **DTAH**

Brent Raymond, Robert Allsopp, René Biberstein

### **R.E. Millward + Associates Ltd**

Melanie Melnyk, Robert Millward, John Burke

### **Arup Canada**

Hilary Holden, Gary Hseuh, Aarshabh Misra, Marc-Paul Gauthier

### **Fabian Papa + Partners**

Fabian Papa, Jason Jenkins

### **N. Barry Lyon Consulting**

Mark Conway

### **Swerhun Facilitation and Decision Support**

Bianca Wylie, Nicole Swerhun, Yulia Pak, Alex Heath

