

**2115-2117 Bloor Street West and 19 Harcroft Road –
Zoning By-law Amendment Application – Final Report**

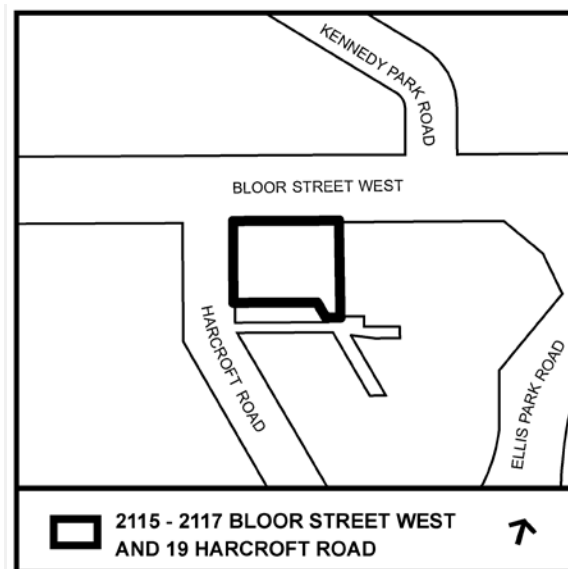
Date:	October 27, 2016
To:	Etobicoke York Community Council
From:	Director, Community Planning, Etobicoke York District
Wards:	Ward 13 – Parkdale-High Park
Reference Number:	15 122324 WET 13 OZ

SUMMARY

This application proposes to amend City of Toronto Zoning By-law No. 569-2013 and former City of Toronto Zoning By-law No. 438-86 to permit the development of an 8-storey mixed-use building, comprised of 60 residential dwelling units, 958 m² of ground floor retail space and a total gross floor area of 6,216 m². A total of 46 below-grade vehicular parking spaces with access via a private downward sloping driveway from Harcroft Road are proposed. A surface loading facility is proposed at the rear of the building, with access via the public lane to the south of the lands. The existing triplex building at 2115 Bloor Street West and the detached dwelling at 19 Harcroft Road are proposed to be demolished.

The proposed development and its implementing site specific amendments to the Zoning By-laws are consistent with the 2014 Provincial Policy Statement and conform to the Provincial Growth Plan and City of Toronto Official Plan.

This report reviews and recommends approval of the application to amend the Zoning By-laws as set out in Attachments 4a and 4b to this report.



RECOMMENDATIONS

The City Planning Division recommends that:

1. City Council amend Zoning By-law No. 438-86, for the lands at 2115-2117 Bloor Street West and 19 Harcourt Road substantially in accordance with the Draft Zoning By-law Amendment attached as Attachment No. 4a.
2. City Council amend City of Toronto Zoning By-law No. 569-2013 for the lands at 2115-2117 Bloor Street West and 19 Harcourt Road substantially in accordance with the Draft Zoning By-law Amendment attached as Attachment No. 4b.
3. City Council authorize the City Solicitor to make such stylistic and technical changes to the Draft Zoning By-law Amendments as may be required.
4. Before introducing the necessary Bills to City Council for enactment, the owner shall:
 - i) Submit a revised Functional Servicing Report for review and acceptance by the Executive Director of Engineering and Construction Services, to determine whether the municipal water, sanitary/combined and storm sewer systems can support the proposed development and whether upgrades or improvements to the existing municipal infrastructure are required; and
 - ii) Enter into a financially secured agreement for the construction of any improvements to the municipal infrastructure, should it be determined that upgrades are required to support the development, according to the Functional Servicing Report accepted by the Executive Director of Engineering and Construction Services.

Financial Impact

The recommendations in this report have no financial impact.

DECISION HISTORY

A Preliminary Report outlining the subject application was considered by Etobicoke York Community Council at its meeting held on May 12, 2015. The Preliminary Report can be viewed at: <http://www.toronto.ca/legdocs/mmis/2015/ey/bgrd/backgroundfile-79346.pdf>

ISSUE BACKGROUND

Proposal

This application proposes to amend City of Toronto Zoning By-law No. 569-2013 and former City of Toronto Zoning By-law No. 438-86 to permit the development of an eight (8) storey mixed-use building at 2115-2117 Bloor Street West and 19 Harcourt Road.

The proposed development would contain office and retail uses on the first floor, 60 dwelling units located on floors 2 to 8, 46 underground vehicle parking spaces and a total of 72 bicycle parking spaces (see Attachment 5 – Application Data Sheet).

The residential gross floor area would be 5,258 m² and the non-residential floor area would be 958 m². Indoor and outdoor amenity space for the future residents of the building is proposed to be 108 m² and 115 m², respectively. The development would have a floor space index of approximately 4.18 times the area of the lot area.

The residential floor area would accommodate a maximum of 60 residential dwelling units, which would be comprised of thirty nine (39) 1-bedroom units, nineteen (19) 2-bedroom units and two (2) 3-bedroom units. The building would have a maximum building height of 26.8 m, inclusive of mechanical and amenity areas.

The main entry/lobby to the residential component of the building would be located on Harcroft Road in the south portion of the building, with vehicle access proposed immediately south of this entry. The retail areas are proposed to have direct pedestrian access from Bloor Street West.

Site and Surrounding Area

The lands are located on the southeast corner of Bloor Street West and Harcroft Road, are generally rectangular in shape, and have grades that increase in elevation from east to west, and from north to south. Currently, the lands are occupied with a triplex building on 2115 Bloor Street West, vacant lands at 2117 Bloor Street West and a detached dwelling on 19 Harcroft Road. Staff have confirmed that the historic number of residential dwelling units on the lands was limited to four, with two being converted to office space, and one owner-occupied unit.

The lands have approximately 43.5 m of frontage on Bloor Street West, 32.9 m of frontage along Harcroft Road and a lot area of approximately 1,500 m².

Land uses surrounding the subject lands are as follows:

North: commercial and residential buildings fronting the north side of Bloor Street West, ranging in height from 2 to 3 storeys and a retirement residence having a height of 5 to 8 storeys.

South: a detached house followed by Hush Lane, and a neighbourhood (Swansea) predominately consisting of two and three-storey detached houses.

East: three and four-storey mixed use buildings and walk-up apartment buildings, with a six-storey mixed use building at Ellis Park Road.

West: three and four-storey mixed-use and residential buildings.

Provincial Policy Statement and Provincial Plans

The Provincial Policy Statement (PPS) 2014 provides policy direction on matters of provincial interest related to land use planning and development. These policies support the goal of enhancing the quality of life for all Ontarians. Key policy objectives include: building strong healthy communities; wise use and management of resources; and protecting public health and safety. The PPS recognizes that local context and character is important. Policies are outcome-oriented, and some policies provide flexibility in their implementation provided that provincial interests are upheld. City Council's planning decisions are required, by the *Planning Act*, to be consistent with the PPS.

The Growth Plan for the Greater Golden Horseshoe provides a framework for managing growth in the Greater Golden Horseshoe including: directions for where and how to grow; the provision of infrastructure to support growth; and protecting natural systems and cultivating a culture of conservation. City Council's planning decisions are required, by the *Planning Act*, to conform, or not conflict, as the case may be, with the Growth Plan for the Greater Golden Horseshoe.

Staff reviewed the proposed development for consistency with the PPS and for conformity with the Growth Plan for the Greater Golden Horseshoe.

Official Plan

The lands fronting Bloor Street West are located within an *Avenues* area on Map 2 - Urban Structure of the Official Plan. *Avenues* are corridors along major streets where reurbanization is anticipated and encouraged in order to accommodate growth.

The lands in their entirety are designated *Mixed Use Areas* on Map 14 - Land Use Plan. The *Mixed Use Areas* designation provides for a range of residential, commercial and institutional uses, and provides criteria to direct the form and quality of development. It is one of four designations identified in the Official Plan providing opportunities for increased jobs and/or population. The Official Plan states that "*Mixed Use Areas* will absorb most of the anticipated increase in retail, office and service employment in Toronto in the coming decades, as well as much of the new housing." However, not all *Mixed Use Areas* will experience the same scale or intensity of development. The policies of *Mixed Use Areas* require new development to provide a transition between areas of different development intensity and scale.

Official Plan Policy 2.2.3.3(b) states that "Development in *Mixed Use Areas* located on *Avenues*, prior to the completion of an Avenue Study has the potential to set a precedent for the form and scale of reurbanization along the *Avenues*. In addition to the policies of the Plan for *Mixed Use Areas*, proponents of such proposals will also address the larger context and examine the implications for the segment of the Avenue in which the proposed development is located.

An Avenue Segment Study will:

- include an assessment of the impacts of the incremental development of the entire Avenue segment at a similar form, scale and intensity, appropriately allowing for distinguishing circumstances;
- consider whether incremental development of the entire Avenue segment as identified in the above assessment would adversely impact any adjacent *Neighbourhoods*;
- consider whether the proposed development is supportable by available infrastructure; and
- be considered together with any amendment to the Official Plan or Zoning By-law at the statutory public meeting for the proposed development.”

Official Plan Policy 2.2.3.3(c) further states, “development in *Mixed Use Areas* on *Avenues* that precedes the completion of an Avenue Study will:

- support and promote the use of transit;
- contribute to the creation of a range of housing options in the community;
- contribute to an attractive, safe and comfortable pedestrian environment that encourages walking and strengthens local retailing;
- provide universal physical access to all publicly accessible spaces and buildings;
- conserve heritage properties;
- be served by adequate parks, community services, water and sewers, and transportation facilities; and
- be encouraged to incorporate environmentally sustainable building design and construction practices.”

Development requiring a rezoning will not be allowed to proceed prior to completion of an Avenue Study unless a review is undertaken that demonstrates to Council’s satisfaction that subsequent development of the entire Avenue segment will have no adverse impacts within the context and parameters of the review.

An Avenue Segment Study for Bloor Street West from Keele Street/Parkside Drive in the east to Windermere Avenue in the west was prepared and submitted in support of this 8-storey mixed-use proposal, to address Official Plan Policy 2.2.3.3 (b) and (c).

The Official Plan contains criteria to evaluate development in *Mixed Use Areas*. Policy 4.5.2 requires that in *Mixed Use Areas* development will:

- create a balance of high quality commercial, residential, institutional and open space uses that reduce automobile dependency and meets the needs of the local community;
- provide for new jobs and homes for Toronto’s growing population on underutilized lands in the Downtown, the *Central Waterfront*, *Centres*, *Avenues* and other lands designated *Mixed Use Areas*, creating and sustaining well paid, stable, safe and fulfilling employment opportunities for all Torontonians;
- locate and mass new buildings to provide a transition between areas of different development intensity and scale, as necessary to achieve the objectives of the Plan,

- through means such as providing appropriate setbacks and/or a stepping down of heights, particularly towards lower scale *Neighbourhoods*;
- locate and mass new buildings so as to adequately limit shadow impacts on adjacent *Neighbourhoods*, particularly during the spring and fall equinoxes;
 - locate and mass new buildings to frame the edges of streets and parks with good proportion and maintain sunlight and comfortable wind conditions for pedestrians on adjacent streets, parks and open spaces;
 - provide an attractive, comfortable and safe pedestrian environment;
 - have access to schools, parks, community centres, libraries and childcare;
 - take advantage of nearby transit services;
 - provide good site access and circulation and an adequate supply of parking for residents and visitors;
 - locate and screen service areas, ramps and garbage storage to minimize the impact on adjacent streets and residences; and
 - provide indoor and outdoor recreation space for building residents in every significant multi-unit residential development.

The Official Plan also contains a number of policies related to Healthy Neighbourhoods (Policy 2.3.1), Housing (Section 3.2.1), Community Services and Facilities (Section 3.2.2) and Toronto Economic Health (Section 3.5) that have been considered in the review of this application. The policies in these sections require development to respect the character of the area, encourage the provision of a full range of housing in terms of form, tenure and affordability and provide direction for a strong and diverse retail sector by promoting a broad range of shopping opportunities for local residents and employees in a variety of settings.

Zoning

The lands are subject to former City of Toronto Zoning By-law No. 438-86. The property is zoned MCR T2.5 C2.0 R2.0, which permits a maximum density of 2.5 times the area of the lot for a mixed-use development, of which a maximum of 2.0 times the area of the lot is permitted to be used for commercial uses only or solely residential use. Permitted uses within this zoning category include a range of residential, retail, commercial, office and institutional uses. The maximum permitted building height is 14 metres.

The lands are also subject to Toronto Zoning By-law No. 569-2013 (see Attachment 3 – Zoning). This By-law is under appeal to the Ontario Municipal Board, but zones the lands CR 2.5 (C2.0; R2.0) SS2 (x1978). Similarly to the MCR zone, the CR zone permits a mix of commercial and residential uses up to a total maximum density of 2.5 times the area of the lot, of which a maximum of 2.0 times the area of the lot is permitted to be used for commercial uses only or solely residential use. The maximum permitted height for the site is 14 m. Building setbacks, angular planes and other performance standards affecting the lands are outlined in the Development Standards Set No. 2 (SS2) of the Zoning By-law.

Site Plan Control

The proposed development is subject to Site Plan Control. A Site Plan application for this development has yet to be submitted.

Reasons for Application

This Zoning By-law Amendment application is required to permit the proposed height and density of the building, as well as address other areas of non-compliance with the current zoning of the lands.

Community Consultation

A community consultation meeting was held on June 18, 2015 at Swansea Town Hall with approximately 20 members of the public in attendance. Issues and concerns related to the application are summarized as follows:

1. Traffic and Parking

Concerns were expressed regarding increased vehicle traffic resulting from the proposed development. Residents living south of Bloor Street West were particularly concerned with additional traffic infiltration through their community.

Concerns were also expressed that the development would result in additional pressures on the existing shortage of on-street parking spaces throughout the community, especially for visitors.

2. School Board Accommodation

Area residents were of the opinion that local schools are currently operating at or above capacity. New development would result in additional pressure on school boards for pupil accommodation.

3. TTC Capacity on Subway Line 2 (Bloor-Danforth)

Concern was expressed by area residents that the Bloor Danforth subway line and the area subway stations are operating at or above capacity during peak hours. Additional development would add to the existing capacity issues.

4. Context and Compatibility

Community members expressed a desire to maintain the small scale, local main street character of Bloor West Village. Comments were made with respect to a pending Avenue Study for Bloor Street West and a Heritage Conservation District Study being initiated to address the issues of character and compatibility.

5. Proposed Building Height, Volume, Materials and Articulation

Comments and concerns were expressed regarding the proposed building height, mass and volume as being inconsistent with the built form character of Bloor West Village, as well as a lack of transition to the neighbourhood to the south, resulting in privacy and skyview impacts.

Agency Circulation

The application was circulated to all appropriate agencies and City divisions. Responses received have been used to assist in evaluating the application and to formulate appropriate Zoning By-law standards.

COMMENTS

Staff are of the opinion that the proposal represents an appropriate redevelopment of the lands, at a scale and intensity that would be compatible and consistent with the existing and planned context for this segment of Bloor Street West, and would further complement the main street activities of Bloor West Village and achieve an efficient use of land, infrastructure and public service facilities.

As previously noted, the lands are located in an area where the Official Plan anticipates and encourages growth, within 400 m of two rapid transit stations (Runnymede and High Park Stations on Line 2 – Bloor-Danforth) and would satisfy many growth objectives of the Provincial Policy Statement and the City's Official Plan.

Provincial Policy Statement and Provincial Plans

The proposed development is consistent with the 2014 PPS. It would accommodate residential infill growth, supporting the policy objectives related to focusing growth in existing settlement areas. The proposal also supports compact, mixed-use development that incorporates compatible employment uses to support livable and resilient communities. The proposal supports efficient land use, reduces land consumption related to residential development, makes efficient use of infrastructure and is transit supportive.

City Council's planning decisions are required to conform, or not conflict, with the Growth Plan for the Greater Golden Horseshoe. In this case, Section 2.2.2 of the Growth Plan states that population and employment growth will be accommodated by directing a significant portion of new growth to the built-up areas of the community through intensification. While this site is located in a built up area, the proposal is considered to represent a modest infill development in conformity with the City's Official Plan. The proposal conforms and does not conflict with the Growth Plan.

Land Use

The site is designated *Mixed Use Areas* in the Official Plan. Staff are of the opinion the proposed development conforms to the *Mixed Use Areas* designation and its development criteria, and is supportive of the Built Form policies of the Official Plan.

In terms of new residential uses, this development would provide a total of 60 new residential units having a mix of unit sizes. Appropriately sized indoor and outdoor amenity areas would be provided within the development for the new residents.

In terms of non-residential use, approximately 958 m² of main floor commercial/retail space would be provided with access directly from the street to complement the main street character of Bloor Street West.

Avenue Segment Study

Staff have reviewed the required Avenue Segment Study and generally concur with the findings.

The principles that were used to identify and review soft sites and their potential built conditions demonstrate that the impacts of the incremental development of the entire Avenue segment at a similar form, scale and intensity would not undermine the goals and objectives of the Official Plan for the Avenue and would not adversely impact adjacent *Neighbourhoods*. The study also noted that sufficient community services and facilities exist in the area to accommodate future residents.

Density, Height and Massing

The Official Plan requires development to comply with criteria pertaining to appropriate built form, location, massing and transition between land use designations. The intent of these policies is to facilitate attractive, comfortable and safe pedestrian environments and provide compatibility of new development with surrounding land use designations.

Built Form policies for *Mixed Use Areas* in the Official Plan require new development proposals to minimize the impact of building density, height and massing on its surroundings through: appropriate design and framing of street edges to create comfortable sun and wind conditions; placing main building entrances so they are directly visible and accessible from the public sidewalk; providing opportunity for active ground floor uses with views and access to adjacent streets, parks and open spaces; and screening and organizing servicing and vehicular access to minimize impacts on the site and its surroundings.

The application proposes a density of 4.18 FSI. The maximum height of the building would be 27 m consisting of main floor retail commercial space and 7 storeys of residential dwelling units above, as well as the combined rooftop mechanical structure and indoor amenity areas. This height is consistent and compatible with the existing surrounding residential development.

The proposed density and maximum building heights are also consistent with the *Mixed Use Areas* policies in the Official Plan and with the zoning of other properties fronting Bloor Street West in this area.

The proposed massing of the development conforms to the policies of the Official Plan with a 1.2 m step back after the fourth storey on the north and west elevation and a progressive step back upwards from the fourth floor on the south elevation. These features will be secured in the Draft Zoning By-laws for this proposal.

Balcony design and building articulation will be reviewed through the Site Plan Approval process to explore opportunities to further address overlook and privacy concerns raised by residents to the south of the lands.

Sun, Shadow and Wind

It is the opinion of staff that shadow impacts would be adequately minimized on the north side of Bloor Street West through the introduction of a 1.2 m step back above the fourth storey and a further step back of 4.25 m above the seventh storey, while limiting the building height to a maximum of 27 m. The rooftop indoor and outdoor amenity spaces as well as the mechanical enclosures have been set back from the north, east and west sides to further minimize shadow projections.

Mid-Rise Buildings Performance Standards

The proposed 8 storey building conforms to the 45-degree front angular plane which is measured from the height of 80% of the Bloor Street West right-of-way width. This includes the mechanical penthouse. The building also provides a 1.2 m step-back on the front elevation at the fifth storey to achieve conformity within this angular plane. This step-back has also been introduced to help strengthen the pedestrian perception zone, thus creating a stronger impression of the lower pedestrian scale elements of the building, and reduces the impact of the buildings mass along Bloor Street West.

The building does penetrate the 45 degree angular plane taken from the rear lot line, including the mechanical penthouse and some balconies. It was recommended by staff that all portions of the proposed building be within the angular plane at the beginning of the application process. Through the review process, however, the proposal was modified to reduce the amount of building mass penetrating the rear angular plane, while reducing the building mass within the pedestrian perception zone in the front elevation. It is the opinion of staff that the proposed building penetration has been limited to a level that would create an appropriate transition in intensity from the building to the *Neighbourhoods* lands to the south.

With respect to the objectives of skyview, overlook and privacy, the applicant has also proposed building elements that would serve to address privacy and overlook conditions related to the *Neighbourhoods* lands to the south, including planters, screens and landscape buffers. It is the opinion of staff that the current limited rear elevation projections appropriately address the objectives of the performance standards with respect to the properties to the south of the lands.

Details associated with the enclosed parking garage ramp are still outstanding, but will be addressed through the Site Plan Control process. Given the constraints of the lot depth, the applicant is proposing an enclosed parking ramp at the rear of the site, in place of a

more typical laneway condition. This enclosure is within the 7.5 m building setback, at a height of approximately 3 m, which will include a 1.5 m landscape buffer and appropriate screening measures that would be secured through the Site Plan Control process.

This ramp will require a vegetative component on top of it to help better screen the balconies and units that impact the rear angular plane. This will help to address issues around privacy and overlook to the *Neighbourhoods* lands to the south.

Overall, staff are of the opinion that the goals and objectives of the performance standards are satisfied on balance. There are additional opportunities to introduce landscaped areas and introduce balcony planters to further address overlook and privacy concerns raised by residents to the south. These elements would be considered and secured through the Site Plan Control process.

Traffic Impact, Access and Parking

A Segment Study Transportation Considerations Report dated March 2015 by BA Group and Urban Transportation Considerations Report dated February 26, 2015 also by BA Group, were submitted in support of the proposed development. Transportation Services staff have reviewed the methodology and conclusions, and concur with the findings of the reports, which concluded that the existing road network can support the proposed development and that improvements to the existing road infrastructure are not required.

With respect to vehicular access to the site, the applicant proposes to accommodate all resident, visitor and retail parking via a parking garage driveway from Harcroft Road that is situated approximately 32.0 m south of Bloor Street West. The location of the proposed driveway has been reviewed and is acceptable to Transportation Services staff from a traffic operations perspective. Further comments on the design of the proposed driveway would be provided through the Site Plan Control process. Additional comments would be provided with respect to parking areas, site circulation and the design and location of parking spaces through the Site Plan Control process.

In terms of on-site parking, the applicant proposes to provide a total of 46 parking spaces for the following uses:

- 34 residential parking spaces;
- 3 visitor parking spaces including one disabled space designated for residential use; and
- 9 retail parking spaces including one disabled parking space designated for retail uses.

Transportation Services staff have no objection to the proposed on-site parking supply.

Servicing

The applicant submitted a detailed Functional Servicing Report that addressed servicing matters including water, stormwater management and sanitary sewer capacity and use.

Engineering and Construction Services staff have reviewed the report and requested that additional analysis be conducted and submitted for review and approval.

In order to ensure the existing City services could accommodate the proposed development, staff are recommending that before introducing the necessary Bills to City Council for enactment, the owner be required to:

- i. Submit a revised Functional Servicing Report for review and acceptance by the Executive Director of Engineering and Construction Services, to determine whether the municipal water, sanitary/combined and storm sewer systems can support the proposed development and whether upgrades or improvements to the existing municipal infrastructure are required; and
- ii. Enter into a financially secured agreement for the construction of any improvements to the municipal infrastructure, should it be determined that upgrades are required to support the development, according to the Functional Servicing Report accepted by the Executive Director, Engineering and Construction Services.

This recommendation would ensure that the Zoning By-laws will not come into force unless these matters are satisfied.

Open Space/Parkland

The Official Plan contains policies to ensure that Toronto's system of parks and open spaces are maintained, enhanced and expanded. Map 8B of the Toronto Official Plan shows local parkland provisions across the City. The lands which are the subject of this application are in an area with 0.43 to 0.79 hectares of local parkland per 1,000 people. The site is in the second lowest quintile of current provision of parkland. The site is in a parkland priority area, as per the City-wide Parkland Dedication By-law 1020-2010.

At the alternative rate of 0.4 hectares per 300 units specified in By-law 1020-2010, the parkland dedication requirement would be 800 m² or 77 % of the site area. However, for sites that are less than 1 hectare in size, a cap of 10% of the development site is applied to the residential use while the non-residential use is subject to a 2% parkland dedication. In total, the parkland dedication requirement is 113 m².

Parks, Forestry and Recreation staff have noted that the applicant would be required to satisfy the parkland dedication requirement through cash-in-lieu of parkland dedication. This is appropriate as there is no suitable location for an on-site parkland dedication.

The amount of cash-in-lieu to be paid will be determined at a time prior to the issuance of a building permit.

Toronto Green Standard

In 2013 City Council updated the two-tiered Toronto Green Standard (TGS) that was adopted by City Council on October 27, 2009. The TGS is a set of performance measures for green development. Tier 1 is required for new development. Tier 2 is a voluntary, higher level of performance with financial incentives. Achieving the Toronto Green Standard will improve air and water quality, reduce green house gas emissions and enhance the natural environment.

The applicant is required to meet Tier 1 of the TGS.

The Draft Zoning By-laws secure performance measures for the following Tier 1 development features: Automobile Infrastructure, Cycling Infrastructure, Storage and Collection of Recycling and Organic Waste.

Other applicable TGS performance measures would be secured through the Site Plan Control process, once the application is submitted.

Community Consultation Comments

With respect to the summary of comments received at the June 18, 2015 Community Consultation meeting earlier in this report, the application process resulted in the following:

1. Traffic and Parking

Transportation Services staff have reviewed the traffic material submitted by the proponent and concluded that the existing road network could support the proposed development and that the amount of parking proposed would satisfy the anticipated parking demand of both future residents and visitors.

2. School Board Accommodation

The four publically funded school boards are circulated development applications. In instances where student accommodation may be an issue for a school board, they typically request that site warning signage and warning clauses in agreements of purchase and sale be required, to inform prospective purchasers that children in the development may be bused outside the neighbourhood to alternative schools.

3. TTC Capacity on Subway Line 2 (Bloor-Danforth)

The Toronto Transit Commission was circulated this application for comment and did not raise transit system capacity as an issue.

4. Context and Compatibility

The proposed development includes ground floor, small scale retail uses, which would complement the main street commercial activity and character of Bloor

West Village to the west. The fine grain retail proposed, coupled with upgraded streetscaping around the lands, would further enhance the main street character of Bloor Street West. Staff are of the opinion that the scale of the proposed development is compatible with the existing built form and character and consistent with recent approved developments in the immediate area and planned context for the area.

The undertaking of a Heritage Conservation District study and an Avenue study have been approved for this area of Bloor Street West, but have not yet been initiated at the time of this report. These studies will provide a vision and regulatory framework to guide the future redevelopment of Bloor Street West.

5. Proposed Building Height, Volume, Materials and Articulation

As noted, staff are of the opinion that the proposed building is compatible and consistent with the existing and planned context for the area. The building mass has been limited through the use of setbacks, step backs and angular planes to provide transition to the neighbourhood, adequately limit shadows on the north side of Bloor Street West and limit overlook and skyview impacts. Additional opportunities to further address these concerns as well as the materiality of the building will be explored through the Site Plan Control process.

Tenure

The proposed development would result in 60 new residential condominium dwelling units.

Conclusion

The proposed amendments to the Zoning By-laws are appropriate and represent good planning. The proposed mixed-use building has been designed to conform to the Official Plan *Mixed Use Areas* policies concerning density, height and massing. Further, the proposed development would enhance the local streetscape and provide a more pedestrian friendly environment. Planning staff are therefore recommending that this application be approved by City Council, subject to the owner satisfying the requirements of the Executive Director of Engineering and Construction Services regarding site servicing matters.

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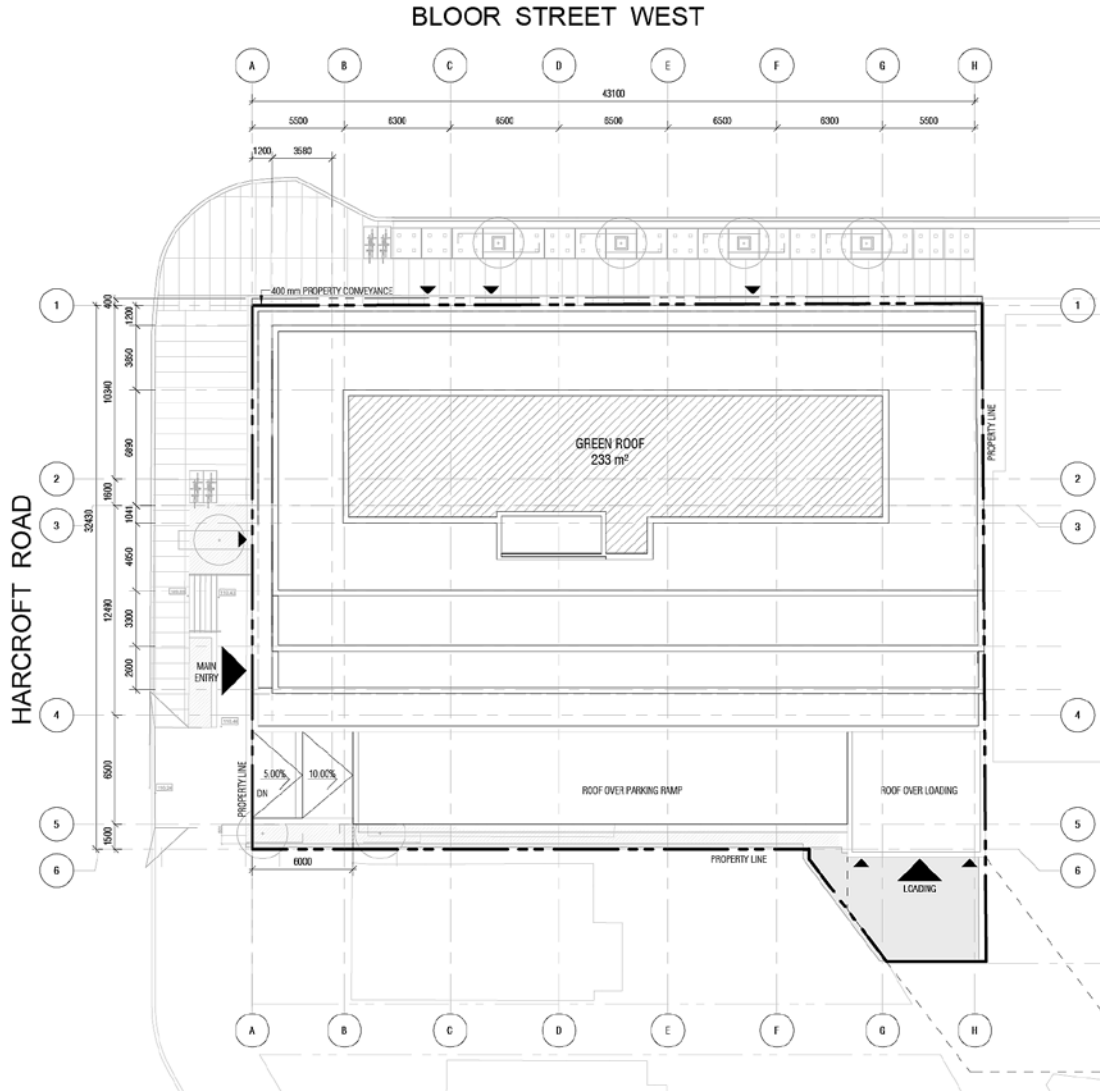
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Neil Cresswell, MCIP, RPP
Director of Community Planning
Etobicoke York District

ATTACHMENTS


Attachment 1: Site Plan
Attachment 2a: West Elevation
Attachment 2b: North Elevation
Attachment 2c: East Elevation
Attachment 2d: South Elevation
Attachment 3: Zoning
Attachment 4a: Draft Zoning By-law Amendment
(Former City of Toronto Zoning By-law No. 438-86)
Attachment 4b: Draft Zoning By-law Amendment
(City of Toronto Zoning By-law No. 569-2013)
Attachment 5: Application Data Sheet

Attachment 1: Site Plan



Site Plan

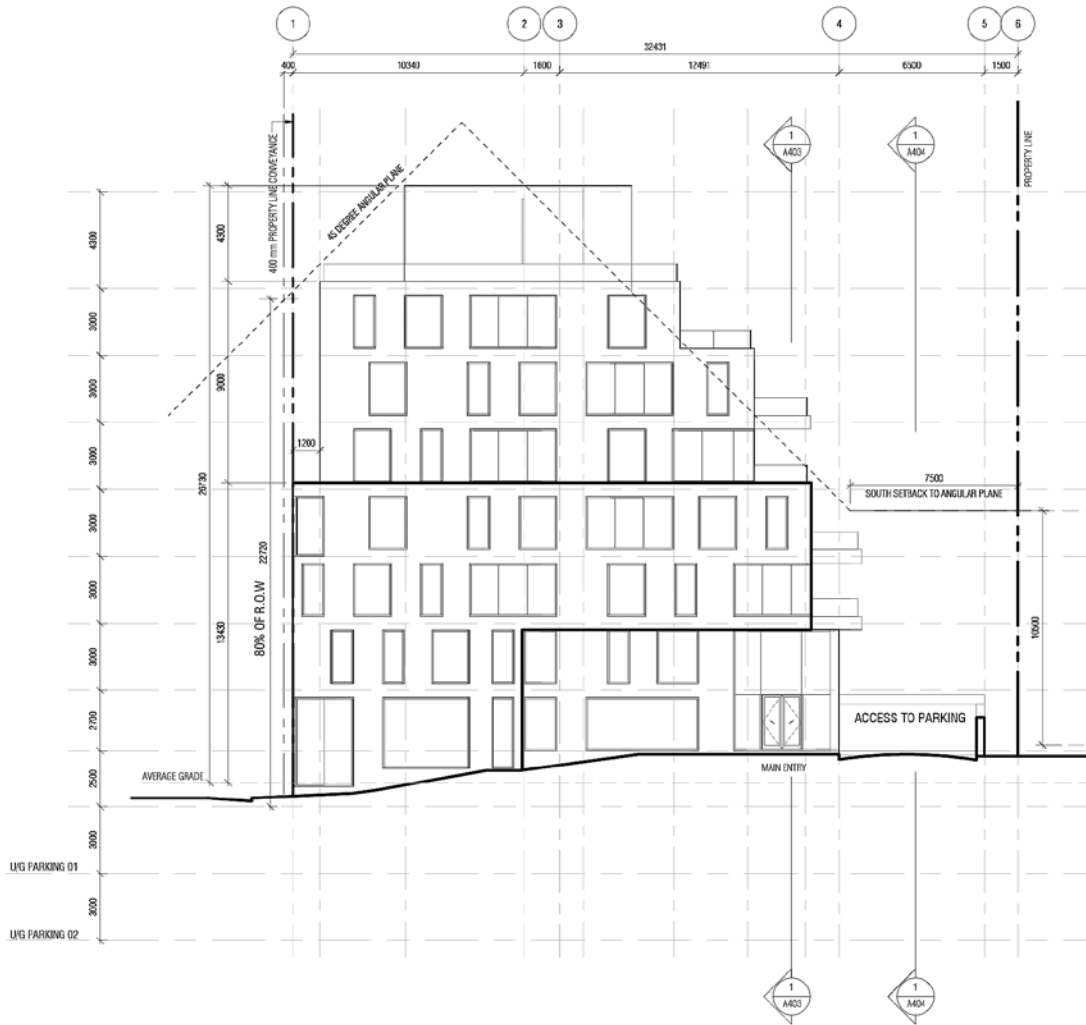
Applicant's Submitted Drawing

Not to Scale 
10/08/2016

2115-2117 Bloor Street West
and 19 Harcourt Road

File # 15 122324 WET 13 OZ

Attachment 2a: West Elevation



West Elevation

Elevations

Applicant's Submitted Drawing

Not to Scale
10/06/2016

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Attachment 2b: North Elevation



North Elevation

Elevations

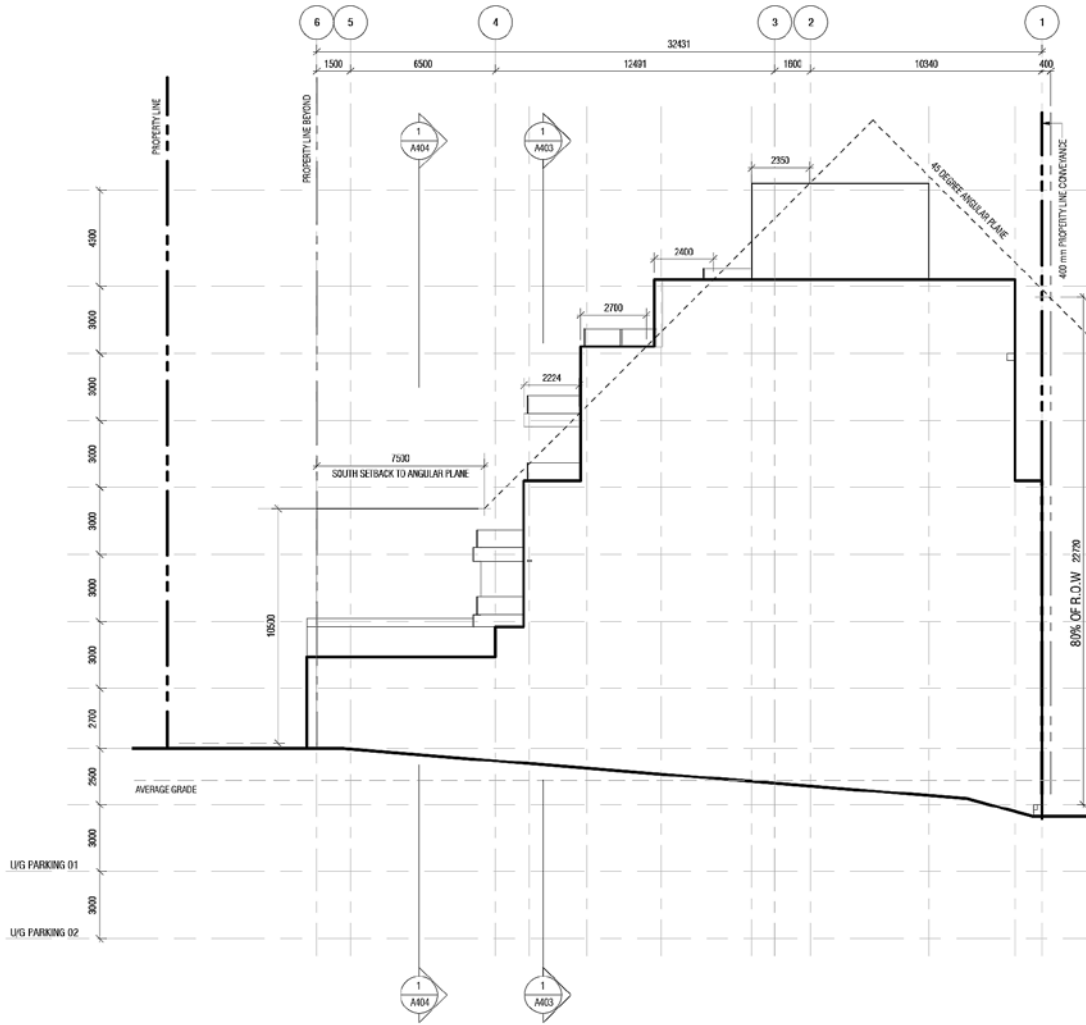
Applicant's Submitted Drawing

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10/06/2016

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Attachment 2c: East Elevation



East Elevation

Elevations

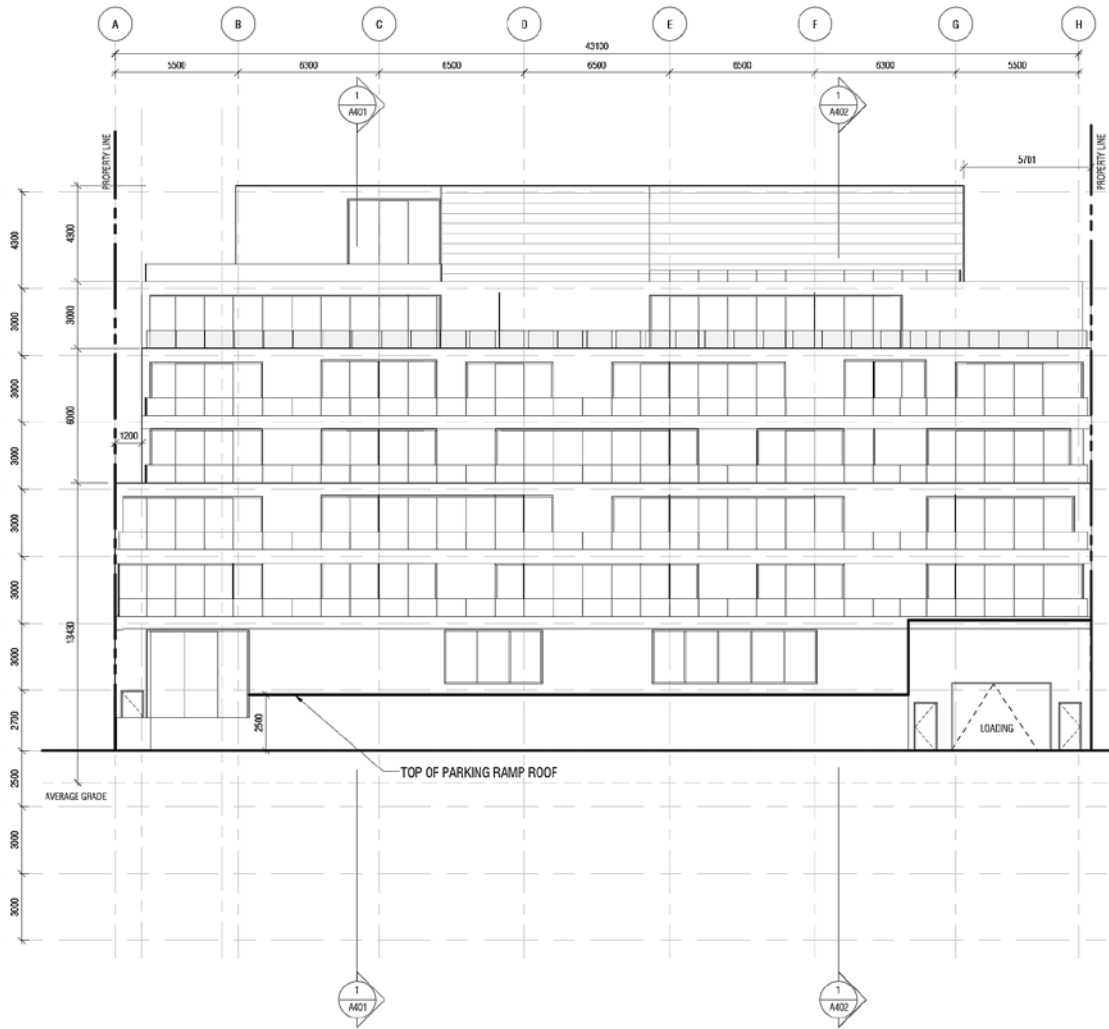
Applicant's Submitted Drawing

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10/06/2016

**2115-2117 Bloor Street West
and 19 Harcourt Road**

File # 15 122324 WET 13 0Z

Attachment 2d: South Elevation



South Elevation

Elevations

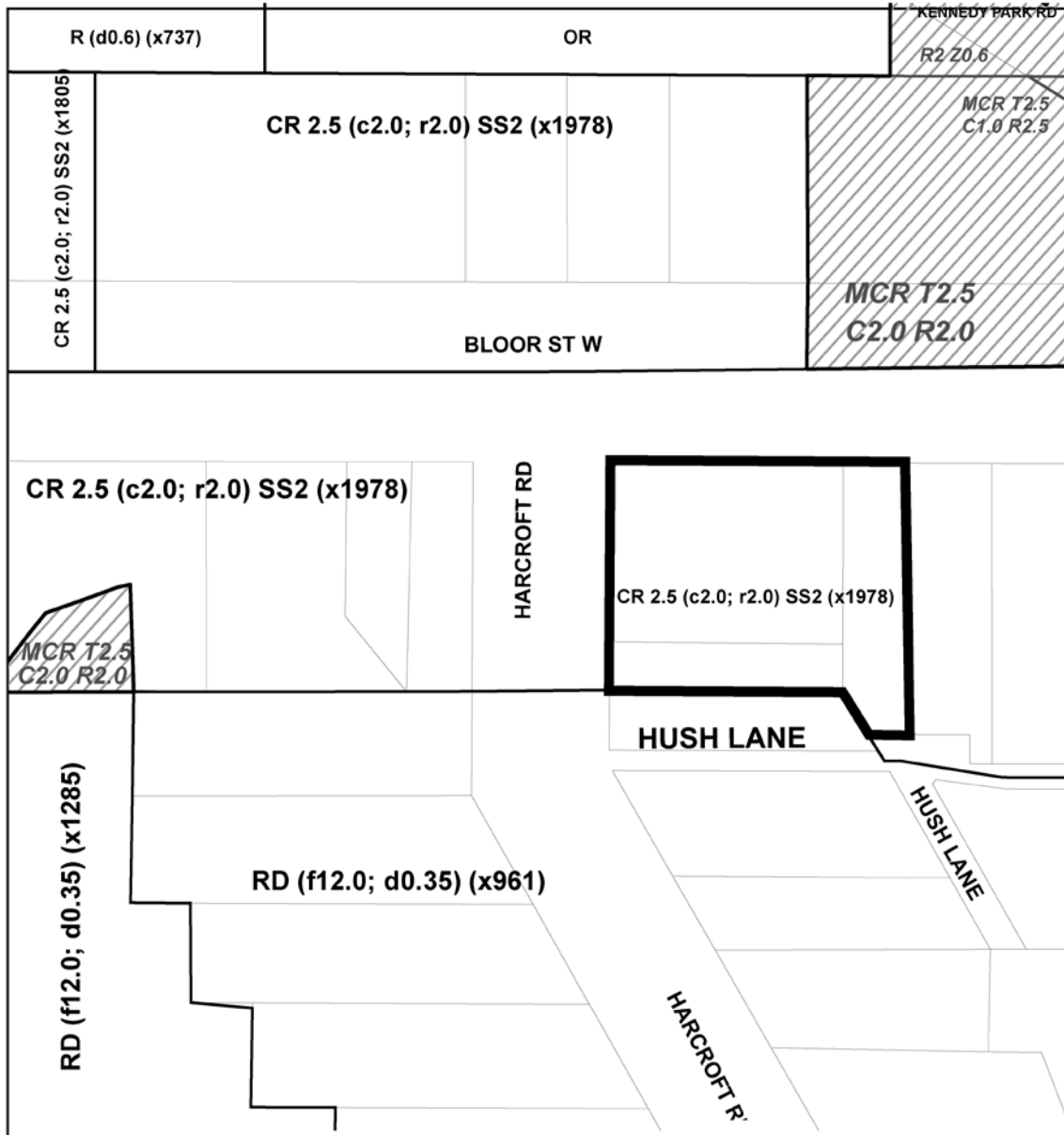
Applicant's Submitted Drawing

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Attachment 3: Zoning



TORONTO City Planning
Zoning By-law 569-2013

**2115-2117 Bloor Street West
 and 19 Harcroft Road**
 File # 15 122324 WET 13 0Z

-  Location of Application
- R** Residential
- RD** Residential Detached
- CR** Commercial Residential
- OG** Open Space Golf Course

-  See Former City of Toronto Bylaw No. 438-86
- R2** Residential District
- MCR** Mixed-Use District



Not to Scale
 Extracted 04/09/2015

**Attachment 4a: Draft Zoning By-law Amendment
(Former City of Toronto Zoning By-law No. 438-86)**

Authority: Etobicoke York Community Council Item XX, as adopted by City of Toronto Council on XX.

CITY OF TORONTO

Bill No.

BY-LAW No. -201X

To amend the General Zoning By-law No. 438-86 of the former City of Toronto with respect to lands known municipally as 2115-2117 Bloor Street West and 19 Harcourt Road.

WHEREAS authority is given to Council by Section 34 of the Planning Act, R.S.O. 1990, c.P. 13, as amended, to pass this By-law; and

WHEREAS Council of the City of Toronto has provided adequate information to the public and has held at least one public meeting in accordance with the Planning Act; The Council of the City of Toronto HEREBY ENACTS as follows:

1. None of the provisions of Section 2 with respect to the definitions of grade and lot, and Sections 4(2)(a); 4(4)(b); 4(6)(b); 4(6)(c); 4(10)(a); 4(12); 4(16) 8(3) PART I, 1, 2 and 3(a); and 8(3) PART II, 1(a)(i); 8(3) Part II 4; 12 (2) 257; of former City of Toronto By-law No. 438-86, as amended, titled "A By-law to regulate the use of land and the erection, use, bulk, height, spacing of and other matters relating to buildings and structures and to prohibit certain uses of lands and the erection and use of certain buildings and structures in various areas of the City of Toronto", shall apply to prevent the erection or use of a mixed use building on the lot, provided that:
 - (a) the *lot* comprises the lands delineated by heavy lines on Map 1, attached to and forming part of this By-law;
 - (b) despite any existing or future consent, partition or division of the *lot*, the provisions of this By-law shall apply to the *lot* as if no consent, partition or division occurred;
 - (c) no above *grade* portion of a building or structure erected or used on the *lot* shall be located other than wholly within the areas delineated by heavy lines on Map 2, attached to and forming part of this By-law, except for the following:
 - (i) cornices, lighting fixtures, awnings, ornamental elements, parapets, trellises, eaves, window sills, guardrails, balustrades, railings,

wheel chair ramps, stairs, stair enclosures, vents, elevator cores, underground garage ramps and their associated structures, fences, safety railings, screens, landscape features;

- (ii) balconies, which may extend to a maximum horizontal projection from an exterior building wall of 2.3 m on the second and third residential levels, 2.5 m on the sixth residential level and 1.5 m for all other residential levels, beyond the heavy lines shown on Map 2.
- (d) no part of any building or structure erected or used on the *lot*, shall exceed the *heights* in metres specified by the numbers following the symbol "H" on the attached Map 2, including any mechanical space and elevator/stair overrun, with the exception of the following:
- (i) structures on any roof used for outside or open air recreation, maintenance, safety, or wind protection purposes, including green roofs, parapets, terrace guards, screens, and window washing equipment, except on areas identified as H 4.5 and H 7.5 on Map 2 attached hereto, which may be used for green roofs only, provided:
 - (1) no part of such structures is higher than the sum of 3.0 metres and the *height* limits specified on Map 2; and
 - (2) the structures do not enclose space so as to constitute a form of penthouse.
- (e) the total combined *residential gross floor area* and *non-residential gross floor area* erected or used on the *lot* shall not exceed 6,500 square metres;
- (f) the *residential gross floor area* erected or used on the *lot* shall not exceed a total of 5,550 square metres and the total number of *dwelling units* shall not exceed 60 on the *lot*;
- (g) the *non-residential gross floor area* erected or used on the *lot* shall:
- (1) not exceed a total of 1,000 square metres, and
 - (2) only be used for one or more of the uses listed in the chart set forth in Section 8(1)(f)(b)(iv) "Retail and Service Shops" and a maximum 200 m² for a restaurant, take-out restaurant or bake shop of *By-law No. 438-86*, as amended and subject to the qualifications listed in such chart as are applicable in a CR district for such use;
- (h) *residential amenity space* shall be provided as follows:

- (i) a minimum of 108 square metres of indoor *residential amenity space* for use by residents of the *mixed-use building* shall be provided in a multi-purpose room or rooms, within the building erected on the *lot*; and
- (ii) a minimum of 115 square metres of outdoor *residential amenity space* shall be provided on the *lot* for use by residents, accessible to, and adjoining a multi-purpose indoor residential amenity area.

(I) the minimum number of *parking spaces* shall be:

Residential Dwelling Unit Parking

0.5 *parking space* for each *bachelor and one-bedroom dwelling unit*;
 0.75 *parking space* for each *two-bedroom or more dwelling unit*;

Visitor Parking:

0.06 *parking space* for each *dwelling unit*.

Retail and Commercial Parking

1 parking space per 100 m² of gross floor area

- (j) if the calculation of the number of required *parking spaces* results in a number containing a fraction, the number must be rounded down to the nearest whole number, but in no case may it be less than one *parking space*;
- (k) the parking facilities required for residential visitors and all driveways or passageways providing ingress thereto shall be shared by the parking facilities and driveways required for residential occupants;
- (l) a minimum of one *loading space-type G* (13m long by 4m wide with a vertical clearance of 6.1m) shall be provided for both residential and non-residential uses;
- (m) a minimum of 54 *bicycle parking spaces- occupant* and a minimum of 2 *bicycle parking spaces - visitors* shall be provided and maintained on the *lot*.

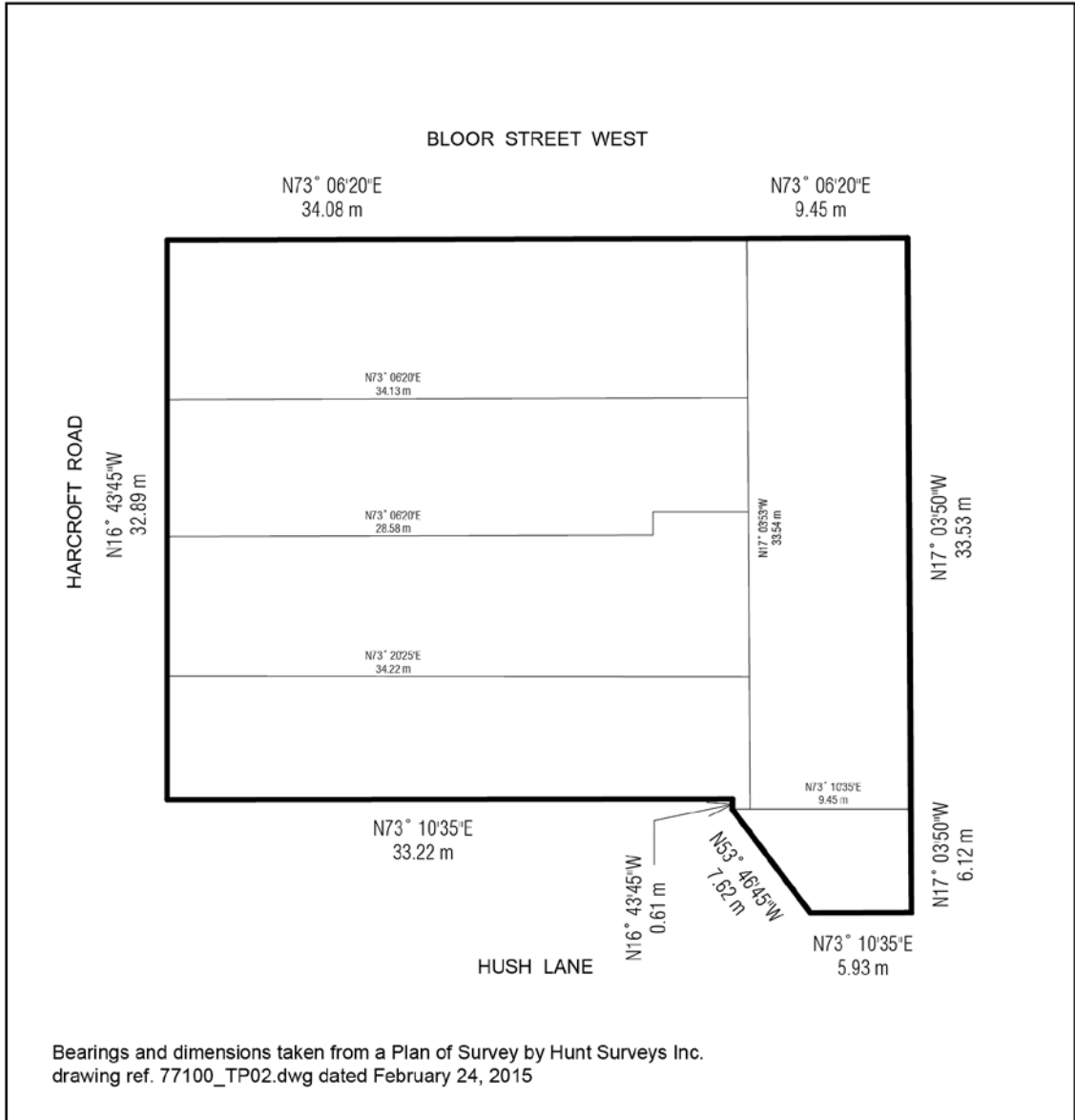
2. For the purposes of this By-law the following definitions shall apply:

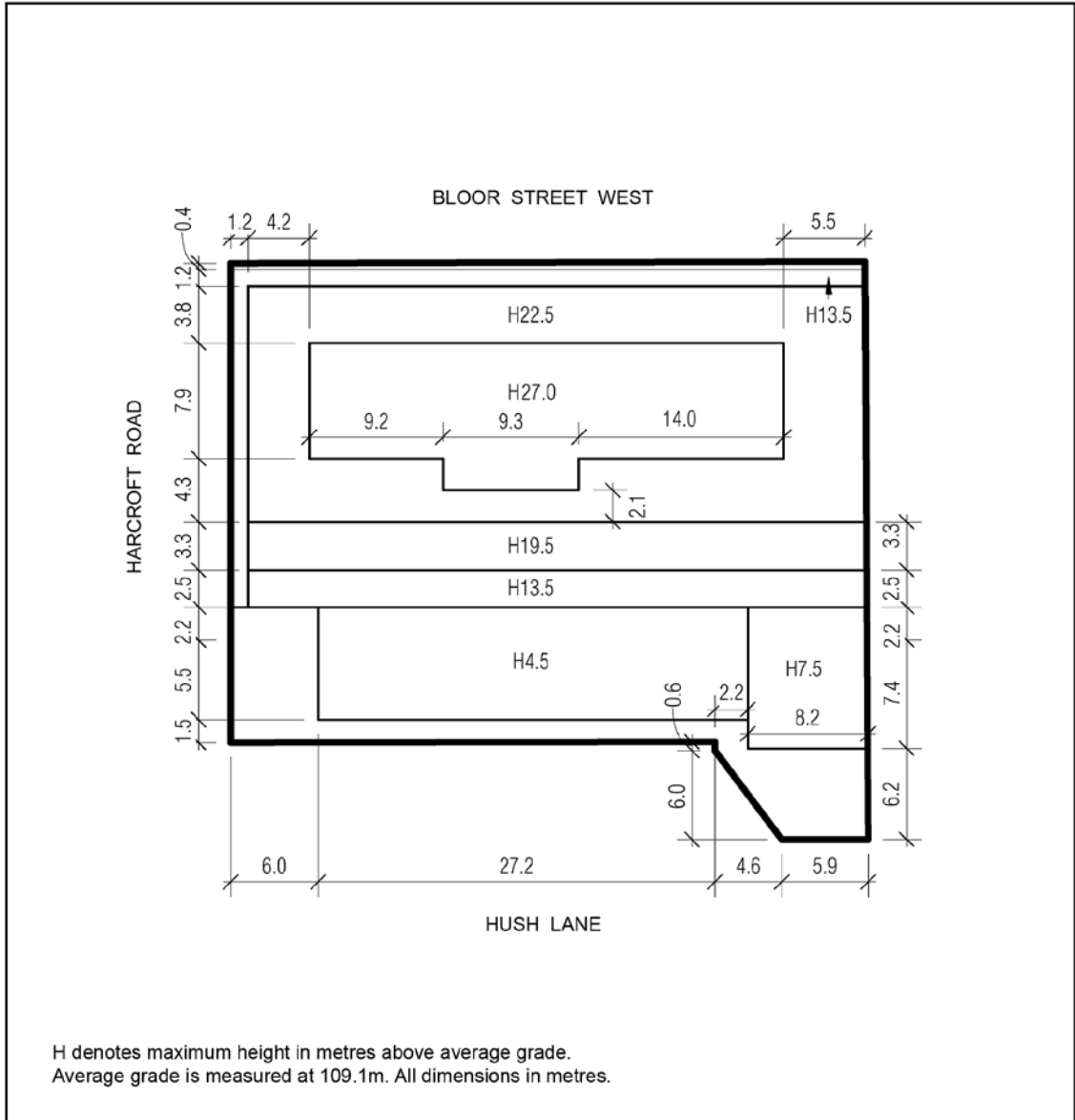
- (i) "*By-law No. 438-86*" means By-law No. 438-86, as amended, of the former City of Toronto being, "A By-law to regulate the use of land and the erection, use, bulk, height, spacing and other matters relating to buildings and structures and to prohibit certain uses of

lands and the erection and use of certain buildings and structures in various areas of the City of Toronto";

- (ii) "*grade*" means an elevation of 109.1 metres Canadian Geodetic Datum;
 - (iv) "*height*" means the vertical distance between *grade* and the highest point of the building or structure;
 - (v) "*lot*" means the lands outlined by heavy lines on Map 1 attached to this By-law;
 - (viii) each other word or expression, which is italicized in this By-law shall have the same meaning as each such word or expression as defined in *By-law No. 438-86*.
3. No person shall use any land or erect or use any building or structure on the *lot* unless the following municipal services are provided to the *lot* line and the following provisions are complied with:
- (a) all new public roads have been constructed to a minimum base curb and base asphalt and are connected to an existing public highway; and
 - (b) all water mains and sanitary sewers, and appropriate appurtenances, have been installed and are operational.
4. Except as otherwise provided herein, the provisions of *By-law 438-86*, as amended, shall continue to apply to the *lot*.

ENACTED AND PASSED this ____ day of _____, 20____.





**Attachment 4b: Draft Zoning By-law Amendment
(City of Toronto Zoning By-law No. 569-2013)**

Authority: Etobicoke York Community Council Item XX, as adopted by City of Toronto Council on XX.

CITY OF TORONTO

BY-LAW No. XXXX - 201X

To amend Zoning By-law No. 569-2013, as amended, with respect to the lands municipally known in 201X as 2115-2117 Bloor Street West and 19 Harcourt Road.

Whereas Council of the City of Toronto has the authority pursuant to Section 34 of the Planning Act, R.S.O. 1990, c. P.13, as amended, to pass this By-law; and

Whereas Council of the City of Toronto has provided adequate information to the public and has held at least one public meeting in accordance with the Planning Act;

The Council of the City of Toronto enacts:

1. The lands subject to this By-law are outlined by heavy black lines on Diagram 1 attached to this By-law;
2. The words highlighted in bold type in this By-law have the meaning provided in Zoning By-law No. 569-2013, Chapter 800 Definitions;
3. Zoning By-law No. 569-2013, as amended, is further amended by amending the zone label on the Zoning By-law Map in Section 990.10 respecting the lands outlined by heavy black lines to CR4.18 (c0.7; r3.6) SS2 (XXXX), as shown on Diagram 2 attached to this By-law;
4. Zoning By-law No. 569-2013, as amended, is further amended by adding Article 900.11. [--] Exception Number [###] so that it reads:

Exception CR (XX)

The lands, or portion thereof as noted below, are subject to the following Site Specific Provisions:

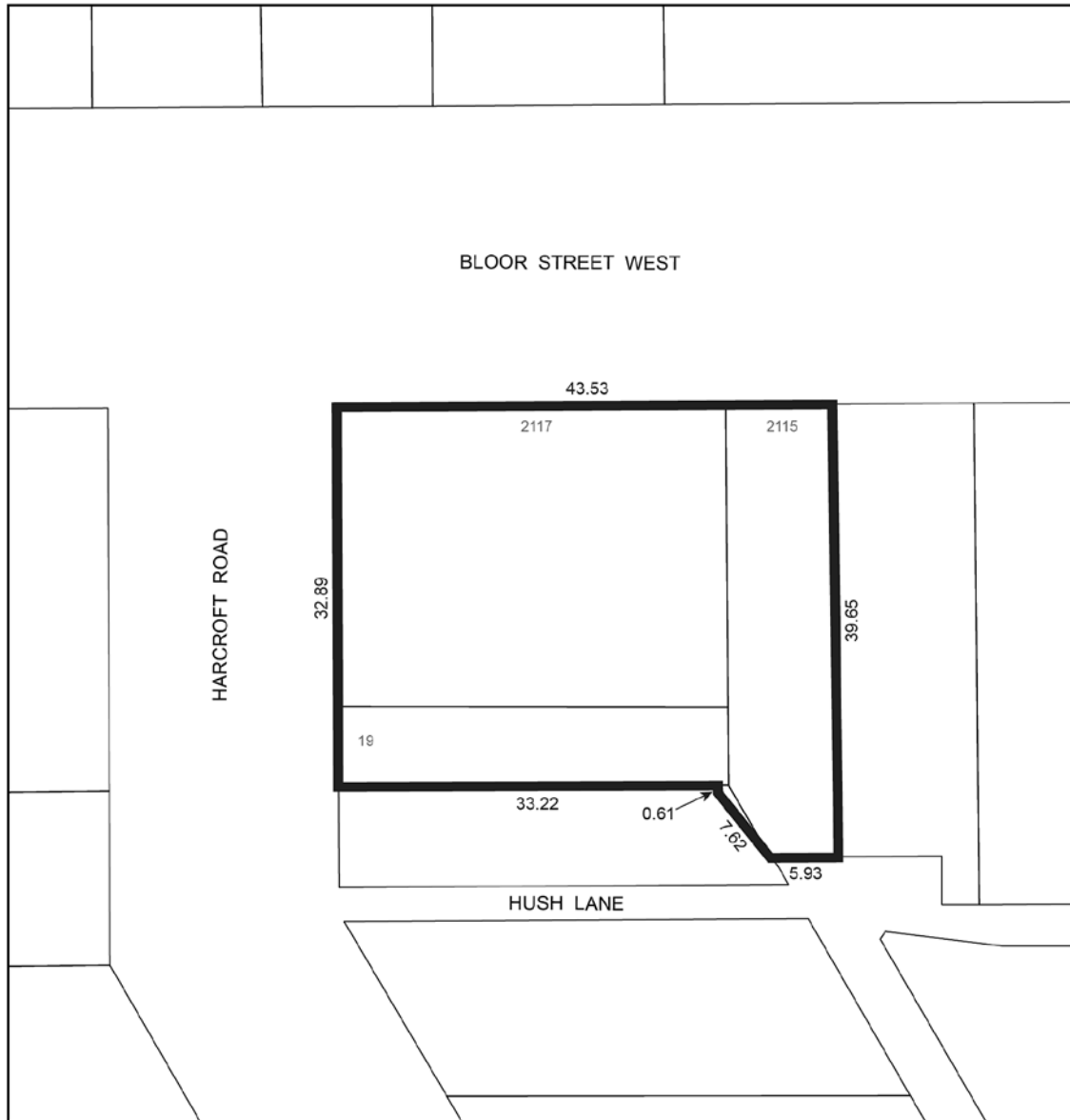
Site Specific Provisions

- (A) Despite Regulation 40.5.40.10(1), height shall be measured from an elevation of 109.1 metres Canadian Geodetic Datum

- (B) Despite Regulation 40.10.40.10(2), the maximum height of a **building** or **structure** is 27 metres.
- (C) Despite Regulation 40.10.40.70(2)(B):
- (i) the minimum **building setback** from the **rear lot line** for an enclosed access ramp to an underground **parking garage** is 1.5 metres
 - (ii) the minimum **building setback** from the **rear lot line** for an enclosed **loading space** is 1.5 metres
- (D) Despite Regulation 40.10.40.70(2)(E),
- (i) a balcony may encroach horizontally into the south facing **angular plane** to a maximum distance of:
 - (a) 2.3 meters for the 6th **storey**;
 - (b) 2.7 metres for the 7th **storey**; and
 - (c) 2.4 metres for the 8th **storey**; and
 - (ii) a main wall may encroach horizontally into the south facing **angular plane** to a maximum distance of:
 - (a) 2.7 meters for the 6th **storey**;
 - (b) 2.4 metres for the 7th **storey**; and
 - (c) 2.4 metres for the 8th **storey**; and
 - (d) 2.4 metres for the enclosed mechanical and amenity space; and
 - (iii) a main wall may encroach horizontally into the east facing **angular plane** to a maximum distance of 1.0 metres for the 7th **storey** and 8th **storey** terrace to a maximum of 1.0 m
- (E) Despite Regulation 40.10.90.10(1), a **loading space** may be located in a **rear yard** that abuts a **lot** in the Residential zone category
- (F) Despite Regulation 40.10.90.40(2), **vehicle** access to the **loading space** may be over a **lot** in a Residential Zone category.
- (G) Despite Regulation 40.10.40.10(5), the required minimum height of the first **storey**, is measured between the floor of the first **storey** and the ceiling of the first **storey**, is 4.5 metres for a minimum of the front 10 metres measured from the main front wall of the mixed used building only.
- (H) Despite Regulation 40.10.50.10 (3), access to a loading facility is permitted.
- (I) Despite Regulation 220.5.10.1 (3), a Type “G” loading facility may be used for both retail and residential use purposes.

- (J) Despite Regulation 40.10.40.50 (1), a minimum of 108 square metres of indoor amenity area and 115 square metres of outdoor amenity are permitted.
- (K) Despite Section 900.11.10 (1978), a restaurant, take-out restaurant or bake shop shall be permitted up to a maximum of 200 square metres of non-residential floor area.

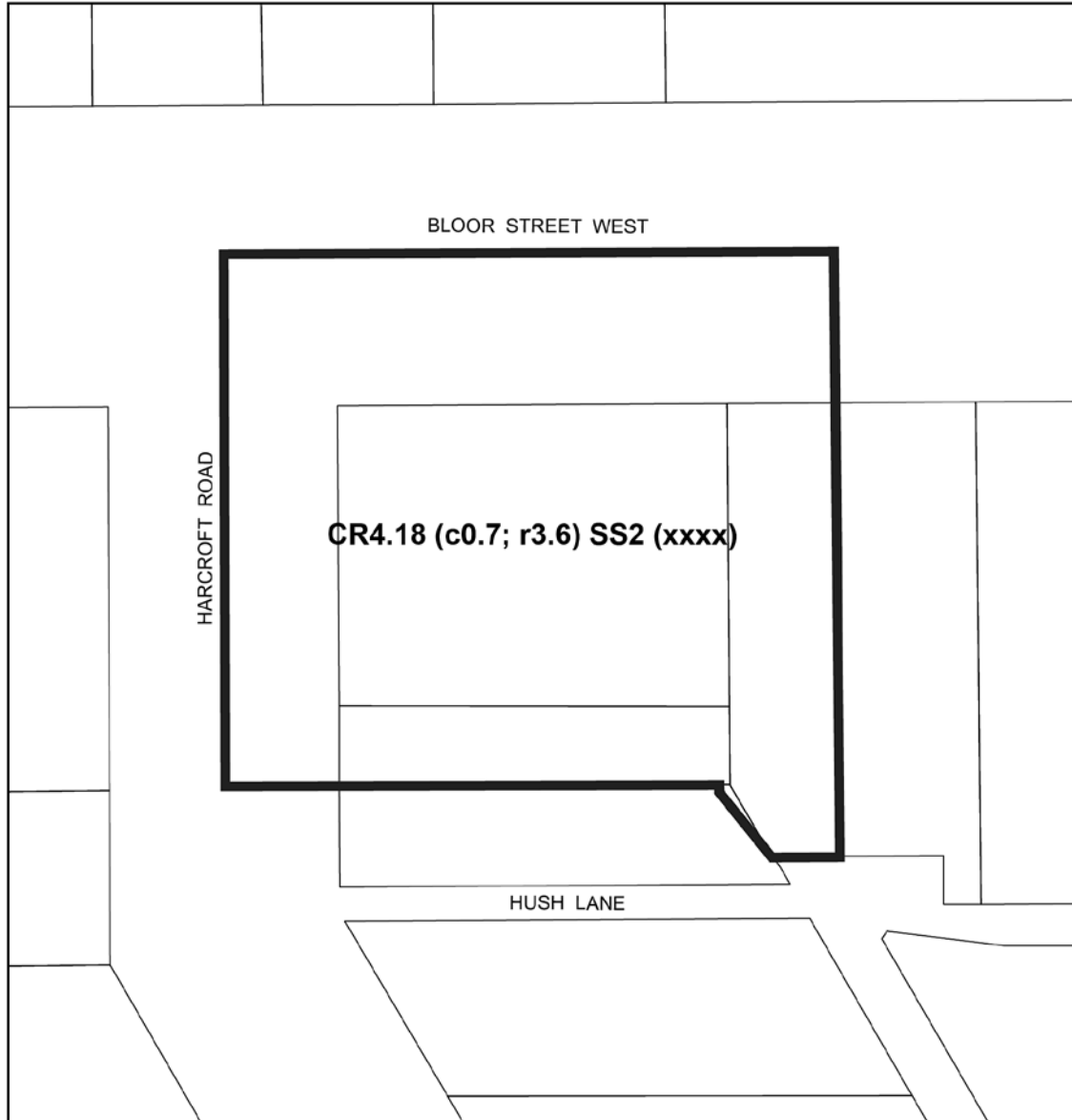
(Seal of the City)



TORONTO City Planning
Diagram 1

**2115-2117 Bloor Street West
 and 19 Harcroft Road**
 File # 15 122324 WET 13 0Z

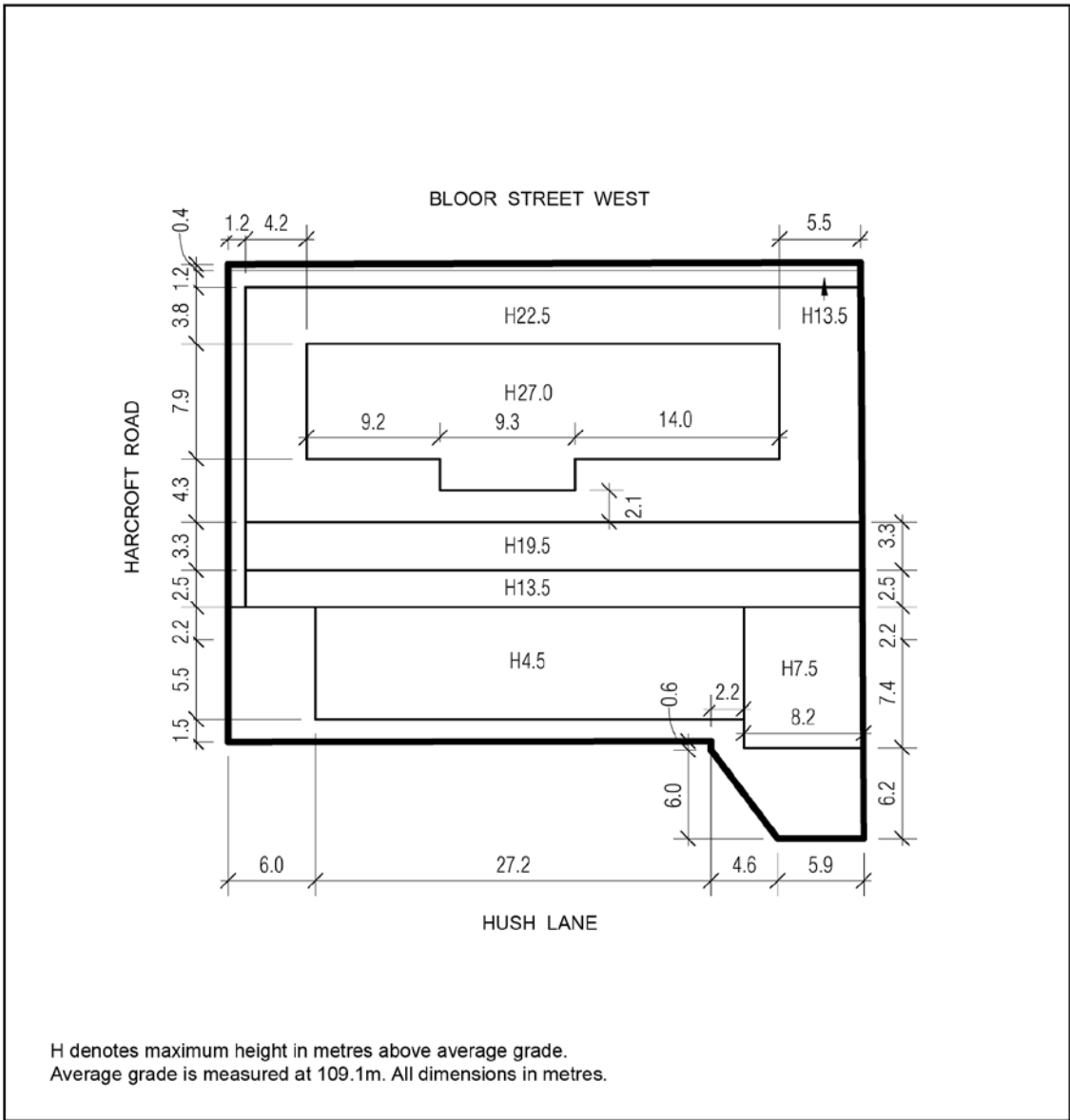
City of Toronto By-Law 569-2013
 Not to Scale
 10/14/2016



TORONTO City Planning
Diagram 2

**2115-2117 Bloor Street West
 and 19 Harcourt Road**
 File # 15 122324 WET 13 0Z

City of Toronto By-Law 569-2013
 Not to Scale
 10/14/2016



Attachment 5: Application Data Sheet

Application Type: Rezoning Application Number: 15 122324 WET 13 OZ
 Details: Rezoning, Standard Application Date: February 27, 2015

Municipal Address: 2115-2117 BLOOR STREET WEST & 19 HARCROFT ROAD

Location Description: PLAN M438 PT LOTS 347 & 349 **GRID W1310

Project Description: Proposed amendments to the City's Zoning By-laws to permit the development of an 8-storey mixed-use building having 60 dwelling units. Non-residential uses including retail are proposed to occupy the ground floor .

Applicant: MMM GROUP LIMITED
Agent: Diamond Schmitt Architects
Architect: Diamond Schmitt Architects
Owner: HARCROFT AND MAIN URBAN PROPERTIES INC

PLANNING CONTROLS

Official Plan Designation: Mixed Use Areas Site Specific Provision:
 Zoning: CR 2.5 (C 2.0; R 2.0) SS2 Historical Status:
 (x1978)
 Height Limit (m): 14 m Site Plan Control Area: Y

PROJECT INFORMATION

Site Area (sq. m):	1499	Height:	Storeys:	8
Frontage (m):	43.5		Metres:	27
Depth (m):	32.9			
Total Ground Floor Area (sq. m):	1115			Total
Total Residential GFA (sq. m):	5258		Parking Spaces:	46
Total Non-Residential GFA (sq. m):	958		Loading Docks	0
Total GFA (sq. m):	6216			
Lot Coverage Ratio (%):	74			
Floor Space Index:	4.18			

DWELLING UNITS

Tenure Type: Condominium
 Rooms: 0
 Bachelor: 0
 1 Bedroom: 39
 2 Bedroom: 19
 3 + Bedroom: 2
 Total Units: 60

FLOOR AREA BREAKDOWN (upon project completion)

	Above Grade	Below Grade
Residential GFA (sq. m):	5258	0
Retail GFA (sq. m):	958	0
Office GFA (sq. m):	0	0
Industrial GFA (sq. m):	0	0
Institutional/Other GFA (sq. m):	0	0

CONTACT: **PLANNER NAME:** Lorne Berg, Senior Planner
TELEPHONE: (416) 394-8230