

City of Toronto

Sherway Area Urban Design & Streetscape Guidelines

Sherway Area Urban Design & Streetscape Guidelines online:
www.toronto.ca/sherwaystudy

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Mandate

BACKGROUND/COUNCIL DIRECTION

In November 2013, City Council directed staff to undertake a review and update of the policy framework for the Sherway Area. Following Council's endorsement of the terms of reference in December 2013, staff initiated the Sherway Area Study with the assistance of a multidisciplinary consulting team to determine appropriate land use planning, urban design, community services and facilities, transportation and servicing capacity for ongoing development of the area.

Sherway Area Planning Study Consultant Team:

Gladki Planning Associates / Planning

DTAH / Urban Design

ARUP / Transportation

The Municipal Infrastructure Group / Servicing

City Council adopted a Directions Report in December 2016, which outlined the key findings of the study that formed the basis for the new Sherway Area Secondary Plan. Council also requested staff to bring forward Urban Design and Streetscape Guidelines for the Sherway Area, generally in accordance with those developed over the course of the study.

In October 2019, the Sherway Area Secondary Plan was brought before City Council for adoption.

Developed as a direct outcome of the Council-directed Sherway Area Study, these Guidelines are intended as an accompanying document to the Official Plan and the Sherway Area Secondary Plan. The Urban Design and Streetscape Guidelines in this document are intended to support high quality, appropriately scaled development in the Sherway Area coupled with a cohesive, green and vibrant public realm.

These guidelines are intended to be read together with all applicable City policies and guidelines.

1.0 Introduction

- 1.1 Background and Role of the Guidelines**
- 1.2 Area Description**
- 1.3 Vision**
- 1.4 Guiding Principles**
- 1.5 Structure**

1.1 BACKGROUND AND ROLE OF THE GUIDELINES

These design guidelines are one component of a comprehensive, four-phased multidisciplinary study of the Sherway Area that identified the future land use mix, density limits, built form, preferred road network and block plan, public realm and streetscape improvements, parks and open space linkages, servicing and community infrastructure requirements.

The guidelines that follow illustrate the essential elements that will shape the emerging community. They communicate the intent of the Sherway Area Secondary Plan. The design framework provides a potential pattern of development blocks reinforced by a system of public streets, parks and open spaces that are scaled to pedestrian activity and movement. Together with the built form, public realm, sustainability and streetscape guidelines, the guidelines will support ongoing implementation, provide a broad perspective for incremental change and present metrics to assist in the review of development proposals.

These guidelines are based on the background analysis and the testing of alternatives completed through the Sherway Area Study. These analyses and explorations informed the guiding principles and resulted in design guidelines that reinforce the intent of the Official Plan and Secondary Plan and demonstrate how they can be implemented.

The Streetscape Guidelines provide direction for the dimensions and details of the boulevard, including both public right-of-way and private lands fronting onto streets. They serve as a toolkit to ensure that existing and new streets are consistent and reinforce the vision for the Sherway Area. The Streetscape Guidelines build upon the existing City standards, to ensure consistency, but also provide details to create a unique character for the Sherway Area that helps build its sense of place.

This is a reference document to help guide private development and public improvements. The Sherway Area Urban Design Guidelines and Streetscape Guidelines are intended to complement each other from a macro and micro perspective, respectively. The guidelines provide an additional level of direction for City Planning policies and guidelines, to work in conjunction with future Precinct Plans and any other area-specific policy documents approved by the City.



Bryant Park, New York City. (c. OLIN)

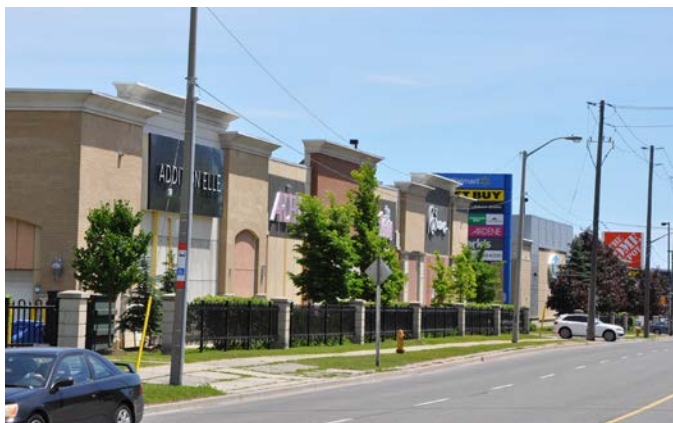
1.2 AREA DESCRIPTION

The Sherway Area is bounded by Highway 427 to the east, the CP rail corridor to the north, the Etobicoke Creek to the west, and the Queen Elizabeth Way (QEW) to the south. These boundaries, generally fixed with limited permeability, form physical barriers to the Sherway Area from its surroundings.

Current Land Uses

The predominant land use in the Sherway Area is retail, anchored by the Sherway Gardens shopping centre. Additional retail in a big box format has developed nearby, generally north of The Queensway and North Queen Street. Employment uses continue to exist in the north portion of the area. South of the shopping centre, four high-rise residential towers have been constructed over the past decade. Further south, two older office buildings abut the QEW. The Trillium Health Centre hospital is located in the western portion of the site.

The Official Plan land use designations found within the Sherway Area are mostly *Mixed Use Areas*, with some *Employment Areas* to be preserved and enhanced along the north edge and south edge of the site. The Trillium hospital lands are designated *Institutional Areas*.



Existing big-box retail buildings north of The Queensway.



Existing office buildings along Evans Avenue.



Sherway Gardens Shopping Centre north expansion.



Existing residential buildings at intersection of The West Mall and Evans Avenue.

Existing Character

The Sherway Area consists of a variety of built form including big-box retail stores, mid-rise office and institutional as well as tall residential buildings. The blocks north of The Queensway are predominantly low-rise big-box commercial structures surrounded by parking. Approximately one third of the Study Area is occupied by the Sherway Gardens property, which sits on the south side of The Queensway.

South of the shopping centre are mid-rise and tall buildings. The mid-rise buildings contain office and institutional uses while the tall buildings are exclusively residential.

The Sherway Area includes part of the Etobicoke Creek ravine. This area is designated *Natural Areas* and provides a robust green edge west of the Sherway Area. Overall, the Sherway Area is currently auto-dominated and does not contain any public parks. The existing public realm is minimal and has little tree canopy. The existing trees in the Sherway Area are struggling and not in optimum health.

Current Mobility Conditions

Streets in the Sherway Area currently prioritize vehicular access. The Queensway provides a pedestrian and cyclist route crossing over the Etobicoke Creek and a connection between Mississauga and Toronto. Transit service is provided by local buses with direct connections to major regional transportation hubs. While sidewalks exist primarily along the edges of large sites, access into the blocks is limited and the presence of expansive surface parking lots discourages walkability.

Site and Planning Constraints

An additional physical feature of the Area is the presence of three hydro corridors running through its western and northern portions. Two corridors currently have above-ground infrastructure hydro towers, with one reserved for future use. The south side of The Queensway includes an easement for Enbridge Gas. Other utility and transportation corridors that define the edges of the Area include the CN Rail line to the north, Highway 427 and the QEW.



Hydro towers at the north end of the Sherway Area.



CP rail line at the northern edge of the Sherway Area.



Sherway Area boundary and location aerial.

1.3 VISION

The Sherway Area has experienced several stages in its evolution, from agricultural lands, to lands developed with primarily institutional and industrial uses in the 1950s, through its emergence as a car-oriented, regional retail destination. The Sherway Area will build on its economic vitality through increased residential and commercial growth coordinated with infrastructure, amenities and services to support a new, prosperous and complete community at a local scale.

Investment in the Sherway Area will provide opportunities to create new streets, parks and open spaces, community facilities and municipal servicing to ensure safe, active, healthy and vibrant neighbourhoods where people will live, work and undertake activities of daily life. Over time, the Sherway Area will emerge as an urbanized node for southwest Toronto, subordinate in scale and massing to the Etobicoke Centre.

The Sherway Area Secondary Plan establishes a clear pattern of development blocks enriched by a system of public streets, parks and open spaces that are scaled to encourage pedestrian activity and movement. Development will occur incrementally, guided by detailed Precinct Plans, ensuring the ongoing functionality and economic sustainability of the area while considering its longer-term growth potential.



Old York Lane, Yorkville, Toronto. (c.DTAH)

1.4 GUIDING PRINCIPLES

A. Promote a Balanced Mix of Land Uses

The transformation of the Sherway Area is anticipated to unfold over a 20- to 30-year time horizon, and should maintain a mix of current and future land uses that coexist harmoniously to collectively create an active, healthy, vibrant and dynamic community. As redevelopment occurs, the relationship of new uses to existing employment and commercial uses and transportation corridors should be carefully considered to minimize potential conflicts.

A1. The Sherway Area will shift to a more broadly balanced mix of land uses, to ensure the creation of a complete community.

A2. Development will be sited, designed and constructed to mitigate adverse effects from nearby *Employment Areas* and transportation infrastructure.

A3. Residential development will be introduced incrementally in appropriate locations, subject to compatibility and mitigation as required, to recognize and respond to existing and planned conditions.



A balance of residential and non-residential uses can coexist harmoniously. (c. DTAH)



A range of building types can support a variety of uses. (c. DTAH)

B. Create a Green and Cohesive Environment

The character of the Sherway Area will be enhanced through a connected system of high-quality parks, open spaces and green links, and an emerging built environment that brings buildings to the street edge with appropriate landscape setbacks. Sustainable development practices will complement improved stewardship and access to the Etobicoke Creek ravine.

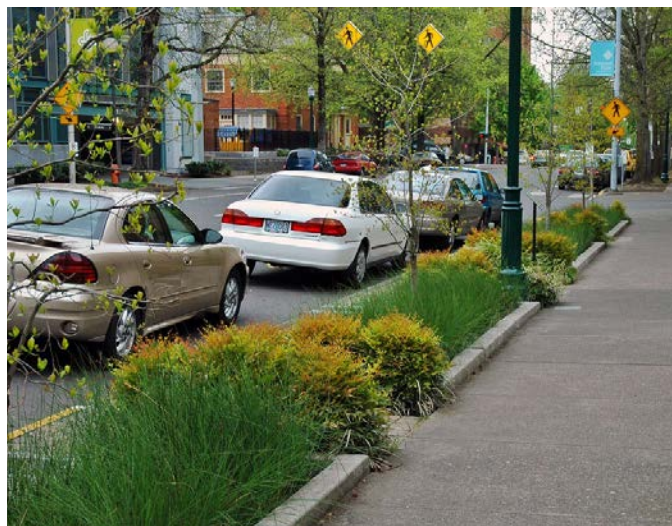
B1. New buildings will establish a cohesive, urban appearance, oriented toward street edges to provide a comfortable pedestrian realm.

B2. The Sherway Area will emerge as a green, safe and attractive place consisting of public streets, parks, promenades, streetscape improvements and Privately Owned Publicly-Accessible Spaces that create a connected neighbourhood and support a range of local social and recreation activities.

B3. Natural areas within and adjacent to the Etobicoke Creek ravine will be protected, restored and enhanced; access to natural areas will be provided at appropriate locations to link them with the parks and open space network of the broader Sherway Area.



Human-scaled built form edges. (Hyattsville, MD; c. DCMUD)



Green streetscaping enhances the public realm. (c. DTAH)

C. Support Community Vitality and Mobility

The Sherway Area will evolve in a manner that reduces reliance on automobile use to and through the emerging community by creating a more walkable and connected local street network, improving cycling connections and facilitating transit use. The vitality of the community will be fostered by introducing new residential populations together with parks, open spaces, facilities, amenities and sufficient municipal services to support growth.

C1. Development will support community-oriented activity and provide a diverse economic base for the area.

C2. Safe and convenient movement through the area and to surrounding neighbourhoods will be enhanced by providing improved opportunities for walking, cycling and public transit use, addressing traffic and congestion issues, and creating new public streets and connections as redevelopment occurs.

C3. Residential development in the area will be coordinated with the provision of new and/or enhanced parks, community services and facilities.

C4. Sufficient water, wastewater and storm servicing infrastructure will be secured to accommodate new development and manage stormwater runoff, together with the integration of green infrastructure.



Integrating community services and facilities with development. (c.DTAH)



Supporting mobility with a generously planted public realm. (c. DTAH)

1.5 STRUCTURE

The Sherway Area covers a considerable land area of over 100 hectares. As new development introduces a growing residential and employment population, there are opportunities to improve the circulation, functionality and livability of the Sherway Area. Establishing a structure for the planned context defines its intended character and identity, and identifies priorities for investment in the public realm for maximum impact and benefit to the emerging community.

The Sherway Area Secondary Plan defines the broad structuring elements of the area, including an expanded public street grid that creates smaller development blocks; preferred locations for new public parks, trails and ravine connections as well as pedestrian mid-block connections; public realm enhancements including Landscaped Points of Entry; and the location of a potential new transit hub. These Guidelines illustrate the intent for each of these elements to contribute to a collective overall identity for the emerging Sherway Area.

At the same time, establishing context at a local level helps to define the character of distinct places within the overall area, guiding their evolution over time while ensuring a logical development pattern. Therefore, the structure of the Sherway Area comprises a collection of diverse precincts that reflect existing and planned local characteristics. A precinct-based approach to planning is intended to shape the cohesive development, built form and public realm character of each distinct precinct.



Foundry Square, City of San Francisco. (c. DTAH)

2.0 Built Form

New buildings play a role in creating good urban places by shaping the pedestrian realm, appropriately addressing existing land uses and incorporating the most recent advances in sustainable building and sound community development principles. Intensification can and should improve overall environmental and community sustainability.

- 2.1 General Massing and Built Form Guidelines**
- 2.2 Setbacks**
- 2.3 Streetwall and Step-backs**
- 2.4 Tall Buildings**
- 2.5 Angular Planes, Transitions and Buffer Edges**
- 2.6 Grade Related Uses**
- 2.7 Parking, Access and Servicing**
- 2.8 Comprehensive Precinct Development**

2.1 GENERAL MASSING AND BUILT FORM GUIDELINES

The scale of new development must be appropriate for its context. Intensification can and should improve overall environmental and community sustainability. Built form analyses conducted through the Sherway Area Study concluded that the policy context, guiding principles, and direction received from the public consultation activities can be satisfied if mid-rise development is the primary form of intensification, with tall buildings sited strategically at key locations within the Study Area.

On the Main Streets, buildings should collectively provide a relatively consistent and contiguous street edge that provides a strong architectural presence. Continuity in the built-up edge of the blocks on the Main Streets such as The Queensway, North Queen Street and The West Mall will create a sense of place and vitality for the pedestrian boulevards and support a viable retail environment.

The built form guidelines are intentionally non-descriptive regarding architectural style and detailing to allow for the widest range of architectural creativity. Buildings frame, define and animate public spaces. They create a consistent street edge and give shape and sense of enclosure which reinforces the pedestrian realm. Grade-related uses, including retail where appropriate, animate the pedestrian environment and provide transition between public and private spaces.



Rockville, Maryland. (c. DTAH)

Low-Rise Buildings are appropriate in the interior of larger and deeper blocks, as well as within retail areas and the *Employment Areas* in the northern portion of the Sherway Area. Townhouses or street-related office and commercial uses will create a finer building scale within the larger blocks and act as a transition from taller, more intense building types.

Mid-Rise Buildings are envisioned as the predominant built form within the Sherway Area. This form of development will provide substantial built-up edges and a mix of uses to give a sense of enclosure as well as reinforcing the pedestrian realm by framing, defining and animating public spaces.

Tall Buildings are to be sited strategically at key locations within the Sherway Area. They can serve as landmarks within the Sherway Area and emphasize important intersections and entrances to the Area.

2.1.1 A progressive contemporary expression of art, architecture and landscape architecture is encouraged in the Sherway Area.



Residential Mixed-Use Mid-Rise (The Queensway; c. DTAH)



Transit Oriented Mixed-Use (Dallas, TX.; c. Steve Hinds).



Residential Tall Buildings. (One Sherway)

2.2 SETBACKS

At present, the physical environment of the Sherway Area does not encourage walking or cycling. Setbacks can enhance the public realm experience and buffer ground level uses from the street. The use of larger, consistent setbacks along North Queen Street and The Queensway increases the perceived public realm in these boulevards and creates a sense of place when well utilized and designed. On other streets, shallower setbacks may allow sufficient space for buffering and programming, but are proportional in scale to narrower rights-of-way and shallow building parcels.

In the Sherway Area, building setbacks along all streets and open spaces will reinforce the Transportation Master Plan goals, enhance retail uses and encourage pedestrian and cycling activities.

2.2.1 Setbacks along the Main Streets may need to be increased to allow for a row of trees on private property, adjacent to the public right-of-way.

2.2.2 Balconies should be inset behind a streetwall to improve tree planting conditions, to provide for an enhanced public realm, and to minimize the visual scale and physical massing of a building and its streetwall.

2.2.3 Below grade parking located within a setback on private property must provide a minimum 1.5 metres of high-quality soil above a well-drained sub-soil or drainage layer.



Filmore Street, City of San Francisco. (c. DTAH)

2.3 STREETWALL

Streetwall buildings, primarily mid-rise buildings and the bases of tall buildings, define the frontages of development blocks and provide a sense of enclosure. Streetwall buildings will vary in height between three to seven storeys to create an interesting and variable composition for the public realm edge.

2.3.1 The streetwall of buildings will vary in height and materiality to create an interesting and variable composition for the public realm edge.

2.3.2 New buildings will be designed with regard for the streetwall height, materiality, and physical expression of any adjacent buildings.

2.3.3 The use of awnings, canopies and signs should be incorporated in street-facing facades to reduce the perception of building height from a pedestrian level.

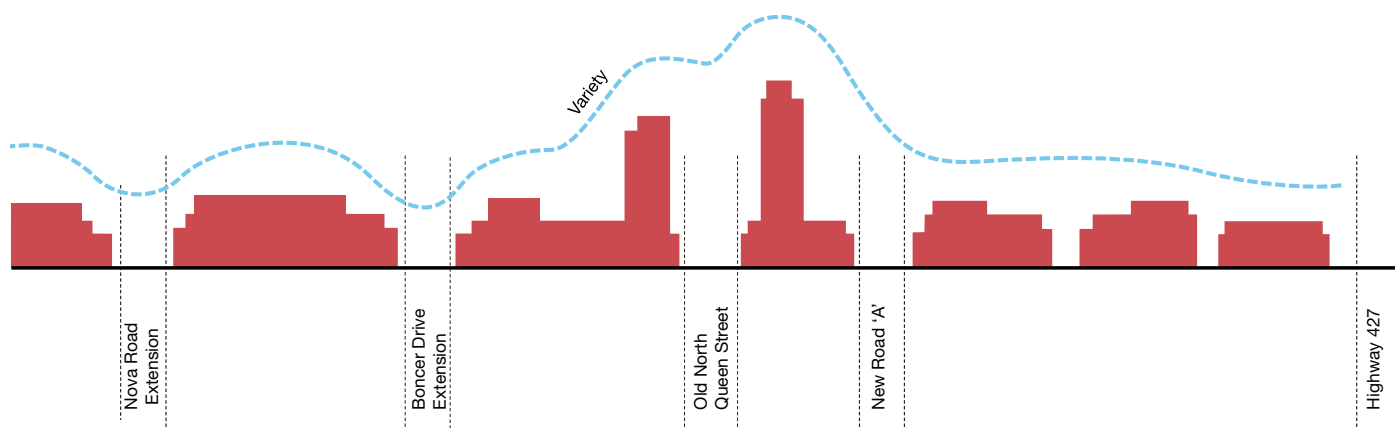
2.3.4 It may be necessary to provide a physical break in a streetwall to create an interesting and variable built form composition, to reduce the physical mass and scale of a development, and to promote and enhance the public realm, connectivity, and the pedestrian experience.



Establishing a consistent streetwall height defines the street edge. (NYC; c. LOT-EK)



Mid-rise buildings should comprise the predominant built form of the Sherway Area. (Sunnyvale, CA; c. Noah Christman)



Conceptual cross-section of the Sherway Area between Nova Road and Highway 427, illustrating a variable streetscape and building heights.

2.4 TALL BUILDINGS

Tall buildings can provide focal points or landmarks at key locations in the Sherway Area. However, it is critical that they do not compromise the integrity and utility of the surrounding public realm.

Tall buildings may be considered at preferred locations where they can serve as landmarks within the Sherway Area, to emphasize important intersections and focused areas of activity. Any tall building proposals will be evaluated against the City's Tall Building Design Guidelines.

2.4.1 Where streets terminate at a Main Street, buildings will be designed to acknowledge the visual terminus using architectural elements.

2.4.2 Tall building heights will reflect the area context, and generally not exceed 30 storeys in height.

2.4.3 Tall building designs in the Sherway Area will achieve a high standard of sustainability and energy efficiency.



RBC Building - tall building with 5 - 7 storey base. (c. DTAH)

2.5 ANGULAR PLANES AND TRANSITIONS

As the Sherway Area includes a variety of existing and potential land uses, special attention must be paid to providing adequate transition and buffer zones to avoid conflict between different uses. This requires addressing functional aspects of separation distances and heights, while maintaining a continuity of uses along street edges, as required.

Residential development adjacent to other land uses will ensure appropriate separation distances and buffer edges. Any development adjacent to parks or natural areas will also ensure comfortable separation distances and buffer edges. All development will be designed to maximize access to sunlight and sky view in adjacent parks and open spaces, according to City performance standards.

2.5.1 All new development will approach design from a precinct perspective to ensure a cohesive character. Comprehensive planning of precincts will result in functional blocks with logical site organization.

2.5.2 Within Precincts that contain a variety of uses, special attention is to be paid to transitions between the different land uses and possible building uses. This requires addressing functional aspects of separation distances and heights, while maintaining a continuity of uses along street edges, as required.

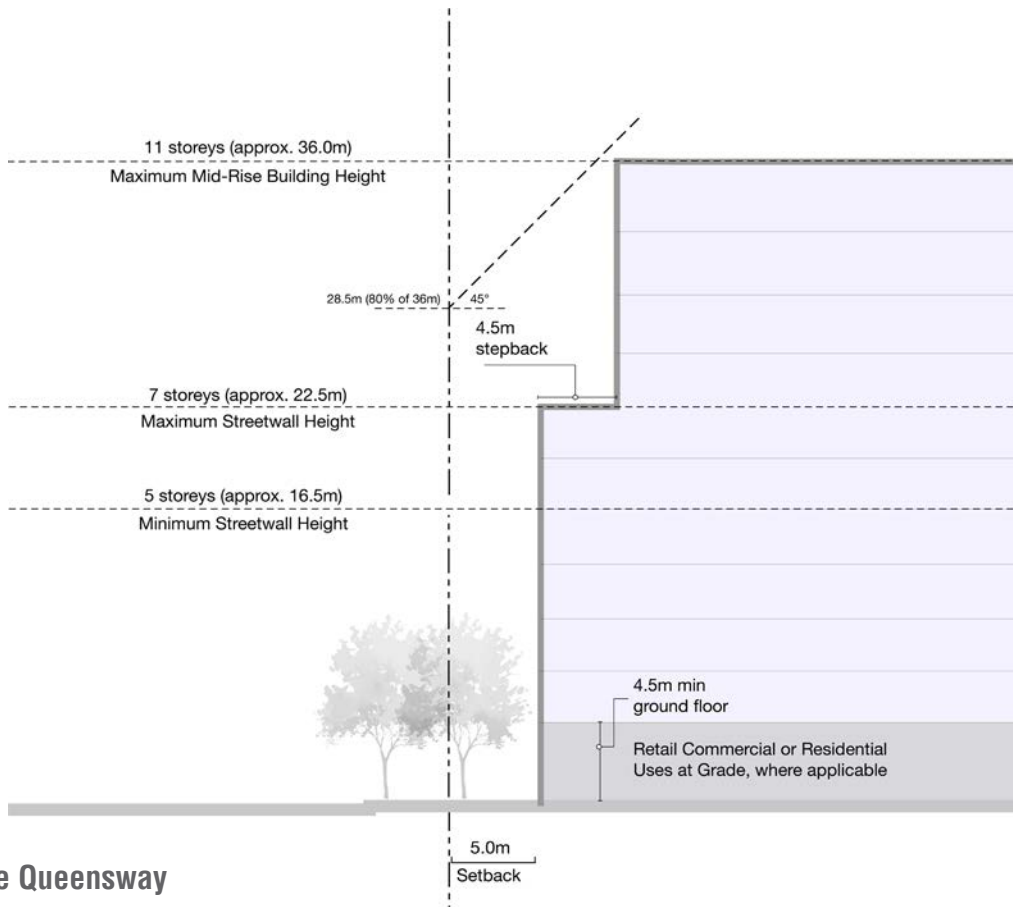
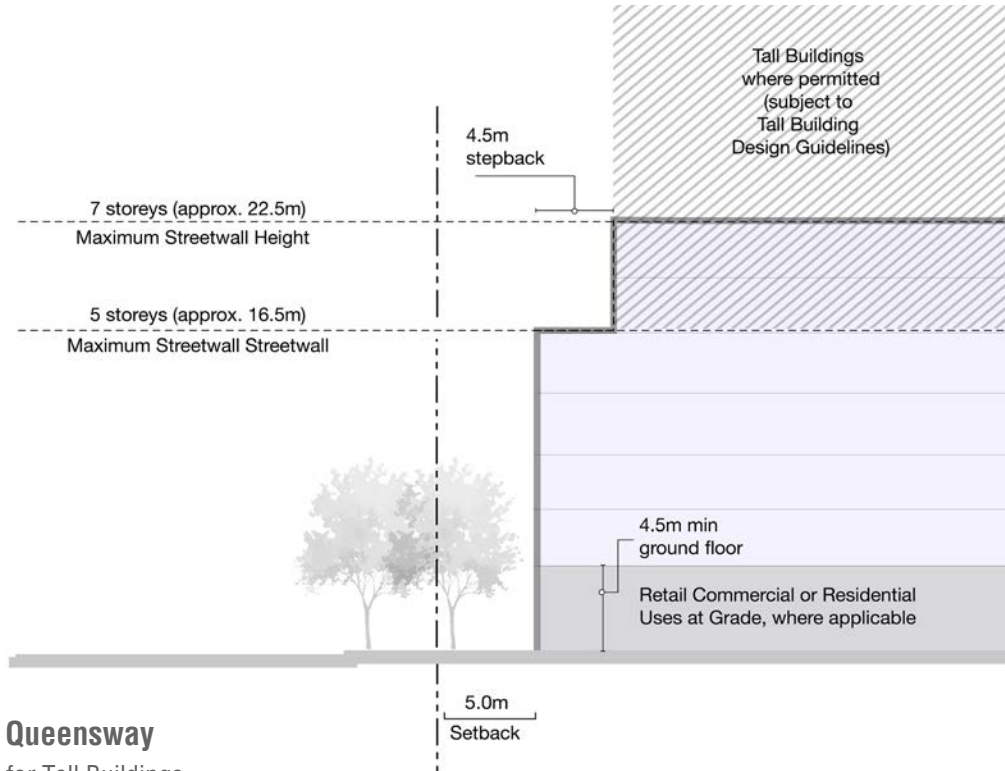
2.5.3 Where commercial uses are less desirable, street-related units are encouraged to articulate the building frontage to reduce its perceived scale, and as a transition from the taller, more intense building types.

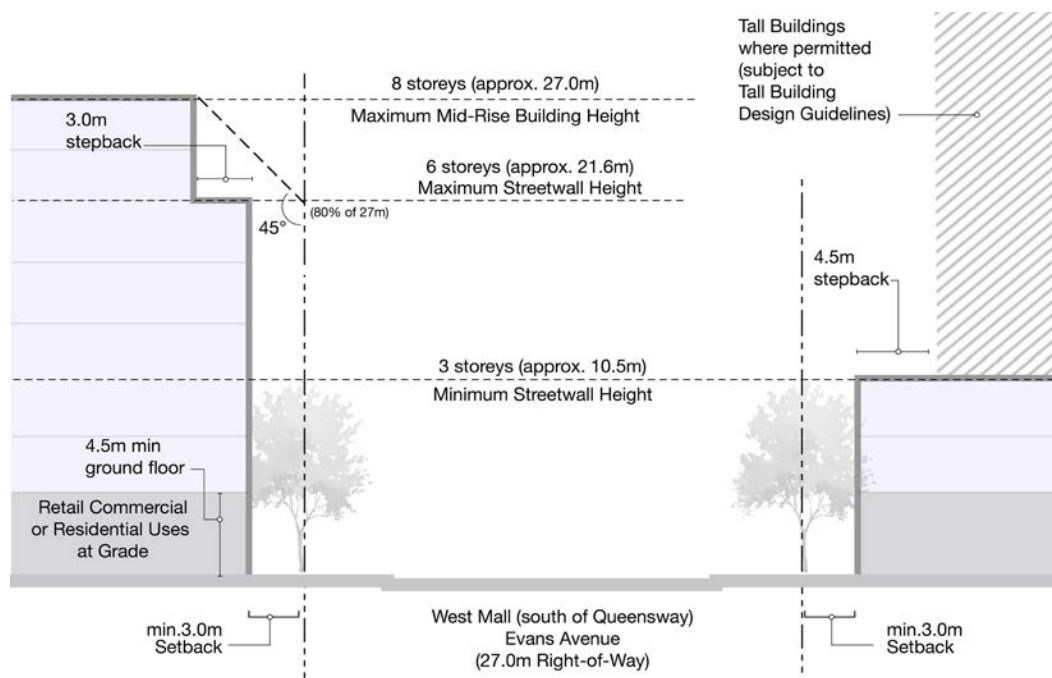
2.5.4 Development adjacent to parks, open spaces or *Natural Areas* will ensure adequate separation distances and buffer edges.

2.5.5 Development will minimize impacts on adjacent parks and open spaces with respect to sunlight and skyview access.

2.5.6 New residential development will provide a greater transition to large format retail and employment uses.

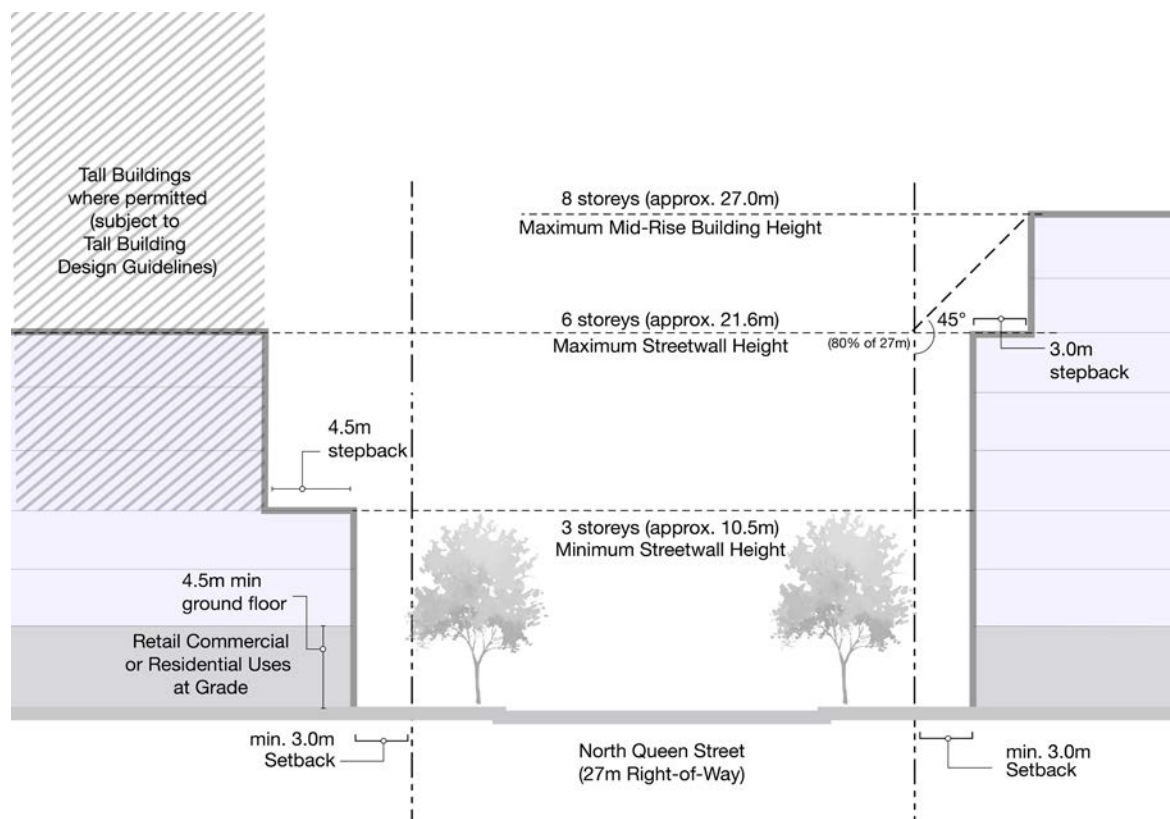
2.5.7 Angular planes along the streets and transitions will be consistent with the figures on the following pages.





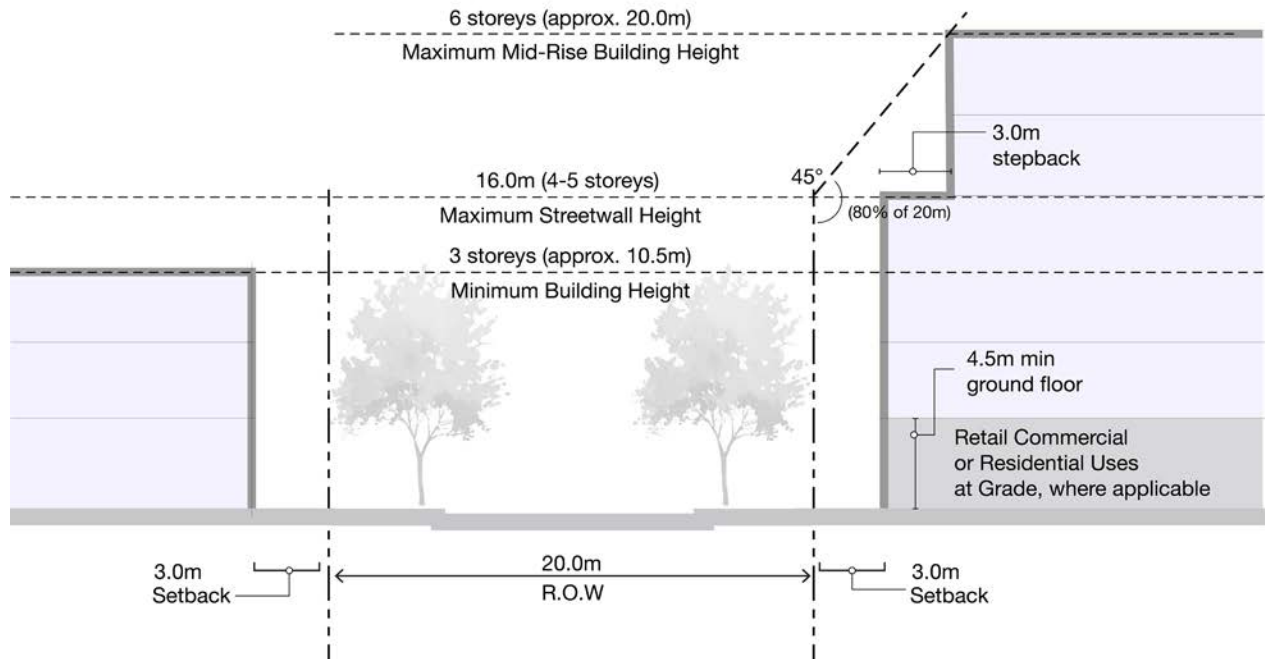
West Mall (South of Queensway) and Evans Avenue

Guidelines for Mid-Rise Buildings and Tall Building Bases



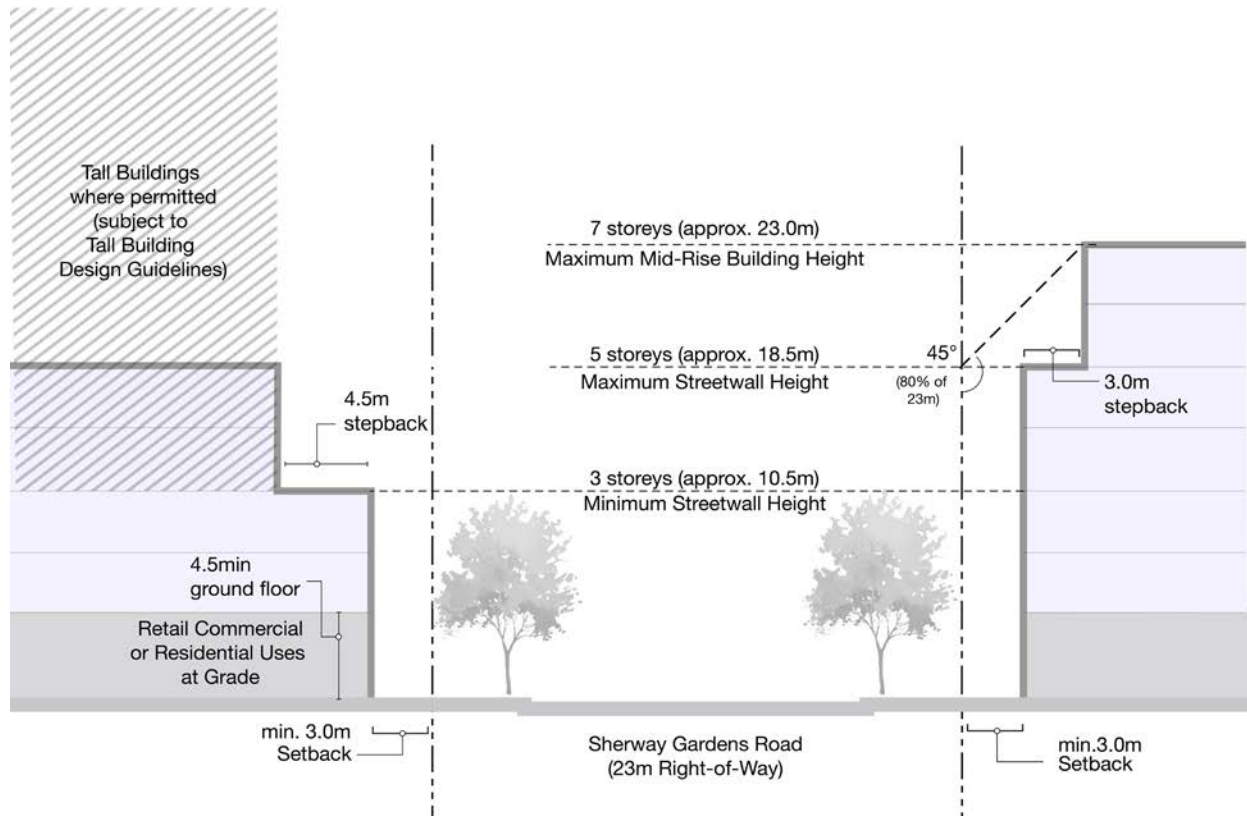
North Queen Street

Guidelines for Mid-Rise Buildings and Tall Building Bases



Neighbourhood Streets

Guidelines for Low-Rise and Mid-Rise Buildings



Sherway Gardens Road

Guidelines for Mid-Rise Buildings and Tall Building Bases

2.6 GRADE RELATED USES

The Sherway Area is currently vehicle-oriented, with parking areas fronting onto streets and activity being focused to the interior of sites. In the future the Sherway Area will strive to bring animation to the streets and public spaces throughout the Area.

Street-related activities are the key initiators for a dynamic and vibrant streetscape and public realm, ensuring a successful and safe neighbourhood. The Sherway Area will evolve to support a walkable 'main street' retail character, where appropriate, and an animated pedestrian environment throughout. The Queensway, North Queen Street and The West Mall are the Main Streets of the Sherway Area.

At-grade retail along the Main Streets will enhance community amenity within the public realm; maintain and improve the health and vibrancy of the shopping area; provide appropriate retail services for the local neighbourhood, and support adjacent transit. On streets where retail uses are not required, development should still incorporate ground floor uses that animate the public realm, such as residential entrances and lobbies, and community spaces.

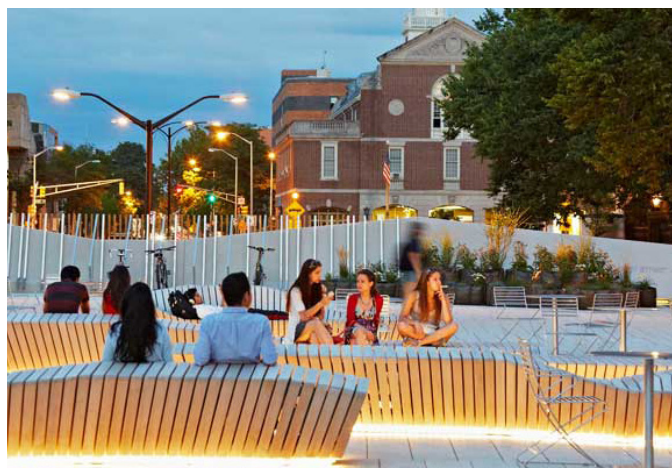
2.6.1 Pedestrian related uses are encouraged along all street fronts to ensure streetscape animation and allow 'eyes on the street', providing ample pedestrian movement opportunities along the street fronts by providing varying uses and points of interest.

2.6.2 Community facilities and services should be located with main entrances visible and accessible from a public street, and adjacent to neighbourhood parks where possible.

2.6.3 Grade-related uses will be designed to ensure full accessibility and connection to the broader mobility network.

2.6.4 To support pedestrian accessibility and comfort, surface parking located in front of buildings will be avoided.

2.6.5 Weather protection should be incorporated at the main entrances to ground floor uses.



The Plaza, Cambridge MA. (c. Stoss)



A vibrant street life is supported by active uses atgrade.

2.7 PARKING, ACCESS AND SERVICING

The practical need to provide parking and servicing access to development should be integrated without compromising the quality and safety of the pedestrian environment. This can be achieved by integrating vehicular and service entrances and functions with the design of the building(s) and overall site.

2.7.1 New development will not include parking in front of a building, to encourage pedestrian activity and safety.

2.7.2 Where parking access and service areas are located in courtyards, buildings and landscapes should be designed to minimize the visual and noise impacts on the quality of the outdoor space.

2.7.3 Within development, coordinate internal pedestrian circulation to best serve existing and anticipated desire lines, for example to transit stops and crosswalks.

2.7.4 On-street parking is generally allowed within the local road network subject to municipal restrictions (i.e. times of day, length of stay, transit operations), to reduce vehicle speed and create additional convenience parking.

2.7.5 Provide below-grade parking within mixed-use and residential redevelopment.

2.7.6 When parking or servicing access can only be provided from a Main Street, the incorporation of an architectural façade across the driveway will continue the rhythm of the streetwall and conceal the functional uses behind it.

2.7.7 When parking can only be provided along any public street, vegetation and architectural screening will be used to conceal the vehicles from street view.

2.7.8 All garage doors are to be treated as an architectural element, and be provided with a material treatment and an attention to design that is intended to complement the building elevation.

2.7.9 The design of all surface parking should incorporate stormwater management practices.



Integrating stormwater management practices in surface parking design. (Humber College; c. City of Toronto)



A portal helps to screen vehicular access and service areas from the Annette Street frontage. (c. City of Toronto)



Consolidated and screened vehicular access area, Lake Shore Blvd. W. (c. City of Toronto)

2.8 COMPREHENSIVE PRECINCT DEVELOPMENT

The configuration of each comprehensive precinct and adjacent blocks should allow for phased development. The overall build out of the entire Study Area—for the most part consisting of large blocks in multiple and consolidated ownership—may take many years to complete.

The Sherway Area Secondary Plan requires comprehensive Precinct Plans to be prepared prior to the redevelopment of each precinct. Preparing these plans will help to ensure that essential elements are not precluded or challenged over time, and assist with defining priorities and outcomes of individual phases and precincts. A phasing plan can also define interim strategies that will lead to cost-effective and successful future phases. An example of the phasing of a large block, such as the Queensway Mixed Use Precinct, can be seen below.

2.8.1 Phasing of each precinct should occur in such a way that considers the impact of each phase on the overall implementation.

2.8.2 The phasing strategy must protect for the essential elements defined in these guidelines, such as the proposed street and block network as well as parks and open spaces.



3.0 Public Realm

Incremental development and capital improvements in the Sherway Area will help advance the transformation of the public realm from "grey" to "green". A cohesive network of parks, open spaces, trails and green streets will encourage active transportation and enhance quality of life throughout the area. The Secondary Plan includes a Greening Strategy to accompany growth.

- 3.1 Parks**
- 3.2 Privately Owned Publicly-Accessible Spaces (POPS)**
- 3.3 Streetscape Greening**
- 3.4 Landscaped Points of Entry**
- 3.5 Public Art**
- 3.6 Trail Connections**
- 3.7 Utility Corridors**

3.1 PARKS

There are currently few parks or landscaped open spaces open to the public within the Sherway Area. Redevelopment and intensification presents the opportunity to build a network of high-quality parks and public spaces that provide a setting for civic and community life by enhancing the identity and character of the Area.

Parks, and other publicly accessible open spaces must form a coherent and accessible green space system. These spaces should each have a unique character and specific functions and contribute to the whole open space system. New public spaces should be high quality environments that support a wide range of roles, allow for a variety of pedestrian uses, and are distinct yet visually connected through the use of contemporary materials and details.

3.1.1 Public parks and open spaces must form a network, provide appropriate space for recreational needs and ensure good visibility, access and safety.

3.1.2 The design of parks should strive to incorporate storm water management practices.

3.1.3 In addition to having public street frontage, parks may also be situated adjacent to utility corridors to enhance and expand open space connections and programming.

3.1.4 Parks are to be provided within each Precinct to advance the transformation of the public realm from "grey" to "green".



Gibson Square Park (c. City of Toronto)



Gibson Square, North York. (c. City of Toronto)



Six Points Park (c. City of Toronto)

3.2 PRIVATELY OWNED PUBLICLY ACCESSIBLE SPACES (POPS)

Privately Owned Publicly-Accessible Spaces (POPS) are open spaces that are open to the public, but remain privately owned and maintained. Because the Sherway Area is currently lacking in parks and open spaces, it is critical to capture every opportunity to create new space for community life. POPS will play an important role in the Sherway Area greening strategy. A range of open space types and outdoor environments will be created through development to augment and complement, but not replace, the public parks secured in the Sherway Area. A variety of types, sizes and functions of POPS, including but not limited to urban plazas, courtyards and mid-block connections, are encouraged throughout the area.

3.2.1 The integration of POPS is encouraged at the ground level of all development to augment the public realm network.

3.2.2 Safe and comfortable mid-block connections are encouraged through development blocks to reduce the scale of the development parcels and create pedestrian scale development.

3.2.3 Considerations for the microclimate resulting from the surrounding buildings will be taken into account when locating a new private open space.

3.2.4 Urban plazas are required at northeast and northwest corners of the intersection of The Queensway and North Queen Street; the south east corner of the intersection of The Queensway and The West Mall; and the west side of the intersection of The West Mall and Sherway Drive.

3.2.5 The design of POPS should strive to incorporate storm water management practices.

3.2.6 Where below-grade parking is located under a POPS, the volume of high-quality soil provided at grade above the drainage layer is to be maximized. Soil volumes should not have to be augmented within above-grade planting beds.

3.2.7 The design of a POPS is to be included in any development consultation with staff and the public.



One Sherway POPS. (c. City of Toronto)



5365 Dundas Street POPS (c. City of Toronto)



Communal landscaped space within residential development. (Sydney, Australia; c. Simon Wood)

3.3 STREETSCAPE GREENING

Building setbacks provide transition from public to private spaces; allow new and existing streets to take on a 'green' character through vegetation and green infrastructure; and provide amenity space for retail and pedestrian access.

3.3.1 Tree planting should line both sides of all existing and proposed streets to soften built form edges and enhance the character of the public realm.

3.3.2 A double row of street trees will be provided on both sides of The Queensway, which includes one row of trees on private property.

3.3.3 Setbacks will be provided as part of the animation of the public realm.

3.3.4 On some public streets, narrow boulevards may necessitate a row of street trees being accommodated within a building setback.

3.4 LANDSCAPED POINTS OF ENTRY

Key intersections within the Sherway Area will include signature landscaping treatments and wayfinding elements to mark the entry points from the highway and adjacent neighbourhoods, and provide a distinctive green identity for the emerging community.

3.4.1 Landscaped Points of Entry should be designed to serve multiple functions, such as placemaking, wayfinding, and locations for public art.

3.4.2 Landscaped Points of Entry are important visual and physical markers within a precinct and special attention should be paid to their design.

3.4.3 The design of Landscaped Points of Entry should strive to incorporate stormwater management practices.

3.4.4 Landscaped Points of Entry should be designed to preserve sightlines at street intersections.



High Park Townhouses. (c. City of Toronto)



Landscaped gateway at Beecroft Road and Park Home Avenue. (c. City of Toronto)



5145 Dundas Street West entry features. (c. City of Toronto)

3.5 PUBLIC ART

High quality public art is important to the enhancement of Toronto's urban fabric. Public art has created landmarks throughout the city and already contributes to the identity and character of many of our neighbourhoods and districts. The Toronto Official Plan encourages the inclusion of public art in all significant private sector developments across the city. The governing principle for the Percent for Public Art Program is that art is a public benefit to be enjoyed and experienced by residents and visitors throughout the city. The privately-owned art is intended to make buildings and open spaces more attractive and interesting and to improve the quality of the public realm.

Public art is well suited to contribute to the local identity of the Sherway Area and enhance the character of the public realm.

3.5.1 Public art is encouraged to be integrated within the site of all development including retail, office, industrial, residential and institutional.

3.5.2 Prominent locations throughout the Sherway Area such as Parks, POPS, Landscaped Points of Entry, Urban Plazas, and Main Streets are all well suited for public art.

3.5.3 Public art will contribute to the overall cultural vitality of the Sherway Area, complement specific qualities of sites, and enhance wayfinding.



Public art in City Place. (c. City of Toronto)



Lincoln Park SoundScape. (Miami, FL; c. DTAH)



The Plaze. (Cambridge, MA; c. Stoss)

3.6 TRAIL CONNECTIONS

New trails will connect throughout the Sherway Area to expand the pedestrian and cycling network. Improved access and visibility to the Etobicoke Creek trail system will provide recreational opportunities in a natural setting, and connections to the broader network beyond the Sherway Area.

3.6.1 Clear signage should be provided for connections with adjacent trail systems within and adjacent to the new development.

3.6.2 New trails will connect throughout the Sherway Area and supplement the pedestrian and cycle network, as identified in the Transportation Master Plan.



Waterfront trail, Toronto. (c. DTAH)

3.7 UTILITY CORRIDORS

The utility corridors that cross the Sherway Area provide the opportunity to introduce a formal off-street pedestrian and cycling network to encourage recreational uses. Appropriate tree planting and other landscape measures will help 'green' these spaces, tying together these lands with a network of pedestrian open spaces and trails.

3.7.1 Utility Corridors will provide space for an expended trail network, where possible, to enhance the envisioned open and green space network within the Sherway Area.

3.7.2 Utility Corridors can serve as an active-transportation connection through the Study Area, provided the safety of the users is accommodated and formalized.

3.7.3 Utility Corridors may be used to enhance other open spaces and their related programming.

3.7.4 Proposed planting and landscape treatments within and adjacent to Utility Corridors will be subject to review by relevant agencies, such as Hydro One, Enbridge Gas or its successors.



Humber Bay Shores wetland, Toronto. (c. City of Toronto)

3.8 SUSTAINABILITY

Sustainable development capitalizes on opportunities to improve the environment, as well as economic and social best practices to ensure the healthy evolution of the Sherway Area. A focus on public realm, stormwater management, water quality, energy efficiency and sustainable materials will contribute to the healthy evolution and long-term viability of Sherway Area.

In the long term, issues such as water quality, energy conservation and use of sustainable materials cannot be ignored. The current street network and extensive surface parking areas are entirely served by conventional storm water drainage systems that are neither efficient nor sustainable. The appropriate future management of these spaces and the adjacent natural areas presents an excellent early opportunity to demonstrate such a commitment to sustainable practices.

3.8.1 Impact to natural features and cultural heritage landscapes will be minimized and the protection of adjacent natural features to the Study Area will be ensured.

3.8.2 Precinct Plans are to adopt Low Impact Development (LID) standards for new development, streets and surface parking lots, such as bioswales, rain gardens, infiltration galleries, and cisterns.

3.8.3 Natural resources will be used in a practical and discrete manner, while using locally produced materials where possible.

3.8.4 The existing streetscape, such as the curbs and below grade infrastructure, will be reused and maintained as much as possible.

3.8.5 All new projects will comply with the most recent City of Toronto tree planting standards and best planting practices.

3.8.6 The planting of native species, especially in proximity to the natural areas, is encouraged to minimize irrigation water use and to discourage invasive or aggressive competition.



Green infrastructure integrated with streetscape design in Portland, Oregon. (c. City of Portland)

4.0 Streetscape Guidelines

New and existing streets are among the key structuring elements of the Sherway Area, and their design will be critical to establishing the character of the neighbourhood. Pedestrian, cycling, roadway and landscaping details must be carefully considered to promote active transportation, transit use and the overall greening of the Sherway Area. These Streetscape Guidelines provide further direction to ensure consistency and cohesiveness within the street network.

- 4.1 Introduction and General Principles**
- 4.2 Enhancing Multi-Modal Mobility**
- 4.3 The Queensway**
- 4.4 North Queen Street & North Queen Street Extension**
- 4.5 The West Mall**
- 4.6 Sherway Gardens Road**
- 4.7 Evans Avenue**
- 4.8 Neighbourhood Streets**
- 4.9 Materials**

4.1 INTRODUCTION AND GENERAL PRINCIPLES

As the Sherway Area is redeveloped, new and existing streets will evolve to support and relate to their planned context, creating places where people live, work, shop and play, all linked with an enhanced pedestrian and cycling network.

The street network is intended to provide choice for how people move around, with an emphasis on safe and comfortable travel. Streets will provide a green and comfortable setting for all users, supporting all uses in the emerging neighbourhood. The design of all streets in the Sherway Area should anticipate the proposed changes in land use, intensity and character as redevelopment occurs, and adhere to the Toronto Complete Street Guidelines (2016).

The street cross sections that follow demonstrate the preferred typical street design. In some cases, such as The West Mall, the section illustrates the existing curb to curb arrangement. In others, such as Evans Avenue, The Queensway and new local neighbourhood streets, the section illustrates the ideal layout, only achieved through reconstruction and moving the curb.

The network includes a number of different street types:

- **Main Streets** have a higher intensity of uses and users than the other streets in the Area, with primarily commercial at grade, better network connections and a greater transportation role. They will support increasing pedestrian activity and provide safe facilities for all users.
- **Employment Streets** will connect to both Main Streets and Neighbourhood Streets, providing access to non-residential portions of the Sherway Area. They will support increasing pedestrian activity, provide safe facilities for all users, and accommodate larger vehicles without compromising safety.
- **Connectors** are streets that provide access to a range of different adjacent land uses, including residential, employment and commercial. They have a high degree of network connectivity and are important for linking different places. Evans Avenue and segments of The West Mall and Sherway Gardens Road are examples of this street type.
- **Neighbourhood Streets** typically have a lower intensity, with a mix of grade related uses, reduced network connectivity, and a lower transportation role. These streets are slower than Main Streets and will provide access to residential and institutional properties. These street will support an increased level of pedestrian activity and provide safe facilities for all users.

4.2 ENHANCING MULTI-MODAL MOBILITY

The Sherway Area Secondary Plan includes policies that will result in a fine-grained local street network grid, and pedestrian and cycling facilities, that will shift dependence away from the automobile and encourage active modes of transportation. As the Sherway Area redevelops and intensifies, it is important for street design to support active ways of moving around, especially for local neighbourhood trips that are easily done by bike or on foot. This in turn will help to alleviate vehicle congestion and support transit operations. Improvements to the streetscape and comprehensive cycling infrastructure will create inviting spaces for people to walk and cycle. In addition, improved sidewalks and new signalized intersections will enhance safety and promote walking between the new neighbourhoods in the Area.



Yorkville Avenue, Toronto. (c. City of Toronto)

4.2.1 Additional signalized crossings at intersections should be introduced to enhance pedestrian movement and safety. Intersections will be designed for multi-modal crossings.

4.2.2 Pedestrian clearway widths along streets will be a minimum 2.1 metres, and 3 metres or greater along Main Streets to achieve a comfortable walking environment.

4.2.3 On-street cycling facilities will be provided along North Queen Street, The West Mall and Evans Avenue.

4.2.4 Fully separated cycling facilities will be provided along The Queensway.

4.2.5 Bicycle parking facilities will be introduced on all public streets to encourage local cycling activity and to service local facilities.

4.2.6 At-grade sheltered bicycle parking will be included within every new development.

4.2.7 The potential for further alignment of additional east-west streets in the Study Area will be considered.

4.2.8 Off-street trails will be provided and enhanced, where feasible, in the hydro corridors and Etobicoke Creek system.

4.2.9 New street lighting and street furniture will be introduced throughout the Sherway Area to enhance pedestrian amenity, and at all transit stops to support current and growing ridership.

4.3 THE QUEENSWAY

The Queensway, defined as an Avenue in the City's Official Plan, is the primary Main Street for the Sherway Area. The Queensway travels east-west through the area, connecting a primarily retail and service commercial precinct east of Highway 427 to the City of Mississauga.

The Queensway will become a more green and gracious street, with trees of a large size and number to moderate its large scale and high volume of vehicles. New buildings will be set back a sufficient distance from the right-of-way to provide generous boulevards for increasing pedestrian activity and broad frontage zones for outdoor cafes and seating that help animate the streetscape. Cycling facilities along The Queensway will further invite and support non-auto movement in the Sherway Area.

On the south side of the street there is an 8.5 metre Enbridge easement. The easement is not parallel to the right-of-way and results in an irregular build-to line and boulevard dimension. The easement will remain privately owned with public access and designed as part of the streetscape. This configuration creates an opportunity to introduce a multi-use path within the easement, providing an attractive off-street cycling facility along with additional greening. This street will require a full reconstruction.

4.3.1 The Queensway will have a consistent 36.0 metre public right-of-way.

4.3.2 A curb-to-curb travelway of 25 metres will be provided, with continuous boulevards of a minimum of 5.5 metres on each side of street. The pedestrian clearway will differ depending on the cycle facility (multi-use trail on south side, or one way cycle tracks on both sides of the street) but will be no less than 2.1 metres.

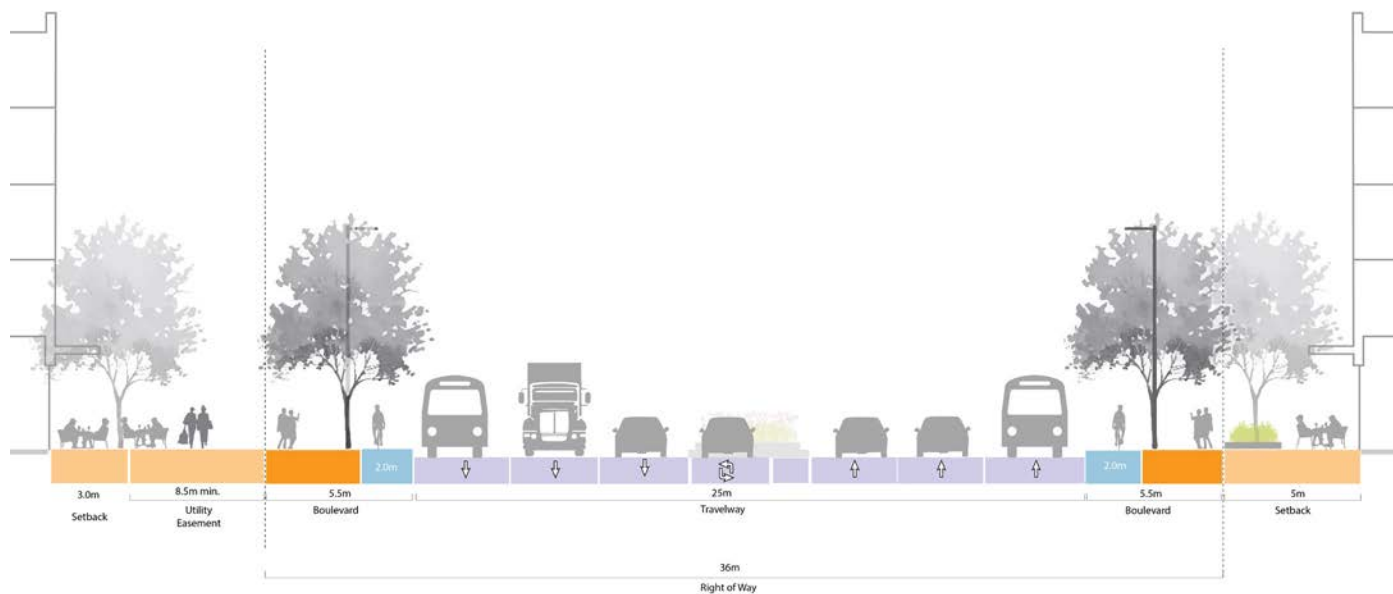
4.3.3 The pedestrian clearway should be separated from the roadway curb by either a furnishing and planting zone or cycle facilities, to ensure pedestrian safety and convenience.

4.3.4 If a multi-use trail is not feasible within the utility easement, cycling facilities are to be protected from roadway vehicles by either grade separation or on-street with physical curbed buffers.

4.3.5 The mid-block lane assignment should include three travel lanes in each direction, and a central median for left hand turning movements. Cycle facilities are not part of the roadway dimension.

4.3.6 Median noses at intersections should be considered to improve pedestrian crossing safety.

4.3.7 The Queensway will include a double row of trees on both the north and south side. A row of street trees may be accommodated within a building setback to achieve this.



The Queensway Section Option 1: Cycle Tracks on both sides



The Queensway Section Option 2: Multi-use Trail in Enbridge Easement

4.4 NORTH QUEEN STREET & NORTH QUEEN STREET EXTENSION

North Queen Street is one of the Main Streets of the Sherway Area. The existing North Queen Street is an important north-south connection. It will be redeveloped with mixed-use buildings with primarily commercial uses at grade. The new North Queen Street Extension will provide an alternative network connection in the Sherway Area, linking The West Mall to North Queen Street and mainly serving the employment uses at the northern boundary of the Area.

4.4.1 The existing portion of North Queen Street will have a consistent 27.0 metre public right-of-way, as per the Official Plan.

4.4.2 A curb-to-curb travelway of 18.6 metres will be provided, with continuous boulevards of 4.2 metres on each side of street.

4.4.3 The North Queen Street Extension to The West Mall will maintain a minimum 26.0 metre public right-of-way, as per the approved 2015 Environmental Assessment.

4.4.4 The mid-block lane assignment should include two travel lanes in each direction and on-street bike lanes.

4.4.5 Street trees will be accommodated within the right-of-way.



North Queen Street Section

4.5 THE WEST MALL

The West Mall is a major north-south route through the Sherway Area. It will vary in character north and south of The Queensway. The northern segment is adjacent to the Etobicoke Creek ravine on the west side, and has limited block frontage on the east side, with boulevards only on the east side of the street. The southern Main Street segment will be redeveloped with streetwall buildings and boulevards on both sides of the street.

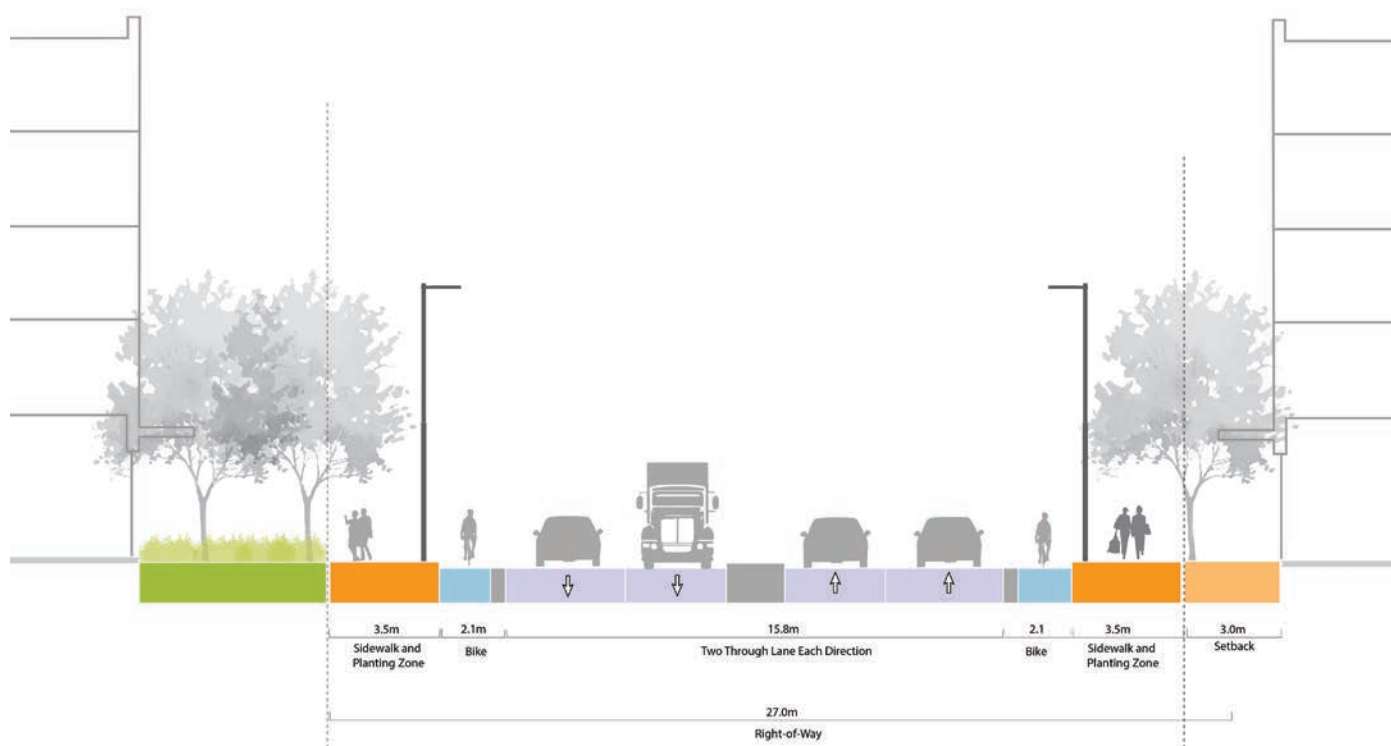
The West Mall is an important route from the *Employment Areas* north of the Sherway Area to the Queen Elizabeth Way. Street tree planting may be constrained in the right-of-way given the limited space between the travelway and the property line of the right-of-way. This street is intended to be constructed within the existing curb-to-curb zone.

4.5.1 The West Mall will have a consistent minimum 27.0 metre public right-of-way.

4.5.2 A curb-to-curb travelway of 20.0 metres will be provided (inclusive of the bike lanes), with continuous boulevards of 3.5 metres on each side of street.

4.5.3 The mid-block lane assignment should include two travel lanes in each direction and on-street bike lanes.

4.5.4 Where street trees cannot be accommodated within the right-of-way, they are to be provided on private property within the setback.



The West Mall Section (South of The Queensway)

4.6 SHERWAY GARDENS ROAD

Sherway Gardens Road travels around the Sherway Gardens Shopping Centre, and varies in character along its length. In some segments it will function as a main street with active uses at grade on both side of the street and a higher volume of pedestrians. In other segments, the street has buildings on only one side with a higher volume of vehicles compared to other users. In both circumstances the street should be designed to reduce travel speeds.

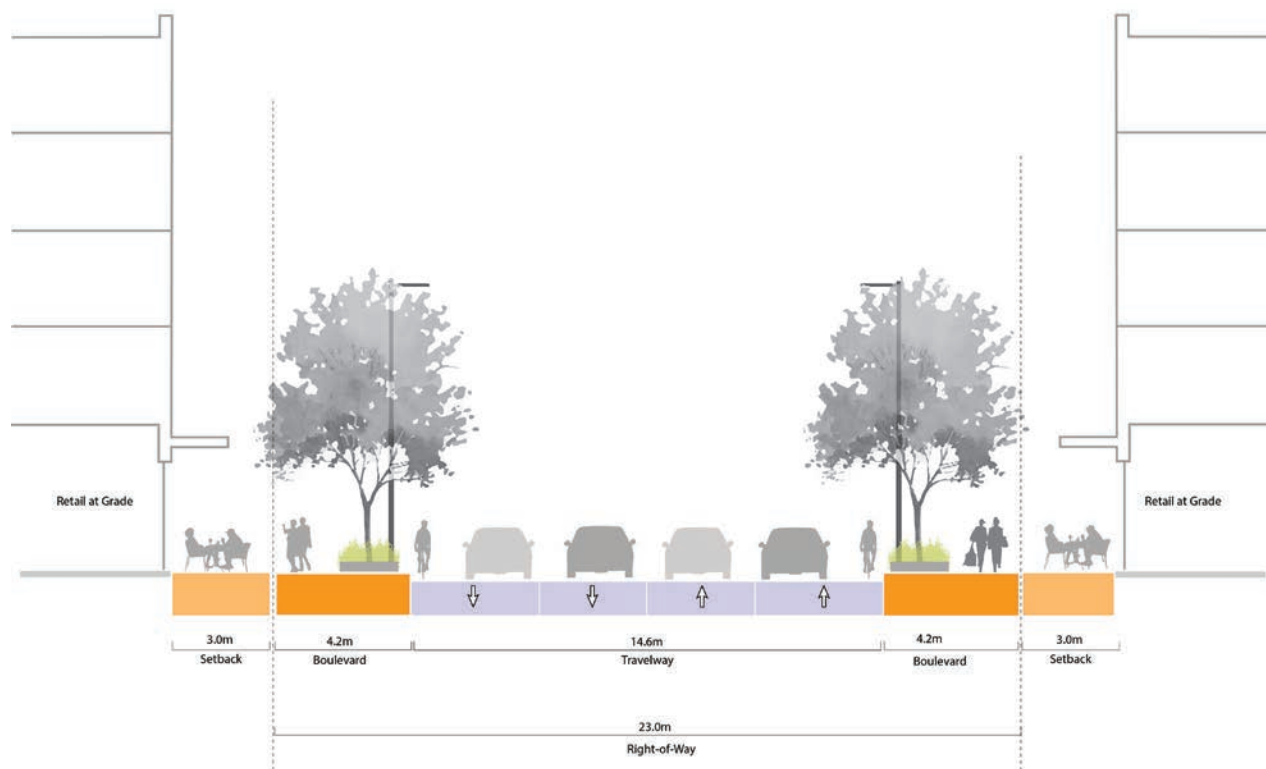
Portions of this road will require realignment to continue the necessary connections to the adjacent public roads. Sherway Gardens Road also intersects with Highway 427 on- and off-ramps at The Queensway. These intersections should be designed to safely accommodate all users, not just the turning movements of vehicles.

4.6.1 Sherway Gardens Road should be realigned with a consistent and minimum 23.0 metre width.

4.6.2 A curb-to-curb travelway of 14.6 metres will be provided, with continuous boulevards of 4.2 metres on each side of street.

4.6.3 The mid-block lane assignment should include two travel lanes in each direction.

4.6.4 Street trees will be accommodated within the right-of-way.



Sherway Gardens Road Section

4.7 EVANS AVENUE

Evans Avenue moves through a variety of different places along its length before terminating at The West Mall in the Sherway Area. Between The West Mall and the Queen Elizabeth Way overpass, the street is characterized by landscape frontages rather than with active grade related uses.

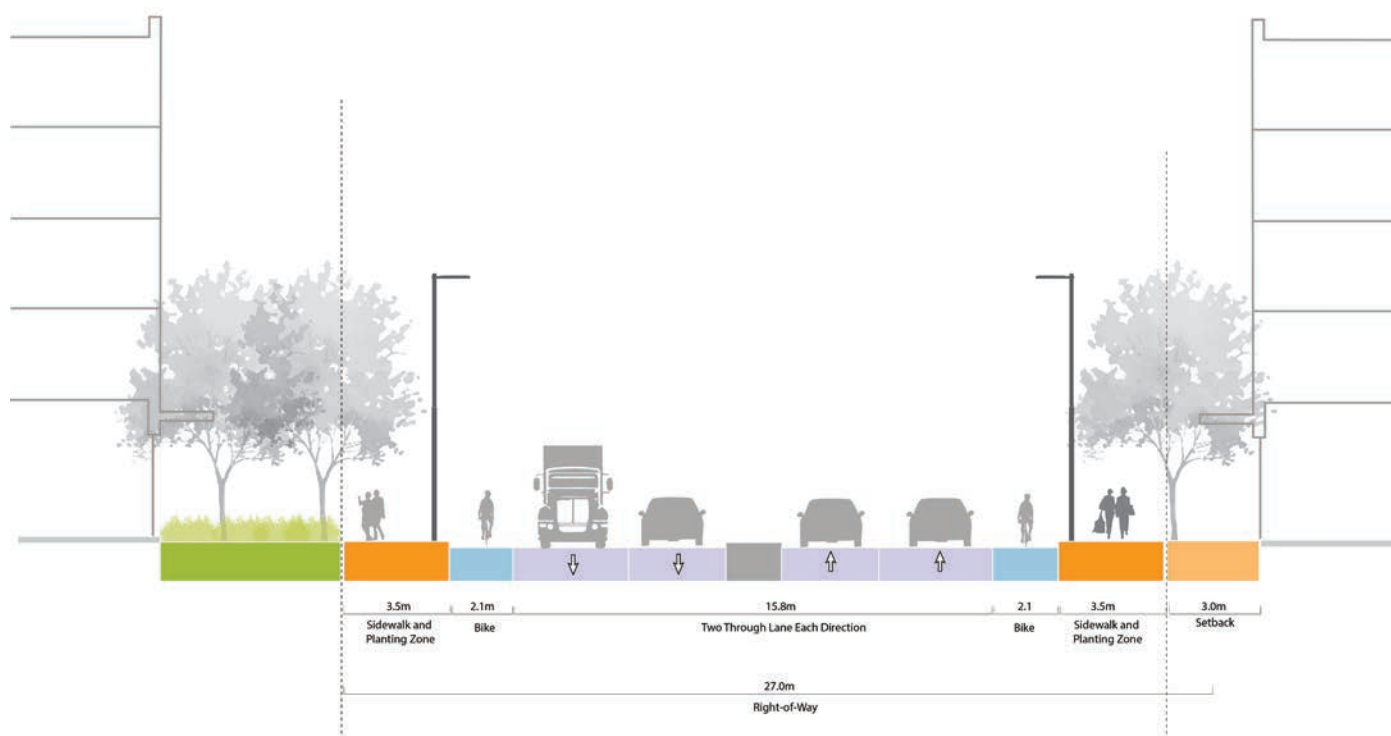
As Evans Avenue is one of only three east-west streets that cross Highway 427 and the Queen Elizabeth Way, its role as a connector is well defined for all users. This street will require a full reconstruction.

4.7.1 Evans Avenue should have a consistent and minimum 27.0 metre public right-of-way.

4.7.2 A curb-to-curb travelway of 20 metres will be provided, with continuous boulevards of 3.5 metres on each side of street.

4.7.3 The mid-block lane assignment should include two travel lanes in each direction and on-street bike lanes per the City of Toronto Bike Plan (2001).

4.7.4 Where street trees cannot be accommodated within the right-of-way, they are to be provided on private property within setback.



Evans Avenue Section

4.8 NEIGHBOURHOOD STREETS

Local neighbourhood streets will provide access from the Main Streets and higher volume roads to residential, commercial, institutional and employment uses. Vehicle traffic will be slower than on the other streets in the Sherway Area, and will support an enhanced pedestrian and cycling environment with additional greening. The design of these streets should anticipate this change in use and character.

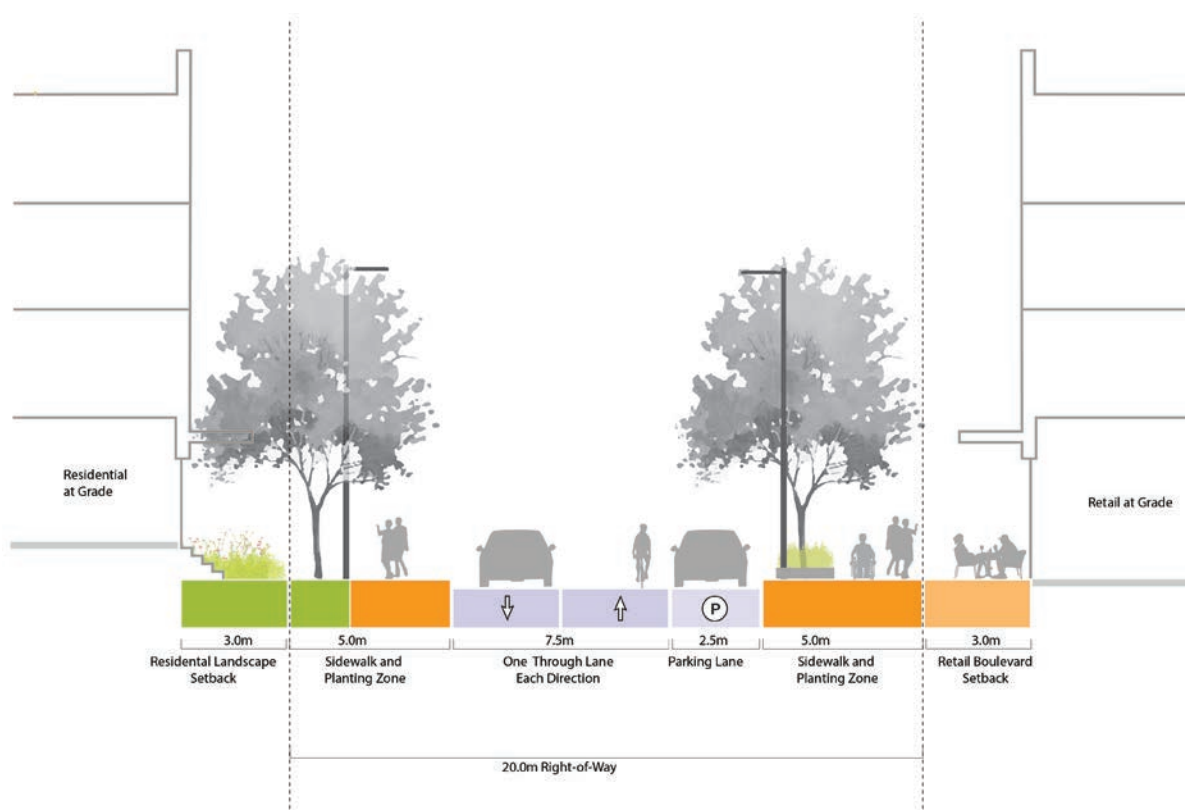
These guidelines and standards apply to existing and new local neighbourhood streets. Nova Road, Boncer Drive, and Sherway Drive are existing streets that will extend into adjacent development blocks.

4.8.1 Neighbourhood streets will have a consistent minimum 20.0 metre public right-of-way width.

4.8.2 A curb-to-curb travelway of 10 metres will be provided, with continuous boulevards of 5.0 metres on each side of a street.

4.8.3 The mid-block lane assignment should include one travel lane in each direction, and allow for on-street parking along one side of the street.

4.8.4 Street trees will be accommodated within the right-of-way.



Typical Neighbourhood Street Section

4.9 MATERIALS

Planting

Trees should be planted on all existing and new streets in the Sherway Area. A naturalistic character is appropriate, as an homage to the adjacent Etobicoke Creek corridor that defines the western edge of the Area. Along public streets, the streetscape should avoid monoculture planting for long stretches, and select different varieties and trees of similar habit, height, leaf shape and colour. Combined with a block-by-block planting scheme, this approach will lessen the impact of one pest or disease specific to a particular tree species on the quality and character of the overall streetscape.

This plan proposes a range of suitable tree species for planting in a sidewalk planting condition. All of these trees are identified in the City of Toronto Tree Planting Solutions in Hard Surfaced Boulevards (2013).

4.9.1 To grow large and healthy trees, provide a minimum of 30 cubic metres of non-compacted soil volume per tree and allow for suitable water and air exchange.

4.9.2 When planted in a row, space trees 8 to 10 metres on-centre. For a double row of trees, stagger the rows to provide suitable soil volume and room for healthy canopy growth.

4.9.3 A raised planter with a short 200mm wide granite or concrete curb may be used to provide an opening for air and water exchange, and help to reduce the cost of structural elements. These planters should be used only where the sidewalk width is 4.2 metres or greater.

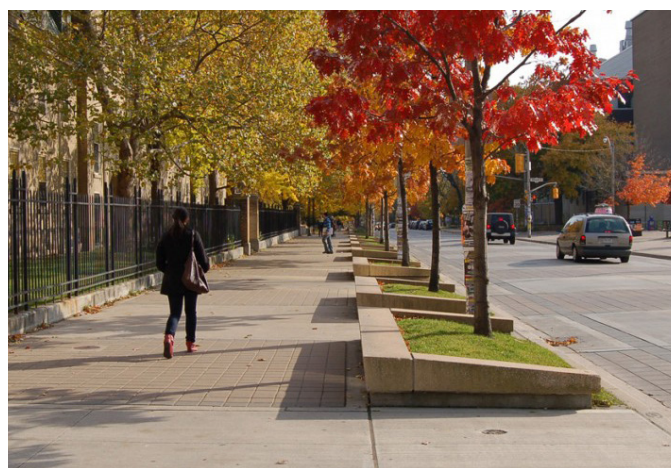
4.9.4 Use salt-tolerant plants near vehicular and pedestrian areas.

4.9.5 Open planters are the preferred detail where space permits a minimum 1.8 metre planter. If open planters are not possible, maintain a minimum tree opening of 1.2 metres. Avoid tree grates and make use of granite mulch or crushed stone.

4.9.6 In certain cases where the sidewalk width is insufficient to accommodate tree planting or conditions are poor, plant trees in the setback on private property.

Table. Suitable Street Trees for Sherway Area

BOTANICAL NAME	COMMON NAME
<i>Acer x freemanii</i>	Freeman Maple
<i>Celtis occidentalis</i>	Common Hackberry
<i>Ginkgo biloba</i>	Ginkgo
<i>Gleditsia triacanthos</i> var. <i>inermis</i>	Thornless Honey Locust
<i>Gymnocladus dioica</i>	Kentucky Coffee-tree
<i>Nyssa sylvatica</i>	Black Gum, Tupelo
<i>Platanus x acerifolia</i>	London Plane-Tree
<i>Quercus bicolor</i>	Swamp White Oak
<i>Quercus macrocarpa</i>	Bur Oak
<i>Quercus muehlenbergii</i>	Chinkapin Oak
<i>Robinia pseudoacacia</i> 'Purple Robe' or 'Frisia'	Black Locust
<i>Tilia x 'Redmond'</i>	'Redmond' Basswood
<i>Tilia cordata</i> / <i>tomentosa</i>	Littleleaf/Silver Linden
<i>Ulmus americana</i> cvs.	White Elm cultivars
<i>Ulmus</i> hybrids	Elm hybrids
<i>Zelkova serrata</i>	Japanese Zelkova



Tree planting along the revitalized St. George Street. (c. Brown + Storey Architects)

Paving

4.9.7 Paving materials and details used throughout the Sherway Area's public spaces should be of high quality and of a related design theme or character unique to the area or the precinct.

4.9.8 Where patterns or non-standard materials are part of an approved streetscape master plan or are proposed and deemed appropriate in principle, all such installations must be AODA compliant and may be subject to review and approval from the City.

4.9.9 All paving details will refer to the City of Toronto Streetscape Manual and standards.

Lighting

4.9.10 New street lighting will be provided on all street and pedestrian routes for safety; to extend the use of public spaces; and further reinforce the aesthetic design of the Sherway Area.

4.9.11 Lighting should be energy efficient, and be able to accommodate LED luminaries when the technology advances to an acceptable level for the City.

4.9.12 Street lights are character defining public realm elements that can add a great deal to the quality of the pedestrian environment. New street lights should be in accordance with the City Standard Light Poles. Luminaires should be at two heights, one for the roadway and another for pedestrians on special streets like the Queensway.

Furnishings

4.9.13 City approved furnishings will be used in the public realm.

4.9.14 A coordinated street furniture program will be established for each Precinct. Furniture within the Sherway Area should present a clean, contemporary and refined aesthetic. Any furnishing should complement the City Of Toronto Street Furnishings Program.

Bike Parking

4.9.15 Bike parking facilities, including sheltered facilities, are required with each new development to encourage cycling.

4.9.16 The City of Toronto bike ring is recommended as a stock item within the right-of-way. If possible, locate the bike rings in a sheltered location related to buildings—beneath overhangs or canopies—to further protect bikes and cyclists.

4.10 STREETSCAPE DETAILS

This section provides additional details for the sidewalks, plantings, and materials that form the streetscape. Sherway Area streets are a major part of the public realm. A cohesive and consistent palette of materials and details are required with robust, clean and contemporary details. The selection of materials will borrow from the best details in the current City of Toronto Streetscape Manual and other recent projects and offer modifications to fit the Sherway Area context.

These details makes reference to the latest recommendations from the City's Tree Planting Solutions effort, Urban Forestry's most recent approach to planting in hard boulevard surfaces to ensure the growth of large, healthy trees, and the relationship of tree planting to boulevard design.

The implementation of each streetscape detail can occur as part of a new construction or reconstruction, or as discrete segments often associated with redevelopment.

In all cases, the preferred pedestrian clearway is provided within the public right of way. Many of the details require tree planting to occur on private property within a setback given the existing dimensions.

A combination of materials is likely throughout the Study Area, with higher quality materials and details associated with the anticipated higher volume pedestrian environments, such as The Queensway, North Queen Street, The West Mall and Evans Avenue, and more basic materials and details for the other streets.

The recommended basic curb and sidewalk material is concrete. Concrete unit pavers have a modular benefit and are recommended for the furnishing and planting zones in special cases.



1. Frontage Zone. Where the street interfaces with adjacent properties. This area need not exist entirely within the public right-of-way if a building setback is present. The dimension of this area will vary and often increase after other space requirements are met.

2. Pedestrian Clear Zone. This is the most important area of all streets. It is where people walk, linger, and wait to cross the roadway. All streets in Toronto shall maintain a minimum pedestrian clearway of 2.1m. A higher minimum dimension is required for many streets and locations with greater pedestrian intensity. See Chapter 5: Sidewalk Dimensions, for more information.

3. Furnishing Zone. This is the space typically between the pedestrian clearway and edge zone or curb, although it may possibly locate elsewhere such as to the other side of the pedestrian clearway. It is where trees and other plantings are situated, and where street furniture and utility poles are normally placed.

4. Edge Zone. This is the space that typically occupies from the face of curb to the Planting and Furnishing zone. This dimension is important to accommodate door swing from curbside parked vehicles and mirror overhang.

5. In-Between Zone. This zone inhabits the space between the Sidewalk Zones and Roadway Zones and often overlaps both. Traditional practice divides the road into two: roadway and roadside. This third zone speaks to the more complex and nuanced nature of streets—elements that are particular to neither the roadway nor roadside.

Sidewalk Zones: Sidewalks are composed of several different zones that together support a convenient and comfortable environment for pedestrians and place making.

Edge Zone: The Edge Zone includes the curb and any setbacks of vertical elements from the face of curb (minimum of 0.5m). This zone may also include the City of Toronto decorative paving band which increases the Edge Zone to 0.8m. The Edge Zone can overlap with the Furnishing and Planting Zone if space for other zones is restricted. The preferred curb width is 0.2 to 0.3m.

Planting and Furnishing Zone: The City of Toronto Streetscape Manual paving band should be located parallel to the back of curb at least 2 paving course wide. In certain cases, the pavers should extend the full width of the furnishing and planting zone. Furniture in this zone should include waste collection, bike rings and seating. Provide standard City of Toronto street lights as required.

Pedestrian Clearway Zone: Maintain a minimum preferred pedestrian clearway of 2.1m in all cases and for all street types. This zone should be increased if space permits and pedestrian volume requires additional width.

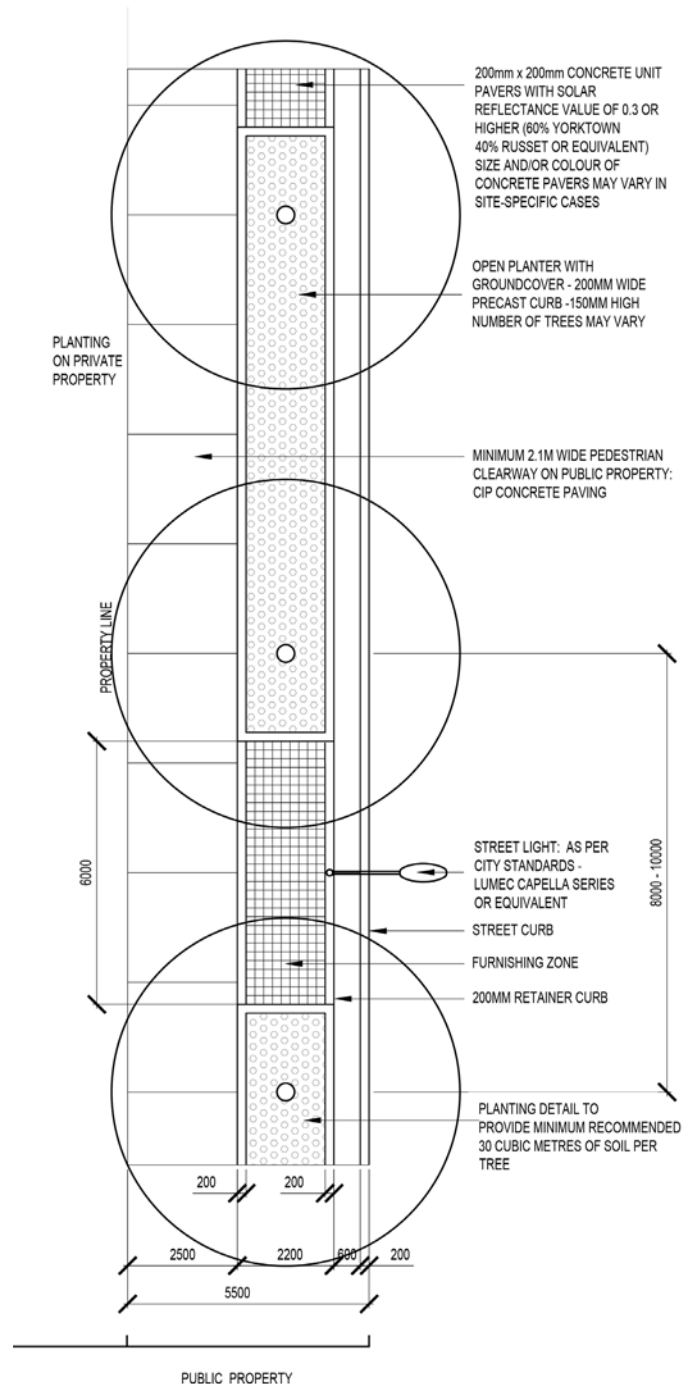
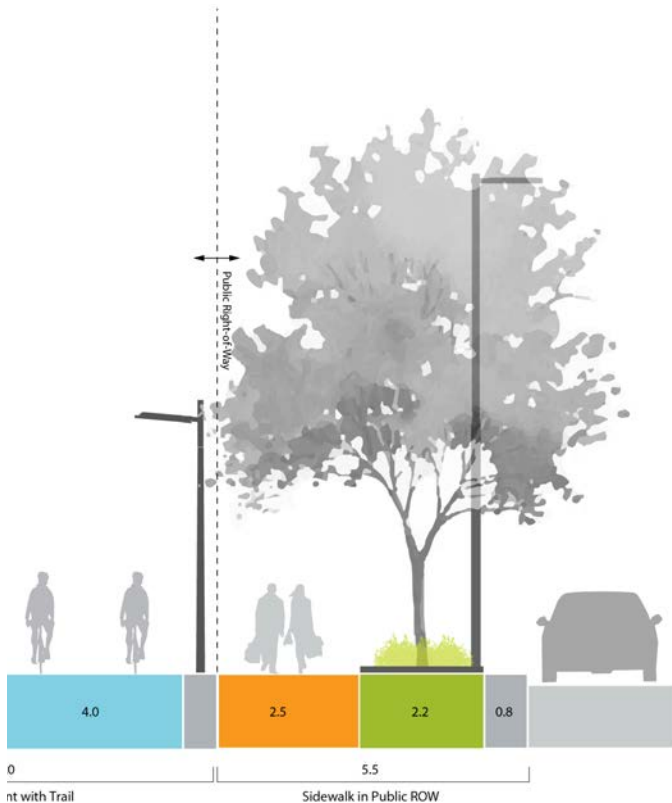
Frontage and Marketing Zone: In most cases, the frontage zone will take place on private property as part of the sidewalk widening setback. This dimension will vary throughout. Please refer to the Streetscape Plan for further detail.

The Queensway

Option 1 This detail illustrates the minimum improvement for the north and south sidewalks, and includes a 2.5m pedestrian clearway, a 2.2m open planter and a 0.8m edge zone. Concrete unit pavers are recommended between the clearway and back of curb. Additional clearway and tree planting is anticipated with development and within the generous setbacks and easements for both sides of The Queensway. Any additional sidewalk width within the public right-of-way (determined through detailed design) should go to the widening of the furnishing and planting zone to improve tree planting conditions.

This detail imagines a cycling facility within the Enbridge easement which will require further study to determine its feasibility.

The Queensway Option 1: Tree Planting adjacent to Roadway

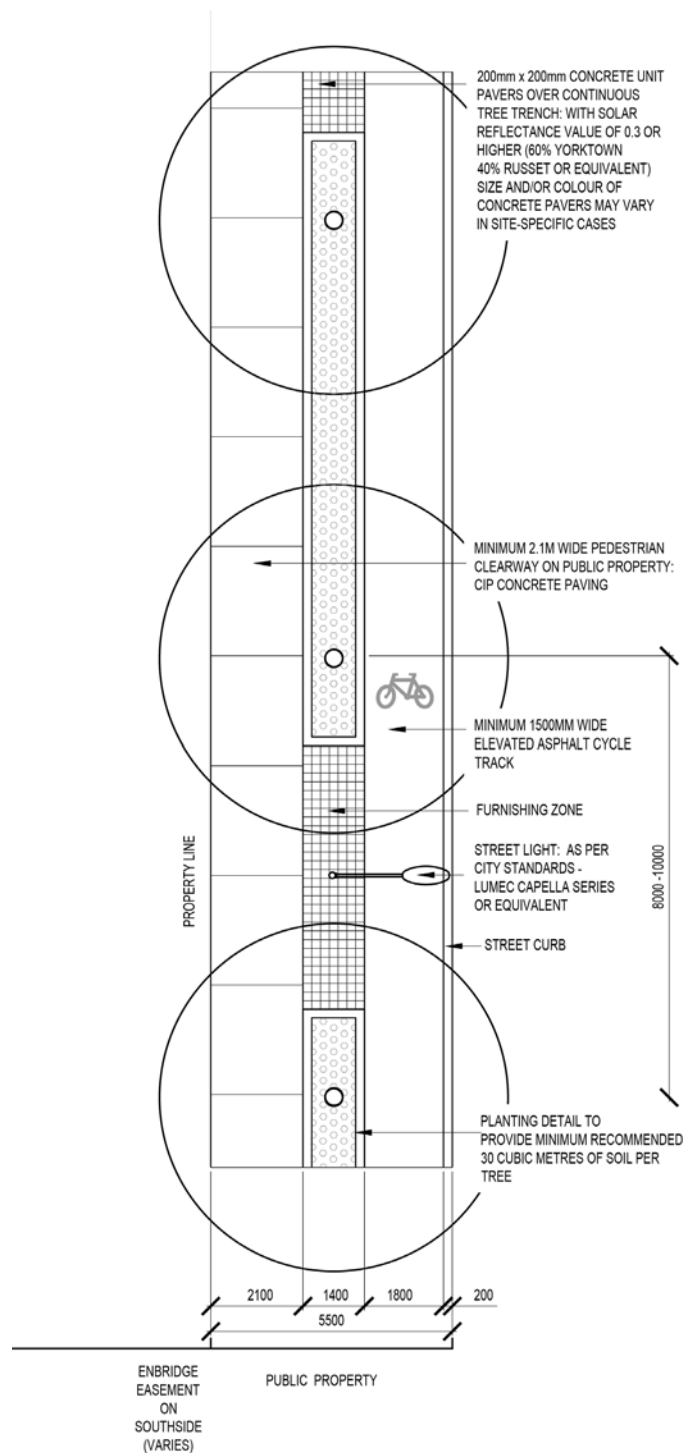
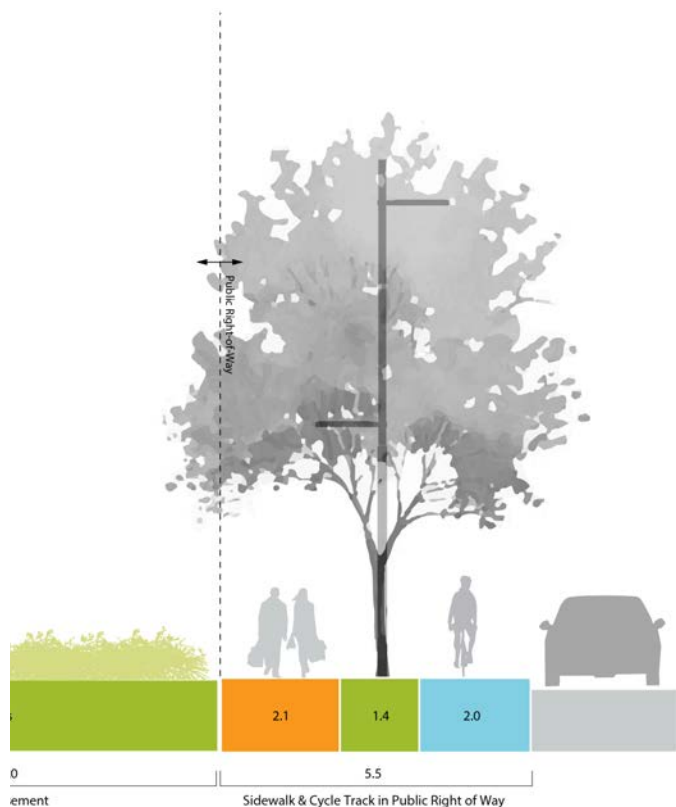


The Queensway

Option 2 illustrates wider north and south sidewalks, and includes a minimum 2.1m pedestrian clearway with a 1.4m planting and furnishing zone. Concrete unit pavers are recommended between the clearway and back of cycletrack. Additional clearway and tree planting is anticipated with development and within the generous setbacks and easements for both sides of The Queensway.

This detail includes a cycling facility within the street right-of-way which will require further study to determine the appropriate facility and dimension.

The Queensway Option 2: Tree Planting between Sidewalk and Cycle Track

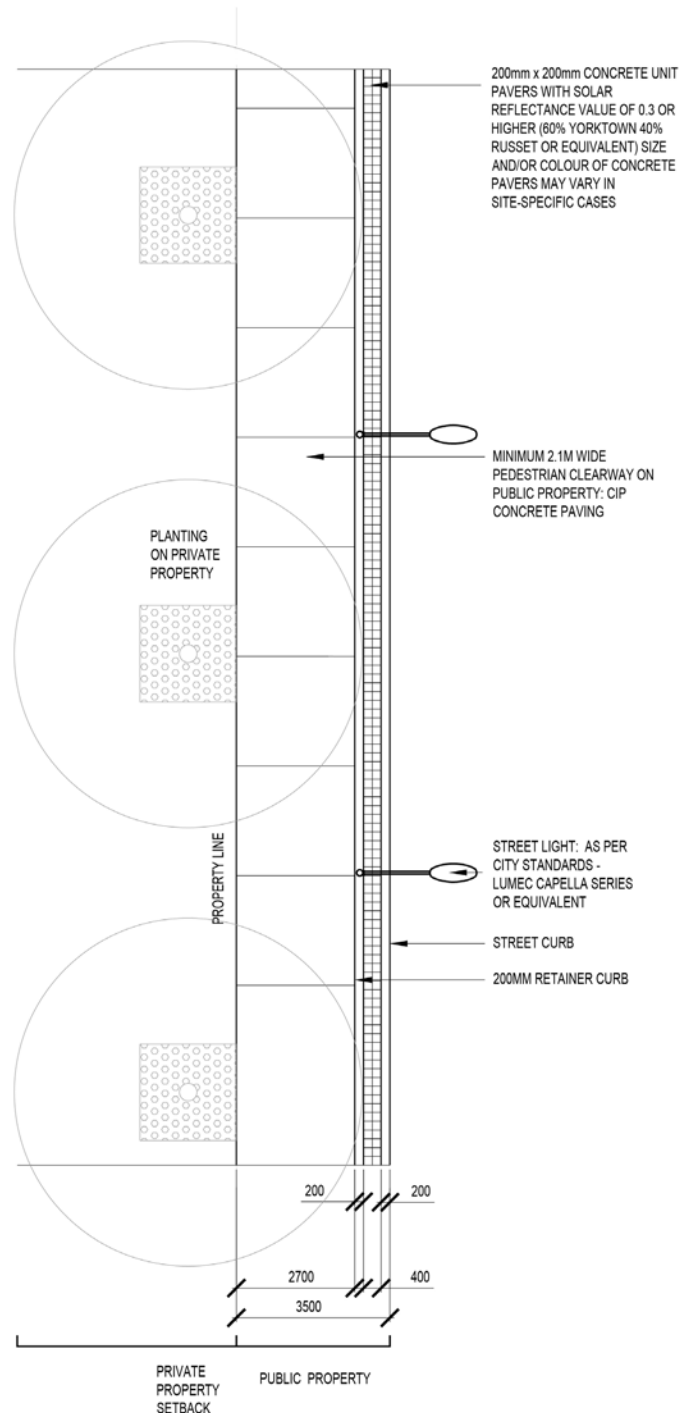
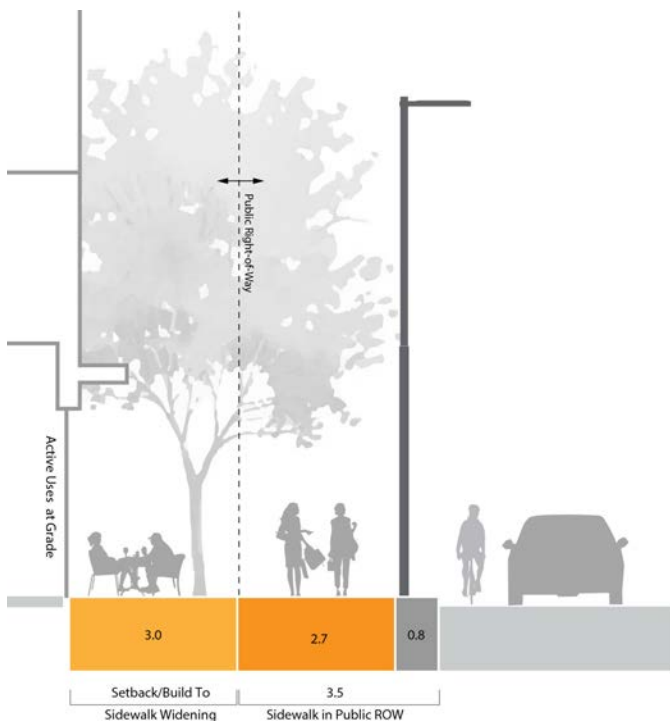


The West Mall

This detail illustrates the typical minimum sidewalk improvement for both sides of the West Mall north and south of the Queensway. The sidewalk design includes a 2.7m pedestrian clearway and a 0.8m edge zone. Tree planting is only recommended in the public right-of-way where sufficient clearance is provided to ensure proper growing conditions and healthy growth.

The pedestrian clearway will include standard cast-in-place concrete. Decorative concrete unit pavers are recommended between the clearway and back of curb. Setbacks are anticipated with development, which can increase the pedestrian clearway and introduce trees and other planting and other greening. Any additional sidewalk width within the public right-of-way determined through detailed design should be allocated to the pedestrian clearway.

The West Mall: Sidewalk with Tree Planting in Private Setback

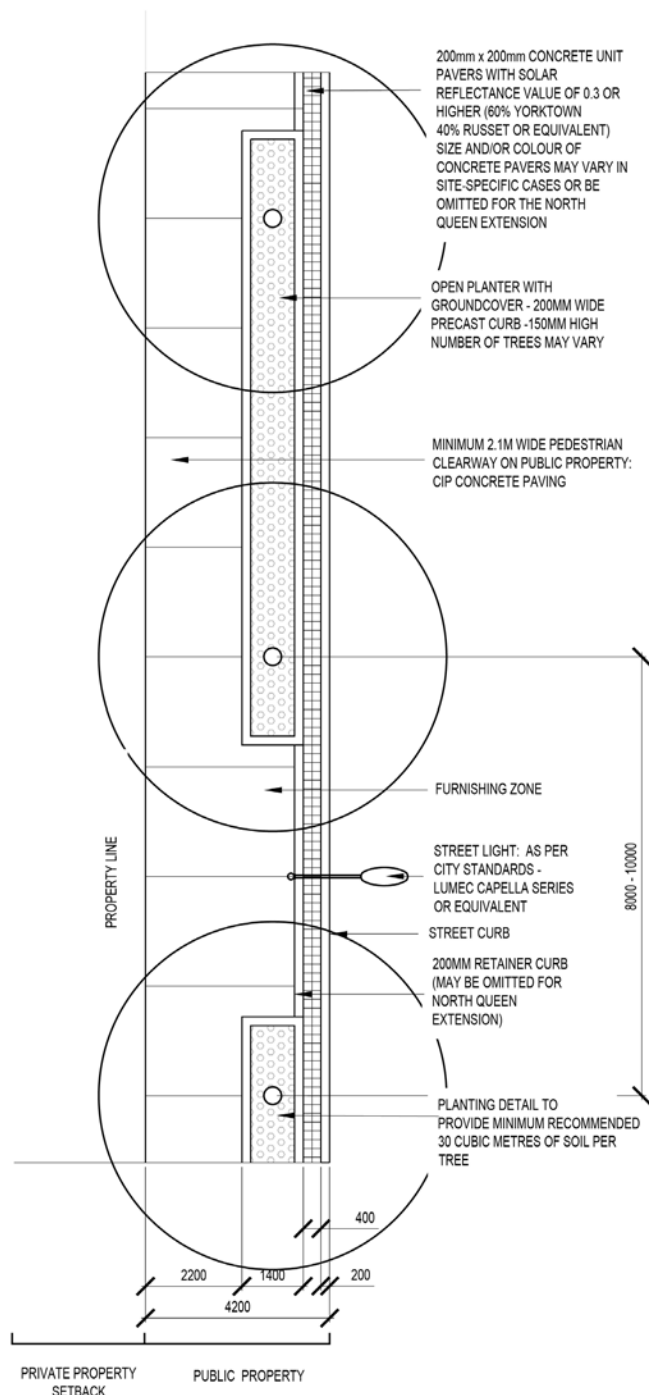
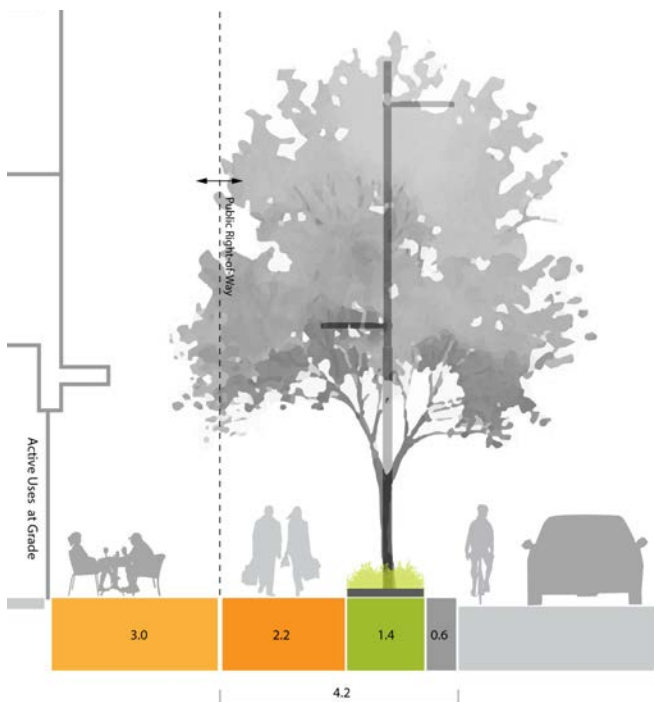


North Queen Street & Sherway Gardens Road

This detail illustrates the North Queen and North Queen Extension sidewalks as well as the re-aligned Sherway Gardens Road, and includes a 2.2m pedestrian clearway, a 1.4m open planter, and a 0.6m edge zone. Additional clearway and tree planting is anticipated with development within required setbacks. Any additional sidewalk width within the public right-of-way (determined through detailed design) should be allocated to the widening of the furnishing and planting zone to improve tree planting conditions.

This detail does not include any cycling infrastructure which will require further study to determine the appropriate facility and dimension.

North Queen Street & Sherway Gardens Road: Sidewalk with Tree Planting in Open Planters

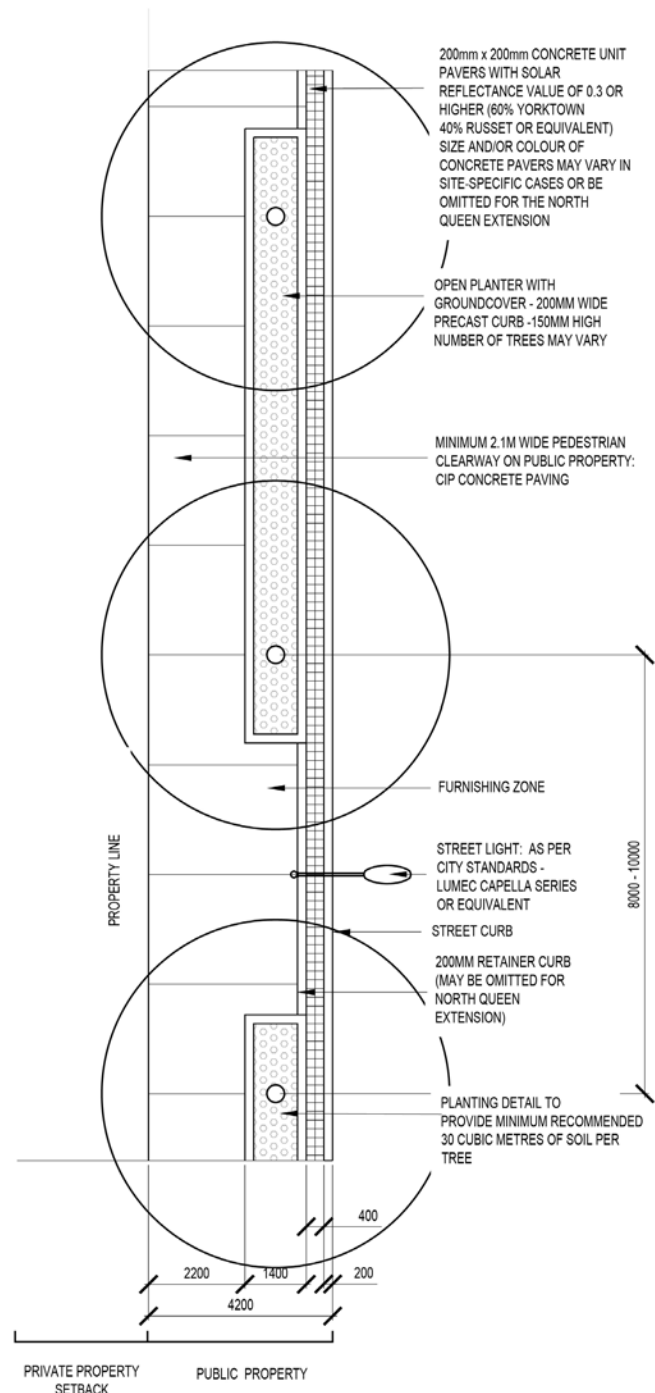
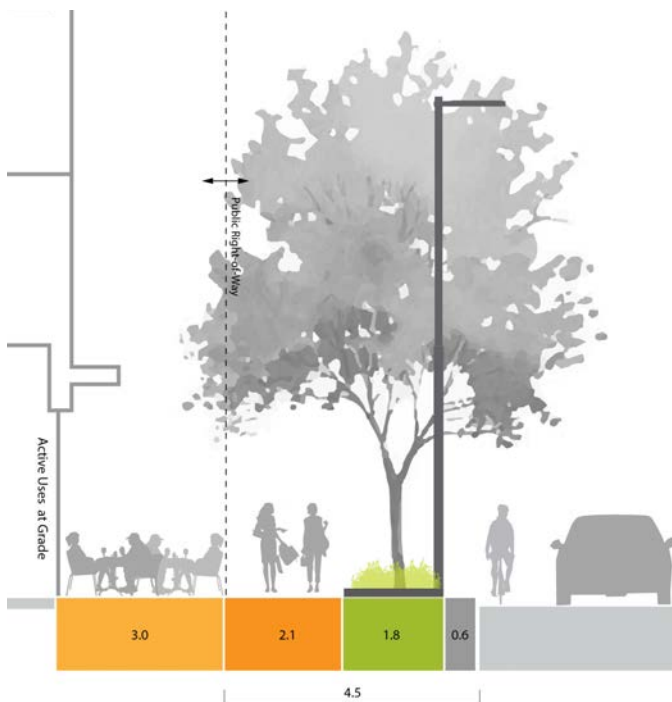


Nova Road and Boncer Drive

This detail illustrates the minimum improvement for sidewalks on Nova Road and Boncer Drive. It includes a minimum 2.1m pedestrian clearway, a 1.8m open planter, and a 0.6m edge zone. Concrete unit pavers are recommended between the clearway and back of curb. Any additional sidewalk width within the public right-of-way (determined through detailed design) should be allocated to the widening of the furnishing and planting zone to improve tree planting conditions.

This detail does not include any cycling infrastructure which will require further study to determine the appropriate facility and dimension.

Nova Road and Boncer Drive: Sidewalk with Tree Planting in Open Planters

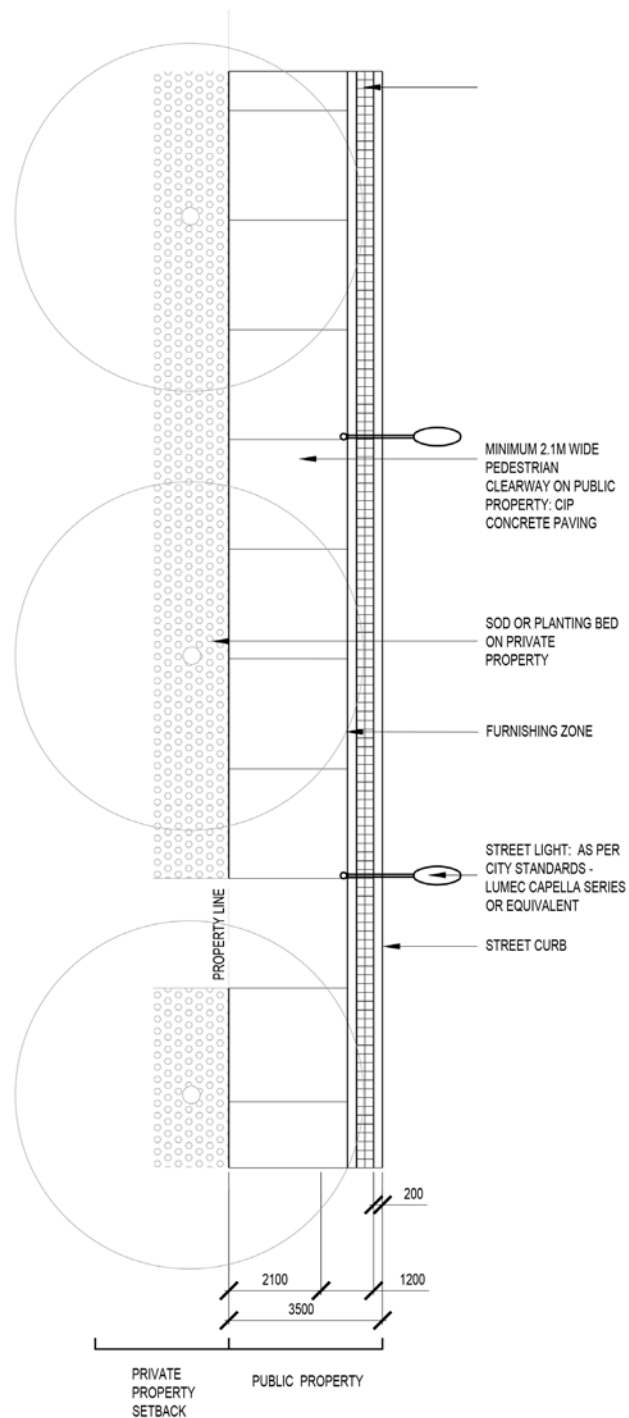
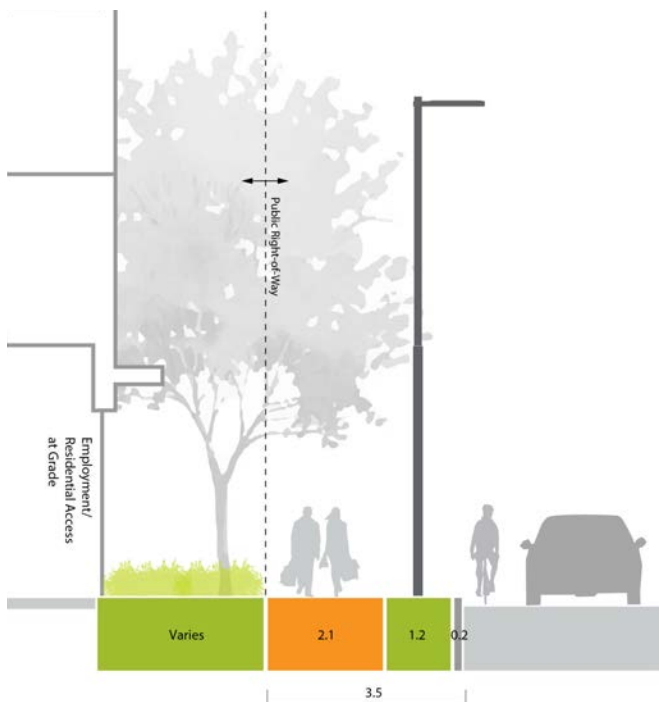


Evans Avenue

This detail illustrates the minimum improvement for sidewalks on Evans Avenue. It includes a minimum 2.1m pedestrian clearway. The furnishing and planting zone can include either vegetation or decorative concrete pavers. Additional frontage and marketing zone or planting is anticipated with setbacks achieved through development. Any additional sidewalk width within the public right-of-way (determined through detailed design) should be allocated to the pedestrian clearway. Tree planting is only recommended in the public right-of-way where sufficient clearance is provided to ensure proper growing conditions and healthy growth.

This detail does not include any cycling infrastructure which will require further study to determine the appropriate facility and dimension.

Evans Avenue: Sidewalk with Tree Planting in Private Setbacks

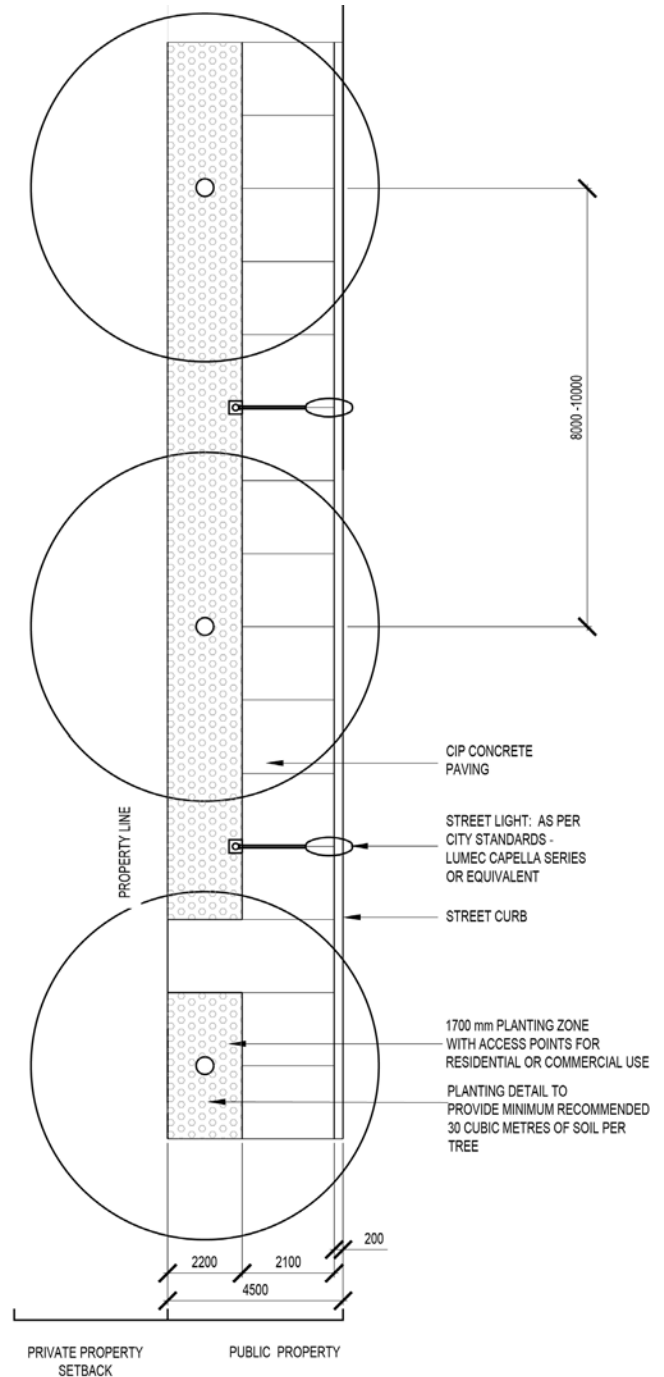
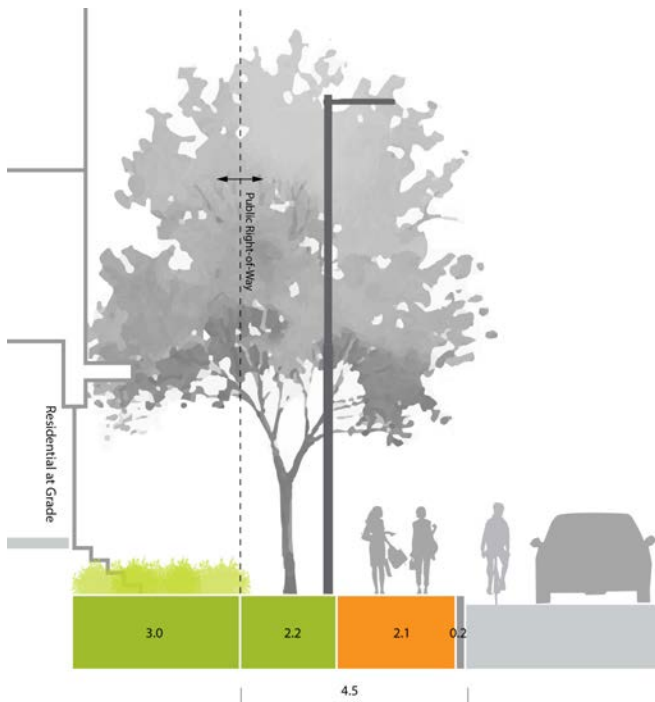


Local Neighbourhood Streets

Option 1 illustrates the minimum sidewalk for Neighbourhood Street with a 20.0m right-of-way. This detail includes a minimum 2.1m pedestrian clearway at the curb and a 2.2m planting zone at the back of sidewalk. Additional landscaping is possible in the minimum 3.0m private setbacks. This sidewalk detail is often considered on streets with residential uses at grade. Any additional sidewalk width within the public right-of-way (determined through detailed design) should be allocated to the widening of the furnishing and planting zone to improve tree planting conditions.

This detail does not include any cycling infrastructure which will require further study to determine the appropriate facility and dimension.

Local Neighbourhood Street: Sidewalk at Curb (Illustrating Residential at Grade)



Local Neighbourhood Streets

Option 2 illustrates the minimum sidewalk for Neighbourhood Street with a 20.0m right-of-way. This detail includes a minimum 2.1m pedestrian clearway, a 1.8m planting zone, and a 0.6m edge zone. Concrete unit pavers are recommended between the clearway and back of curb. Additional landscaping is possible in the minimum 3.0m private setbacks. This sidewalk detail is often considered on streets with active retail or residential uses at grade. Any additional sidewalk width within the public right-of-way (determined through detailed design) should be allocated to the widening of the furnishing and planting zone to improve tree planting conditions.

This detail does not include any cycling infrastructure which will require further study to determine the appropriate facility and dimension.

Local Neighbourhood Street: Tree Planting at Curb (Illustrating Active Uses at Grade)

