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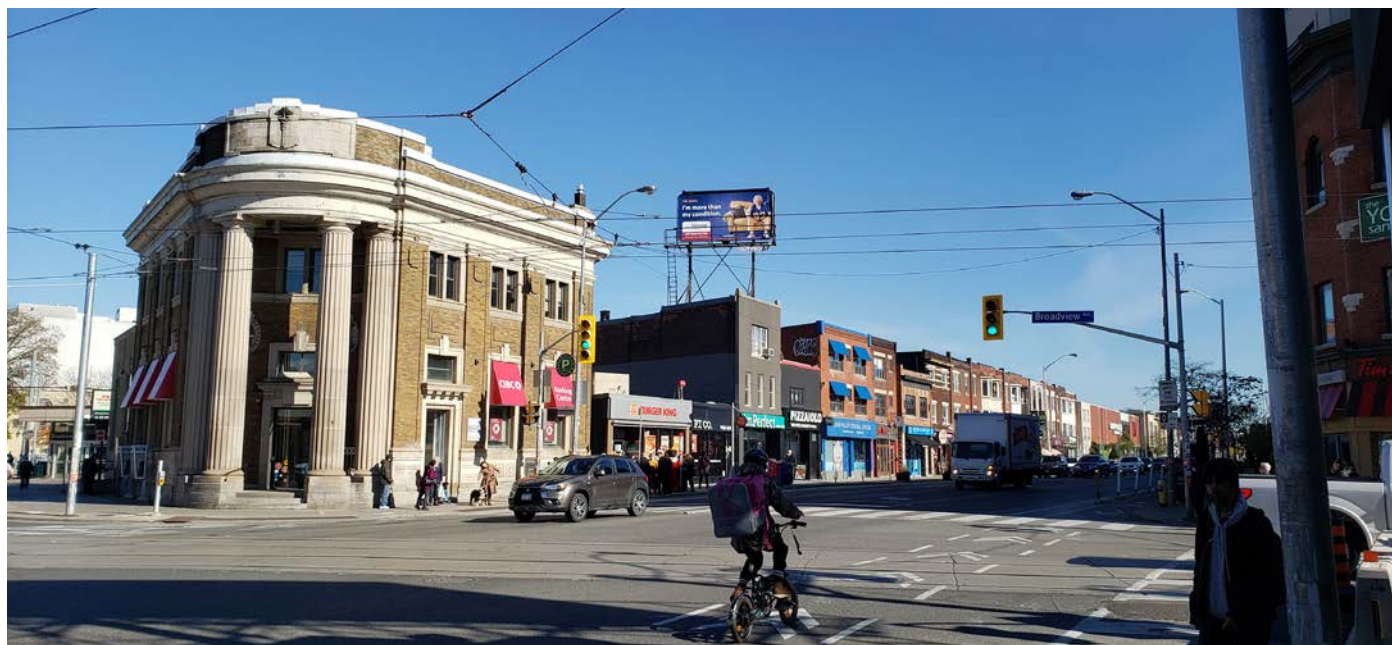
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Two-to-three-storey main street buildings frames most part of the Study Area creating a consistent streetwall.

5.1 BUILT FORM CHARACTERISTICS

The goals and objectives of the built form guidelines and standards were carefully crafted to ensure the existing historic main street character of the Danforth Avenue is maintained while permitting additional growth on the main street to support the City's growth objectives.

- Provide appropriate density with the goal of adding new housing (of various building typologies) along the corridor.
- Ensure that the existing historic main street character of the Danforth and its vibrant small-scale storefronts are conserved, maintained and enhanced with new developments.
- Minimize the impact on sensitive land uses such as *Neighbourhoods* and *Parks & Open Spaces Areas*.
- Enhance and increase the space for public realm improvements to sidewalks, pedestrian connections, open spaces and amenities.
- Eliminate any net new shadow of new developments on the sidewalks located on the north side of the Danforth between 12:00pm to 5:00pm from March 21st to September 21st.
- Provide an appropriate transition to *Neighbourhoods*.



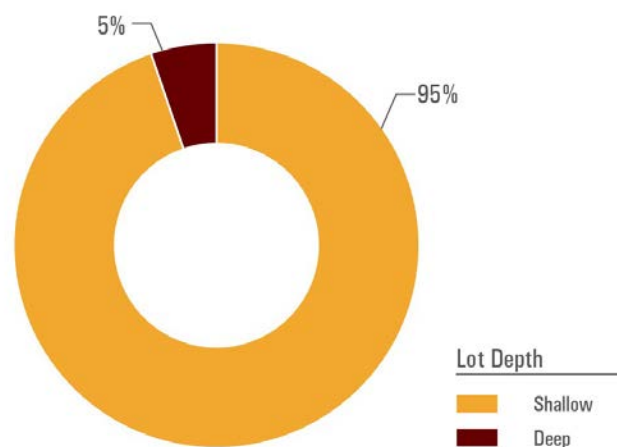
Conceptual Illustration with 8-storey Mid-rise Buildings along Danforth Avenue.

The built form guidelines will provide details for the following Policy Areas with various built form types and conditions including new mid-rise developments as well as low-scale additions on existing buildings.

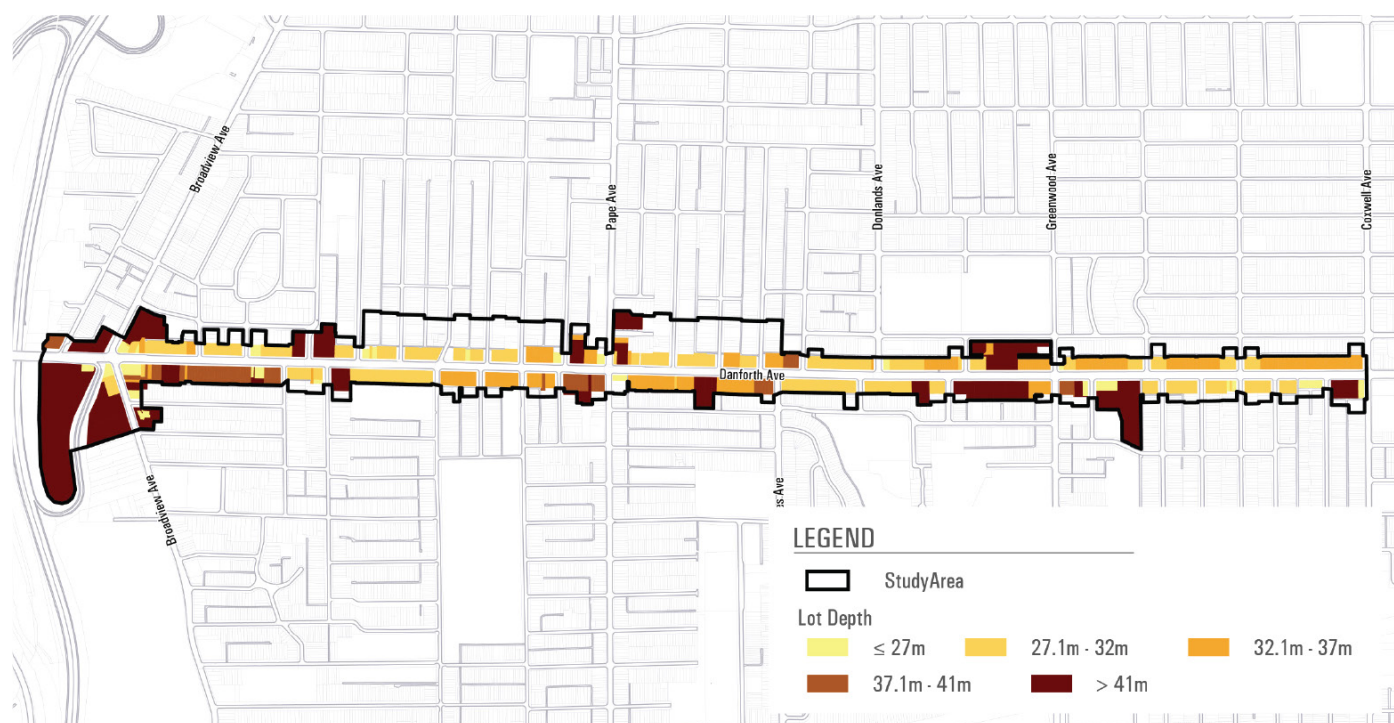
5.1.1 SITE CHARACTERISTICS

The majority of the lots within the Study Area have similar lot characteristics; and are generally narrow and shallow. When compared to the city-wide Mid-rise Building Performance Standards, approximately 95% of the lots within the Study Area are considered 'shallow lots'. For the purposes of the Study, lots with less than 41.0 metres are identified as shallow lots, with the vast majority of the lots have depths close to 30 metres.

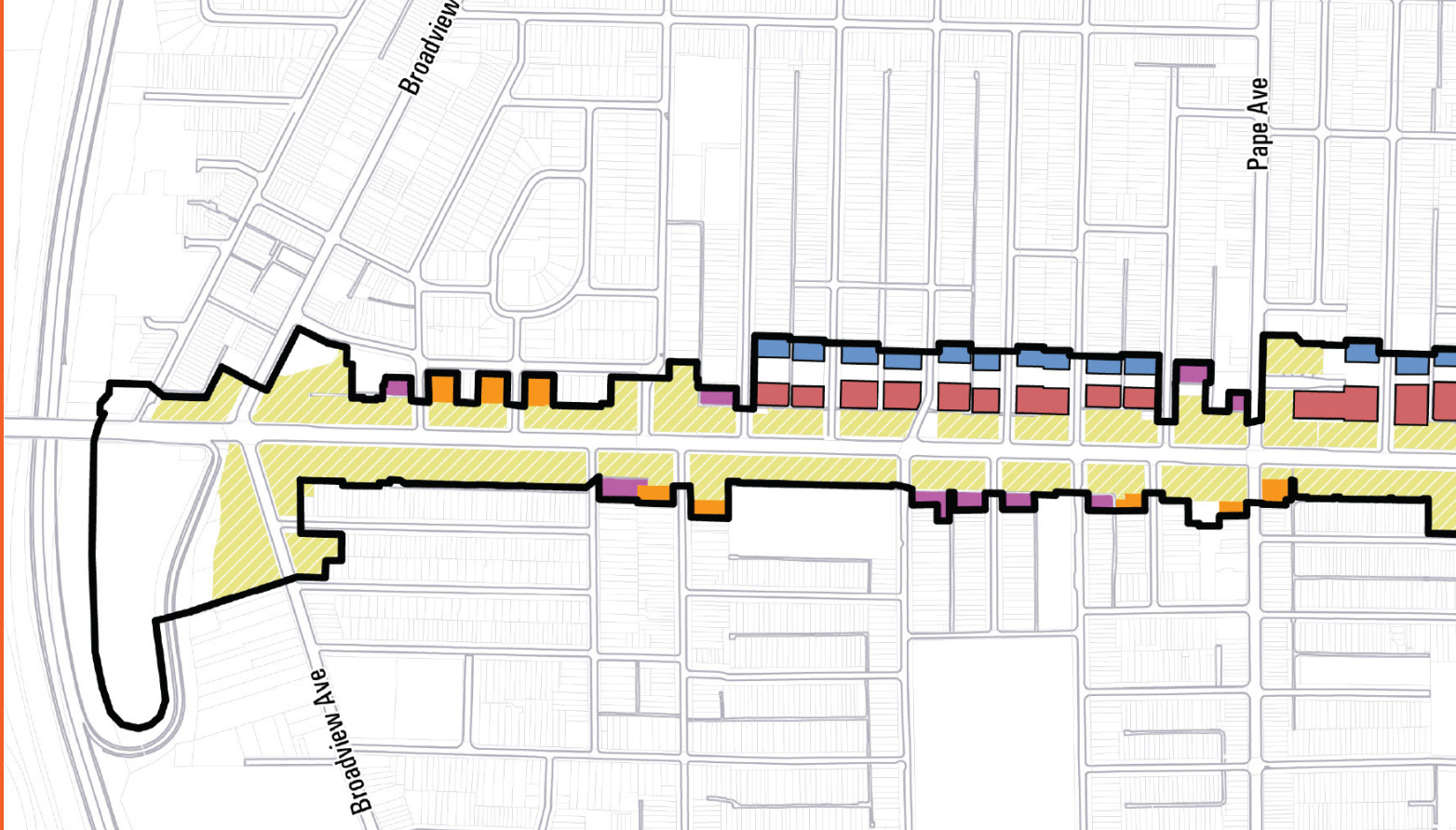
Due to the limited sizes of the lots and lot characteristics within the area, mid-rise building developments may require lot consolidations to support mid-rise development. There are other considerations including, but not limited to, heritage conservation and public realm extensions which should also be reviewed to demonstrate the appropriateness of mid-rise development on a site.



Close to 95% of the total number of the lots are identified as Shallow lots as per the city-wide Mid-rise Building Performance Standards.



Map 1: Lot depth



Map 2: Location of different Policy Areas

5.2 POLICY AREAS

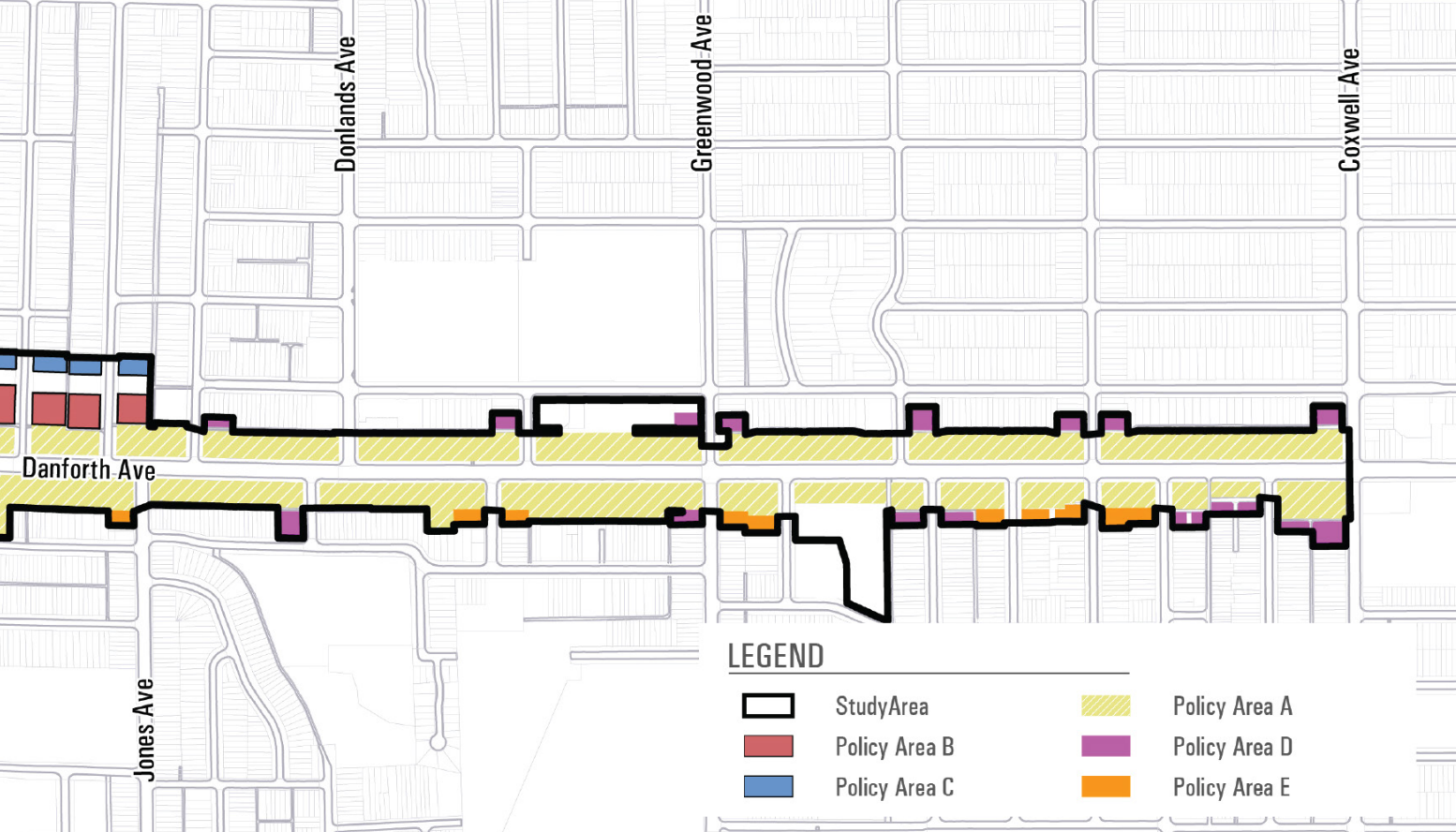
The Study Area has been divided into five Policy Areas based on the existing land use and built context. Lots designated *Mixed Use Areas* fronting onto the Danforth have been identified as Policy Area A. The *Neighbourhoods* designated lots in behind Policy Area A will be identified as either Policy Area B, C, D, or E. The Policy Area boundaries are shown on Map 2.

- Policy Area A applies to lands designated *Mixed Use Areas* fronting onto the Danforth.
- Policy Area B generally applies to the lands between the Danforth-fronting *Mixed Use Areas* lands and the open space network on the north side of the Danforth.
- Policy Area C applies to lands located on the northern edge of the open space network, north of the Danforth.
- Policy Area D includes between 2 to 3 lots north or south of the Danforth when there is a public lane separating *Mixed Use Areas* and *Neighbourhoods* designated lands.
- Policy Area E includes between 2 to 3 lots north or south of the Danforth when there is no public lane separating *Mixed Use Areas* and *Neighbourhoods* designated lands.



Diagram 1: This diagram shows how greater density and housing can be achieved along the Danforth corridor by changing the permissions on the sites within Policy Area B. These permissions allow up to 5 storeys on lots to the north of the Danforth frontage. Increased heights and the removal of lot specific side and rear yard setbacks are also permitted to achieve low-rise apartment style built form. The proposed massing allows for context appropriate buildings along The Danforth and on the blocks behind.

These Policy Areas have been established to shape new development while also providing an adequate transition to the lower scale *Neighbourhoods*.



New mid-rise buildings in Policy Area A will have a rear transition that is unique to the Danforth and takes into consideration the Study objectives listed in Section 5.3.1. When a development in Policy Area A is coupled with a development within Policy Area B, D or E, the rear transition is modified and there is an overall development gain as detailed further within this Section. Policy Areas B, C, D and E are intended to provide a gradual transition to the established Neighbourhoods to the north and south of the Danforth.

This unique built form approach will allow for greater density without compromising the existing built fabric on the Danforth and adverse impacts onto lands designated *Neighbourhoods*.



City staff studied various mid-rise options along Danforth Avenue including a 12-storey option and determined that buildings higher than 8-storey are inappropriate along the street.



Conceptual illustration with 8-storey mid-rise buildings along Danforth Avenue.

5.3 POLICY AREA A

As noted earlier, lands designated *Mixed Use Areas* will be identified as Policy Area A and mid-rise buildings up to seven storeys with a modified built form approach will be permitted. In areas where new developments can be built with developments within one of the identified transition areas (Policy Area B, D, or E), buildings up to eight storeys will be permitted in Policy Area A with a further modified built form approach that would include a lesser rear transition requirement.

5.3.1 MID-RISE BUILDINGS IN POLICY AREA A

Detailed and extensive planning and built form analyses were performed by City staff to find an alternate design approach that is unique to Danforth Avenue while still maintaining the intent of the Avenues and Mid-rise Buildings Guidelines and the overarching Official Plan.

Additional setbacks and stepbacks will be required along the Danforth to maintain access to sky view and sunlight as well as conserving the historic main street character and transition to heritage properties. Development adjacent to or on top of heritage buildings will be subject to a Heritage Impact Assessment.

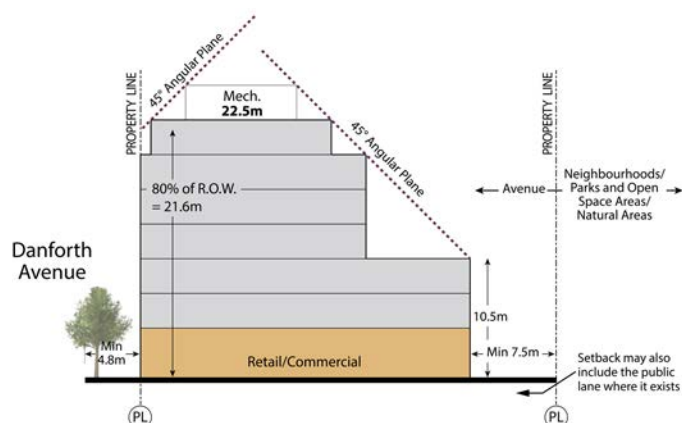


Diagram 2: Cross-section in accordance with city-wide standards for shallow lots which is not intended to be used in this urban design guidelines.

GUIDELINES

Mid-rise Building Heights

- The maximum building height in Policy Area A will be 24.0 metres (excluding mechanical penthouse), for a total built form of up to seven storeys.
- When developments within Policy Area A are paired with new development in adjacent Policy Area B, D, or E, the maximum building height in Policy Area A will increase to 27.0 metres (excluding mechanical penthouse), for a total built form of up to eight storeys.
- Mechanical penthouses will be limited to a maximum height of 5.0 metres above the roof level established in 5.4 (a) and (b).
- A ground floor height of 4.5 metres is required, unless an existing heritage built form prohibits this from occurring. In these instances a minimum ground floor height of 3.5 metres may be supported subject to the findings of a Heritage Impact Assessment.

Building Setbacks and Step-backs

- Provide a minimum curb to building face dimension of 4.8 metres, except where a smaller width exists due to existing heritage properties. Cantilevers above the ground floor are discouraged and will not be considered if they encroach into the 4.8 metres setback zone.
- For new developments, provide a minimum 3.0 metres stepback from the building face at a height of 14.0 metres or less along the Danforth frontage. If the prevailing streetwall height is established by an existing heritage condition, provide the 3.0 metres setback from the height of the roof of the heritage building.
- For all developments on the north side of the Danforth, provide a 2.0 metres or more stepback from the building face at a height of 24.0 metres or less along the Danforth and any flanking street frontage.
- For all mid-rise developments on the south side of Danforth Avenue, provide an additional 4.0 metres stepback from the building face at a height of 24.0 metres or less along the Danforth and any flanking street frontage. Continue to provide a minimum 2.0 metres stepback on any flanking streets.
- Mid-rise buildings with frontage greater than 48.0 metres along the Danforth should provide variations in building facades to ensure that the buildings are not overly long.

Rear Transition

- j. Provide a minimum rear yard setback of 7.5 metres which may include a public or private lane where it exists.
- k. For mid-rise developments with a maximum height of 24.0 metres, provide a minimum 1.5 metres stepback at a maximum height of 14.0 metres and an additional 3.0 metres stepback at a maximum height of 17.0 metres from the rear building face.
- l. For mid-rise developments with a maximum height of 27.0 metres, provide a minimum 1.5 metres stepback at a maximum height of 14.0 metres from the rear building face.

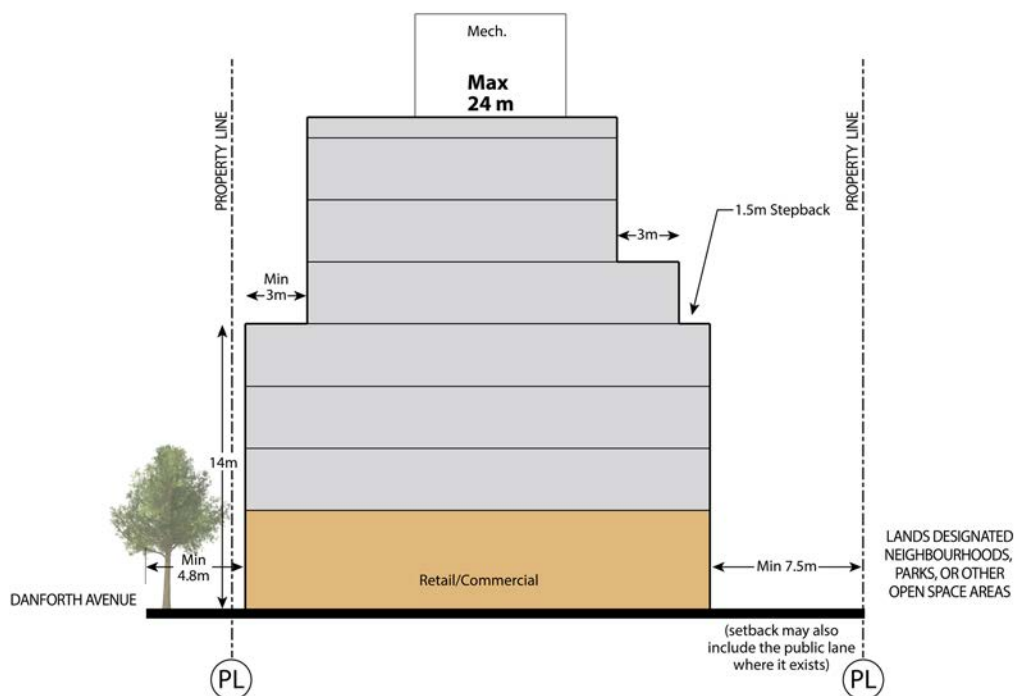


Diagram 3: Updated Standard 7-storey Mid-Rise Building Section

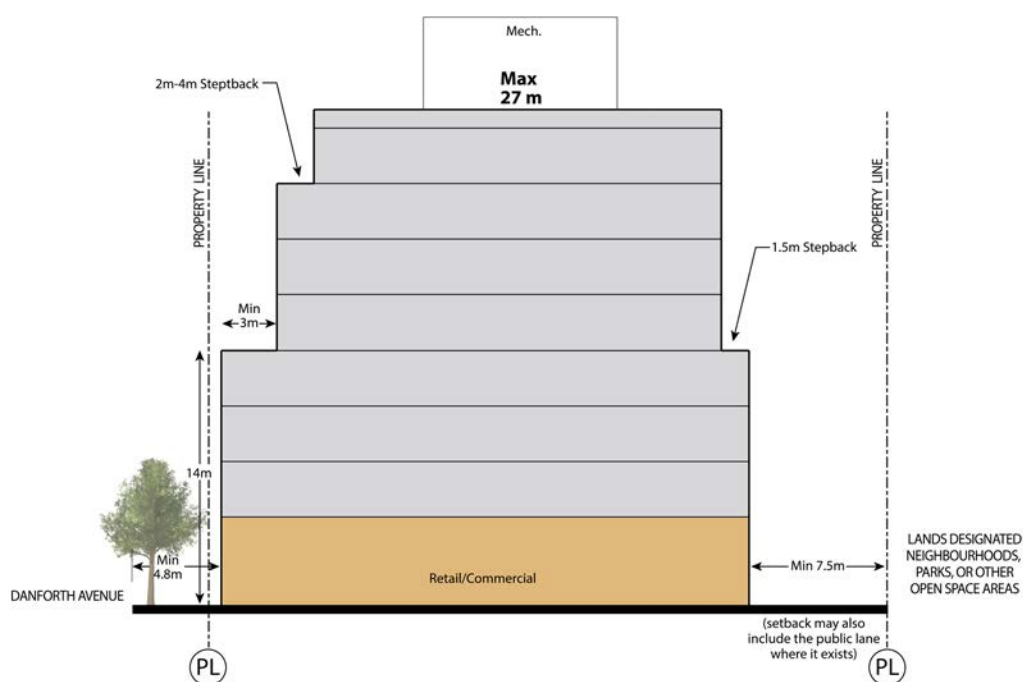


Diagram 4: Updated 8-storey Mid-Rise Building Section

5.3.2 BUILDING ON AND/OR ADJACENT TO HERITAGE PROPERTIES

The Cultural Heritage Resource Assessment (CHRA) has identified numerous properties in the Study Area as having heritage potential. Those properties will be further assessed to determine a recommendation for inclusions on the Heritage Register. New developments located on or adjacent to designated properties on the Heritage Register will be required to conserve the heritage attributes of those properties, as well as the historic main street character reflected in the prevailing streetwall height, fine-grained lot pattern, fenestration, materiality and articulation. All development applications on or adjacent to properties on the Heritage Register are subject to a Heritage Impact Assessment (HIA) and may require a site-specific approach in building design that could be beyond the requirements of these guidelines.

GUIDELINES

New mid-rise buildings on or adjacent to heritage properties will:

- Provide a minimum 3.0 metres building setback above the existing streetwall height.
- For corner properties, provide a minimum 3.0 metres building setback above the existing streetwall height along the flanking street elevation.
- Provide fenestration, materiality and articulation that are compatible with the onsite and/or adjacent heritage properties.
- Avoid cantilevers and balcony projections into the building setback above heritage properties.
- Minimize impacts to heritage properties and their prominence within the existing historic main street context of the Study Area.

7-storey Mid-rise Building with existing Heritage Properties



- Prevailing heritage streetwall height
- Minimum 3m setback above heritage streetwall
- Materiality and articulation should be compatible to heritage properties on lower podium
- Minimum 3m setback at level 6 or at 17m above grade
- Minimum 1.5m setback at level 4 or at 14m above grade

8-storey Mid-rise Building with existing Heritage Properties



- Minimum 2m-4m Building Setback at Level 8 subject to shadow impact on public realm
- Minimum 1.5m Setback at Level 4 or at 14m above grade

5.4 POLICY AREA B

Policy Area B includes lands designated *Neighbourhoods* located between a portion of Policy Area A and the existing Toronto Parking Authority (TPA) parking facilities or public parks located between Chester Avenue to Langford Avenue.

GUIDELINES

- a. Refer to Map 2 the precise location of Policy Area B.
- b. The maximum height of new buildings within Policy Area B is 16.0 metres excluding the mechanical penthouse.
- c. Mechanical penthouses will be limited to a maximum height of 5.0 metres above the roof level.
- d. Provide unit accesses with generous landscaped front yards facing public street frontages to maintain existing neighbourhood street character.
- e. Provide unit accesses facing onto the existing Toronto Parking Authority parking facilities or public parks via a Greenway as shown in Section 4.7. Refer to Diagram 5 for more details.
- f. Provide a minimum building side yard setback of 6.1 metres along the property line abutting the existing TPA facilities or public parks.
- g. Provide a minimum setback of 1.0 metres on the side and rear of new developments where the property lines abut an existing public or private lane.
- h. Provide a minimum setback of 3.0 metres at a maximum height of 12.0 metres from the building edges on East, West, and North property lines to create pedestrian-scaled streetwall and to mitigate shadow impact onto the Greenway.
- i. Provide minimum of 3.5 metres setback at the second level beyond 11.0 metres of building length along the property line abutting a public or private lane.
- j. Ensure that all parking or loading parking ramps and/or loading areas are accessed from a private or public lane.
- k. Provide high-quality materials that are complementary to and reflect the character of Neighbourhoods to the north.
- l. Refer to Diagram 6 for additional clarification on Section 5.5 (a)-(g) on setback and setback requirements.



Permitted building typology in Policy Area B includes Low-rise apartments, hybrid buildings, stacked townhomes, and townhomes. Credit: Ben Rahn, Avryll McNair



Diagram 5: Buildings should frame public streets and the existing Toronto Parking Authority facilities and/or public parks.

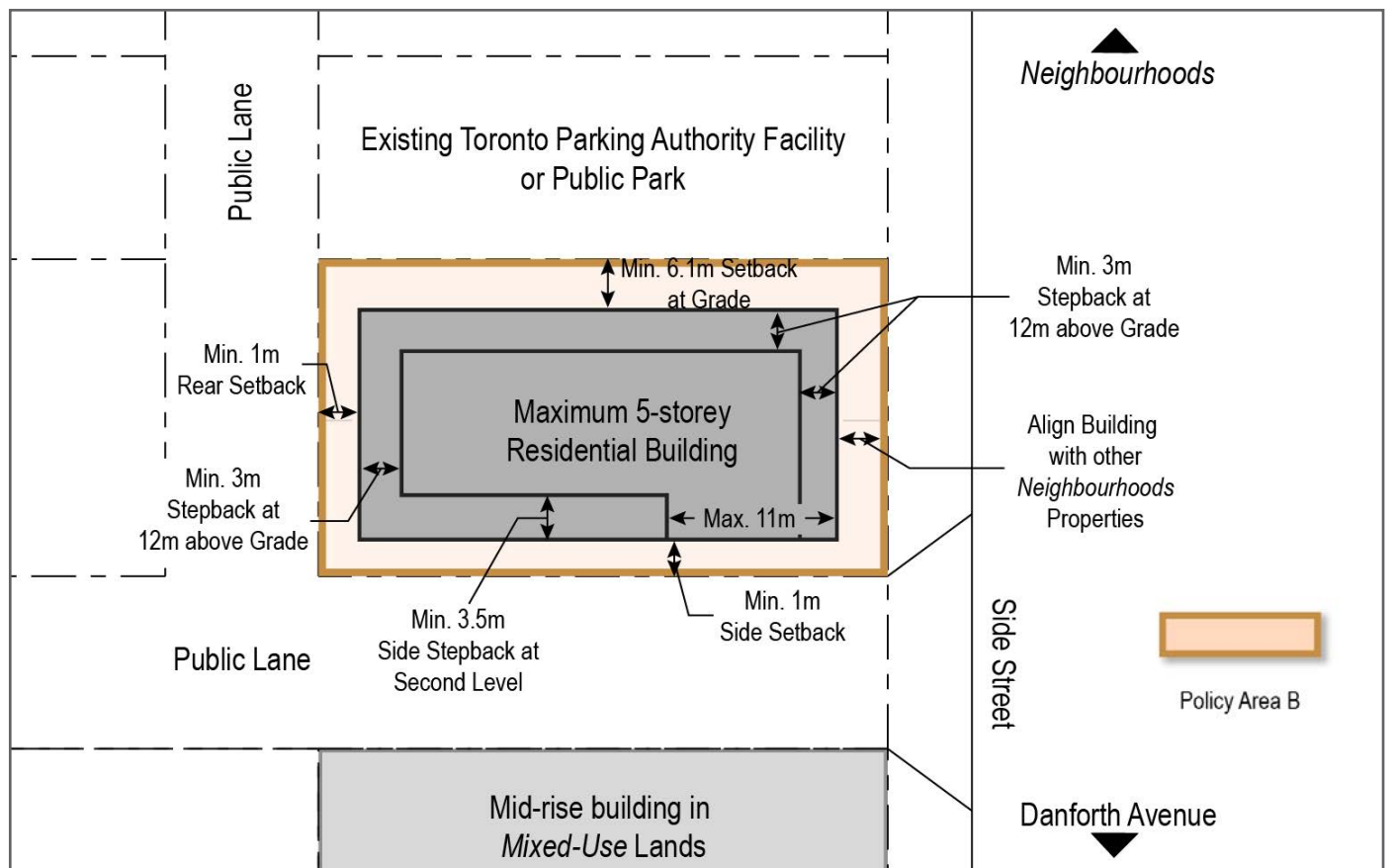


Diagram 6: Setback and stepback requirement for Policy Area B.

5.5 POLICY AREA C

Policy Area C includes lands designated *Neighbourhoods* on the north side of the existing TPA parking facilities and/or public parks. Policy Area C includes all the lots within a 19.0 metres depth from the north property lines of the City-owned TPA parking facilities and public parks. Policy Area C has been designed to provide a clear edge to the *Neighbourhoods* to the north as well as an active edge to the Greenway which is created through redevelopment.

GUIDELINES

- Lands within Policy Area C have been identified on Map 2 in Section 5.3.
- The maximum height of new buildings in Policy Area C is 12.0 metres excluding the mechanical penthouse.
- Mechanical penthouses will be limited to a maximum height of 4.0 metres above the roof level established in Section 5.6 (b).
- New developments should frame the newly-created Greenway as outlined in Section 4.7 with ground-related units.
- Provide units with ground access fronting onto the newly created Greenway as outlined in Section 4.7 with a minimum 6.1 metres side yard setback at grade.
- Provide a minimum side yard setback of 1.0 metre and a minimum of 3.5 metres beyond 11.0 metres of building length along the property line abutting *Neighbourhoods*.
- Provide high-quality materials that are complementary to and reflect the character of *Neighbourhoods* to the north.
- Provide a minimum rear yard setback of 1.0 metre.
- Provide a front yard setback with generous landscaping equivalent in depth and complementary to the existing street character of the surrounding *Neighbourhoods*.
- Refer to Diagram 8 for additional clarification on setback and stepback requirements.



Diagram 7: Buildings should frame the existing TPA facility or public park with grade-related units fronting onto Greenways.



An example of a green corridor framed by residential units.

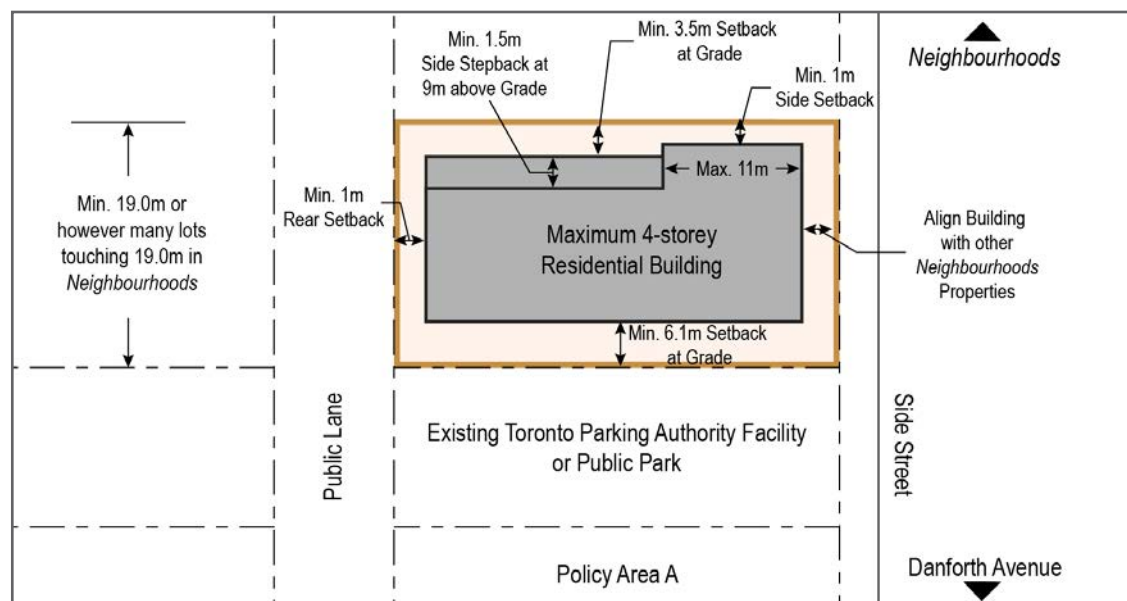


Diagram 8: Setback and stepback requirements for Policy Area C.

5.6 POLICY AREA D

Policy Area D includes a minimum of two or a maximum of three *Neighbourhoods* lots on the north or south side streets perpendicular to the Danforth when there is a private or public lane bisecting the two Policy Areas.

GUIDELINES

- Lands within Policy Area D have been identified on Map 2 in Section 5.3.
- Low-rise apartments, hybrid buildings, stacked townhomes, and townhomes are permitted in Policy Area D.
- The maximum height of new buildings in Policy Area C is 14.0 metres excluding the mechanical penthouse.
- Mechanical penthouses will be limited to a maximum height of 4.0 metres above the roof level established in Section 5.7 (c).
- Provide a front yard setback with generous landscaping equivalent in depth and complementary to the existing street character of the surrounding *Neighbourhoods*.
- Provide a minimum side yard setback of 1.0 metres and 3.5 metres beyond 11 metres of building length along the property line abutting *Neighbourhoods*.
- Provide a minimum side yard setback of 1.0 metres along the property line abutting a public or private lane.
- Provide high-quality materials that are complementary to and reflect the character of surrounding *Neighbourhoods*.
- Refer to Diagram 9 for additional clarification on setback and stepback requirements.

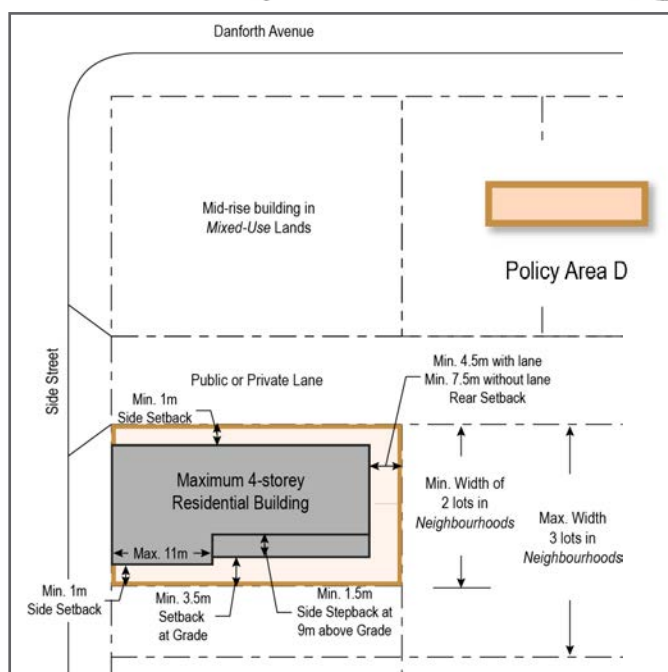


Diagram 9: Setback and stepback requirement for Policy Area D.

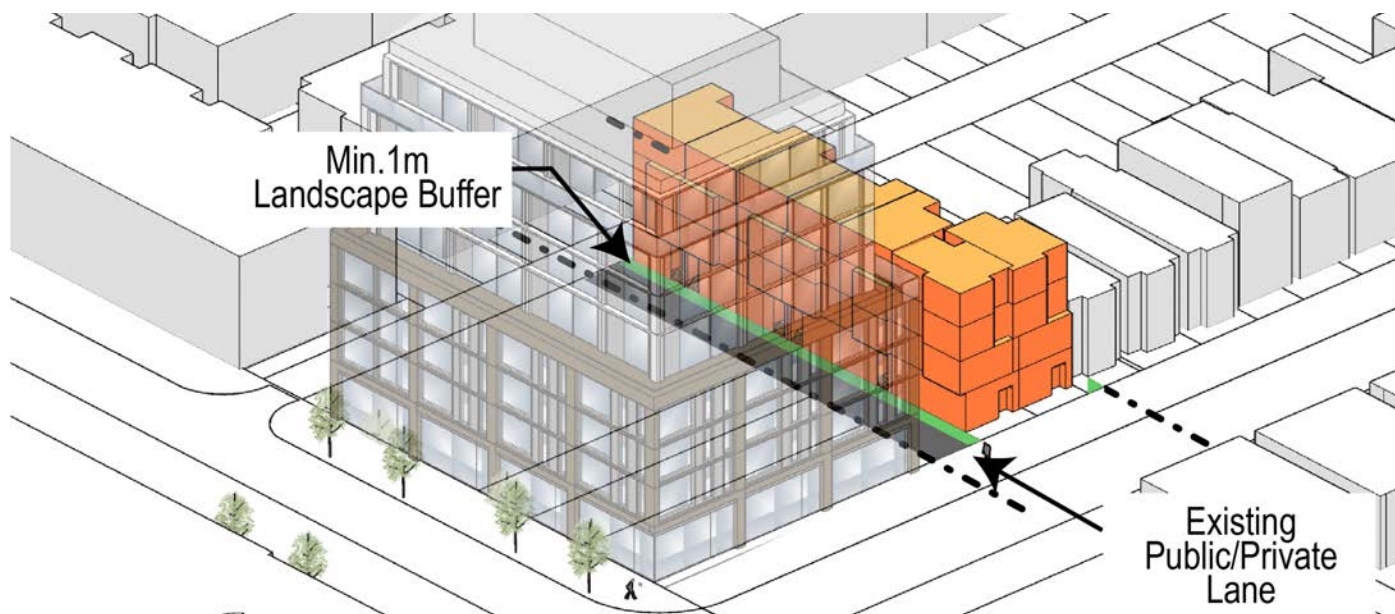


Diagram 10: Policy Area D includes two to three lots located behind the lands designated *Mixed Use Areas* separated by an existing private or public lane.

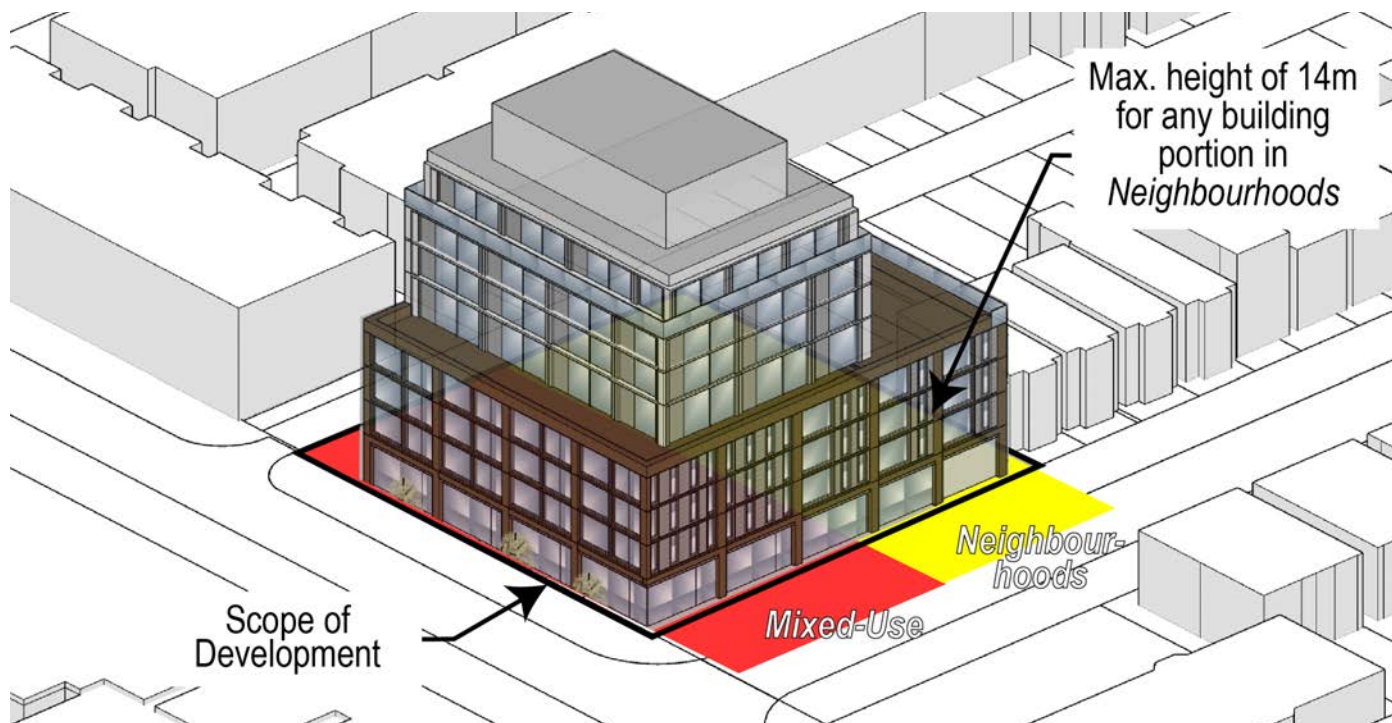


Diagram 11: Policy Area E is located in the *Neighbourhoods* portion of a development and the maximum allowable building height is 14 metres.

5.7 POLICY AREA E

The Policy Area E includes a minimum of two or a maximum of three *Neighbourhoods* lots on the north or south side streets perpendicular to the Danforth when there is no private or public lane bisecting the two Policy Areas. Any built structure in Policy Area E is an extension of the mid-rise building on the *Mixed Use* Area with appropriate setback and stepbacks.

GUIDELINES

- Lands within Policy Area E have been identified on Map 2 in Section 5.3.
- The maximum building height in Policy Area E is 14.0 metres including all mechanical equipment.
- Provide a minimum 3.5 metres side yard setback from the property line that is perpendicular to the side street.
- Provide a minimum 4.5 metres rear yard setback abutting an existing public or private lane or a minimum 7.5 metres without a lane.
- Provide a minimum 1.5 metres setback for the upper portion of the new development from lands designated *Neighbourhoods*.
- Provide high-quality materials that are complementary to and reflect the character of surrounding *Neighbourhoods*.
- Refer to Diagram 12 for additional clarification on setback and stepback requirements.

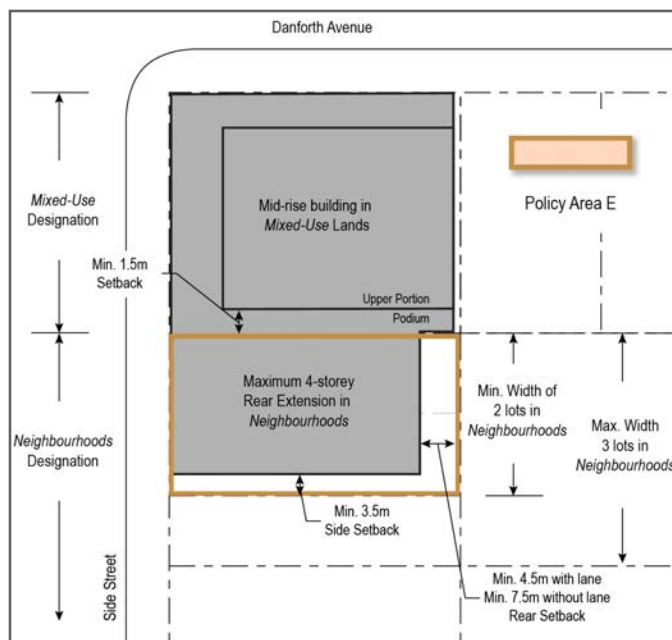


Diagram 12: Setback and stepback requirement for Policy Area E.

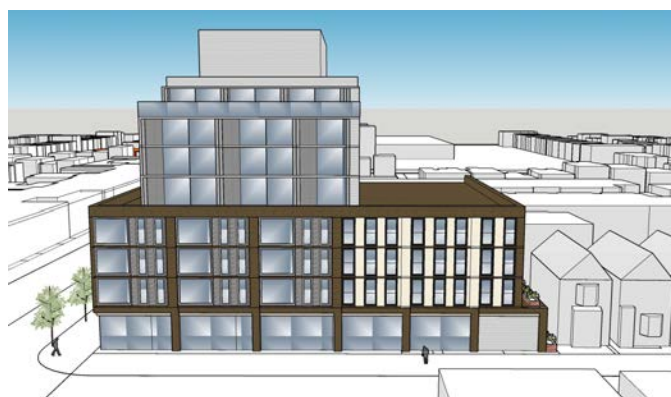
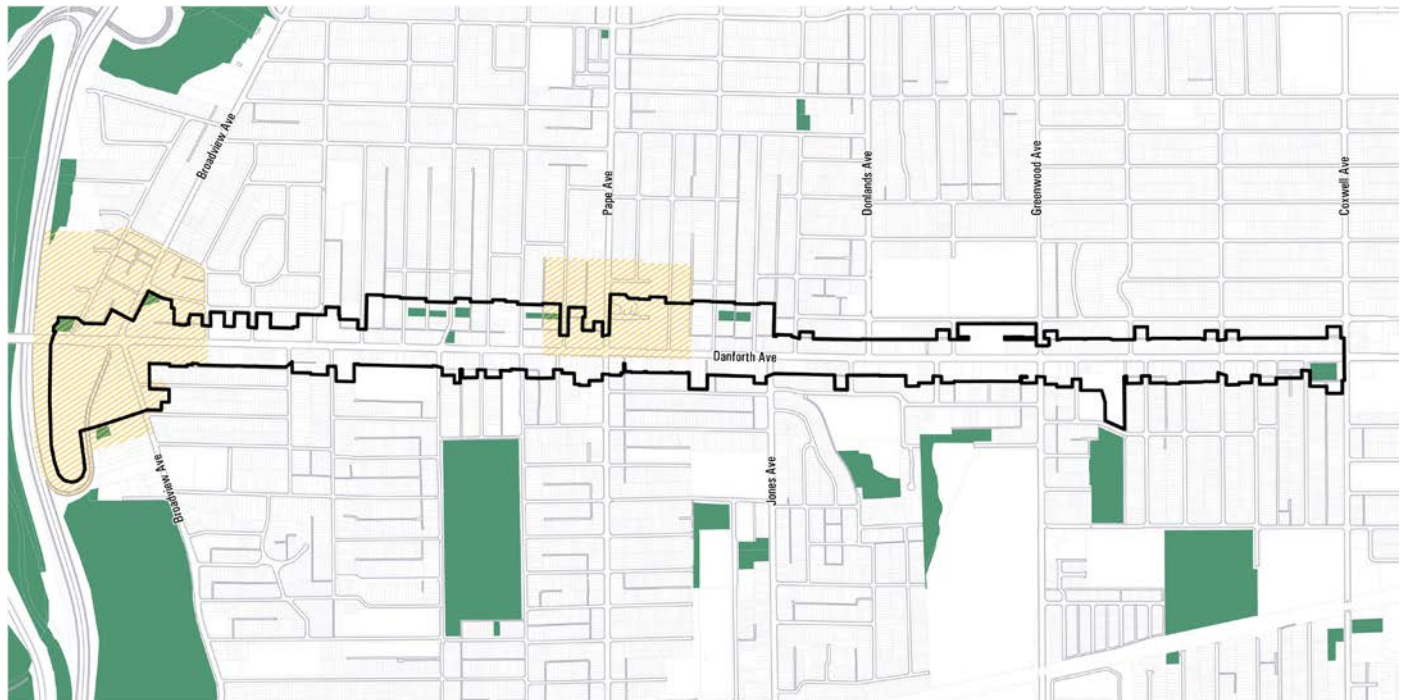


Diagram 13: Conceptual illustration of a mid-rise development which includes Policy Area E.

5.8 POTENTIAL NODES

The study identifies two potential nodes, at the Broadview Avenue and Pape Avenue subway interchange stations. These nodes provide for future opportunities to intensify and bring more affordable housing, employment, and community uses that would benefit from proximity to transit services offered at these stations.

These two nodes will require further study and until the study work is completed, this urban design guideline document will apply to the lands identified as Nodes within the study boundary. Other City documents, policies and guidelines will be applied to the Nodes outside of the study boundary.



The precise boundary of the two nodes are subject to further study.

5.9 BUILDING DESIGN

Danforth Avenue's historic main street character is defined by a fine-grain pattern of buildings with narrow retail frontages, pedestrian-oriented streetscape and smaller-scaled built forms. The architectural rhythm defined by the narrow lot pattern is fundamental to creating the vibrant storefronts and patios which are treasured by the community. Furthermore, the warm masonry material and articulation on the building facades are important elements of the built fabric of Danforth Avenue.

New buildings and additions should find ways to harmoniously fit-in with the surrounding historic main street context by considering building elements, such as articulation of building bays, references to horizontal datum lines, building materials, fenestration, side wall conditions, and the design of access and loading areas.

GUIDELINES

- a. Design new buildings to be complementary with the historic main street context by respecting the prevailing characteristics of Danforth Avenue.
- b. Design new development to be compatible with heritage properties, but avoid replicating historic architectural styles.
- c. Design the base of new buildings with solid materials such as brick and stone for the new streetwall to maintain and reinforce the historic main street character.
- d. Design new buildings to be compatible with the design, scale, form and massing of the prevailing streetwall and the Study Area's main street character.
- e. Avoid large and continuous glass surfaces on buildings and storefronts. Divide the building bays with solid materials to create a fine-grain pattern.
- f. Provide articulation, textures, and relief in the cladding of buildings to create depth and interest in the facade design.

New Developments and/or Additions on Heritage Properties

- g. Heritage properties should be conserved, maintained and enhanced as visually prominent in overall building design. New developments and additions should be compatible with the existing heritage property and the historic main street character of Danforth Avenue.
- h. Conserve and restore original exterior building features rather than replacing them. Replace only those original building features that have deteriorated beyond repair.
- i. Use exterior materials that are visually compatible with the prevailing historic main street character of the Study Area, and that do not negatively impact the integrity of the heritage property.
- j. Select cladding materials for new developments and additions on or adjacent to heritage properties to be distinctive from but compatible with the heritage property.



An example of good use of materiality and articulation on the facade design which is compatible with the existing heritage character. Credit: OFFICEArchitecture, Harhay Development

5.9.1 GROUND FLOOR HEIGHT, STOREFRONT SIZE AND DESIGN

Fine-grain retail stores and consistent ground floor heights are key characteristics of Danforth Avenue with significant cultural heritage value. These street frontages help make the street vibrant and lively with numerous patios and retail activities. New developments and additions should continue to provide these fine-grain retail patterns to enhance the existing historic main street character. These guidelines will provide different ways to create a more desirable retail environment for the community and visitors.



The fine-grain historic main street character is an integral part of Danforth Avenue.

GUIDELINES

- a. Ensure the design of the ground floors of new developments are compatible with the existing fine-grain historic main street context of Danforth Avenue and meets the following:
 - i. Ground floor height between 3.5- to 4.5 metres for mid-rise buildings;
 - ii. Ground floor frontage with building bays and vertical articulations of 6.0 to 8.0 metres in width; and
 - iii. Ground floor unit sizes generally in keeping with existing fine-grain character of the street.
- b. Provide recessed building entrances along the street frontage to create a consistent building articulation and rhythm at a pedestrian level and do not impede on the pedestrian clearway with door swings.
- c. Ensure the overall storefront design respects and enhances the architectural design of the existing building and the historic main street character of the Study Area.
- d. Design the Danforth frontage of the building as the primary retail frontage if the new development is on a corner lot.
- e. Provide a knee wall/window base with a height that is generally consistent with adjacent retail frontages along the street.
- f. Use transparent glass for storefront glazing and conserve the original windows historic components and features of storefronts.
- g. Align the signage band on the building to be consistent with adjacent storefront designs along the street.
- h. Avoid installing backlit and oversized box-signage on the signage band that obscure architectural details of the building or extend beyond the signage band area.
- i. Refer to the City's Retail Design Manual for more details on developing ground floor retail spaces.



Avoid using continuous glass store frontages for buildings with long frontages on Danforth Avenue

5.9.2 BUILDING MATERIAL

One of the local identities of Danforth Avenue within the Study Area is defined by the use of brick and stone along the existing streetwall. This main street character, which is typical of many main streets in Toronto, should be maintained and reinforced through use of high-quality materials and articulation for new developments fronting onto the street. Brick or stone material which is consistent of the existing character should be used for the lower part of new developments which frames the street.

GUIDELINES

- Use solid materials such as brick and stone for the new streetwall to reinforce the architectural character of the area.
- Limit the use of continuous glass frontages on the ground floor for buildings with long frontages on the Danforth. Divide long building frontages with vertical articulation created by solid materials to reinforce the fine-grain historic main street character of Danforth Avenue.
- Provide articulation, textures, and reliefs in the cladding of buildings to create depth and interest in the facade design.



New developments should provide articulation and textures in cladding design which are complimentary to the local character of the Danforth.

5.9.3 VEHICULAR ACCESS, LOADING AREA AND UTILITY

The vehicular access and/or loading areas of new developments should be located away from public view. Curb cuts, garage access, servicing and loading should be located towards the rear of new developments or towards the side streets. New developments' loading and servicing activities should be internalized and any parking spaces should be contained within below-grade structures. New curb cuts for vehicles will be avoided on Danforth Avenue right-of-way to minimize interruptions on the public realm and continuous streetscape improvements.

GUIDELINES

- Avoid curb cuts, vehicular accesses, surface parking, and loading areas along the Danforth Avenue frontage.
- Provide access to site servicing and parking at the rear of the building/site from a private/public lane or from the side street.
- Incorporate access stairs, garbage collection/storage areas, and loading areas into the rear of the building.
- Provide decorative screenings or planted landscape buffers to reduce the negative impacts of vehicular access, loading, and utilities.



New developments' loading and servicing activities should be avoided from the right-of-way of Danforth Avenue.

5.10 ADDITIONS AND LOW-RISE BUILDINGS

Low-scale additions may be constructed on top of the existing buildings. New additions on top of low-rise buildings that provide additional density with minimal impact on the existing built fabric are permitted and encouraged.

These guidelines will ensure that new additions have minimal impact on the historic main street character of Danforth Avenue.

GUIDELINES

- a. Design new additions and low-rise buildings to maintain and respect the prevailing streetwall height of adjacent properties.
- b. Provide a minimum 3.0 metres stepback above the existing heritage building or prevailing streetwall height for new additions.
- c. For corner properties, provide a minimum 3.0 metres building stepback above the prevailing streetwall height along the north-south side street elevation.
- d. Avoid upper level projections such as cantilevers, balconies and canopies into the building stepback area.
- e. Maximum ground floor height is 4.5 metres and the minimum ground floor height is 3.5 metres.
- f. No windows permitted along the side yard lot lines unless on a corner lot on the street facing frontage.

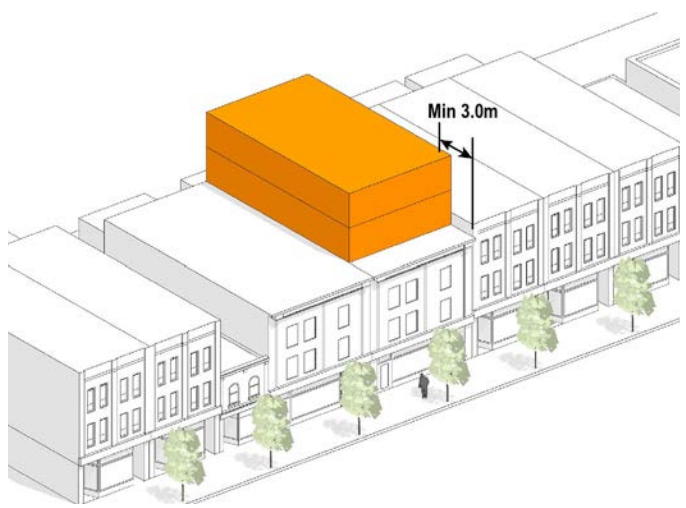


Diagram 14: Any new addition should have a minimum stepback of 3.0 metres from the edge of the prevailing street wall.

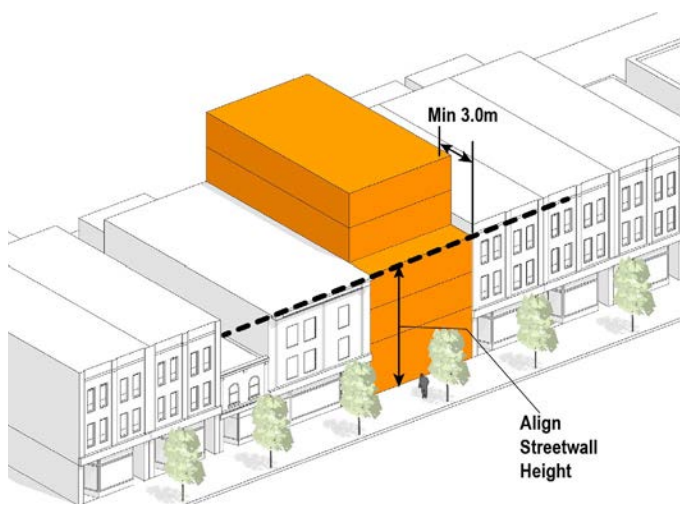


Diagram 15: New low-scale development should maintain and align the prevailing streetwall.

6.0 Mobility

6.1 Destination Danforth Complete Streets Pilot

6.1 DESTINATION DANFORTH COMPLETE STREETS PILOT

The Destination Danforth Complete Streets Pilot was implemented in 2020 to provide support for local businesses, improve safety and comfort for all users, and enable people to use all modes of transportation along the corridor. The pilot included identifying opportunities for expanded patio space on the street right-of-way and on-street cycling facilities along Danforth Avenue.

In December 2021, City Council approved the ActiveTO Cycling Network Expansion projects installed in 2020 currently in place as permanent bikeways, and in doing so, authorized the necessary by-law amendments, to retain them as permanent installations, including Danforth Avenue (cycle tracks from Broadview Avenue to Dawes Road), along with an extension of Victoria Park Avenue to be installed in 2022.

The completion of the Complete Streets Pilot provided an insight on how the public realm expansion on the street right-of-way could succeed without disrupting any modes of transportation. The following guidelines provides further guidance on how future streetscape of the Danforth continues to be a successful complete street.

GUIDELINES

- New developments will, where possible, contribute to the evolution of Danforth Avenue as a complete street by providing improvements to the public realm that will include more seating, landscaping/stormwater retention, bike share station and parking, etc.
- Design all patio zones to ensure adequate pedestrian space, adherence to accessibility requirements, and safe cycling. Patios in the right-of-way must adhere to CafeTO guidelines and process.
- Avoid loading and servicing activities from the right-of-way of Danforth Avenue. If this is not possible, ensure dedicated loading activities do not interfere with pedestrian amenities or public realm improvements.
- Coordinate, where possible, location-specific adjustments (ramps, concrete curb and planter removals) to facilitate accessible pickup and drop off.

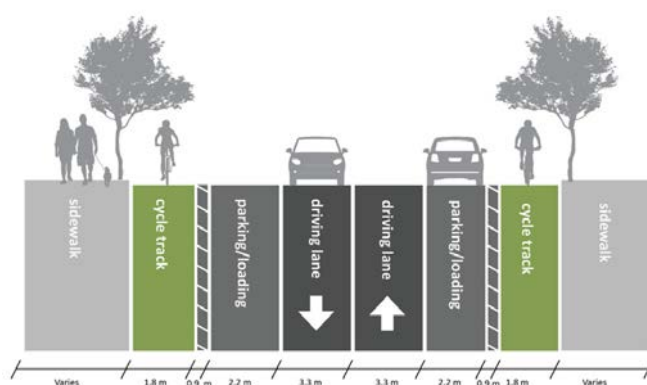


Figure 1: Danforth Avenue Complete Streets Pilot Design Cross Section (without curbside patios)



Figure 2: Danforth Avenue Complete Streets Pilot Design Cross Section (with Curbside Patios)



Installing new bike lanes was an integral part of the Destination Danforth Complete Streets Pilot in 2020.

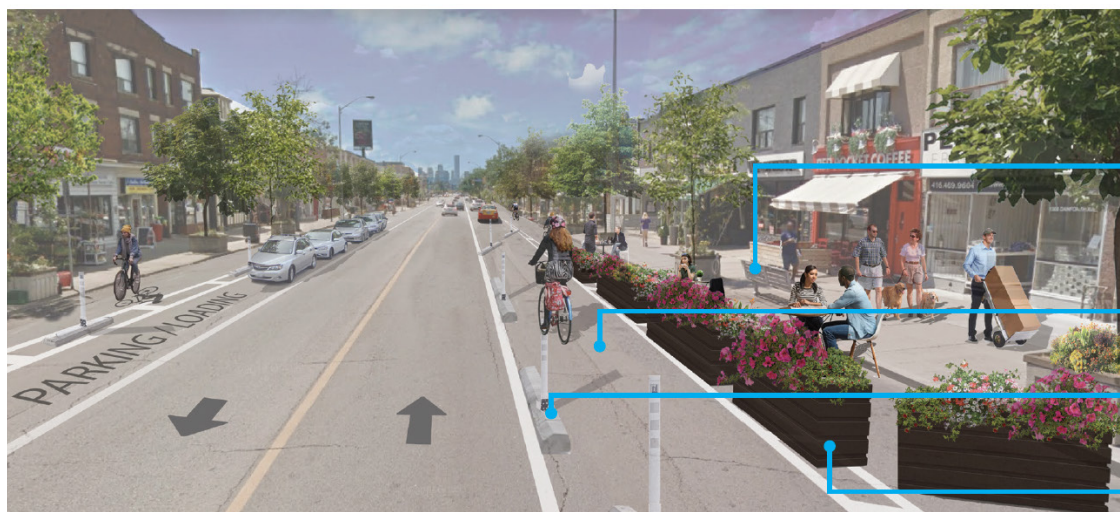


Figure 3: Typical Design with Expanded Curb Side Patio Option - Rendering

Expanded patios to serve food establishments in the curb lane will temporarily replace on-street parking

Cycle Track to deviate around expanded patios providing additional buffer between diners and vehicle lanes

Curbs and posts to protect cyclists from motor vehicles

Planters to define expanded patio areas

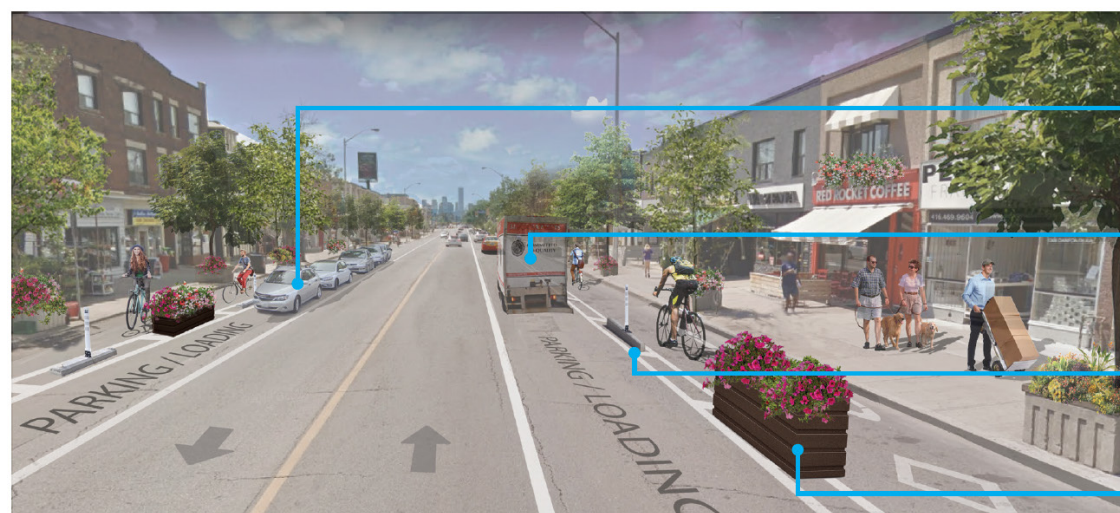


Figure 4: Typical Design with Parking/Loading on Both Sides - Rendering

24/7 Parking Lanes on both sides of the street

Loading opportunities to be provided at key locations

Curb and Posts to improve cycling safety and support cycling for all ages and abilities

Planters to beautify the corridor at key locations

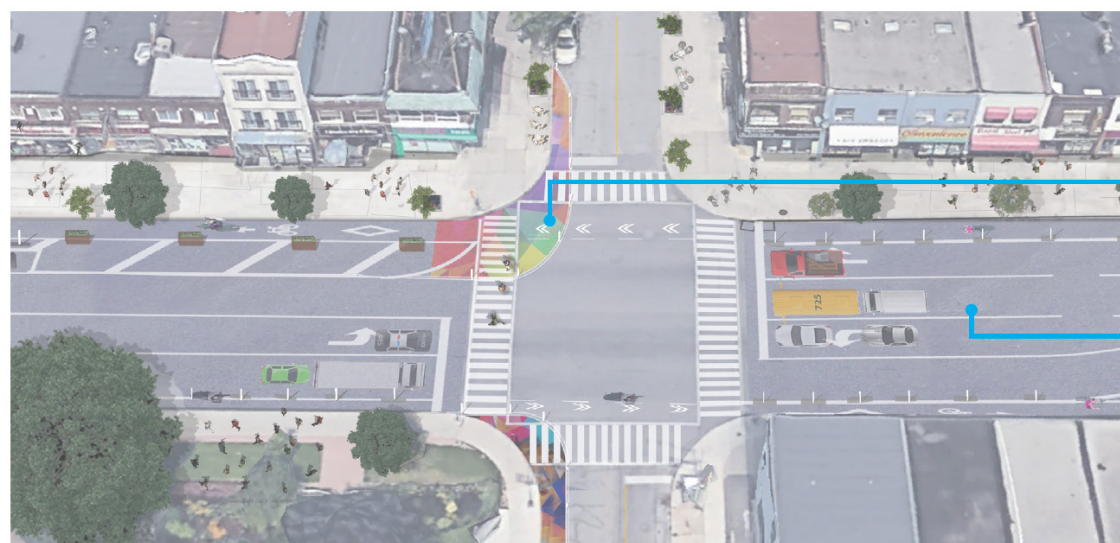


Figure 5: Typical Design with Parking/Loading on Both Sides - Aerial View

Artistic Curb Extensions at key intersections along the corridor

Two through lanes (one in each direction). Additional turn lanes to be provided at each intersection

Appendices

Appendix A

Map 1 - Study Area Map

Appendix B

Map 2 - Heritage Inventory Map

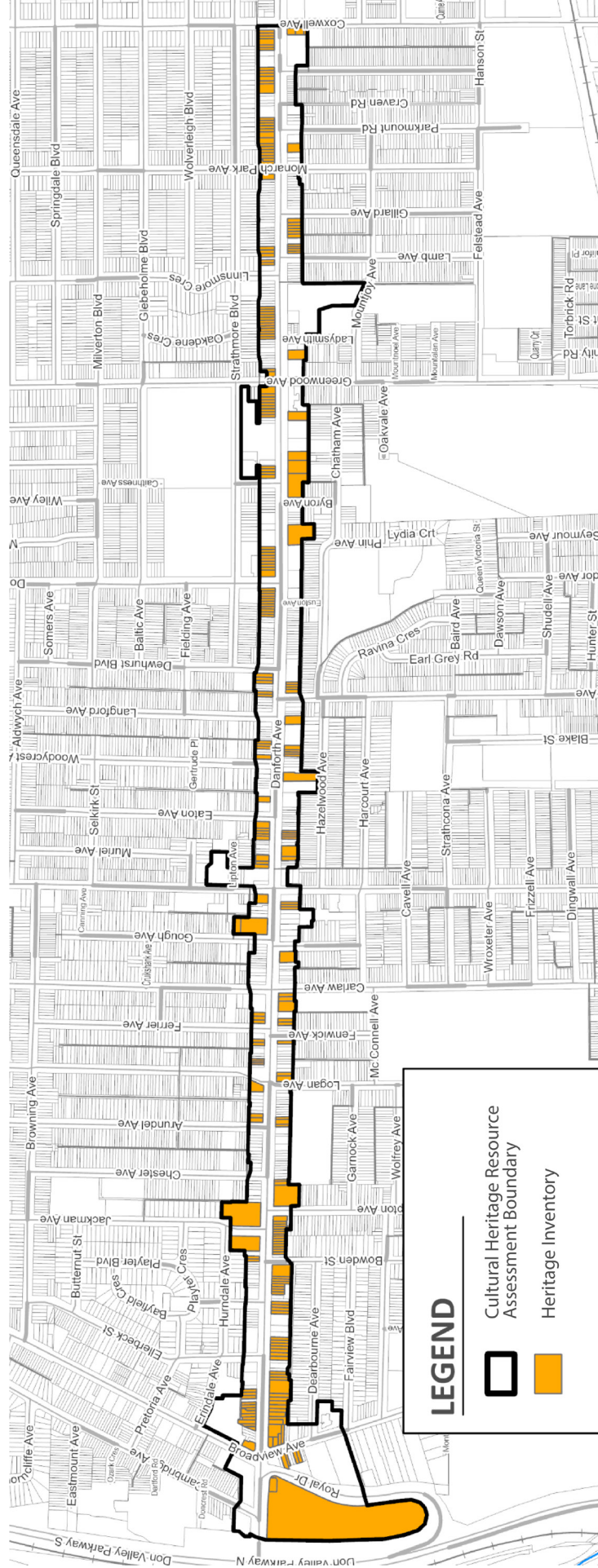
Appendix C

Map 3 - Lot Depth Map

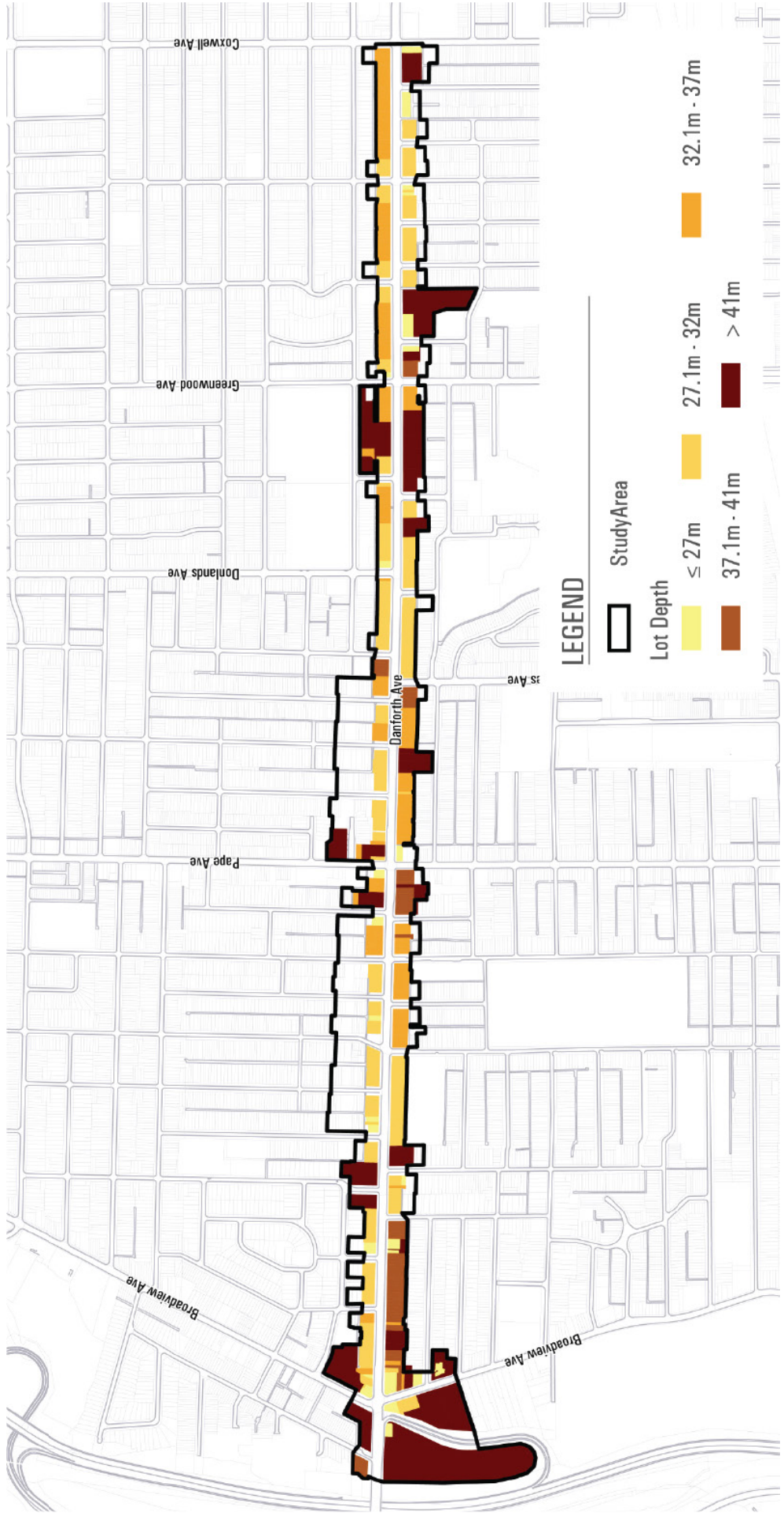
Appendix D

Heritage Inventory

Appendix B Map 2 - Heritage Inventory Map



Appendix C Map 3 - Lot Depth Map



APPENDIX D - HERITAGE INVENTORY

Address	Heritage Status	Date of Construction	Building Type
742 and 744 Broadview Avenue	Heritage Potential	c.1899	House-form with storefront addition
749 and 751 Broadview Avenue	Heritage Potential	c.1909	Main street commercial row
750 Broadview Avenue	Heritage Potential	c.1899	Main street commercial row
752 Broadview Avenue	Heritage Potential	c.1899	Main street commercial row
753 Broadview Avenue	Heritage Potential	c.1911	Main street commercial row
757 Broadview Avenue and 95 Danforth Avenue	Listed	1909	Main street commercial block
1 Danforth Avenue	Heritage Potential	1963	School
55 Danforth Avenue	Part IV	1920-21	Public Lavatory
90 Danforth Avenue	Listed	c.1918	Bank-Landmark
114 and 120 Danforth Avenue	Heritage Potential	c.1919	Main street commercial block
117 Danforth Avenue	Heritage Potential	c.1913	Main street commercial block
119, 123, and 129 Danforth Avenue	Heritage Potential	c.1913	Main street commercial block
124 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
128 Danforth Avenue	Heritage Potential	c.1939	Main street commercial row
131, 135, 139, and 143 Danforth Avenue	Heritage Potential	c.1924	Main street commercial block
132, 136, 140, 146, 148, 156, 162, and 164 Danforth Avenue	Heritage Potential	c.1924	Main street commercial block
147 Danforth Avenue	Listed	1919	Theatre-Landmark
161, 165, and 169 Danforth Avenue	Heritage Potential	c.1922	Main street commercial block
185, 189, and 193 Danforth Avenue	Heritage Potential	1921	Main street commercial block
199, 201, 205, and 209 Danforth Avenue	Heritage Potential	c.1919	Main street commercial block
237 and 241 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
245 and 249 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block

Address	Heritage Status	Date of Construction	Building Type
261, 265, 269, and 273 Danforth Avenue	Heritage Potential	c.1919	Main street commercial block
279, 281, and 283 Danforth Avenue	Heritage Potential	c.1924	Main street commercial block
285 Danforth Avenue	Heritage Potential	c.1911	Place of worship-Landmark
298 and 300 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
309, 311, 315, 319, 323, 327, 333, 335, 337, and 341 Danforth Avenue	Heritage Potential	c.1919	Main street commercial row
310 Danforth Avenue	Heritage Potential	c.1923	Place of worship-Landmark
345, 347, 351, 353, and 355 Danforth Avenue	Heritage Potential	c.1919	Main street commercial block
348 Danforth Avenue	Heritage Potential	1987	Main street commercial block
359 Danforth Avenue	Heritage Potential	c.1910	Place of worship-Landmark
373 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
375 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
439, 443, and 449 Danforth Avenue	Heritage Potential	c.1919	Main street commercial row
440 Danforth Avenue	Heritage Potential	c.1919	Main street commercial block
444 Danforth Avenue	Heritage Potential	c.1919	Main street commercial row
480 Danforth Avenue	Heritage Potential	c.1929	Bank-Landmark
481 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
487 Danforth Avenue	Heritage Potential	c.1919	Main street commercial block
488, 490, and 492 Danforth Avenue	Heritage Potential	c.1909	Main street commercial block
510 and 516 Danforth Avenue	Heritage Potential	c.1919	Main street commercial block
519 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
525 Danforth Avenue	Heritage Potential	c.1919	Main street commercial row
526, 532, 536 Danforth Avenue	Heritage Potential	1913	Main street commercial block
529 Danforth Avenue	Heritage Potential	c.1919	Main street commercial row
541 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block

Address	Heritage Status	Date of Construction	Building Type
551 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
557 Danforth Avenue	Heritage Potential	c.1929	Bank-Landmark
583 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
71 Gough Avenue	Heritage Potential	c.1919	House-form, detached
606 Danforth Avenue	Listed	1914-26	Place of worship-Landmark
639, 641, and 643 Danforth Avenue	Heritage Potential	c.1919	Main street commercial block
646 Danforth Avenue	Listed	1925	Bank-Landmark
674, 676, 680 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
681 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
685 Danforth Avenue	Heritage Potential	c.1938	Post office
702 Danforth Avenue	Heritage Potential	c.1930	House-form with storefront addition
704, 706, 708 Danforth Avenue	Heritage Potential	1922	Main street commercial block
705, 707, 709, and 713 Danforth Avenue	Heritage Potential	c.1919	Main street commercial row
710 Danforth Avenue	Heritage Potential	c.1939	Main street commercial block
744 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
777 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
798 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
801, 803, 805, and 807 Danforth Avenue	Heritage Potential	c.1919	Main street commercial row
810, 812, and 818 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
837 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
855, 859, 861, and 867 Danforth Avenue	Heritage Potential	1923	Main street commercial row
862 Danforth Avenue	Heritage Potential	1922	Main street commercial row
866 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row

Address	Heritage Status	Date of Construction	Building Type
884 and 886 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
888 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
950, 954, 958, and 962 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
966, 972, 974, and 980 Danforth Avenue	Heritage Potential	c.1919	Main street commercial row
982, 988, and 990 Danforth Avenue	Heritage Potential	c.1919	Main street commercial row
5 Donlands Avenue	Heritage Potential	c.1924	Pre-war apartment
1000, 1002, 1008, and 1010 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
1014, 1020, 1022, 1028 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
1015 Danforth Avenue	Heritage Potential	c.1939	Place of worship-Landmark
1095 Danforth Avenue	Heritage Potential	1965	Place of worship-Landmark
1096, 1098, 1104, and 1106 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
1111 Danforth Avenue	Heritage Potential	1930	Funeral Home
1117 Danforth Avenue	Heritage Potential	c.1929	Pre-war apartment
1158, 1160, 1162, and 1164 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
1166, 1170, 1174, and 1180 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
1169 Danforth Avenue	Heritage Potential	c.1929	Pre-war apartment
1182 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
1190 Danforth Avenue	Heritage Potential	c.1924	Bank-Landmark
1194 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
1213 Danforth Avenue	Part IV	1935	Theatre-Landmark
1232, 1236, and 1238 Danforth Avenue	Heritage Potential	c.1918	Main street commercial row

Address	Heritage Status	Date of Construction	Building Type
1242, 1246, 1252, 1254, 1258, and 1260 Danforth Avenue	Heritage Potential	c.1919	Main street commercial row
1262, 1266, and 1268 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
1316 and 1318 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
1328, 1330, and 1336 Danforth Avenue	Heritage Potential	c.1919	Main street commercial row
1331 and 1333 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
1335 and 1337 Danforth Avenue	Heritage Potential	1918	Main street commercial block
1347 Danforth Avenue	Heritage Potential	c.1924	Main street commercial row
1351, 1355, 1359, 1365, and 1367 Danforth Avenue	Heritage Potential	1922	Main street commercial block
1416 Danforth Avenue	Heritage Potential	c.1919	Bank-Landmark
1426 Danforth Avenue	Heritage Potential	c.1922	Main street commercial block
1428, 1430, 1432, 1434, 1436, 1442, 1446, 1450, 1458, 1464, 1468, 1472, and 1474 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
1435 Danforth Avenue	Heritage Potential	c.1939	Pre-war apartment
1506 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
1516, 1520, 1524, 1526, and 1528 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
1550, 1552, and 1556 Danforth Avenue	Heritage Potential	c.1929	Main street commercial row
1562, 1564, and 1568 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
1573 Danforth Avenue	Heritage Potential	c.1929	Main street commercial block
690 and 692 Coxwell Avenue	Heritage Potential	c.1939	Pre-war apartment

