

Metrolinx Transit Expansion Projects – Second Quarter 2022

Date: May 25, 2022

To: Executive Committee

From: General Manager, Transportation Services and Executive Director, Transit
Expansion Division

Wards: All

REASON FOR CONFIDENTIAL INFORMATION

This report deals with a proposed or pending disposition of land by the City of Toronto.

SUMMARY

The Province of Ontario's Subways Program, being delivered by Metrolinx, is a significant investment towards transit expansion within the City of Toronto. The Subways Program consists of the Ontario Line (OL), Scarborough Subway Extension (SSE), Eglinton Crosstown West Extension (ECWE) and Yonge North Subway Extension (YNSE) projects.

Metrolinx continues to advance the delivery of the Subways Program on an accelerated basis. City staff continue to play a crucial role in the planning, design and implementation of the Subways Program. Some of the tasks being done by City staff include: reviewing draft studies and technical drawings; negotiating required legal agreements; participating in commercial procurement process meetings; approving construction and traffic management plans; granting planning approvals; and, advocating and providing input on local interests and concerns.

This report:

- Provides an update on components of the Subways Program that have achieved milestones since staff last reported to City Council in July 2021 and addresses several related City Council directives;
- Seeks approval for temporary road closures to enable OL Lakeshore East Joint Corridor (LSE JC) Early Works, and delegated authority for such closures up to and including 365 days on the OL LSE JC until the end of 2026;
- Seeks approval for temporary long-term traffic lane, sidewalk and multi-use trail closures on Eglinton Avenue West to support construction of the ECWE's elevated guideway and tunnel portals;
- Seeks approval for a temporary road diversion to provide continuity of vehicular and pedestrian connectivity on Eglinton Avenue East around the SSE's Tunnel

Boring Machine extraction shaft site, approval to designate the diversion road as a public highway and implement traffic by-laws for approximately 27 months, upon registration of the City's ownership interest in lands and construction, acceptance, and commissioning of the diversion road;

- Seeks approval to temporarily close Eglinton Avenue East, between Midland and Huntington Avenues, for approximately 27 months to support SSE work;
- Provides information on mitigation measures to reduce construction impacts and Metrolinx's communication to keep communities updated during construction; and,
- Seeks authority to dispose of a parcel of City land to Metrolinx for the SSE and exempt Metrolinx from the City's Fair Wage Policy.

RECOMMENDATIONS

The General Manager, Transportation Services and Executive Director, Transit Expansion Division recommend that:

OL LSE Joint Corridor Early Works

1. City Council approve the temporary bicycle lane closures on Logan Avenue for the purpose of Ontario Line Early Works construction, as per the following details:
 - a. Temporary closure of the northbound and southbound bicycle lanes to bicyclists on Logan Avenue, respectively, between Dundas Street East and a point 150 metres north, from April 1, 2024 to May 31, 2024, inclusive.
 - b. Temporary closure of the northbound bicycle lane to bicyclists on Logan Avenue, between Dundas Street East and a point 150 metres north, from January 1, 2025 to March 31, 2025, inclusive.
 - c. Temporary closure of the southbound bicycle lane to bicyclists on Logan Avenue, between Dundas Street East and a point 150 metres north, from April 1, 2025 to May 31, 2025, inclusive.
 - d. Temporary closure of the northbound and southbound bicycle lanes to bicyclists on Logan Avenue, respectively, between Dundas Street East and a point 150 metres north, from June 1, 2025 to July 31, 2025, inclusive.
2. City Council approve the traffic regulation amendments on Logan Avenue associated with the Ontario Line Early Works construction and authorize the requisite associated amendments to the City of Toronto Municipal Code Chapter 950, Traffic and Parking as per the following details:
 - a. Prohibit stopping of vehicular traffic at all times on the east side of Logan Avenue, between a point 39 metres north of Dundas Street East and a

point 111 metres further north, from October 1, 2023 to July 31, 2025, inclusive.

- b. Prohibit stopping of vehicular traffic at all times on the west side of Logan Avenue, between a point 37 metres north of Dundas Street East and a point 113 metres further north, from October 1, 2023 to July 31, 2025, inclusive.
3. City Council approve the temporary bicycle lane and traffic lane closures on Dundas Street East for the purpose of Ontario Line Early Works construction, as per the following details:
 - a. Temporary closure of the eastbound and westbound dedicated bicycle lanes to bicyclists on Dundas Street East, between Logan Avenue and Wardell Street, from April 1, 2023 to April 30, 2024 and from January 1, 2025 to July 31, 2025, inclusive.
 - b. Temporary closure of the westbound left-turn lane for vehicular traffic on Dundas Street East, from Wardell Street and a point 39 metres east, from April 1, 2023 to April 30, 2024 and from January 1, 2025 to July 31, 2025, inclusive.
 - c. Temporary closure of the eastbound left-turn lane for vehicular traffic on Dundas Street East, from Logan Avenue to a point 27 metres west, from April 1, 2023 to April 30, 2024 and from January 1, 2025 to July 31, 2025, inclusive.
4. City Council approve the traffic regulation amendments on Dundas Street East associated with the Ontario Line Early Works construction and authorize the requisite associated amendments to the City of Toronto Municipal Code Chapter 950, Traffic and Parking as per the following details:
 - a. Prohibit the west bound left-turn movements for vehicular traffic on Dundas Street East at Wardell Street, from April 1, 2023 to April 30, 2024 and from January 1, 2025 to July 31, 2025, inclusive.
 - b. Prohibit the eastbound left-turn movements for vehicular traffic on Dundas Street East at Logan Avenue, from April 1, 2023 to April 30, 2024 and from January 1, 2025 to July 31, 2025, inclusive.
5. City Council approve the temporary lane closures for pedestrians and vehicular traffic on Queen Street East for the purpose of the Ontario Line Early Works construction, as per the following details:
 - a. Temporary closure of the north sidewalk for pedestrians on Queen Street East, between De Grassi Street and McGee Street, from June 1, 2023 to August 31, 2023 and from November 1, 2024 to November 30, 2024, inclusive.

- b. Temporary closure of the westbound curb lane for vehicular traffic on Queen Street East, between De Grassi Street and a point 40 metres west of Empire Avenue, from June 1, 2023 to November 30, 2023 and from November 1, 2024 to February 28, 2025, inclusive.
 - c. Temporary closure of the south sidewalk for pedestrians on Queen Street East, between Strange Street and McGee Street, from April 1, 2023 to May 30, 2023, from December 1, 2023 to December 31, 2023, and March 1, 2025 to March 31, 2025, inclusive.
 - d. Temporary closure of the eastbound curb lane for vehicular traffic on Queen Street East, between Strange Street and McGee Street, from April 1, 2023 to May 31, 2023, December 1, 2023 to April 30, 2024, and March 1, 2025 to June 30, 2025, inclusive.
- 6. City Council approve the traffic regulation amendments on Queen Street East associated with the Ontario Line Early Works and authorize the requisite associated amendments to the City of Toronto Municipal Code Chapter 910, Parking Machines and Metres and Chapter 950, Traffic and Parking as per the following details:
 - a. Rescind the existing parking machine regulation in effect from 9:00 a.m. to 9:00 p.m., Monday to Friday; 8:00 a.m. to 9:00 p.m., Saturday; 1:00 p.m. to 9:00 p.m. Sunday, at a rate of \$2.00 per hour and for a maximum period of three hours, on the north side of Queen Street East, between McGee Street and a point 40 metres west of Empire Avenue, from June 1, 2023 to November 30, 2023 and from November 1, 2024 to February 28, 2025, inclusive.
 - b. Prohibit stopping of vehicular traffic at all times on the north side of Queen Street East, between De Grassi Street and a point 40 metres west of Empire Avenue, from June 1, 2023 to November 30, 2023 and from November 1, 2024 to February 28, 2025, inclusive.
 - c. Rescind the existing stopping prohibition in effect from 4:00 p.m. to 6:00 p.m., Monday to Friday, except public holidays on the south side of Queen Street East, between Strange Street and McGee Street, from April 1, 2023 to May 31, 2023, December 1, 2023 to April 30, 2024, and March 1, 2025 to June 30, 2025, inclusive.
 - d. Prohibit stopping of vehicular traffic at all times on the south side of Queen Street East, between Strange Street and McGee Street, from April 1, 2023 to May 31, 2023, December 1, 2023 to April 30, 2024, and March 1, 2025 to June 30, 2025, inclusive.
- 7. City Council approve the traffic regulation amendments on De Grassi Street associated with the Ontario Line Early Works and authorize the requisite associated amendments to the City of Toronto Municipal Code Chapter 925, Permit Parking as per the following details:

- a. Rescind the existing permit parking regulation in effect from 12:01 a.m. to 10:00 a.m. daily and 4:00 p.m. to 6:00 p.m., Monday to Friday on the odd (east) side of De Grassi Street, from Queen Street East to a point 30 metres further north, from November 1, 2024 to June 30, 2025, inclusive.
8. City Council approve the temporary lane closures on Eastern Avenue for the purpose of the Ontario Line Early Works construction, as per the following details:
 - a. Temporary closure of the westbound curb lane on Eastern Avenue for vehicular traffic, between 10 metres west of Dibble Street and a point 70 metres east of Lewis Street, from November 1, 2022 to October 31, 2026, inclusive.
 - b. Temporary closure of the eastbound curb lane on Eastern Avenue for vehicular traffic, between 50 metres west of Dibble Street and a point 50 metres east of Lewis Street, from November 1, 2022 to October 31, 2026, inclusive.
9. City Council approve the traffic regulation amendments on Eastern Avenue associated with the Ontario Line Early Works construction and authorize the requisite associated amendments to the City of Toronto Municipal Code Chapter 950, Traffic and Parking as per the following details:
 - a. Rescind the existing stopping prohibition in effect from 7:00 a.m. to 9:00 a.m., Monday to Friday, except public holidays on the north side of Eastern Avenue, between 30 metres west of Dibble Street and a point 50 metres east of Lewis Street, from November 1, 2022 to October 31, 2026, inclusive.
 - b. Prohibit stopping of vehicular traffic at all times on the north side of Eastern Avenue, between 30 metres west of Dibble Street and a point 50 metres east of Lewis Street, from November 1, 2022 to October 31, 2026, inclusive.
 - c. Rescind the existing parking prohibition in effect at all times on the north side of Eastern Avenue between 30 metres west of Dibble Street and a point 50 metres east of Lewis Street.
 - d. Rescind the existing stopping prohibition in effect from 4:00 p.m. to 6:00 p.m., Monday to Friday, except public holidays on the south side of Eastern Avenue, between 30 metres west of Dibble Street and a point 50 metres east of Lewis Street, from November 1, 2022 to October 31, 2026, inclusive.
 - e. Rescind the existing parking prohibition in effect at all times on the south side of Eastern Avenue between 30 metres west of Dibble Street and a point 40.3 metres further west.

- f. Prohibit stopping of vehicular traffic at all times on the south side of Eastern Avenue, between 30 metres west of Dibble Street and a point 50 metres east of Lewis Street, from November 1, 2022 to October 31, 2026, inclusive.
10. City Council amend the City of Toronto Municipal Code Chapter 937, Temporary Closing of Highways, to delegate to the General Manager, Transportation Services, until December 31, 2026, inclusive, the authority to temporarily close to pedestrians and vehicular traffic highways or portion of highways for a period up to and including 365 consecutive days but ending no later than December 31, 2026, inclusive, with the exception of those highways listed in Section 937-4 of Chapter 937, as required for the purpose of the construction of the Ontario Line Early Works in the Lakeshore East Joint Corridor and City Council exempt the General Manager, Transportation Services, in carrying out this delegated authority from Section 937-5 of Chapter 937, that being the requirement to notify the local Ward Councillor of the pending closure and the requirement to report on the proposed closure if so requested by the local Ward Councillor.
11. City Council amend the City of Toronto Municipal Code Chapter 27, Council Procedures, to provide that the current delegation to Community Council to temporarily close public lanes or public alleys, local roads, collector roads, and minor arterial roads, and major arterial roads on which the proposal complies with all relevant City by-laws and policies does not include closures delegated to the General Manager, Transportation Services, in carrying out the authority in Part 10 above.
12. City Council direct the General Manager, Transportation Services to request Metrolinx to expand its use of a 24 hour, 7 days a week hotline to accept and track complaints and reports of issues associated with the Ontario Line Early Works in the Lakeshore East Joint Corridor.
13. City Council direct the General Manager, Transportation Services to work with Metrolinx to provide details of long-term road occupancy permits to the public on project websites, consistent with current practices deployed on the Metrolinx Eglinton Crosstown Light Rail Transit, Finch West Light Rail Transit and GO Expansion.
14. City Council approve the temporary closure of the southbound curb lane and adjacent pavement shoulder to vehicular traffic on the Don Valley Parkway, between 150 metres north of the Leaside Bridge and a point 550 metres south of the Leaside bridge, from 11:00 p.m. on Friday, August 5, 2022 to 5:00 a.m. on Monday, August 8, 2022 and from 11:00 p.m. on Friday, August 12, 2022 to 5:00 a.m. on Monday, August 15, 2022 to facilitate the borehole work related to the Metrolinx Ontario Line project.
15. In the event of that the borehole work cannot occur on the dates and times referenced in Part 14 above because of poor weather conditions or other unforeseen circumstances, City Council approve the temporary closure to

vehicular traffic of the southbound curb lane and adjacent pavement shoulder on the Don Valley Parkway, between 150 metres north of the Leaside Bridge and a point 550 metres south of the Leaside bridge, from 11:00 p.m. on Friday, August 19, 2022 to 5:00 a.m. on Monday, August 22, 2022 and from 11:00 p.m. on Friday, August 26, 2022 to 5:00 a.m. on Monday, August 29, 2022 to facilitate the borehole work related to the Metrolinx Ontario Line project.

16. City Council approve the temporary closure to vehicular traffic of the southbound curb lane and adjacent pavement shoulder on the Don Valley Parkway, between 150 metres north of the Leaside Bridge and a point 550 metres south of the Leaside bridge, from 11:00 p.m. on Tuesday, September 13, 2022 to 5:00 a.m. on Wednesday, September 14, 2022, from 11:00 p.m. on Tuesday, October 11, 2022 to 5:00 a.m. on Wednesday, October 12, 2022 and from 11:00 p.m. on Tuesday, November 8, 2022 to 5:00 a.m. on Wednesday, November 9, 2022 to facilitate the water level monitoring work related to the Metrolinx Ontario Line project.
17. City Council authorize the City Solicitor to introduce the necessary bills to give effect to City Council's decision and City Council authorize the City Solicitor to make any necessary clarifications, refinements, minor modifications, technical amendments, or by-law amendments as may be identified by the City Solicitor or General Manager, Transportation Services, in order to give effect to Parts 1 to 16 above.

ECWE Long-Term Road Closures for Elevated Guideway and Tunnel Portal Construction & SSE LS2 Diversion Road

18. City Council authorize installation of traffic control signals at the intersection of Richview Road and Scarlett Road.
19. City Council approve a temporary closure to pedestrian traffic of the north sidewalk on Eglinton Avenue West, between Scarlett Road and a point 450 metres west, from April 1, 2025 to September 30, 2027, inclusive, for the purpose of the Eglinton Crosstown West Extension project.
20. City Council approve a temporary closure to vehicular traffic of the westbound curb and median lanes on Eglinton Avenue West, between Scarlett Road and a point 450 metres west, from April 1, 2025 to September 30, 2027, inclusive, for the purpose of the Eglinton Crosstown West Extension project.
21. City Council approve a temporary closure to pedestrian traffic of the north sidewalk on Eglinton Avenue West, between Scarlett Road and a point 230 metres east, from October 1, 2024 to September 30, 2027, inclusive, for the purpose of the Eglinton Crosstown West Extension project.
22. City Council approve a temporary closure to pedestrian traffic of the north sidewalk on Eglinton Avenue West, between a point 275 metres east of Scarlett Road and Emmett Avenue, from October 1, 2024 to September 30, 2027, inclusive, for the purpose of the Eglinton Crosstown West Extension project.

23. City Council approve a temporary closure to pedestrian traffic of the north sidewalk on Eglinton Avenue West, between Emmett Avenue and Jane Street, from October 1, 2024 to September 30, 2027, inclusive, for the purpose of the Eglinton Crosstown West Extension project.
24. City Council approve a temporary closure to vehicular traffic of the westbound curb lane on Eglinton Avenue West, between Scarlett Road and a point 230 metres east, from October 1, 2024 to September 30, 2027, inclusive, for the purpose of the Eglinton Crosstown West Extension project.
25. City Council designate the northerly westbound lane on Eglinton Avenue West, between Scarlett Road and a point 200 metres east, for westbound right-turns only.
26. City Council approve a temporary closure to pedestrian traffic of the north sidewalk on Eglinton Avenue West, between a point 170 metres west of Pearen Street and a point 230 metres further west, from January 1, 2023 to December 31, 2030, inclusive, for the purpose of the Eglinton Crosstown West Extension project.
27. City Council approve a temporary closure to cycling and pedestrian traffic of the north side multi-use trail on Eglinton Avenue West, between Jane Street and Glenvalley Drive, from January 1, 2023 to December 31, 2030, inclusive, for the purpose of the Eglinton Crosstown West Extension project.
28. City Council approve a temporary closure to vehicular traffic of the westbound curb lane on Eglinton Avenue West, between a point 120 metres east of Jane Street and Weston Road, from January 1, 2023 to December 31, 2030, inclusive, for the purpose of the Eglinton Crosstown West Extension project.
29. City Council approve a temporary closure to pedestrian traffic of the north sidewalk on Eglinton Avenue West, between Weston Road and a point 250 metres east, from January 1, 2023 to December 31, 2030, inclusive, for the purpose of the Eglinton Crosstown West Extension project.
30. City Council approve a temporary closure to cycling and pedestrian traffic of the north side multi-use trail on Eglinton Avenue West, between Weston Road and a point 250 metres east, from January 1, 2023 to December 31, 2030, inclusive, for the purpose of the Eglinton Crosstown West Extension project.
31. City Council approve a temporary closure to vehicular traffic of the westbound curb lane on Eglinton Avenue West, between Weston Road and a point 180 metres east, from January 1, 2023 to December 31, 2030, inclusive, for the purpose of the Eglinton Crosstown West Extension project.
32. City Council approve a temporary closure to vehicular traffic of the eastbound curb lane on Eglinton Avenue West, between a point 30 metres east of Jane

Street and Keelesdale Road, from January 1, 2023 to December 31, 2030, inclusive, for the purpose of the Eglinton Crosstown West Extension project.

33. City Council approve the installation of temporary protected two-way cycle tracks in the eastbound curb lane on Eglinton Avenue West, between a point 30 metres east of Jane Street and Keelesdale Road, from January 1, 2023 to December 31, 2030, inclusive, for the purpose of the Eglinton Crosstown West Extension project.
34. City Council prohibit stopping at all times on the south side of Eglinton Avenue West, between a point 30 metres east of Jane Street and Keelesdale Road, from January 1, 2023 to December 31, 2030, inclusive, for the purpose of the Eglinton Crosstown West Extension project.
35. City Council temporarily close Eglinton Avenue East, between Midland Avenue and Huntington Avenue, to both pedestrians and vehicular traffic from October 1, 2022 to December 31, 2024, inclusive, for purposes of the Scarborough Subway Extension project.
36. City Council authorize public highway designation for the diversion road, between Midland Avenue and Huntington Avenue, as shown in Attachment 1 of the report (May 25, 2022) from the General Manager, Transportation Services and the Executive Director, Transit Expansion Division to become effective upon the registration of the transfer to the City of the required interest in lands forming the diversion road and the registration of the highway dedication by-law on those lands. The diversion road will be known at the Eglinton Avenue East.
37. City Council authorize the appropriate City officials to take all steps necessary to implement the proposed dedication referred to in Part 36 above, including requesting the City Solicitor to prepare and submit the relevant bills and to pay any costs necessary to register the resultant by-laws, if required.
38. City Council authorize the General Manager, Transportation Services, to negotiate, enter into and execute an agreement with Metrolinx on terms and conditions satisfactory to the General Manager, Transportation Services, in relation to, among other things, the construction, maintenance and warranty work of the diversion road; as well as to negotiate, enter into and execute any necessary agreements and documents, and take steps and measures necessary, on behalf of the City, to temporarily exercise the City's authority.
39. City Council designate a 40 km/h speed limit on the Eglinton Avenue East diversion road, between Midland Avenue and Huntington Avenue.
40. City Council prohibit stopping at all times on both sides of the Eglinton Avenue East diversion road, between Midland Avenue and Huntington Avenue.
41. City Council approve a temporary closure to vehicular traffic of Commonwealth Avenue at Eglinton Avenue East, from October 1, 2022 to December 31, 2024, inclusive, for purposes of the Scarborough Subway Extension project.

42. City Council authorize the General Manager, Transportation Services, in issuing any requisite permits to include additional permit terms and conditions as the General Manager of Transportation Services deems necessary and appropriate.
43. City Council authorize the appropriate City officials to submit directly to Council at the appropriate time any necessary bills to amend the appropriate City of Toronto Municipal Code Chapters, and any Schedules to the Chapters, to reinstate the traffic and parking regulations to what they were immediately prior to the by-law amendments made in connection with the report (May 25, 2022) from the General Manager, Transportation Services and the Executive Director, Transit Expansion Division.
44. City Council authorize the appropriate City officials to submit directly to Council at the appropriate time any necessary bills to amend the effective date of any dedication by-law(s) and/or traffic and parking regulation by-law(s) made in connection with the report (May 25, 2022) from the General Manager, Transportation Services and the Executive Director, Transit Expansion Division to, among other things, insert a specific effective date, in order to give effect to Parts 26 to 42, inclusive.
45. City Council authorize the City Solicitor to introduce the necessary bills to give effect to City Council's decision and City Council authorize the City Solicitor to make any necessary clarifications, refinements, minor modifications, technical amendments, or by-law amendments as may be identified by the City Solicitor or General Manager, Transportation Services, in order to give effect to Parts 18 to 44, inclusive, above.

General

46. City Council authorize the Director, Transaction Services, Corporate Real Estate Management to enter into and execute an agreement (the "Agreement") and/or any such ancillary or related agreements, amendments, extensions and renewals as may be necessary with respect to the transfer of the City-owned property at 1269 Danforth Road to Metrolinx at less than fair market value but at the cost the City paid for the property in 2018, plus all additional City costs associated with the expropriation and demolition of the property as outlined in Confidential Attachment 1, all on such terms and conditions as are satisfactory to the Director, Transaction Services, Corporate Real Estate Management and in a form acceptable to the City Solicitor.
47. City Council authorize the public release of Confidential Attachment 1 following the closing of the transaction under the Agreement.
48. City Council authorize the proceeds from the disposition of 1269 Danforth Road be deposited in the Land Acquisition Reserve Fund (XR1012).
49. City Council amend City of Toronto Municipal Code Chapter 67, Fair Wage, Schedule A – Fair Wage Policy, to exempt Metrolinx from the City's Fair Wage

Policy requirements for the following three transit projects: the Subways Program, the Light Rail Transit Program, and the GO Expansion Program.

FINANCIAL IMPACT

The relevant financial information related to 1269 Danforth Road is set out in Confidential Attachment 1 to this report, as it involves a proposed disposition of land by the City of Toronto.

Metrolinx and the Project Co. are responsible for all costs, including payment of fees to the City for the occupancy of the right-of-way.

The Chief Financial Officer and Treasurer has reviewed this report and agrees with the financial impact information.

DECISION HISTORY

City Council's itemized Decision History is provided in Attachment 2.

COMMENTS

1. Subways Program Q2 2021 to Q2 2022 – Status Update

1.1. Ontario Line

Figure 1 shows the overall OL alignment, including the joint OL-GO Corridor.



Figure 1 – OL Alignment

Source: Metrolinx

1.1.1. Project-Wide Milestones

Between Q2 2021 and Q4 2021, Metrolinx published several Early Works reports for components of the Ontario Line (OL), including for Corktown Station¹, Lower Don Bridge – Don Yard², East Harbour³ and the Lakeshore East Joint Corridor⁴. Each site-specific Early Works report outlined local environmental conditions, described and assessed environmental impacts, and detailed measures to mitigate implementation impacts. All required Early Works reports have been published and will support contracts to conduct critical work in advance of the larger packages of work such as the Rolling Stock, Systems, Operations and Maintenance (RSSOM) Contract, Southern Civil Contract and Northern Civil Contract.

On February 7, 2022, Metrolinx published a draft Environmental Impact Assessment (EIA) report evaluating the OL's entire alignment. The draft EIA report was available for public and stakeholder review from February 7 to March 9, 2022. Participants engaged in Metrolinx's EIA-focused Virtual Open Houses were particularly concerned about noise and vibration, business supports, property impacts, environmental conservation and ongoing community engagement. City staff also commented on several aspects of the EIA, including but not limited to: Noise and Vibration, Natural Environment, Traffic and Transportation, Land Use and Socio-Economic, Cultural Heritage and Archaeology. In addition, City staff provided technical advisory comments reiterating the need for

¹ <https://www.metrolinxengage.com/en/content/ontario-line-corktown-early-works-report>

² <https://www.metrolinxengage.com/en/content/ontario-line-early-works-lower-don-bridge-and-don-yard>

³ <https://www.metrolinxengage.com/en/content/ontario-line-early-works-east-harbour-station>

⁴ <https://www.metrolinxengage.com/en/content/ontario-line-early-works-lakeshore-east-joint-corridor>

Metrolinx to uphold commitments, including avoiding or reducing tree and vegetation removals and parkland impacts.

On March 27, 2022, Metrolinx officially broke ground for Early Works at Exhibition Station. Early Works upgrades along the alignment will support major construction, anticipated to begin as part of the RSSOM and Southern Civil Contract packages of work, currently in the Request for Proposal (RFP) stage of procurement.

On April 8, 2022, Metrolinx released the final EIA report⁵ with a description of concerns raised during the review period. The report provided details on Metrolinx's plan to mitigate the concerns raised about the alignment and the impacts to the implementation timeline as a result of these plans. A statement of completion indicating that Metrolinx will proceed with the OL as described in the EIA report has also been published.

Although all EA reports have been finalized and published, the City expects Metrolinx to continue working collaboratively with staff during upcoming detailed design and implementation phases to ensure mitigation measures minimize impact to existing infrastructure, planned capital projects, and the local community while demonstrating consistency with the City's planning framework. Supplementary Metrolinx-led reports that evaluate and consider the impacts of the OL will also be critical to guide the implementation of the project while protecting City infrastructure, assets, and services. City staff will continue to work closely with Metrolinx to provide feedback on the studies and reports that evaluate and consider the impacts of the OL.

1.1.2. Segment-Specific Updates

A) North: Science Centre to Pape South

Maintenance and Storage Facility (MSF)

Between Q2 2021 and Q4 2021, Metrolinx continued to meet with local stakeholders, including business owners, property tenants and community leaders in the Thorncliffe Park community to discuss the MSF proposal, site selection criteria, community impacts as well as tenant and business relocation plans. Engagement included bi-weekly meetings with business tenants affected by the location of the MSF, as well as separate bi-weekly meetings with interested or affected local residents. Metrolinx's conversations with the community focused on impact mitigation, economic impacts and enhanced community consultation strategies.

Through these discussions, Metrolinx has committed to a flexible business, tenant and community organization relocation strategy that adapts to individual requirements and ensures continued and uninterrupted economic activity via financial compensation. Metrolinx has also advised that there is sufficient space for all the planned relocations to be within the Thorncliffe Park community. Metrolinx continues to identify relocation opportunities directly in consultation with businesses, tenants and community organizations, is working to further discuss and finalize property transactions, and mitigate impacts in the area of the MSF.

⁵ <https://www.metrolinxengage.com/en/content/full-report-environmental-impact-assessment-report>

At Virtual Open House on September 16, 2021, Metrolinx presented design objectives for the public walls and landscaping around the MSF. Participants were also informed of plans to establish two working groups that would discuss design principles and priorities for the MSF and areas below the elevated guideway. Metrolinx simultaneously launched a public survey encouraging the Thorncliffe Park and Flemingdon Park residents to share their thoughts on potential design options for the public-facing perimeter walls, fences and landscaping around the MSF, as well as the public spaces below the elevated guideways. Survey submissions have been released and are being reviewed to structure the two working groups, which are anticipated to start in late 2022. The working groups will provide a forum for discussion focused on outstanding community concerns, including: how to ensure the MSF blends into the built environment; how the guideway will be integrated with the streetscape and surrounding businesses; and, how lands required to deliver the transit project can incorporate new community uses.

The City, through its participation in the working groups, will identify opportunities for the mitigation of land use, community and environmental impacts and continually requests that Metrolinx ensure Councillors and community leaders are briefed as the project develops.

B) East Segment: Pape South to East Harbour

Lakeshore East Joint Corridor (LSE JC)

The OL alignment, which includes the Gerrard, Leslieville and East Harbour Stations, will be located within the existing LSE corridor. The LSE corridor currently accommodates the GO and VIA service.

OL LSE JC - Environmental Assessment and Public Engagement

Metrolinx published a draft Joint Corridor Early Works report for public and stakeholder review from September 23 to October 24, 2021. City staff submitted feedback on this report, which focused on impacts, both temporary and permanent, to the adjacent parks and neighbourhoods arising out of the reconstruction of the LSE JC. The final LSE JC Early Works report was provincially approved on December 12, 2021, allowing the project to proceed to construction, which is anticipated to begin in late fall 2022. Early Works in the LSE JC, as well as impacts and mitigation measures are further detailed in City staff report TE32.24⁶.

With the release of the draft Early Works report, Metrolinx hosted two Virtual Open Houses on September 23 and October 5, 2021 to present design options for retaining and noise walls, explain construction sequencing, and highlight the study's key findings. At the September 23, 2021 Virtual Open House, Metrolinx explained that potential new green space would be created adjacent to Jimmie Simpson Park, Bruce Mackey Park, McCleary Playground and the Gerrard-Carlav Parkette after construction. In total, Metrolinx anticipates that it can provide approximately 2,600 square metres of green space lining the parks adjacent to rail corridor.

⁶ <http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2022.TE32.24>

LSE JC - Retaining and Noise Walls

Through MM36.48⁷, Council requested Metrolinx seek the opinion of City staff on the specifications and design of the LSE JC retaining and noise walls and ensure the noise wall specifications are sufficient. Generally, Metrolinx has been engaging with City staff on an on-going basis to discuss anticipated noise and vibration impacts.

Metrolinx has committed to install noise walls on both sides of the LSE JC, with a minimum height of five metres; however, these noise walls will only be one part of their integrated plan to mitigate noise and vibration. Other strategies to manage noise and vibration in the LSE JC may also include procuring quieter OL trains that will run on continuously welded rail with no joints and/or rubber mats placed under the track structures for GO trains. Metrolinx's final solutions will be determined once the Northern Civil Contractor begins detailed design work.

As part of a commitment to ensure that spaces around the corridor reflect local interests and priorities, Metrolinx established a Design Excellence Working Group (DXWG) in January 2022. The DXWG is led by Metrolinx, with support from their Technical Advisors, SvN Architects & Planners and HDR, Inc. Subject matter experts, including City staff from the Transit Expansion Division, City Planning, Parks, Forestry and Recreation, and Transportation Services participate in the DXWG. To date, the DXWG has met on numerous occasions over six months to consider potential design solutions and to address the concerns of citizens in the area. Terms of Reference, meeting summaries, and presentation documents can be found on Metrolinx's website⁸. The DXWG will ultimately provide recommendations to Metrolinx Senior Management for consideration. Through feedback from the stakeholders and City staff involved in the DXWG, Metrolinx will now be hosting a design competition to find a suitable design for the noise walls in the LSE JC.

Other Anticipated LSE JC Impacts

Per MM31.12⁹, City staff were directed to report on impacts to infrastructure, social housing and the overall neighbourhood from shifting tracks to the west side of GO tracks for the proposed 2 kilometre above ground section of the OL.

i) Supportive Housing

Discussions between the City, including the Housing Secretariat, Metrolinx and supportive housing providers, namely Fontbonne Ministries and Sisters of St. Joseph, on solutions to address potential impacts continued throughout 2021 as OL planning advanced. Metrolinx has conveyed details about the activities, impacts and measures that will be implemented to mitigate construction impacts at or in proximity to Fontbonne Place. Mitigation measures discussed have involved white noise machines, sound curtains, vibration pads and, glazed windows. Metrolinx has noted it will also deploy localized and site-specific measures to ensure impact minimization, such as:

⁷ <http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2021.MM36.48>

⁸ <https://www.metrolinxengage.com/en/content/ontario-line-neighbourhood-updates-east-joint-corridor-design-excellence-working-group>

⁹ <http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2021.MM31.12>

- Utilizing construction equipment compliant with noise level specifications listed under the Ministry of Environment, Conservation, and Parks guidelines;
- Operating equipment with effective muffling devices and utilizing additional equipment silencers/mufflers;
- Minimizing simultaneous operation of equipment;
- Staging of construction away from noise sensitive locations where feasible; and,
- Restricting construction hours, including performing construction during daytime where feasible.

Metrolinx has assured the City that it will take the necessary steps to mitigate the impact of noise and vibration to Fontbonne Place residents. Metrolinx has also advised City staff that construction work in proximity to Fontbonne Place will largely be conducted within the Metrolinx-owned LSE JC. However, Metrolinx will acquire the Fontbonne Place parking lot for construction staging and the station itself but has committed to provide alternative parking in the area and a safe pick-up and drop-off area for the residents. Metrolinx will communicate potential, alternative parking solutions and impacts to the City or community in advance of any work. There are no proposed changes to Fontbonne Place's main entrance. Access and drop-off is anticipated to remain consistent with the existing conditions.

ii) Trees and Parks

To prepare for the Early Works planned in the LSE JC, Metrolinx has begun protecting or removing all required trees and vegetation within the Metrolinx-owned rail corridor. Metrolinx will either protect or have to remove trees and vegetation beyond the Metrolinx-owned corridor between Eastern Avenue and Gerrard Street East. Timelines for removing vegetation beyond the Metrolinx-owned corridor are still being determined. Metrolinx has committed to only remove trees required for the work within the construction footprint, and protect any remaining trees using fencing and/or barriers. Other measures Metrolinx will implement to manage the natural environment in the LSE JC include:

- Identification of any species at risk;
- Implementation of erosion and sediment control measures to minimize ecological impacts; and,
- Utilization of T-wall construction to minimize impact as it allows for construction from within the rail corridor.

Additional ecological impacts and associated mitigation measures are identified in the LSE JC Early Works Report. Metrolinx has reiterated that it will follow protocols set out in the Vegetation Guideline (2020)¹⁰ when removing trees and vegetation within the Metrolinx-owned rail corridor, and ones associated with municipal permitting processes and compensation for trees outside of Metrolinx-owned property.

At present, Metrolinx continues to develop a tree inventory and arborist report. The City requires Metrolinx to complete and issue the arborist report and an inventory of all City-

¹⁰ https://www.metrolinxengage.com/sites/default/files/mx_vegguide-final_draft_s001-gen-7761-005_reduced_size.pdf

owned or private trees within six meters of the construction limits to allow for a better understanding of proposed impacts. City staff will continue to discuss opportunities for advanced plantings and will work with Metrolinx to reduce tree removals beyond the Metrolinx-owned property limits.

City divisions will continue to review and provide comments to Metrolinx on Joint Corridor drawings and technical reports through the Transit Expansion Division. Staff will continue to rely on the Metrolinx-led studies and any relevant information that can be shared to assess implications to the City. Comments will continue to focus on how the project impacts the City's Official Plan policies related to Parks and Open Space, Ravine Strategy and Implementation Strategy, TOCore, Reconciliation Action Plan and Strategic Forest Management Plan, among other City strategies.

C) Downtown Segment - Osgoode to Don Yard

Early Works and archeological assessments are currently underway at the First Parliament and future Corktown Station sites. Archeological investigations are intended to uncover any artifacts from Indigenous Nations, remains of Upper Canada's First and Second Parliament Buildings, and remnants of early Toronto industry.

Throughout Early Works, licensed archaeologists will continue to be on site monitoring demolition and removal of at-grade components of structures, in coordination with Indigenous field monitors. Metrolinx will also continue to lead working groups with City staff that will develop comprehensive plans to commemorate the site's historical and cultural significance while ensuring that the development is consistent with Indigenous interests as well as legislative requirements. Through EX31.2¹¹, City Council directed staff to report back on Indigenous consultation undertaken at the First Parliament and Corktown site. Additional information about Metrolinx's working groups, approach to Indigenous engagement and ongoing archaeological monitoring work is detailed in Attachment 3.

Through MM36.19¹² City Council directed staff to request Metrolinx to host a public meeting on the Moss Park Station location and the proposed construction technique. On October 7, 2021¹³, Metrolinx hosted a Virtual Open House¹⁴ which focused on:

- Moss Park Station's site context and location;
- Metrolinx's coordination with the City-led Moss Park Revitalization Project; and,
- Required vegetation removals, compensation and landscape plans.

Participants were particularly interested in project-wide plans and timelines, construction methodology, community engagement and station designs.

D) West Segment Exhibition to Queen-Spadina

On June 3, 2021, a RFP was issued to five pre-qualified proponents by Metrolinx and Infrastructure Ontario (IO) for Exhibition Station's Early Works, and on November 16,

¹¹ <http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2022.EX31.2>

¹² <http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2021.MM36.19>

¹³ <https://www.metrolinxengage.com/en/content/ontario-line-live-downtown-%E2%80%93-october-7-2021>

¹⁴ https://www.metrolinxengage.com/sites/default/files/ol-downtown_voh-oct_2021.pdf

2021, the contract was awarded to Kenaidan Contracting. Early Works construction officially began on March 27, 2022 and include the construction of a new tunnel, entrance building and platform; shifting GO tracks; and, construction of a temporary pedestrian bridge.

1.1.3. OL Next Steps

Metrolinx is targeting the following OL milestones throughout the remainder of 2022 and early 2023:

- Financial Close for Rail Systems, Stations, Operations and Maintenance (RSSOM) and Southern Civil Contract (Summer 2022)
- Commence Early Works construction for the Lower Don Bridges (Summer 2022)
- Commence Early Works construction in the LSE Joint Corridor (Fall 2022)
- Launch Request for Qualification (RFQ) for Northern Civil Contract (Fall 2022)
- Launch Request for Proposals (RFP) for Northern Civil Contract (Summer 2023)
- Commence Advance Utility Works at Queen and Osgoode Stations (Late 2022)
- Commencing RSSOM and Southern Civil Contract work (2023)

A construction start date for the Northern Civil Contract has not been announced. City staff will continue to request a more comprehensive project-wide schedule for the OL from Metrolinx and are committed to reporting back through future reports to City Council as major milestones for project work becomes available.

1.2. Scarborough Subway Extension (SSE)

The SSE extends Line 2 (Bloor-Danforth) by 7.8 kilometres from Kennedy Station to a new terminus at Sheppard Avenue East and McCowan Road. The SSE will run below grade and consists of three stations along McCowan Road at Lawrence Avenue, Bushby Drive and Sheppard Avenue East. Figure 2 illustrates the SSE alignment, station locations and emergency exit buildings.

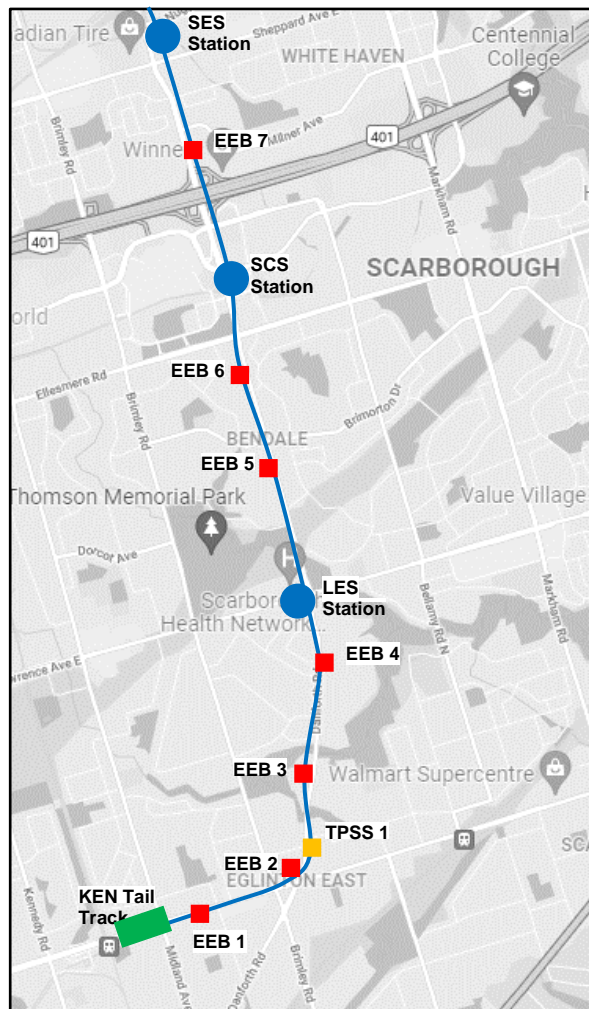


Figure 2 – SSE Alignment and Construction Sites
Source: Metrolinx

1.2.1. Q2 2021 to Q2 2022 Milestones

While planning, design and engineering work continues to progress, Strabag Inc., Metrolinx's SSE Advance Tunnel contractor, officially broke ground on the project in May 2021 at the northeast corner of Sheppard Avenue and McCowan Road. The site also serves as the launch area for the tunnel boring machine (TBM) that will later be converted into the Sheppard-McCowan station site.

On September 17, 2021, Metrolinx and IO released a RFQ to identify teams interested in designing and building the Stations, Rail and Systems package of work. This package includes: three new stations with associated bus terminal facilities; emergency exit buildings; traction power substations; and, SSE integration with TTC infrastructure at Line 2 Kennedy Station, among other activities. A shortlist of qualified proponents were invited to participate in the RFP process on February 9, 2022 and it is expected that Metrolinx and IO will select the final proponent by fall 2022.

1.2.2. Public Consultation and Engagement

Metrolinx hosted two Virtual Open Houses between Q4 2021 and Q1 2022 to provide project updates. Metrolinx held an additional Virtual Open House on March 8, 2022 for businesses in proximity to McCowan and Pitfield Roads specifically. Metrolinx's presentation focused on responding to outstanding community concerns, including ones about headwall construction and mitigation measures. Metrolinx is currently organizing Construction Liaison Committees (CLCs), which will include representation from interested community members, resident groups, businesses and Business Improvement Areas (BIAs) in proximity to each future SSE station location, and the Midland-Kennedy-Eglinton area.

1.2.3. 1269 Danforth Road

In June 2018, the City expropriated a private-property at 1269 Danforth Road for the purpose of building one of the traction power substations on the then TTC-led SSE project. At the time, the property was improved with a free standing retail plaza of approximately 7,400 square feet. The City terminated all the leases and demolished the building. As of September 1, 2019, Metrolinx assumed the responsibility for the construction of the SSE project from TTC. As a result, Metrolinx has requested the transfer of title from the City to Metrolinx.

Metrolinx required access to the property in November 2021 to begin early construction works, and as such the City and Metrolinx entered into a licence agreement that will be terminated upon the transfer of title to Metrolinx. Currently, City staff have approval authority for disposal of City-owned lands to Metrolinx at fair market value. However, in the spirit of partnership, and as this parcel was acquired by the City solely for the purposes of the SSE, City staff are requesting City Council authority to dispose this parcel to Metrolinx at less than fair market value but at the cost the City paid for the property in 2018, plus all costs associated with the expropriation and the cost to demolish the building, as outlined in Confidential Attachment 1.

1.2.4. Next Steps

As SSE design progresses towards 60 per cent completion, Metrolinx will continue to work with City staff and will share updated station designs and layouts, including visuals, with City staff and the community.

1.3 Eglinton Crosstown West Extension (ECEW)

The ECWE will extend Line 5 - Eglinton Crosstown Light Rail Transit (ECLRT) from the existing Mount Dennis Station to Renforth Station, spanning 9.2 kilometres. Figure 3 illustrates the alignment.



Figure 3 – Eglinton Crosstown West Extension

Source: Metrolinx

1.3.1. Q2 2021 to Q2 2022 Milestones

In summer 2021, West End Connectors, the consortium working on the Eglinton Crosstown West Extension's (ECWE) first Advance Tunnel Contract, began launch site preparations at Renforth Drive and Commerce Boulevard.

On December 2, 2021, Metrolinx released a RFQ for the second Advance Tunnel Contract, which will support work on a 500 meter tunnel between Jane Street and the future Mount Dennis ECLRT station, amongst other activities. Metrolinx also released an RFQ for the elevated guideway segment between Scarlett Road and Jane Street on December 10, 2021. The ECWE's elevated guideway will be approximately 1.5 kilometres in length, emerging from the tunnel just east of Jane Street, running north of Eglinton Avenue West, before heading back underground west of Scarlett Road.

On April 13, 2022, Metrolinx also officially broke ground for the ECWE and commenced tunneling on the six kilometer twin tunnels that extend eastward from Renforth Drive towards an extraction shaft at Scarlett Road.

1.3.2. Public Consultation & Engagement

Between Q2 2021 and Q4 2021, Metrolinx hosted five Virtual Open Houses to provide updates on work progressing at the launch shaft site. Other Virtual Open Houses during

this time focused on the preliminary design of the tunnel's east portal, the elevated guideway and pedestrian bridge¹⁵.

At the final Virtual Open House in 2021, Metrolinx provided information about Kipling-Eglinton Station. Metrolinx's presentation provided station entrance location options to participants and focused on responding to outstanding concerns about traffic, station size and design as well as the woodlot Kipling Avenue and Eglinton Avenue West. Participants at the session were interested in the EA process, and actively discussed design refinements for the road realignment and station site, entrance and configuration to minimize ecological impact to the existing woodlot. During the meeting, Metrolinx affirmed there will be compensation for any trees that must be removed. At the March 30, 2022 Virtual Open House, Metrolinx noted that top-rated priorities when it came to Kipling-Eglinton station entrance construction were protecting the woodlot, followed by reducing construction timelines. The Hybrid station entrance option was also found to be the preferred location, with 55 per cent of respondents voting in favour of it.

On April 27, 2022, Metrolinx shared the latest plans for the two elevated stations (Scarlett Road and Jane Street) and the elevated guideway survey results at a Virtual Open House. Updates were also provided about long-term lane closures, the multi-use path/cycle track, and the Fergy Brown Access Road.

1.3.3. Next Steps

In late-May 2022, West End Connectors will begin work at the extraction shaft and portal site, located west of Scarlett Road at Eglinton Avenue. The contractor will also continue headwall construction along Eglinton Avenue at Martin Grove, Kipling, Islington, and Royal York intersections, which is necessary to support future station and emergency exit building work. The second Advance Tunnel Contract and elevated guideway contract are both anticipated to be awarded in 2023. Construction for the remaining scope, namely the Stations, Rail and Systems package of work is expected to begin in 2025-2026.

1.4. Yonge North Subway Extension (YNSE)

The YNSE will extend Line 1 (Yonge-University-Spadina) from Finch Station through to Vaughan, Markham and Richmond Hill. Metrolinx's latest plans propose five stations along an 8 kilometre route. Figure 4 illustrates the alignment.

¹⁵ <https://www.metrolinxengage.com/en/content/eglinton-crosstown-west-extension-live-meetings>

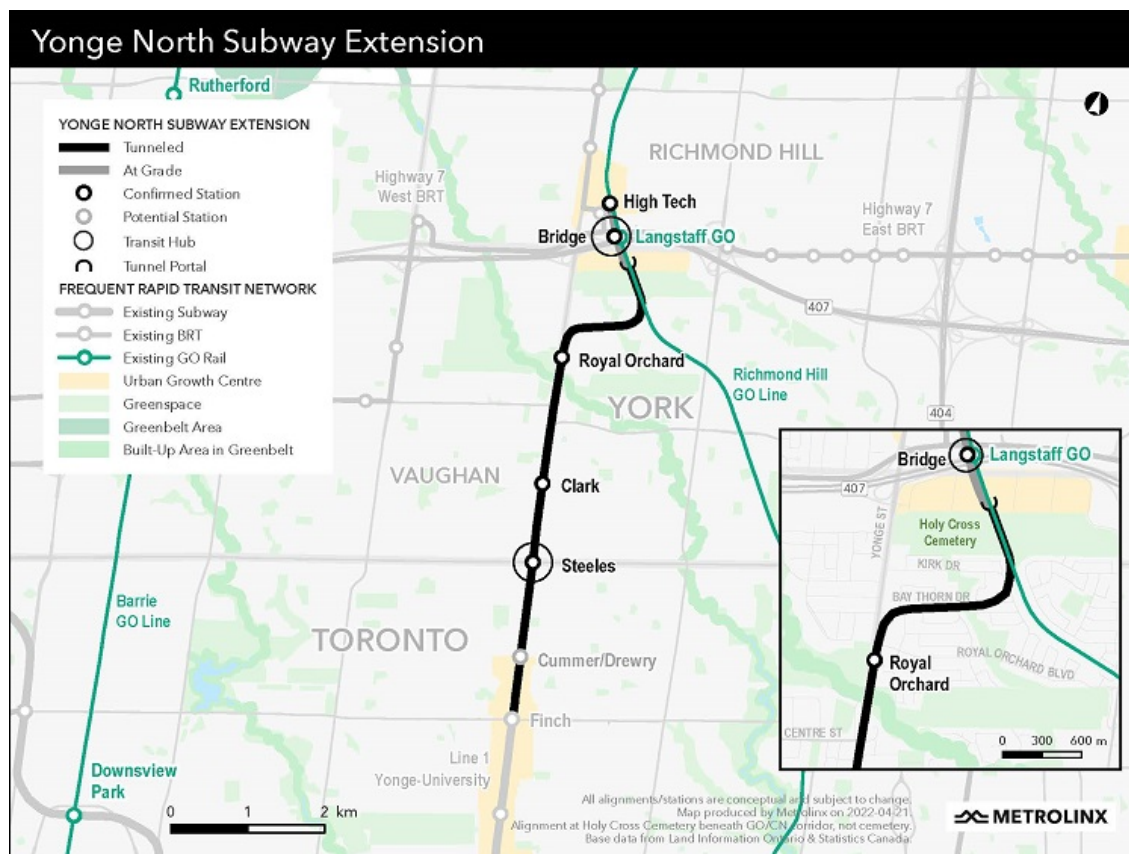


Figure 4 – YNSE Alignment

Source: Metrolinx

1.4.1. Q2 2021 to Q2 2022 Milestones

Between Q2 2021 and Q2 2022, Metrolinx and IO undertook additional work to refine the YNSE's project scope. In December 2021, Metrolinx issued an RFQ for a proponent to conduct Early Works that will upgrade the existing Line 1 Finch Station so that it may connect to the future YNSE, amongst other advanced preparatory activities. On April 22, 2022, Metrolinx announced that three proponents would advance to the RFP stage and that it anticipates identifying the successful Early Works proponent before fall 2022.

On July 16, 2021, Metrolinx released a *Neighbourhood Station Analysis*¹⁶ presenting the benefits and trade-offs of Neighbourhood Stations (i.e., stations located in residential areas that are not expected to grow at the pace of areas surrounding other stations) to inform which fourth station to include in the project along with Steeles, Bridge and High Tech stations. The analysis identified Clark Station as the best performing, followed by Cummer and Royal Orchard stations.

In February 2022, Metrolinx released the YNSE Environmental Project Report (EPR) Addendum¹⁷ for public review. The Addendum updates existing environmental studies, previously completed in 2009 and 2014, using the revised route, stations and

¹⁶ <https://www.metrolinx.com/en/regionalplanning/projectevaluation/benefitscases/YNSE-Neighbourhood-Stations-Analysis-Final.pdf>

¹⁷ <https://www.metrolinxengage.com/en/content/yonge-north-subway-extension-environmental-project-report-overview>

maintenance and storage facility plans. Metrolinx is currently updating the Addendum by adding a description of actions to address public and stakeholder concerns.

In April 2022, the Province of Ontario announced the addition of a fifth station to the YNSE project at Royal Orchard Boulevard in Thornhill. The Province noted that the station's construction funding will be provided by proceeds from the Province's planned transit-oriented community sites at the future Bridge and High Tech stations¹⁸.

1.4.2. Public Consultation and Engagement

In October and December 2021, and on January 5, 2022, Metrolinx hosted Virtual Open Houses to provide the public with information on project benefits, route improvements, the property acquisition process, Early Works at Finch Station, ecological investigations and timelines. During the EPR Addendum's public review period, Metrolinx hosted four focused Virtual Open Houses from February 17 to March 10, 2022¹⁹. Metrolinx's presentations provided participants with an overview of the EPR and a summary of noise and vibration, natural environment, cultural heritage and soil and groundwater findings pertaining to the tunneled and surface segments.

1.4.3. Next Steps

Throughout 2022, Metrolinx plans to provide project briefings to community groups and other stakeholders. Metrolinx is also planning to host a Virtual Open House focused on Finch Station's Early Works before mid-2022. Metrolinx anticipates opening a community office in mid-2022 and expects Finch Station Early Works construction to start in fall 2022. The planned date to begin the main construction on the project is late 2023. Metrolinx will update procurement and construction timelines as they progress through the next phase of planning and design, but remain committed to an in-service date of 2029-2030, after the OL is in service.

2. OL LSE JC Early Works Construction - Temporary Road Closures and Community Impacts

To accommodate the OL tracks, Metrolinx will undertake Early Works to widen the existing LSE rail corridor and realign existing rail tracks between Gerrard Street East and Eastern Avenue. The significant elements of the Early Works construction will include the following:

- Reconstruction of existing GO Bridges at Logan Avenue, Dundas Street East, Queen Street East and Eastern Avenue;
- Construction of new OL Bridges at Logan Avenue, Dundas Street East and Eastern Avenue;
- Grading and drainage on the GO and OL corridor;
- Track realignment and construction between Logan and Eastern Avenues;
- Construction of retaining walls and noise barrier walls along the rail corridor; and
- Utility relocation.

¹⁸ <https://news.ontario.ca/en/release/1002064/ontario-building-40000-more-homes-near-transit-in-york-region>

¹⁹ <https://www.metrolinxengage.com/en/content/yonge-north-subway-extension-live-meetings>

Figure 5 shows the location of bridge works at Logan Avenue, Dundas Street East, Queen Street East and Eastern Avenue. Metrolinx will construct the new OL tracks and bridge at Queen Street East as part of the future OL North Civil Project Co. works.

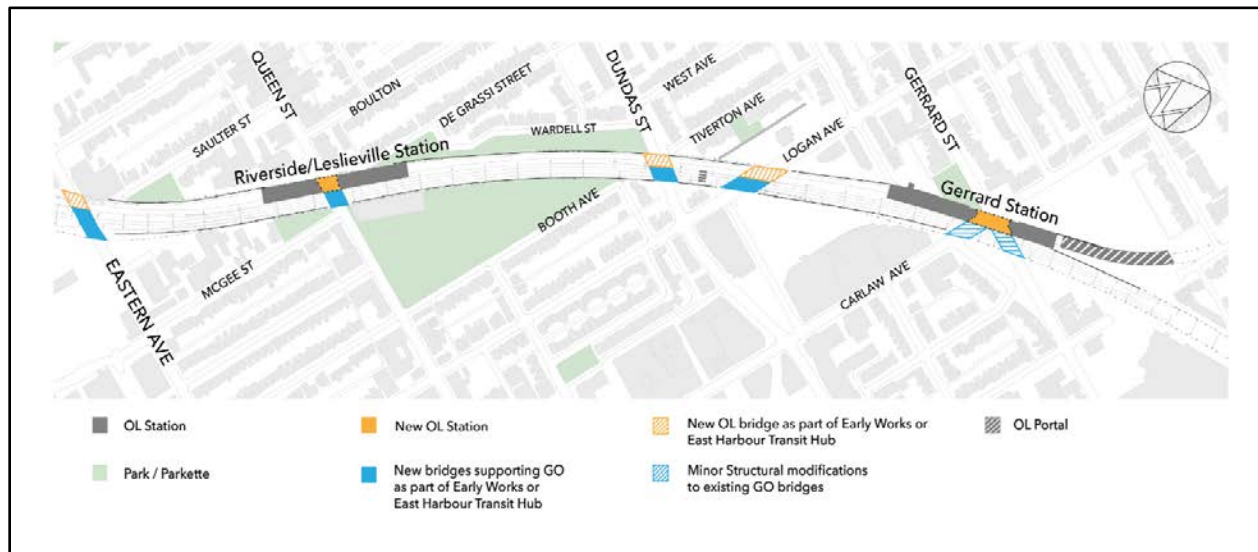


Figure 5 – Bridge Works Locations

Source: Metrolinx

Metrolinx will complete the Early Works construction in the following four phases between February 2023 and April 2026, except for the construction of the Eastern Avenue bridge which will start in November 2022 and end in October 2026:

- Phase 1 - utility relocation, construction of access ramps, reinforcement of existing structural elements;
- Phase 2 - bridge work, track construction, and retaining walls on the south side of the rail corridor;
- Phase 3 - bridge work, track construction and retaining walls on the north side of the rail corridor; and,
- Phase 4 - removal of temporary tracks and access ramps and restoration of sites to pre-construction conditions.

Metrolinx requires temporary road closures to complete the bridge works at Logan Avenue, Dundas Street East, Queen Street East and Eastern Avenue. In addition, Metrolinx requires temporary rail corridor access points from the City roads adjacent to the bridge locations so that trucks with construction material can access the rail corridor. Figures 6 and 7 show the location of rail corridor access points on the south and north sides of the rail corridor, respectively.

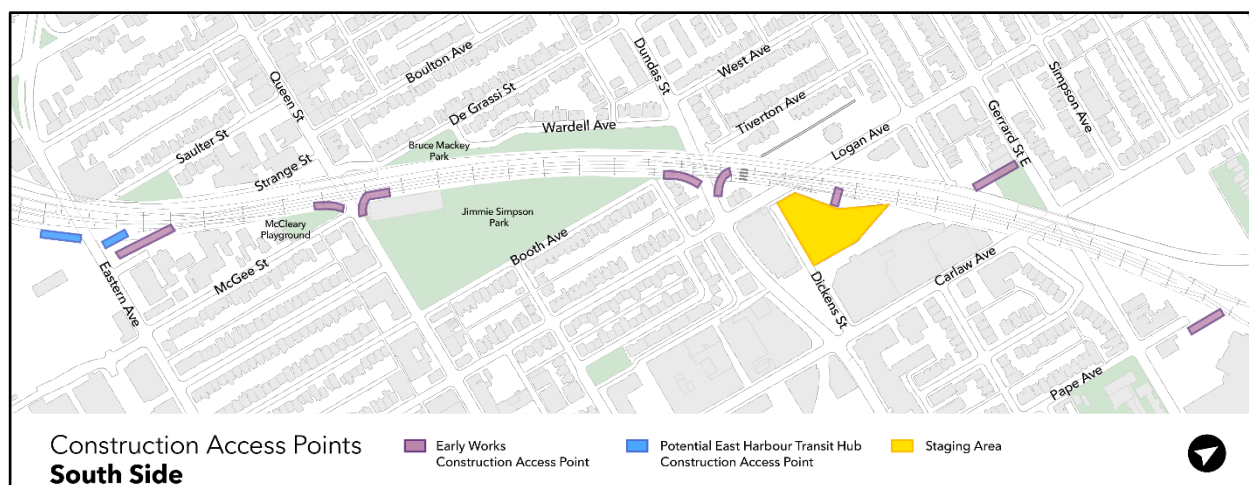


Figure 6 – Rail Corridor Access Points (South Side)

Source: Metrolinx

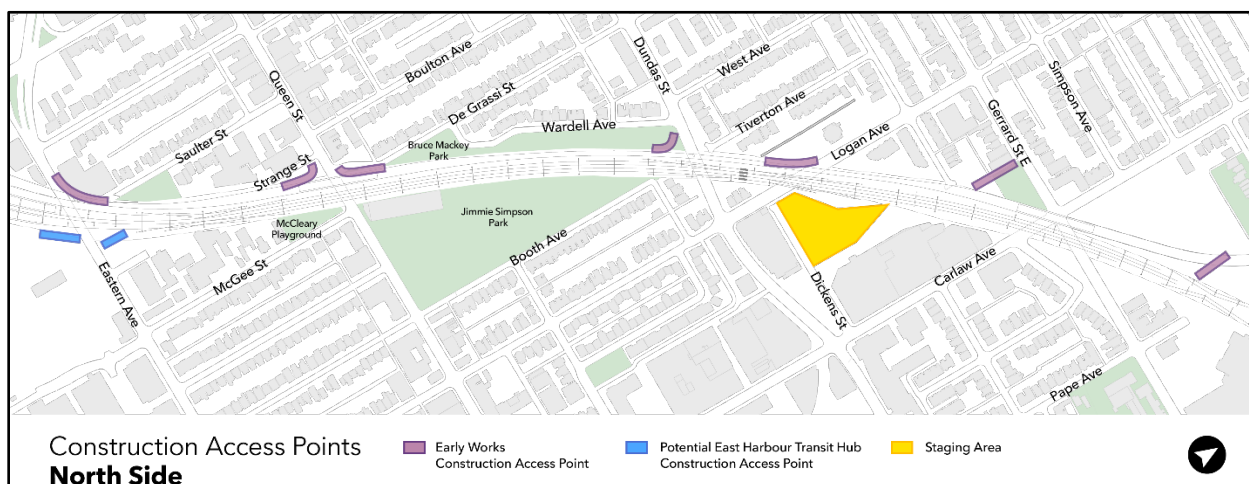


Figure 7 – Rail Corridor Access Points (North Side)

Source: Metrolinx

Metrolinx will require 14 access points (7 on the south side and 7 on the north side) along the rail corridor. Only 8 access points will be utilized at any given time as the work will take place in phases. The access points on the south side will be operational during Phases 1 and 2 of construction, which are planned to take place from April 2023 to August 2024. Additionally, the access points on the north side will be in operation during Phase 3 of construction, which is planned to take place from August 2024 to January 2026. Metrolinx may require two additional temporary driveways at the Eastern Avenue rail bridge to facilitate bridge and station construction as part of the East Harbour Transit Hub development.

2.1. Bridge Works

2.1.1. Logan Avenue

Metrolinx will construct the new GO and OL bridges on Logan Avenue, with a two-span deck supported by columns in the centre of the roadway. The new bridge will be 5 metres high, with construction undertaken in three phases between April 2023 and July 2025.

Phase 1 will take place between April 2023 and September 2023 and involves utility relocation on the east and west sides of the bridge.

Phase 2 will take place between October 2023 and May 2024 and involves work on the southern portion of the bridge. The work includes demolition of the existing bridge abutments, construction of the new bridge abutments on the east and west sides of the bridge, and construction of the centre support column. Metrolinx will complete work in the rail corridor from June 2024 to December 2024 and there will be no road or sidewalk closures at the Logan Avenue Bridge.

Phase 3 will take place between January 2025 and July 2025 and involves work on the northern portion of the bridge. The work includes demolition of the existing bridge abutments, construction of the new bridge abutments on the east and west sides of the bridge, and construction of the centre support column.

Figures 8 and 9 show the typical traffic management setup during the bridge abutment and centre support column works.

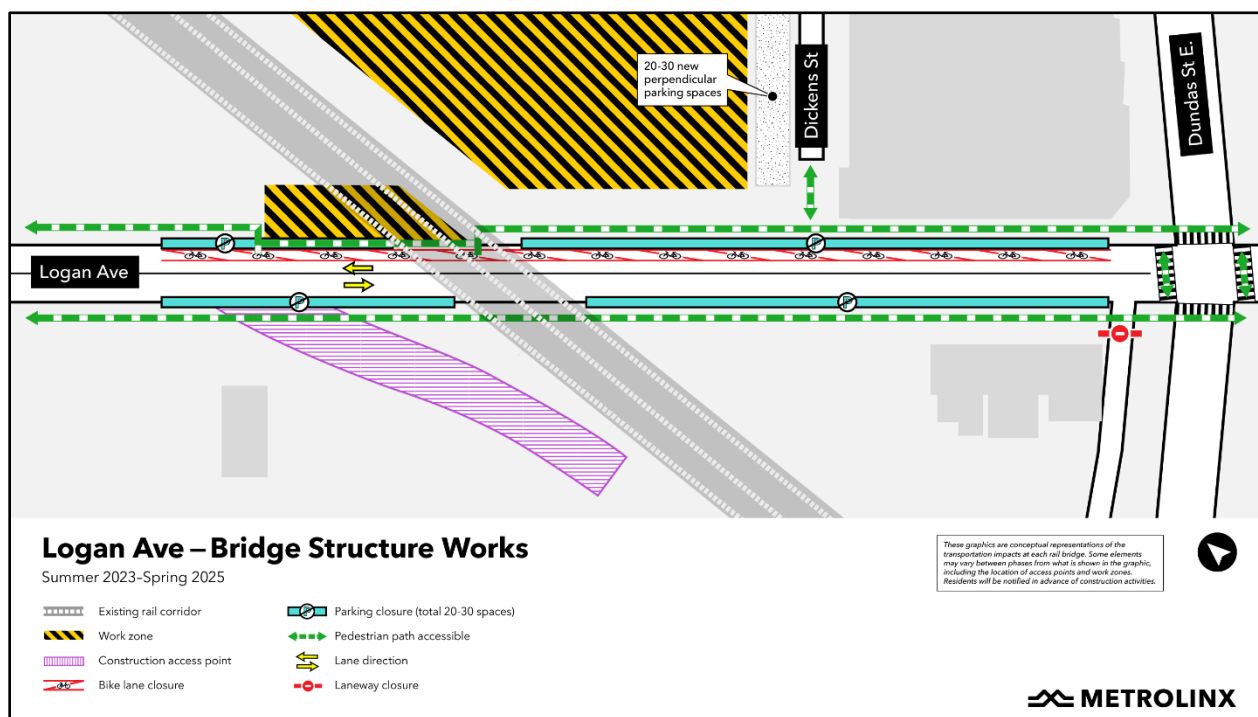


Figure 8 – Logan Avenue Traffic Management during Bridge Abutment Works Source: Metrolinx

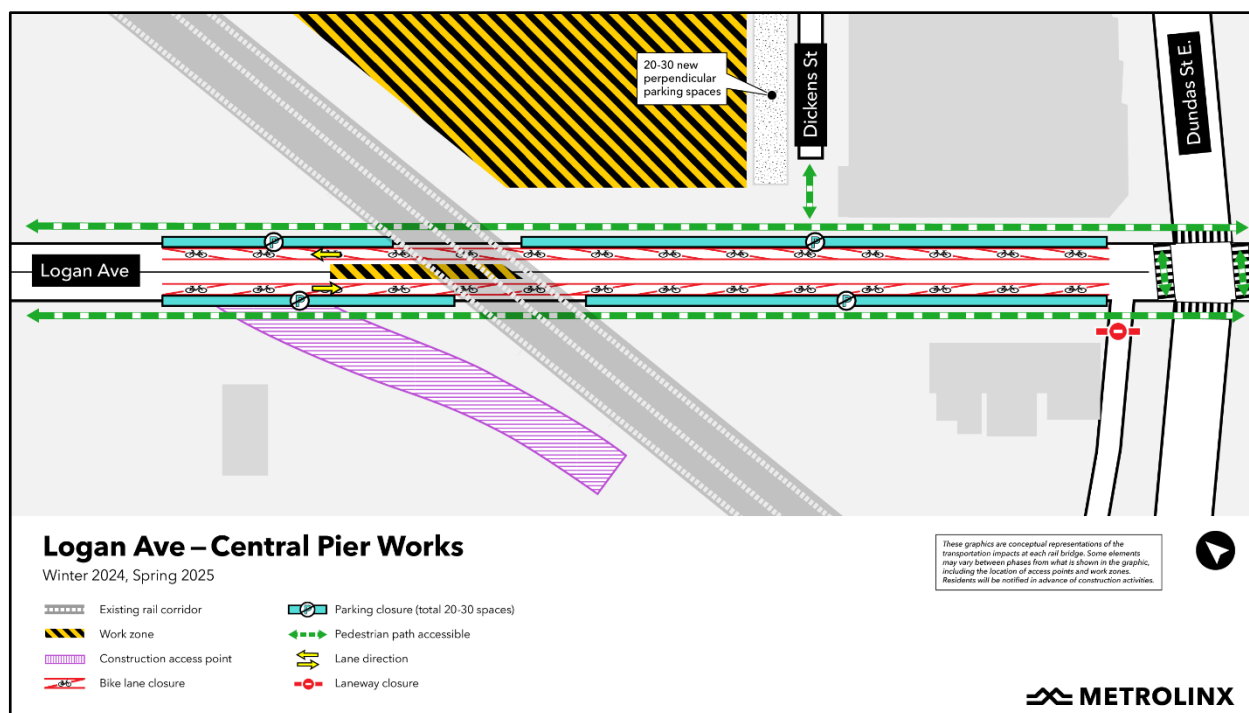


Figure 9 – Logan Avenue Traffic Management during Bridge Centre Support Column Works Source: Metrolinx

Metrolinx requires the following temporary long-term lane closures on Logan Avenue to complete the bridge works.

- Closure of the northbound and southbound bicycle lanes on the east and west sides of Logan Avenue, respectively, between Dundas Street East and a point 150 metres north, from April 1, 2024 to May 31, 2024.
- Closure of the northbound bicycle lane on the east side of Logan Avenue, between Dundas Street East and a point 150 metres north, from January 1, 2025 to March 31, 2025.
- Closure of the southbound bicycle lane on the west side of Logan Avenue, between Dundas Street East and a point 150 metres north, from April 1 2025 to May 31, 2025.
- Closure of the northbound and southbound bicycle lanes on the east and west sides of Logan Avenue, respectively, between Dundas Street East and a point 150 metres north, from June 1, 2025 to July 31, 2025.

The above listed long-term temporary closures and associated traffic by-law amendments will require City Council approval. The traffic impacts are discussed below:

Auto Access Impacts

Metrolinx will maintain a 3.3 metre wide northbound and southbound traffic lane on Logan Avenue during all phases of work involving utility relocations and bridge structure works.

During the east side utility and bridge works, Metrolinx will maintain two-way traffic on Logan Avenue by shifting the traffic lanes on the west side of the bridge. Similarly, during the west side works, Metrolinx will shift the traffic lanes on the east side of the bridge to complete the west side bridge works. During work on the centre support columns, traffic lanes will be shifted outwards towards the support structures of the bridge.

As a result of traffic lane shifts during the bridge works, Metrolinx will require the temporary removal of up to 28 on-street 3-hour maximum parking space on the east and west sides of Logan Avenue at the rail bridge. Metrolinx is currently working with the City to accommodate up to 25 parking spaces on Dickens Street.

The bridge deck work will require up to 10 short-term full road closures on Logan Avenue, between First Avenue and Dundas Street East. The full closures will take place between 8 p.m. on Fridays and 6 a.m. on Mondays. Based on the current schedule, three weekend full road closures will be required in June-July 2023, two weekend closures in February-March 2024, three weekend closures in October-November 2024, and two weekend closures in May-June 2025.

Pedestrian Impacts

Metrolinx will provide a minimum 1.8 metre wide protected and covered pedestrian walkways on both sides of Logan Avenue during the bridge works. For pedestrian safety, Metrolinx will keep the temporary walkways free of any debris, loose material, snow, ice, and will ensure proper work zone set-up with adequate advisory signage.

During the short-term weekend full road closures, pedestrians will be directed to detour around the closure via West Avenue, First Avenue and Dundas Street East. Advanced advisory signage will be strategically installed to inform pedestrian of the closures.

Cycling Impacts

Metrolinx requires periodic closures of bicycle lanes on Logan Avenue at the rail bridge in one or both directions for a cumulative total of nine months. During the closure of bicycle lanes, cyclists will be directed to share travel lanes with general traffic. Metrolinx, through its Early Works contractor, will install shared lane pavement markings and "Share the Road" advisory signage along with advance signage, strategically located for maximum visibility, to provide guidance to road users as they approach the work zone. During the short-term full road closures, cyclists will be detoured on Carlaw Avenue via Gerrard Street East and Dundas Street East.

Transit Impacts

There is currently no transit service on the subject section of Logan Avenue.

Emergency Vehicle Impact

There are no temporary road closures that would require emergency vehicles to detour from the area, except during weekend closures when advance notice would be provided

to ensure emergency services are fully aware of the temporary closure. During weekend closures, vehicles will be instructed to detour around the closure via alternate corridors.

Business Access Impact

Metrolinx will work with City staff, including the Transit Expansion and Economic Development and Culture Divisions to monitor impacts to local businesses. However, Metrolinx has confirmed that it will maintain all business and property accesses during the bridge works.

2.1.2. Dundas Street East

Metrolinx will construct the new GO and OL bridges on Dundas Street East, with a two-span deck supported by columns in the centre of the roadway. The new bridge height will be 5 metres, with construction undertaken in three phases between April 2023 and July 2025.

Phase 1 work will involve utility relocation under the bridge. Metrolinx plans to undertake these works between April 2023 and July 2023 with phased construction on the south and north sides of the bridge.

Phase 2 work will involve demolition of the existing bridge support structures (abutments and centre support columns) and construction of the new bridge support structures on the east side of the bridge between August 2023 and April 2024.

In between May 2024 and December 2024, Metrolinx will work in the rail corridor and there will be no road closures under the Dundas Street East rail bridge.

Phase 3 work will involve demolition of the existing bridge support structures (abutments and centre support columns) and construction of the new bridge support structures on the west side of the bridge between January 2025 and July 2025.

Figures 10 and 11 show the typical traffic management setup during the bridge abutment and centre support column works at the Dundas Street rail bridge.

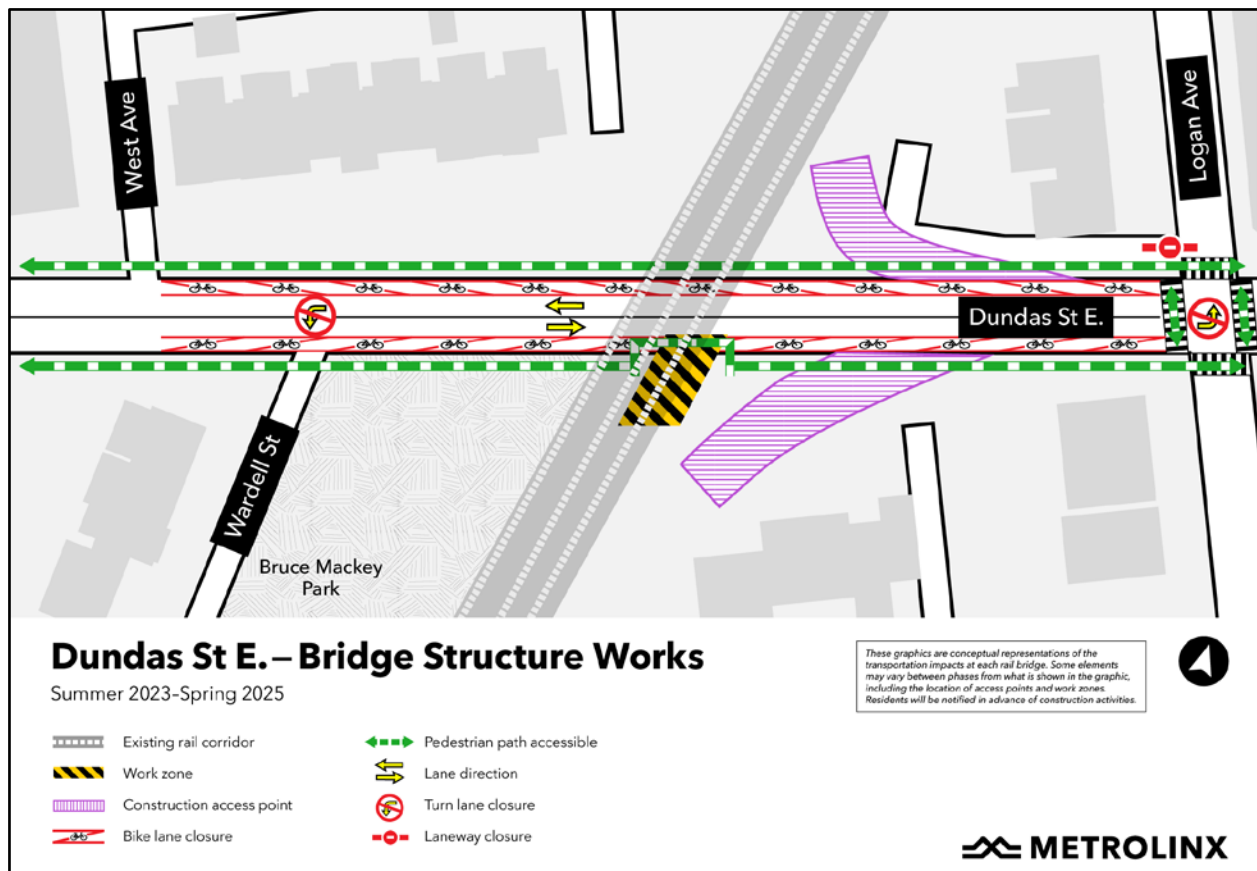


Figure 10 – Dundas Street Traffic Management during Bridge Abutment Works

Source: Metrolinx

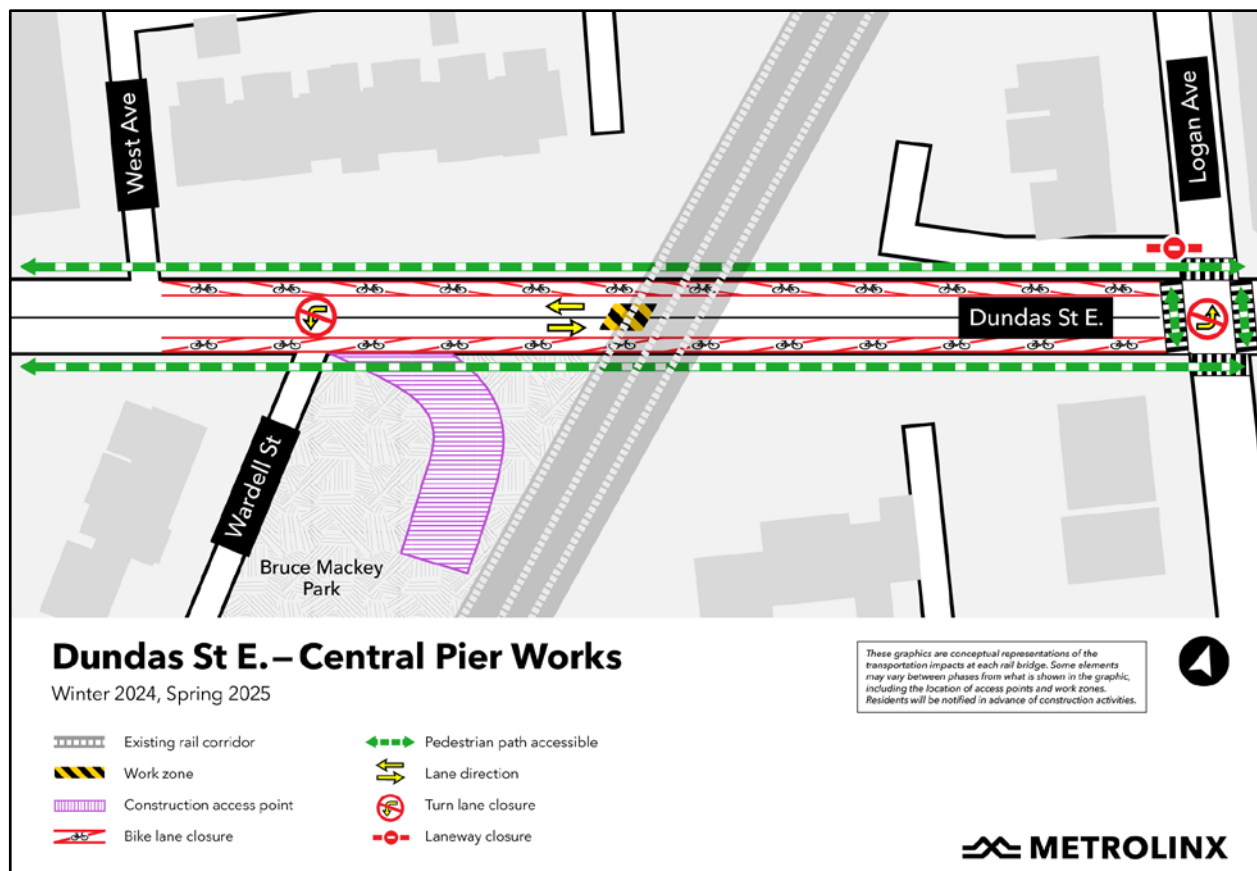


Figure 11 – Dundas Street Traffic Management during Bridge Centre Column Works

Source: Metrolinx

Metrolinx requires the following temporary long-term lane closures on Dundas Street East to complete the bridge works.

- Closure of the eastbound and westbound dedicated bicycle lanes on Dundas Street East, between Logan Avenue and Wardell Street, from April 1, 2023 to April 30, 2024 and from January 1, 2025 to July 31, 2025.
- Closure of the westbound left-turn lane and prohibition of the westbound left-turn movement on Dundas Street East at Wardell Street, from April 1, 2023 to April 30, 2024 and from January 1, 2025 to July 31, 2025.
- Closure of the eastbound left-turn lane and prohibition of the eastbound left-turn movement on Dundas Street East at Logan Avenue, from April 1, 2023 to April 30, 2024 and from January 1, 2025 to July 31, 2025.

The above listed long-term temporary closures and associated traffic by-law amendments will require City Council approval. The traffic impacts are discussed below:

Auto Access Impacts

Metrolinx will maintain a 3.4 metres wide eastbound and westbound traffic lane on Dundas Street East during all phases of work involving utility relocation and bridge support structures.

During the south side bridge work, the eastbound traffic lane on Dundas Street East, between Wardell Street and Logan Avenue, will be shifted and maintained on the north side of the bridge. Similarly, during the north side bridge work, the westbound traffic lane on Dundas Street East, between Logan Avenue and Wardell Street will be shifted and maintained on the south side of the bridge. Additionally, during the bridge central support column works, the traffic lanes on Dundas Street, between Wardell Street and Logan Avenue, will be shifted outward towards the support structures of the bridge.

As a result of traffic lane shifts, Metrolinx will require a temporary closure of the westbound left-turn lane and movement on Dundas Street East at Wardell Street. Additionally, Metrolinx will also require a temporary closure of the eastbound left-turn lane and movement on Dundas Street East at Logan Avenue.

The bridge deck work will require up to eight short-term full road closures on Dundas Street East, between Wardell Street and Logan Avenue. Vehicular traffic will be diverted to alternative parallel routes during these road closures. The full closures will take place between 8 p.m. on Fridays and 6 a.m. on Mondays. Metrolinx has informed that up to maximum two full road closures will be scheduled in May-June 2023, February-March 2024, January-February 2025, and May-June 2025. The short-term full road closures on Dundas Street East will be scheduled so as not to coincide with short-term full road closures on Queen Street East and Eastern Avenue.

There is no existing on-street parking in the vicinity of the Dundas Street East Bridge. Additionally, no loading zones will be impacted.

Pedestrian Impacts

Metrolinx will provide a minimum 1.8 metre wide pedestrian walkways on both sides of Dundas Street East during the bridge works. During the short-term full road closures, pedestrians will be directed to detour via West Avenue, First Avenue and Logan Avenue. Metrolinx will install advanced advisory signage to inform pedestrians of the temporary full road closure. To ensure safety of pedestrians, Metrolinx will keep the temporary walkways free of any debris, loose material, snow and ice and will ensure proper work zone set-up with adequate advisory signage.

Cycling Impacts

Bicycle lanes on Dundas Street East, between Logan Avenue and Wardell Street, will be closed from April 2023 to April 2024 and from January 2025 to July 2025. The cycling lanes will be closed to maintain sidewalks on both sides of Dundas Street East. During bicycle lanes closure, cyclists on this section of Dundas Street East will share the available traffic lanes with the general traffic. Metrolinx through its Early Works contractor will install shared lane pavement markings and "Share the Road" advisory signage along with advance signage, strategically located for maximum visibility, to provide guidance to road users as they approach the work zone.

Transit Impacts

There is currently no transit service on the subject section of Dundas Street East.

Emergency Vehicle Impact

There are no temporary road closures that would require emergency vehicles to detour from the area, except during weekend closures when advance notification would be provided. During weekend closures, vehicles will be instructed to detour around the closure via alternate corridors.

Business Access Impact

All business accesses will be maintained during construction.

2.1.3. Queen Street East

Metrolinx will reconstruct the existing GO Bridge on Queen Street East to accommodate the realigned GO tracks. The new GO Bridge will include a single-span deck with no supports in the centre of the roadway. Additionally, the new bridge height will be 5 metres and will be constructed as part of the future North Project Co. P3 contract in three phases between April 2023 and June 2025.

Phase 1 work will involve utility relocation under the bridge. Metrolinx plans to undertake these works between April 2023 and July 2023 with phased construction on the south and north sides of the bridge.

Phase 2 work will involve demolition of the east side of the bridge (abutments, bridge support columns and deck) and reconstruction of the new bridge structures (abutments and deck) between August 2023 and April 2024.

In between May 2024 and October 2024, Metrolinx will work in the rail corridor and there will be no lane or sidewalk closures under the Queen Street rail bridge.

Phase 3 work will involve demolition of the west side of the bridge (abutments, bridge support columns and deck) and reconstruction of the new bridge structures (abutments and deck) between November 2024 and June 2025.

Figures 12 and 13 show the typical traffic management setup during the bridge structure works at the Queen Street East rail bridge.

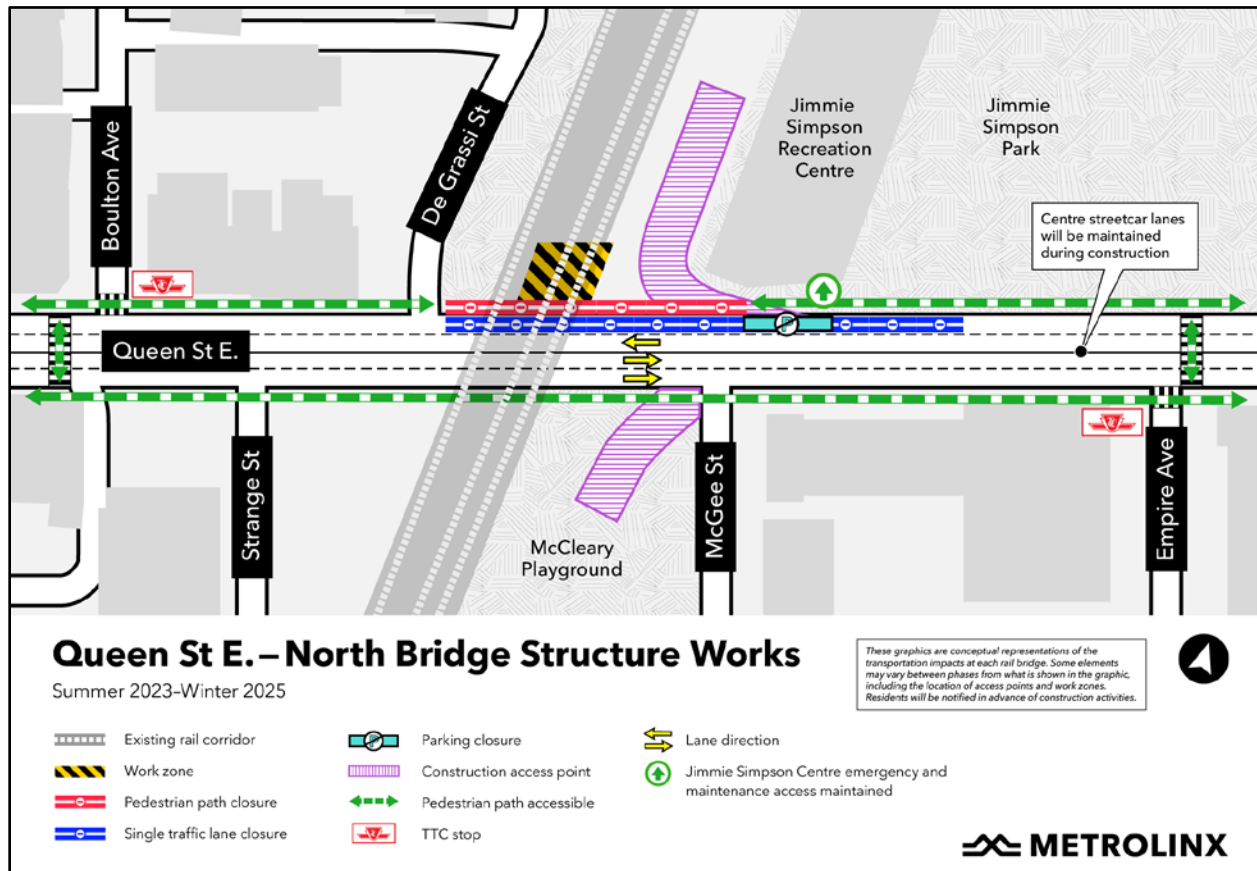


Figure 12 – Queen Street Traffic Management during North Side Bridge Structure Works Source: Metrolinx

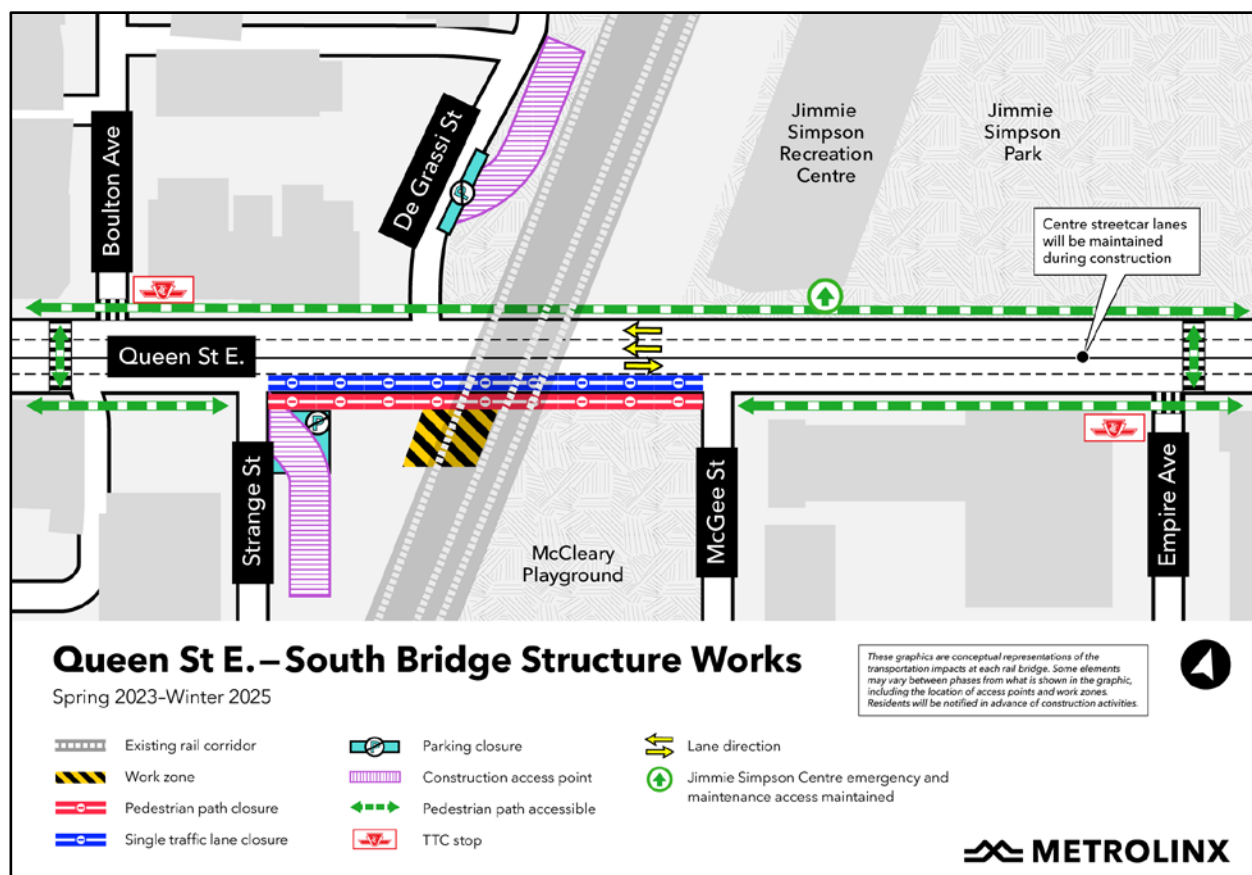


Figure 13 – Queen Street Traffic Management during South Side Bridge Structure Works Source: Metrolinx

Metrolinx requires the following temporary long-term lane closures on Queen Street East to complete the bridge works.

- Closure of the north sidewalk on Queen Street East, between De Grassi Street and McGee Street, from June 1, 2023 to August 31, 2023 and from November 1, 2024 to November 30, 2024.
- Closure of the westbound curb lane on Queen Street East, between De Grassi Street and a point 40 metres west of Empire Avenue, from June 1, 2023 to November 30, 2023 and from November 1, 2024 to February 28, 2025.
- Closure of the south sidewalk on Queen Street East, between Strange Street and McGee Street, from April 1, 2023 to May 30, 2023, from December 1, 2023 to December 31, 2023, and March 1, 2025 to March 31, 2025.
- Closure of the eastbound curb lane on Queen Street East, between Strange Street and McGee Street, from April 1, 2023 to May 31, 2023, December 1, 2023 to April 30, 2024, and March 1, 2025 to June 30, 2025.

The above listed long-term temporary closures and associated traffic by-law amendments will require City Council approval. The traffic impacts are discussed below:

Auto Access Impacts

As shown in Figure 12, Metrolinx will require a temporary closure of the westbound curb lane on Queen Street East, between a point 40 metres west of Empire Avenue and De Grassi Street, for approximately 10 months to complete the north side bridge construction. With the temporary westbound curb lane closure in place, Queen Street East in the vicinity of the rail bridge will operate as one westbound and two eastbound lanes. Additionally, the temporary closure of the westbound curb lane will result in temporary removal of two on-street "Pay and Display" parking spaces on the north side of Queen Street East in front of the Jimmie Simpson Community Centre. Access to Jimmie Simpson Community Centre loading area will be maintained.

Similarly, Figure 13 shows a temporary closure of the eastbound curb lane on Queen Street East, between Strange Street and McGee Street, for approximately 11 months to complete the south side bridge construction. With the eastbound curb lane closure in place, Queen Street East in the vicinity of the rail bridge will operate as one eastbound and two westbound lanes.

In addition to the temporary curb lane closures, Metrolinx will require up to 10 short-term weekend full road closures to facilitate bridge deck work. The full closures will take place between 8 p.m. on Fridays and 6 a.m. on Mondays. Vehicular traffic will be diverted to alternative parallel routes during these road closures. Based on the latest schedule provided by Metrolinx, up to 10 full closures will be spread throughout the construction with two full road closures scheduled in July-August 2023, October 2023, May-June 2024, September-October 2024, April-June 2025. The short-term full road closures on Queen Street East will be scheduled to not coincide with the short-term full road closures on Dundas Street East and Eastern Avenue.

Pedestrian Impacts

Metrolinx will require alternating sidewalk closures on Queen Street East at the rail bridge to facilitate utility relocations and bridge support demolition work.

During the north side works, Metrolinx will require the north sidewalk closure on Queen Street East, between McGee Street and De Grassi Street, for approximately four months. Pedestrians will be directed to the south sidewalk at the nearest signalized pedestrian crossings at Boulton Avenue and Empire Avenue, located approximately 70 metres to the west and 100 metres to the east of the bridge, respectively.

Similarly, during the south side works, Metrolinx will require the south sidewalk closure on Queen Street East, between Strange Street and McGee Street, for approximately four months. Pedestrians will be directed to the north sidewalk at the nearest signalized crossings at Boulton Avenue and Empire Avenue.

After the utility relocation and bridge support demolition work completion, a temporary protected and covered pedestrian walkway with a minimum 1.8 metres width will be maintained on both sides of Queen Street East. To ensure safety of pedestrians, Metrolinx will keep the temporary walkways free of any debris, loose material, snow and ice and will ensure proper work zone set-up with adequate advisory signage.

During the short-term full road closures, pedestrians will be directed to detour around the closure via Broadview Avenue, Eastern Avenue, and McGee Street. Advanced advisory signage will be strategically installed to inform pedestrians of the temporary closure.

Cycling Impacts

There are currently no dedicated bicycle lanes on Queen Street East. Cyclists on Queen Street East will continue to share the available travel lanes with the general traffic during the bridge works.

Transit Impacts

As part of the Metrolinx work in the vicinity of the Queen Street East rail bridge, streetcar service will need to be temporarily replaced by buses to facilitate the permanent relocation of aerial traction power cables to underground, construction of new duct bank and chambers, and permanent pole relocations to facilitate the bridge works. Based on the information provided by Metrolinx, TTC bus replacement service on Queen Street East will be provided for up to 7 months to facilitate completion of the bridge works and streetcar related civil works. Additionally, during the short-term full road closures, TTC 501 Queen streetcar service will be unavailable.

This work will be coordinated with other TTC and City capital works including track replacement, watermain replacement, and short-term full road closures. TTC and Metrolinx will coordinate on providing the bus bridging and informing the travelling public in advance. The existing transit stops on Boulton Avenue and Empire Avenue will be maintained.

Emergency Vehicle Impact

Except for the short-term temporary full road closures, the temporary curb lane closures on Queen Street East at the rail bridge will not require detour of emergency vehicles.

Business Access Impact

Metrolinx will work with City staff, including the Transit Expansion and Economic Development and Culture Divisions to monitor impacts to local businesses. However, Metrolinx has confirmed that it will maintain all business and property accesses during the bridge works.

2.1.4. Eastern Avenue

The Eastern Avenue bridge works are part of the East Harbour Transit Hub project and will include reconstruction of the existing GO Bridge and reconstruction of the new OL Bridge. Metrolinx will remove the existing three-span GO Bridge and replace it with a 5 metres high two-span Bridge. Metrolinx will also construct a separate OL Bridge on Eastern Avenue.

The bridge works will take place in three phases between November 2022 and June 2026. The utility relocation work is planned between May 2022 and November 2022.

Phase 1 work will take place between November 2022 and October 2023 and involve bridge works on the south side of the GO Bridge.

Phase 2 will take place between November 2023 and June 2025 and involve bridge works on the north side of the GO Bridge.

Phase 3 will take place between July 2025 and October 2026 and involve bridge works on the OL Bridge and street-level improvements.

Figures 14 and 15 show the typical traffic management setup during the bridge structure works at the Eastern Avenue rail bridge.

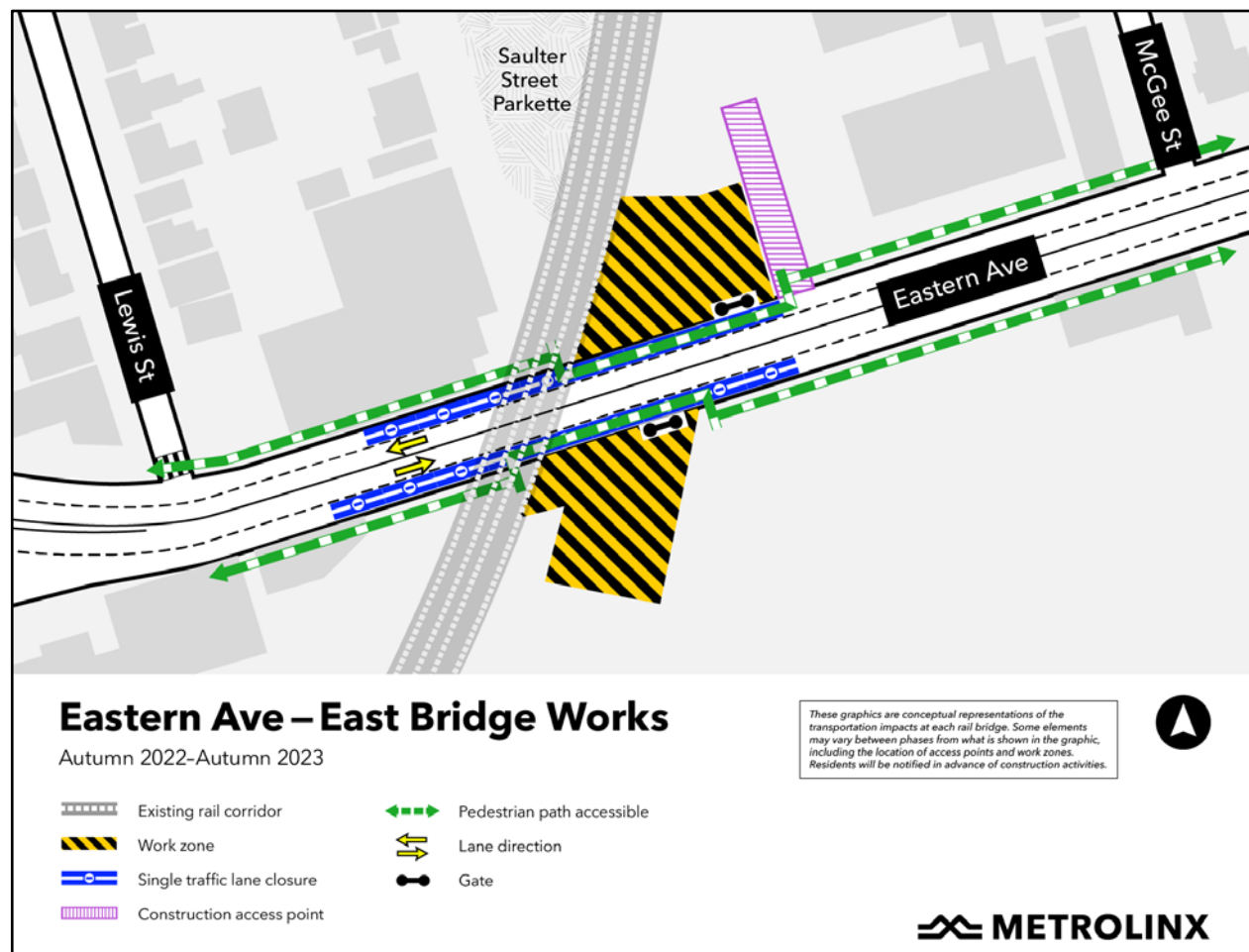


Figure 14 – Eastern Avenue Traffic Management during East Side Bridge Structure Works Source: Metrolinx

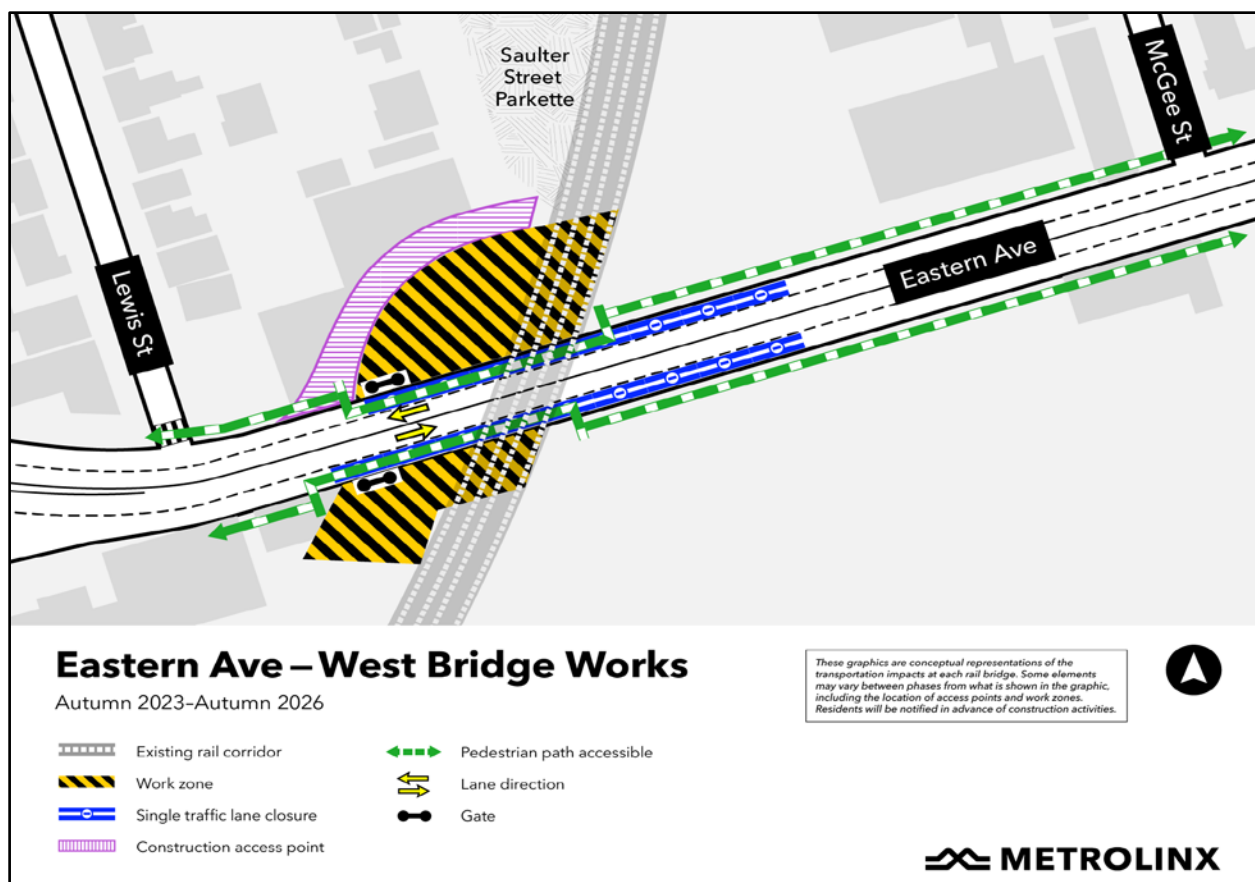


Figure 15 – Eastern Avenue Traffic Management during West Side Bridge Structure Works Source: Metrolinx

Metrolinx requires the following temporary long-term lane closures on Eastern Avenue to complete the bridge works.

- Closure of the westbound curb lane on Eastern Avenue, between 10 metres west of Dibble Street and a point 70 metres east of Lewis Street, from November 1, 2022 to October 31, 2026.
- Closure of the eastbound curb lane on Eastern Avenue, between 50 metres west of Dibble Street and a point 50 metres east of Lewis Street, from November 1, 2022 to October 31, 2026.

The above listed long-term temporary road closures and associated traffic by-law amendments will require City Council approval. The traffic impacts are discussed below:

Auto Access Impacts

Metrolinx will require temporary closure of the eastbound and westbound curb lanes on Eastern Avenue at the rail bridge to facilitate the bridge works. The curb side lane closures will be utilized to accommodate temporary pedestrian walkways in the construction zone.

In addition to the curb lane closures, Metrolinx will require up to 13 short-term weekend full road closures to facilitate bridge deck work. The timing of the short-term full road closures on Eastern Avenue will be confirmed as the design progresses and construction planning is confirmed. The weekend closures will take place between 8 p.m. on Fridays and 6 a.m. on Mondays. Vehicular traffic will be diverted to alternative parallel routes during these road closures. The short-term temporary full road closures on Eastern Avenue will be scheduled to not coincide with the short-term full road closures on Queen Street East and Dundas Street East and other adjacent roadways.

There is no existing on-street parking in the vicinity of the Eastern Avenue railway bridge. No loading zones will be impacted.

Pedestrian Impacts

During the construction of Eastern Avenue, covered temporary pedestrian walkways will be provided on both sides of Eastern Avenue to maintain pedestrian access in the vicinity. The curbside lanes in both directions will be used to accommodate a minimum 1.8 metres wide temporary covered and protected walkways to provide pedestrian access within construction zone.

During short-term full road closures, pedestrian detours will be designated, likely on Queen Street East via Broadview Avenue and Carlaw Avenue. To ensure safety of pedestrians, Metrolinx will keep the temporary walkways free of any debris, loose material, snow and ice and will ensure proper work zone set-up with adequate advisory signage.

Cycling Impacts

There are currently no dedicated bike lanes on Eastern Avenue. Cyclists will continue to share the travel lanes with the general traffic.

Transit Impacts

There is no existing transit service on Eastern Avenue that will be affected by construction.

Emergency Vehicle Impact

There are no road closures that would require emergency vehicles to detour from the area, except during the weekend full road closures. During the weekend full road closures, vehicles will be instructed to detour around the closure via alternate routes.

Business Access Impact

Metrolinx will work with City staff, including the Transit Expansion and Economic Development and Culture Divisions to monitor impacts to local businesses. However, Metrolinx has confirmed that it will maintain all business access during construction.

The following sections of the report discuss the rail corridor access points and their impacts on the adjacent roadways, including on-street parking and community.

2.2. Rail Corridor Access Points and Impacts

Metrolinx will require temporary driveways from the city roads next to the rail corridor to complete the Early Works in the rail corridor. The temporary driveways will be established on both sides of the rail corridor as the construction vehicles cannot cross the rail tracks due to frequency of the GO trains. Metrolinx contractor will use these driveways to facilitate the following:

- Movement of construction equipment including cranes, dozers, excavators, motor graders, water trucks, drilling rigs, concrete pump, front end loaders, compactors, etc.
- Delivery vehicles required to dispose excavated material, import granular material, concrete, pre-cast wall units (6 metres tall), noise barrier wall posts (10 metres long), panels, rail (over 20 metres long), concrete ties, utility ties, etc.
- Emergency equipment including accident recovery machines, fire trucks, EMS, etc.

As mentioned earlier in the report and shown in Figures 6 and 7, Metrolinx requires 14 temporary driveways from the city roads next to the rail corridor to complete the Early Works in the rail corridor. Metrolinx may require two additional temporary driveways at the Eastern Avenue rail bridge to facilitate bridge and station construction as part of the East Harbour Transit Hub development.

At any given phase of construction, only 8 driveways will be operational. These driveways will generally be 5 metres wide with 2 metre wide shoulders. The ramps to the rail corridor will be engineered with shoring and protected to avoid conflicts with pedestrians and other road users.

Based on the information provided by Metrolinx, driveway construction is expected to take approximately two to three weeks, during which disruptions to adjacent sidewalks may be experienced. Metrolinx will provide temporary protected pedestrian walkways during construction of the driveways.

The proposed temporary driveway and the retaining wall construction in the vicinity of the Logan Avenue rail bridge will result in narrowing of the public laneway located in the north-west corner of the intersection of Dundas Street West and Logan Avenue. The public laneway narrowing will permanently restrict the vehicular access to private parking spaces for the residences along the west side of Logan Avenue at Dundas Street East. Metrolinx will engage with the impacted property owners regarding the lost parking spaces and the arrangement of alternate parking. Additionally, permanent closure of the public laneway and the required Council approval will be discussed in a future staff report.

Additionally, the wide truck turning movements at the proposed temporary driveway on the east side of De Grassi Street at Queen Street East will impact the CaféTO location

on the west side of De Grassi Street, between 2024 and 2025. The truck turning movement at the De Grassi Street driveway will also impact two on-street permit parking spaces.

2.2.1. Haul Routes

During the operation of the temporary driveways, Metrolinx expects up to 5 trucks per hour at each operational driveway. Metrolinx will establish a dedicated staging area just north of Dickens Street to facilitate construction work in the LSE JC. To reduce queuing and idling on the City roads, construction trucks with material will wait in this staging area and proceed to different access points when required. Additionally, the temporary driveways will have sufficient space to accommodate trucks waiting for a gap to complete their outbound turning movement onto the adjacent City roadway.

No truck movements on routes with nearby schools will take place during school opening (8 a.m. to 9 a.m.) and closing time (3 p.m. to 4 p.m.) to ensure that potential conflicts with pedestrians, school children and any school bound traffic are avoided.

Furthermore, Metrolinx will provide several trained traffic control persons at each of the temporary driveway to guide road users, especially pedestrians and construction vehicles, and avoid any conflicts. Additionally, Metrolinx will ensure forward-in and forward-out truck movements with proper sightlines of the road users, including pedestrians.

Figures 16, 17 and 18 show the potential haul routes to various access points along the rail corridor.

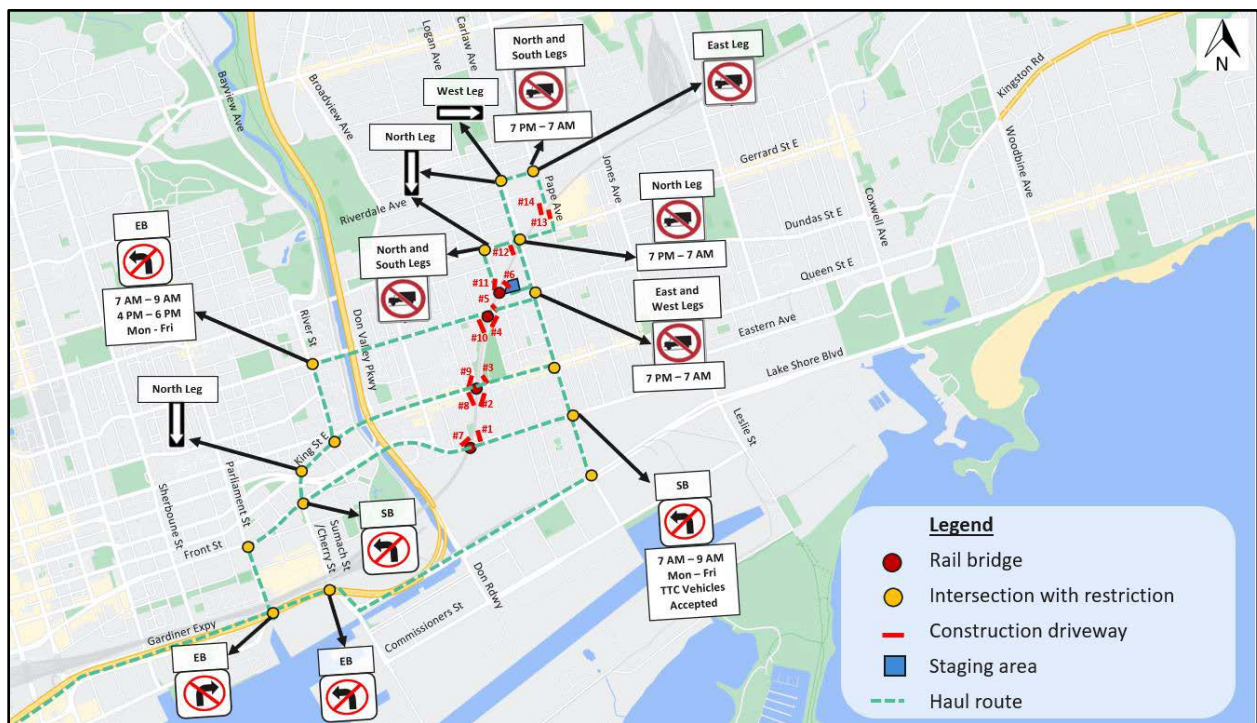


Figure 16 – Potential Haul Routes from the west of the Corridor

Source: Metrolinx

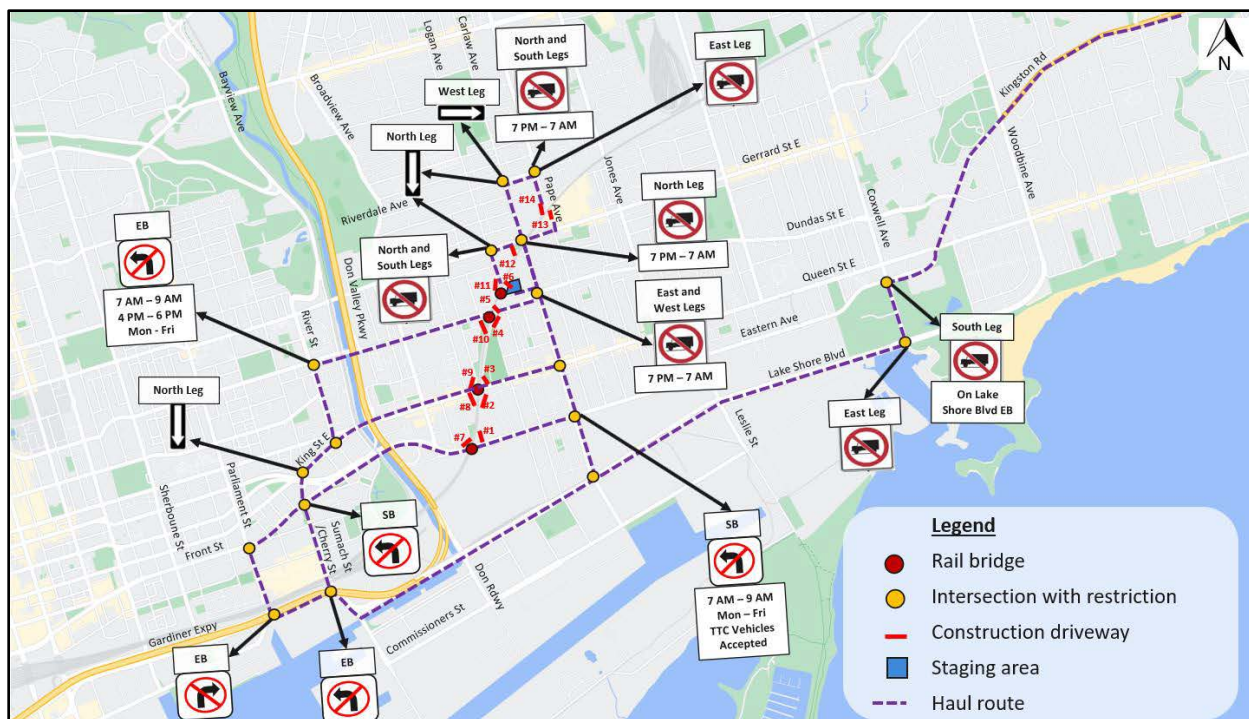


Figure 17 – Potential Haul Routes from the east of the Corridor

Source: Metrolinx

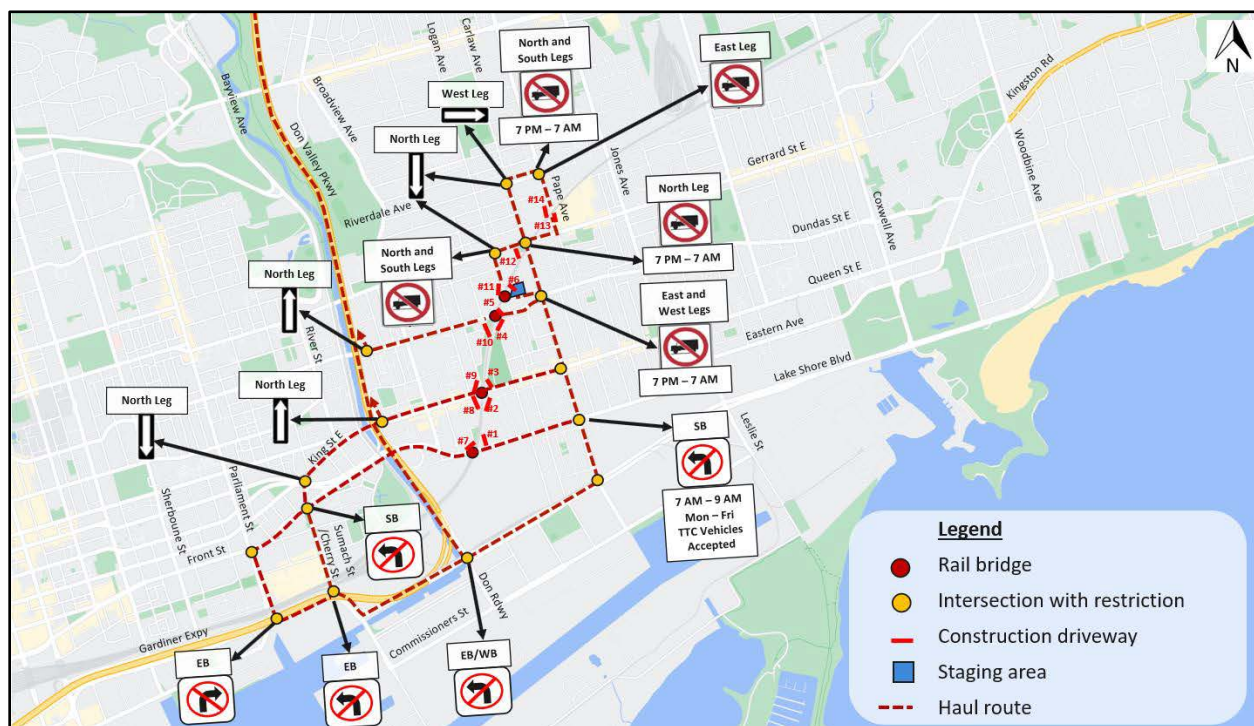


Figure 18 – Potential Haul Routes from the north of the Corridor

Source: Metrolinx

Construction trucks are anticipated to use Lake Shore Boulevard East and the Gardiner Expressway to access the rail corridor from the west, Kingston Road and Lake Shore Boulevard East to access the rail corridor from the east, and the Don Valley Parkway to access the rail corridor from the north. Key arterial roadways for the haul routes include Carlaw Avenue, Parliament Street, and Cherry Street.

Haul routes and truck volumes will be confirmed by the Metrolinx contractor as the project progresses and will follow existing turning and truck restrictions.

As mentioned earlier, construction trucks will access the rail corridor through temporary driveways from the adjacent City roads. Some construction trucks will travel to the staging area at Dickens Street and wait for their turn to access the rail corridor. Figure 19 shows potential routes that can be used to travel between the staging area and the temporary driveways along the rail corridor.

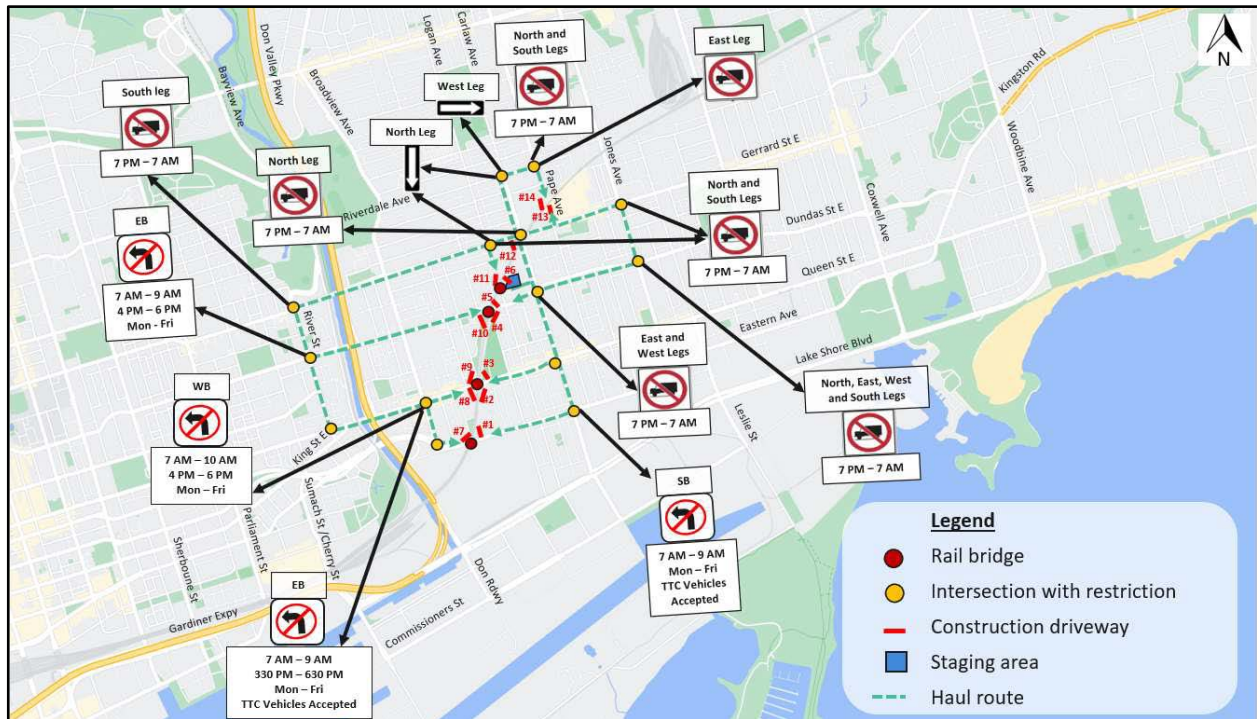


Figure 19 – Potential Haul Routes – Staging area to the Rail Corridor

Source: Metrolinx

2.3. Parking Impacts

Figure 20 shows the parking impacts (number and type of parking spaces with duration of removal) of the Metrolinx proposed temporary lane closures and driveway operations.

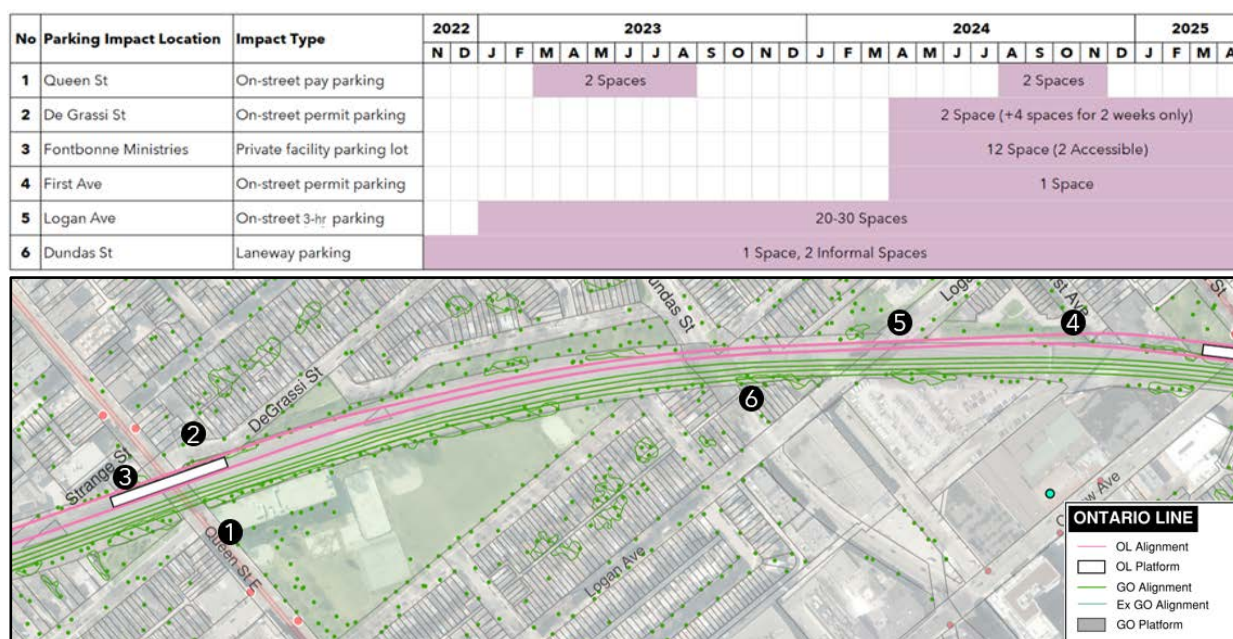


Figure 20 – Early Works Parking Impacts

Source: Metrolinx

Two on-street "Pay and Display" parking spaces, shown as Location 1 in Figure 20, on Queen Street East fronting the Jimmie Simpson Community Centre will be temporarily removed due to the curb lane closures. Additionally, the construction driveway on De Grassi Street at Queen Street East, shown as Location 2 in Figure 20, will require temporary removal of up to six permit parking spaces on the east side of De Grassi Street.

The construction driveway on Strange Street at Queen Street East, shown as Location 3 in Figure 20, will temporarily remove 12 private parking spaces (including two accessible parking spaces) associated with the Fontbonne Ministries facility.

The driveway on First Avenue, shown as Location 4, will remove one on-street parking space. Additionally, the temporary lane closures for the bridge works on Logan Avenue, shown as Location 5 in Figure 20, will remove up to 28 on-street 3-hr maximum parking spaces. Finally, the driveway on Dundas Street at Logan Avenue, shown as Location 6 in Figure 20, will disrupt and narrow the public laneway on the west side of Logan Avenue, just north of Dundas Street East. This will result in removing one backyard parking space and two informal laneway parking spaces.

2.4. Travel Time Impacts

Metrolinx's consultant has studied the weekday a.m. and p.m. period traffic impacts of the OL and City-led works using a microsimulation computer model. The traffic model is based on the City's traffic model for the Gardiner Expressway Rehabilitation study and focuses on the transportation network bounded by Bayview-Don Valley Parkway interchange to the north, Woodbine Avenue to the east, Commissioners Street to the south and York Street to the west. The traffic model estimates travel time on the east-west and north-south corridors due to overlapping impacts of the concurrent projects in the study area.

The vehicular travel time impacts were studied for the following scenarios:

- Base Case - 2022 Existing Conditions with projects started before the OL Early Works.
- Scenario 1 - includes all projects from the Base Case and OL Early Works.
- Scenario 2 - includes all projects from Scenario 1 and planned Capital Works construction projects (2022 - 2025) by the City, TTC and Waterfront Toronto and other OL Works within the traffic model area, including the planned Queen Street closure in downtown between Bay Street and Victoria Street.

Figures 21 to 24 show the major projects programmed to take place in close proximity of the OL Early Works. City staff have been reviewing the City Capital Works program to ensure proper coordination with the OL Early Works. These discussions are ongoing and will either advance, defer or bundle these projects with the OL Early Works. In addition, City staff will continue to coordinate with Metrolinx to ensure any planned construction work on the parallel routes is kept to a minimum and that overall congestion on the road network is minimized as far as possible. For the purpose of this traffic assessment, all the yearly projects were assumed to occur concurrently with the OL Early Works construction, thus providing a very conservative (worst case) estimate of travel time impacts.

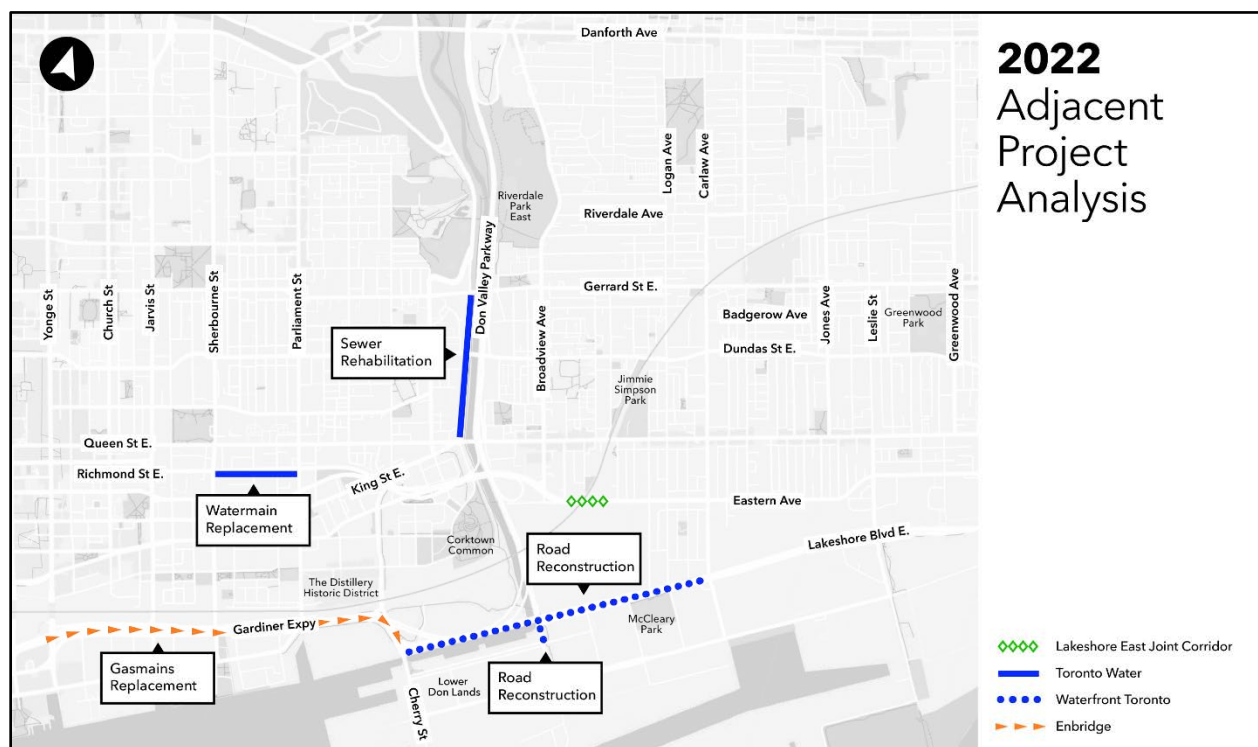


Figure 21 – 2022 Capital Works near the OL Early Works

Source: Metrolinx

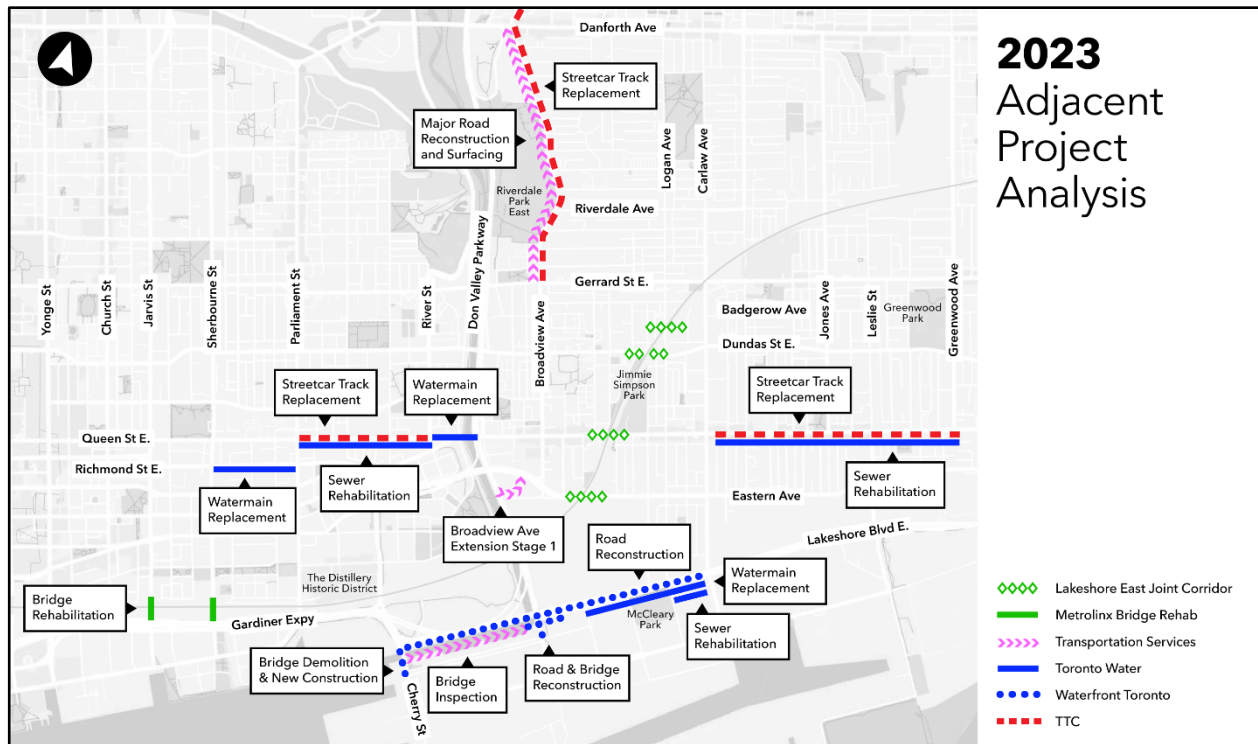


Figure 22 – 2023 Capital Works near the OL Early Works

Source: Metrolinx

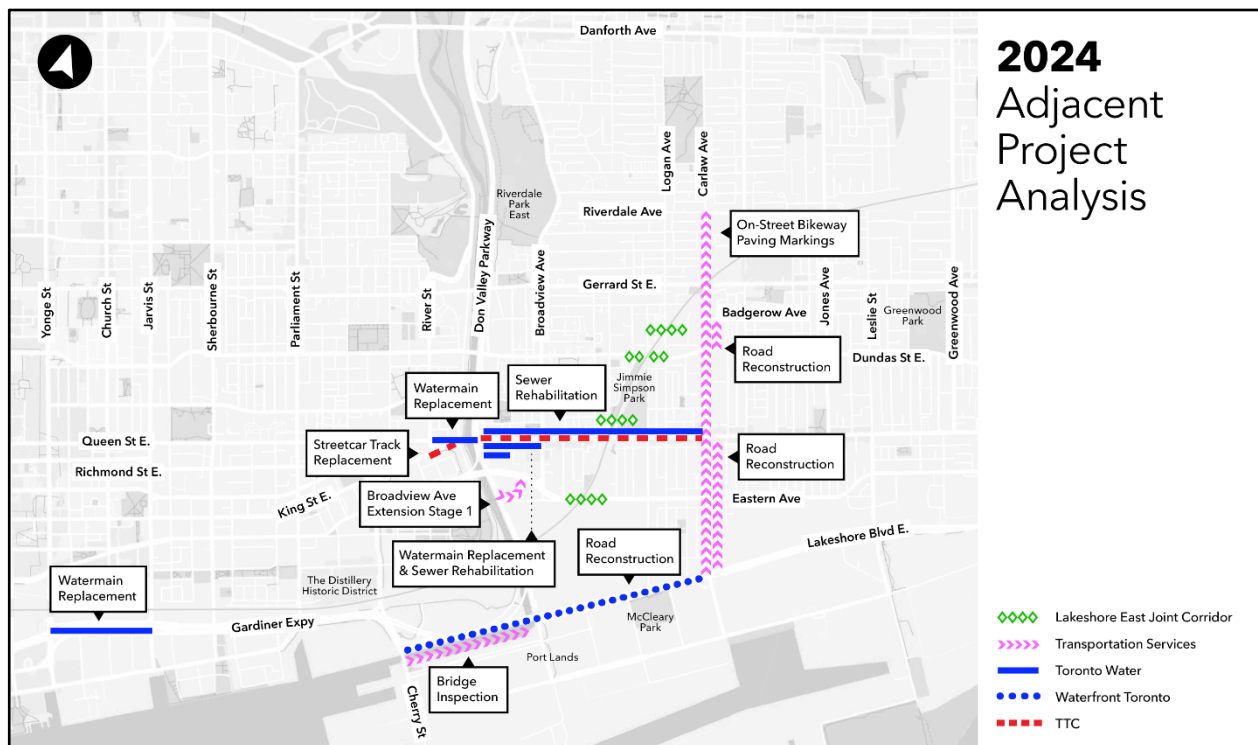


Figure 23 – 2024 Capital Works near the OL Early Works

Source: Metrolinx

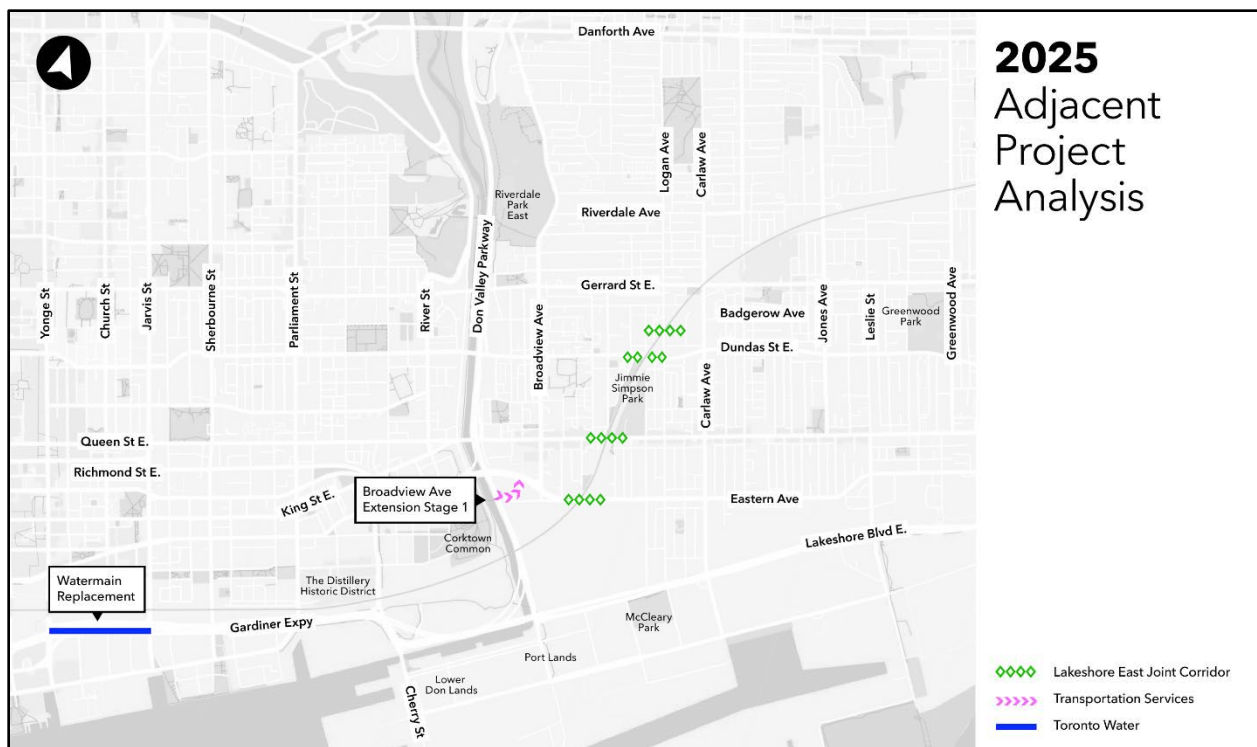


Figure 24 – 2025 Capital Works near the OL Early Works

Source: Metrolinx

Figures 25 to 28 show the estimated travel time on the east-west and north-south roads in the study area for the Base Case and Scenarios 1 and 2.

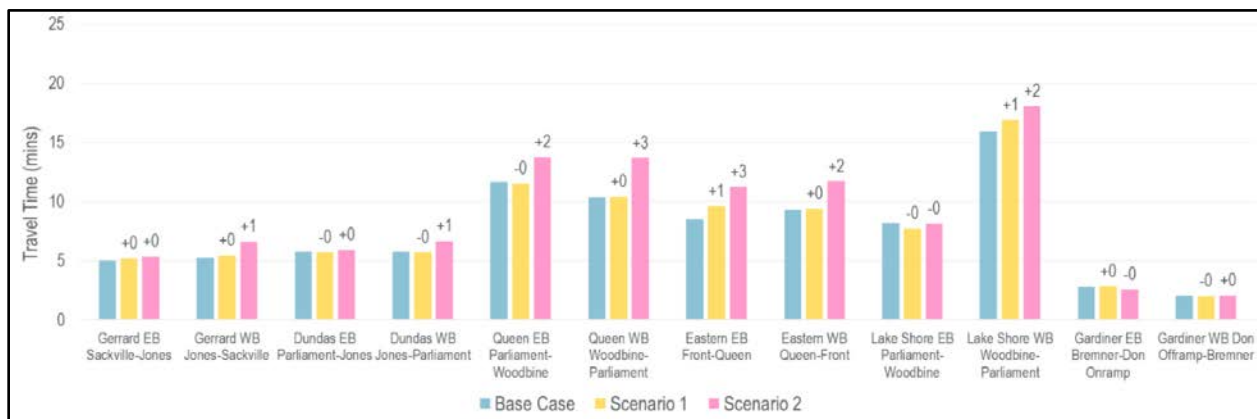


Figure 25 – A.M. Peak Hour Travel Time on East-West Roads

Source: Metrolinx

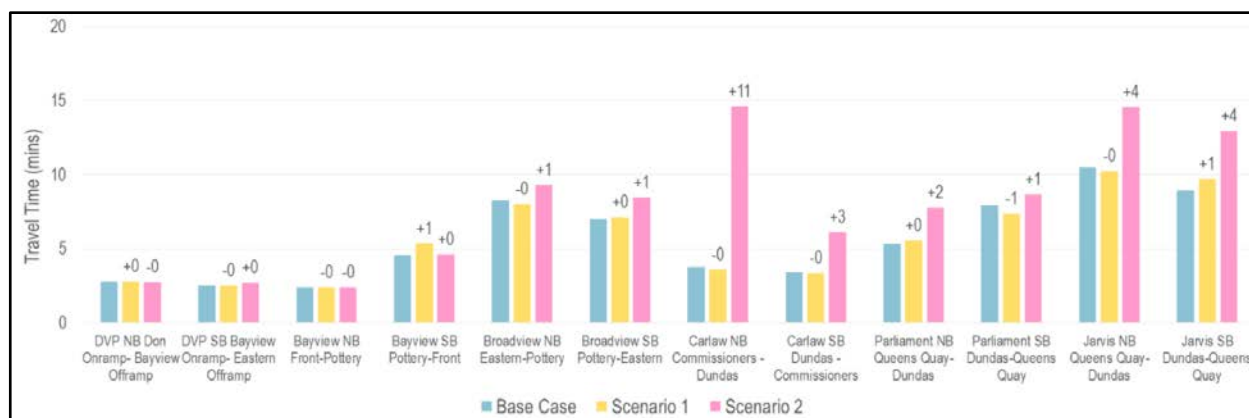


Figure 26 – A.M. Peak Hour Travel Time on North-South Roads

Source: Metrolinx

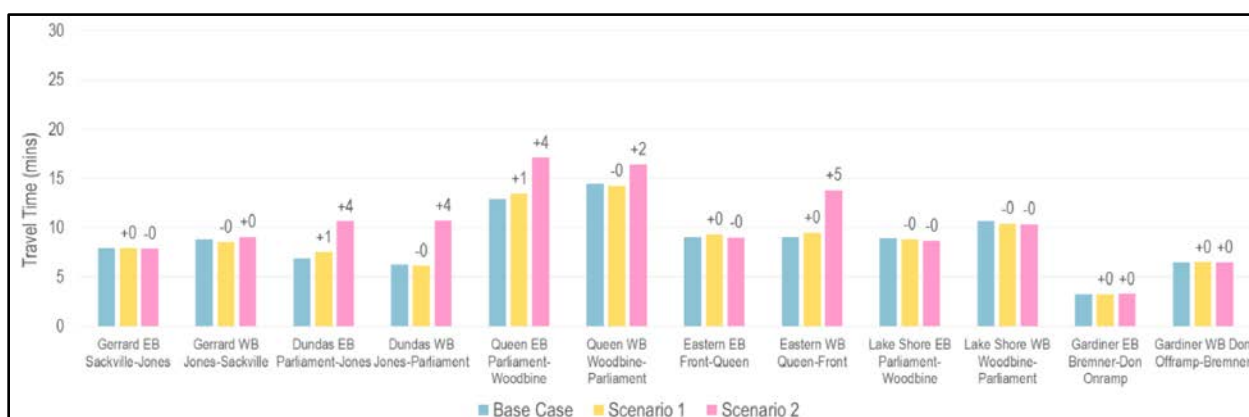


Figure 27 – P.M. Peak Hour Travel Time on East-West Roads

Source: Metrolinx

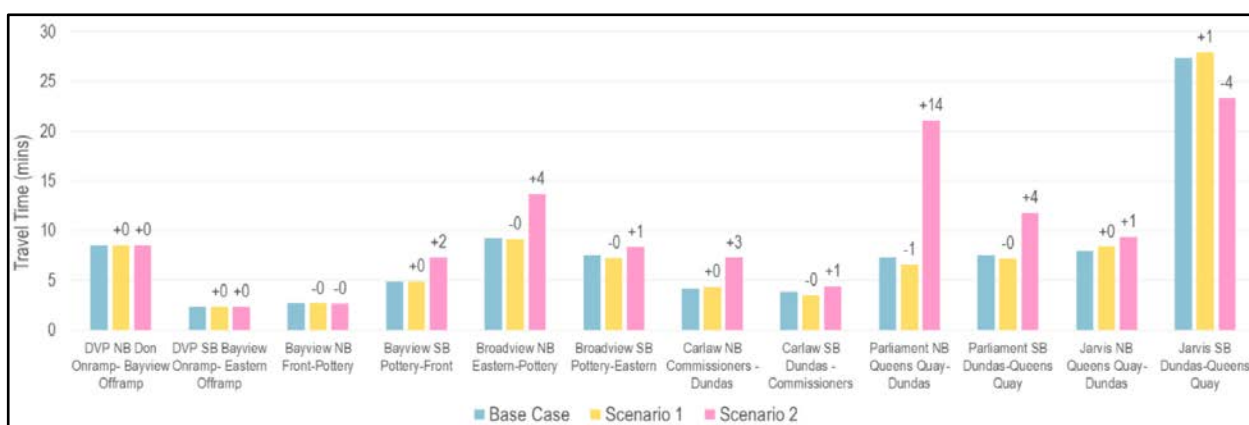


Figure 28 – P.M. Peak Hour Travel Time on North-South Roads

Source: Metrolinx

As shown in Figure 25, the a.m. peak hour travel times in Scenario 1 (including OL Early Works) are almost identical to the Base Case. However, in Scenario 2 (addition of the concurrent projects by the City, TTC and Waterfront Toronto), the a.m. peak hour travel times on the east-west roads are longer than in the Base Case and Scenario 1.

In Scenario 2 during the a.m. peak hour, additional travel time on the east-west roads is mainly expected on Queen Street, Eastern Avenue and Lake Shore Boulevard. Travel time on Queen Street (approx. 13 minutes) and Eastern Avenue (approx. 11 minutes) is expected to increase by 2 to 3 minutes in both directions. Additionally, travel time on

Lake Shore Boulevard (approx. 17 minutes) in the study area is expected to increase by up to 2 minutes.

For the north-south roads during the a.m. peak hour (Figure 26), in Scenario 2 Carlaw Avenue (approx. 15 minutes) is expected to experience a significant travel time increase of 11 minutes in the northbound direction due to road capacity reductions related to the planned capital works projects. The traffic model predicts the additional travel time on the segment of Carlaw Avenue, between Commissioners Street and Eastern Avenue. Additionally, in Scenario 2, travel time on Jarvis Street and Parliament Street are expected to increase by up to 4 minutes and 2 minutes, respectively, during the a.m. peak hour.

As shown in Figure 27, the p.m. peak hour travel times in Scenario 1 (including OL Early Works) are expected to be almost identical to that of the Base Case. However, in Scenario 2 (addition of the concurrent projects by the City, TTC and Waterfront Toronto), the p.m. peak hour travel times on the east-west roads are expected to be longer than in the Base Case and Scenario 1.

In Scenario 2 during the p.m. peak hours, additional travel time on the east-west roads is expected on Dundas Street, Queen Street and Eastern Avenue. Travel time on Dundas Street (approx. 11 minutes) is expected to increase by up to 4 minutes in both directions, observed mainly due to capacity constraints at the intersection of Dundas Street and River Street.

Additionally, in Scenario 2, the p.m. peak hour travel time on Queen Street (approx. 13 minutes) in the study area is expected to increase by up to 4 minutes in the eastbound direction and 2 minutes in the westbound direction, respectively. The lane reduction on Queen Street, between Parliament Street and Greenwood Avenue, are expected to result in higher travel time relative to the Base Case. Furthermore in Scenario 2, the p.m. peak hour travel time on Eastern Avenue westbound (approx. 14 minutes) is also expected to increase by 5 minutes.

For the north-south roads during the p.m. peak hour (Figure 28), travel time on Parliament Street northbound (approx. 21 minutes) is expected to increase by up to 14 minutes due to increased travel demand following traffic diversions in Scenario 2. The closure of the Eastern Avenue ramp to Don Valley Parkway is expected to cause traffic diversions on Parliament Street.

Additionally, in Scenario 2, the p.m. peak hour travel time on Broadview Avenue northbound (approx. 13 minutes) and Carlaw Avenue northbound (approx. 7 minutes) are expected to increase by up to 4 minutes and 3 minutes, respectively.

Based on the Metrolinx traffic model, travel time impacts due to the OL Early Works in the LSE JC area are expected to be minor. However, the combined impact of the OL Early Works and the planned City-led, TTC, Waterfront Toronto projects and other OL Works within the traffic model area, will cause reduced roadway capacity and increased traffic congestion and vehicular travel time.

2.5. Construction Mitigation Measures

City staff will coordinate with Metrolinx to implement safe work zone accommodations ensuring the safety and mobility of all road users during the Early Works construction. The following sections of the report discuss the construction mitigation measures that City staff will require Metrolinx to include as part of their construction proposal.

Pedestrians

In keeping with the City's Vision Zero Road Safety Plan, City staff will work closely with Metrolinx and its contractors to safely accommodate vulnerable road users in work zones. Under the current construction proposal, Metrolinx requires a pedestrian detour to alternative routes during the short-term (2-3 days) full road closures for the bridge deck works. Additionally, Metrolinx requires a pedestrian detour to an alternative opposite sidewalk during the utility relocation and bridge support demolition work (approx. four months per side) at the Queen Street Bridge. City staff will require Metrolinx to install advanced advisory signage at the nearest signalized intersection advising the pedestrians of the temporary sidewalk closure to ensure pedestrian safety.

The temporary walkways in the work zone will comply with the Accessibility for Ontarians with Disability Act (AODA) guidelines and accommodate accessible, detectable, stable and slip-resistant surfaces. Additionally, Metrolinx's contractor will provide temporary curb ramps to ensure easy access for people with mobility challenges. The temporary walkways will be well lit, protected from vehicular traffic and covered when required to protect pedestrians from falling debris or overhead hazards. The Metrolinx contractor will maintain the temporary walkways free of dust, dirt, loose material, snow, and ice.

Additionally, Metrolinx's contractor will ensure adequate sight lines are available at the site accesses and traffic control persons are positioned to avoid any conflicts between the construction vehicles and pedestrians.

Bicyclists

Metrolinx requires periodic closures of bicycle lanes on Logan Avenue at the rail bridge in one or both directions for a cumulative total of eight months. Additionally, Metrolinx requires temporary closure of the eastbound and westbound bicycle lanes on Dundas Street East, between Wardell Street and Logan Avenue, from April 2023 and April 2024 and from January 2025 to July 2025 to complete the bridge works. During bicycle lane closures, Metrolinx through its Early Works contractor will install shared lane pavement markings and "Share the Road" advisory signage to direct motorists and cyclists to share the available lane, along with advance signage strategically located for maximum visibility, to provide guidance to road users as they approach the work zone.

There are currently no dedicated bicycle lanes on Queen Street East and Eastern Avenue. Therefore, cyclists will continue to share the available travel lane with the general traffic during the bridge works and detour to alternative routes during the short-term full road closures. Metrolinx will install appropriate signage for motorists and people cycling to share the road.

City staff will require Metrolinx to provide wider travel lanes in the work zone to ensure sufficient space between cyclists and the general vehicular traffic. Metrolinx's contractor will maintain the available travel lanes and bicycle lanes free of dust, debris, ice, and construction material.

Transit Users

Currently, there is no transit service on Logan Avenue, Dundas Street East, and Eastern Avenue. The 501 Queen streetcar service on Queen Street East, and nearby transit stops will be maintained except during the short-term full road closures. Metrolinx will work with TTC to provide information on transit detours during the short-term full road closures. Additionally, TTC will replace the Queen 501 streetcar service with bus service during the streetcar track rehabilitation project, between Parliament Street and Carlaw Avenue, programmed from 2023 to 2024.

Motorists

Metrolinx will utilize travel demand management and traffic management strategies to minimize the impact of the OL Early Works on network-wide congestion and maintain safety and mobility in the work zones.

Metrolinx's traffic model has predicted the congested traffic movements at the boundary road intersections and determined traffic signal timing modifications required to maintain acceptable level of mobility on the roadway network. With technical input from Metrolinx, the City will implement traffic signal timing modifications to minimize the travel time delay. Furthermore, frequent monitoring of the traffic operations will be undertaken to ensure traffic signal modifications are optimized and reflect any changing conditions, for example, due to observed issues or a new or revised work zone.

As mentioned earlier in the report, except for the short-term full road closures, Metrolinx will maintain at least one travel lane in each direction during the construction works at the Logan Avenue, Dundas Street East, Queen Street East and Eastern Avenue Bridges. Metrolinx will install advanced advisory signage including changeable message signs at the network level to inform road users of the road closures. Additionally, the Metrolinx contractor will submit the Road Disruption Activity Reporting System (RoDARS) notification and inform the web-based network service providers such as Google and Waze etc. of the road closures.

During the Early Works construction, Metrolinx will encourage site construction workers to take transit to work sites, thus reducing additional congestion on the City roads and additional demand on the nearby parking facilities. Private vehicle parking will not be allowed in the work zones. Additionally, to reduce traffic congestion, Metrolinx contractor will minimize construction truck staging and idling on the City roads. The construction truck haul routes will be developed in accordance with Vision Zero principles including lower speed limit and forward-in and forward-out truck movements at all construction driveways.

Furthermore, Metrolinx and its contractor will avoid construction truck haul routes near schools where feasible and restrict truck movements to outside of school start and finish times.

2.5.1. Rail Corridor Access Points

Metrolinx and its contractor will actively manage pedestrian safety at the construction driveways by ensuring good sightlines for pedestrians and construction vehicle driveways. Additionally, Metrolinx will install temporary traffic signage warning the road users of construction vehicle ingress and egress. Metrolinx will deploy trained traffic control persons to manage the construction vehicle movements at the construction driveways. Metrolinx will ensure forward-in and forward-out truck movements at the construction driveways.

Parking Mitigation

Metrolinx will accommodate the impacted on-street overnight permit parking and daytime parking spaces at nearby off-street facilities. Figure 29 shows the locations of the affected parking spaces and replacement spaces.



Figure 29 – On-street and Private Parking Replacement

Source: Metrolinx

Metrolinx is currently exploring temporary accommodation of lost on-street permit parking spaces at 8-10 De Grassi Street. Additionally, Metrolinx is working with City staff to temporarily accommodate up to 25 parking spaces on Dickens Street to replace impacted parking spaces on Logan Avenue. Metrolinx will compensate Toronto Parking Authority for lost revenue of the removed daytime "Pay & Display" parking spaces. Additionally, Metrolinx will maintain two accessible parking spaces associated with Fontbonne Ministries on-site and the remaining private parking spaces at 875 Queen Street East. Metrolinx will engage with the impacted property owners regarding the loss of private parking spaces at the Logan Avenue laneway and potential mitigation.

Permit Requirements

In accordance with the Toronto Municipal Code Chapter 743, Streets and Sidewalks, Use Of, Metrolinx's contractor is required to obtain a permit before undertaking any street work or temporary street occupation within the City road right-of-way. As part of the permit application, Metrolinx's contractor will need to prepare and submit traffic control plans that are compliant with the City and Provincial policies and standards. In accordance with the Toronto Municipal Code Chapter 743, the permit application requires the contractor to pay applicable fees, provide work details (scope, location, duration, and technical drawings), a traffic control plan, evidence of insurance, and any additional information required. These plans will be carefully reviewed by the City staff to ensure safety and mobility needs of the travelling public, businesses and community are met. Once implemented, City staff, in coordination with Metrolinx and Project staff, will monitor the installation to ensure its compliance with the approved traffic control plan. City staff will ensure coordination in logistical planning between Metrolinx work sites, City-led project work sites, developer work sites and any other works taking place on or adjacent to the public right of way. The City Construction Hub Coordinator will also be a "single point of contact" for these parties, as well as for business and the public/residents.

The City Transportation Services staff reviews the permit application, traffic control plan and supporting documents with the following guiding principles:

- Vision Zero - Safe accommodation of all road users, especially pedestrians and people cycling, in the construction zone including conformance with the Accessibility for Ontarians with Disability Act (AODA) guidelines
- Traffic signage plan in conformance with the applicable Ontario Traffic Manual Book 7 and Book 18 guidelines with clear guidance to all road users in the work zone
- Access to transit, emergency services, residences, and businesses
- Minimize impact on the on-street and permit parking spaces
- Minimize neighbourhood infiltration and implement any traffic calming measures to discourage speeding
- Minimum impact of construction operations on the road network and adjacent land uses, including construction noise impact on the nearby noise-sensitive land uses
- Minimize conflicts between pedestrians and construction vehicles specifically at the site driveways
- Haul route plan with no construction vehicles using residential streets
- Traffic signal modifications for efficient traffic flow in the area
- Coordination with other planned works in the area to minimize concurrent work on parallel routes

Upon satisfactory review of the permit application, the requested permit is issued with terms and conditions including standard permit conditions outlined in Appendix A of the Toronto Municipal Code Chapter 743 ("Use of Streets and Sidewalks") and special

permit conditions specific to the site. The special permit conditions may include the following:

- Advanced construction notification to the city work zone traffic coordinator, affected residents and businesses
- Work location and specific work hours
- Road Disruption Activity Reporting System (RoDARS) notification
- Special accommodations for pedestrian and bicyclist safety including AODA requirements
- Provision of trained traffic control persons or paid duty police officers
- Enhanced traffic signage
- Time or route restrictions on construction vehicle movements
- Work zone and adjacent street network cleanliness
- Work zone lighting
- Restriction on construction crew private vehicle parking in work zones and on city roads
- Restriction on haul truck staging and idling on city roads

In the event the permit holder violates the terms and conditions of the permit, the issued permit may be cancelled in accordance with authority given by Chapter 743.

2.6. Temporary Delegation of Authority

The Ontario Line Early Works construction at the rail bridges and in the LSE JC will require traffic lanes, bicycle lanes, and sidewalk closures. To address any changes in traffic control and address road safety-related issues during the construction, Transportation Services needs to act expeditiously to address the issues and permit continuance of the construction work.

Following Eglinton Crosstown Light Rail Transit (ECLRT), Finch West Light Rail Transit (FWLRT) and GO Expansion Project experience, the need to report on closures exceeding 30 days would be problematic in terms of the risks it would present to schedule adherence by Metrolinx and its contractors. Given that Council report cycles are numerous months in duration, there is a risk that the City of Toronto's approval process could introduce delays to construction schedules. In addition, this volume of reporting would overwhelm Transportation Services staff whose time could otherwise be spent reviewing plans, working with stakeholders (Metrolinx, contractors, affected Ward Councillors and the local communities) and conducting work zone coordination to minimize construction impacts on the local communities and the travelling public.

To ensure that Transportation Services staff respond promptly for construction of the Ontario Line Early Works in the LSE JC, it is recommended that the General Manager, Transportation Services, be given the delegated authority until December 31, 2026, inclusive to temporarily close to vehicular and pedestrian traffic any highway or portion of a highway, with the exception of those highways listed in Section 937-4 of Code Chapter 937, as required for the purposes of construction of the Ontario Line Early Works in the Lakeshore East Joint Corridor, for a period up to and including 365

consecutive days but ending no later than December 31, 2026, inclusive. It is also recommended that, in carrying out this delegation, the General Manager, Transportation Services, be exempt from Section 937-5 of Chapter 937, that being the requirement to notify the local Ward Councillor of the pending closure and the requirement to report on the proposed closure if so requested by the local Ward Councillor. It should be noted that City Council has previously granted a similar delegation of authority on the other subway expansion projects, including the SSE and ECWE.

2.7. Communication and Community Engagement

Metrolinx will implement a Communication Plan to proactively engage the areas residents and businesses, provide construction updates, address concerns and minimize disruptions during the OL Early Works construction.

The Metrolinx Communication Plan will include the following:

- A designated project website and other information portals such as social media, e-newsletter, construction notices, public meetings with details on construction activities, schedule, impact and mitigation;
- Communications regarding traffic impacts and proposed mitigation within the zone of influence along the LSE Joint Corridor on anticipated traffic, community impacts and mitigation plans;
- 24/7 communication contacts during the Early Works construction;
- A Complaints Protocol for timely response to complaints; and,
- Establishment of CLC and Traffic Management Committee.

The Metrolinx project website will include the construction schedule for Early Works in the LSE JC, which will be updated regularly with impacts and mitigation. Additionally, regular construction and traffic staging information will be shared with the areas residents and businesses through the social media, weekly e-newsletters, public meetings (virtual and in-person), pop-up events and CLCs.

To address project-related questions or concerns in a timely manner, Metrolinx has established a community office at 770 Queen Street East. The community office contact information will be posted on the project website and at the construction sites. Additionally, Metrolinx's contractor will also provide an appropriately staffed 24-hour phone number and email address for residents and businesses to call and report their concerns directly to the Metrolinx contractor. Metrolinx will ensure their contractor employs sufficient resources to monitor the construction activities, address concerns and make adjustments as required during the OL Early Works construction. The contact details will also be shared on the project website.

Metrolinx will establish a complaint protocol to address concerns from residents and businesses during the construction. As part of the protocol, Metrolinx contractor's phone number and email address will be shared with the Toronto 311 service. Any complaints received through the 311 service or through the local councillor's office will be directed to this dedicated email or phone number. The complaints will be logged and resolved by Metrolinx and their contractor project team in collaboration with the City staff.

Metrolinx will establish a CLC for the OL Early Works in Ward 14 consisting of local Councillor's staff, businesses, other stakeholders and residents. The CLC will discuss project updates, advanced information on upcoming project activities, including road closures, and will identify and address any stakeholder concerns. Metrolinx will arrange CLC meetings frequently, beginning in September 2022. Metrolinx will evaluate the frequency of CLC meetings for on an on-going basis. The monthly meeting frequency with the community has proven successful on other Metrolinx transit projects to provide enough time to share updates and responses to community concerns. Additionally, there will be bi-weekly Traffic Management Committee (TMC) meetings for the OL works, attended by the City's Transportation Services Division, City Emergency Services, Toronto Police, Metrolinx and their contractor. The TMC will review the traffic impacts of the Early Works construction work and coordinate on implementation of mitigation measures for the safety of road users and community.

Additionally, Metrolinx will fund a City staff position of a Project Manager – BIA Field Ambassador. This staff person will be responsible for working with local BIAs to communicate and address business related concerns associated with Metrolinx construction projects.

2.8. Don Valley Parkway Borehole Work

The Don Valley Crossing is one of two OL bridge structures across the Don Valley Parkway. It will be located just south of the Millwood Road bridge structure. Relocation of a large sanitary sewer (1.5 metre diameter) is required due to conflict with a proposed bridge pier. The sewer relocation will be constructed through an Advanced Works contract, which will also include a retaining wall adjacent to the southbound traffic lanes of the Don Valley Parkway. Prior to construction of the retaining wall, two geotechnical boreholes, with a depth of up to 29 metres, located in the southbound shoulder area on the Don Valley Parkway, just south of Millwood Road bridge structure, will be required to confirm engineering feasibility.

Closure of the southbound curb lane and pavement shoulder between 150 metres north of the Leaside Bridge and a point 550 metres south of the Leaside Bridge will be required to complete this work. Traffic safety concerns will be mitigated with crash trucks and TC-12 arrow board.

The construction duration for the borehole work is anticipated to be two weekends from the following dates for which City Council approval is required to allow Metrolinx to complete the borehole work:

- 11 p.m. on August 5, 2022 to 5 a.m. on August 8, 2022
- 11 p.m. on August 12, 2022 to 5 a.m. on August 15, 2022
- 11 p.m. on August 19, 2022 to 5 a.m. on August 22, 2022
- 11 p.m. on August 26, 2022 to 5 a.m. on August 29, 2022

Additionally, Metrolinx requires three nightly southbound curb lane and pavement shoulder closures for water level monitoring. The nightly lane closures for water level monitoring are required on the following dates:

- 11 p.m. on September 13, 2022 to 5 a.m. on September 14, 2022
- 11 p.m. on October 11, 2022 to 5 a.m. on October 12, 2022
- 11 p.m. on November 8, 2022 to 5 a.m. on November 9, 2022

3. Scarborough Subway Extension LS2 Diversion Road

As discussed above, the SSE is currently in its "tunnelling phase" where Strabag Inc. is actively operating the TBM Launch Site (LS-1) located in the north-east corner of the intersection of Sheppard Avenue East and McCowan Road. The TBM will travel south along the project alignment to an extraction shaft (LS-2) located on Eglinton Avenue East, east of Midland Avenue.

The TBM Extraction Shaft LS-2 site will be setup within the eastbound and westbound lanes on Eglinton Avenue East, east of Midland Avenue. A road diversion around the Extraction Shaft site will be required to provide continuity of east-west vehicular and pedestrian movements on Eglinton Avenue East.

The road diversion is anticipated to be in place from October 1, 2022 to December 31, 2024. Upon completion of the tunnelling operation, the LS-2 site will be handed over to the Stations, Rail and Systems (SRS) Project Co., responsible for construction of the new station buildings, emergency exit buildings, and all rail and communication related works.

Figure 30 shows the LS-2 site layout and the diversion road around the site. The detailed road alignment for the temporary diversion road is shown in Attachment 1.

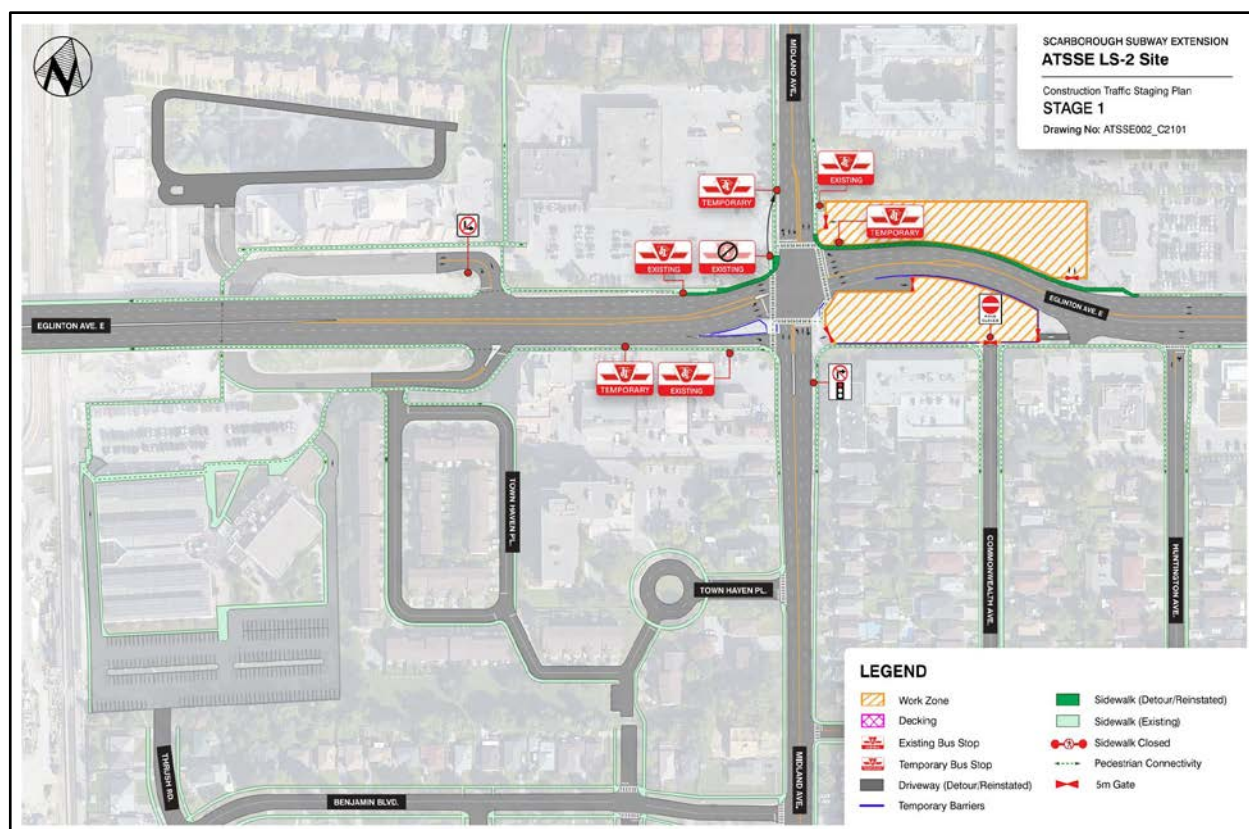


Figure 30 – SSE LS-2 Site Layout and Diversion Road

Source: Metrolinx

The diversion road will maintain two eastbound and two westbound through traffic lanes on Eglinton Avenue East. All east-west and north-south turn lanes will be maintained at the intersection of Eglinton Avenue East and Midland Avenue. Additionally, a 1.8 metres wide pedestrian walkway will be provided along the north side of the diversion road. The south sidewalk on Eglinton Avenue East will be maintained. It is noted that the TTC will also be operating Line 3 bus replacement service for an interim period starting in Q4 2023 when Line 3 Scarborough shuts down. The replacement buses will operate south on Midland Avenue and west on Eglinton Avenue East to Kennedy Station. This movement will not be impacted by the construction and will be maintained at all times.

A portion of the diversion road will be constructed on lands currently owned by Metrolinx. The diversion road will be dedicated as a public highway and traffic by-laws will be enacted and enforced by the City. The dedication by-law and proposed traffic by-law amendments will take effect once the Eglinton Avenue East diversion road has been constructed by Metrolinx to the satisfaction of the City's Engineering Construction Services Division, and accepted and commissioned, and the required land interest in the diversion road has been transferred to the City. It is anticipated that the diversion road will be in place for a period of 27 months (anticipated to be from October 1, 2022 to December 31, 2024, inclusive).

The City and Metrolinx legal and real estate teams are working collaboratively to transfer the required land interest in the diversion road to the City. Additionally, the City and Metrolinx project teams are working on finalizing the maintenance responsibilities agreement for the diversion road.

The LS-2 site setup will result in the closure of Commonwealth Avenue at Eglinton Avenue East for the duration of the construction (anticipated to be from October 1, 2022 to December 31, 2024, inclusive). Residents of Commonwealth Avenue will be able to connect with Midland Avenue and Eglinton Avenue through Brussels Road.

4. ECWE Long-term Road Closures - Elevated Guideway and Tunnel Portal Construction

Metrolinx will deliver the ECWE in the following four main contracts, as detailed in Table 1 below.

ECWE Contract	Project Co.	Major Construction Activities
Advanced Tunnel Contract - ATC 1	West End Connectors	<ul style="list-style-type: none"> • Tunnelling work from Renforth-Eglinton Station to the Tunnel Portal west of Scarlett Road • Tunnel Boring Machine Launch and Extraction shafts • Headwalls for Stations and Emergency Exit Buildings
Elevated Guideway - EG	In market for procurement	<ul style="list-style-type: none"> • Elevated guideway from the tunnel portal west of Scarlett Road to the tunnel portal east of Jane Street
Advanced Tunnel Contract - ATC 2	In market for procurement	<ul style="list-style-type: none"> • Tunnelling work from the tunnel portal east of Jane Street to the ECLRT Mount Dennis Station
Stations, Rail and Systems Contract - SRS	Future procurement	<ul style="list-style-type: none"> • Stations, and ancillary buildings

Table 1 – Metrolinx ECWE Work Packages

Source: Metrolinx

Metrolinx has performed a constructability assessment of the EG and ATC 2 works and determined the location, maximum number and duration of temporary long-term road closures required to complete these works. These requirements were developed through detailed discussions with City staff and ensure a safe environment for pedestrians, cyclists, vehicles and construction staff.

The road closure requirements are the maximum permitted for the proponents in the EG and ATC 2 procurement. The Project Co. procurement process incentivizes the proponents to reduce the number and duration of road closures. Therefore, it is expected that the successful proponent design would require fewer temporary road

closures during construction than discussed in this report, resulting in reduced impacts on the community and road users.

Additionally, Metrolinx will incorporate significant penalty provisions in the project contract with the Project Co. to deter the actual number and duration of road closures from exceeding the bid submission. This is in accordance with Metrolinx and City staff's efforts to minimize the community impact of the long-term road closures and maintain traffic mobility during the construction.

In the event that any additional road closure time is required, either as a result of bids received (which is considered unlikely due to the contract structure) or during the construction phase (for example as a result of unforeseen circumstances) then City staff would carefully examine these and bring forward new recommendations to City Council for consideration, if necessary.

Through this report, City Council approval is sought for the long-term temporary road closures for the EG and ATC 2 works and impacts on the road users and community are further discussed below. Additionally, this section of the report discusses the mitigation measures to reduce the construction impacts and Metrolinx's communication strategy during construction of the project.

Figures 31 and 32 show the ECWE alignment from the Scarlett Tunnel Portal to the Mount Dennis Station.

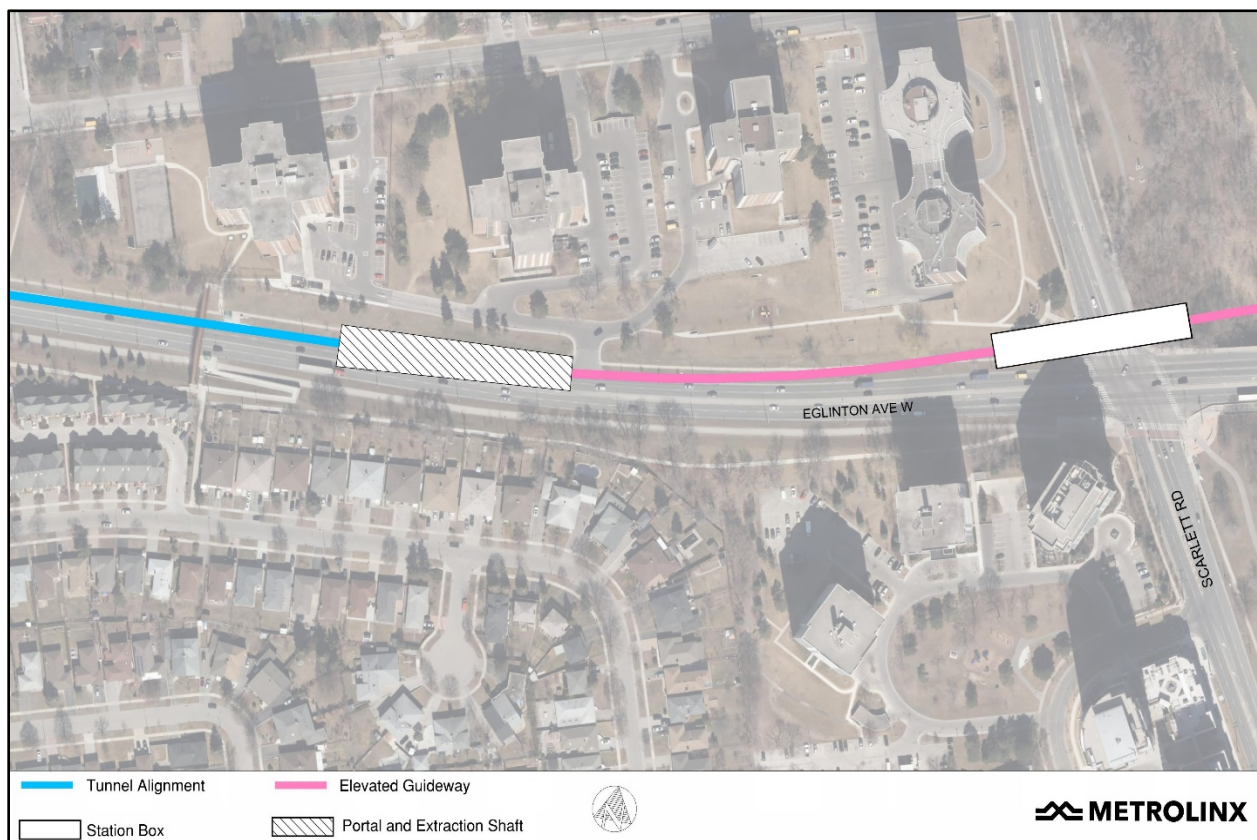


Figure 31 – ECWE Scarlett Portal and Station

Source: Metrolinx

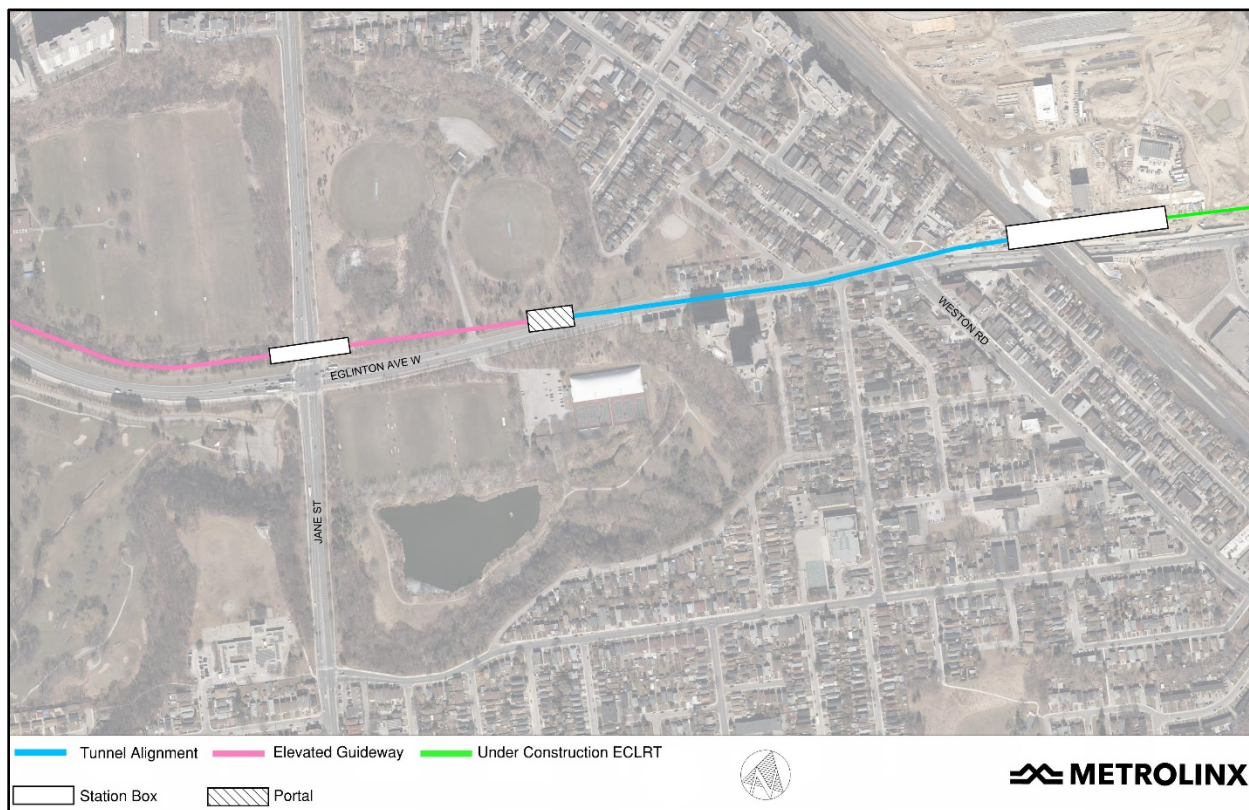


Figure 32 – ECWE Jane Portal and Station

Source: Metrolinx

The construction work for the EG and ATC 2 contracts will take place on the north side of Eglinton Avenue West, between west of Scarlett Road to east of Weston Road. Traffic Management Lands covering the planned construction works for the EG and ATC 2 contracts are shown in Table 2:

Construction Work	Traffic Management Lands Limits on Eglinton Avenue West
Extraction Shaft & Scarlett Tunnel Portal	From 450 metres east of Royal York Road to Scarlett Road
Elevated Guideway	From 450 metres east of Royal York Road to Pearen Street
Launch Shaft & Jane Tunnel Portal	From 300 metres west of Jane Street to Pearen Street
Cross Passage	From 150 metres east of Jane Street to Weston Road
Extraction Shaft at Mount Dennis Station	From 200 metres west of Weston Road to Black Creek Drive

Table 2 – Traffic Management Lands for EG and ATC 2 Contracts

Source: Metrolinx

The following sections of the report discuss the construction activities and resultant impacts in various sections of the Traffic Management Lands.

4.1. West of Scarlett Road

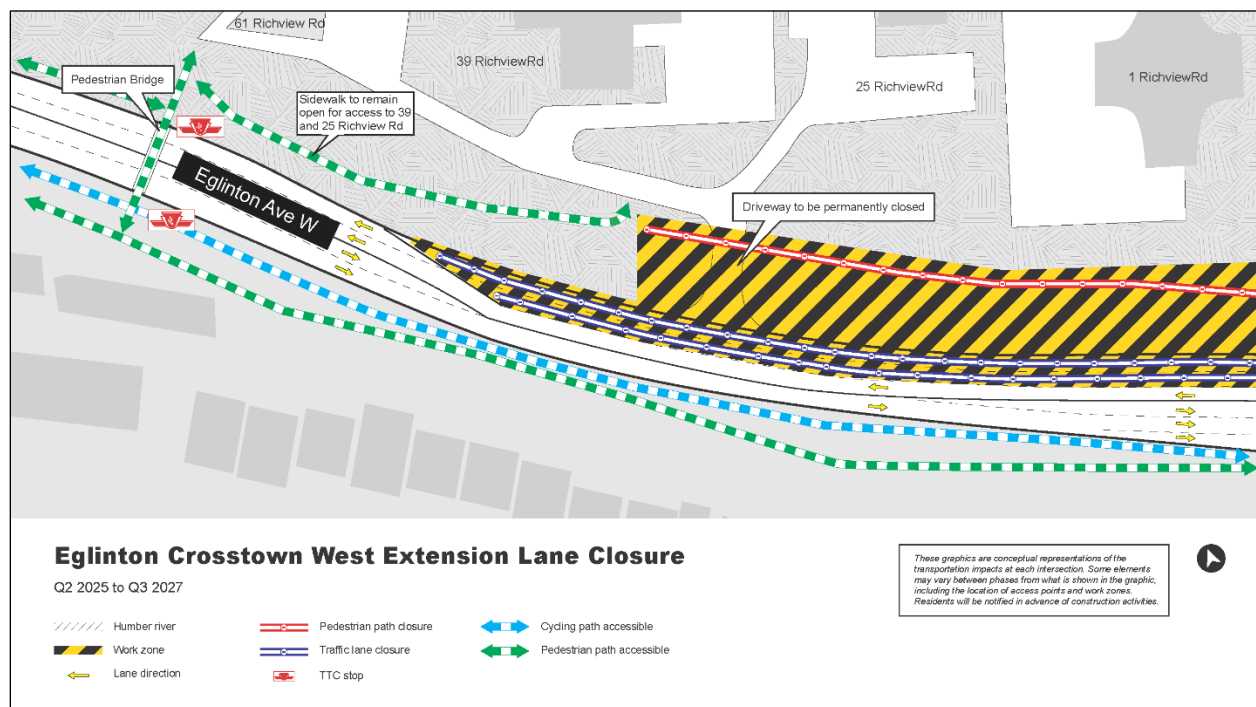


Figure 33 – ECWE Extraction Shaft and Scarlett Tunnel Portal Work Site

Source: Metrolinx

As shown in Figure 33, the construction impacts at the Extraction Shaft and Scarlett Tunnel Portal Work Site include the following:

- Permanent closure of the south access to 25, 39 and 61 Richview Road properties.
- Temporary closure of the north sidewalk on Eglinton Avenue West, between Scarlett Road and a point 450 metres west, from April 1, 2025 to September 30, 2027.
- Temporary closure of the westbound curb and median lanes on Eglinton Avenue West, between Scarlett Road and a point 450 metres west, from April 1, 2025 to September 30, 2027.

The construction impacts of the Extraction Shaft and Scarlett Tunnel Portal are discussed below:

Pedestrian Impacts

During construction of the Extraction Shaft and Scarlett Tunnel Portal, the north sidewalk on Eglinton Avenue West and a point 450 metres will be temporarily closed for approximately 2.5 years. During the north sidewalk closure, the south sidewalk and its connection to the existing pedestrian bridge on Eglinton Avenue West will be maintained at all times. Additionally, Metrolinx will maintain a pedestrian connection between the pedestrian bridge and the properties at 25 and 39 Richview Road.

Appropriate advanced signage will be positioned at strategic locations to ensure pedestrians are informed of this temporary sidewalk closure. In addition, warning

signage will be posted at the signalized intersection of Scarlett Road at Eglinton Avenue West.

Cycling Impacts

During the construction of the Extraction Shaft and Scarlett Tunnel Portal, Metrolinx will maintain the south side Multi-Use Path (MUP) on Eglinton Avenue West, west of Scarlett Road.

Auto Access Impacts

Metrolinx requires closure of the westbound lanes on Eglinton Avenue West, between Scarlett Road and a point 450 metres to facilitate construction of the Extraction Shaft and Scarlett Tunnel Portal for approximately 2.5 years. With the closure in place, Metrolinx will maintain two-way traffic (one eastbound and one westbound traffic lane) by shifting the traffic lanes to the south side of the subject section of Eglinton Avenue West.

Transit Impacts

Existing TTC transit service will be generally maintained, although construction activity reducing intersection and road capacity may slow down transit operations for customers, especially during busy peak times. Bus stops will also generally be maintained, and any relocations will be coordinated with the TTC.

Property Access Impacts

Except for the south access to 25, 39 and 61 Richview Road properties, Metrolinx will maintain all business and property accesses during the construction work.

The location of the Scarlett Tunnel Portal and transition of the elevated guideway to below grade will result in permanent closure of the south access to 25, 39 and 61 Richview Road properties. After the closure of the south access, the intersection of Richview Road and Scarlett Road will provide the main gateway to the properties along Richview Road and will experience an additional 200 diverted vehicles during the morning and afternoon peak hours.

Metrolinx's traffic consultant has assessed the morning and afternoon peak hour traffic impacts of the access closure on the boundary road intersections of Scarlett Road at Richview Road and Scarlett Road at Eglinton Avenue West.

The key findings of the traffic analysis are as below:

- The southbound left-turn vehicles at the intersection of Eglinton Avenue West and Scarlett Road are expected to experience an additional delay of 21 seconds in the morning peak hour and 9 seconds in the afternoon peak hour.
- The intersection of Scarlett Road and Eglinton Avenue West is expected to continue operating at an acceptable level of traffic operations.

- The eastbound right-turn movement at the intersection of Eglinton Avenue West and Richview Road will incur the highest diverted traffic volume during the a.m. peak hour. The eastbound vehicular queues on Richview Road are expected to extend beyond the exit driveway from 1A and 1B Richview Road properties during the morning peak hour, thus temporarily delaying the residents exiting the residential towers.

In an effort to mitigate the vehicular queuing issues and improve pedestrian safety at the intersection of Scarlett Road and Richview Road, Metrolinx's consultant has conducted a traffic signal warrant analysis as per the Ontario Traffic Manual Book 12 guidelines.

With a traffic signal controlled configuration, overall pedestrian safety at the intersection improves due to provision of protected crosswalks and the eastbound vehicular queue in the morning peak hour shows slight improvement. Given this, in concordance with the Metrolinx traffic study, the City's Transportation Services Division recommends installation of a new traffic signal at the intersection of Scarlett Road at Richview Road.

Metrolinx will install the new traffic signal as part of the ECWE project and will coordinate with TTC on relocation of the existing TTC bus stop in the north-east corner of the intersection of Scarlett Road and Richview Road.

4.2. Scarlett Road to Jane Street

The section of Eglinton Avenue West, between Scarlett Road and Jane Street, will experience construction activities for the Elevated Guideway.

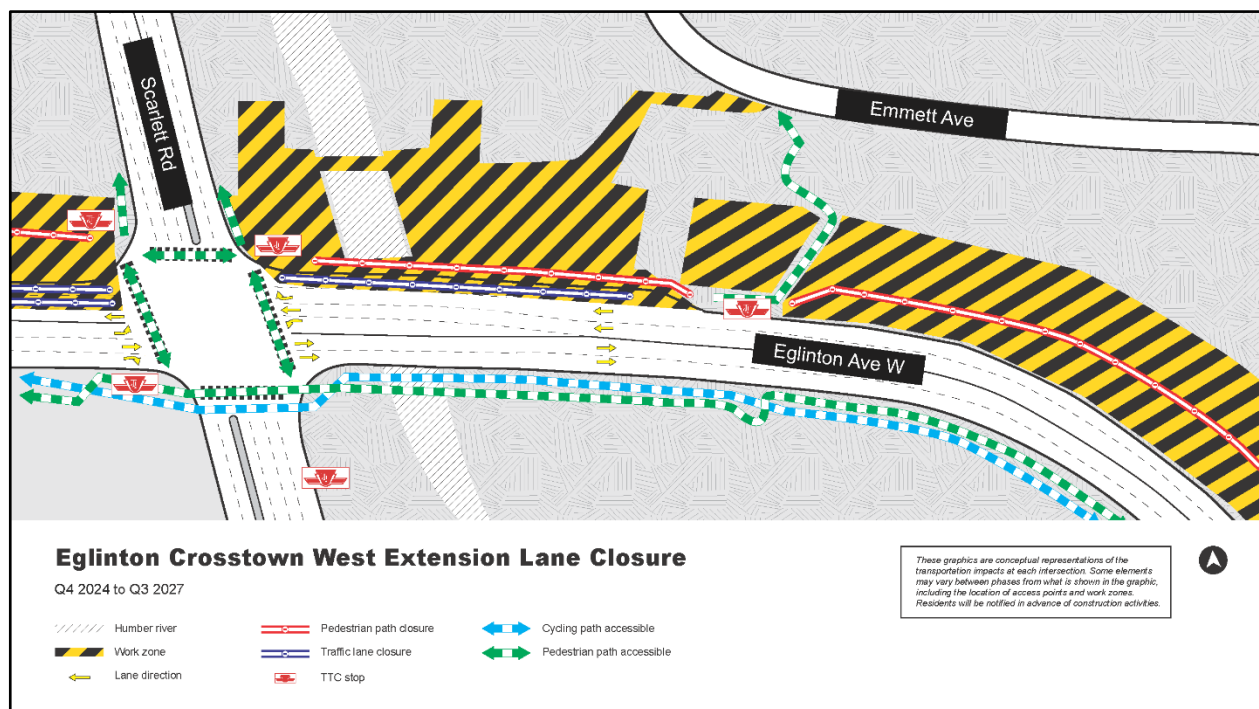


Figure 34 – ECWE Elevated Guideway Work Site

Source: Metrolinx

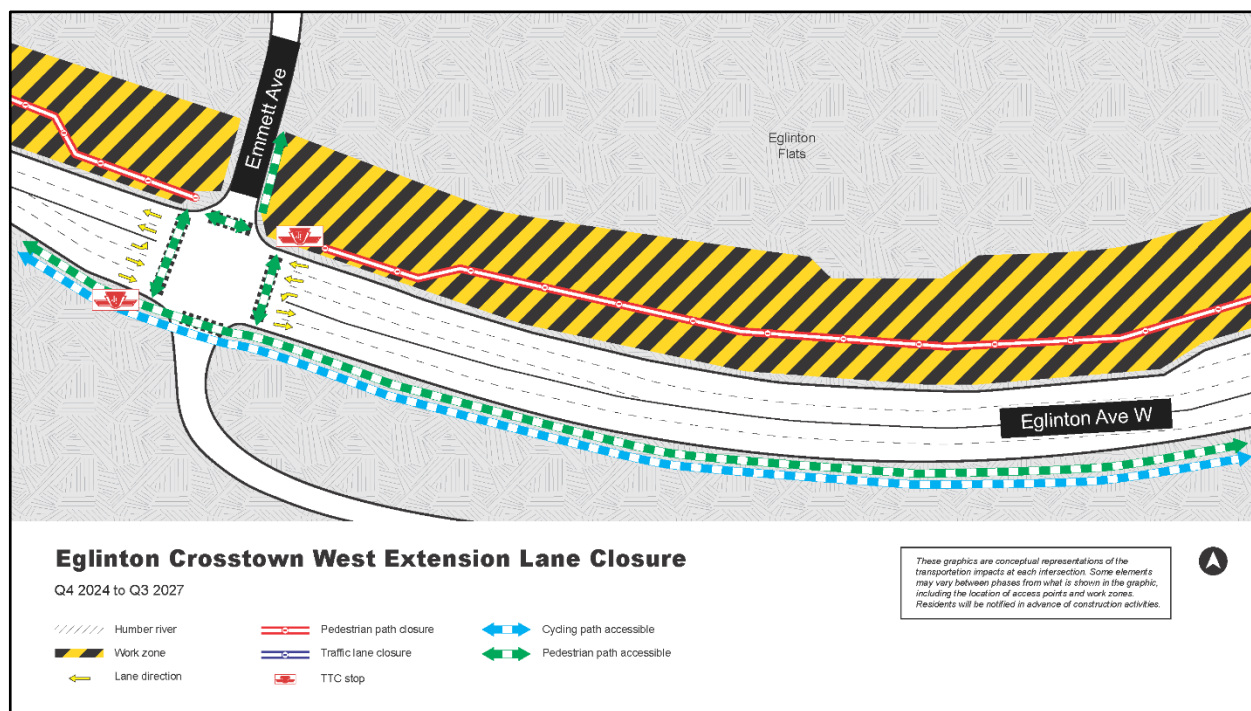


Figure 35 – ECWE Elevated Guideway Work Site

Source: Metrolinx

As shown in Figures 34 and 35, the impacts of the Elevated Guideway construction include the following:

- Temporary closure of the north sidewalk on Eglinton Avenue West, between Scarlett Road and a point 230 metres east, from October 1, 2024 to September 30, 2027.
- Temporary closure of the north sidewalk on Eglinton Avenue West, between a point 275 metres east of Scarlett Road and Emmett Avenue, from October 1, 2024 to September 30, 2027.
- Temporary closure of the north sidewalk on Eglinton Avenue West, between Emmett Avenue and Jane Street, from October 1, 2024 to September 30, 2027.
- Temporary closure of the westbound curb on Eglinton Avenue West, between Scarlett Road and a point 230 metres east, from October 1, 2024 to September 30, 2027.

The impacts of the Elevated Guideway construction are discussed below:

Pedestrian Impacts

The construction work for the Elevated Guideway will require closure of the north sidewalk on Eglinton Avenue, between Scarlett Road and Jane Street. The south sidewalk will remain available to pedestrians. Appropriate advanced signage will be positioned at strategic locations to ensure pedestrians are informed of this temporary sidewalk closure. In addition, warning signage will be posted at the signalized

intersections of Eglinton Avenue West at Scarlett Road, Eglinton Avenue West at Emmett Avenue, and Eglinton Avenue West at Jane Street.

Additionally, Metrolinx will maintain the existing pedestrian connectivity between Emmett Avenue and the existing TTC bus stop on Eglinton Avenue West.

Cycling Impacts

During the construction of the Elevated Guideway, Metrolinx will maintain the south side MUP on Eglinton Avenue West, between Scarlett Road and Jane Street.

Auto Access Impacts

Metrolinx requires closure of the westbound curb lane on Eglinton Avenue West, between Scarlett Road and a point 230 metres east for approximately three years to facilitate construction of the Elevated Guideway. With the lane closure in place, Metrolinx will maintain two eastbound and two westbound through lanes in the mid-block section of Eglinton Avenue, between Scarlett Road and Jane Street.

Transit Impacts

Existing TTC transit service will be generally maintained, although construction activity reducing intersection and road capacity may slow down transit operations for customers, especially during busy peak times. Bus stops will also generally be maintained, and any relocations will be coordinated with the TTC.

Property Access Impacts

Metrolinx will maintain all property and business accesses during the construction works.

4.3. Jane Street to east of Weston Road

The section of Eglinton Avenue West, between Jane Street and Weston Road, will experience construction activities related to the Elevated Guideway, Launch Shaft, Jane Tunnel Portal, Cross Passage, and Extraction Shaft at the Mount Dennis Station.

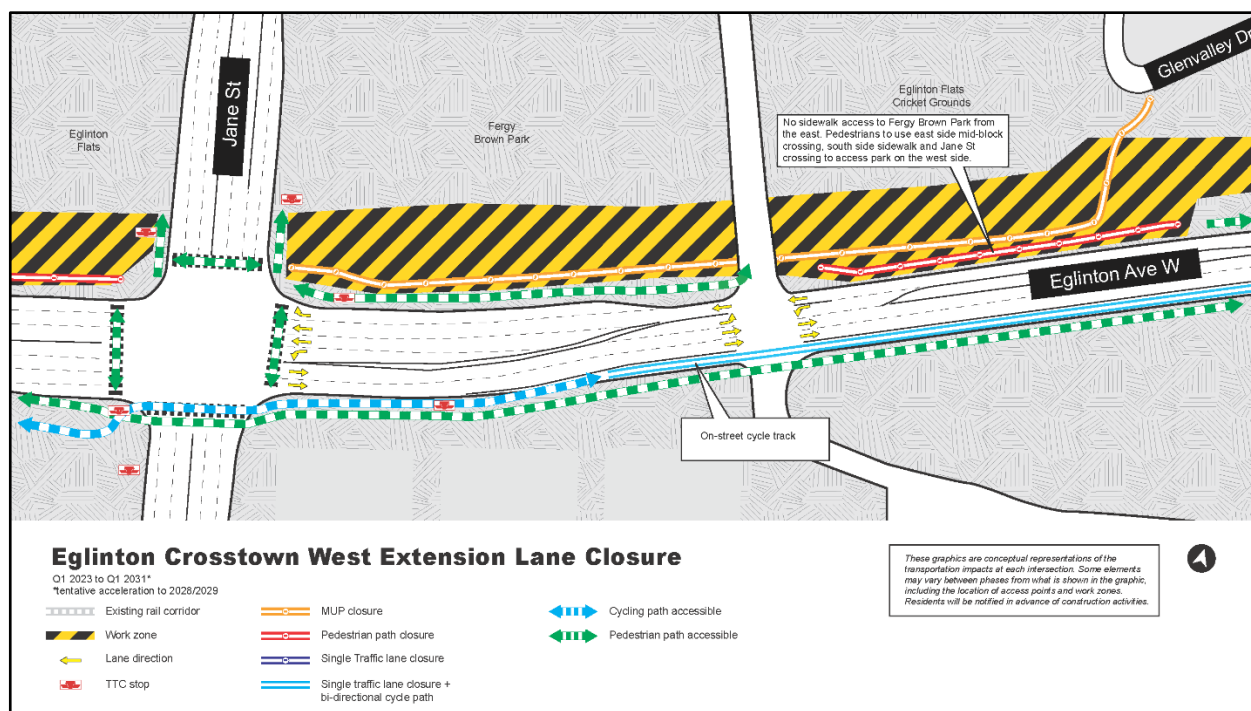


Figure 36 – ECWE Elevated Guideway, Launch Site and Jane Tunnel Portal Work Sites

Source: Metrolinx

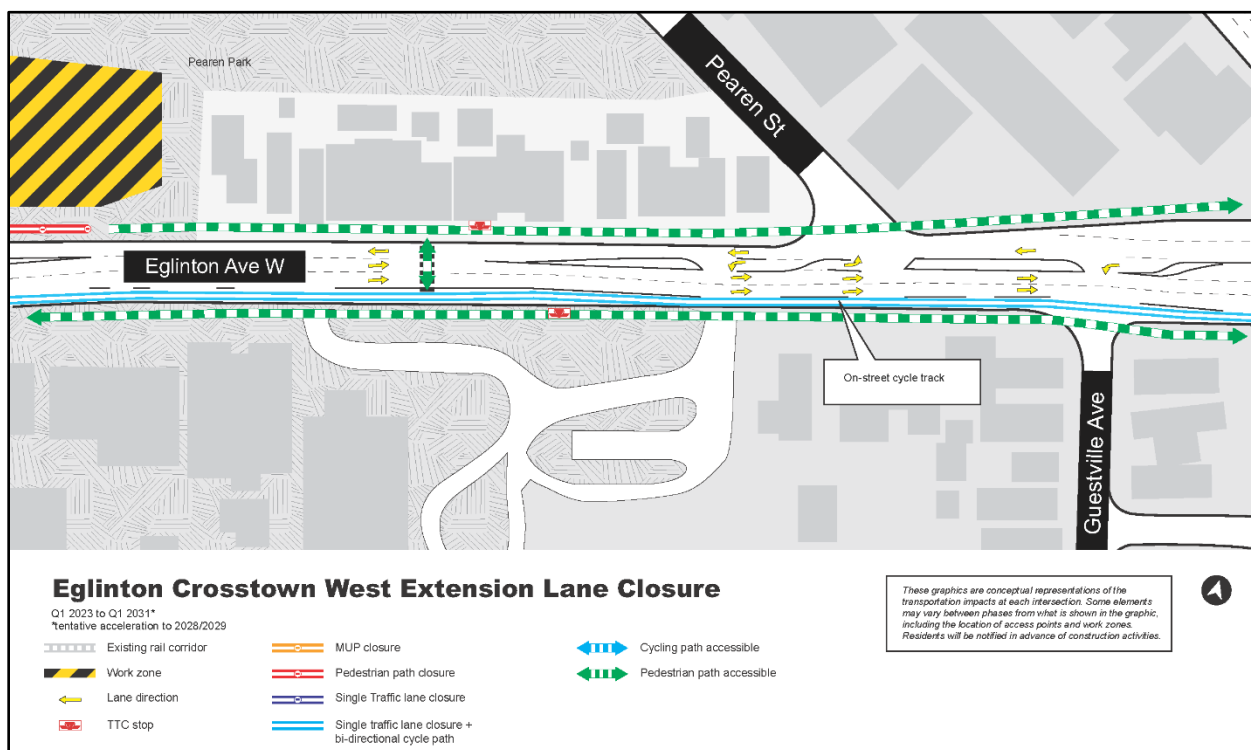


Figure 37 – ECWE Elevated Guideway, Launch Site and Jane Tunnel Portal Work Sites

Source: Metrolinx

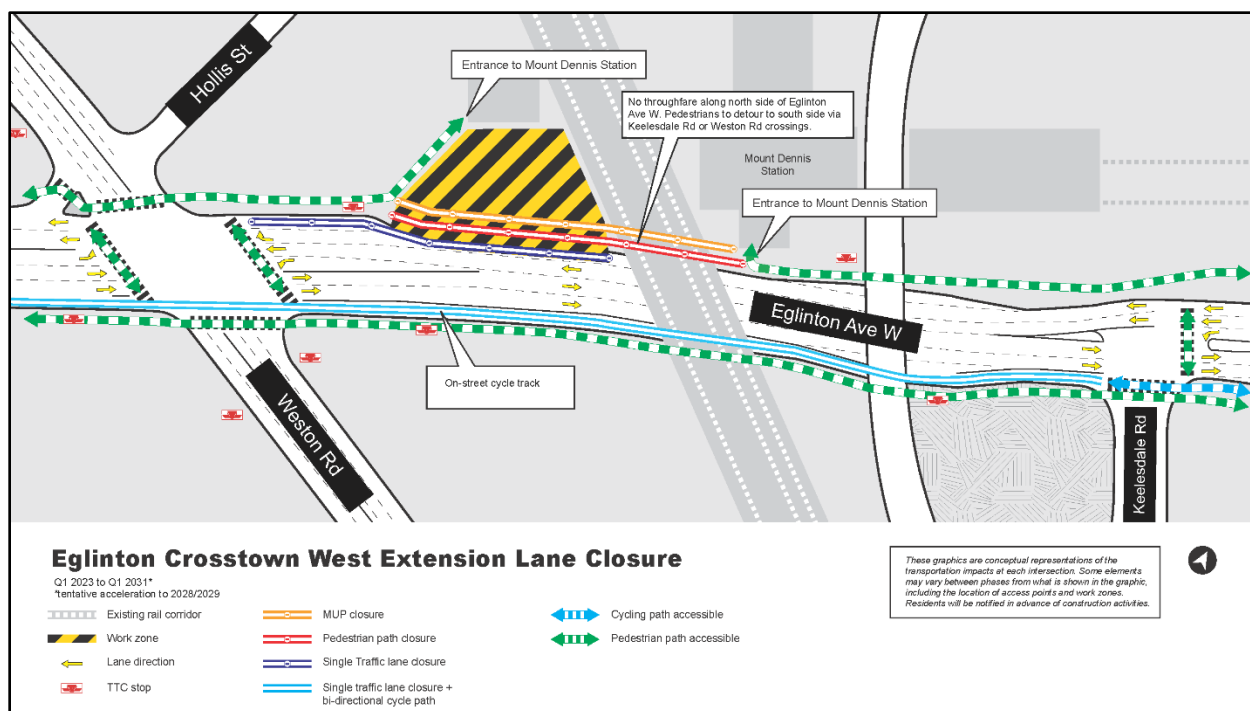


Figure 38 – ECWE Work Site at Mount Dennis Station

Source: Metrolinx

As shown in Figures 36, 37 and 38, the construction impacts of the Elevated Guideway, Launch Site, Jane Tunnel Portal, Cross Passage and Extraction Shaft at the Mount Dennis Station include the following:

- Temporary closure of the north sidewalk on Eglinton Avenue West, between a point 170 metres west of Pearen Street and a point 230 metres further west, from January 1, 2023 to December 31, 2030.
- Temporary closure of the north side multi-use trail on Eglinton Avenue West, between Jane Street and Glenvalley Drive, from January 1, 2023 to December 31, 2030.
- Temporary closure of the westbound curb lane on Eglinton Avenue West, between a point 120 metres east of Jane Street and Weston Road, from January 1, 2023 to December 31, 2030.
- Temporary closure of the north sidewalk and multi-use trail on Eglinton Avenue West, between Weston Road and a point 250 metres east, from January 1, 2023 to December 31, 2030.
- Temporary closure of the westbound curb lane on Eglinton Avenue West, between Weston Road and a point 180 metres east, from January 1, 2023 to December 31, 2030.
- Temporary closure of the eastbound curb lane on Eglinton Avenue West, between a point 90 metres east of Jane Street and Keele Road, from January 1, 2023 to December 31, 2030.

The impacts of the construction activities are discussed below:

Pedestrian Impacts

The construction work for the Elevated Guideway and Jane Tunnel Portal on the north side of Eglinton Avenue West will require a temporary closure of approximately 60 metres of the north sidewalk on Eglinton Avenue West, east of the Fergy Brown Park access, to facilitate construction of the Elevated Guideway and Jane Tunnel Portal.

Additionally, a portion of the north sidewalk on Eglinton Avenue West, between Weston Road and a point 250 metres east will be closed to facilitate Extraction Shaft work at the Mount Dennis Station. During this, Metrolinx will maintain a pedestrian walkway to ensure connectivity to the Mount Dennis Station, when it becomes operational.

These sidewalk closures are required for approximately 7 years. The Metrolinx contractor will install advanced advisory signage at strategic locations to inform pedestrians of the temporary sidewalk closures. In addition, warning signage will be installed at the nearest signalized intersections and pedestrian crossings.

The south sidewalk on Eglinton Avenue, between Jane Street and Black Creek Drive will be maintained during the construction work.

Cycling Impacts

The multi-use trail on the north side of Eglinton Avenue West, between Jane Street and Glenvalley Drive, will be closed for approximately 7 years to facilitate the ECWE construction works. A temporary protected on-street cycle track will be maintained in the eastbound curb lane on Eglinton Avenue West, between Jane Street and Keelesdale Road.

Auto Access Impacts

The construction works will require closure of the westbound curb lane on Eglinton Avenue West, between a point 120 metres east of Jane Street and a point 180 metres east of Weston Road. Additionally, the eastbound curb lane on Eglinton Avenue, between a point 90 metres east of Jane Street and Keelesdale Road will be required to accommodate an on-street cycle track. These lane closures will be required for approximately 7 years.

With the lane closures in place, Metrolinx will realign the traffic lanes to maintain two eastbound and one westbound lane on Eglinton Avenue, between Jane Street and Keelesdale Road.

Transit Impacts

Existing TTC transit service will be generally maintained, although construction activity reducing intersection and road capacity may slow down transit operations for customers, especially during busy peak times. Bus stops will also generally be maintained, and any relocations will be coordinated with the TTC.

Property Access Impacts

Metrolinx will maintain all property and business accesses during construction.

4.4. Construction Impacts Mitigation

Metrolinx and the Project Co. will utilize traffic management strategies to minimize the impact of the ECWE works on network-wide traffic congestion and maintain safety and mobility of all road users in the work zones. City staff will coordinate with Metrolinx and the Project Co. to implement site specific safe work zone accommodations.

With technical input from Metrolinx, the City will implement traffic signal timing modifications to minimize impacts on the boundary road network. Furthermore, frequent monitoring of the traffic operations will be undertaken to ensure modifications are optimized and reflect any changing conditions, for example, due to observed issues or a new or revised work zone.

The Project Co. will install advanced advisory signage including portable variable message signs at the network level to inform road users of the road closures. Additionally, road closure information will be provided through the project website, social media and traditional media to encourage alternative routes or travel schedules. The Project Co. will submit the Road Disruption Activity Reporting System (RoDARS) notification and inform the web-based network service providers such as Google and Waze etc. of the road closures.

In order to encourage alternative modes of travel and reduce auto trips, Metrolinx will maintain existing transit service and transit stops in the construction zone. Additionally, safe cycling connectivity will be maintained in the work zones.

Metrolinx and the Project Co. will install advanced advisory signage to guide the bicyclists in the work zones. Additionally, all designated bicycle lanes in the work zones will be kept free of dust, debris and ice.

The ECWE works will maintain safe pedestrian connectivity in the work zones. In the event of a sidewalk closure, the Project Co. will install advanced advisory signage to safely direct the pedestrians to alternative sidewalk at the nearest signalized crossings. The Project Co. will ensure pedestrian walkways in the work zones are kept free of dirt, loose material, snow and ice and ensure that accessibility standards are met.

At the construction sites, site accesses and the hoarding will be designed to ensure clear sightlines for the truck drivers and pedestrians and traffic control persons will be positioned to avoid any conflicts between the construction vehicles and pedestrians. The Project Co. will prepare a construction vehicle haul route plan, which will be reviewed by the City staff to ensure the impacts on the traffic flow can be adequately mitigated and that pedestrian and cycle safety is given the highest priority.

The haul trucks will not be allowed to stage on the City roads and vehicle idling will be discouraged and enforced in accordance with the City's Idling Control By-law.

Additionally, Metrolinx and the Project Co. will encourage site construction workers to take transit to the work sites. This will reduce traffic congestion and additional demand on the nearby parking facilities. Private vehicle parking will not be allowed in the work zones.

Finally, in accordance with the Toronto Municipal Code Chapter 743, Metrolinx contractors will obtain a permit before undertaking any street work or temporary street occupation within the City road right-of-way. As part of the permit application, the Project Co. will need to prepare and submit traffic control plans that are compliant with the City and Provincial policies and standards. These plans will be carefully reviewed by the City staff to ensure safety and mobility needs of the travelling public, businesses and community are met. Once implemented, City staff, in coordination with Metrolinx and Project staff, will monitor the installation to ensure its compliance with the approved traffic control plan. In the event the permit holder violates the terms and conditions of the permit, the issued permit may be cancelled in accordance with authority given by Chapter 743.

4.5. Communication and Stakeholder Engagement

Metrolinx will implement a communication plan to actively engage the area residents, businesses, and elected officials, provide construction updates, address concerns and minimize disruptions during the ECWE construction works.

Metrolinx's Communication Plan will include the following:

- A designated project website and other information portals such as social media, e-newsletter, construction notices, public meetings with details on construction activities, schedule, impact and mitigation;
- Area-wide communication regarding traffic impacts and mitigation within the area next to the construction sites and wider geographic area with anticipated traffic and community impacts;
- 24/7 communication contacts during the project construction;
- A Complaints Protocol for timely response to complaints; and,
- Establishment of Construction Liaison Committee (CLC) and Traffic Management Committee (TMC).

Metrolinx's project website will provide updates on the construction schedule, activities, impacts and mitigation. Additionally, Metrolinx will share construction and traffic staging information through social media channels, weekly e-newsletters, public meetings and CLCs.

To address project-related questions or concerns, Metrolinx will provide an appropriately staffed 24-hour phone number and email address to enable residents and businesses to call and report their concerns directly to the Metrolinx contractor. The dedicated phone number and email address information will be shared with the Toronto 311 service and on the project website.

Metrolinx will establish a complaint protocol to address concerns from residents and businesses during the construction. Any complaints received through the City 311

service or through the local councillor's office will be directed to this dedicated email or phone number. The complaints will be logged and resolved by Metrolinx and their contractor project team in collaboration with City staff.

During the construction of the ECWE project, Metrolinx will establish a CLC consisting of local Councillor's office staff, businesses, residents, etc. At monthly meetings, the CLCs will discuss project updates, advanced information on upcoming project activities, including road closures, and will identify and address any stakeholder concerns. Additionally, there will be bi-weekly Traffic Management Committee (TMC) meetings for the ECWE works, attended by the City's Transportation Services Division, City Emergency Services, Toronto Police, TTC, Metrolinx and their contractor. The TMC will review the traffic impacts of the ECWE construction work and coordinate on implementation of mitigation measures for the safety of road users and the community.

Finally, Metrolinx will continue with its one-on-one briefings with the local Councillors to address specific milestones and emerging issues. City staff will also hold timely one-on-one meetings with the local Councillors to provide information and advice on concerns related to the project. Briefings will be provided prior to any significant road closure required for project construction work. Further, City staff will liaise with Metrolinx and Project Co. to minimize disruption during construction activities.

6. Building Transit Faster Act

It is important to note that pursuant to certain provisions in the *Building Transit Faster Act* (BTFA), Metrolinx can, on notice, require for construction of a priority transit project:

- Use, occupation, modification, temporary closure of a municipal highway or “municipal right of way”; and
- Use of, access to or modification of sewage works, water works or fire hydrants under municipal ownership or control, and “municipal services” related to this infrastructure.

Under BTFA, the Province requires Metrolinx to make reasonable efforts to reach agreement with the municipality for the municipal service and right of way access. If Metrolinx and the municipality are not able to agree on the terms even though Metrolinx made reasonable efforts to reach agreement, the Minister of Transportation may develop a municipal service and right of way order, which may include terms such as compensation. The order may require the municipality to provide the municipal service and right of way access set out in the order, and set terms governing Metrolinx and the municipality in respect of the municipal service and right of way access.

Under the BTFA, priority transit projects means the SSE, ECWE, OL and YNSE and any other prescribed provincial transit project.

As such, should the Province exercise its legislative powers through a Minister's Order, the City's process for municipal service and right of way access, including its ability to impose conditions via a permit (e.g. temporary street occupation permit or street work permit) will be by-passed.

6. Subways Program - Agreement-in-Principle Update

Since 2021, staff from the City's Transit Expansion Division have been working with counterparts at the Ministry of Transportation (MTO) to advance a Subways Program Agreement-in-Principle (AIP). The AIP is intended to build on the Ontario-Toronto Transit Partnership Preliminary Agreement²⁰. The AIP further details the roles, responsibilities and obligations of the Province and the City with regards to the Subways Program. Efforts to finalize the AIP terms for City Council's approval is on hold due to the provincial writ period and the forthcoming municipal election. Staff will seek City Council approval of the finalized AIP at the first opportunity during the next term of City Council, likely January 2023.

The AIP will also seek to address a number of City Council directions regarding the Subways Program, including community benefits, business supports, and construction mitigation matters. City staff are using lessons learned and best practices from the implementation of the Eglinton Crosstown LRT to guide negotiations on the AIP.

The Province continues to reiterate its commitment to ensuring its Subways Program benefits local communities and residents, mitigates the impacts and duration of construction to the extent possible, and ensures businesses are supported throughout the construction period. To that end, Attachment 4 provides Metrolinx's summary of its approach to community benefits/supports.

7. Metrolinx Transit Projects – City's Fair Wage Policy

In order to facilitate effective construction coordination and reduce the duration, frequency and depth of impact on communities during construction, the City works in consultation with Metrolinx to determine which of the City's planned capital projects can be effectively undertaken by Metrolinx as part of Metrolinx's construction of transit projects. Where the City requests Metrolinx undertake these capital projects on behalf of the City, often referred to as "Additional Infrastructure", Metrolinx determines, in accordance with the relevant Master Agreement between itself and the City, if it is feasible and willing to take on this work. Subject to agreement by Metrolinx and the City, both parties then enter into a Municipal Infrastructure Agreement outlining roles, responsibilities, and obligations.

While the City typically delivers capital projects directly, there are circumstances where delivery by Metrolinx is more effective and less intrusive on impacted communities. In order to facilitate this Additional Infrastructure, this report seeks City Council authority to exempt Metrolinx from the City's Fair Wage Policy (Policy) requirements for the following three transit projects: the Subways Program, the Light Rail Transit Program, and the GO Expansion Program.

The Policy is intended to ensure that the City does business with contractors, sub-contractors and suppliers who do not discriminate against their workers. The Policy is a long standing City policy that is intended to ensure that workers that are employed by

²⁰ <https://www.toronto.ca/wp-content/uploads/2020/02/94f1-02-12-2020-Final-Prelim-Agreement-Ontario-Toronto-Transit-Partnership-AODA.pdf>

the City's contractors are paid appropriately. The Policy empowers the Fair Wage Office to set minimum pay rates (and certain benefits and working conditions) for work across a broad range of industries. In the construction industry, the Policy works in conjunction with the Labour Trades Contractual Obligations in the Construction Industry Policy²¹. The Fair Wage Policy is broad in scope and plays an important role in assisting the City to ensure fairness for workers on City-tendered projects.

As the Fair Wage Policy is a City policy codified as a by-law in the Municipal Code, it is not binding on Metrolinx. As a result, Metrolinx has notified the City that it is unable to abide by the City's Fair Wage Policy when delivering Additional Infrastructure, thereby potentially jeopardizing the City's ability to use the Additional Infrastructure tool to best coordinate its capital projects with Metrolinx's project construction. In addition, as Metrolinx undertakes the vast majority of its projects using a public-private partnership (P3) model, Metrolinx has advised that abiding by the Policy would require Metrolinx to offer the same provisions for contractors working on Metrolinx projects as those working on the City's Additional Infrastructure works. Metrolinx has indicated that this could potentially increase the cost to Metrolinx when undertaking its own projects and also unnecessarily create additional challenges in managing complex contracts.

Should the City not waive Fair Wage Policy requirements for Metrolinx, Metrolinx would not undertake Additional Infrastructure work on the City's behalf. This may result in the following negative implications for the City:

- Create ineffective construction coordination and may require the City to interface with Metrolinx's contractors directly in the same vicinity;
- Delay crucial capital work;
- Require multiple construction disruptions in short succession thereby impacting the same local residents, businesses and streets, and extending disruption to traffic in the same area for a longer duration; and,
- Potentially increase pricing for the work in question due to the inefficiency caused by tendering separately rather than through one contract.

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²¹ <https://www.toronto.ca/wp-content/uploads/2017/07/906e-labour-trades-contractual-obligations.pdf>

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ATTACHMENTS

Confidential Attachment 1 – 1269 Danforth Avenue

Attachment 1 – SSE Eglinton Ave. East Temporary Diversion Road Drawing

Attachment 2 – City Council Decision History

Attachment 3 – Metrolinx's approach to Indigenous engagement at the First Parliament and future Corktown Station sites

Attachment 4 – Metrolinx's Community Benefits/Supports Approach