

545 Lake Shore Boulevard West Zoning Amendment – Final Report

Date: April 1, 2022

To: Toronto and East York Community Council

From: Director, Community Planning, Toronto and East York District

Ward: 10 - Spadina-Fort York

Planning Application Number: 18 241642 STE 20 OZ

SUMMARY

This application proposes to amend the Zoning By-law to permit a mixed-use development with residential, office and retail uses. There is a designated heritage building on the north portion of the site known as the Crosse and Blackwell building that is proposed to be conserved and re-used for office purposes. Residential and retail uses are proposed to be located in a new 13 and 21-storey building on the south portion of the site. The proposal includes a total of 412 dwelling units, 8,938 square metres of office space, 146 vehicle parking spaces and 468 bicycle parking spaces. A new 70 square metre Privately-Owned and Publicly-Accessible Open Space (POPS) is proposed at the southwest corner of the site.

The proposed development is consistent with the Provincial Policy Statement (2020) and conforms with A Place to Grow: Growth Plan for the Greater Golden Horseshoe (2020), and conforms with the Official Plan and Central Waterfront Secondary Plan.

The proposal conserves a significant heritage building, presents an appropriate built form that is compatible with the surrounding context and proposes public realm enhancements in keeping with the Official Plan and the Central Waterfront Secondary Plan. This report reviews and recommends approval of the proposed Zoning By-law Amendment.

RECOMMENDATIONS

The City Planning Division recommends that:

1. City Council amend the Zoning By-law, for the lands at 545 Lake Shore Boulevard West substantially in accordance with the draft Zoning By-law Amendment attached as

Attachment No. 10 to the report from the Director, Community Planning, Toronto and East York District dated April 1, 2022.

2. City Council authorize the City Solicitor to make such stylistic and technical changes to the draft Zoning By-law Amendment as may be required.

3. Before introducing the necessary Bills to City Council for enactment, require the owner to enter into an Agreement pursuant to Section 37 of the Planning Act as follows:

a) The community benefits recommended to be secured in the Section 37 Agreement are as follows:

i. At such time as the By-law comes into full force and effect, the owner shall make a cash contribution of \$500,000.00 to be allocated towards the provision of affordable housing in the Housing Now development at 150 Queens Wharf Road;

ii. Prior to the issuance of the first above-grade building permit for development on Block A as identified in the Zoning By-law Amendment, the owner shall make a cash contribution of \$690,000.00 to be allocated towards the provision of affordable housing in the Housing Now development at 150 Queens Wharf Road;

iii. Prior to the issuance of the first above-grade building permit for development on Block B as identified in the Zoning By-law Amendment, the owner shall make a cash contribution of \$2,310,000.00 to be allocated towards the provision of affordable housing in the Housing Now development at 150 Queens Wharf Road;

iv. All cash contributions referred to in Recommendation 3(a)i-iii, shall be indexed upwardly in accordance with the Statistics Canada Construction Price Index for Toronto, calculated from the date of the execution of the Section 37 Agreement to the date the payment is made;

v. In the event the cash contributions referred to in Recommendation 3(a)i-iii. have not been used for the intended purpose within three (3) years of the By-law coming into full force and effect, the cash contribution may be redirected for another purpose, at the discretion of the Chief Planner and Executive Director, City Planning, in consultation with the Ward councillor, provided that the purpose is identified in the Toronto Official Plan and will benefit the community in Ward 10;

vi. The owner shall construct, provide and maintain a Privately-Owned and Publicly-Accessible Open Space ("POPS") at 545 Lake Shore Boulevard West with a minimum area of 70 square metres at the southwest corner of the property in the location generally identified in the Zoning By-law Amendment, with the specific configuration and design of the POPS to be

determined and secured in the context of Site Plan Approval for the development to the satisfaction of the Director, Community Planning, Toronto and East York District, in consultation with the Ward Councillor; and

vii. Prior to the earlier of one (1) year from any non-residential or residential use or occupancy, including interim occupancy pursuant to the Condominium Act, 1998, and registration of the first condominium for the portion of the site identified as Block B in the Zoning By-law Amendment, except as otherwise agreed by the Chief Planner and Executive Director, City Planning, due to unforeseen delays (eg. weather), the owner shall prepare all documents and convey to the satisfaction of the Chief Planner and Executive Director, City Planning and the City Solicitor, free and clear of encumbrances, for nominal consideration and at no cost to the City, a public access easement in perpetuity in favour of the City over the privately-owned publicly accessible open space set out in Recommendation 3.vi. on terms set out in the Section 37 Agreement satisfactory to the Chief Planner and Executive Director, City Planning. The owner shall be responsible, at its own expense, to prepare, submit to the City for approval and deposit all required reference plans to describe the easement.

b) The following matters are also recommended to be secured in the Section 37 Agreement as a legal convenience in support of the development:

i. the owner shall prepare all documents and convey, a 2.1 metre wide public pedestrian access easement to the City, in a location generally described as a north/south access over the eastern portion of the site between Queens Quay West and Lake Shore Boulevard West, on terms to be set out in the Section 37 Agreement and with details of the location and final design to be determined and secured in the context of site plan approval for the development and that such easement lands be maintained by the owner at its sole cost. The conveyance of the easement shall be free and clear of encumbrances, for nominal consideration and at no cost to the City all to the satisfaction of the Chief Planner and Executive Director, City Planning and the City Solicitor. The owner shall be responsible, at its own expense, to prepare, submit to the City for approval and deposit all required reference plans to describe the easement;

ii. the owner shall prepare all documents and convey a public pedestrian access easement to the City, in a location generally described as an east/west connection from Bathurst Street to the central open space courtyard and to the north/south access over the eastern portion of the site referred to in Recommendation 3b.i., on terms set out in the Section 37 Agreement and with details of the location and final design to be determined and secured in the context of site plan approval for the

development and that such easement lands be maintained by the owner at its sole cost. The conveyance of the easement shall be free and clear of encumbrances, for nominal consideration and at no cost to the City all to the satisfaction of the Chief Planner and Executive Director, City Planning and the City Solicitor. The owner shall be responsible, at its own expense, to prepare, submit to the City for approval and deposit all required reference plans to describe the easement;

iii. provisions to require that, in the event of a future severance of the property, the property owners shall enter into an agreement with the City which may take the form of a Limiting Distance Agreement and will be on terms satisfactory to the City Solicitor, which will have the effect of precluding the construction or extension of any building above the heritage building, on the property identified as Block A in the Zoning By-law Amendment;

iv. prior to the commencement of any excavation or shoring work, the owner shall submit a Construction Management Plan to the satisfaction of the General Manager of Transportation Services and the Chief Building Official and Executive Director, Toronto Building, in consultation with the Ward Councillor and thereafter in support of the development, will implement the Plan during the course of construction. The Construction Management Plan will include, but not be limited to the following construction-related details: noise, dust, size and location of staging areas, location and function of gates, dates of significant concrete pouring, lighting details, vehicular parking and queuing locations, street closures, parking and access, refuse storage, site security, site supervisor contact information, and a communication strategy with the surrounding community, and any other matters requested by the General Manager, Transportation Services, in consultation with the Ward Councillor;

v. provisions that require, prior to the commencement of any demolition, excavation and shoring work, and prior to any building permit the owner shall provide required easements and technical reports to the City for the purposes of reconstruction, maintenance, repair, protection and monitoring of the Garrison Creek Stormwater Sewer that traverses the site to the satisfaction of the Chief Engineer and Executive Director, Engineering and Construction Services, as set out in Attachment 11 to the report dated April 1, 2022, from the Director, Community Planning, Toronto and East York District; and

4. Before introducing the necessary Bills to City Council, require the applicant to address all outstanding comments in Section A of the memo from Engineering and Construction Services dated March 30, 2022 in relation to servicing matters, and any subsequent memo prepared in response to applicant resubmission(s) to the satisfaction of the Chief Engineer and Executive Director, Engineering and Construction Services.

5. Direction be given to the Chief Building Official that City Council hereby implements Site and Area Specific Policy 181, Policy d) set out in Section 7 of the Toronto Official Plan, and exempts the proposed development at 545 Lake Shore Boulevard West set out in the report of the Director, Community Planning, Toronto and East York District, dated April 1, 2022, from the requirement to convey land for park purposes as a condition of development as contemplated in Article III of Chapter 415 of the Toronto Municipal Code, as amended.

FINANCIAL IMPACT

The City Planning Division confirms that there are no financial implications resulting from the recommendations included in this report in the current budget year or in future years.

DECISION HISTORY

On June 29, 2018 City Council adopted Official Plan Amendment 417 to implement the findings of the Bathurst Quay Neighbourhood Plan Study. Council's decision and Official Plan Amendment 417 may be found here:

<http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2018.TE33.9>

A Preliminary Report on the application was adopted by Toronto and East York Community Council on April 24, 2019. Toronto and East York Community Council's decision may be found here:

<http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2019.TE5.34>

On December 15, 2021 City Council stated its intention to designate the property at 545 Lake Shore Boulevard West under Part IV, Section 29 of the Ontario Heritage Act. Council's decision may be found here:

<http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2021.CC38.16>

On March 9, 2022, City Council approved the alterations to the heritage property at 545 Lake Shore Boulevard West to allow for the construction of a two-storey addition to the heritage building. Council's decision may be found here:

<http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2022.TE31.14>

SITE AND SURROUNDING AREA

The site is located on the east side of Bathurst Street and extends the full block between Lake Shore Boulevard West to the north and Queens Quay West to the south. See Attachment No. 1 for the Location Map. Currently, the Crosse and Blackwell heritage building is situated on the north portion of the site with frontages along Lake

Shore Boulevard West and Bathurst Street, and a surface parking lot with 100 parking spaces is situated on the south portion of the site. The existing building is currently used as a City respite centre and the parking lot is operated by the Toronto Parking Authority.

The surrounding uses include:

North: Opposite the site on the northeast corner of Lake Shore Boulevard West and Bathurst Street in the restored Loblaws heritage building and 37 and 41 storey residential towers recently constructed at 500 Lake Shore Boulevard West. A 29 storey residential building is located immediately east of 500 Lake Shore Boulevard West, at the northwest corner of Dan Leckie Way and Lake Shore Boulevard West.

South: Opposite the site on the southeast corner of Eireann Quay and Queens Quay West is the co-located Waterfront Neighbourhood Centre, the Waterfront and City schools and the St. Stephens Waterfront child care centre. Further east on the south side of Queens Quay West are the Portland Slip, Marina Quay and the Music Garden. Further south on Eireann Quay is the Billy Bishop Toronto City Airport pedestrian tunnel building, the Canada Malting Silos and the Corlick building at 5 Eireann Quay. Little Norway Park is situated on the southwest corner of Eireann Quay and Queens Quay West.

West: Opposite the site is a 16 storey residential building at the northwest corner of Bathurst Street and Queens Quay West. A gas station is situated on the southwest corner of Bathurst Street and Lake Shore Boulevard West. Further west are residential buildings ranging from 3 to 23 storeys in height.

East: Two 11-storey residential buildings are situated adjacent to the east. Further east are 11 to 12 storey residential buildings along the north side of Queens Quay West extending to Spadina Avenue.

THE APPLICATION

The Proposal

The application proposes a mixed-use development with residential, office and retail uses. The existing Crosse and Blackwell heritage building situated on the north portion of the site is proposed to be conserved and used for office purposes. On the south portion of the site, a new residential building 21 and 13 storeys and retail uses on the ground floor, is proposed. A centrally located courtyard is proposed between the office building and the new residential building with a pedestrian walkway access to the courtyard proposed from Bathurst Street. A total of 412 dwelling units and 8,938 square metres of office space are proposed. A two level below grade parking garage is proposed on the south portion of the site. In total 146 vehicle parking spaces and 468 bicycle parking spaces are proposed. A 70 square metre privately-owned publicly accessible open space is proposed at the northeast corner of Bathurst Street and

Queens Quay West and a pedestrian walkway is proposed along the east boundary of the site extending between Queens Quay West and Lake Shore Boulevard West.

Detailed project information can be found on the City's Application Information Centre at: <https://www.toronto.ca/city-government/planning-development/application-information-centre>.

See Attachment No. 2 for the Application Data Sheet, Attachment No. 4 for the Site Plan and Attachment Nos. 6 to 9 for the Building Elevations.

	Original Proposal (October 2018)	1st Revision (July 2021)	2nd Revision (December 2021)
Heritage Building	Partial Conservation	Full Conservation	Full Conservation
Height – Storeys (metres)	39 (133) 15 (60) 10 (38)	22 (72) 12 (40)	21 (68.75) 13 (43.7)
Total GFA	54,704 m ²	37,509 m ²	38,479 m ²
Residential GFA	37,754.49 m ²	27,624 m ²	28,733 m ²
Office GFA	12,572 m ²	8986 m ²	8,938 m ²
Retail GFA	3,846 m ²	899 m ²	808 m ²
Density	7.1 times site area	5.1 times site area	5.25 times site area
Dwelling Units	508	401	412
Vehicle Parking	243	144	146
Bicycle Parking	609	466	468

Summary of Revisions to Proposal

The application has been revised since the initial submission and the changes to the proposal are summarized in the table below.

Application Submission Requirements

The following reports/studies were submitted in support of the application:

- Survey Plans
- Architectural Plans, Elevations and Sections
- Landscape Plans
- 3D-Modelling
- Heritage Impact Statement
- Archaeological Report
- Arborist Report
- Tree Preservation Plan
- Sun/Shadow Study
- Planning Rationale Report
- Community Services and Facilities Study

- Pedestrian Level Wind Study
- Transportation Impact Study
- Draft Zoning By-law Amendment
- Toronto Green Standard Checklist
- Energy Efficiency Report
- Noise Impact Study
- Vibration Study
- Servicing Report
- Storm Water Management Report
- Environmental Impact Study Phase 1 and Phase 2
- Complete Community Assessment
- Aviation Review

The information submitted with the application can be found here:

<https://www.toronto.ca/city-government/planning-development/application-information-centre>.

Reasons for Application

The proposal requires an amendment to Harbourfront Zoning By-law 289-93 to vary performance standards including: an increase in height, density, permitted uses, setbacks, amenity space, and parking and loading standards.

Site Plan Control

The proposal is subject to Site Plan Control. Two Site Plan Control Applications were submitted on September 22, 2021. These include an application for the north portion of the site which contains the office building (File No. 21 214505 STE 10 SA) and an application for the south portion of the site that contains the residential and retail building (File No. 21 214507 STE 10 SA).

Draft Plan of Condominium

A draft plan of condominium application was submitted on November 2, 2021 (File No. 21 233007 STE 10 CD).

Agency Circulation Outcomes

The application together with the applicable plans and reports noted above, have been circulated to all appropriate agencies and City Divisions. Responses received have been used to assist in evaluating the application and to formulate appropriate Zoning By-law standards.

POLICY CONSIDERATIONS

Planning Act

Section 2 of the Planning Act sets out matters of provincial interest which City Council shall have regard to in carrying out its responsibilities, including but not limited to the following:

- adequate provision and efficient use of communication, transportation, sewage and water services and waste management systems;
- orderly development of safe and healthy communities;
- adequate provision of a full range of housing, including affordable housing;
- appropriate location of growth and development;
- promotion of development that is designed to be sustainable, to support public transit and to be oriented to pedestrians; and
- promotion of a built form that is well designed, encourages a sense of place, and provides for public spaces that are of high quality, safe, accessible, attractive and vibrant.

Provincial Land-Use Policies: Provincial Policy Statement and Provincial Plans

Provincial Policy Statements and geographically specific Provincial Plans, along with municipal Official Plans, provide a policy framework for planning and development in the Province. This framework is implemented through a range of land use controls such as zoning by-laws, plans of subdivision and site plans.

Provincial Plans are intended to be read in their entirety and relevant policies are to be applied to each situation. The policies of the Plans represent minimum standards. Council may go beyond these minimum standards to address matters of local importance, unless doing so would conflict with any policies of the Plans.

All decisions of Council in respect of the exercise of any authority that affects a planning matter shall be consistent with the PPS and shall conform with Provincial Plans. All comments, submissions or advice affecting a planning matter that are provided by Council shall also be consistent with the PPS and conform with Provincial Plans.

The Provincial Policy Statement (2020)

The Provincial Policy Statement (2020) (the "PPS") provides policy direction province-wide on land use planning and development to promote strong communities, a strong economy, and a clean and healthy environment. It includes policies on key issues that affect communities, such as:

- the efficient use and management of land and infrastructure;
- ensuring the sufficient provision of housing to meet changing needs including affordable housing;
- ensuring opportunities for job creation;

- ensuring the appropriate transportation, water, sewer and other infrastructure is available to accommodate current and future needs; and
- protecting people, property and community resources by directing development away from natural or human-made hazards; and
- conserving significant built heritage resources.

The provincial policy-led planning system recognizes and addresses the complex inter-relationships among environmental, economic and social factors in land use planning. The PPS supports a comprehensive, integrated and long-term approach to planning, and recognizes linkages among policy areas.

The PPS is issued under Section 3 of the Planning Act and all decisions of Council in respect of the exercise of any authority that affects a planning matter shall be consistent with the PPS. Comments, submissions or advice affecting a planning matter that are provided by Council shall also be consistent with the PPS.

The PPS recognizes and acknowledges the Official Plan as an important document for implementing the policies within the PPS. Policy 4.7 of the PPS states that, "The official plan is the most important vehicle for implementation of this Provincial Policy Statement. Comprehensive, integrated and long-term planning is best achieved through official plans."

A Place to Grow: Growth Plan for the Greater Golden Horseshoe (2020)

A Place to Grow: Growth Plan for the Greater Golden Horseshoe (2020) came into effect on August 28, 2020. This was an amendment to the Growth Plan for the Greater Golden Horseshoe, 2019. The Growth Plan (2020) continues to provide a strategic framework for managing growth and environmental protection in the Greater Golden Horseshoe region, of which the City forms an integral part. The Growth Plan (2020), establishes policies that require implementation through a Municipal Comprehensive Review (MCR), which is a requirement pursuant to Section 26 of the Planning Act.

Policies not expressly linked to a MCR can be applied as part of the review process for development applications, in advance of the next MCR. These policies include:

- Directing municipalities to make more efficient use of land, resources and infrastructure to reduce sprawl, contribute to environmental sustainability and provide for a more compact built form and a vibrant public realm;
- Directing municipalities to engage in an integrated approach to infrastructure planning and investment optimization as part of the land use planning process;
- Achieving complete communities with access to a diverse range of housing options, protected employment zones, public service facilities, recreation and green space, and better connected transit to where people live and work;
- Retaining viable lands designated as employment areas and ensuring redevelopment of lands outside of employment areas retain space for jobs to be accommodated on site;

- Minimizing the negative impacts of climate change by undertaking stormwater management planning that assesses the impacts of extreme weather events and incorporates green infrastructure;
- Recognizing the importance of watershed planning for the protection of the quality and quantity of water and hydrologic features and areas; and
- Conserving and promoting cultural heritage resources to support the social, economic and cultural well-being of all communities.

The Growth Plan (2020), builds upon the policy foundation provided by the PPS and provides more specific land use planning policies to address issues facing the GGH region. The policies of the Growth Plan (2020), take precedence over the policies of the PPS to the extent of any conflict, except where the relevant legislation provides otherwise.

In accordance with Section 3 of the Planning Act all decisions of Council in respect of the exercise of any authority that affects a planning matter shall conform with the Growth Plan (2020). Comments, submissions or advice affecting a planning matter that are provided by Council shall also conform with the Growth Plan (2020).

Staff have reviewed the proposed development for consistency with the PPS (2020) and for conformity with the Growth Plan (2020). The outcome of staff analysis and review are summarized in the Comments section of the Report.

Toronto Official Plan

The City of Toronto Official Plan is a comprehensive policy document that guides development in the City, providing direction for managing the size, location and built form compatibility of different land uses and the provision of municipal services and facilities. Toronto Official Plan policies related to building complete communities, including heritage preservation and environmental stewardship may be applicable to any application.

The City of Toronto Official Plan can be found here: <https://www.toronto.ca/city-government/planning-development/official-plan-guidelines/official-plan/>.

On September 21, 2020 the Minister of Municipal Affairs and Housing Issued Notices of Decision approving Official Plan Amendments ("OPAs") 479 and 480. OPA 479 (Public Realm) and OPA 480 (Built Form) were adopted as part of the Five-Year Official Plan Review. The OPAs replace Sections 3.1.1, 3.1.2, and 3.1.3 of the Official Plan with new and revised policies for the public realm, built form and built form types, which respond to Council's direction to amend the urban design policies of the Official Plan.

The Official Plan Amendments can be found at: <http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2019.PH11.4>

This application has been reviewed against the policies of the City of Toronto Official Plan, including the Central Waterfront Secondary Plan as follows:

Chapter 2 - Shaping the City

The site is within the Downtown on Map 2 - Urban Structure of the Official Plan. The Downtown is anticipated to accommodate growth that is not intended to be spread uniformly across the whole of Downtown.

Section 2.2 Structuring Growth in the City: Integrating Land Use and Transportation

This section states that the Plan protects the integrity of the City's transportation network, and steers future growth to areas that are well served by transit, including the Downtown. The integration of transportation and land use planning is critical in achieving the overall aim of increasing accessibility throughout the City.

Policy 2.2.2 states growth will be directed to the Downtown in order to among other matters: a) use municipal land, infrastructure and services efficiently; b) concentrate jobs and people in areas well served by surface transit and rapid transit stations; d) promote mixed use development to increase opportunities for living close to work and to encourage walking and cycling for local trips; and f) facilitate social interaction, public safety and cultural and economic activity.

Policy 2.2.2.3 a)(i) provides that the City's transportation network will be maintained and developed to support the growth management objectives of the Plan by protecting and developing the network of rights-of-way shown on Map 3 and Schedules 1 and 2 by: acquiring over time the additional property needed to achieve the designated width and the conveyance of land for widening from abutting property owners.

Section 2.2.1 Downtown: The Heart of Toronto

This section states that the Downtown plays a vital role as the City's economic and cultural hub and is critical to the health and prosperity of the entire region that surrounds it. Toronto's Downtown includes a portion of the Central Waterfront, which offers unique opportunities for substantial employment and residential growth and for upgrades and expansion to the public realm.

The Plan recognizes that many of the activities that make the Downtown successful and vibrant are interdependent and linked through the Downtown's public realm. Well designed connections between the core of the City and the Central Waterfront are important to the vitality of the Downtown as a great place to live and renewed Central Waterfront will create new opportunities for business development and contribute in an important way to the image of the Downtown and the entire City. The Downtown is where our history is richest, but it is also where we continue to rebuild to accommodate a growing economy and a changing society. Given that this is one place in Toronto where "change is constant", we must ensure that our built heritage is respected, nurtured and celebrated.

Policy 2.2.1.6 states that the Downtown Toronto Urban Growth Centre will be planned to optimize the public investment in higher order transit within the Centre and thus should exceed the minimum combined growth density target of 400 residents and jobs per hectare set out in the Growth Plan.

Policy 2.2.1.9 states the quality of Downtown will be improved by among other matters: supporting the development of complete communities; and developing buildings that are shaped, scaled and designed to enhance liveability.

Policy 2.2.1.10 states the architectural and cultural heritage of Downtown will be preserved by designating buildings, districts and open spaces with heritage significance by working with owners to restore and maintain historic buildings.

Policy 2.2.1.13 states that priority will be given to improving walking, cycling and transit access and Policy 2.2.1.15 states that a program of street improvements will be implemented to enhance the pedestrian environment and expand the cycling network with measures undertaken to make walking and cycling in Downtown safe, convenient and comfortable.

Chapter 3 - Building a Successful City

Section 3.1.1 The Public Realm

The public realm policies provide guidance on the roles and key relationships between elements of the public realm as well as direction on the expansion, enhancement and maintenance of the public realm through development review and capital projects.

Policy 3.1.1.2 states that the public realm will: provide the organizing framework and setting for development; foster complete, well-connected walkable communities that meet daily needs of people and support a mix of activities; support active transportation and public transit use; provide a comfortable, attractive and vibrant, safe and accessible setting for civic life and social interaction; contribute to the identity and physical character of the City and its neighbourhoods; be functional and fit within a larger network; and contribute to the City's climate resilience.

Policy 3.1.1.6 states that City streets are significant public open spaces that connect people and places, and new and existing streets will incorporate a Complete Streets approach and be designed to perform their diverse roles by: balancing the needs and priorities of the various users and uses within the right-of-way; space for trees, landscaping and green infrastructure and space for other street elements such as utilities and services, boulevard cafes and street furniture, and ensuring the safety of users of all ages and abilities.

Policies 3.1.1.13 states that sidewalks and boulevards will be designed to provide safe, attractive, interesting and comfortable spaces for users of all ages and abilities and Policy 3.1.1.16 states that the preservation, long-term growth and increase in the amount of healthy trees will be a priority for all development.

Policies 3.1.1.19, and 3.1.1.20 provide direction on the location, design and accessibility of new parks and open spaces, including POPS, to ensure they are prominent, visible, functional and accessible, designed for users of a variety of ages and abilities, provide a range of amenities and are well integrated into the broader public realm.

Section 3.1.2 The Built Form

The built form policies provide principles on key relationships of the location and organization of development, its massing and appropriate amenity within the existing and planned context to inform the built form and ensure that new buildings will promote and achieve the overall objectives of the Official Plan.

Policy 3.1.2.1 states that development will be located and organized to fit with its existing and planned context and provides guidance on how this is achieved. Development is expected to frame and support adjacent streets, lanes, parks and open spaces to promote civic life and the use of the public realm, provide additional setbacks or open spaces at street intersections, and provide ground floor uses, clear windows and entrances that allow views from and, where possible access to, adjacent streets, parks and open spaces.

Policy 3.1.2.2. states that development will provide accessible open space, where appropriate, and Policy 3.1.2.3 states that development will protect privacy within adjacent buildings by providing setbacks and separation distances from neighbouring properties and adjacent building walls containing windows.

Policy 3.1.2.4 states that development will locate and organize vehicle parking, vehicular access and ramps, loading, servicing, storage areas, and utilities to minimize their impact and improve the safety and attractiveness of the public realm, the site and surrounding area by: using shared service areas; consolidating and minimizing the width of driveways and curb cuts across the public sidewalk; providing underground parking, where appropriate; and limiting new, and removing existing, surface parking and vehicular access between the front face of a building and the public street or sidewalk;

Policy 3.1.2.5 states that development will be located and massed to fit within the existing and planned context, define and frame the edges of the public realm with good street proportion, fit with the character, and ensure access to direct sunlight and daylight on the public realm by: providing streetwall heights and setbacks that fit harmoniously with the existing and/or planned context; and stepping back building mass and reducing building footprints above the streetwall height.

Policy 3.1.2.6 states that development will be required to provide good transition in scale between areas of different building heights and/or intensity of use in consideration of both the existing and planned contexts of neighbouring properties and the public realm.

Policy 3.1.2.10 states that development will promote civic life and provide amenity for pedestrians in the public realm by: providing improvements to adjacent boulevards and sidewalks, co-ordinated landscape improvements in setbacks, weather protection, landscaped open space within the development site, safe direct pedestrian routes and tree plantings throughout the site and public art where the developer agrees to provide this.

Policy 3.1.2.11 states that new indoor and outdoor amenity spaces that consider the needs of residents of all ages and abilities should be provided as part of multi-unit residential developments, and Policy 3.1.2.12 states that indoor and outdoor amenity spaces are encouraged with non-residential development.

Section 3.1.3 Built Form - Building Typologies

This section provides policy direction for three scales of building types – Townhouse and Low-Rise Apartments, Mid-Rise, and Tall – for residential, office and mixed-use intensification.

Tall buildings are an intensive form of growth that comes with both opportunities and challenges. Tall buildings play a role in achieving residential and employment growth objectives in the Downtown. Tall buildings should only be considered where they can fit into the existing or planned context, and where the site's size, configuration and context allows for the appropriate design criteria to be met.

Policy 3.1.3.1 encourages a mix of building types on sites that can accommodate more than one building. Where a development includes more than one building, the site will be designed to ensure appropriate site organization and building locations that: provide parcels of appropriate size and shape for the mix of building types; define and support existing and proposed streets, lanes, parks and open spaces at appropriate scales; ensure appropriate spacing of buildings; and ensure appropriate transition in scale between buildings of different scales and types and other lower-scaled uses.

Policy 3.1.3.7 states that tall buildings are generally greater in height than the width of the adjacent right-of-way and Policy 3.1.3.8 states that tall buildings should typically be designed to consist of three parts – a base, a tower and a top – that are carefully integrated into a single whole.

Policy 3.1.3.9 states that the base portion of tall buildings should: a) respect and reinforce good street proportion and pedestrian scale; and b) be lined with active, grade-related uses.

Policy 3.1.3.10 states that the tower portion of a tall building should be designed to: reduce the physical and visual impacts of the tower from the public realm; limit shadow impacts on the public realm and surrounding properties; maximize access to sunlight and open views of the sky from the public realm; limit and mitigate pedestrian level wind impacts; and provide access to daylight and protect privacy in interior spaces within the tower.

Policy 3.1.3.11 states that Policy 3.1.3.10 should be achieved by: stepping back the tower from the base building; generally aligning the tower with, and parallel to, the street; limiting and shaping the size of tower floorplates above base buildings; providing appropriate separation distances from side and rear lot lines as well as other towers; and locating and shaping balconies to limit shadow impacts.

Policy 3.1.3.12 states that the top portion of a tall building should be designed to: integrate roof top mechanical systems into the building design; contribute to the surrounding skyline identity and character; and avoid up-lighting and excessive lighting.

Section 3.1.5 Heritage Conservation

This section provides policy direction on the identification of potential heritage properties, conservation of heritage properties and on development adjacent to heritage properties.

Policy 3.1.5.2 states that properties of potential cultural heritage value or interest will be identified and evaluated to determine their cultural heritage value or interest consistent with provincial regulations, where applicable, and will include the consideration of cultural heritage values including design or physical value, historical or associative value and contextual value, and in Policy 3.1.5.3 heritage properties of cultural heritage value or interest will be protected by being designated under the Ontario Heritage Act and/or included on the Heritage Register.

Policy 3.1.5.4 states that properties on the Heritage Register will be conserved and maintained consistent with the Standards and Guidelines for the Conservation of Historic Places in Canada, as revised from time to time and as adopted by Council.

Policy 3.1.5.5 requires proposed alterations or development on or adjacent to a property on the Heritage Register to ensure that the integrity of the heritage property's cultural heritage value and attributes will be retained

Policy 3.1.5.6 states that the adaptive re-use of properties on the Heritage Register is encouraged for new uses permitted in the applicable Official Plan land use designation, consistent with the Standards and Guidelines for the Conservation of Historic Places in Canada.

Policy 3.1.5.26 states that new construction on, or adjacent to, a property on the Heritage Register will be designed to conserve the cultural heritage values, attributes and character of that property and to mitigate visual and physical impact on it.

Policy 3.1.5.27 states that where it is supported by the cultural heritage values and attributes of a property on the Heritage Register, the conservation of whole or substantial portions of buildings, structures and landscapes on those properties is desirable and encouraged. The retention of façades alone is discouraged.

Policy 3.1.5.28 states that the owner of a designated heritage property will be encouraged to enter into a Heritage Easement Agreement where the City considers additional protection beyond designation desirable due to the location, proposed alteration, and/or the nature of the property.

Section 3.2.1 Housing

This section provides direction to encourage the provision of a full range of housing, in terms of form, tenure and affordability.

Policy 3.2.1.1 states a full range of housing, in terms of form, tenure and affordability will be provided to meet the current and future needs of residents.

Policy 3.2.1.4 states assistance will be provided to encourage the production of affordable housing either by the City or in combination with senior government programs and initiatives.

Chapter 4 - Land Use

Policy 4.5 Mixed Use Areas

The subject site is designated Mixed Use Areas on Map 18 of the Official Plan. See Attachment No. 3 - Official Plan Land Use Map 18. Mixed Use Areas are intended to provide a broad range of commercial, residential and institutional uses in single-use or mixed-use buildings.

Policy 4.5.2 sets out the development criteria for Mixed Use Areas to ensure, among other matters, that the location and massing for new buildings achieves transitions between areas of different development intensity and scale, frames the edges of streets and parks with good proportion and maintains sunlight and comfortable wind conditions for pedestrians on adjacent streets, parks and open spaces. Development in Mixed Uses Areas should also provide attractive, comfortable and safe pedestrian environments and have access to schools, parks, community centres, libraries and child care. Development should also take advantage of nearby transit services, provide good site access and circulation and opportunities for green infrastructure.

Official Plan Amendment 417 - Bathurst Quay Neighbourhood Plan

Official Plan Amendment 417 provides policy direction for revitalization of the Canada Malting Silos site and Marina Quay West to create a community and cultural hub in the Bathurst Quay neighbourhood, as well as, policies that support enhancements to community services and facilities and public realm improvements in the neighbourhood.

Official Plan Amendment No. 231 - Office Replacement Policies

The office replacement policies of Official Plan Amendment 231 require the replacement of office space on sites designated Mixed Use Areas containing 1,000 square metres or more of office space, where residential development is proposed.

OPA 352 – Downtown Tall Building Setback Area

On October 5-7, 2016, City Council adopted Official Plan Amendment (OPA) 352 – Downtown Tall Building Setback Area (currently under appeal). The purpose of OPA 352 is to establish the policy context for tall building setbacks and separation distances between tower portions of tall buildings in the Downtown. At the same meeting, City Council adopted area-specific Zoning By-laws 1106-2016 and 1107-2016 (also under appeal), which provide the detailed performance standards for portions of buildings above 24 metres in height.

Chapter 6 - Secondary Plans

Central Waterfront Secondary Plan

The site is situated within the Central Waterfront Secondary Plan (CWSP) area and is designated Existing Use Areas in the Secondary Plan. Existing Use Areas are governed by the policies of the existing Official Plan and Zoning By-law for the site.

The CWSP sets out policies for waterfront renewal based on four core principles:

1. Removing Barriers/Making Connections;
2. Building a Network of Spectacular Waterfront Parks and Public Spaces;
3. Promoting a Clean and Green Environment; and
4. Creating Dynamic and Diverse New Communities.

The CWSP includes a series of initiatives or 'Big Moves' intended, along with implementing policies, to promote waterfront renewal. The Secondary Plan considers the removal of barriers and improved connections as essential to waterfront renewal.

The Plan provides that waterfront streets be remade as places with distinct identities and be enhanced through high quality design and landscaping. Queens Quay is an important connecting link along the waterfront and Bathurst Street is a connecting link between the waterfront and the central city. Both streets are identified as key pedestrian links in the Secondary Plan.

The policies of the Secondary Plan seek to create special places along the waterfront, and develop strategies to attract tourism in order to strengthen Toronto's role as the cultural capital. The policies provide that designated heritage buildings will be conserved for creative reuse in their original locations. Sustainability is also a core principle of waterfront renewal. The Secondary Plan promotes mixed use communities to provide opportunities to live and work close together; pedestrian and cycling routes that are safe, attractive, comfortable and generously landscaped; and improvements to

water quality through the use of innovative storm water management, servicing and energy technologies.

The Central Waterfront Secondary Plan can be found here: <https://www.toronto.ca/city-government/planning-development/official-plan-guidelines/official-plan/>.

Chapter 7 - Site and Area Specific Policies

Site and Area Specific Policy 181 and 185 - Harbourfront and Bathurst Quay

The site is subject to Site and Area Specific Policies (SASPs) 181 and 185 of the Official Plan.

SASP 181 recognizes the unique recreational opportunities with the waterfront and encourages a range of park and recreational opportunities. The SASP also states that Harbourfront is exempt from a by-law enacted under Section 42 of the Planning Act requiring the conveyance of land for park or other recreational purposes as a condition of development or redevelopment. The SASP also provides that only parking facilities incorporated in a building or structure are to be developed in Harbourfront.

SASP 185 permits parking facilities for the Billy Bishop Toronto City Airport on the lands.

The outcome of staff analysis and review of relevant Official Plan policies and designations and the Central Waterfront Secondary Plan are summarized in the Comments section of the Report.

Zoning

The site is zoned Commercial Residential (CR) by Harbourfront Zoning By-law 289-93, as amended. See Attachment No. 4 - Harbourfront Zoning Map. The CR zone permits a range of residential uses, as well as, retail and service uses, community services and facilities, office uses and a communications and broadcasting establishment.

The Zoning By-law permits a maximum building height of 31 metres and a maximum combined gross floor area for residential and non-residential uses of 21,000 square metres. A maximum of 20,500 square metres is permitted for residential uses and a maximum of 8,500 square metres is permitted for non-residential uses.

The property is not subject to City-wide Zoning By-law 569-2013.

City-Wide Tall Building Design Guidelines

City Council has adopted city-wide Tall Building Design Guidelines and directed City Planning staff to use these Guidelines in the evaluation of tall building development applications. The Guidelines establish a unified set of performance measures for the evaluation of tall building proposals to ensure they fit within their context and minimize

their local impacts. The Tall Building Design Guidelines may be found here:
<https://www.toronto.ca/legdocs/mmis/2013/pg/bgrd/backgroundfile-57177.pdf>.

Growing UP: Planning for Children in New Vertical Communities Guidelines

On July 28, 2020, City Council adopted the updated Growing Up: Planning for Children in New Vertical Communities Urban Design Guidelines (the "Growing Up Guidelines"). The update was based on the continued review and assessment from the draft guidelines adopted by Council in July 2017. The objective of the Growing Up Guidelines is that developments deliver tangible outcomes to increase liveability for larger households, including families with children at the neighbourhood, building, and unit scale. The Growing up Guidelines may be found here:

<https://www.toronto.ca/legdocs/mmis/2017/pg/bgrd/backgroundfile-103920.pdf>

Pet Friendly Design Guidelines and Best Practices for New Multi-Unit Buildings

The purpose of these guidelines are to guide new developments in a direction that is more supportive of a growing pet population, considering opportunities to reduce the current burden on the public realm, and provide needed pet amenities for high density residential communities. The Pet Friendly Guidelines may be found here:

<https://www.toronto.ca/wp-content/uploads/2019/12/94d3-CityPlanning-Pet-Friendly-Guidelines.pdf>

Harbourfront Design Guidelines

The Harbourfront Design Guidelines set out overall objectives for built form and the public realm in Harbourfront. The objectives include a stepping down in building heights from the higher, denser areas of the central core, south to the water's edge as well as a general stepping down in building heights in Harbourfront from the east to the west. The Guidelines recommend that any development on the subject site should retain the existing Crosse and Blackwell building and any re-use or redevelopment of the site should be sensitive to the existing structure. Particularly, the height and setback of any additions to the building should be such that the visual prominence of the existing historic building is maintained.

Waterfront Design Review Panel

Prior to the submission of the zoning amendment application, the applicant presented development concepts to the Waterfront Design Review Panel on March 21, 2018. The concepts proposed a mixed-use development with residential, office and retail uses including an indoor market with three massing scenarios including:

- Two towers with heights of 38 and 33 storeys
- Two towers with heights of 33 and 20 storeys
- Two towers with heights of 39 and 17 storey and a mid-rise building of 9 storeys

Comments from Panel members included:

- Consider how the project fits within the Bathurst Quay Neighbourhood Plan.
- Members noted that the development was considered an opportunity to bring life to the Queens Quay West frontage and that the facade along Queens Quay West should be powerful.
- Members felt the hexagonal pavilion on the heritage building at the northwest corner of the site should be the entrance to the complex.
- Members recommended that the proposal respect the 25 metre tower separation distance as set out in the Tall Building Design Guidelines and the existing building datum along Queens Quay West.
- Members noted the importance of the design for the streetscapes.

The current proposal was considered by the Waterfront Design Review Panel at its meeting of December 15, 2021. Comments included:

- The Panel appreciated the major revisions made from the former scenarios including the full retention of the historic building and the reduction in tower heights and density.
- Members noted that the design has many different formal architectural motifs, and recommended design simplification.
- Panel members felt that the POPS was a positive element of the proposal, and suggested further study of its design and the location of the columns to help connect and enhance the pedestrian experience on Queens Quay West and Bathurst Street.
- Members suggested breaking up the retail space along Queens Quay West into smaller store fronts to create a stronger neighborhood characteristic.
- Members suggested extending the soft landscape up to the corner of Bathurst Street and Lake Shore Boulevard West to improve the pedestrian experience and minimize the “hostile” feeling of the corner.
- Members noted the importance of the Queens Quay West frontage and suggested considering a more performative landscaping strategy.
- Members recommended that the applicant consider higher sustainability targets to futureproof the building and provide flexibility to meet zero carbon emissions.

The minutes of the Waterfront Design Review Panel meetings are available here: <https://www.waterfrontoronto.ca/nbe/portal/waterfront/Home>

COMMUNITY CONSULTATION

Two Community Consultation meetings have been held.

An in person meeting was held on March 19, 2019. Staff from City Planning, the Ward Councillor and approximately 130 people attended. Comments and main issues raised by participants included:

- The overall density and height of the buildings in relation to nearby buildings.
- Impacts of the 39 storey tower on the heritage building.
- Impacts of the proposal on views from existing buildings to the lake.
- Shadow impacts on surrounding properties.
- Transportation and traffic issues including: concerns with the level of existing traffic in the neighbourhood, insufficient capacity in existing street cars, concern that insufficient parking was proposed and pedestrian safety concerns.

A virtual community meeting was held on September 27, 2021 to consider the revised proposal. Staff from City Planning and Transportation Services, the Ward Councillor and 108 people attended. Comments and main issues raised by participants included:

- The height of the proposed 22-storey building on the northeast corner of Bathurst Street and Queens Quay West was a main issue of concern, particularly in relation to the height of the existing 16-storey building on the northwest corner of Bathurst Street and Queens Quay West.
- Impacts on views to the lake from nearby buildings.
- Shadow impacts on surrounding properties.
- Transportation and traffic issues including: the proposed reduction in parking, concerns about traffic congestion, potential for pick-up and drop-off activity on adjacent streets, and pedestrian safety.
- Concern was noted with the proposed courtyard location in the centre of the site and public accessibility.
- Concerns were noted with building setbacks and the need for more open space on the south side of the proposal along Queens Quay West.
- Concerns were noted that affordable housing was not included in the development.
- Participants encouraged the applicant to ensure that the building had sustainable design.
- Participants noted that the conservation of the heritage building was a positive change to the proposal.

The applicant and City staff also attended meetings with the Bathurst Quay Neighbourhood Association (BQNA) to discuss the revised proposal on April 14, 2021 and July 13, 2021. City staff attended additional meetings with the Bathurst Quay Neighbourhood Association on March 10, 2021, August 18, 2021, February 2, 2022 and March 8, 2022 to discuss the proposal.

Through these discussions the BQNA recommended changes to the design of the development in relation to the public realm including providing increased setbacks along Bathurst Street and Queens Quay West to allow for additional pedestrian amenities, and relocating the open space from the centre of the site to Queens Quay West. In addition, the BQNA made recommendations in relation to building height including moving the taller element to the north portion of the site as initially proposed, or reducing the tower height to 16 storeys to match the height of the existing building to the west, with the density distributed to the east portion of the building. The BQNA stressed the importance of affordable housing and requested that affordable housing be

incorporated into the development. Traffic issues were also noted as a concern by the BQNA.

Written submissions have also been received from the Bathurst Quay Neighbourhood Association, the Harbourfront Community Association, Arcadia Co-operative (680 Queens Quay West), the Atrium Condominium (650 Queens Quay West), the Board of Directors for the 600 Queens Quay West Condominium and Member of Parliament Kevin Vuong.

Statutory Public Meeting Comments

In making their decision with regard to this application, Council members have been given the opportunity to view the oral submissions made at the statutory public meeting held by the Toronto and East York Community Council for this application, as these submissions are broadcast live over the internet and recorded for review.

COMMENTS

Planning Act

It is staff's opinion that the proposed development has regard for the relevant matters of provincial interest in the Planning Act, including: the orderly development of safe and healthy communities; the appropriate location of growth and development; the adequate provision and efficient use of communication, transportation, sewage and water services and waste management systems; and the promotion of a built form that is well designed, encourages a sense of place, and provides for public spaces that are of high quality, safe, accessible, attractive and vibrant.

Provincial Policy Statement and Provincial Plans

The proposal has been reviewed and evaluated against the PPS (2020) and the Growth Plan (2020). Staff have determined that the proposal is consistent with the PPS and conforms with the Growth Plan (2020).

Provincial Policy Statement (PPS) (2020)

- Policies 1.1.1, 1.1.2, 1.1.3.1 and 1.1.3.1 of the PPS promote efficient land use patterns in a settlement area identified in the Official Plan for intensification and a varied mix of land uses. Further, Policy 1.3.1 promotes economic development by encouraging compact mixed use development that incorporates compatible employment uses to support liveable and resilient communities.

The site is located within the Downtown and Central Waterfront which is an area identified in the Official Plan for intensification for both residents and jobs. The site is also designated Mixed Use Areas in the Official Plan, a designation intended for growth. The proposal includes a mix of uses including office floor space.

- Policy 1.1.3.3 states planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development accommodating a significant supply and range of housing options through intensification and redevelopment. In addition, Policy 1.6.7.4 promotes a land use pattern, density and mix of uses that minimizes the length and number of vehicle trips and supports current and future use of transit and active transportation.

The application proposes a mix of residential, office, retail and open space uses and has good access to transit, including the Queens Quay West and Bathurst Street streetcars, as well as GO service at the nearby Exhibition Go Station. The site is well connected in relation to cycling and pedestrian infrastructure including the Martin Goodman Trail that extends along the south of Queens Quay West opposite the site and the waterfront promenade along the water's edge further south.

- Policy 1.5.1(a) of the PPS states that healthy, active communities should be promoted by planning public streets, spaces and facilities to be safe, meet the needs of pedestrians, foster social interaction and facilitate active transportation and community connectivity.

The proposal has been modified since the initial submission with increased building setbacks now proposed along Queens Quay West and Bathurst Street to widen the pedestrian zones along these streets. A pedestrian walkway has been introduced along the east side of the site providing a new mid-block connection between Queens Quay West and Lake Shore Boulevard West, and a new POPS is proposed at the southwest corner of the site at Queens Quay West and Bathurst Street. As well, a central open space courtyard is proposed on the site with a pedestrian connection to the courtyard from Bathurst Street. These public realm and open space improvements will enhance connections through the site and from the City to the waterfront, and provide gathering places for residents in the Bathurst Quay neighbourhood.

- Policy 1.7.1 e) of the PPS indicates long-term economic prosperity should be supported by among other matters, encouraging a sense of place, promoting well-designed built form and cultural planning, and conserving features that help define character, including built heritage resources and cultural heritage landscapes. In addition, Policy 2.6.1 of the PPS directs that, significant built heritage resources and significant cultural heritage landscapes shall be conserved.

The application proposes the conservation and reuse of the Crosse and Blackwell building, a significant heritage resource, consistent with the PPS.

A Place to Grow: Growth Plan for the Greater Golden Horseshoe (2020)

- Policy 1.2.1 of the Growth Plan sets out the Guiding Principles for how land is developed, resources are managed and protected and public dollars are invested. The principles support the achievement of complete communities that are designed to support healthy and active living and meet people's needs for daily living throughout an entire lifetime. They prioritize intensification and higher densities in

strategic growth areas to make efficient use of land and infrastructure and support transit viability. They also promote a range of housing options and conservation of cultural heritage resources.

- The Policies of Sections 2.2.1, 2.2.3, 5.2.4 and 5.25 provide direction on managing growth within the Greater Golden Horseshoe, including directing growth to areas such as the Toronto Downtown Urban Growth Centre (UGC), and sets out forecasts and targets for levels of growth in these areas.

The subject site is within the Downtown UGC, a strategic growth area defined in the Growth Plan (2020). The Growth Plan provides that the Downtown UGC, encompassing an area generally bordered by Bathurst Street, the midtown rail corridor and Rosedale Valley, Don River and Lake Ontario, will be planned to achieve, by 2031, or earlier, a minimum density target of 400 residents and jobs combined per hectare. The application conforms to the policy direction of the Growth Plan with respect to intensification in the Downtown UGC.

- Policy 2.2.1.4 provides that the Growth Plan will support the achievement of complete communities that: feature a diverse mix of land uses including residential and employment uses and convenient access to local stores, services and public service facilities; provide a diverse range and mix of housing options, and provide for more compact built form and a vibrant public realm including public open spaces.

The proposal provides a mix of residential and employment uses as well as new open space for the residents of the Bathurst Quay neighbourhood. The application will also support the provision of new affordable housing in the local area. As well, residents and workers in the new development will have access to a variety of existing community services and facilities and parks in the Bathurst Quay neighbourhood, including the Waterfront Neighbourhood Centre, the Waterfront and City schools and the St. Stephen's Waterfront Childcare Centre opposite the site on the south side of Queens Quay West. A new public plaza and cultural centre in the Corlick building are also planned on the Canada Malting Silos site to the south at 5 Eireann Quay.

- Policy 2.2.2.3(b) instructs municipalities to develop a strategy to achieve minimum intensification targets in delineated built-up areas, and identify an appropriate type and scale of development and transition of built form to adjacent areas.

The proposal has been modified through the application review process such that an appropriate development type and scale and built form transition has been achieved.

- Section 2.2.5 sets out policy directions in relation to employment. Policy 2.2.5.3 provides that retail and office uses will be directed to locations that support active transportation and have existing or planned transit. Further, Policy 2.2.5.4 provides that in planning for employment, surface parking will be minimized and the development of active transportation networks and transit-supportive built form will be facilitated.

The site is well served by transit, cycling and pedestrian infrastructure. The proposal includes a large amount of office floor area, removes surface parking and adds to the system of pedestrian connections within the area.

- Policy 4.2.7.1 of the Growth Plan provides that cultural heritage resources will be conserved in order to foster a sense of place and benefit communities, particularly in strategic growth areas.

The application proposes the conservation of the heritage building on the site in compliance with this policy.

- Policy 5.2.5.6 states municipalities are to develop and implement urban design and site design official plan policies and other supporting documents that direct the development of a high quality public realm and compact built form.

As such, the City's Official Plan and design guidelines have direct relevance for assessing Growth Plan conformity. It is City Planning staff's opinion that the application conforms with the Growth Plan. The applicable Official Plan policies and relevant guidelines and their link in assessing Growth Plan conformity are examined in the sections below.

Land Use

This application has been reviewed against the Official Plan and Central Waterfront Secondary Plan policies described in the Policy Consideration Section of the Report as well as the policies of the Toronto Official Plan as a whole.

The proposed residential, office, retail and open space uses are permitted uses within the Mixed Use Areas designation of the Official Plan and Existing Use Areas designation in the Central Waterfront Secondary Plan for the site. As well, the proposal to restore the existing heritage building for office uses supports the office replacement policies of OPA 231. Much of the site is currently occupied by surface parking and the policies of the Official Plan and Secondary Plan encourage the removal of the surface parking lots to support public realm and built form objectives. The proposed land uses conform with the applicable Official Plan and Central Waterfront Secondary Plan policies.

Density and Height

The Built Form policies of the Official Plan provide that new development should fit within its existing and planned context, and limit its impact on neighbourhood streets, parks, open spaces and properties. The Mixed Use Areas policies of the Official Plan provide that the location and massing for new buildings should achieve transitions between areas of different development intensity and scale.

The site is situated within the Bathurst Quay neighbourhood in Harbourfront. The Harbourfront Design Guidelines set out overall objectives in relation to building height within Harbourfront. The objectives include a stepping down in heights from the higher, denser areas of the central core, south to the water's edge as well as a general stepping down in heights from York Street in the east to Bathurst Street and in the west.

Development to the north of Lake Shore Boulevard West within the Fort York and City Place neighbourhoods is of high intensity and scale with buildings ranging in height from 29 to over 60 storeys. These include the recently constructed development at 500 Lake Shore Boulevard West opposite the site that includes the restored Loblaws heritage building as well as two towers of 37 and 41 storeys with heights of 120 and 131 metres respectively and the existing 29 storey building at the northwest corner of Lake Shore Boulevard West and Dan Leckie Way.

Development within the Bathurst Quay neighbourhood is generally of low to mid-rise intensity and scale with some taller buildings in the neighbourhood. Existing buildings to the east and west of the site include the 16-storey building (44.6 metres) to the west at 650 Queens Quay West, and two 11-storey buildings (34.3 metres) to the east at 600 Queens Quay West. Residential buildings on the south side of Lake Shore Boulevard West and further south in the neighbourhood are generally lower scale up to 12 storeys with the building at 90 Stadium Road taller at 23 storeys (70 metres). The density of existing development within the Bathurst Quay neighbourhood ranges from approximately 1.2 to 5.9 times the area of the properties.

The heritage building on the north portion of the site is currently 2.5 - 4 storeys tall. In the initial submission a 39-storey tower was proposed over the heritage building with much of the heritage building demolished. Two taller elements were also proposed on the south portion of the site at 15 storeys (60 metres) and 10 storeys (38 metres). Overall a density of 7.1 times the area of the site was proposed.

The revised application removes the 39-storey tower and proposes the full conservation of the heritage building, with a two-storey addition. Office uses are proposed in the restored heritage building. The tallest portion of the heritage building is proposed to be 5 storeys (23 metres), which is below the current maximum height of 31 metres permitted in the Zoning By-law. The residential building to be located on the south portion of the site is proposed to have heights of 21 storeys (69 metres) and 13 storeys (43.7 metres). The proposed density of the revised proposal is 5.25 times the area of the site.

City Planning staff consider the proposed building heights and density appropriate for this site. The development achieves a transition down in height and density for the areas of higher scale and intensity to the north as set out in the Official Plan, Growth Plan, and the Harbourfront Design Guidelines. The restored heritage building on the north half of the site will be lower than the permitted height within the existing zoning by-law, and is lower than a number of existing buildings within the Bathurst Quay neighbourhood.

The 13-storey massing of the new building along Queens Quay West is compatible in scale and character to existing buildings to the east and west along the north side of the street and fits appropriately within the existing context. The 21-storey tower is proposed to be located at the northeast corner of the site at the intersection of two prominent streets. While proposed to be taller than the existing building on the northwest corner of the intersection of Bathurst Street and Queens Quay West, the new tower will be separated by over 30 metres from the existing 16 storey building to the west. As well, the proposed 21-storey tower is comparable in height with the existing 23-storey building in the neighbourhood to the west at 90 Stadium Road.

The overall density of the development proposed at 5.25 times the area of the site, is consistent with other developments within the Bathurst Quay neighbourhood and represents an appropriate level of intensification on the site. City Planning staff consider the proposed building heights and density consistent with the Official Plan and provincial policies.

Building Massing

Through the review of the application Planning staff requested that the applicant consider a more continuous street wall massing in the building design along the Queens Quay West frontage. Staff also recommended that the base building height generally align with those of the adjacent buildings to the east and west at approximately 28 metres, with a stepback above. Along Bathurst Street, staff recommended that a lower base building element that generally aligned with the existing heritage building be introduced along the west face of the new residential building. As well, staff requested that tower floorplates and separation distances be consistent with the Tall Building Design Guidelines.

In the current proposal a street wall building is proposed along the Queens Quay West frontage. The base building along most of the Queens Quay West frontage is proposed to be 8 storeys (27.5 metres high) and this base building extends along the east side of the residential building adjacent to the driveway. A stepback of 2.9 metres is proposed at level 9 above the base building in these locations. At Level 11 and above, a curve is introduced along the south and east building faces with step backs ranging from 2.5 to 4.5 metres. The base building massing along Queens Quay West is consistent with those of existing buildings to the east and west reinforcing the street wall heights along this important waterfront street. The proposed stepbacks above the base building will help to ensure that the base building is the defining element along Queens Quay West.

In the revised proposal a 4-storey element is proposed along the west face of the residential building along Bathurst Street that aligns with the height of the existing heritage building (18.3 metres) to the north, with a small stepback above. A taller element is proposed from Levels 5 to 17 that wraps the corner at the intersection of Bathurst Street and Queens Quay West. A step back is proposed at Level 18 ranging from 2.5 to 4.6 metres. The tower floorplate ranges in size from 620 to 760 square metres.

City Planning staff are satisfied with the revised building massing. The proposed lower scale element on the west side of the new building along Bathurst Street reinforces the scale of the adjacent heritage building, consistent with the Official Plan and Tall Building Guidelines. The taller element at the corner of Bathurst Street and Queens Quay West will give prominence to the intersection of these two important waterfront streets in the Bathurst Quay neighbourhood. The smaller tower floorplate and setback of the upper levels of the building will help reduce the visual impact of the tower at the pedestrian level, consistent with the Official Plan and Guidelines.

Streetscapes

The Tall Building Design Guidelines recommend that along the primary street frontages of a tall building site that a sidewalk zone at least 6 metres wide be secured to provide sufficient space for pedestrians, trees, and other amenities such as seating, bicycle parking, public art and spill out areas for retail activity such as patios. To achieve an appropriate sidewalk width, the base of a tall building may need to be set back farther from the property line than the distance prescribed in the Zoning By-law.

Through the review of the application City Planning staff recommended that increased building setbacks be provided along Bathurst Street and Queens Quay West. Given the importance of these waterfront streets and the high levels of pedestrian activity in this area, sidewalk zones greater than 6 metres were recommended. As well, City Planning staff and residents requested that an open space be introduced on the south side of the site along Queens Quay West. Opportunities for a publicly accessible open space at the northeast corner of Queens Quay West and Bathurst Street had been explored through the previous Bathurst Quay Neighbourhood Plan Study. Staff and the applicant also discussed the potential to widen the Bathurst Street boulevard by narrowing the street through a portion of the site and straighten its alignment.

Queens Quay West and Bathurst Street

The revised submission proposes increased ground floor building setbacks along the Queens Quay West and Bathurst Street frontages of the site. Along Queens Quay West the ground floor is proposed to be setback 7 metres with a resulting sidewalk zone of 10 metres and greater in some locations. Along Bathurst Street, ground floor building setbacks of 1.2 to 2.4 metres are proposed for the new building on the south portion of the site with sidewalk zones ranging from 8 to 10 metres through this area. The sidewalk zone adjacent to the existing heritage building on the north half of the site is wider, in some areas over 10 metres.

Staff consider the increased building setbacks a positive element of the revised proposal. Widened sidewalk zones along Queens Quay West and Bathurst Street provide opportunity for public realm enhancements along both streets. Adequate space will be available for street trees and a range of amenities to activate the streets, such as seating, lighting and other pedestrian amenities, as well as spill out areas for retail activity such as patios.

Staff recommends that the detailed streetscape designs be determined and secured through the site plan approval process. This would include co-ordination with the public realm improvements proposed in the Bathurst Quay Neighbourhood Plan along Eireann Quay to the south, as well as, further exploration of modifications to the alignment of Bathurst Street to widen the boulevard adjacent to the site.

Privately-Owned Publicly Accessible Open Space (POPS)

The revised submission proposes a Privately-Owned Publicly Accessible Open Space (POPS) on the southwest corner of the site at the intersection of Bathurst Street and Queens Quay West. The POPS space is proposed to be approximately 70 square metres in area.

Staff consider the proposed POPS to be a positive element of the revised proposal. It will provide opportunity to highlight the significance of this intersection, and add to the system of parks and open spaces in the area complementing the existing park (Little Norway Park) and basketball court (Waterfront Neighbourhood Centre) located on the southwest and southeast corners of the intersection respectively. The POPS will also provide opportunities for a gathering space for the community and visitors with space for amenities such as seating, landscaping and public art.

Staff recommend that the POPS be secured in the Section 37 Agreement and its final configuration and design be secured through the site plan approval process.

Lake Shore Boulevard West

The heritage building extends along much of the Lake Shore Boulevard West frontage of the site, and is setback 2.5 to 5 metres from the north property line. The overall sidewalk zone is currently 6 metres wide adjacent to the main portion of the heritage building and is reduced to 5 metres near the intersection of Lake Shore Boulevard West and Bathurst Street beside the hexagonal entry pavilion. A larger grassed area with existing trees is located between the hexagonal entry pavilion and the east wing of the building. Currently the sidewalk is situated at the curb along Lake Shore Boulevard West.

The landscape concept plan submitted with the application proposes streetscape enhancements along Lake Shore Boulevard West. The sidewalk is proposed to be relocated closer to the building and street trees planted closer to the curb separating the sidewalk from the street. As well, seating is proposed within the larger open space area between the entry pavilion and east wing of the building. The streetscape enhancements will help to improve the pedestrian experience along this busy street.

Staff recommend that the detailed design for the Lake Shore Boulevard West streetscape be determined and secured through the site plan approval process.

Housing

The Official Plan, Central Waterfront Secondary Plan, as well as, provincial policies recognize the importance of providing a full range of housing with new development. The provision of affordable, secure, and diverse housing stock to meet housing needs of a wide range of people throughout their life cycle is important to achieving complete communities. Through the review of the application staff discussed affordable housing opportunities as a part of the proposal with the applicant. This included considerations for affordable housing within the proposed development, as well as contributions to affordable housing in the community.

The applicant has agreed to provide a contribution of \$3,500,000.00 for affordable housing as a part of the Section 37 benefits for the development. Planning staff recommend that this contribution be directed to the Housing Now initiative on the nearby City-owned lands at 150 Queens Wharf Road that was approved by Council at its meeting on November 9, 2021. The 150 Queens Wharf Road Housing Now initiative includes 282 rental dwelling units, of which between 33% - 50% of new units will be provided as affordable rental housing. The Section 37 contributions would assist in the implementation of this important Housing Now affordable housing project.

Unit Mix

The Growing Up Guidelines provide direction on the recommended mixture of residential unit types and unit sizes for multi-unit developments. The Growing Up Guidelines includes performance standards, which represent tangible measures to accommodate the growing demand for family-oriented housing in vertical communities. The Guidelines recommend that a building provide a minimum of 25 percent large units, ten percent of the units should be three-bedroom units and 15 percent of the units should be two-bedroom units.

The revised submission proposes a total of 412 dwelling units. Of these 23 (5.5%) are studio units, 274 (66.5%) are proposed one-bedroom units, 67 (16%) are two-bedroom units and 48 (12%) are three-bedroom units. City Planning staff are of the opinion that the proposed unit mix supports the objectives of the Growing Up Guidelines and the applicable provincial and Official Plan policies, and recommends that the larger units be secured in the zoning by-law.

Amenity Space

The Built Form policies of the Official Plan state that new indoor and outdoor amenity spaces are to be provided as part of multi-unit residential developments and are to consider the needs of residents of all ages and abilities over time and throughout the year. Official Plan policy 4.5.2 k) states that in Mixed Use Areas, development will provide indoor and outdoor recreation space for building residents in every significant multi-unit residential development.

The application proposes 824 square metres of indoor amenity space (2 square metres per unit) and 966 square metres of outdoor amenity space (2.3 square metres per unit) within the new residential building. The indoor and associated outdoor amenity spaces provide for a range of amenities including spaces for children. An outdoor area for dogs and an indoor dog washing area are also proposed. Staff consider the amount of both the indoor and outdoor amenity space proposed acceptable. Staff will secure the amenity space in the zoning by-law, and recommend the detailed design of the amenity areas be determined and secured through the site plan approval process.

Heritage Impact and Conservation Strategy

The property at 545 Lake Shore Boulevard West is listed on the City's Heritage Register. It contains the Crosse and Blackwell Building which is a landmark building at the intersection of Bathurst Street and Lake Shore Boulevard West. It was constructed in 1927 by the notable architects Chapman and Oxley for the British food production company Crosse and Blackwell and it is a unique example of a former industrial headquarters that was designed with Art Deco and Beaux Arts stylistic influences. At its meeting on December 15, 2021 City Council approved the issuance of a Notice of Intention to Designate this property under Part IV, Section 29 of the Ontario Heritage.

Heritage Planning staff has reviewed the Heritage Impact Assessment submitted in support of the revised proposal prepared by ERA Architects Inc., for conformity with the Planning Act, the Provincial Policy Statement, the Growth Plan and the City's Official Plan heritage policies and the Standards and Guidelines for Conservation of Historic Places in Canada.

The original proposal for this site retained much of the original facades of the Crosse and Blackwell Building but included considerable alterations to it as part of a redevelopment that included a residential tower above the heritage building. The proposal was subsequently revised to remove the residential tower from above the Crosse and Blackwell Building. The revised scheme retains the whole of the heritage building in-situ and modifies it for use as commercial offices. The revised proposal retains the hexagonal pavilion of the existing structure in its entirety, including the original configuration of the primary entrance. A 2-storey roof addition and single storey rear addition are proposed.

Staff support the proposed alterations in the context of the conservation and mitigation strategies proposed for the site and are of the opinion that the proposal has been designed to conserve the cultural heritage values, attributes and character of the heritage property. The proposal contains a strong conservation program for the subject heritage property at 545 Lake Shore Boulevard West and does not affect the cultural heritage value of the adjacent heritage property at 500 Lake Shore Boulevard West (the Loblaw's Building).

City Planning Staff are satisfied the proposal meets the intent of the Planning Act, the Provincial Policy Statement, the Growth Plan (2020) and the City of Toronto's Official Plan heritage policies.

At its meeting of March 9, 2022 Council approved the alterations proposed to the Crosse and Blackwell building under S. 33 of the Ontario Heritage Act and granted authority for the City to enter into a Heritage Easement Agreement with the owner of the property subject to conditions including:

- That the related zoning by-law amendment has come into full force and effect;
- That prior to the introduction of the bills for the zoning by-law amendment the owner enter into a Heritage Easement Agreement with the City, and provide a detailed Conservation Plan that is consistent with the Conservation Strategy set out in the Heritage Impact Assessment;
- That prior to site plan approval for the proposal, the owner provide final site plan drawings in accordance with the approved Conservation Plan, provide a Heritage Lighting Plan, detailed Landscape Plans, a Signage Plan and an Interpretation Plan all to the satisfaction of Heritage Planning; and
- That prior to the issuance of any permit for all or any part of the property (except for permits for minor repairs and maintenance to the existing heritage building) the owner provide building permit drawings keyed to the approved Conservation Plan, a letter of credit (including provisions for upward indexing) to secure all work included in the approved Conservation Plan, Lighting Plan and Interpretation Plan, and provide full documentation of the existing heritage property, all to the satisfaction of Heritage Planning.

Future Consent Application and Limiting Distance Agreement

The applicant has advised that they are proposing to sever the property in the future through a Planning Act consent application. Two parcels are proposed to be created. One parcel would include the heritage building with office uses on the north portion of the site, and the second parcel would include the residential and retail building on the south portion of the site. As vehicular access, parking and loading are proposed to be shared between uses, and pedestrian connections and the courtyard extend through both parcels, rights-of-way and easements will be needed between the two new properties to facilitate these connections and service arrangements. Staff recommend that these rights-of-way and easements be determined and secured as necessary through the site plan approval process for the two current site plan applications.

As well, Planning staff recommend that the requirement for a limiting distance agreement between the property owners and the City be secured in the Section 37 agreement, to limit development on the north parcel that will contain the heritage

building. The agreement would provide that no new development be permitted above the heritage building other than the additions as currently proposed.

Parkland

The Official Plan contains policies to ensure that Toronto's system of parks and open spaces are maintained, enhanced and expanded. Map 8B of the City of Toronto Official Plan shows local parkland provisions across the City. The lands which are the subject of this application are in an area with 3.00 + hectares of local parkland per 1,000 people. The site is in the highest quintile of current provision of parkland. The site is in a parkland acquisition priority area, as per Chapter 415, Article III, of the Toronto Municipal Code.

The site is also subject to Site and Area Specific Policy (SASP) 181 in the Official Plan. SASP 181 states that Harbourfront is exempt from a by-law enacted under Section 42 of the Planning Act requiring the conveyance of land for park or other recreational purposes as a condition of development or redevelopment. SASP 181 reflects the master planning approach in Harbourfront that occurred during the 1970's and 1980's. The framework for waterfront revitalization in Harbourfront identified the lands that were to be conveyed to the City, to create the comprehensive system of parks and open spaces that are found throughout Harbourfront today including those within Bathurst Quay, and also identified the private development parcels within Harbourfront, most of which have been developed.

In order to give effect to the policy relating to an exemption to parkland as set out in SASP 181, Staff recommend that direction be given to the Chief Building Official that the proposed development on the lands at 545 Lake Shore Boulevard West be exempt from the requirement to convey land for park purposes as a condition of development as contemplated in Article III of Chapter 415 of the Toronto Municipal Code, as amended. This exemption would apply only to the development as currently proposed.

Open Space Courtyard

The revised application proposes an open space courtyard centrally located on the site. The landscape concept plan proposes that the courtyard be designed with paving treatments incorporating the Waterfront Toronto design motifs, soft landscaping, lighting, seating and other amenities. The courtyard is intended to separate the existing heritage building from the new development. It also provides opportunity for outdoor activities including public uses, as well as space for private events for the office workers and residents within the development. Access to the courtyard is proposed from the office and residential buildings, as well as from Bathurst Street through a covered walkway.

Staff consider the proposed courtyard a positive aspect of the proposal as it serves to separate the heritage building from the new development consistent with the recommendations in the Harbourfront Design Guidelines, and will provide additional outdoor space for a range of activities and events. Staff recommend that a public access easement over the walkway from Bathurst Street and through/within the

courtyard be secured in the Section 37 agreement, and that the detailed design of the walkway and courtyard be determined and secured through the site plan approval process.

Shadow Impacts

The tall building policies of the Official Plan provide that the tower portion of a tall building should be designed to limit shadow impacts on the public realm and surrounding properties. Through the review of the application residents expressed concern with potential shadow impacts from the proposed development on surrounding properties and particularly on the Waterfront Neighbourhood Centre to the south as the Waterfront Neighbourhood Centre is planning to install solar panels on the roof of the building.

The applicant has submitted a Shadow Study and this has been reviewed by Planning staff. The Shadow Study indicates that the incremental shadows cast by the building will be limited as the height of the heritage building on the north portion of the site is lower than the as-of-right height that is currently permitted in the Zoning By-law. The Study indicates that there are incremental shadows exclusive to the development extending to property on the northwest corner on Lake Shore Boulevard West and Bathurst Street at 9:18 a.m. during the Spring and Fall equinoxes. This shadow moves off of the property by 10:18 a.m. There are no incremental shadow impacts on surrounding properties during most of the day until 5:18 p.m. where incremental shadows exclusive to the development extend to the north façade of the Waterfront Neighbourhood Centre and to the western portion of the Music Garden. At 6:18 p.m. the shadow extends to the north end of the roof of the Waterfront Neighbourhood Centre.

The Study indicates that during the Summer solstice incremental shadows will extend to the property on the southwest corner of Bathurst Street and Lake Shore Boulevard West at 9:18 a.m. and will have moved off of the property by 10:18 a.m. There are no shadow impacts on the residential building adjacent on the northwest corner of Bathurst Street and Queens Quay West. There are no incremental shadows on surrounding properties through much of the day until 4:18 p.m. where incremental shadows extend to the south boulevard along Queens Quay West. At 5:18 and 6:16 p.m. incremental shadows extend to the south portion of the roof of the Waterfront Neighbourhood Centre. The Study indicates that during the Winter solstice there are no incremental shadow impacts on surrounding properties until 2:18 pm when incremental shadows extend to the southwest portion of Canoe Landing Park. There are no incremental shadows exclusive to the development on the park after 4:18 p.m.

Staff consider the shadow impacts from the proposed development to be acceptable.

Wind Study

A Pedestrian Level Wind Study was submitted for the proposal and has been reviewed by staff. The Study concluded that all grade-level areas, including surrounding

sidewalks, walkways, transit stops and building access points are predicted to experience conditions that are considered acceptable for their intended pedestrian uses throughout the year.

The Study found that wind comfort conditions for most of the outdoor amenity terraces will be comfortable for sitting year round, with the exception of the north and northwest portions of the Level 13 amenity terrace. The Study found that these portions of the terrace will experience wind comfort conditions for standing only for periods of the year. The children's outdoor space is proposed in this area of the terrace. Planning staff recommend that wind mitigation measures be implemented for these areas of the Level 13 amenity terrace and secured through the site plan approval process.

Traffic Impact, Access, Parking and Loading

The applicant submitted a Transportation Considerations Report and Addendums, which assess existing traffic levels and future traffic levels anticipated to be generated by the development, as well as proposed site access, loading, and vehicular and bicycle parking for the initial and revised development proposals.

Traffic Impact

The Transportation Consideration Report estimates that the proposed development will generate approximately 50 and 60 two-way vehicular trips during the morning and afternoon peak hours, respectively. However, based on the study, the existing uses at the site generates a total of 50 and 65 two-way vehicular trips during the morning and afternoon peak hours respectively. Accordingly, after adjustment for the existing trips generated by the site, there will be no net change in the two-way vehicular trips for the morning peak hours and a net decrease of approximately five two-way vehicular trips during the afternoon peak hours, respectively.

Given this level of trip generation, the Report concluded that the projected site traffic will have minimal impacts on area intersections and can be acceptably accommodated on the adjacent road network. As well, the report concluded that the driveway will operate well under future total conditions and will be able to accommodate the proposed site traffic volumes. Transportation Services staff have reviewed the report and find the impacts acceptable.

Access

Access is proposed from a 7 metre wide driveway located along the east portion of the site extending between Queens Quay West and Lake Shore Boulevard West. A pick-up and drop-off facility with capacity for 4 vehicles is proposed off of the driveway. Transportation Services staff consider the proposed access arrangement acceptable.

Vehicle Parking

A two level below grade parking garage is proposed to be constructed on the south portion of the site. A total of 146 vehicle parking spaces are proposed within the garage including 102 residential parking spaces, 3 car share spaces, and 41 spaces for visitors and non-residential uses on the site. Parking for the office uses within the heritage building is proposed to be located in the parking garage on the south portion of the site. Transportation Services staff reviewed the parking study and have advised that the amount of parking proposed is acceptable and will be sufficient to serve the development.

In the event that the heritage building is restored for office uses in advance of the construction of the residential building, parking for the office uses is proposed to be accommodated on the existing surface parking lot on the site. However, parking will be unavailable on-site for the office uses while the residential building is under construction. Planning staff recommend that provisions be included in the zoning by-law to address the temporary situation where parking for the office uses may be unavailable on the site during construction of the parking garage on the south portion of the site. Staff also recommend that measures to address parking during construction be determined and secured through the site plan approval process.

Bicycle Parking

A total of total of 468 bicycle parking spaces are proposed, including 401 long term and 67 short-term spaces. The spaces are proposed to be located on the ground floor and within the P1 level of the parking garage. Shower and change facilities are also proposed within the P1 level of the parking garage. The proposed number of bicycle parking spaces exceeds the requirements of the Toronto Green Standard and is acceptable.

Loading

A total of four loading spaces are proposed to serve the development and three of these will be located on the south portion of the site including two within the below grade parking garage. The existing heritage building has one surface loading space situated on the south side of the building, and this space is proposed to be maintained and enclosed with the restoration of the building. Transportation Services staff has reviewed the loading strategy and find the number and type of loading spaces acceptable including the sharing of loading spaces between uses.

Road Widening

In order to satisfy the Official Plan requirement of a 45 metre right-of-way for this segment of Lake Shore Boulevard West a road widening dedication along the Lake Shore Boulevard West frontage of the site is required. The widening would extend to the limits of the existing heritage building (2.5 to 5 metres) along the Lake Shore Boulevard West frontage of the site. The future roading widening will be identified in the amending

zoning by-law and would be conveyed to the City through the site plan approval process.

Midblock Connection

Through the review of the application Planning staff requested that a mid-block connection be provided between Queens Quay West and Lake Shore Boulevard West. The revised application proposes a 2.1 metre wide pedestrian connection along the east boundary of the site adjacent to the driveway. The landscape concept plan proposes an enhanced paving treatment for the walkway as well as the driveway with bollards separating the spaces for pedestrian safety. Staff recommends that a public access easement over the walkway be secured in the Section 37 agreement and that the final design of the walkway be determined and secured through the site plan approval process.

Servicing

Functional Servicing and Stormwater Management reports were submitted with the initial and revised submissions and were reviewed by Engineering and Construction Services staff. Staff advise that additional information is needed in relation to ground water quality and ground water discharge rates. The additional information submitted in relation to water, sanitary and storm sewers is currently under review by Toronto Water. As a result, Planning staff are recommending that should Council approve the Zoning By-law Amendment application, that before introducing the necessary Bills for enactment the applicant address outstanding items related to servicing outlined in the memo from Engineering and Construction Services dated March 30, 2022 to the satisfaction of Engineering and Construction Services.

Garrison Creek Trunk Stormwater Sewer

The Garrison Creek stormwater trunk sewer traverses the northeast corner of the site. Engineering and Construction Services staff have advised that an easement 7.82 metres in width centred on the Garrison Creek stormwater trunk will be required for the purposes of its reconstruction, maintenance and repair. Given the significance of this infrastructure, reports will also be required to ensure protection of the Garrison Creek stormwater trunk and for ongoing monitoring during and after construction. These reports will be required prior to any building permits being issued on the site. Engineering staff are recommending that these requirements in relation to the Garrison Creek stormwater trunk be secured as a condition of the zoning amendment approval.

Archaeological Assessment

A Stage 1 Archaeological Resource Assessment was submitted with the application. City staff have reviewed the report and concur with the conclusions that there are no further archaeological concerns regarding the subject property.

Tree Preservation

The application is subject to the provisions of the City of Toronto Municipal Code, Chapter 813, Trees, Articles II (Trees on City Streets) and III (Private Tree Protection), commonly referred to as the 'Street Tree By-law' and the 'Private Tree By-law'. The applicant submitted an Arborist Report, a Tree Preservation and Removals Plan and Landscape Plans.

There are eleven existing trees that are subject to provisions of the City's Street Tree and the Private Tree By-laws. Eight are City street trees situated on the north portion of the site along Bathurst Street and east of the site on Lake Shore Boulevard West. The remaining three trees are private trees situated east and west of the hexagonal entry pavilion in the grassed areas on either side of the building. The application proposes to remove three of the City street trees and retain five. The three privately owned trees are proposed to be retained. The application proposes to plant twenty-one new trees including eighteen street trees along the Bathurst Street, Queens Quay West and Lake Shore Boulevard West frontages of the site, as well as three trees within the central courtyard.

Urban Forestry requires the submission of an application for the proposed removal of the three City trees and a permit for the any trees that would be injured. As well, securities for the protection of existing street trees and for new streets proposed to be planted are required. The location of replacement trees on-site and street trees will be determined and secured through the site plan approval process.

Toronto Green Standard

Council has adopted the four-tier Toronto Green Standard (TGS). The TGS is a set of performance measures for green development. Applications for Zoning By-law Amendments, Draft Plans of Subdivision and Site Plan Control are required to meet and demonstrate compliance with Tier 1 of the Toronto Green Standard. Tiers 2, 3 and 4 are voluntary, higher levels of performance with financial incentives. Tier 1 performance measures are secured on site plan drawings and through a Site Plan Agreement or Registered Plan of Subdivision.

The applicant is required to meet Tier 1 of the TGS. The applicant is encouraged to achieve Tier 2 or higher to advance the City's objectives for resilience and to achieve net-zero emissions by 2040 or sooner. Performance measures for the Tier 1 development features will be secured through the Zoning By-law process including automobile infrastructure, cycling infrastructure, and the storage and collection of recycling and organic waste. Other applicable TGS performance measures will be secured through the Site Plan Approval process.

Section 37

The Official Plan contains policies pertaining to the provision of community benefits in exchange for increases in height and/or density pursuant to Section 37 of the Planning

Act. While the proposed development exceeds the height and density limits of the existing Zoning By-law, the application is consistent with the objectives and policies of the Official Plan, and thus constitutes good planning.

The community benefits recommended to be secured in the Section 37 Agreement are as follows:

- a financial contribution of 3,500,000.00 towards the provision of affordable housing on the City-owned Housing Now site at 150 Queens Wharf Road; and
- a publicly-accessible, privately owned open space (POPS) on the southwest corner of the site, with an area of 70 square metres, and all associated details, terms and conditions in relation to its design, timing for conveyance and owner obligations.

The following matters in support of the development are to be secured in a Section 37 Agreement as a legal convenience.

- a 2.1 metre wide public access easement along the east side of the site connecting Queens Quay West and Lake Shore Boulevard West;
- a public access easement from Bathurst Street to the open space courtyard and to connect to the walkway along the east side of the site;
- a construction management plan;
- matters related to the Garrison Creek Trunk Storm Sewer that traverses the site;
- provision for a limiting distance agreement in relation to the heritage building in the event of future severance of the site; and
- conformity to the Toronto Green Standard.

Conclusion

The proposal has been reviewed in relation to the policies of the PPS (2020), the Growth Plan (2020), the Official Plan and Central Waterfront Secondary Plan. Staff are of the opinion that the proposal is consistent with the PPS (2020) and does not conflict with the Growth Plan (2020). Furthermore, the proposal is in keeping with the intent of the Official Plan and Central Waterfront Secondary Plan, particularly as it relates to an appropriate level of intensification for the site's location, the conservation of the Crosse

and Blackwell heritage building, a built form that is contextually appropriate and proposed public realm enhancements that will add to the system of waterfront parks and open spaces. Staff recommend that Council support approval of the application.

CONTACT

Susan McAlpine, Senior Planner
Tel. No. (416) 392-7622
E-mail: Susan.Mcalpine@toronto.ca

SIGNATURE

Lynda H. Macdonald, MCIP, RPP, OALA, FCSLA
Director, Community Planning, Toronto and East York District

ATTACHMENTS

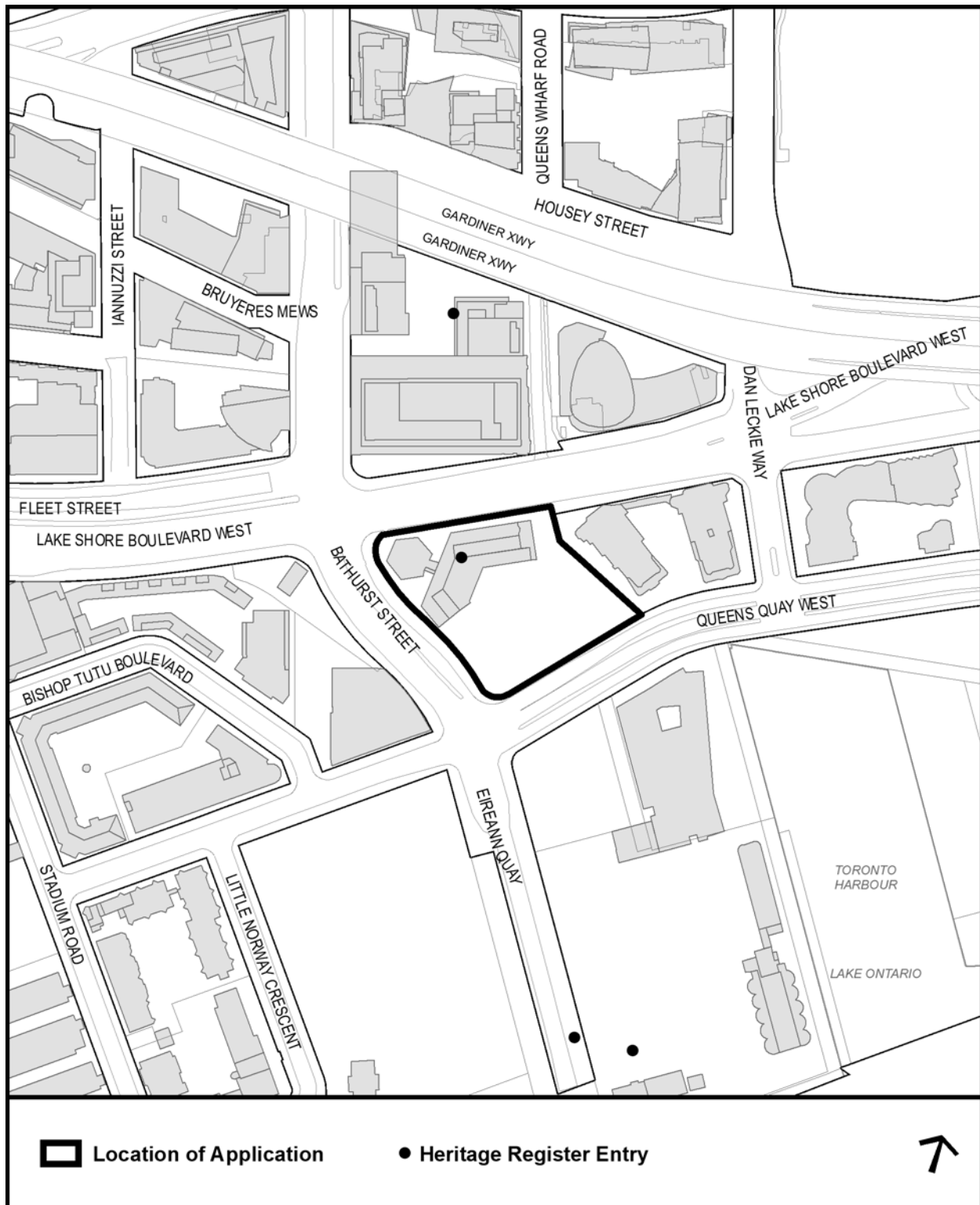
City of Toronto Data/Drawings

Attachment 1: Location Map
Attachment 2: Application Data Sheet
Attachment 3: Official Plan Land Use Map
Attachment 4: Existing Zoning By-law Map
Attachment 10: Draft Zoning By-law Amendment
Attachment 11: Engineering Conditions Garrison Creek Trunk Stormwater Sewer

Applicant Submitted Drawings

Attachment 5: Site Plan
Attachment 6: East Elevation
Attachment 7: North Elevation
Attachment 8: South Elevation
Attachment 9: West Elevation

Attachment 1: Location Map



Attachment 2: Application Data Sheet

Municipal Address: 545 Lake Shore Blvd.W. Date Received: October 12, 2018

Application Number: 18 241642 STE 20 OZ

Application Type: Rezoning

Project Description: Rezoning application to permit a mixed-use development with residential, office, and retail uses. There is a designated heritage building on the site known as the Crosse and Blackwell building that is proposed to be conserved and re-used for office purposes. Residential and retail uses are proposed in a new 13 and 21-storey building on the south portion of the site. A total of 412 dwelling units, 146 vehicle parking spaces and 468 bicycle parking spaces are proposed. A 70 m² Privately-Owned and Publicly-Accessible Open Space (POPS) is proposed.

Applicant	Agent	Architect	Owner
Hunter & Associates Ltd. 15 Gervais Drive, Suite 503 Toronto, ON M3C 1Y8		Sweeny & Co. Architects 124 Peter St. Suite 1601 Toronto, ON M5V 2H2	545 Lakeshore West Property Inc. 1075 Bay St. Suite 400 Toronto, ON M5S 2B1

EXISTING PLANNING CONTROLS

Official Plan Designation:	Mixed Use Areas	Site Specific Provision:	Yes (SASP 181,185)
Zoning:	CR	Heritage Designation:	Designated
Height Limit (m):	31	Site Plan Control Area:	Yes

PROJECT INFORMATION

Site Area (sq m):	7,324	Frontage (m):	279	Depth (m):	86
-------------------	-------	---------------	-----	------------	----

Building Data	Existing	Retained	Proposed	Total
Ground Floor Area (sq m):	2,074		4,974	4,974
Residential GFA (sq m):			37,755	37,755
Non-Residential GFA (sq m):	8,037		16,949	16,949
Total GFA (sq m):	8,037		37,509	54,704
Height - Storeys:	3,4		5, 21,13	5, 21,13
Height - Metres:	14		23,69,44	23,69,44

Lot Coverage Ratio(%): 67.91

Floor Space Index: 5.25

Floor Area Breakdown	Above Grade (sq m)	Below Grade (sq m)
Residential GFA:	28,733	
Retail GFA:	808	
Office GFA:	8,938	
Industrial GFA:		
Institutional/Other GFA:		

Residential Units by Tenure	Existing	Retained	Proposed	Total
Rental:				
Freehold:				
Condominium:			412	412
Other:				
Total Units:			412	412

Total Residential Units by Size

	Rooms	Bachelor	1 Bedroom	2 Bedroom	3+ Bedroom
Retained:					
Proposed:		23	274	67	48
Total Units:		23	274	67	48

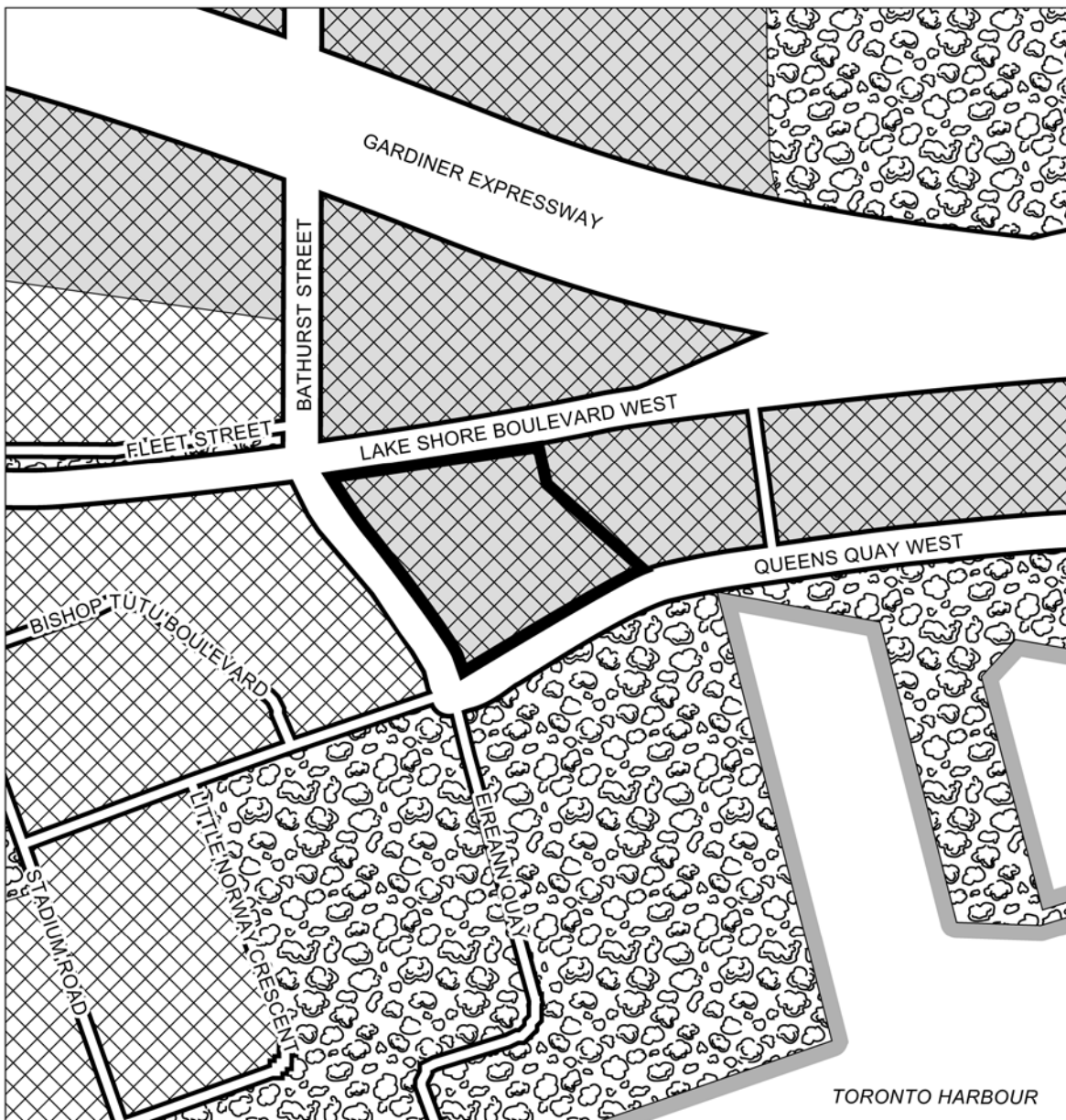
Parking and Loading

Parking Spaces:	146	Bicycle Parking Spaces:	468	Loading Docks:	4
-----------------	-----	-------------------------	-----	----------------	---

CONTACT:

Sue McAlpine, Senior Planner
(416) 392-7622
Susan.Mcalpine@toronto.ca

Attachment 3: Official Plan Land Use Map



Official Plan Land Use Map #18

545 Lake Shore Boulevard West

File # 18 241642 STE 20 0Z

	Location of Application		Parks & Open Space Areas
	Apartment Neighbourhoods		Other Open Space Areas
	Mixed Use Areas		

Not to Scale
 12/07/2019



Zoning By-Law No. 289-93

545 Lake Shore Boulevard West

File # 18 241642 STE 20 0Z

Study Area Boundary

UT Utility and Transportation

R4 Residential District
CR Mixed-Use District
MCR Mixed-Use District

Mixed-Use District	<i>T</i>	Industrial District
Industrial District	<i>G</i>	Parks District
Industrial District	<i>G2</i>	Parks District
Industrial District	<i>Gh</i>	Parks District
	<i>Gm</i>	Parks District

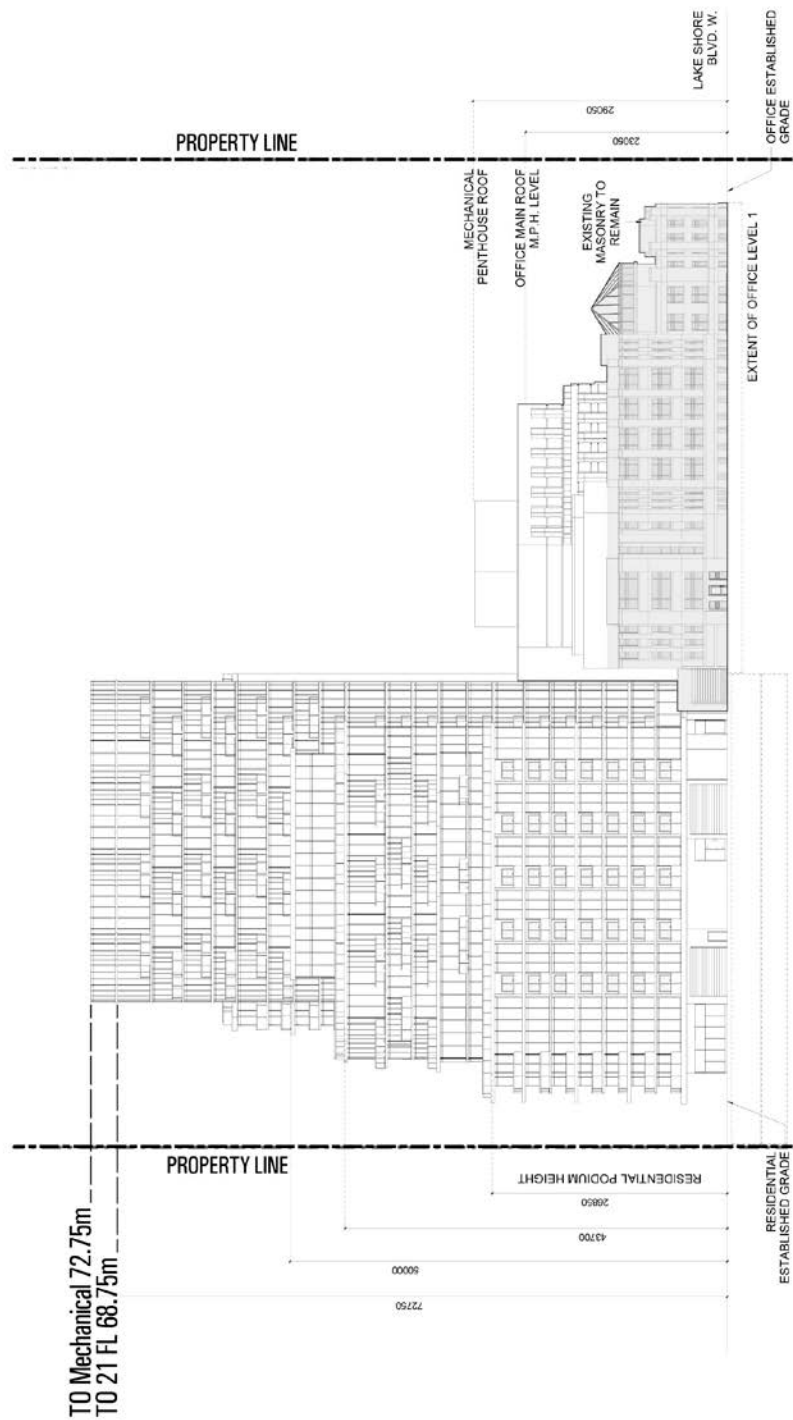


Not to Scale
Extracted: 04/01/2022

Attachment 5: Ground Floor Plan

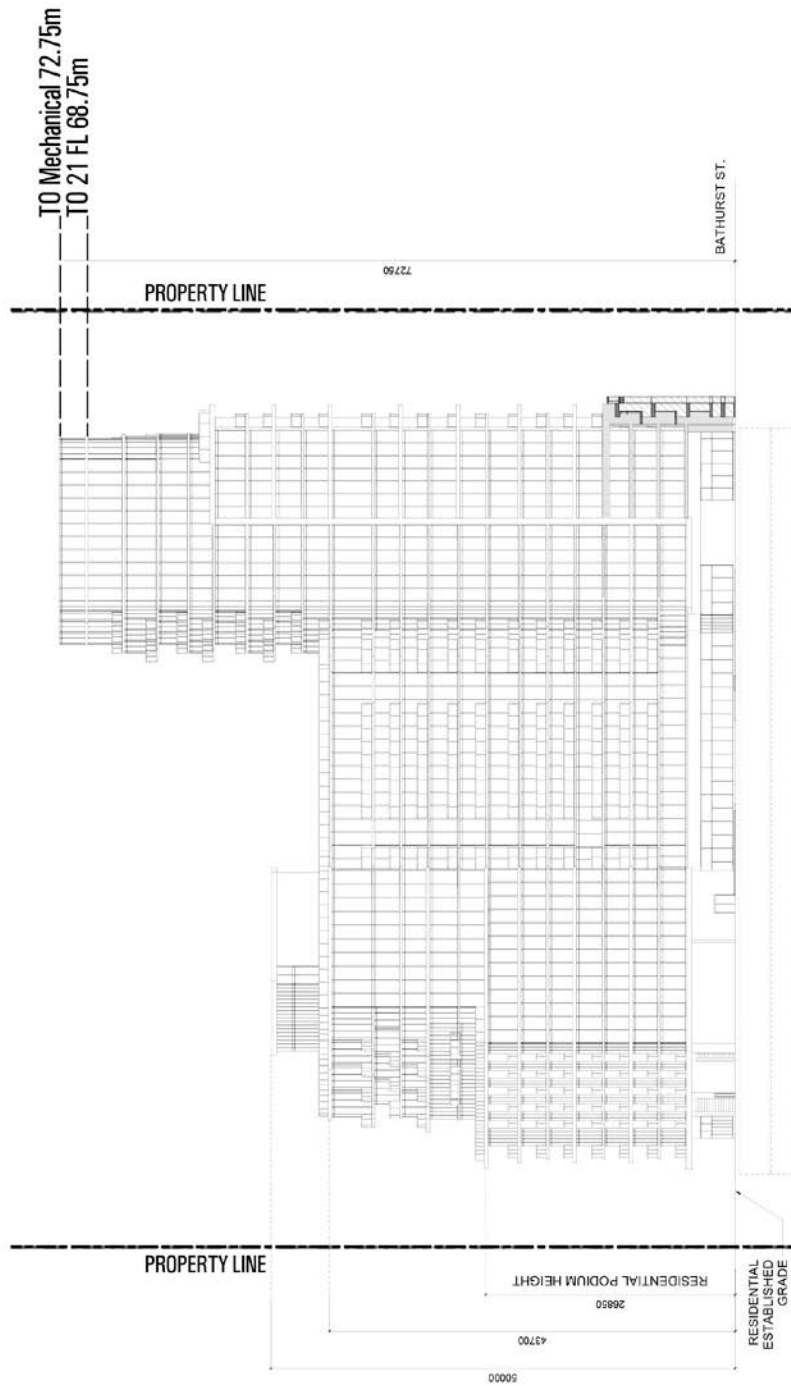


Attachment 6: East Elevation



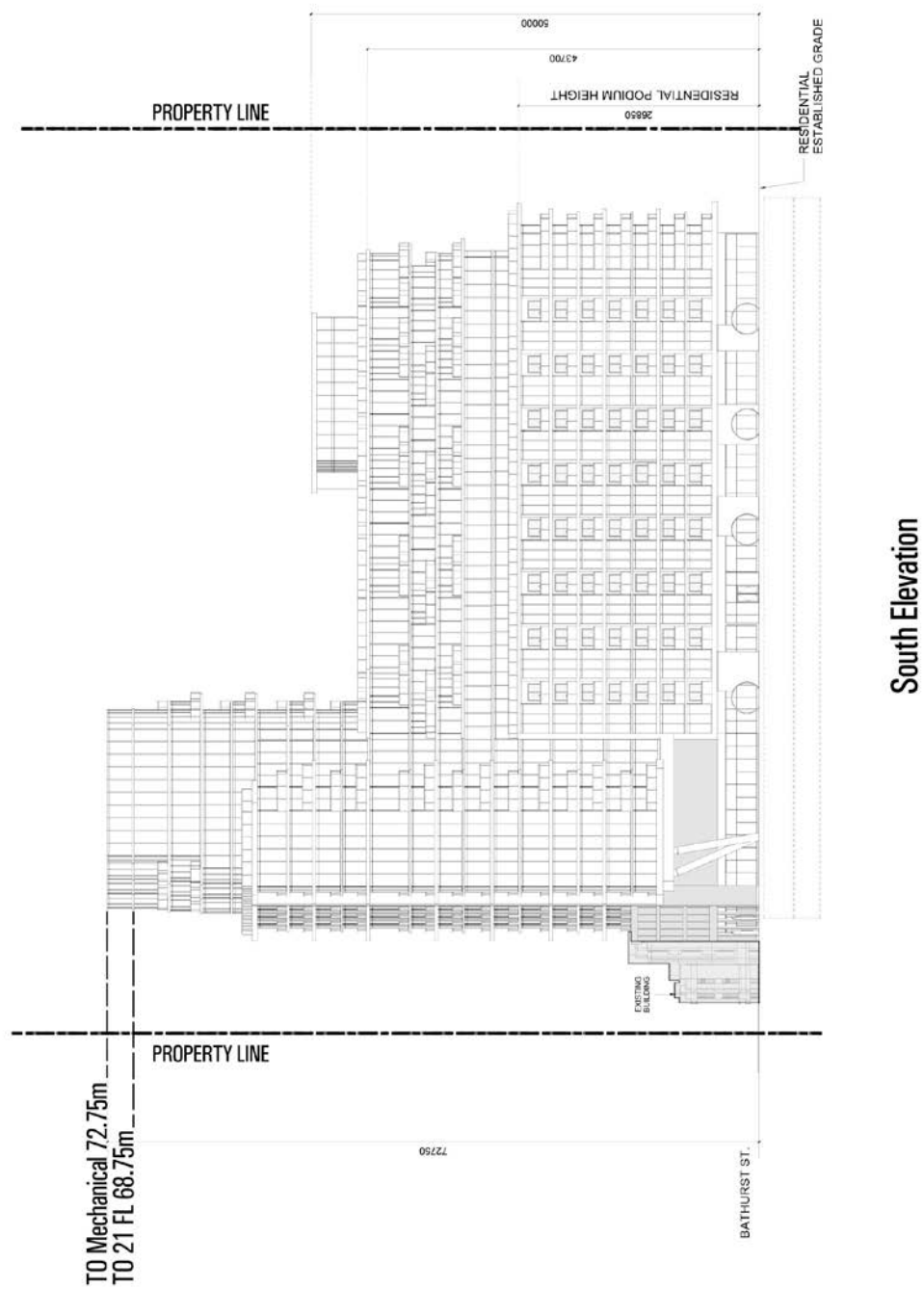
East Elevation

Attachment 7: North Elevation



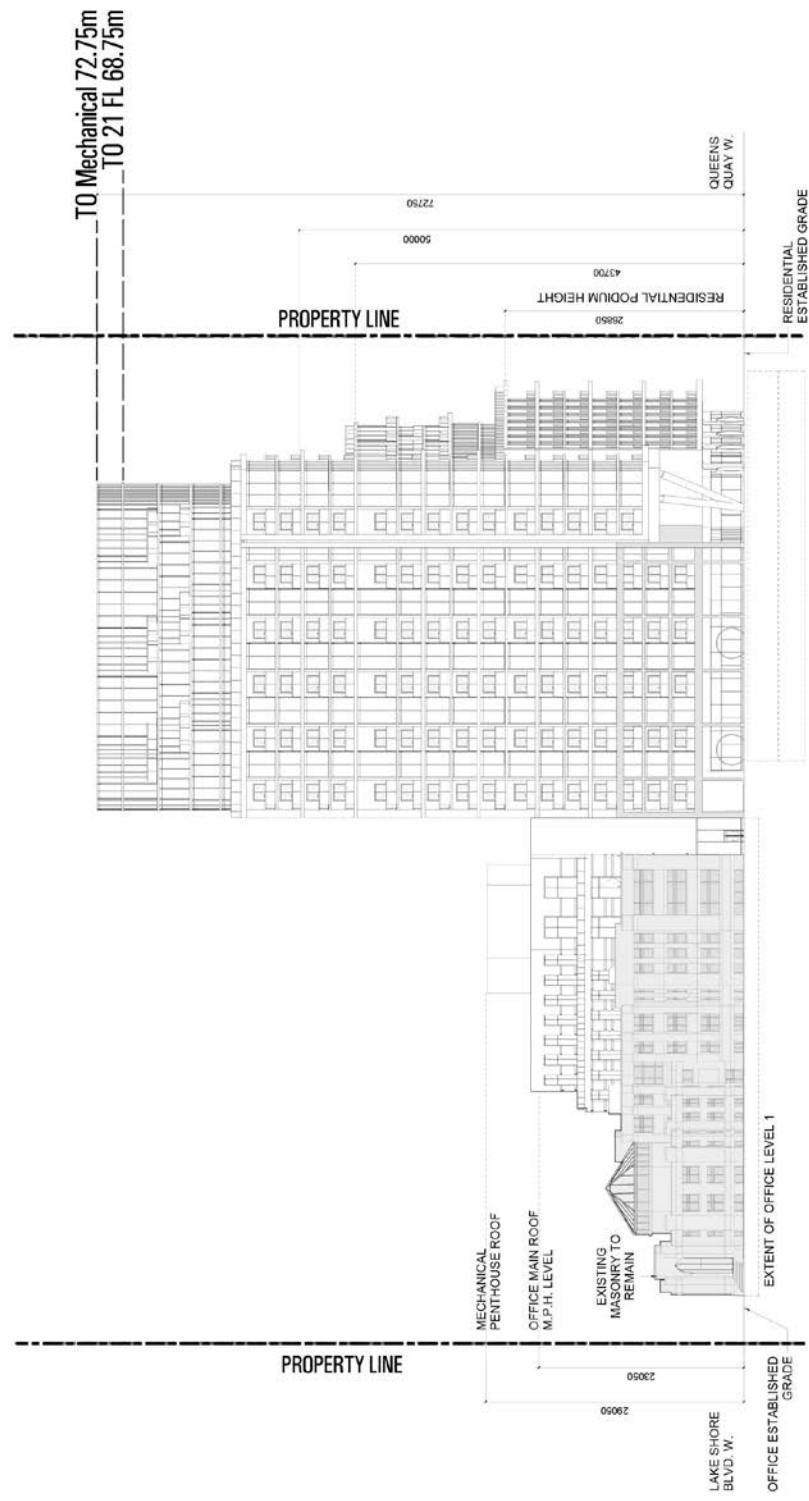
North Elevation

Attachment 8: South Elevation



South Elevation

Attachment 9: West Elevation



West Elevation

Attachment 10: Draft Zoning By-law Amendment Harbourfront Zoning By-law 289-93

To be available at the April 21, 2022 Toronto and East York Community Council Meeting

Attachment 11: Recommendation 3.b.v. Requirements for Garrison Creek Trunk Stormwater Sewer

- a. Prior to the issuance of any building permit included but not limited to demolition, shoring and excavation, regardless of the sequence of construction for any building on the development site, the owner shall prepare all documents and convey, for nominal consideration and at no cost to the City, an easement 7.82 metres in width centred on the existing Garrison Creek storm trunk, for the purpose of reconstruction, maintenance and repairs of the existing Garrison Creek Culvert that currently traverses the northeast corner of the site, such lands to be free and clear of all physical and title encumbrances, all to the satisfaction of the the Chief Engineer and Executive Director, Engineering and Construction Services and the City Solicitor;
- b. The owner shall be responsible, at its expense, to prepare, submit to the City for approval and deposit all required reference plans to describe the easement being conveyed in section a. above at no cost to the City to the satisfaction of the City Solicitor;
- c. Prior to the issuance of the any building permit including but not limited to demolition, shoring and excavation, regardless of the sequence of construction for any building on the development site, the owner shall prepare and submit an independent report prepared by a professional engineer addressing all the items for the protection and monitoring of the Garrison Creek Culvert located within the lands related to the construction to the satisfaction of the Chief Engineer and Executive Director, Engineering and Construction Services;
- d. Prior to the issuance of any building permit including but not limited to the demolition, shoring and excavation, regardless of the sequence of construction for any building on the development site, the owner shall at its sole cost and expense, submit to the Chief Engineer and Executive Director, Engineering and Construction Services, for review and acceptance, a construction management report that includes but is not limited to the following:
 - i. all the items and requirements as per the report referred in the section c. for the protection and monitoring of the Garrison Creek Culvert located within the lands during construction;
 - ii. details of the impact of the proposed design of the building structure on the existing Garrison Creek Culvert;
 - iii. the proposed horizontal and vertical clearances between any part of the building and the Garrison Creek Culvert;
 - iv. the structural loading applied on or near the storm culvert as well as the impacts of such loading; and

- v. the construction procedures as they relate to any excavation, dewatering of the site, necessary monitoring plans (including pre-construction and post construction videos of the Garrison Creek Culvert), backfill material to be used, recommendations to avoid any impacts on the Garrison Creek Culvert, and any other information necessary in order to demonstrate that the integrity of the Garrison Creek Culvert will not be adversely impacted as a result of the proposed development or the construction of the services required for the lands;
- f. Prior to the issuance of below-grade building permits for each of the commercial and residential components of the development, prepare and submit updates/addendums to the reports or new reports as identified in sections c. and d. above as may be required by the Chief Engineer and Executive Director, Engineering and Construction Services in the context of applications for site plan approval having regard to the nature and location of the proposed construction;
- g. Implement all recommendations and requirements of the reports referred in sections c. and d. above to the satisfaction of the Chief Engineer and Executive Director, Engineering and Construction Services until the completion of the construction of the development;
- h. Provide certification from the professional engineer that the installation of the protective barriers around the existing Garrison Creek Storm Sewer has been completed to the satisfaction of the Chief Engineer and Executive Director, Engineering and Construction Services;
- i. Prior to the issuance of any building permit including but not limited to the demolition, shoring and excavation, regardless of the sequence of construction for any building on the development site, the owner shall provide the City with financial security by way of an irrevocable letter of credit in an amount of \$5 million dollars for the protection of the Garrison Creek Culvert. The letter of credit shall be in effect for the duration of the construction of the site;
- j. The owner shall provide Insurance in connection with its obligations for the protection of the Garrison Creek Culvert;
- k. The owner shall indemnify the City against all claims for loss or damage which may occur to the proposed work as result of a leak or break in the City infrastructure; and the owner shall also indemnify the City against all claims for loss or damage which may occur to the proposed work for any future construction activity upon, or within the City easement, to repair, install, modify or replace the existing City infrastructure by City forces or by contractors hired by the City to carry out this work;
- l. Prior to the issuance of any building permit including but not limited to the demolition, shoring and excavation, regardless of the sequence of construction for any building on the development site, the owner shall submit a

Construction Management Plan with all the requirements of the reports referred in the sections c. and d. above and necessary construction details to the satisfaction of the Chief Engineer and Executive Director, Engineering and Construction Services, General Manager, Transportation Services and the Chief Building Official;

- m. The owner shall prepare and submit updates/addendums to the accepted Construction Management Plan identified in section l. above or, if required, more specific Construction Practices and Procedures Plans in the context of site plan approval for the development as may be required by the Chief Engineer and Executive Director, Engineering and Construction Services having regard to the nature and location of the proposed construction;
- n. The owner agrees to obtain all necessary approval and permits as may be required and agrees to implement the provisions of the accepted Construction Management Plan, and any other related measures as required by the Chief Engineer and Executive Director, Engineering and Construction Services in the construction of services, buildings and structures on the lands to the satisfaction of the Chief Engineer and Executive Director, Engineering and Construction Services, the General Manager, Transportation Services, and the Chief Building Official; and
- o. The owner and the engineering consultant will advise Toronto Water and Engineering and Construction Services immediately of any damage to the Garrison Creek Storm Sewer witnessed on-site or through data recorded by the monitoring program.