

APRIL 2024

LIBERTY VILLAGE



PUBLIC REALM STRATEGY



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Liberty Village Public Realm Strategy

April 2024

Acknowledgements

Land Acknowledgement

We acknowledge the land within the area covered by the Liberty Village Public Realm Strategy is the traditional territory of many nations including the Mississaugas of the Credit, the Anishnabeg, the Chippewa, the Haudenosaunee and the Wendat peoples and is now home to many diverse First Nations, Inuit and Métis peoples. We also acknowledge that Toronto is covered by Treaty 13 with the Mississaugas of the Credit.

African Ancestral Acknowledgement

The City of Toronto acknowledges all Treaty peoples – including those who came here as settlers – as migrants either in this generation or in generations past – and those of us who came here involuntarily, particularly those brought to these lands as a result of the Trans-Atlantic Slave Trade and Slavery. We pay tribute to those ancestors of African origin and descent.

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1.0 Introduction

1.1 Purpose

The Liberty Village Public Realm Strategy introduces a vision for high-quality streetscapes, parks and open spaces, and right-of-way connections. It also provides principles, guidelines, and an implementation strategy to create a well-connected, walkable, attractive, safe, functional, and accessible public realm.

The strategy includes short, medium and long-term priorities to be implemented through collaboration with the Liberty Village community including residents, businesses, property owners, employees and other key community stakeholders.

This strategy is a vision document and not a master plan and does not make any changes to land use. It identifies opportunities for public realm enhancements to be delivered through individual development applications, partnerships, or capital works. The strategy may be changed or altered to respond to growing needs and trends.

The strategy is informed by City policy, community input, and technical analysis, including the Liberty Village Streets Plan and Liberty Village Precinct Parking Study. The Streets Plan has assessed existing and planned conditions in the study area and identified and recommended traffic operations and street design measures to improve road safety, traffic, and mode choice in the neighbourhood.

1.2 Background-Existing Public Realm Network

Liberty Village emerged as an industrial area in the 1850s when two rail lines expanded westwards into the undeveloped Garrison Commons area to facilitate growing industrial demands. This rail expansion served to establish

the physical boundaries for the future Liberty Village, and industrial uses soon followed.

By the late nineteenth century, Liberty Street had developed as the east-west spine for the neighbourhood, connecting Bathurst Street to Dufferin Street. Additional north-south streets were established during the nineteenth and early twentieth centuries with the roads located in the west end of Liberty Village connecting to King Street West.




The proximity to the rail lines was key to the industrial operations. As the area developed, additional spur lines and road linkages connecting to the main rail lines were built, resulting in the street layout in the area we see today. Additional internal street connections were added over time to the east of the area and are bounded by the rail line to the north. All the internal roads terminate at the southern rail line. By the early twentieth century Liberty Village was an established industrial hub.

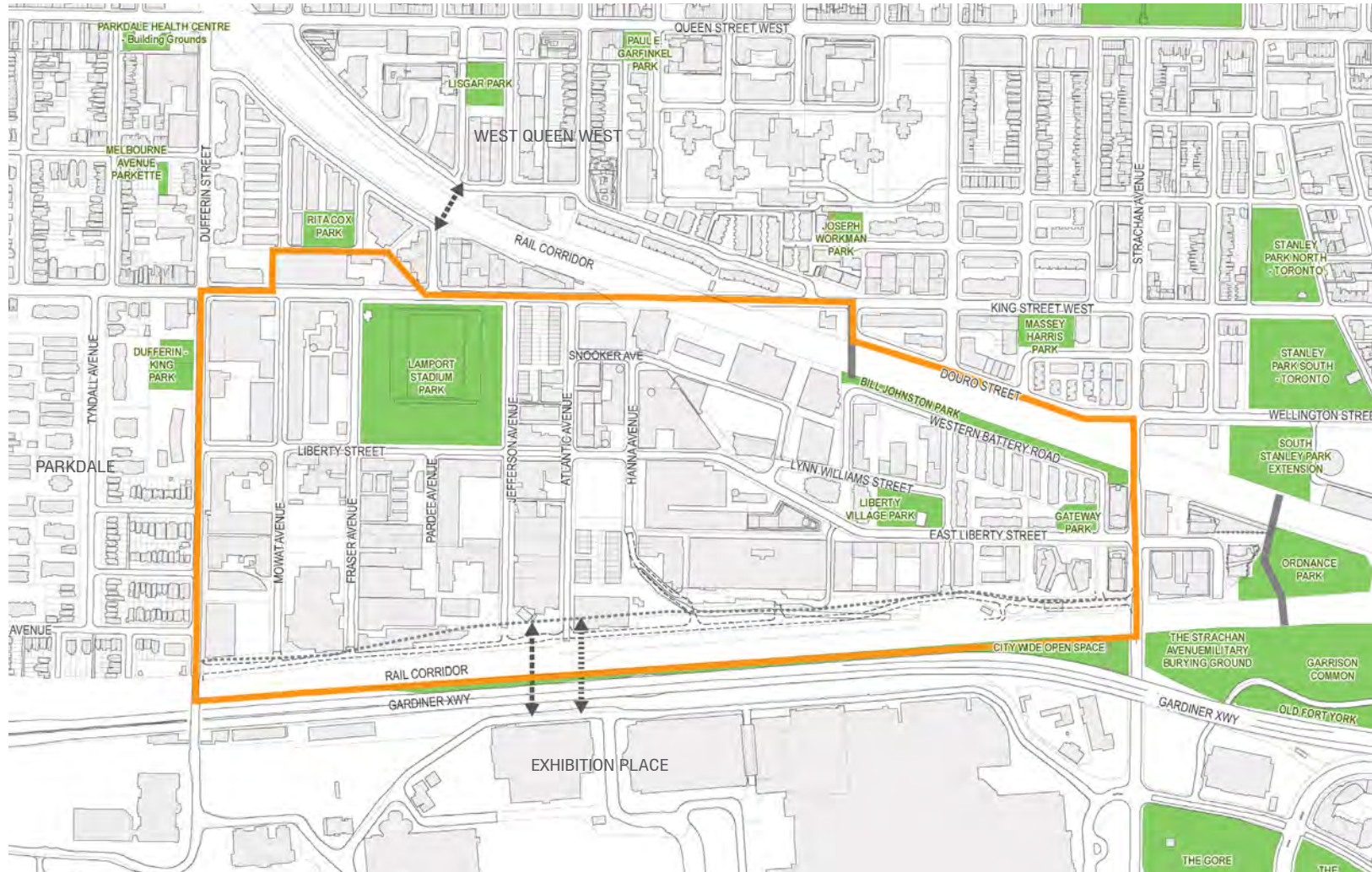
The end of the Second World War saw the beginning of the decline of the industrial hub as many industries relocated out of the city. Eventually, artists began to move into the empty factory buildings and established live/work spaces and art collectives. In the 1990s and early 2000s, Liberty Village garnered interest of employers and the historic industrial buildings were adapted for office uses. This was soon followed by an increase in residential buildings concentrated in the eastern half of the neighbourhood.

Today the study area has two distinct areas. On the east side of Liberty Village, between Hanna Avenue and Strachan Avenue, a number of residential buildings were constructed. On the west side of Liberty Village, between Dufferin Street and Hanna Avenue, existing buildings have been adaptively reused for new office, creative and information technology spaces.

1.3 Study Area

The boundaries of the Study Area are: King Street West and the CN/CP rail corridor to the north, Strachan Avenue to the east, the GO/Metrolinx rail corridor and the Gardiner Expressway to the south, and Dufferin Street to the west.

- Legend**
-  Study Area
 -  Existing Parks
 -  Existing Pedestrian and Cycling Bridge
 -  Future Pedestrian and Cycling Bridge
 -  Exhibition GO Tunnel
 -  Future Liberty New Street:
 -  2022 schematic amended alignment Ontario Line Environmental Impact Assessment Report
 -  2016 Environmental Assessment



Map 1: Study Area

Character Areas

West Liberty Village: Employment Area

The western portion of Liberty Village includes low to medium scale commercial and former industrial buildings, surface parking lots, and a City park containing Lamport Stadium. Numerous industrial heritage buildings within the area contribute to Liberty Village's unique built character, many with heritage attributes.

Owing to its primarily industrial past, portions of the streets and sidewalks in Liberty Village west of Hanna Avenue often have poor pedestrian environments with limited tree planting and street furniture and in some instances, boulevard parking. The Liberty Village Business Improvement Area (BIA) has made some improvements to these streetscapes.

The lands in this area were designated in the Official Plan as Employment Areas; however, through the Municipal Comprehensive Review process and a 2024 settlement on Official Plan Amendment 231 that applies to these lands, this area has now been redesignated Regeneration Areas to permit a mix of employment and residential uses.

East Liberty Village: Mixed-Use Residential Area

The lands in Liberty Village south of King Street, east of Hanna Avenue to Strachan Avenue, were part of a redevelopment of former vacant industrial lands known as the Inglis Manufacturing Lands. Over the last two decades, the former industrial lands have redeveloped into a mixed-use community containing retail, townhouse complexes, and residential towers. As a result of these redevelopments, the population in Liberty Village has increased.

This character area is designated Mixed Use Areas in the Official Plan, a designation that reflects the current urban fabric of the area.

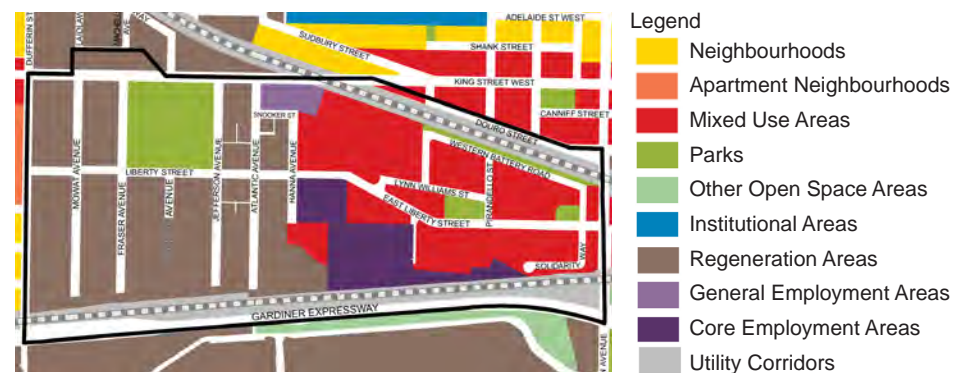
Surrounding Area

(Refer to map on page 5)

Further south, on the south side of the rail corridor, are Exhibition Place and Ontario Place. A below grade pedestrian connection through the Exhibition GO transit station connects these areas to Liberty Village, in addition to the east and west periphery streets at Strachan Avenue and Dufferin Street. In preparation for FIFA 2026, the BMO Field, located in Exhibition Place, will add an additional 17,756 seats, which increases the total capacity to 47,736 seats.

The area east of Strachan Avenue has also experienced mixed-use redevelopment in the form of towers. Two recently delivered parks, Ordnance Park and a new park at 801 Wellington Street West sit on either side of the rail corridor and connect north to Stanley Park. Southeast of the study area is the Garrison Common Park and Fort York National Historic Site.

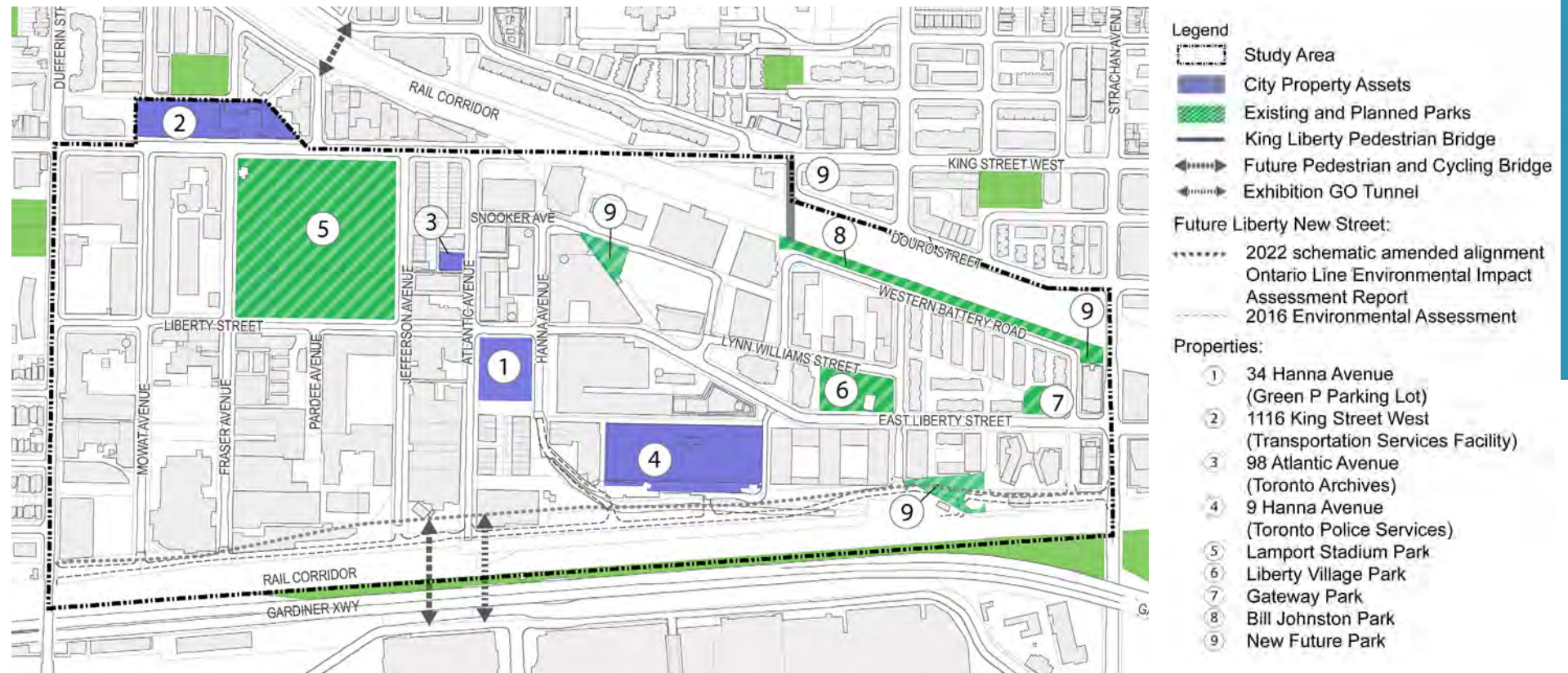
To the west of Dufferin Street and north of King Street is the Parkdale community. This community has a mix of residential forms, ranging from single family to apartments interspersed with institutional uses, community uses, parks, schools and places of worship.



Map 2: Official Plan Land Use Map

1.4 City Assets

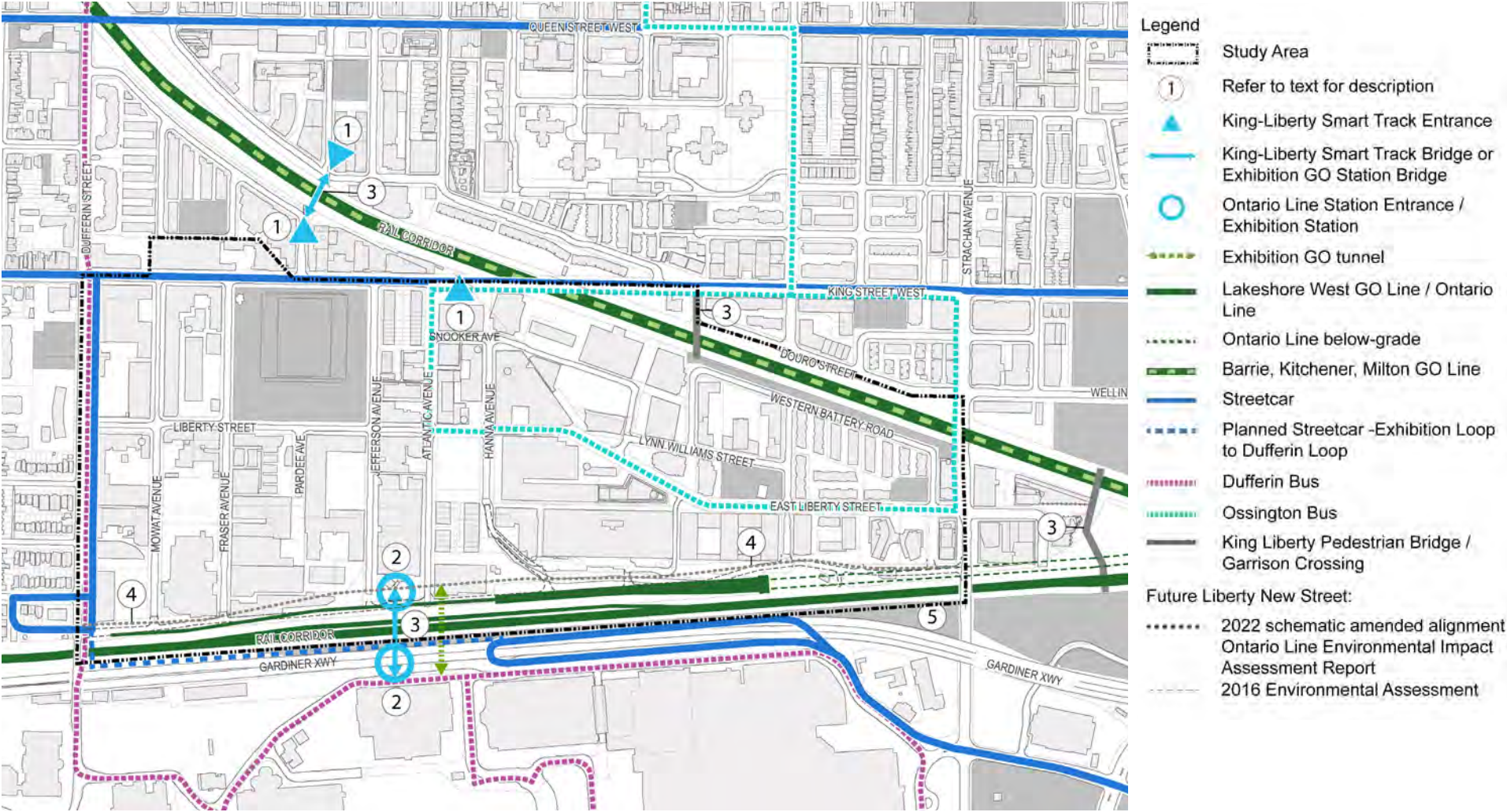
The City of Toronto owns a number of sites within Liberty Village. These sites present an opportunity for the City to provide community facilities, parks and open spaces, and public realm enhancements, provided funding is directed to these city-building opportunities.



Map 3: City-Owned Sites

1.5 Infrastructure and Special Projects

With an expected number of transit infrastructure projects in the broader area for SmartTrack, Ontario Line, Exhibition GO, the planned Liberty New Street, and the King Liberty pedestrian bridge, there is a need to plan for increased mobility and pedestrian connections throughout the area.



Map 4: Transit Network, Infrastructure and Special Projects

Refer to map on page 8 for locations.

1. King-Liberty Station

The King-Liberty Station is a planned Smart Track station with entry access from King Street West, from Joe Shuster Way, and from Sudbury Street on the other side of a bridge access across the rail corridor. In June 2023, early works construction has started on the station. The completion date is targeted for 2028.

2. Exhibition Station

Exhibition Station will see improvements such as relocated north and south platforms (to accommodate the new Ontario Line), an extension to the existing tunnel leading to a new entrance, and new exit building located off Atlantic Avenue with an upgraded elevator at the south side of the station. Temporarily, the station will include a pedestrian bridge connecting the north and south sides.

3. Pedestrian Bridges

Connections across the rail corridor to the north of Liberty Village are provided by the recently constructed King Liberty pedestrian bridge to improve links to Queen Street and areas further north. The Garrison Crossing south bridge brings people from Liberty Village to Fort York and the waterfront.

The planned addition of a Smart Track pedestrian bridge linking Joe Shuster Way and Sudbury Street/Abell Street will provide a western crossing. The south connection across the rail corridor at Exhibition GO station is planned for a bridge in addition to capacity improvements to the existing tunnel.

4. Liberty New Street

The future west-east Liberty New Street will be transformative for the area. The City's approved 2016 Environmental Assessment plan for Liberty New Street was modified through Metrolinx in 2022 to adjust the alignment north due to their expanded rail corridor requirements to accommodate the Ontario Line. The street includes two lanes of traffic, a bi-directional cycling route on the south side, and sidewalks on both sides of the street. The design is in-progress and will be combined with the improvements to Exhibition Station as well as the proposed Metrolinx Transit-Oriented Development at 1 and 1A Atlantic Avenue and 1 Jefferson Avenue.

5. The Bentway

The Bentway is a public space underneath the Gardiner Expressway, providing a year-round program of events and activities. Currently the Bentway is located south of Garrison Common and its programming is expected to expand further west and east.

2.0 Public Realm Strategy

The industrial history of Liberty Village has defined the distinct character of its public realm. The area is characterized by a series of fine-grained routes and connections through the neighbourhood that are separate from the primary street network. This series of connections are reflective of the industrial landscape typology that developed from the 1850s through to the First World War.

As the Industrial Revolution advanced in the nineteenth century manufacturing and industry became increasingly specialized. This often necessitated the construction of: a series of functionally interdependent buildings each with their own uses organized around a central internal open space; or exterior connections to facilitate the factory operations. This typology has been described as resembling a cloister or campus due to this interrelationship between the built form and open spaces.

Today, residents of and visitors to Liberty Village utilize these routes and mid-block connections that have transformed into active, retail and service-rich spaces.

The Public Realm Strategy seeks to expand and enhance this distinct character while identifying improvements for a greener, safer, and more active public life in Liberty Village.

2.1 Objectives

Seven objectives guide the creation and application of the Strategy. These objectives are:

- Create new parks and open spaces, and expand and enhance existing parks and open spaces;
- Increase soft landscaping;
- Maximize tree planting with conditions for mature growth;
- Provide pedestrian-priority design;
- Retain, enhance and expand existing mid-block connections and identify potential new mid-block connections;
- Enhance priority streets for improvements: Liberty Street and East Liberty Street, Jefferson Avenue, Atlantic Avenue, and Hanna Avenue;
- Encourage active transportation such as walking and cycling;
- Improve connections to the parks and open space network surrounding Liberty Village; and
- Expand and enhance connections to the wider public realm network.



2.2. Existing Public Realm

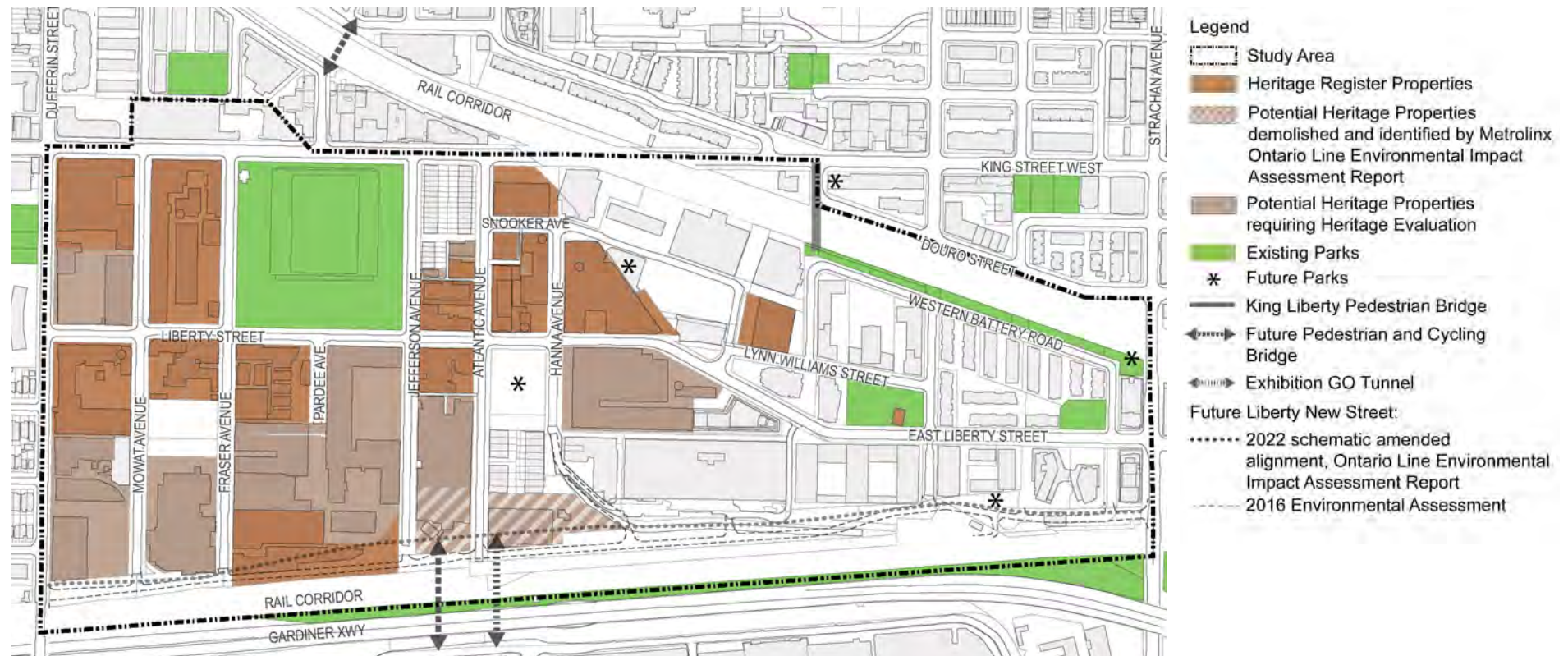
2.2.1 Existing Historic Fabric

Liberty Village, particularly west of Hanna Avenue, is defined by its historic industrial use and its associated extant warehouse buildings and complexes of the late-nineteenth to mid-twentieth-century. Numerous industrial buildings provide the area with this character, some of which are listed or designated on the City of Toronto's Heritage Register.

Other historic buildings with heritage potential may be added to the Heritage Register following heritage evaluation. Plans to expand the public realm, including the location of open spaces, must consider and where possible,

enhance the cultural heritage value of properties on the Heritage Register and the industrial character of the area.

The map below identifies properties that are on the Heritage Register, as well as properties, constructed prior to 1965, that require heritage evaluation to determine their cultural heritage value or interest according to Ontario Heritage Act Reg. 9/06. By the year 1965, the industrial development of the Liberty Village area that defines the character of the area was largely complete.

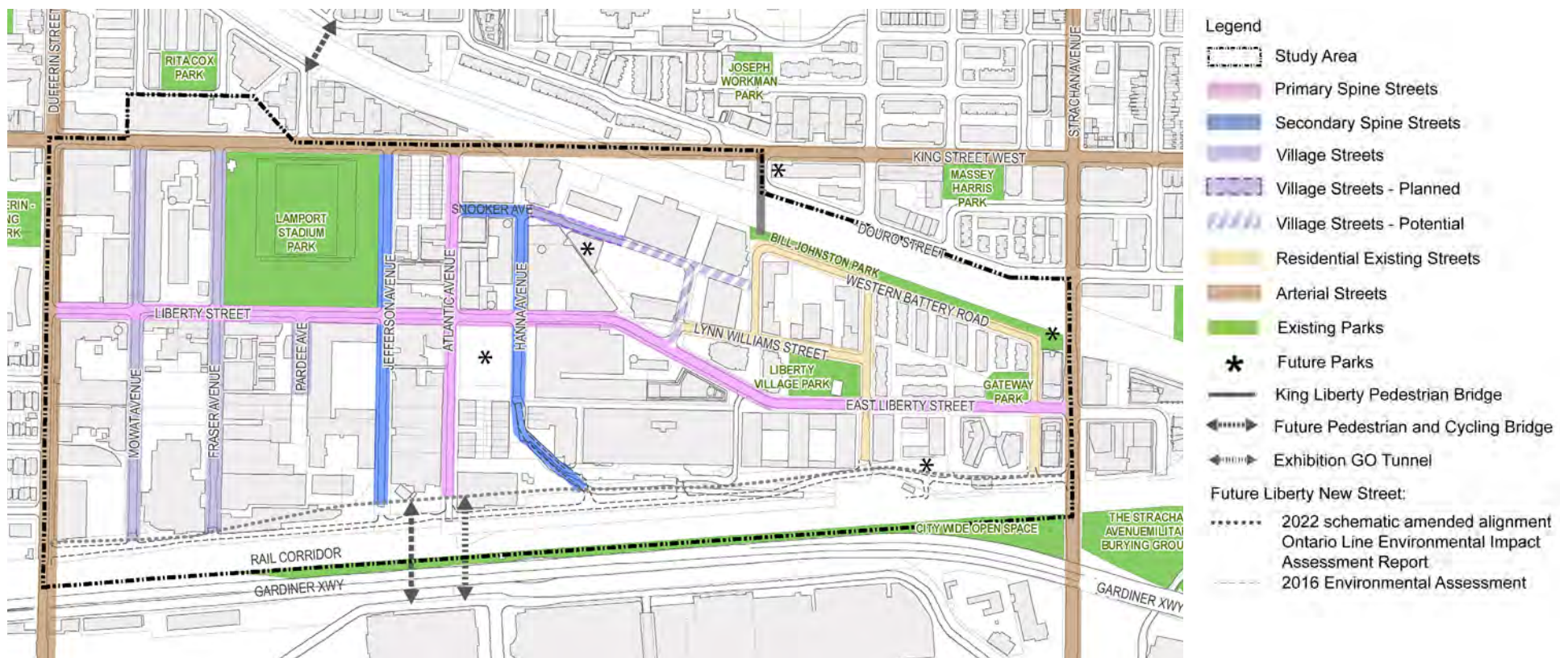


Map 5: Existing and Potential Heritage Properties

2.2.2 Existing Street Types

The map below shows street types in Liberty Village. The primary spine streets are Liberty Street/East Liberty Street and Atlantic Avenue. The secondary spine streets are Jefferson Avenue, Hanna Avenue, and Snooker Avenue. These streets define the central village area, which will be complemented by the new planned park at 34 Hanna Avenue.

Surrounded by arterial streets and the rail corridor, the streets within Liberty Village do not extend past the area to the wider street network. However for pedestrians and cyclists, the existing and new bridges and tunnel will provide important links into and out of Liberty Village.



Map 6: Street Types

2.3 Parks and Open Spaces

Introduction

Parks and open spaces play a critical role in supporting vibrant, liveable and healthy communities and environments, especially in densely populated neighbourhoods like Liberty Village. As Liberty Village continues to grow and change, so too must the parks and open space that serve this community. The City is actively pursuing opportunities to deliver new parks and open spaces and enhance existing parks, guided by the City's Parkland Strategy.

2.3.1 Parks

The Parkland Strategy provides a long-term and strategic park planning framework for guiding investment in parkland across the city. The Parkland Strategy sets out four guiding principles which align with and inform the recommendations of the Liberty Village Public Realm Strategy. These include:

- **EXPAND** the parks system by creating new parks to support growth and address gaps to ensure an effective parks system that will support the needs of a livable, diverse city.
- **IMPROVE** the function and enhance the visual appeal of existing parks, to promote community cohesion, ecological sustainability, and health and wellbeing through active living, access to nature, and the provision of spaces for rest, relaxation, and leisure.
- **CONNECT** parks and other open spaces, physically and visually, and leverage opportunities to use other open spaces so that people, communities and wildlife have abundant access and seamless navigation to and through the parks and open space system.
- **INCLUDE** everyone by removing barriers so that parks and other open spaces are inclusive and inviting places that are equitably accessible for people of all ages, cultures, genders, abilities, and incomes.

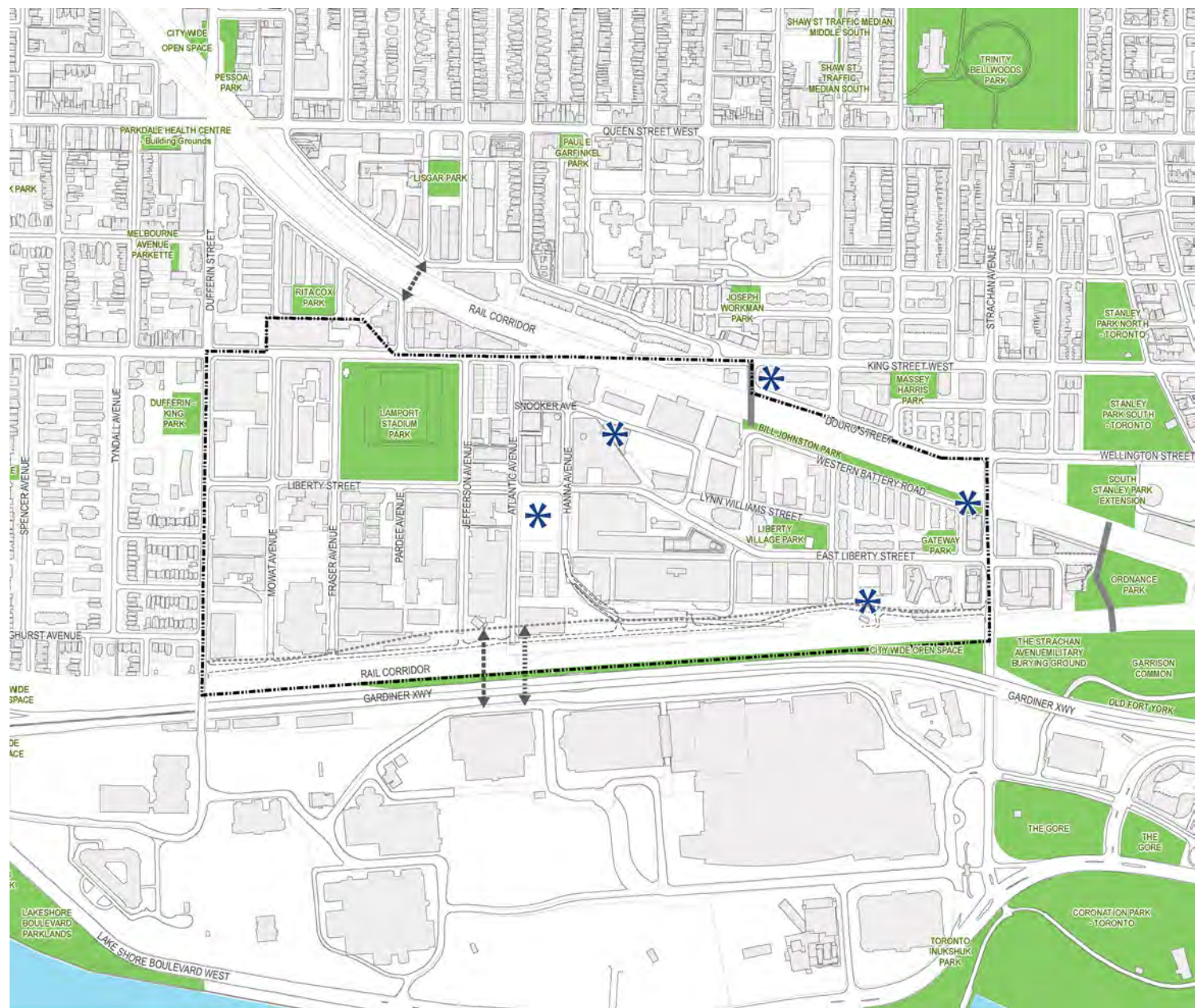
Existing Park Network

The study area includes four City-owned parks, totalling approximately 4.18 hectares or 11.9 acres, and described in the table below.

| Park Name | Address | Size | Facilities & Features |
|----------------------|-------------------------|-----------------------|--|
| Lamport Stadium Park | 1155 King St. West | 37,300 m ² | Lamport stadium, play space, outdoor parking lot, pathways, temporary respite centre. |
| Liberty Village Park | 70 East Liberty Street | 4,200 m ² | Play space, splashpad, public art, horticultural beds, seating areas, drinking fountain, pathways. |
| Bill Johnston Park | 65 Western Battery Road | 4,800 m ² | Off-leash area, drinking fountains, horticultural beds, pathways, seating areas. |
| Gateway Park | 40 East Liberty Street | 1,800 m ² | Horticultural beds, seating areas pathways. |

Analysis on parkland provision presented in the Parkland Strategy suggests that much of the study area falls below 8 square metres of parkland per person. This measure takes into account access to parkland outside of the immediate study area including parks such as Stanley Park to the east and Dufferin-King Park to the west.

The average City-wide parkland provision rate for residents is about 28 square metres. Areas under 12 square metres per person are considered to have low parkland provision. Much of the Study Area is also identified as an Area of Parkland Need, which considers several factors including overall parkland provision, anticipated impacts of growth, and portion of low income residents."

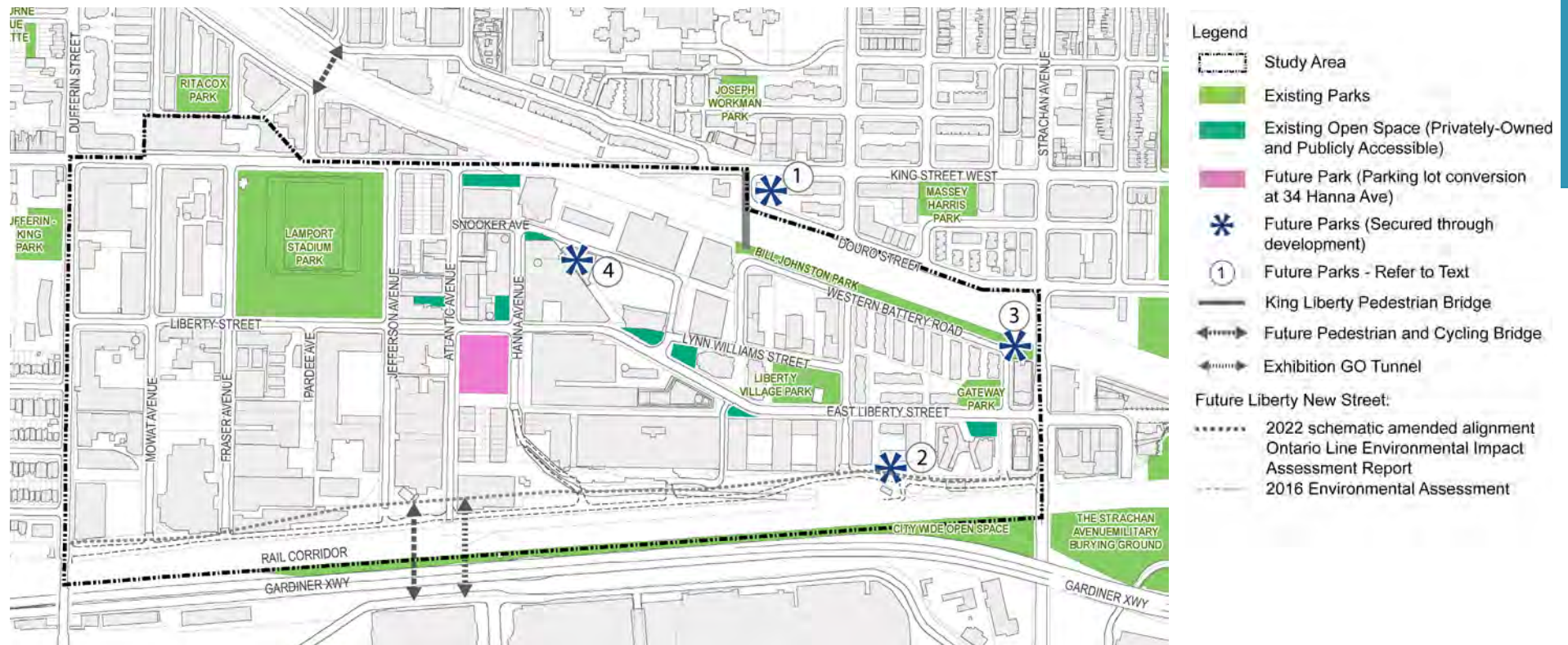


- Legend**
- Study Area
 - Existing Parks
 - ✳ Future Parks
 - Existing Pedestrian and Cycling Bridge
 - Future Pedestrian and Cycling Bridge
 - Exhibition GO Tunnel
- Future Liberty New Street:**
- 2022 schematic amended alignment Ontario Line Environmental Impact Assessment Report
 - 2016 Environmental Assessment

Map 7: Context of Surrounding Parks and Open Spaces

Growing the Parks and Open Space Network

The City has several ways of delivering new parks and expanding existing parks. The primary tool is parkland dedication. Section 42 of the Planning Act authorizes the City to require a portion of lands under development be conveyed to the City for parkland. The City may also accept payment equivalent to the value of the parkland dedication, known as “cash-in-lieu”. The City can directly acquire land for the purpose of parks and may use the above noted cash-in-lieu reserve funds to do so. Sometimes the ownership of City lands is internally transferred from another Division to the Parks, Forestry & Recreation Division to create a new park.



Map 8: Parks and Open Spaces

Future Parks and Open Spaces

The City is planning to create a new park at 34 Hanna Avenue through an internal transfer from the Toronto Parking Authority to Parks, Forestry and Recreation. The new park will be approximately 4,900 square metres in size. This site will continue to operate as a parking lot for the FIFA World Cup 2026. The design of and the public engagement for the park is planned to start in late 2024 / early 2025 so that construction can start promptly after FIFA World Cup 2026 ends.

In addition to 34 Hanna Avenue, there are currently four new parks planned within or immediately adjacent to the study area. These parks have been secured in full or in part through parkland dedication from development. Those parks are as follows. Refer to map on page 15 for locations:

1. A new parkette, approximately 230 square metres in size, secured through on-site parkland dedication from the development at 1071 King Street West. This parkette will connect to the planned extension of the West Toronto Rail Path.
2. A new park secured through on-site parkland dedication from the development at 39-51 East Liberty Street. The size and timing of this new park will be determined in coordination with Environmental Assessments being undertaken for the Ontario Line.
3. A new park, approximately 670 square metres in size, secured through on-site parkland dedication from the development at 19 Western Battery Road. It will operate as a shared-use outdoor space with the public child care operation at 19 Western Battery Road.
4. A new park, approximately 1,250 square metres in size. secured in conjunction with the development at 61-85 Hanna Avenue. Approximately 912 square metres is being generated through an onsite parkland dedication and Council has endorsed the City's intent to purchase a further 371 square metres from Section 42 parkland reserve funds. The development would also include a privately-owned publicly accessible space (POPS).

Recent & Planned Parks Improvements

Liberty Village Park underwent upgrades in 2022 including a new splashpad, junior play areas with swings and rubberized surfacing, a senior parkour play structure, and updates to other amenities including pathways, seating, planting areas, lighting, and irrigation. These upgrades are complete and the park is open to public use.

Planned playground improvements for Lamport Stadium Park are being deferred. City staff are undertaking a review of stadium operations, management and long-term use opportunities. Any future improvements to the existing play areas will be considered in the context of this review to ensure an optimized approach to enhancing the design and functionality of this park.



Photo of Liberty Village Park improvements

GUIDELINES

1. Design Guidelines for Parks

The design and programming of new parks should respond to the following criteria:

- a. ensure a functional size, shape and topography to support a variety of programmatic opportunities;
- b. provide sufficient frontage on public streets to support physical and visual connections and to create a safer and more inviting condition;
- c. present a seamless relationship between streets, parks and other elements of the public realm;
- d. support expansion of the urban tree canopy by providing appropriate conditions for mature tree growth including adequate sunlight and soil volumes;
- e. deliver pedestrian pathways that reflect existing desire-lines between community destinations and contribute to the broader pedestrian network that is convenient, comfortable and accessible;
- f. consider opportunities for park expansion if adjacent properties redevelop, where possible;
- g. provide public amenities that foster social interaction, including seating areas, interactive public art, or play facilities like chess tables.
- h. incorporate site-specific place-making opportunities through public art, heritage interpretation and cultural expression, including celebrating Indigenous histories;
- i. continue to support a community-based planning and design process for creating interesting and engaging parks that are accessible, comfortable, and consider safety measures for day and evening;
- j. support a microclimate with comfortable sun and wind conditions to ensure that parks and open spaces can be enjoyed year-round;
- k. explore opportunities to incorporate municipal green infrastructure to enhance the ecological function of the public realm, supports storm water management and promote biodiversity, in accordance with best practices and standards;

- l. achieve a balance between hardscaped open space that can support high-volume pedestrian activity, while expanding opportunities for soft landscaping with appropriate measures to ensure the long-term health of vegetation and trees;
- m. be designed and built with high-quality and durable materials, including appropriate plantings, which facilitate ongoing maintenance and support intensity of use.

2. Park Relationship with Development

New development adjacent to parks and open spaces should respond to the following criteria:

- a. be located and oriented to maximize public access and views to parks and open spaces;
- b. be designed to have animated uses at grade;
- c. provide for casual overlook, increasing the safety of parks and open spaces;
- d. be located and designed to ensure wind conditions in parks are suitable for comfortable sitting and standing;
- e. be located and designed to maximize sunlight and minimize shadowing on parks;
- f. provide an appropriate interface between public and private lands;
- g. avoid locating loading and servicing areas, and mechanical equipment, including venting, abutting parks; and where avoidance is not possible, provide visual screens between parks and loading and servicing areas
- h. achieve appropriate setbacks between park and building face in order to address fire separation requirements and to ensure any exterior building features and amenities can be provided, serviced, and maintained wholly on the development site without impacts to adjacent park.
- i. incorporate outdoor pet relief areas on site to reduce impacts on parks and open spaces.

2.3.2 Privately-Owned Publicly Accessible Open Spaces (POPS) and Enhancements

The park network in Liberty Village is complemented by other public realm elements. A number of initiatives have taken place to expand the public realm by creating privately-owned, publicly accessible open spaces. These are valuable assets in the overall public realm network and should be continued as a provision in future development as per Official Plan policies.

There are several privately-owned but publicly accessible open spaces within the study area. Refer to map on page 19 for locations:

1. A landscaped area with public art, seating and pathway is located at the southeast corner of King Street and Atlantic Avenue.
2. There is small landscaped plaza with seating and a fountain located at 51 Snooker Avenue.
3. A larger hardscaped plaza with seating areas was delivered as part of the development at 99 Atlantic Avenue.
4. Two landscaped areas are located on either side of Lynn Williams Street near East Liberty Street, with public art, seating and planting beds.
5. A landscaped plaza with patio seating and front setback enhancements were provided as part of the 60-80 Atlantic Avenue development application.
6. Public art with a landscaped plaza will be provided as part of the 51 East Liberty Street development and is under construction.

These privately-owned spaces can play a valuable role in supporting connectivity and pedestrian amenities, but do not replace the need for publicly-owned park space which provides secure and long-term opportunities to deliver City services, programming and facilities.

The map on page 19 demonstrates areas where future POPS and/or public realm enhancements (such as landscaped areas, forecourts and wider sidewalks) should be considered in new development, while taking into account heritage and potential heritage buildings. Combinations of publicly accessible spaces with mid-block connections and view terminus opportunities should be explored.

The potential areas for open space/ public realm enhancement on properties were considered where there are:

- setbacks to the facades of heritage or potential heritage buildings and existing buildings, and gaps in building frontages.
- additional setbacks for open spaces anticipated on Liberty New Street as identified in the Environmental Assessment and Exhibition GO Station/ Development public presentations, and
- surface parking lots.

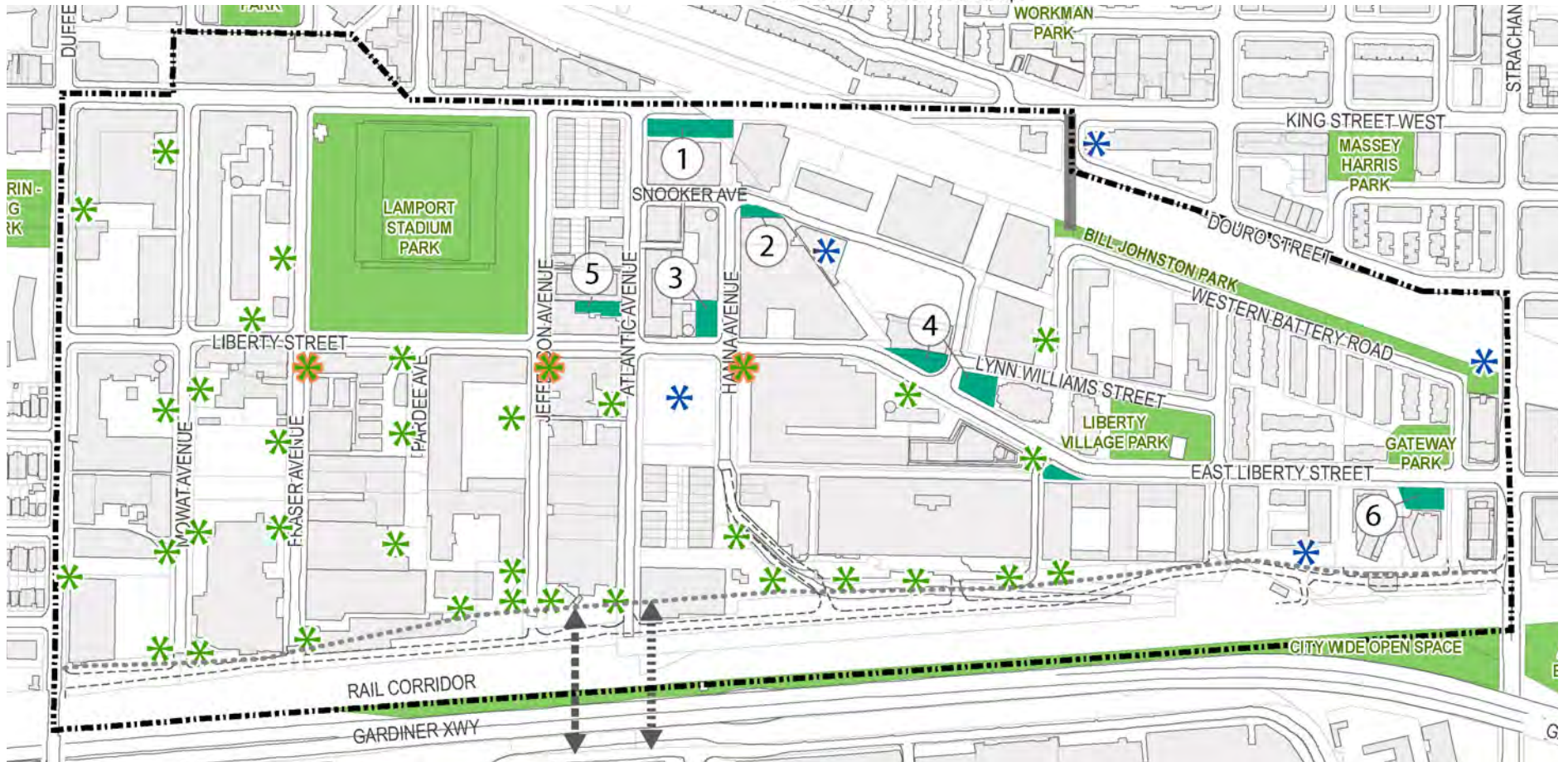
Additional sites and opportunities may be identified through development applications. The sites under study will provide different opportunities depending on their location, size and their relationship with their adjacent properties and the larger context. The size and programming of these open spaces is not determined yet and the map on page 19 only reflects potential locations for future enhancements

GUIDELINES

1. In addition to Official Plan policies and the [City of Toronto POPS guidelines](#), to address issues specific to Liberty Village, all open spaces should maximize:
 - a. tree planting with conditions for mature tree growth;
 - b. landscape planting areas and softscape, while providing seating and other amenities, such as lighting; and
 - c. seating areas with shade from trees, and sense of enclosure.

Legend

- Study Area
- Existing Parks
- Existing Open Space (Privately-Owned Publicly Accessible) / Public Realm Enhancement
- Existing Open Space - Refer to text
- Potential for Privately-Owned Publicly Accessible Open Space / Public Realm Enhancement
- Potential for Tree Planted Curb Extension to be provided with Potential Privately-Owned Publicly Accessible Open Space or Public Realm Enhancement. Refer to Curb Extensions text and map.
- Future Parks
- King Liberty Pedestrian Bridge
- Future Pedestrian and Cycling Bridge
- Exhibition GO Tunnel
- Future Liberty New Street:
 - 2022 schematic amended alignment Ontario Line Environmental Impact Assessment Report
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Map 9: Open Space and Public Realm Enhancement Opportunities
 Note: Further opportunities will be explored with each development application.

2.4 Mid-block Connections

Mid-block connections are essential assets of an urban area, providing a quieter human-scaled walking environment as compared to city streets. These connections can become vibrant places with patios, art, and other animating uses and design features. Mid-block connections also help to break up long blocks, contributing to permeability and reducing walking distances/times.

Mid-block connections are part of the character of Liberty Village and every development should retain, enhance, and expand the mid-block connection network, providing public access through Liberty Village.

The map presents existing and potential mid-block connections in Liberty Village. Mid-block connections are predominantly secured through redevelopment and, as such, the precise location of these connections will be secured through development applications. All mid-block connections must provide a surface easement for public access.

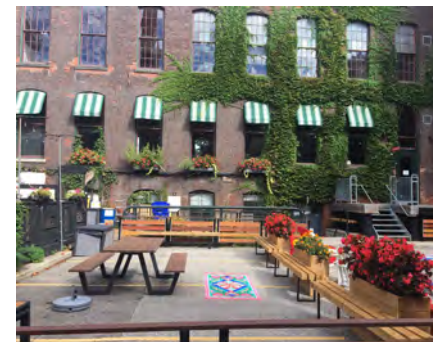
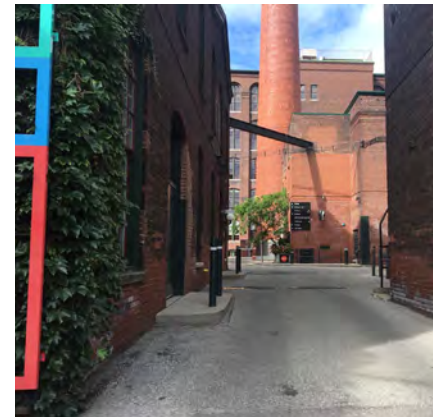
New connections or improvements to existing connections should be provided as follows. Refer to map on page 21 for locations:

a. Improve Jefferson to Atlantic Avenue private lane/ driveway:

Enhanced design pavement treatments to the private laneway will strengthen the link between the east Snooker/Hanna area to Jefferson Avenue and Lamport Park. If the block redevelops, opportunity to align a mid-block connection with Snooker Avenue should be considered.

b. Improve West-East Driveway north of Toronto Police Services building and adjacent to 171 East Liberty Street and 5 Hanna Avenue:

This predominantly privately-owned driveway functions as a public lane. The western portion north of the residential building at 5 Hanna Avenue is a public lane. It is an important link to the Exhibition GO station and south corridor bridge and tunnel. Should the adjacent sites redevelop, improvements should secure public access, pedestrian-friendly design and improved pedestrian-scaled lighting. Due to its importance, enhancements to this mid-block connection should be explored.



Existing mid-block connection in the Carpet Factory, with view from Fraser Avenue, leading to a courtyard and network of mid-block connections. This is one example in the area of combining open spaces with mid-block connections.



Existing mid-block connection at Hanna Avenue and Snooker Street. Well-designed mid-block connections can better accommodate future links and increased pedestrian volume as the city grows. This could lead to a potential future walkway on the existing King Street West rail bridge..



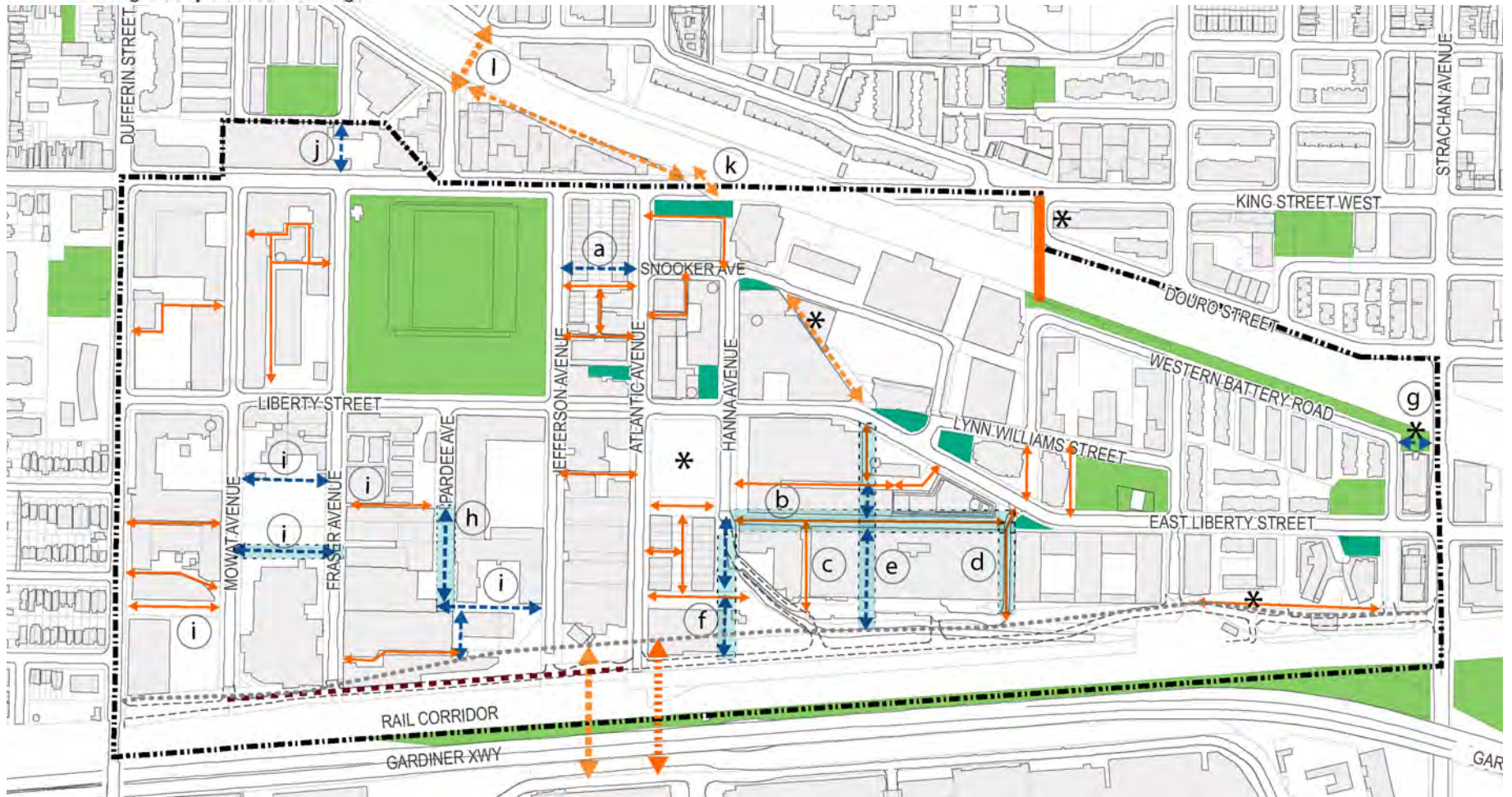
Existing mid-block connection in Liberty Market.



Existing art and special paving on the mid-block connection from Jefferson Avenue to Atlantic Avenue.

Legend

-  Study Area
-  Refer to text for description
-  Existing Mid-block Connections
-  Planned Mid-block Connections
-  Future Potential Mid-block Connections: New connections or improvements to existing
-  Enhancement for Key Mid-block Connections
-  Existing South Liberty Trail (Privately-Owned) to be replaced with Liberty New Street
-  King Liberty Pedestrian Bridge
-  Exhibition GO Tunnel
-  Future Pedestrian and Cycling Bridge
-  Existing Parks
-  Existing Open Space (Privately-Owned and Publicly Accessible)
-  Future Parks
- Future Liberty New Street:
 -  2022 schematic amended alignment Ontario Line Environmental Impact Assessment Report
 -  2016 Environmental Assessment



Map 10: Mid-block Connections

Note: Further opportunities for mid-block connections and enhancements will be explored with each development application.



a. Jefferson Avenue to Atlantic Avenue



b. West-East Driveway north of the Toronto Police Services building



c. West of the Police Services building



d. East of the Police Services building



e. North-South connection through Police Services site

c. Improve Connection West of the Toronto Police Services building:

Adjacent to residential uses, this lane should be designed to mitigate impacts such as vehicular and lighting disturbance, and provide setbacks for landscape and privacy.

d. Improve Connection East of the Toronto Police Services building:

This King West Laneway will have increased usage when the Liberty New Street is constructed, as it aligns with the existing King-Liberty Pedestrian Bridge. A sidewalk and setback to provide landscaping and privacy, with appropriate lighting as needed, should be provided with any redevelopment.

e. New North-South Connection through the Toronto Police Services site and sites from 25 to 171 East Liberty Street:

A mid-block connection through the sites to connect East Liberty Street to Liberty New Street will break down the scale of the block for pedestrians. An interior connection opportunity is shown through the potential heritage building at 171 East Liberty Street. Due to its importance, enhancements to this mid-block connection should be explored.

f. New North-South Connection through the Metrolinx 1 Atlantic Avenue site:

Due to the geometry of the proposed Hanna Avenue extension in the Liberty New Street design, a connection directly linking to Liberty New Street is needed. Due to its importance, enhancements to this mid-block connection should be explored

g. Potential Connection from Western Battery Road to Strachan Avenue:

An alternate connection to Strachan Avenue through the future expanded Bill Johnston Park just north of 19 Western Battery Road should be explored further, including its feasibility.

h. New Connections from Pardee Avenue:

An extension of the street with a mid-block connection would link to Liberty New Street to the east of the heritage property at 7 Fraser Avenue, and link to a west-east connection to Jefferson Avenue. This connection would provide consolidated servicing and loading areas similar to typical mixed use lane systems in the City.



f. Potential future mid-block connection to extend Hanna Avenue



g. Potential future connection from Western Battery Road to Strachan Avenue



h. Potential future connection to extend Pardee Avenue to the greater network



i. Potential west-east connections, Mowat to Fraser Avenue shown



j. King Street to Rita Cox Parkette

i. New West-East Connections:

Potential mid-block connections that continue the east-west connectivity are identified on the map. Existing connections should be improved. Further mid-block connection opportunities should be explored.

j. New King Street to Rita Cox Parkette Connection, through the city-owned site:

Should this site redevelop, at minimum one mid-block connection leading north from King Street West to the parkette should be provided.

k. New pedestrian and cycling access on the existing King Street rail bridge:

Potential access on the existing rail bridge would connect across King Street West, spanning from the restaurant /retail open space adjacent to 109 Atlantic Avenue and 85 Hanna Avenue to the planned King-Liberty station building. This could connect to a walkway at the rear of 1100 King Street West.

l. New King-Liberty Smart Track Station Bridge:

The planned bridge will allow pedestrian and cycling access across the rail corridor, from the Joe Shuster Way station entrance to Sudbury Street near Abell Avenue.

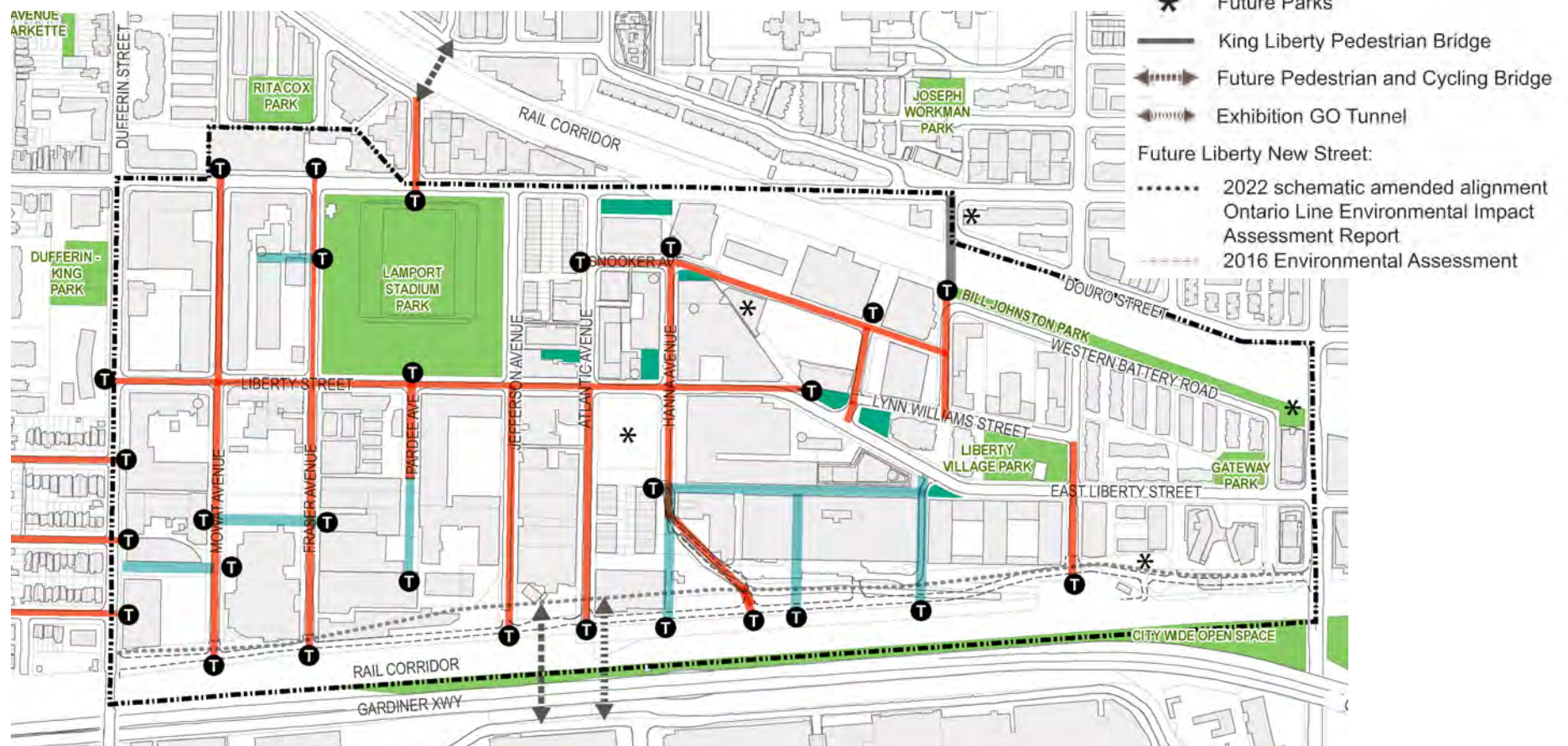
GUIDELINES

As mid-block connections continue to be a core part of Liberty Village's character, the Strategy recommends the following in the retention, enhancement, or expansion of mid-block connections. in accordance with the map on page 21.

1. Development and capital works should retain, enhance, and expand mid-block connections and network.
2. The locations of new mid-block connections should align with other mid-block connections or pedestrian routes to provide a continuous linked pedestrian network. Context block plans for development applications should indicate the mid-block connection network of Liberty Village.
3. Mid-block connections should be visible from the street, with clear sight-lines for legibility and safety.
4. Where mid-block connections include shared vehicular-pedestrian traffic, these connections should provide dedicated pathways that meet accessibility standards and limit the number of curb cuts.
5. Where mid-block connections are adjacent to residential uses, landscaping and other design solutions, such as building setbacks, should be proposed to increase privacy to those residential uses.
6. All mid-block connections should be designed as high quality, usable spaces, complying with the following design directions:
 - a. The width of the connection should be sized to be a comfortable, safe and inviting environment for pedestrians and cyclists;
 - b. Existing private and public laneways should be widened, where possible. This can allow for small format and narrow frontage retail at-grade, in conjunction with new development;
 - c. Pedestrian-scale lighting should be provided for safety and visibility;
 - d. Special pavement treatments should be considered to emphasize to the public that the connection is publicly accessible. For mid-block connections that are dedicated to pedestrians and cyclists, high quality pavement materials should be considered;
 - e. Provide durable street materials for heavier vehicles, such as garbage and delivery trucks;
 - f. Animated frontages and active uses at grade should be provided wherever possible to increase pedestrian safety, vitality and visibility, have sitting areas when appropriate, and allow for outdoor patios and other forms of spill-out activities; and
 - g. Signage or other forms of wayfinding should be provided, where feasible.

2.5 View Termini

A view terminus occurs when a street, lane or mid-block connection ends. The view at the end naturally becomes a focal point as it is highly visible and can provide an opportunity for visual interest. Attractive elements of built form, landscape and public art in these areas will contribute to the character and quality of the public realm and provide landmarks to orient the public. Due to their visual importance to the public realm, view termini should not end with undesirable conditions such as loading spaces or parking ramps.



Map 11: View Termini Locations

Note: Further opportunities will be explored with each development application.

GUIDELINES

1. The following design directions should be considered to enhance and improve the termini of views:

- a. Special landscape treatment
- b. Public art
- c. Preservation and enhancement of heritage and character-defining buildings
- d. Creating distinct architectural designs
- e. Directing unsightly elements away from these areas, such as garbage, service and parking garage access, loading areas
- f. Providing discrete signage



Photo of a view terminus opportunity on Fraser Avenue for a mid-block connection, west-east from Fraser Avenue to Mowat Avenue. Refer to map on view terminus locations. This mid-block connection is also an enhancement opportunity.



Photo of a view terminus opportunity on Hanna Avenue, at the end of the important west-east mid-block connection aligned with East Liberty Street. The east end of this connection has an existing art feature as its view terminus. This existing mid-block connection is an enhancement opportunity.



Photo of a view terminus in the King-Spadina area at the end of the laneway south of Richmond Street West, with an architectural feature at 134 Peter Street.

2.6 Gateways

Gateways are distinct entry points into a community, that can create a sense of place through public realm enhancements. Some of the gateways in Liberty Village have been improved through design, landscaping, and public art by the Liberty Village BIA. The map highlights the gateways within Liberty Village.

Creative tools can be used to accentuate these entry points, including:

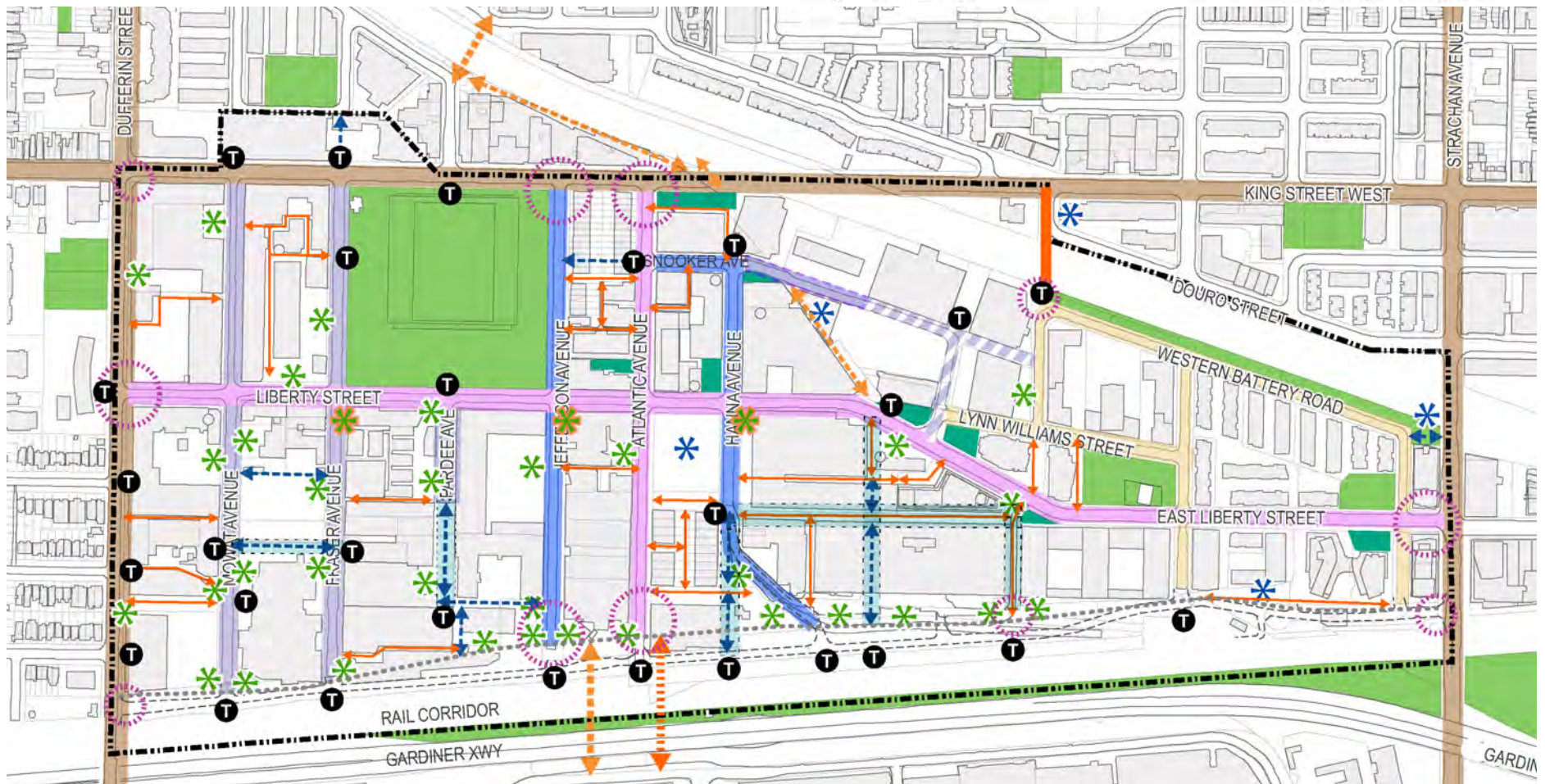
- Special landscape treatment;
- Tree planting, lighting, upgraded sidewalk treatment and street furniture;
- Landmark plazas and open spaces; and
- Public art.



Map 12: Gateways

2.7 Public Realm Structure Plan

The map below combines individual maps on gateways, mid-block connections, view termini, parks, and open space and public realm enhancement opportunities. As a demonstration map, a site by site review is required to identify further opportunities.



Map 13: Public Realm Structure Plan
 Note: Further opportunities will be explored with each development application.
 Refer also to Curb Extensions text and map, pages 37-39 for more information.

2.8 Priority Streetscape Improvement Areas

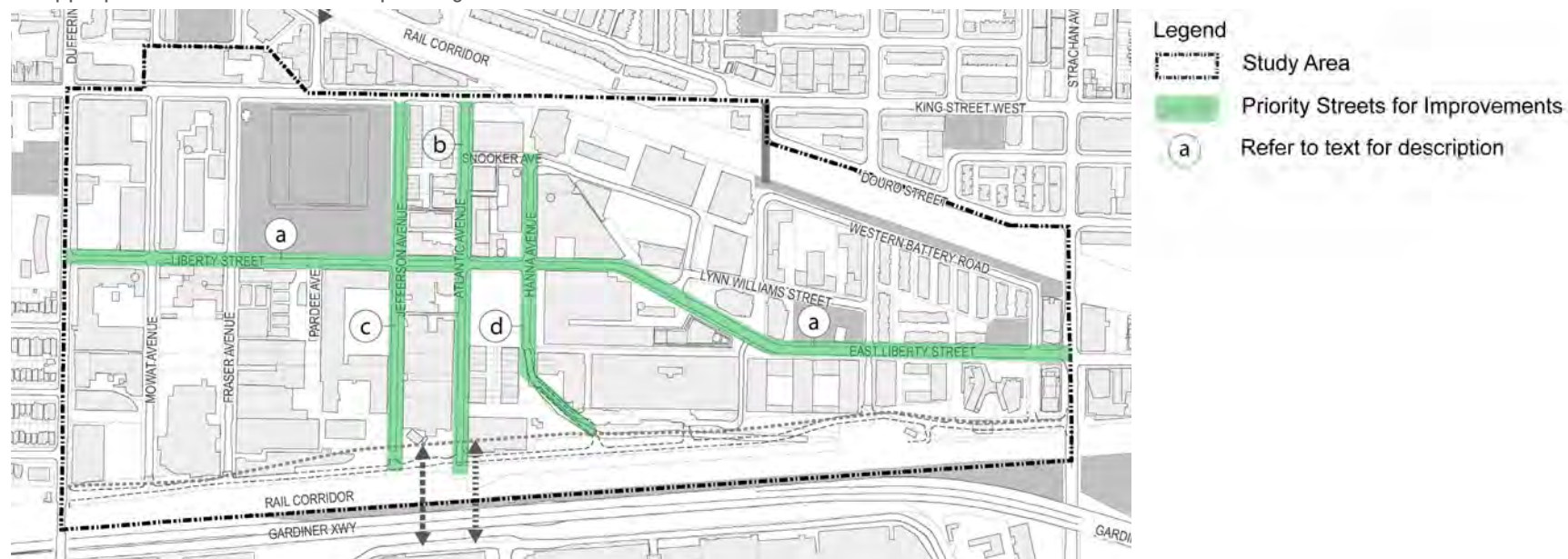
The streets highlighted in the map below indicate priority areas for improvements that were considered with the community and BIA for capital works, and can also inform new development. These improvements can include increasing soft landscaping and tree planting, increasing space for the public realm, improving safety at pedestrian crossings, and linking parks to streets.

The improvements to these priority streets vary in cost and delivery. Examples of lower cost improvements include BIA banners and pole wraps, BIA benches, pavers and building mural art. Examples of medium to high cost improvements include landscaped curb extensions, seating and planting areas, replacement of boulevard asphalt with sod/planting, tree planting with soil cells and/or continuous trenches, and pedestrian lighting. These improvements are also predicated on a review of underground public utilities and the provision of an appropriate soil volume for tree planting.

Below is a description of each priority street and its potential for improvements. Additionally, the Appendix of this Public Realm Strategy provides case study demonstrations of these public realm improvements.

a. Liberty and East Liberty Streets

Liberty and East Liberty Streets connect the west employment-dominant lands to the east residential community. As an important street across Liberty Village, improvements recommended as part of this Strategy are: considerations for the removal of boulevard parking to build new sidewalks (where none exist today) or widening of existing sidewalks; increase in soft landscaping and tree planting with conditions supportive of mature tree growth; and provision of tree soil cells under potential cycling facilities.



Map 14: Priority Streetscape Improvement Areas

b. Atlantic Avenue

Atlantic Avenue serves as a north-south spine that will connect the King-Liberty Smart Track station on King Street to the modified Exhibition GO Ontario Line station. To improve the pedestrian experience of the street for residents, workers, and transit-users, replacing boulevard parking with widened sidewalks and additional landscaping and tree planting is recommended. Opportunities to widen the sidewalk at the bus stop south of King Street West should be explored.

c. Jefferson Avenue

An increase in pedestrian activity is anticipated on Jefferson Avenue due to the future Smart Track bridge spanning the north rail corridor, with a station entry on Joe Shuster Way leading to the King Street West crosswalk and Jefferson Avenue.

South of Liberty Street, Jefferson Avenue is predominantly characterized by boulevard parking between the building and curb. Improvements recommended include: considerations for the removal of boulevard parking and sidewalk installation; tree planting; landscaped curb extensions along Jefferson Avenue acquired through new development and/or capital projects.

North of Liberty Street, improved linkages to Snooker Avenue and Hanna Avenue are recommended. A pedestrian crossing west-east across Atlantic Avenue and improved existing mid-block connection treatment would better link Jefferson Avenue to the Snooker Avenue and Hanna Avenue areas.

d. Hanna Avenue, South of Liberty Street

South of Liberty Street, providing wide sidewalks with tree planting and improving a future connection to New Liberty Street is recommended. The 2022 Environmental Assessment undertaken by Metrolinx indicates a Hanna Avenue Extension connecting to the New Liberty Street. Currently, Hanna Avenue south of Liberty Street suffers from missing sidewalks. Any adjacent redevelopment in advance of the construction of New Liberty Street should provide sidewalks and landscape as necessary.



a. Liberty Street/East Liberty Street



b. Atlantic Avenue



c. Jefferson Avenue



d. Hanna Avenue, South of Liberty Street

2.9 Existing Sidewalks and Boulevards

Due to its primarily industrial past, portions of the sidewalks in Liberty Village (particularly west of Hanna Avenue) are often poor pedestrian environments with limited space for pedestrian routes, trees and street furniture. In recent years, the Liberty Village BIA has made some improvements to these streetscapes.

One of the key issues is the prevalence of boulevard parking (parking between the building face and the curb). These areas can be further reviewed with stakeholder consultation to determine conversion to sidewalk and landscape, focusing on the priority streets and sidewalks identified.

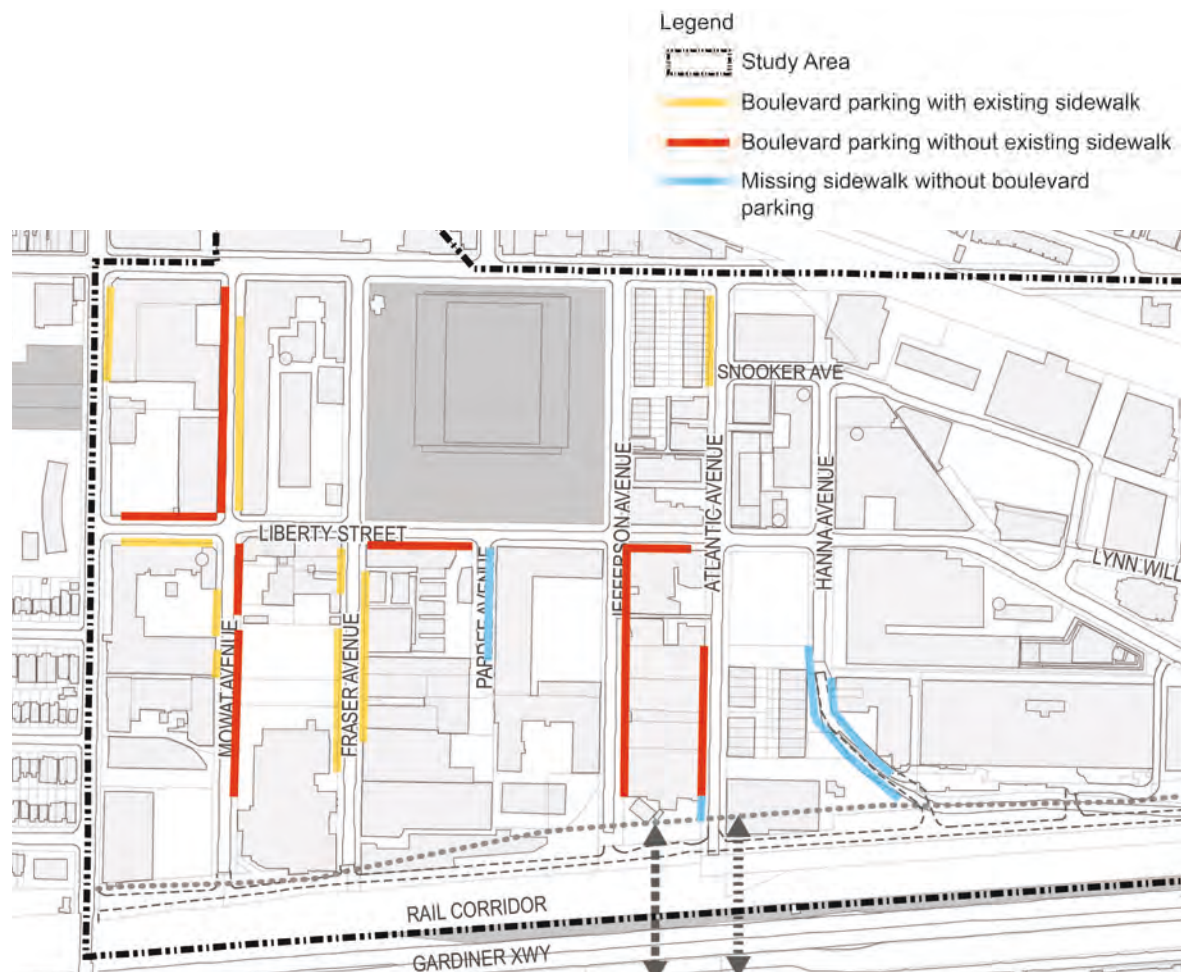


Photo of boulevard parking with existing sidewalk, on Liberty Street between Mowat Avenue and Dufferin Street.



Photo of boulevard parking without existing sidewalk, on Liberty Street at the northwest corner of Mowat Avenue.



Photo of a missing sidewalk without boulevard parking on Atlantic Avenue near the Exhibition GO

The Strategy recommends improvements:

1. Existing sidewalks can be improved through the removal of boulevard parking and should be explored.
2. Tree planting and other soft landscaping features will be installed in sidewalks, particularly for priority streets, together with major capital road work.
3. Redevelopment presents opportunity to improve sidewalks. When a development is proposed, City staff will request applicants to provide a wide sidewalk from building face to curb, in combination with tree planting and heritage building considerations, as applicable.



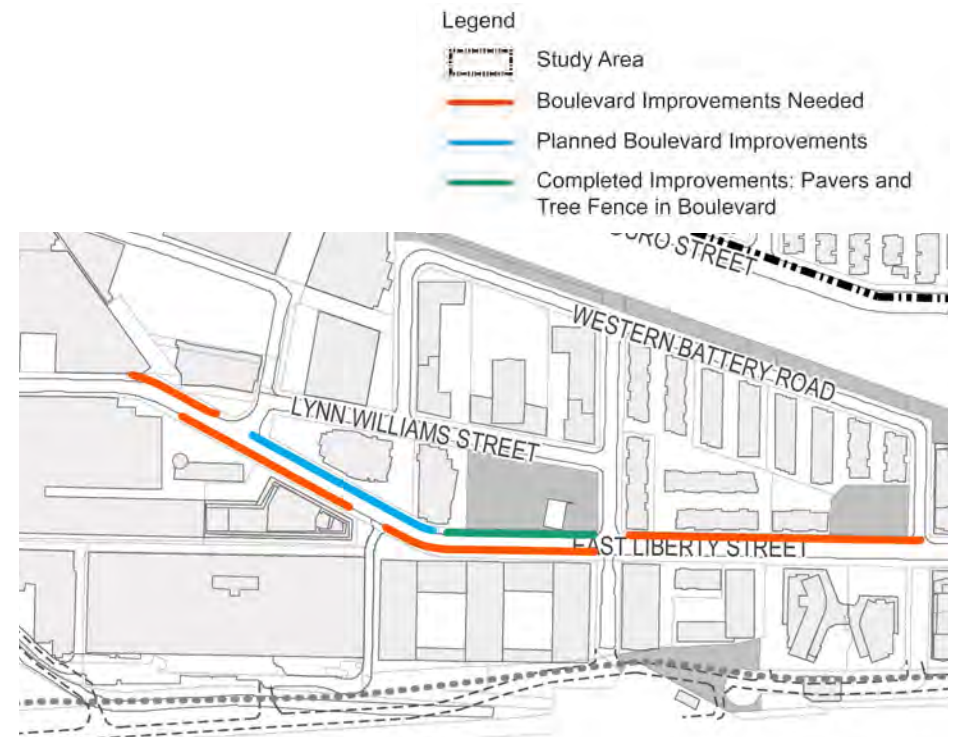
Photo of East Liberty Street boulevard needing repair due to worn-out sod and pedestrian volumes.



Photo of East Liberty Street boulevard improvements, north side, abutting Liberty Village Park

Boulevard Improvements- East Liberty Street

Due to the increased volume of pedestrians and dogs in the residential area, there are worn-out sod areas in the boulevard, identified on the map below. The existing paver and tree fence detail in front of Liberty Village Park has addressed these issues and is planned by the City to be extended to the west up to Lynn Williams Street. Additional funding will be required for other areas.



Map 16: East Liberty Boulevard Improvements

2.10 Streetscape

Historically the public realm in this former industrial area placed emphasis on vehicular movements. With the increase in residential population to date, a greater emphasis must be placed on rebalancing to address the needs for all users to provide walkability, accessibility and pedestrian comfort.

Redevelopment presents opportunity to improve sidewalks. When a development is proposed, City staff will request applicants to provide a wide sidewalk from building face to curb, in combination with tree planting and heritage building considerations, as applicable.

It is imperative to increase tree planting with conditions to achieve maturity for trees in Liberty Village, as the area has one of the lowest tree canopy coverages in the city. (Refer to Trees and Landscape section). Trees not only add greening visually but provide significant climate change mitigation: cooler temperatures for heat and holding stormwater for increases in precipitation.

GUIDELINES

In keeping with Official Plan policies, all new development will provide the following to respond to the needs of the area.

Reference Documents

1. Streetscape design, details and furnishing should be in conformity with the City of Toronto Streetscape Manual.
2. Green Infrastructure details are referred to in the City of Toronto Streetscape Manual and implement provincial policies (Growth Plan 3.2.7 and 3.2.1) to address stormwater and plan for impacts of a changing climate. New development and infrastructure projects will incorporate appropriate low impact development and green infrastructure.

3. New sidewalks or improvements to existing sidewalks through major capital road work or through redevelopment, should also be designed as Green Streets and Complete Streets in accordance with the City's Green Street Technical Guidelines and Complete Streets Guidelines.

Sidewalks

4. Sidewalk, tree planting and landscaping will be provided along frontages of the development. Boulevard parking (between the property line and curb) will not be permitted for new development.
5. Curb cuts for driveways and servicing: In addition to Official Plan policies to consolidate and minimize curb cuts for all streets, new curb cuts should be avoided on Liberty and East Liberty Streets. This is to ensure pedestrian safety on this main spine of Liberty Village.
6. Curb extensions should be considered - refer to section on Curb Extensions, page 37.
7. Continuous sidewalks should be provided where possible. Abrupt changes in sidewalk alignments should be avoided.

Streetscape Widths

8. Where there are no heritage and historic building issues constraining this space, the sidewalks widths should reflect Urban Design Guidelines such as the Tall Building Guidelines minimum 6m sidewalk width, and Mid-rise Building Guidelines minimum 4.8m sidewalk width. These sidewalk zone widths leave enough space for tree planting, street furniture, a minimum 2.1 metre pedestrian clearway, and capacity to handle anticipated pedestrian flow. These widths should also apply, as appropriate, to streets that are owned privately but which provide public access easements.

Streetscape Setbacks

9. Buildings should be set back from property lines where possible, to create wider sidewalks and provide landscaping and tree planting.
10. At intersections, particularly with transit stops, sidewalks should be widened to provide enough space for standing and easy movement. New development should provide sufficient setbacks.
11. In constrained areas with minimum flexibility to widen the sidewalk, basic improvements to the pavement material, better lighting and street furniture should be considered.



Photo of River Street near King Street East: Example of building faces that have been set back from the property line by 2-3m, with tree trunks sited at an additional 3m from the property line. This space along with soil cells have resulted in healthy tree planting.

Tree Planting and Landscape

12. Where street trees are located beside the building face, (between the property line and sidewalk) often due to utility conflicts, a minimum of 3m is required from the tree trunk to the building face, or to a structural canopy/projection, to accommodate tree maintenance, and provide health to maturity.
13. The existing streetscape in Liberty Village has predominantly sidewalks located at the curb and trees planted inboard (between the property line and building face). To retain existing trees and maintain consistent sidewalk locations, additional space from the building face to the tree zone should be provided.
14. In order to ensure that mature trees can survive and flourish along sidewalks, building cantilevers are not recommended for new redevelopment. A cantilever may be permitted together with a larger building setback. As well, weather protection, such as awnings and canopies, may be permitted, subject to review.
15. In constrained areas with minimum opportunity to plant trees within the public right-of-way (ROW), other greening improvements on private property should be considered.
16. Tree planting openings and softscape: Unless there are unusually constricted pedestrian areas or unique public realm elements, tree planting should be in large softscape planting or permeable areas, with as large an opening as possible. Due to the area having one of the lowest tree canopy coverages in the city, it is imperative that tree health and provisions for trees are a priority.

17. Tree planting with more than the minimum required soil volume is encouraged through larger tree planting areas and the use of soil cells and/or continuous soil trenches.
18. Soil for tree planting and landscape: Good quality soil will be provided to replace existing soil, particularly in areas converted from hardscape. The historically industrial nature of the area typically has poor quality soil. Refer to TS 5.10 Construction Specification for Growing Medium, specifically, "Boulevard Mix".
19. Identify constraints and opportunities for tree planting and growth with appropriate subsurface utility investigation and multi-divisional coordination.
20. When trees are not possible because of utility conflicts, provide open planters with perennials and shrubs.

Uses

21. New developments should have active uses at-grade to create vitality and vibrancy. Where space is available, outdoor patios and market zones should be provided.
22. New developments should maximize transparency on the ground level to animate streets.

Liberty New Street

23. New development should provide tree planting, landscaped setback and pedestrian zone widening as needed along the north sidewalk of Liberty New Street.
24. Should sites redevelop prior to the construction of the street, the site design will anticipate and coordinate with the Liberty New Street design.
25. Tree planting on the development sites fronting Liberty New Street is imperative for climate change considerations, given the increased solar gains from the south orientation facing a large open expanse of rail corridor. Provide tree planting for summer shade with space and details conducive to mature tree growth.

Hanna Avenue

26. An extension of Hanna Avenue to Liberty New Street is proposed with the Liberty New Street Environmental Assessment. In the case where development is proposed in advance of the construction of this extension, and where there are gaps in planned works, sidewalks should be provided along the Hanna Avenue frontage with city review.



Photo of Douro Street: Similar to Liberty New Street, Douro Street abuts the rail corridor. Tree planting and landscaped setbacks have been provided on development sites fronting the street.

2.11 Tree Planting and Landscape

Based on a 2018 tree canopy assessment, Urban Forestry estimates that Liberty Village has 5.7 per cent canopy cover - this is far below the city-wide average of approximately 28-31 per cent canopy cover. The land cover analysis also estimates that over 42 per cent of Liberty Village is made up of impervious land cover.

The predominantly residential side of Liberty Village has a larger tree canopy cover than the lands dominated by employment uses. In order to maximize tree planting, all redevelopment, regardless of whether it is privately led or City-led, should provide conditions that promote mature tree growth. Additionally, converting existing boulevard parking to expanded sidewalks with tree planting areas and planting beds, will contribute towards increasing the overall soft landscaping of the area.

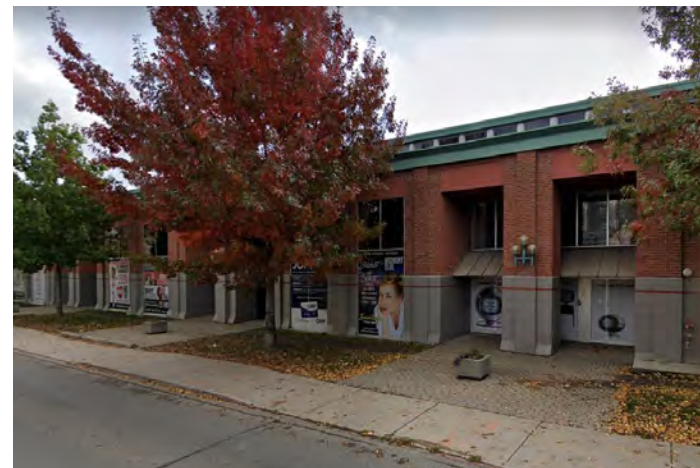
Tree Planting Objectives

With regards to tree planting, the Strategy recommends the following to improve tree planting across Liberty Village:

- Achieve soil volume requirements and new tree planting areas, and include opportunities to add additional soil volume to existing tree planting areas;
- Provide utility review to quality Level A to determine feasibility of tree planting, particularly in areas that can be converted from asphalt to planting beds;
- Improve existing soil quality, and mitigate compaction conditions; and
- Protect soil, tree planting area, and sod by using an approved tree fence or concrete surround.



Example of tree planting on King Street, with greater soil volume on the right side versus the left, providing bigger trees.



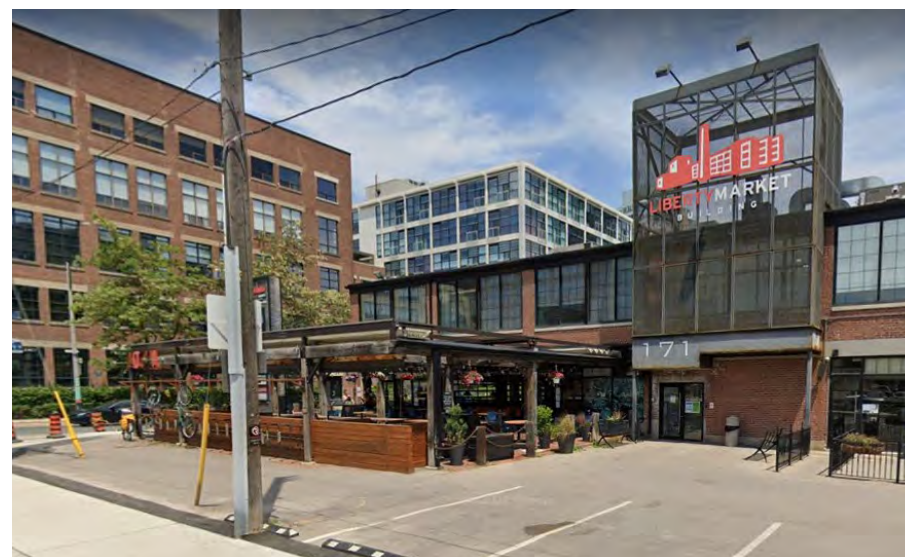
Example of tree planting on Liberty Street with wide softscape openings to promote tree health and larger trees, versus having small openings in pavement.

There also exist opportunities within the current context to add trees. Some of these opportunities include:

- King Street West near Dufferin Street, south side frontage: Removing concrete to expand the tree planting bed to improve tree health;
- Southeast corner of Hanna Avenue and Liberty Street: For this visually prominent corner, conversion of the asphalt in front of the patio area to allow for tree planting beds fronting Hanna Avenue, as well as converting parking spaces to planting, can improve the soft landscaping of this corner;
- Existing boulevard parking areas: As identified on page 33, the dimension from tree trunk to building face should be generally a minimum of 3 metres to allow the tree access to sunlight and space to mature; and
- Existing tree planting: Existing areas, such as on East Liberty Street, should be provided with soil cells beneath the sidewalk to increase soil volume and improve conditions for tree growth.
- As a general rule, all improvements and redevelopments should look to maximize soft landscaping and tree planting. The appendices include Tree Planting Demonstration Plans.



King Street near Dufferin Street: These trees would benefit from the removal of as much paving as possible, to provide larger planting beds for healthier trees.



Hanna Avenue and East Liberty Street, southeast corner: There is room for tree planting beside the patio, for this visually prominent corner. Removal of boulevard parking and conversion to softscape/ planting would improve the public realm on Hanna Avenue.

2.12 Curb Extensions (Bump-outs)

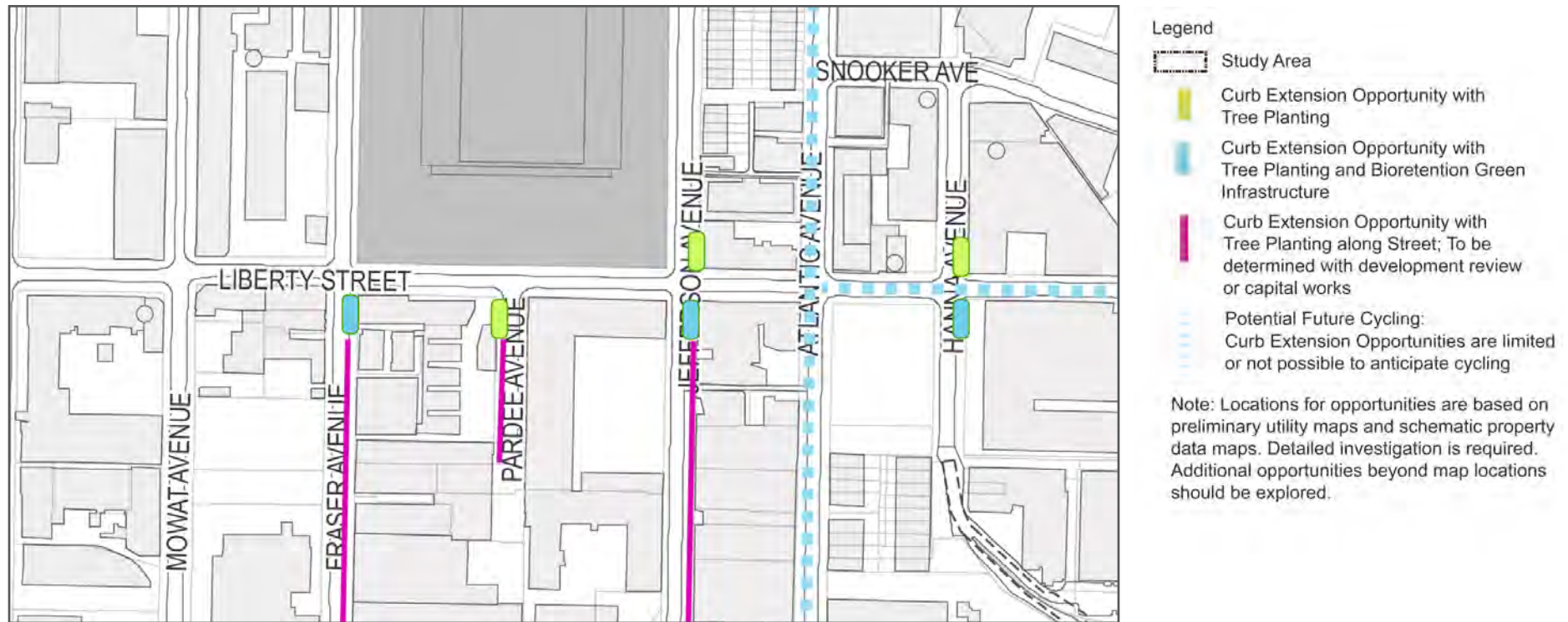
Curb extensions or 'bump-outs' are localized road narrowings where the pavement width is reduced to improve pedestrian visibility, reduce crossing distance, and control the speed of vehicles. These extensions also provide opportunities for wider sidewalks, soft landscaping, and street furniture. Curb extensions can be planned for if there is excess room in the road, subject to City approval.

All curb extensions that are considered appropriate should undergo consultation with the local BIA and follow the City of Toronto's standards and guidelines.

In addition to curb extension opportunities, due to the previous industrial history of the area with wider vehicular pavement widths, projects should consider opportunities to reduce the overall width of the road in order to apply current traffic lane width and radii guidelines.



Photos of Roncesvalles Avenue curb extensions with landscape and seating

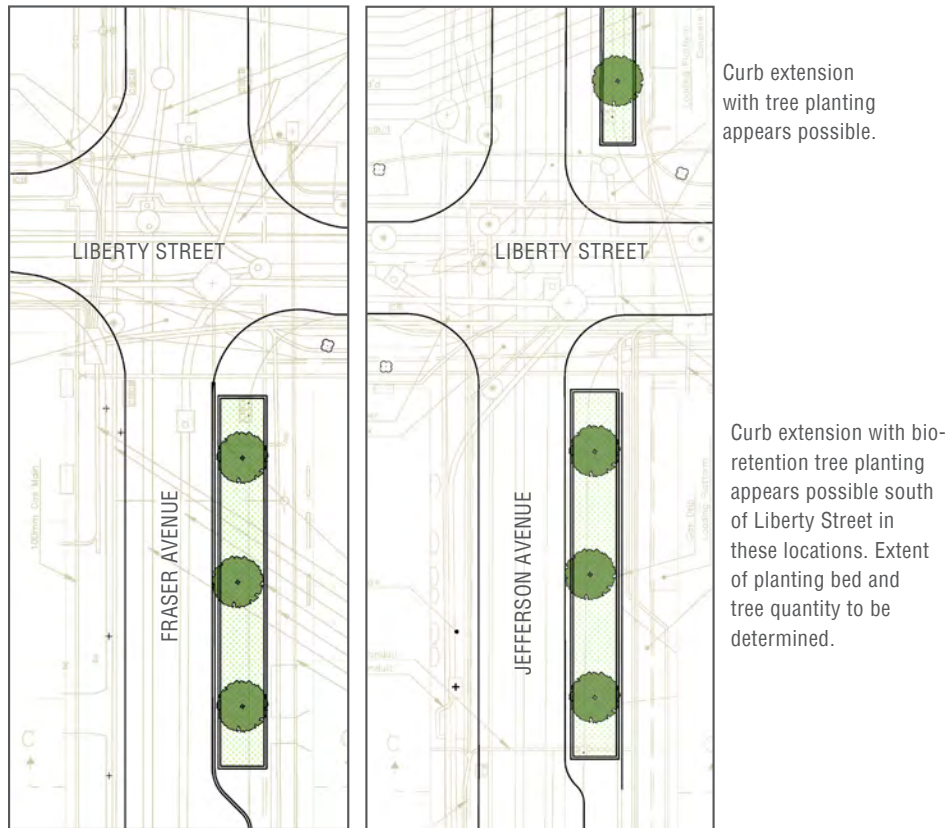


Map 17: Potential Curb Extension Locations with Tree Planting near Liberty Street

Note: Further opportunities will be explored with each development application.

Based on preliminary utility information, the demonstration map indicates areas along Liberty Street flanking streets, where curb extensions with tree planting should be provided with development. Detailed utility review is required for verification along with city review. Some areas as indicated may be able to provide trees with the wider bio-retention planting beds as per Green Infrastructure details.

- Atlantic Avenue, East Liberty Street and portions of Liberty Street indicated on the map: Curb extension opportunities are limited or not possible to anticipate space for cycling. In some areas cycling facilities and improvements to tree soil volume can be a combined detail, such as the existing cycling facility on Harbour Street.
- Liberty Street: In general curb extensions on Liberty Street do not appear to be possible due to constrained vehicular pavement widths.



Map 18: Schematic Demonstration Plans of Fraser Avenue and Jefferson Avenue for Curb Extensions with Tree Planting

These have been assessed with preliminary utility review; further detailed utility investigation is required. 3.5m wide bed to inside of curbs is shown as per as per Green Infrastructure Details.

GUIDELINES

1. Curb extensions or 'bump outs' will be considered by development where deemed appropriate by the City, in consultation with the BIA. Coordinated tree planting, landscape and sidewalk provisions within the curb extensions will be undertaken. Opportunities may exist in areas with no on-street parking, and at intersections where on-street parking is prohibited. Refer to:
 - [City of Toronto Curb Extensions Guide](#)
 - [City of Toronto Traffic Calming Guide](#)
2. Sidewalk and Boulevard: Through development review and capital projects, traffic lane width guidelines and curb radii guidelines should be used to find opportunities to maximize width of boulevard to increase sidewalk size and opportunities for soft landscaping/tree planting. Refer to:
 - [City of Toronto Standards Designing & Constructing City Infrastructure](#)
3. Curb extensions should be combined with pedestrian safety improvements such as reduced curb radii, where appropriate.
4. New development and capital works will consider locations for tree planted curb extensions. Green infrastructure should be provided where technically feasible. Refer to:
 - [City of Toronto Construction Specifications and Drawings for Green Infrastructure](#)
5. As per the map, flanking streets of Liberty St in particular (streets that intersect with Liberty Street), should be particularly prioritized for tree planted curb extensions.
6. Provide street furniture and other amenities where appropriate.

2.13 Furnishings and Lighting

All furnishings, such as seating areas, public art, bike racks, and garbage bins, should meet the standards of the City, in consultation with the Liberty Village BIA. Additional furnishings to the public realm will be implemented through major road work and redesign as well as through redevelopment. New development should provide appropriate lighting on the building facades to animate and illuminate the street.

Additional lighting in Liberty Village can assist in creating a safer public realm. The following areas should be improved with proper lighting design:

- the north-south streets south of Liberty Street, particularly Jefferson Avenue;
- areas along the east-west Toronto Police Services lane; and
- adjacent to Lamport Stadium Park.



2.14 Other Considerations

Interim Public Realm Improvements until Liberty New Street is Constructed

Consider interim public realm improvements until such time as the Liberty New Street is constructed. These improvements can include north and south street terminations and pedestrian connections east and west.

Billboards

Removal of billboards, particularly those billboards that abut residential uses.

King Liberty Pedestrian Bridge

The community have raised concerns regarding security and maintenance with the King Liberty pedestrian bridge, stair enclosures, and elevators. The City is monitoring the situation and will bring forward safety measures such as additional lighting and graffiti management, as appropriate.

An upcoming Mural Project is planned for the Douro Street entrance to the bridge through StreetARToronto. StreetARToronto is a program through the City's Graffiti Management Plan that addresses graffiti and vandalism in an innovative manner through community-engagement street art while making a positive contribution to the public realm.

3.0 Action

The implementation of this Strategy depends on the City, its partners in the development industry, and the Liberty Village BIA. There are actions that can be made relatively quickly and do not require Council approval or lengthy design and review periods. Other actions that are more complex, impact a wider area, or require major capital work can take time.

The action plan below outlines three phases of implementation, with approximate timelines.

Short Term Action (6-18 months)

Short-term implementation measures typically take place between 6-18 months. These measures do not generally require Council approval, are relatively small/located intervention, and require minimal data/analysis.

The following short-term implementation measures include:

- Improvements to pavement markings.
- Initiation of a community park design process for the future park at 34 Hanna Avenue.
- Review of operations, management and long-term use opportunities for Lamport Stadium.

Mid-Term (1-5 years)

Mid-term implementation measures typically take place between 1-5 years. These measures may include Council approval and further public engagement, as well as further study. The following mid-term implementation measures include:

- Conversion of parking lot to public park at 34 Hanna Avenue.
- Installation of traffic calming measures, parking regulation changes, complex pavement markings, and intersection controls.
- Development application provisions for the enhancement of public realm, including wider sidewalks, landscape and street trees.
- Pursue improvements to existing parks and open spaces through the development review process, and capital and operating planning process.






Long-Term (5+ years)

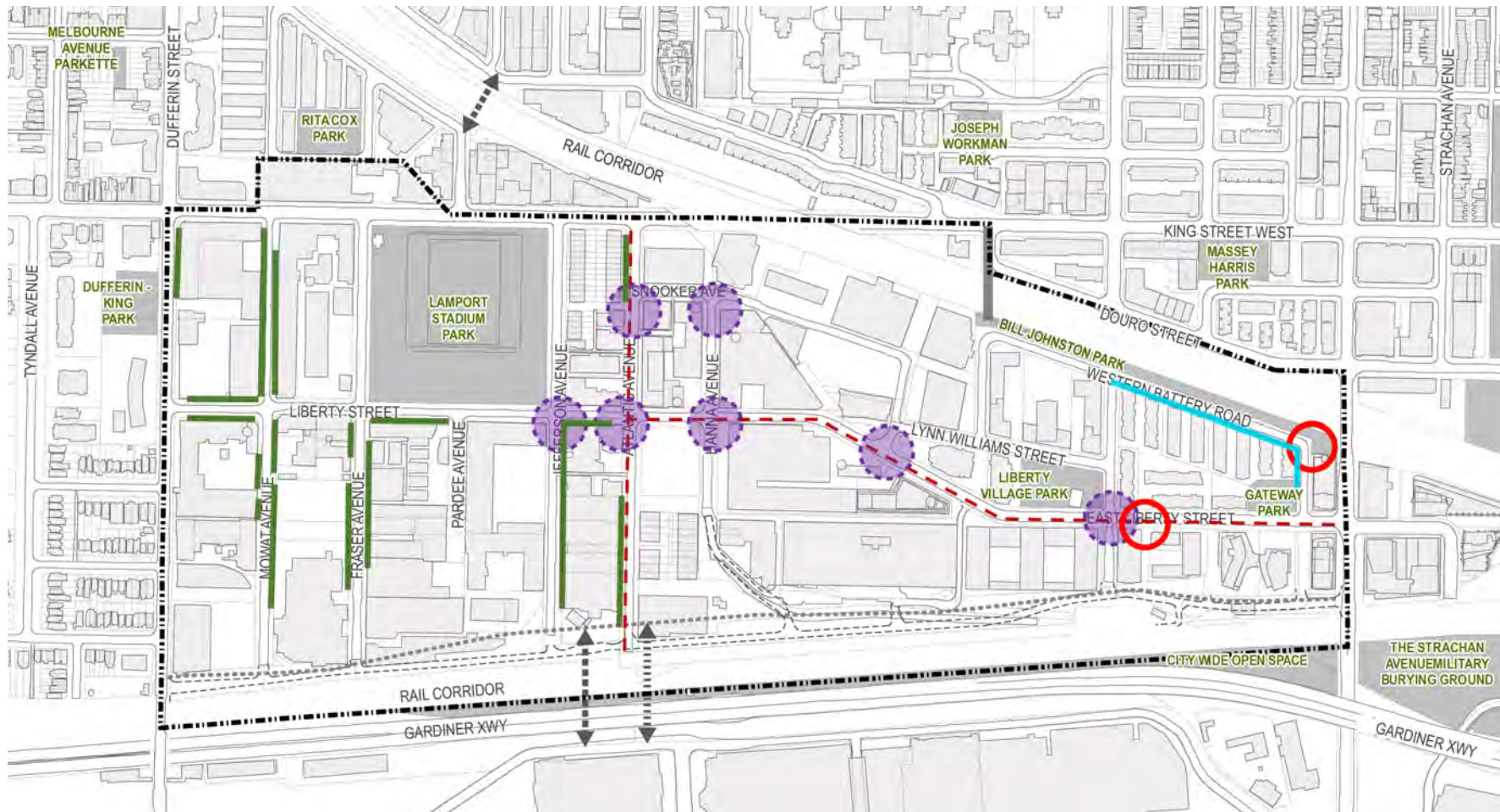
These measures take the longest to implement (upwards of 5 years); however, the City will continue to find opportunities to include these implementation measures through either redevelopment or with capital work programs.

These measures generally require Council approval, have neighbourhood-wide impacts, require further study and analysis, and require further public engagement. Some of these measures are also typically bundled with planned road resurfacing or reconstruction. The following long-term implementation measures include:

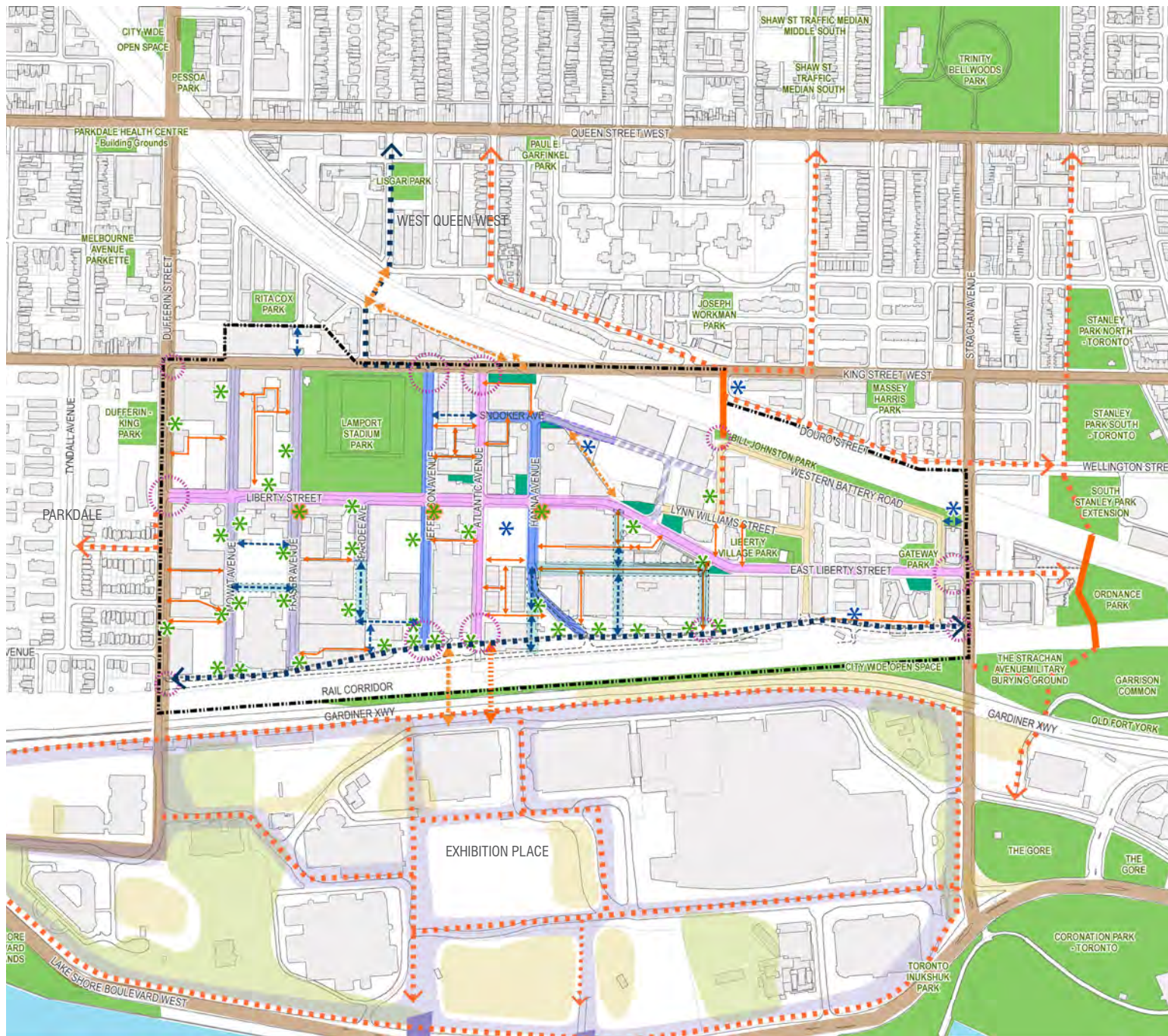
- Road re-design (i.e. sidewalks and curb extensions).
- Creation of Liberty New Street.
- Mid-block connections and privately-owned publicly accessible open spaces secured through redevelopment proposals.
- Pursue opportunities to deliver new parks or expand existing parks through parkland dedication from development, City-led acquisitions or internal transfers. Particular attention should be given to areas south of Liberty Street.

Legend

-  Intersection safety improvement (e.g. zebra marking, curb extension)
-  Potential conversion of boulevard parking to sidewalk/public realm improvement
-  Sightline improvement
-  Candidate location for speed humps
-  Potential future cycling facilities



Map 19: Transportation Streets Plan



- Legend**
- Study Area
 - Context - Key Pedestrian Links - Existing
 - Context - Key Pedestrian Links - Planned
 - Existing Parks
 - Existing Open Space (Privately Owned Publicly Accessible) / Enhancement
 - * Potential for Open Space (Privately Owned Publicly Accessible) / Enhancement
 - * Potential for Tree Planted Curb Extension with Open Space (Privately Owned Publicly Accessible) / Enhancement
 - * Future Parks
 - Existing Mid-block Connections
 - Planned Mid-block Connections
 - Future Mid-block Connections: New connections or improvements to existing
 - Enhancement for Key Mid-block Connection
 - Major Gateway
 - Minor Gateway
 - Primary Spine Streets
 - Secondary Spine Streets
 - Village Streets
 - Village Streets - Planned
 - Village Streets - Potential
 - Residential Existing Streets
 - Arterial Streets
 - King Liberty Pedestrian Bridge / Garrison Crossing
 - Exhibition GO Tunnel
 - Future Pedestrian and Cycling Bridge
- Future Liberty New Street:**
- 2022 schematic amended alignment
 - 2016 Environmental Assessment
- Exhibition Place (concept diagram shown):**
- Existing and Potential Open Space
 - Potential Flexible Open Space (programming, parking, operations etc.)

Map 20: Public Realm Structure Plan in Context

4.0 Appendices

Appendix A: Liberty New Street

The alignment of the new west-east street is being adjusted for Metrolinx requirements. The plan below is from the approved 2016 Environmental Assessment. The approved 2022 Ontario Line Environmental Impact Assessment Report included amendments to the rail corridor with increased space required in this area, however an amended Liberty New Street design to accommodate these changes is in-progress at this time.

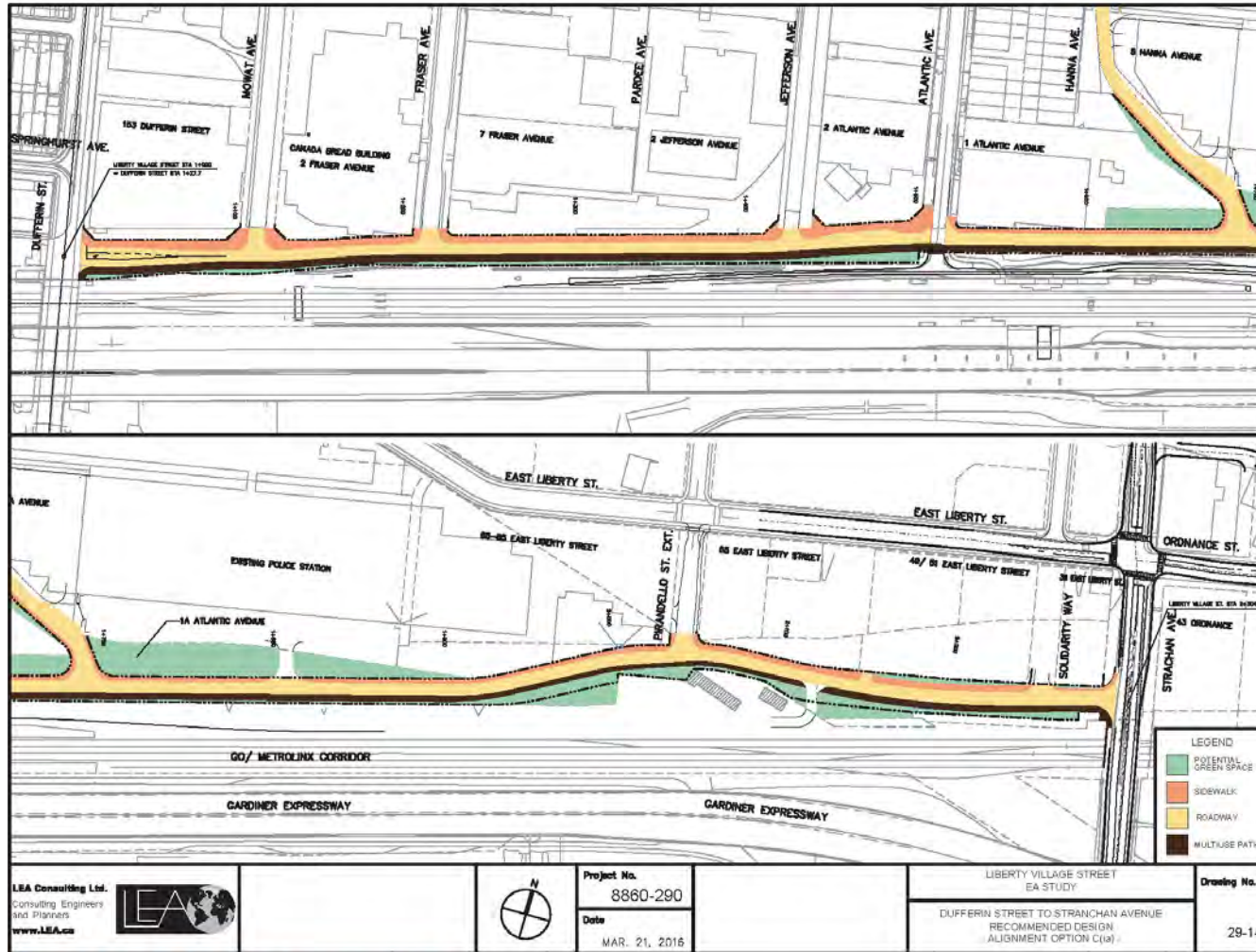


Figure 7-2 Option 2C – East-West Link with Multi-Use Path

Map: Liberty New Street, 2016 Environmental Assessment Plan

Appendix B: Tree Planting Demonstration Plans

The following six case studies illustrate public realm improvements if existing parking is contemplated to be removed and replaced with tree planting areas. Each case study is subject to further review of underground utilities to ensure there are no conflicts. While the plans were intended to inform potential capital works opportunities, the information may be useful for public realm improvement requirements for development applications.

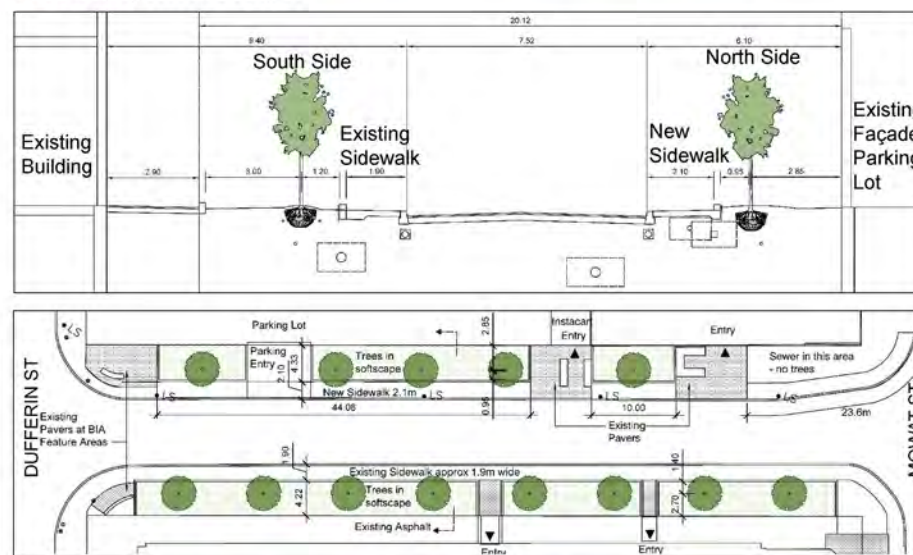
Funding options for the implementation of these works could be explored through various means including the capital budget as well as previous accumulated Section 37 funding and Community Benefits Charge funding.

1. Liberty St Dufferin St to Mowat St

If boulevard parking removal is contemplated:
Tree planting at north and south sides, from Dufferin St to Mowat St, appears possible from preliminary utility review.



Photo looking east on Liberty St



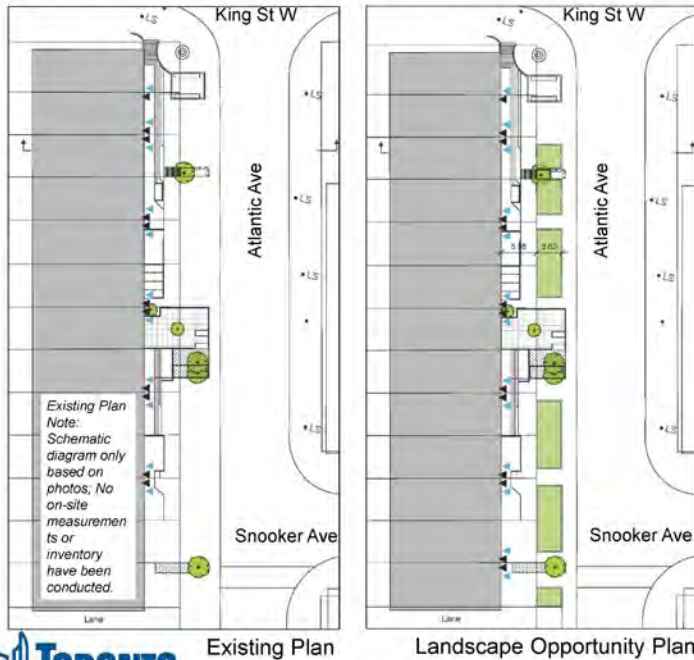
Note: To confirm tree planting potential, on-site below-grade utility investigation is required.

2. Atlantic Ave King St to Lane

If boulevard parking removal is contemplated:
 Landscaping opportunities are at west side, from King St to Lane. Trees appear possible but would need to design for a potential cycle track.



Photo of west side of Atlantic Ave, south of King St



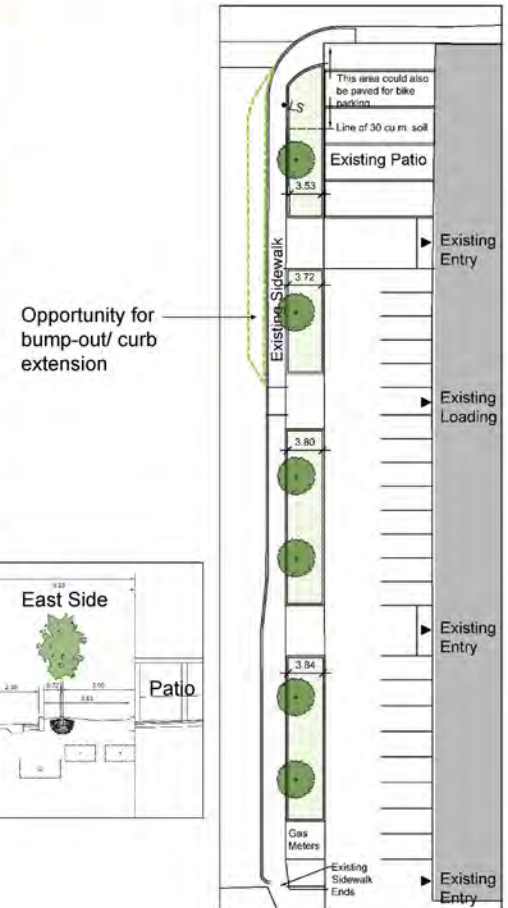
Note: To confirm tree planting potential, on-site below-grade utility investigation is required.

3. Hanna Ave South of Liberty St

If boulevard parking removal is contemplated:
 Tree planting at east side and bump-out option appear possible from preliminary utility review.

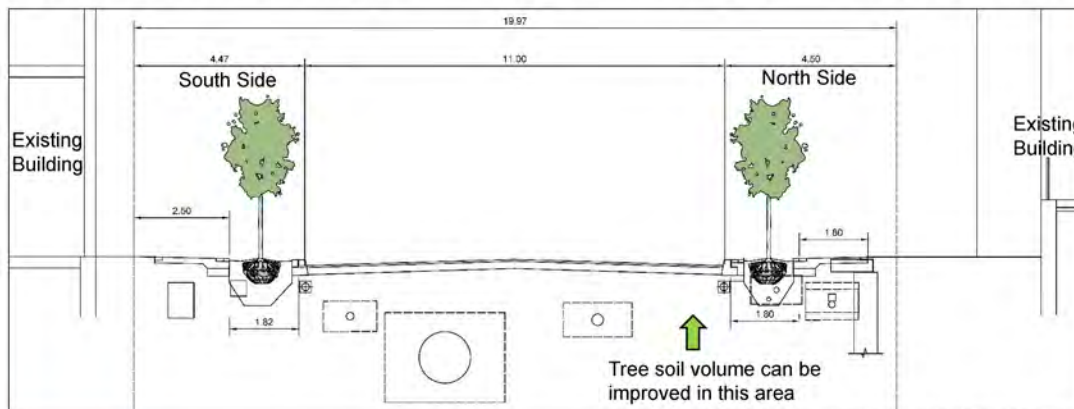


Photos of southeast corner of Hanna Ave and Liberty St.



6. East Liberty St – Greening Opportunities

- If on-street parking is removed, bump-outs might be considered. The ideal locations for these bump-outs to improve tree planting appears to be on the north side between Pirandello Street to Lynn Williams Street.
- Many areas on East Liberty Street have utility conflicts that makes existing tree planting improvements difficult.



Section through East Liberty Street at 150 East Liberty



Photo on East Liberty St, west of Pirandello St, looking west

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