
5055 DUNDAS STREET WEST

Development in the former Michael Power Land will be consistent with the following urban design guidelines. The guidelines are to be read in conjunction with the applicable site specific zoning by law, and the urban design policies in the Official Plan.

LOCATION

5055 Dundas Street West is located on the south side of Dundas Street West between Maybelle Avenue and Dunbloor Avenue. It spans to the north side of Bloor Street, immediately west of the rail underpass.

GUIDELINES

1. PUBLIC REALM

Streetscape

Appropriate streetscape treatment will be provided on existing and new streets to the satisfaction of the City of Toronto along all public frontages to promote the safest use of the public street.

The design and grading of the new street will be co-ordinated with the adjacent property owners.

The streetscape along Dundas Street will be graded to ensure level access from the street.

Bloor Street

The Bloor Street frontage will be landscaped to form an accessible and safe street edge. Detailed landscape design will be reviewed to ensure appropriate landscape treatment of the existing retaining wall and slope.

Streetscape improvements along Bloor Street under the rail overpass will be incorporated to improve pedestrian comfort and safety.

Safety

Natural surveillance opportunities are encouraged by strategically orienting building entrances and other active uses near vulnerable areas.

Environmental Impact: Sun and Wind

A wind study for the entire site will be submitted to the City. Should wind mitigation measures be deemed necessary, the appropriate design changes will be incorporated prior to submission for Site Plan Review. The applicant will obtain certification from wind consultants indicating that the final building design has taken sufficient measure, particularly along the Bloor Street frontage, within the park area, and at building corners. The incorporation of design features such as setbacks and stepped building facades, mitred corners, balconies and canopies, stepped elevations and podiums will help intercept or diffuse winds at the pedestrian level.

Shadow impact on public areas will be minimized.

Public Walkway (Mabelle westward)

This walkway serves to connect residential communities to the existing school and proposed park, as well as providing access to adjacent streets and the subway. It must be clear and accessible 24 hours a day.

The public walkway will be at least 5 metres wide; barrier free, easily understood and visible from surrounding areas wherever possible. Ensure that the design of the walkway clearly delineates public, semi-public and private property and maximizes natural surveillance opportunities.

Trees are to be planted on either side at 6.0 metres spacing. The lighting and pavement will be to City standards.

The view termination of at the westerly limit of the walkway will receive special treatment to encourage sufficient views and connections.

Public Stairs (from Bloor Street to 5055 Dundas Street Lands)

The design of this pedestrian connection will be done in conjunction with City of Toronto Urban Development Services staff. It must be direct, clear and safe, and contain public streetscape elements such as sidewalks, lighting and tree planting in addition to park-like elements such as benches, a focal point and stairs or ramps. A barrier free design solution is preferable.

Minimum width of the stairs and/or ramp will be 5.0 metres, and adjacent landscaping will be low maintenance.

Public Park on Dundas Street:

The design and programming of the park will be co-ordinated with City of Toronto Economic Development Culture and Tourism staff.

The park will be graded to be level at the Dundas Street frontage.

Buildings A, B and C are encouraged to have direct access into the park.

2. BUILT FORM

Buildings A, B and C

Buildings B and C will be designed to frame the south edge of the park and Building A will be designed to appropriately frame the east edge of the park.

Each building will be designed with its own architectural character and distinct details such as articulation, fenestration and materials and roofline treatment. A consistent, lower building base is encouraged in the form of townhouse expression, or a cornice or material change at the 3 or 4 storey height.

The facades of buildings will be articulated and fenestrated in a fashion that breaks down large-scale building mass and avoids large exposed expanses of blank walls.

Private landscaped courtyards will be designed to provide active as well as passive outdoor areas. Play equipment, seating areas, allotment gardens, BBQ areas, tennis courts, are examples.

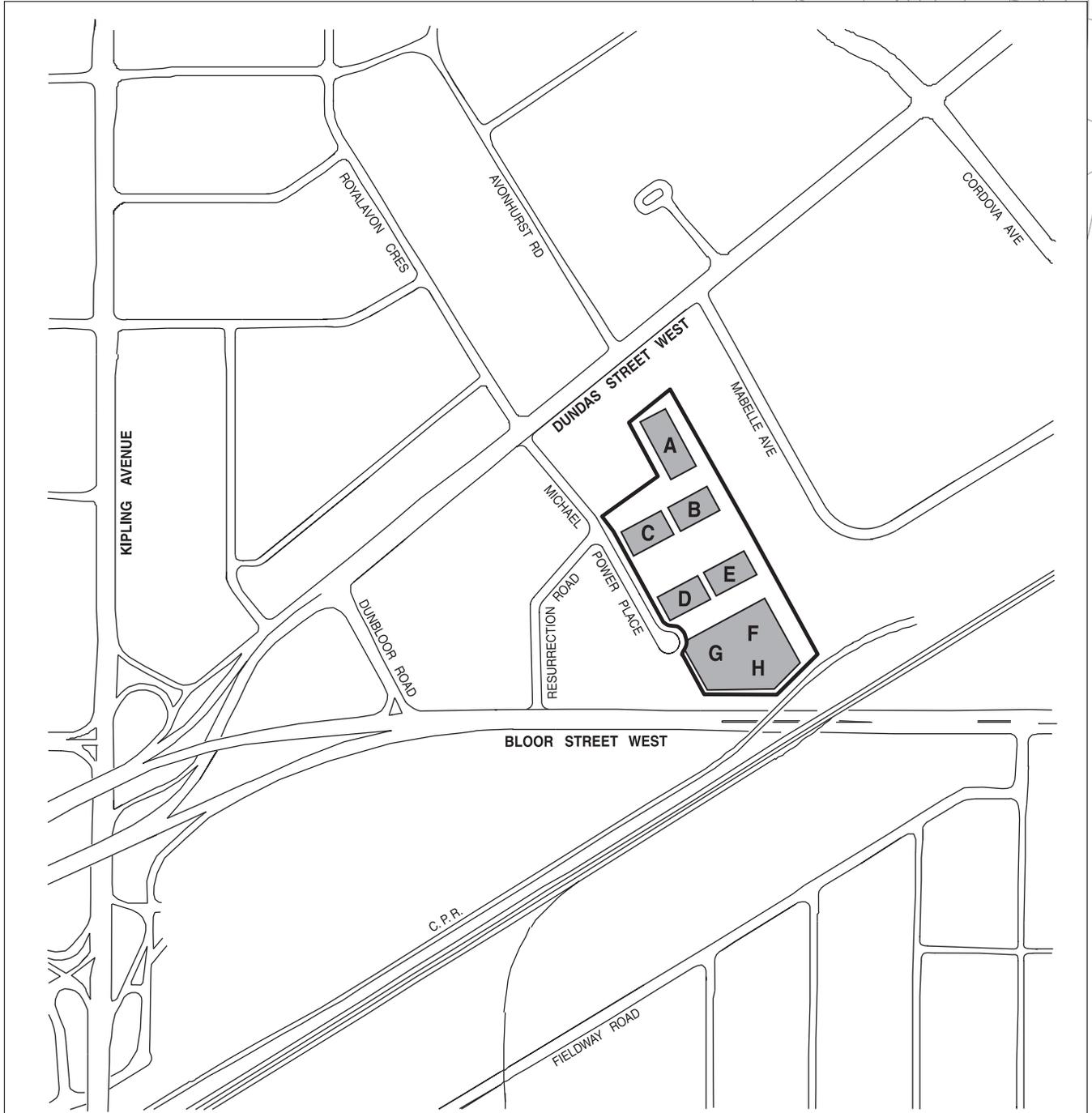
Underground parking garages will be organized to allow ventilation to occur vertically on the building face above pedestrian levels rather than within publicly accessible areas at grade.

Buildings F, G and H

At the time of Site Plan Submission for the first of buildings F, G or H, a comprehensive concept plan will be submitted illustrating how the remaining density permitted by the By-law is proposed to be deployed within the permitted height.

The taller buildings at the south portion of the site (F, G and H) will take the form of point towers.

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5055 Dundas Street West Building Blocks

MAP A



Not to Scale
04-03

Buildings G and H will be oriented to address the alignment of Bloor Street. Buildings G and H may incorporate grade related habitable space fronting onto Bloor Street where grade permits, in order to animate the street frontage.

Direct pedestrian access to the main floor level of the Bloor Street buildings is desirable either directly from the Bloor Street frontage, or via the public stairs. This is to promote a Bloor Street entrance and presence for the building.

A pedestrian entrance into the underground parking level from the Bloor Street frontage will be designed and treated architecturally as an entrance to the development. Lighting and weather protections will form part of the entrance.

Buildings will be setback from adjacent buildings and open spaces an appropriate distance to ensure good light, view and privacy between the buildings and open spaces.

Underground access ramps will be incorporated into the ground floor of buildings where possible. Where servicing and access ramps are within the block interiors, they will be incorporated into the landscape design, and not form the focus for private landscaped areas.

3. SITE PLAN ORGANIZATION

Site Plan

These guidelines encourage street and pedestrian related development with buildings generally sited parallel to the public streets and along the edges of open spaces and parks. Grade related dwellings, lobbies, and other appropriate uses are encouraged on these faces to provide safe, animated streets and open spaces. Open spaces in the middle of the blocks is for private use and for servicing the developments.

Setbacks

Setback areas between new buildings and adjacent public areas are to be landscaped to provide an appropriate setting for new buildings, to integrate with existing buildings, and provide amenity for residents and visitors. The required 18 metres setback along the easterly property line will serve as a landscaped area.

Adequate setbacks of buildings above and below grade will be provided where possible to ensure that existing mature trees can be integrated into new development. In cases where underground parking is below landscaped areas, sufficient soil depth is required to ensure mature growth of trees.

Address

To encourage safe and convenient access, buildings located on the east side of the new public road will have pedestrian entrances facing and immediately adjacent to the public street. Lobbies and security areas will be visible and accessible from the public sidewalk. Buildings located off of the new public road will have main entrances directly accessible from the public sidewalk.

Multiple entrances along a street promote its use. Grade related units are encouraged at the base of residential buildings. Access to retail uses from interior spaces is discouraged.

Entrances to buildings will include weather protection.

Parking and Servicing

Parking, automobile drop off and servicing access will be carefully planned and designed so as to minimize interference with the continuity of public sidewalk and the regularity of street tree planting, and will have a minimal physical and visual impact on the public streets and accessible open spaces.