



## Broadview Avenue

Danforth Avenue to O'Connor Drive

**City of Toronto**

Broadview Planning Study - Urban Design Guidelines - Revised June 2016

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The City of Toronto City Planning Division would like to thank all those who participated in the community consultation process. This document has been enhanced by your knowledgeable and helpful feedback.

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# Mandate

On November 13, 2013, Toronto City Council requested that the City Planning Division initiate a study of Broadview Avenue between Danforth Avenue and O'Connor Drive in consultation with the Ward Councillor and the local community. The mandate calls for a study that will develop design guidelines to address "how new development will complement the existing built form, identify opportunities for public realm improvements, and plan for the transportation impacts of new development."

As part of this study, City Planning has developed Urban Design Guidelines for this portion of Broadview Avenue. The guidelines are intended to support vibrant, high quality, appropriately scaled, single or mixed-use development in the Study Area. The guidelines are consistent with the Official Plan and the Site and Area Specific Policy (SASP) No. 509 for the Study Area. The SASP indicates that the guidelines will be used as a tool to evaluate proposed development in the area and ensure that such development is consistent with the Official Plan. The guidelines will implement the Official Plan policies, provide an understanding of the area's local character, explain how development will be sensitive to the local character, promote an animated public realm, provide built form guidelines, identify buildings of heritage interest, and provide options for improving transportation conditions.

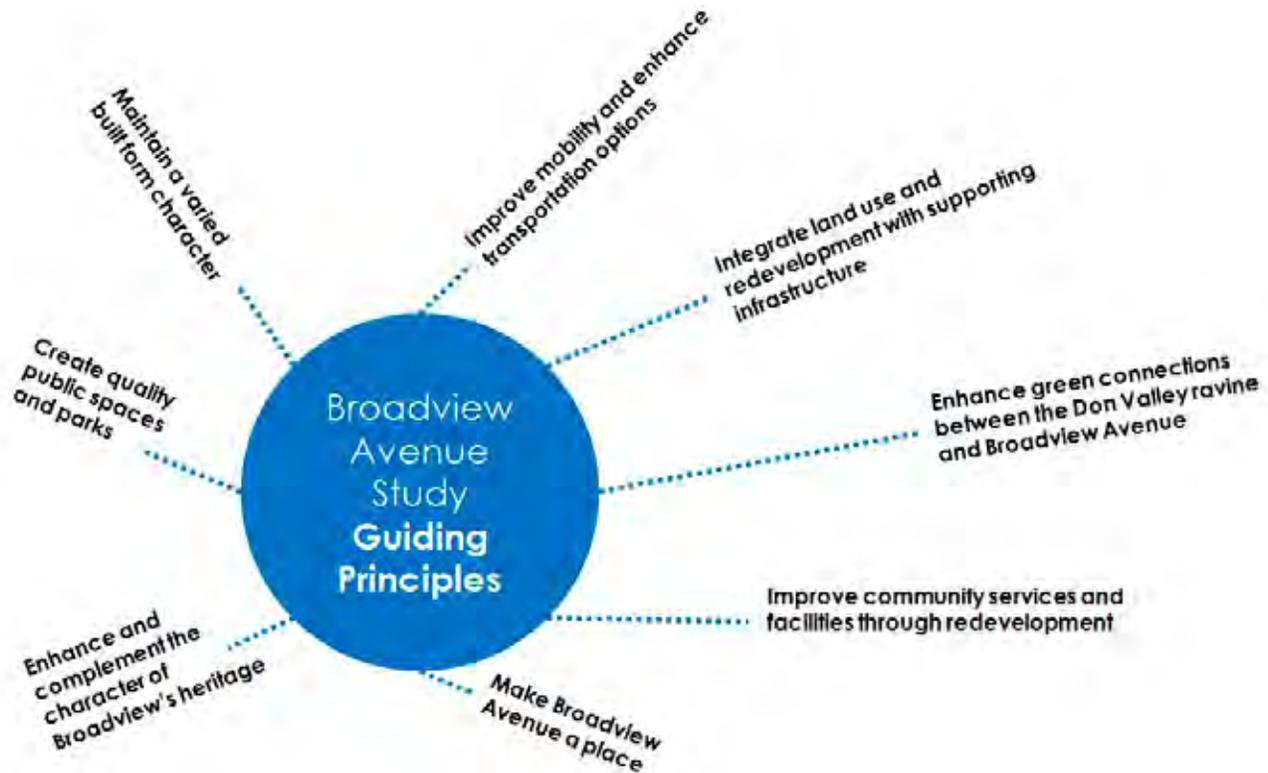
The Official Plan provides a long-term vision for the City that is intended to be supported by implementation plans, strategies and guidelines. These supporting documents, including the subject guidelines, provide detailed implementation guidance that is consistent with the Official Plan and responds to changing circumstances and priorities.



# Executive Summary

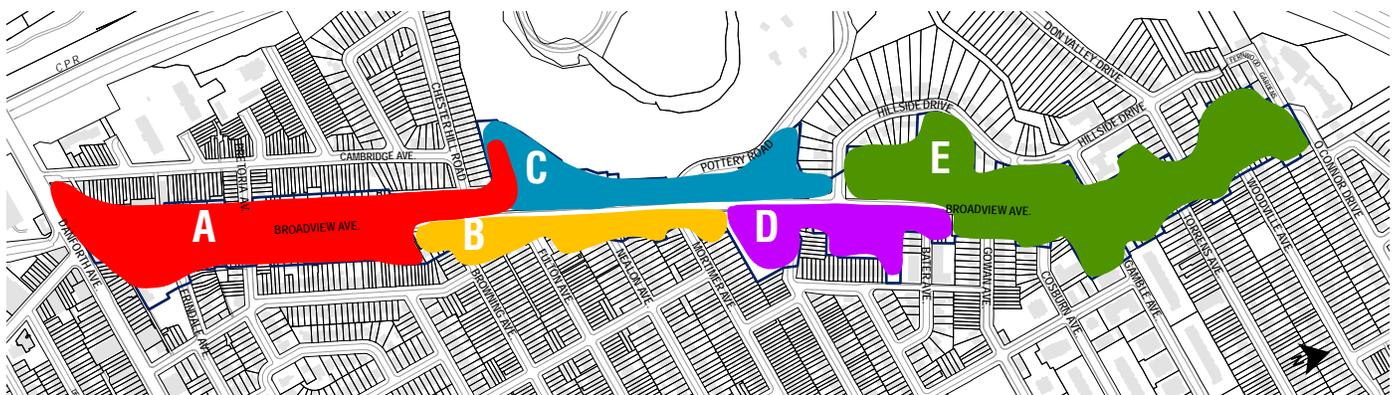
## Guiding Principles

The Study Team, in consultation with the community, developed the guiding principles shown below. The guiding principles are reflected in the vision and planning framework for the Study Area.



## Options

Informed in part by the community consultations, the Study Team identified five character areas for which a number of built form options, public realm improvements, parks and open spaces, green connections, and transportation options were developed and assessed. These elements then formed the basis for a general planning framework.



Character Areas Map

# Guidelines

The guidelines and standards reflect the five character areas as well as the area's natural and cultural heritage.

## Valley Gateway

Green connections between Broadview Avenue and the Don Valley in the form of the planting of new native tree species as well as maintaining public access to views towards the Valley is encouraged.

## Heritage

New development will retain, enhance and commemorate the heritage attributes of buildings and the history of the area. Commemorative signage is recommended at appropriate locations.

## Parks and Open Spaces

New parks will be included as part of any future redevelopment on the properties at 1099 and 1015 Broadview Avenue. A network of green connections and new parks are encouraged, as well as, improvements to front yard landscape areas, public lanes and existing parks.

## Land Use

Residential and mixed-use development is encouraged in the Study Area as a place where people can live, work and play. Street-related retail at grade is desirable in appropriate locations to support the character of the Study Area.

## Built Form

Mid-rise built form is appropriate for all parts of the Study Area, with the exception of the areas designated as *Neighbourhoods*. Other low scale buildings such as townhouses may be appropriate due to lot size and configuration.

Tall buildings are not appropriate in the Study Area.

## Height

Height restrictions are informed by local context and the policy directions for the Avenues.

Mid-rise buildings will be limited to a height up to 20 metres. That height may be further limited by angular planes (reflecting lot depth). The study recognizes that on large sites, buildings taller than the existing right-of-way may be appropriate provided certain conditions are addressed.

## Angular Planes and Transition

Angular planes will be applied to all sites within the Study Area except *Neighbourhoods*, in keeping with the standards developed in the Avenues and Mid-rise Buildings Study. These will support appropriate transition between existing *Neighbourhoods* and any new developments, as well as forming appropriate street wall heights and relationships along Broadview Avenue.

## Setbacks

A system of setbacks to support wider sidewalks, retail amenity spaces, and/or green landscaping in front of ground-level residential uses are encouraged.

## Stepbacks

Stepbacks will be required for new mid-rise buildings. This is intended to provide a consistent scale to the streetscape and allow for light penetration and sky views.

## Density

Appropriate density will be determined on a case by case basis when development applications are made. Such applications should be consistent with the intensification policies for Avenues and the Site and Area Specific Policy for the Study Area.

## Guidelines (cont'd)

### Streetscape

The Study Team developed recommended streetscape improvements for Broadview Avenue without widening the right-of-way. Appropriate front building setbacks from the street will support an improved pedestrian realm that features broader sidewalks, street tree planting, landscaped areas and bicycle rings where possible.

### Transportation

The Study identified a number of approaches for improving vehicle, transit, pedestrian and bicycle linkages. Informed by existing conditions and the guiding principles, three Transportation Alternative Planning Solutions, differing in their components, were developed. Ultimately, a solution that continues to utilize the existing right-of-way may be identified pending further study. The Transportation Planning Solutions will be achieved through development and capital improvement programs.

### Community Services and Facilities Strategy

The Study Team reviewed the types and number community agencies and City divisions serving the area. A follow-up assessment on priorities, facilities, capacity and gaps in service delivery is recommended as development proceeds. A strategy for ensuring that Community Recreation Centres, Libraries, Child Care, Schools, Human Services, and Community Agencies to serve the growing population is established. Ongoing monitoring of CS&F needs and priorities is also recommended.

### Storm, Sanitary & Water Infrastructure

In order to enhance the performance of the City's storm drainage infrastructure, the City's Wet Weather Flow Management Guidelines (WWFMG) will continue to be implemented for all developments. The City in 2015 undertook upgrades to the storm infrastructure in the Study Area. The resurfacing of Broadview Avenue is expected to begin in 2016, as a continuation of similar work on O'Connor Drive. In terms of sanitary sewer and water infrastructure, new development will be evaluated with existing guidelines and upgrades will be undertaken where it is required.



# 1. Introduction

## 1.1 STUDY AREA

The Study Area includes Broadview Avenue and all properties fronting onto it, between Danforth Avenue and O'Connor Drive with the Don Valley acting as a natural barrier to the west. This portion of Broadview Avenue has a length of approximately 1.5 km and gently slopes downwards from north to south. The existing width of the Broadview right-of-way is uneven, but it is predominantly 20 metres. The Study Area is immediately

surrounded by low density neighbourhoods, with the Don Valley located to the west. Danforth Avenue and Pape Avenue main streets are in close proximity. *Apartment Neighbourhoods* are located along Cosburn Avenue northeast of the Study Area. There are also *Apartment Neighbourhoods* further west along the valley. TTC bus routes as well as the Broadview Subway Station are located in the Study Area.



Map of the Study Area



Aerial photograph of the Study Area (2013)

## 1.2 HISTORY OF THE AREA

The history of Broadview Avenue can be traced back to the 1800s and earlier, when aboriginal trails were located in the area. Broadview Avenue was commissioned in 1799. It was originally called the Mill Road and subsequently Don Mills Road because of the mills and brickwork industries located in the Don Valley. During the expansion of Toronto in the late 1800s the lands around Danforth Avenue were subdivided and a new streetcar line was opened on Broadview Avenue to Downtown Toronto.

The station for the streetcar was located in the general location of the present subway station at Broadview and Danforth Avenues. The construction of the Prince Edward Viaduct in 1918 became the next catalyst for growth in the area because it connected the west part of the City to the east.

Further growth occurred after the Second World War resulting in accelerated residential developments. Several historic buildings were demolished to make way for a number of low-scale and high rise apartment buildings presently located in the Study Area. At the same time, the impacts of such development on the Don Valley became a major concern resulting in the formation of a number conservation groups to prevent further degradation of the valley.



Looking towards Danforth and Broadview, pre-Bloor Viaduct (ca. early 1910s).  
Source: City of Toronto Archives

## 1.3 PURPOSE

Today, Broadview Avenue north of the Danforth, requires clarity on the form and scale of intensification. Recent development pressure may signal incremental growth and reurbanization, presenting an opportunity to define the Study Area's future. Broadview Avenue should become a place in its own right and not simply a thoroughfare to Downtown Toronto.

This Study was initiated by the City to address how new development will complement the existing built form, identify opportunities for public realm improvements, and plan for the transportation impacts of new developments, resulting in a planning framework and recommendations to guide future development and public realm planning.



Recent development on Broadview Avenue



View towards north of Broadview from Danforth, 2014

# 1.4 CHARACTER STATEMENT

Historically, Broadview Avenue was primarily known as the access to the mills and other industries located in the Todmorden Mills. Today, the physical and visual connections to these historical sites and the Don Valley Ravine are still defining character elements of Broadview.

Broadview's proximity to the Danforth and the TTC Broadview subway station has shaped the southern segment of Broadview as a mixed-use area with retail at grade and residential uses above. As it extends further north from the Danforth, the character is predominantly residential, with deep landscaped front yards and a more open feel to the street. There are a variety of building typologies along this stretch of Broadview, contributing to its unique character. This very diverse range of built form includes

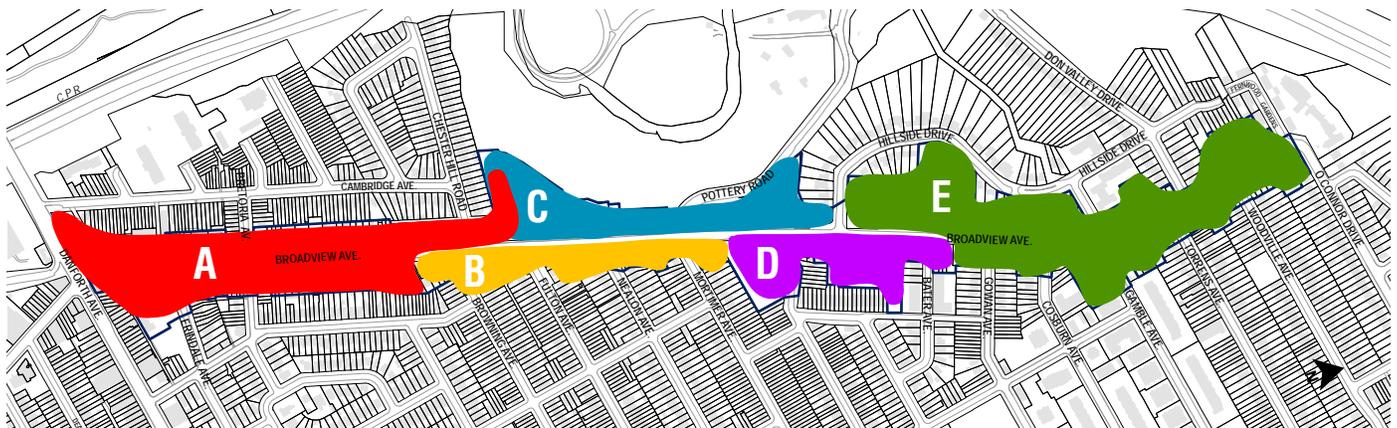
semi and single-detached buildings, townhouses, low-rise non-residential buildings, low-scale apartment buildings and high-rise apartment buildings.

The age, materials and details of existing buildings also contribute to the area's character. Two historic villages, Todmorden and Chester, developed along the road starting in the mid-19th century. As a result, existing buildings exhibit patterns related to this character, such as deeper setbacks from the street, recessed entrances, and spaces between buildings for access to the rear.

Due to this diversity, five character areas were identified within the Study Area. Sections 3.1 - Existing Character and Section 4.3 - Built Form elaborate more about each character area.



Broadview Avenue Community Vision Diagram



Character Areas Map

## 1.5 INGREDIENTS OF THE VISION FOR THE STUDY AREA

As part of the Study, members of the community were asked to identify ingredients that describe their vision of the Study Area in the future.

The community offered the following key words as the basis of the vision of the Study Area:



## 1.6 VISION

The Study's overall vision and objectives are based on the Official Plan policies as well as community input. The vision

contemplates a liveable area that accommodates future growth with transportation and linkages to green space networks.

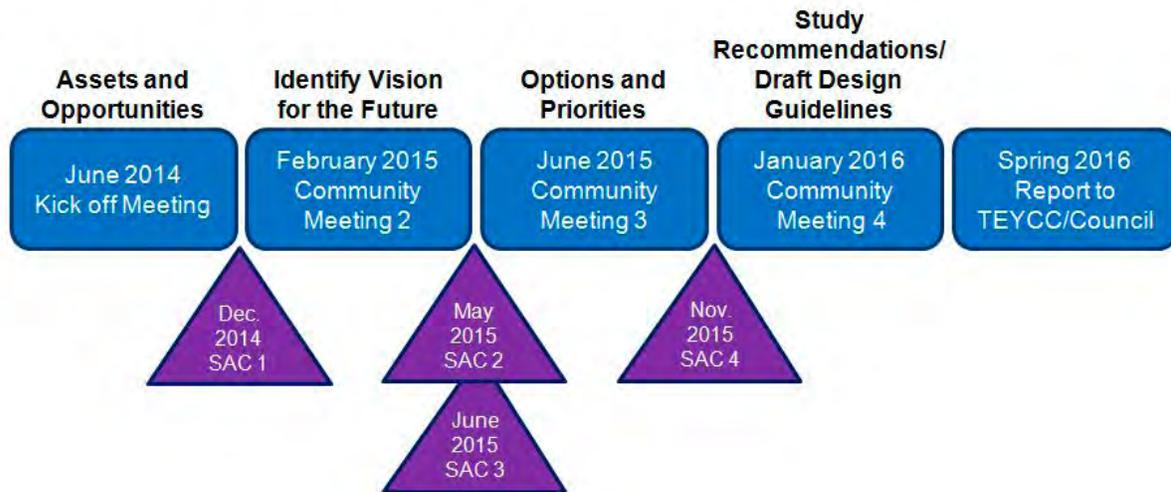
### Vision Statement

Broadview Avenue is a special and unique place; its history and character are intertwined with the Don River Valley, including its indigenous history and Toronto's early industrial era. The street provides linkages and opportunities to view and experience the natural heritage of the Don Valley and the Don River. It features a green, connected network of public spaces and a people-friendly public realm. Broadview Avenue is the focus of adjacent communities and neighbourhoods. The street welcomes and sustains new businesses and offers a setting for incremental mid-rise development that complements the street's character and varied built form, and recognizes the area's heritage. As a transportation corridor, Broadview serves a variety of users and modes of transportation.

## 1.7 STUDY PROCESS

The Study was conducted in four phases:

- The first phase was to introduce the Study to the community, understand the local context and issues, and identify assets and opportunities;
- In the second phase, a vision for the area was developed and a range of alternative options were explored;
- The third phase was to determine the options and priorities, and test and confirm these options; and
- The fourth and final phase was to develop a planning framework.



## 1.8 COMMUNITY AND STAKEHOLDER CONSULTATION

Stakeholders and the community were involved in all phases of the study process, which included:

- Four Stakeholder Advisory Committee (SAC) meetings
- Four Community Consultation Meetings (CCM)

The Study Team met with the SAC and the public to present the purpose and scope of the Study, discuss the key findings, and solicit feedback from participants. A full account of all public engagement events and the input received is available at [www.toronto.ca/broadviewstudy](http://www.toronto.ca/broadviewstudy).



Open House before the Community Consultation Meeting



Public exploring the ingredients of a Vision for Broadview Avenue



## 2. Planning Context

### 2.1 PROVINCIAL POLICIES

#### 2.1.1 THE PLANNING ACT, R.S.O. 1990

The *Planning Act* gives the City of Toronto the responsibility to ensure that provincial interests are met. Studies, such as the Broadview Avenue Study and Official Plan Policies resulting from this Study, are two of the tools the City uses to ensure that matters of Provincial interest are adequately addressed.

#### 2.1.2 THE PROVINCIAL POLICY STATEMENT (PPS)

Section 3 of the Planning Act requires that decisions affecting planning matters “shall be consistent with” the Provincial Policy Statement (PPS). The PPS, 2014, provides policy direction on matters of provincial interest related to land use planning and development. These policies support the goal of enhancing the quality of life for all Ontarians.

Key objectives include: building strong healthy communities by accommodating a range and mix of uses; wise use and management of resources; and protecting public health and safety. The PPS recognizes that local context and character is important. Policies are outcome-oriented, and some policies provide flexibility in their implementation provided that provincial interests are upheld.

#### 2.1.3 THE GROWTH PLAN FOR THE GREATER GOLDEN HORSESHOE (GGH)

The Growth Plan for the GGH is a framework for implementing Ontario’s vision for building strong, prosperous communities by managing growth to the year 2031. Principles expressed in the GGH include directions for where and how to grow; the provision of infrastructure to support growth; and protecting natural systems and cultivating a culture of conservation.

### 2.2 CITY OF TORONTO POLICIES

#### 2.2.1 OFFICIAL PLAN

The City of Toronto’s Official Plan provides a vision for a successful city by directing growth into strategic areas, setting infrastructure priorities, and identifying implementation strategies. The vision of the Plan is about creating an attractive and safe City that evokes pride, passion and a sense of belonging - a City where people of all ages and abilities can enjoy a good quality of life.

#### 2.2.2 URBAN STRUCTURE

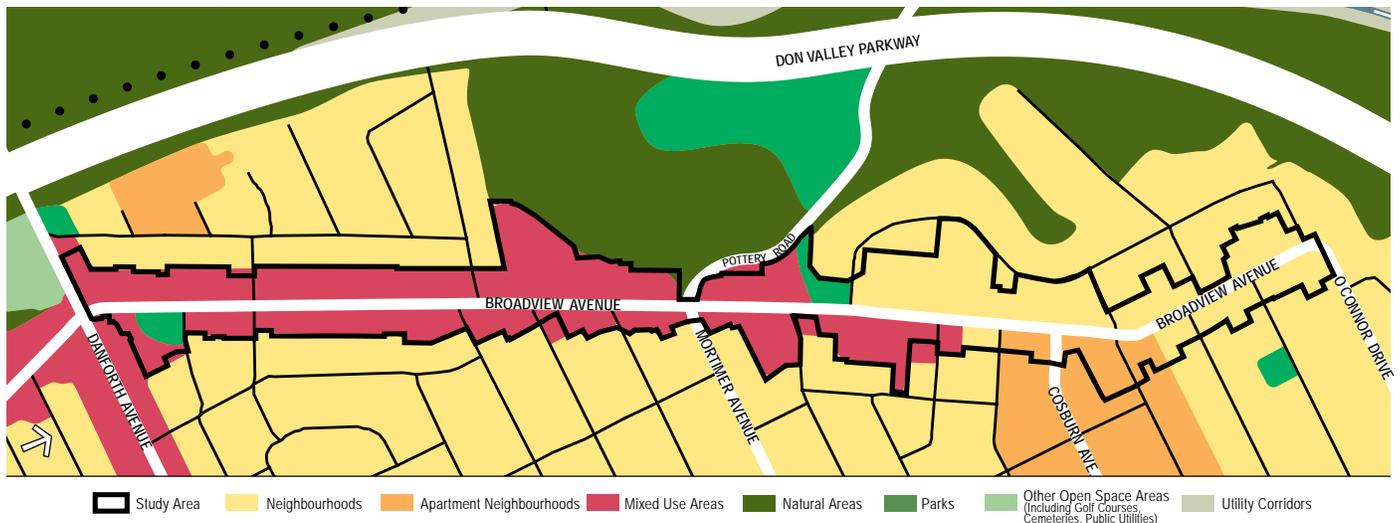
The Study Area is located within an Avenue on the Urban Structure Map (Map 2) and is designated as a Mixed Use Area (Map 18) in the City of Toronto Official Plan.

Avenues are “important corridors along major streets where reurbanization is anticipated and encouraged to create new housing and job opportunities, while improving the pedestrian environment, the look of the street, shopping opportunities and transit service for community residents”, according to Section 2.2.3 of the Plan.

The Avenues will be reurbanized incrementally on a site-by-site basis and over the course of several years. The framework for new development on each Avenue is generally established through an Avenue Study with the intent of encouraging reinvestment and intensification on underdeveloped Avenues. For varying reasons, certain Avenues are prioritized for Avenue Studies.

The Official Plan notes that not all Avenues are the same. “Each Avenue is different in terms of lot sizes and configurations, street width, existing uses, neighbouring uses, transit service and streetscape potential. Some of the Avenues function as traditional “main streets” that serve the local and broader communities with a variety of nonresidential uses. There is no ‘one size fits all’

## Official Plan Map



program for reurbanizing the Avenues". In light of the foregoing, a planning analysis of this portion of Broadview Avenue was considered appropriate in order to develop urban design guidelines that reflected the local context and complemented the Official Plan policies.

Section 2.2.3 Avenues, Policy 3.a) states that development may be permitted on the Avenues prior to an Avenue Study, provided an applicant has submitted an Avenue Segment Study as part of the application. Proposed development that is accompanied by an Avenue Segment Study will be considered on the basis of all of the policies of the Official Plan. Avenue Segment Studies are required to discuss the implications for the portion of the Avenue resulting from the proposed development and whether the proposed development would be setting a positive precedent for future development of the remainder of the Avenue.

### 2.2.3 OFFICIAL PLAN LAND USE DESIGNATIONS

The lands fronting on Broadview between Danforth Avenue and Hillside Drive are designated *Mixed Use Areas* on Map 18 in the City's Official Plan. The lands on the east side of Broadview, between Danforth Avenue and Bater Avenue are also designated *Mixed Use Areas*. The lands generally north of Hillside Drive and Bater Avenue to O'Connor Drive are designated *Neighbourhoods*.

Within *Mixed Use Areas* a broad range of uses are encouraged, providing opportunities to live, work and shop within the same area. *Mixed Use Areas* are anticipated to accommodate growth, with new service, employment and residential uses all locating within these diverse areas in a manner that is appropriate to their surrounding contexts, with adequate transportation and infrastructure servicing.

*Neighbourhoods* contain low scale residential buildings, schools, parks and a limited number of shops and services which serve the local community. Development within *Neighbourhoods* is intended to preserve and enhance the existing built form and land use character. The approach to managing change in Toronto's *Neighbourhoods* and green space system emphasises maintenance and enhancement of assets.

The Official Plan policies recognize that Toronto's system of parks and open spaces will continue to be a necessary element of city-building as the City grows and changes. Maintaining, enhancing and improving the system requires adding new parks and amenities, designing high quality parks, protecting access to publicly accessible open spaces, promoting and using private open spaces and recreations facilities as well as parkland acquisition strategies.

## 2.3 ZONING BY-LAW

The lands in the Study Area fall within two different zoning by-laws that are in force for this segment: the former Borough of East York Zoning By-law 6752 and the former City of Toronto Zoning By-law 438-86, as amended.

The former City of Toronto Zoning By-law 438-86 applies to all the properties fronting on the west side of Broadview Avenue between Danforth Avenue and 956 Broadview Avenue. The same By-law applies to all the properties fronting on the east side of Broadview Avenue from Danforth Avenue to 947 Broadview. The lands noted above are zoned as Commercial Residential.

The East York Zoning By-law 6752 applies generally to all the properties from 958 Broadview (Estonian House) on the west side of Broadview and 957 Broadview, on the east side of Broadview Avenue to O'Connor Drive. The properties on the west side of Broadview Avenue are zoned Residential ranging from low density to high density residential. 958 Broadview Avenue is

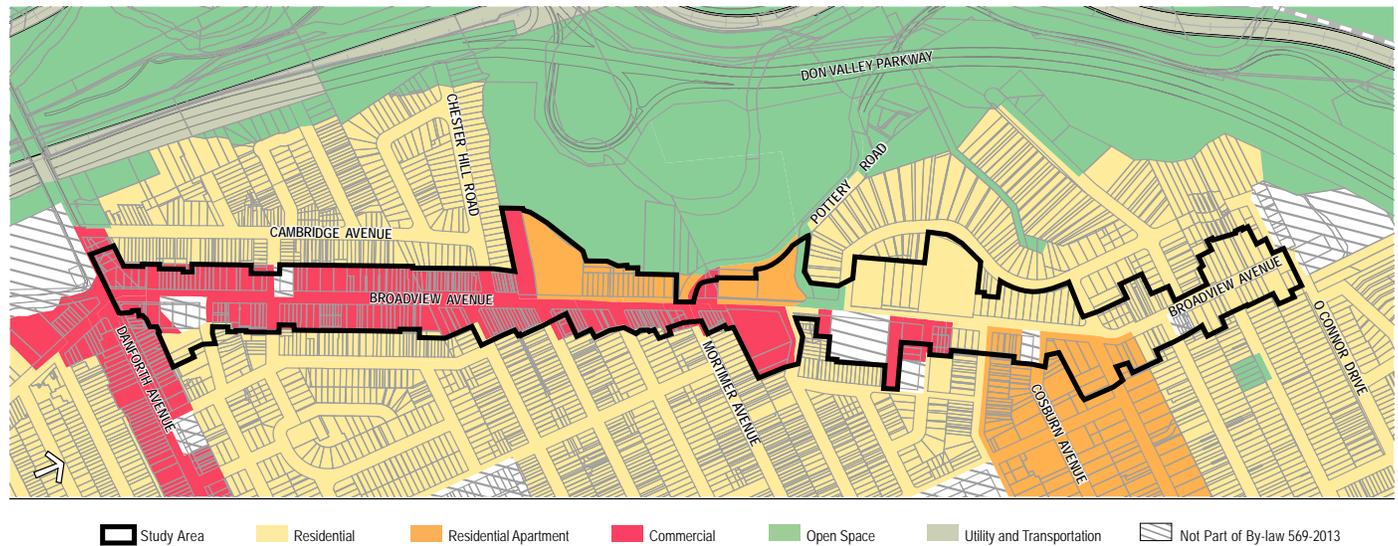
zoned Commercial. The properties on the east side of Broadview Avenue in this area are zoned Commercial or Residential.

The properties regulated by the former City of Toronto Zoning By-law have height limits of 14 metres, except the properties on the Danforth Avenue that have height limits of 16 metres. The properties regulated by the Borough of East York Zoning By-law have a height limit ranging from 8.5 metres to 10.5 metres.

There are properties located in the Study Area with site specific Zoning By-laws allowing permission greater than what the base By-law permits.

The new harmonized City of Toronto By-law 569-2013 (under appeal), applies to all the lands in the Study Area and reflects the existing zoning categories and standards under the former City of Toronto Zoning By-law 438-86 and the East York Zoning By-law 6752.

### Consolidated Zoning Map



## 2.4 ADDITIONAL RELEVANT CITY OF TORONTO DOCUMENTS

### 2.4.1 AVENUES AND MID-RISE BUILDINGS STUDY

Toronto City Council at its meeting of July 8, 2010 adopted the recommendations contained in the staff report prepared by City Planning entitled Avenues and Mid-Rise Buildings Study and Action Plan, with modifications. The main objective of this Study was to encourage future intensification along Toronto's "Avenues" that is compatible with the adjacent neighbourhoods through appropriately scaled and designed mid-rise buildings.

The Avenues and Mid-rise Buildings Study identifies a list of best practices, categorizes Avenues based on historic, cultural and built form characteristics, establishes a set of performance standards for new mid-rise buildings, and identifies areas where the performance standards should be applied.

The Performance Standards are intended to be used as tools to implement both the Official Plan's Avenues and *Neighbourhoods* policies, maintaining a balance between reurbanization and stability. The Performance Standards provide guidance pertaining to the size, shape and quality of mid-rise buildings and are intended to implement Section 2.3.1 of the Official Plan.

### 2.4.2 TOWNHOUSE GUIDELINES

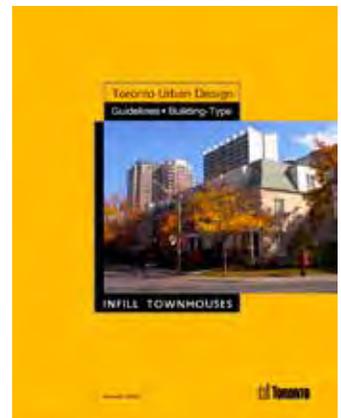
Townhouses are defined as low-rise, grade-related, attached residential units constructed in rows or blocks. They share similar design characteristics with single-family detached and semi-detached houses such as front doors facing the street, natural through-ventilation, and grade related outdoor space. Townhouse developments can provide infill housing at densities appropriate for existing stable neighbourhoods, and a relationship between new housing and public streets and open spaces. The Infill Townhouse Design Guidelines are applicable City-wide and will be considered in relation to any new townhouse development within the Study Area. The Infill Townhouse Design Guidelines are currently under reviewed and a report from City Planning staff is being prepared.

### 2.4.3 OTHER RELEVANT DOCUMENTS

Other guidelines that have been adopted by City Council to advance the vision, objectives and policies of the Official Plan may be used to evaluate development in the Study Area as appropriate.

At the current time, the relevant guidelines include:

- Bird-friendly Development Guidelines
- Sign By-law
- Streetscape Manual
- Toronto Green Standards



# 3. Existing Conditions

## 3.1 CHARACTER-DEFINING ELEMENTS

Situated on the east side of the Don River Valley, Broadview Avenue is a transportation spine, running diagonally from Danforth Avenue to O'Connor Drive. While the valley acts as a natural barrier to east-west movement, it remains a natural heritage asset for the Study Area and beyond. Pottery Road and the Bloor Viaduct are the only roads that provide east-west connections to Downtown Toronto.

The built form in the Study Area is eclectic with a number of buildings dating back to the 1800s and early 1900s; however, many buildings were built from the 1950s to the present time as well. The Study Area has a range of residential, commercial and institutional uses in single or mixed use buildings. The variety of buildings include single-detached and semi-detached residential buildings, duplexes, townhouses, 2-3 storey single and mixed-used apartment buildings, 4-5 storey retail/office buildings, and 15-22 storey apartment buildings. The tall apartment buildings are all located on the west side of Broadview Avenue with views into the Don Valley.

The built form is shaped partly by the variety of lot sizes, lot configurations, frontages and depths, which contributes to Broadview's unique character. Some of the lots between Mortimer Avenue and Browning Avenue are oriented diagonally to Broadview.

The planned right-of-way width of Broadview is identified as 20 metres in the Official Plan. The existing right-of-way width varies from 17 to 27 metres with no plans for future road widening. There are a number of east-west streets that do not form 4-way intersections with Broadview. Key intersections in the Study Area are located at Danforth and Broadview, Pretoria and Broadview,

Pottery Road/Mortimer Avenue and Broadview, and Chester Hill Drive/Browning Avenue and Broadview.

Broadview's public sidewalk widths generally range between 2 to 4 metres, but occasionally there are wider sidewalks in the range of 6 to 8 metres. The distance, or setback, between the main building face and the public sidewalk has a larger variation, in the range of 1 to 20 metres, being generally narrower towards the south of the Study Area and wider towards the north. The landscape treatment of this setback varies as well, ranging between paved surface, sometimes with planter boxes, and expansive green spaces. Buildings toward the south, which are mainly mixed-use buildings with retail at grade, are characterized by narrower setbacks and recessed entrances. On the other hand, buildings toward the north, near the residential high-rise apartment buildings, are defined by deep setbacks, driveways, and expansive front landscaping. It is important to note that most of the lots on Broadview do not have rear lanes, resulting in vehicular access from Broadview Avenue.

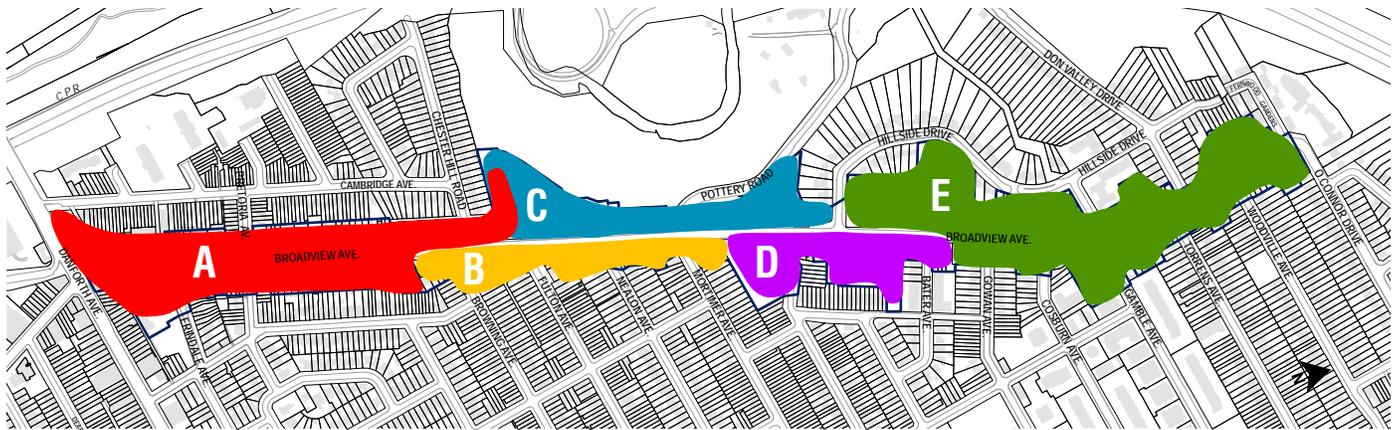
In terms of streetscape and street furniture, there are a number of bus stops, with some providing shelters and seating. Apart from the bus stops, few benches and bicycle rings are installed. The street lights are standard roadway specifications, and power lines are above grade on the east and west side of the street.

Commercial buildings within the Study Area also vary. There are a number of small, medium and large format retail stores which range in size from 100 to 500 square metres at the ground level. These exist in the form of corner stores, restaurants, offices, medical clinics, and other institutional uses. Of special mention is one large grocery store over 1,000 square metres.

The character of Broadview is also reflected in five identified character areas: A, B, C, D and E within the Study Area.

- Area “A” is close to the Danforth, has a main street feel with mixed uses and corresponds with the location of the 19th and early 20th century settlements of Chester and Playter Estates. Current zoning allows a maximum height of 14 to 16 metres.
- Area “B” is characterized by small to medium size lots with diagonal orientation and a mix of business offices and residential uses in house form buildings. Current zoning allows a mix of uses at a maximum height of 14 metres near Browning Avenue and 10.5 metres near Mortimer Avenue.

- Area “C” is characterized by large lots, expansive front yards and views into the Don Valley. Most of the high-rise apartment buildings within the Study Area are located here.
- Area “D” is characterized by a variety of lot sizes comprising small, medium and large lots and a diversity of built forms. Current zoning allows commercial and residential uses at a maximum height of 10.5 metres (3 storeys). It also corresponds with the location of the 19th century settlement of Todmorden.
- Area “E” is a stable neighbourhood area with predominantly small lots and houses. However, there are two large lots at the Massey Centre and the Salvation Army sites. Like Area “D”, this area was also part of the settlement of Todmorden.



Character Areas Map

## 3.2 HERITAGE

Broadview Avenue has been called different names during its long history, including Don Road, Mill Road, Mill Street and Don Mills Road. Broadview provided access to the saw and grist mills in the valley and other industries located at Todmorden Mills Historical Site, 67 Pottery Road. The Study Area includes two historic villages, Todmorden and Chester (sometimes called Doncaster), that developed along the road starting in the mid-19th century. As a result, the Study Area exhibits a relaxed village-like character. Buildings are often set back from the street with landscaped front yards. Laneways and private walkways from the street to the rear of properties create spaces between buildings and views to the residential areas behind.

The pattern of development in the Study Area corresponds to the division of historic farm lots that were surveyed in the 1790s. The Study Area spans across Lots 11, 12, 13, and 14, Concession 2 from the Bay (See 1878 map on the opposite page). These 200-acre farm lots extended from modern Danforth Avenue to O'Connor Drive, more or less east from the Don River to today's Donlands Avenue.

The village of Todmorden was established by the 1850s in Lot 13 (Character Area D, portions of Character Area C and E) along what is now Broadview Avenue. Its main intersection was at what is now called Pottery Road, which led to the older industrial complex in the river valley. Many of the historic buildings on the main road were demolished in the 1950s and 1960s and replaced with commercial, mixed-use or apartment buildings with larger building footprints.

The northern portion of the Study Area is an extension of Todmorden Village and is predominantly residential in character. There is a cluster of houses constructed in the 20th century inter-war period and a small proportion of houses that are associated with Todmorden Village.



Buildings in Lot 11, historic Chester Village (Character Area A), are an eclectic mix of building forms.



Buildings in Lot 12 (Character Areas B and C)



Buildings in Lot 13 and Lot 14, historic Todmorden Village (Character Areas D, E and portions of C).



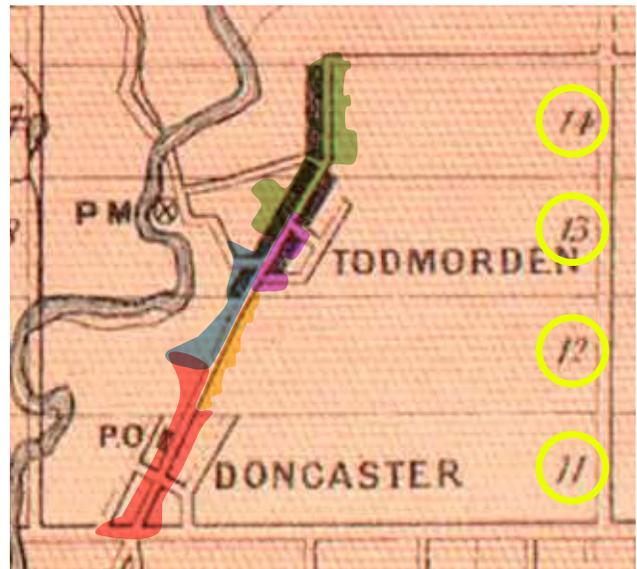
Chester Village was established in Lot 11 (Character Area A), after the Playter family began subdividing their lands in 1855. Pretoria and Broadview Avenues was the central intersection in Chester Village. This portion of the Study Area is an eclectic mix of building types and construction periods, ranging from Victorian workers' cottages to 19th-century commercial buildings and apartments from the 1920s to the 1970s. Buildings are oriented perpendicular to the street with residential properties typically set back with front yard landscaping. Some commercial properties are built to the front property line, adding to the eclecticism of the streetscape.

The east side of Broadview Avenue in Lot 12 (Character Area B) is primarily residential in character. A large proportion of buildings here were constructed once the "Helliwell Estate" was annexed to the City of Toronto in 1912 and the land was subdivided into buildings lots, and after the Don section of the Bloor Street Viaduct opened in 1918, causing a wave of development east of the Don River. These houses diverge from the historic lot pattern established in Chester Village and meet the street at a diagonal. This lot pattern more closely reflects the incorporation of the area into the Toronto street grid, as opposed to an extension of Chester Village, which was annexed by the City of Toronto in 1909.

The northwestern stretch of Character Area A is in Lot 12. It extends the Chester Village lot pattern, although most buildings here were developed during the same era as Character Area B.

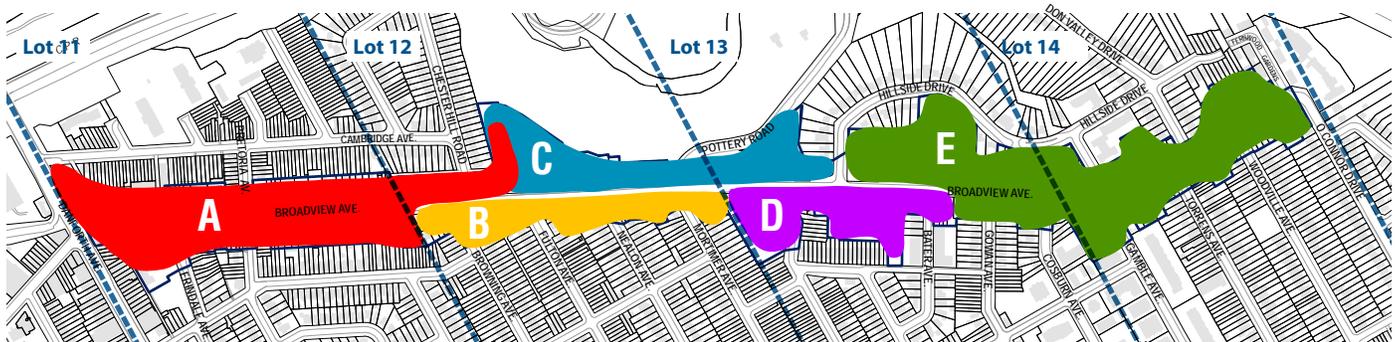
Chester Public School (now Estonian House) predates these and was constructed in 1890. A row of high-rise residential buildings are located on the west side of Broadview Avenue, where the slope of the river valley meets the west side of the Study Area.

The northwestern stretch of Character Area A is in Lot 12. It extends the Chester Village lot pattern, although most buildings here were developed during the same era as Character Area B. Chester Public School (now Estonian House) predates these and was constructed in 1890. A row of high-rise residential buildings are located on the west side of Broadview Avenue, where the slope of the river valley meets the west side of the Study Area.



1878 Historical Atlas of York, Miles & Co indicating historic farm lots, Lot 11, 12, 13 and 14, Concession 2 from the Bay, with Broadview Planning Study Character Areas overlay.

### Character Areas & Historic Farm Lot Divisions



Map of the Study Area. Dotted blue lines indicate the 1790s division of farm lots, also shown in image above.

### 3.3 NATURAL HERITAGE

The Study Area is adjacent to the Don Valley Natural Heritage System which is located to the west. The City's Official Plan policies generally recognize the importance of natural heritage system in promoting clean air, soil, water and supporting the general well-being of the City. The Don valley is essential to the character of the area. The valley provides shade and habitat, recreation and amenity, it helps clean the air, contributes to green links to the avenue and nearby neighbourhood and supports ecosystem diversity. The Official Plan also recognizes the complexity of the natural environment and emphasizes the importance of paying special attention to natural heritage features and function. To this end, protecting, enhancing and restoring natural heritage features is a high priority in City building decisions. The Official Plan stresses the importance of assessing the impacts of human activity in general and new development particular that are adjacent to natural heritage systems.

Located in the Don Valley are the Todmorden Mills Heritage Site and Wildflower Preserve, Evergreen Brickworks and Fantasy Farm. The Todmorden Mills Heritage Site was opened in recognition of the early settlers of the Don Valley who had provided lumber, flour, beer, paper and bricks to the City of Toronto. The site is operated by the City of Toronto, Museum Services.

The Wildflower Preserve with walking trails is a natural habitat, including upland and bottomland forests, dry and wet meadows, swamp lands and a pond. The preserve supports fauna and flora such as dragonflies, water striders Creek Chub, Brook Stickleback and Sunfish. Various birds including Ducks, Kingfishers, Great Egrets, Great Blue Herons and Black-Crowned Night Herons visit the area. The Evergreen Brickworks is a community environmental centre that helps individuals, groups and communities to live, work and play in a sustainable manner.

#### Existing Parks and Publicly Accessible Open Spaces

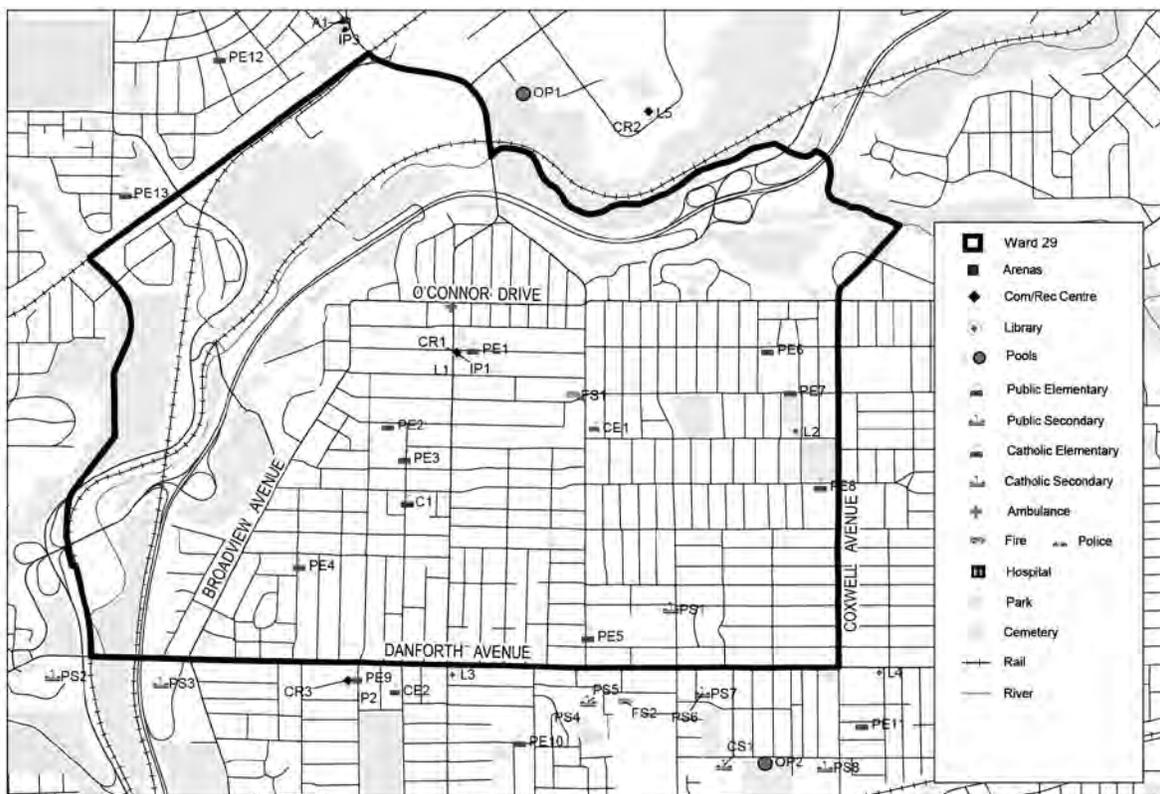


### 3.4 DEMOGRAPHIC PROFILE

A Community Services and Facilities (CS&F) Study Area and Context Area extends beyond the Study Area, including Broadview to Pape Avenue, between O'Connor and Danforth Avenue. The following excerpt from the 2013 CS&F Profile highlights the Study's main findings from its demographic analysis. More detailed maps are found in Appendix D - CS&F Maps. The demographic profile reveals the following prevailing characteristics of Broadview North compared to the City:

- The total population in the CS&F area is 11,560, whereas the population for the City is 2,615,090 during the same period;
- There is a higher proportion of residents in this area between the ages of 25 and 64 years in the Study Area at 63.10% compared to 57.5% for the City;
- The Study Area has a higher proportion of married couples without children at 35% compared to 26% for the City;
- Apartment buildings having five or more storeys are the largest private dwelling structure type at 59% compared to 28% for the City;
- The Study Area has a higher percentage of renter households at 66% compared to 45.4% for the City;
- Immigrants who arrived before 1980 comprise 36% of the population compared to 27% for the City;
- The labour force participation rate at 66% comparable to the City which is 64%;
- The City of Toronto is slightly more affluent than the Study Area as 30% earn over \$80,000 in the City compared to 21% for the area;
- There are smaller household sizes in this area in comparison with the City. 42% live in a one-person household with 32% for the City.

The largest private dwelling structure type in the Study Area is apartments having more than five storeys occupied primarily by renters, suggesting a need for outdoor amenity space and community recreation amenities to serve a population living in high density. The Study Area is also less affluent than the City, suggesting a need for affordable and accessible CS&F space.



Map of Community Services and Facilities - Ward 29

## 3.5 COMMUNITY SERVICES AND FACILITIES

### Libraries

There is no library in the Study Area, however there are two located in the CS&F area, at Pape Avenue and Danforth (710 Pape Avenue) and a satellite facility at 1081 Pape Avenue (Todmorden Room Library). A new library facility has been identified by the community as a priority for the Study Area.

### Child Care

The Study Area contains 3 child care centres at the Massey Centre (1102 Broadview), Estonian House (958 Broadview) and Mini Bluebird Montessori (905 Broadview). 10 Child Care Centres are located in the larger CS&F Study Area, comprising non-profit and for-profit facilities.

### Schools

There are no schools in the Study Area; however, the Toronto District School Board (TDSB) operates three schools in the CS&F area – the Westwood Middle School, Jackman Avenue Junior Public School, and Chester Elementary School. Child care services are provided at Jackman Avenue Junior Public School and Chester Elementary School.

### Estonian House

The Estonian House is located at 958 Broadview. It serves primarily as a cultural and community centre, providing a wide range of community services and programs for the Estonian community and the larger community.

### Places of Worship

Iglesia Evangelica Hispana Church is located at 804 Broadview Avenue in a house form building reminiscent of the Village of Chester. The Estonian Baptist Church is located at 883 Broadview and it supports new immigrants in the areas of financial aid, housing and jobs.

### Human Services/Community Agencies

There are 25 Human Services Agencies within the CS&F Study Area, which provide a range of support services to individuals and families. Services provided include community health, self-employment training for youths and immigrants, language interpretation, immigrant settlement services and legal services. Employment support programs are deemed to be of particular importance to residents in the CS&F Study Area because of the concentration of new immigrants and the higher than average poverty rates.

### Parks & Open Spaces

Three parkettes are located within the Study Area and six additional parks and parkettes are located nearby. The Broadview Subway Station Parkette, Charles Sauriol Parkette and the Gamble Parkette are located in the Study Area. Playter Gardens, Agnes McPhail Park, Carlaw Avenue Parkette, Livingston Park, Logan Avenue Parkette and Westwood Park are located nearby. The Charles Sauriol Parkette recently underwent restorative work including improved site access and visibility, new benches, tree restoration and improvements to drainage and connections to the Don Valley ravine.

The Study Area is within an area which the Municipal Code identifies as a Parkland Acquisition Priority Area. Within these priority areas development is subject to an Alternative Parkland Dedication Rate which is higher than the dedication rate in non-priority areas, and requires the contribution of new parkland or cash-in-lieu of parkland. Cash-in-lieu may be used for the acquisition new parkland or for the improvement of existing parks within the vicinity of the development.

## 3.6 TRANSPORTATION

### Transit

Broadview Avenue north of Danforth Avenue is an important transportation corridor; though it is challenged, like many streets, by the need to accommodate pedestrians, public realm space, cyclists, transit use and automobiles.

Broadview Subway Station, located at Danforth Avenue and Broadview, provides access to the Bloor-Danforth line (Line 2) which runs in an east-west direction. Broadview station makes important transit connections with the streetcars and buses. The 504 and 505 streetcars travel in mixed traffic south of Danforth Avenue. Buses make connections at Broadview Station and travel north in mixed traffic of Danforth Avenue along Broadview, 8 Broadview, 62 Mortimer, 87 Cosburn, 100 Flemington Park and the 322 Coxwell night bus provide local service to the surrounding communities. Broadview station is one of the top 10 stations on Line 2 with the highest ridership and remains an important transit connection.

### Pedestrians and Cyclists

Broadview is a walkable street that provides important connections for pedestrians looking to access transit, goods and services, schools, residential housing and places of employment.

As part of the cycling network, Broadview Avenue north of Danforth Avenue is identified as a suggested on-street connection for cyclist. It provides connections to the bike lanes on Danforth Avenue west of Broadview via the contra-flow bike lane on Chester Hill Road and the suggested on-street routes on Cambridge Avenue. It also connects to the bike lane along Cosburn Avenue east of Broadview and to the bike sharrows that travel south into the valley via Pottery Road.

### Automobile

Automobile traffic currently and historically has been relatively stable over the past decade along Broadview Avenue north of Danforth Avenue. Like many other typical streets in Toronto, it experiences peak conditions during certain times of the day.

### Parking

There are several types of parking areas along Broadview Avenue as it reflects the various land uses of the Study Area. In Broadview south of Pretoria, on-street parking provides short term off-peak parking to those visiting local restaurants, offices, and retail shops. There is a Green P surface parking lot containing 93 parking spaces at the corner of Erindale Avenue and Ellerbeck Street, which provides public parking. Some single detached residential properties have front yard parking, while multistory residential buildings have parking available on-site to its residents and their visitors. In areas where there is a mix of residential and retail-commercial uses, properties have some private parking and a few customer parking on site.

# 4. Broadview Design Guidelines

## 4.1 GUIDING PRINCIPLES

The following principles form the basis of the urban design guidelines and recommendations that will guide development in the Broadview Avenue Study Area.

### 1. Enhance Green Connections Between the Don Valley Ravine and Broadview Avenue

**Principle:** Create green connections between the Study Area and the natural heritage areas (Don Valley ravine).

The rich natural heritage of the Don Valley is an asset that Broadview can build upon. In order to bring this attribute to the forefront, connections will be established by major tree planting on Broadview Avenue, Pottery Road and other local streets where possible. The natural environment/ecosystem will be supported by encouraging high environmental performance for new development. In addition, development adjacent to the natural heritage system will be evaluated to determine development impacts and identify mitigation strategies. Pottery Road's "Green Gateway" to the Don Valley should be preserved and enhanced.

### 2. Enhance and Complement the Character of Broadview's Heritage

**Principle:** The character and significance of the area's history in the form of the early buildings, settlements and industries should be highlighted, preserved and commemorated.

From the indigenous settlements in the valley, to the European settlements of Chester, Todmorden, and Playter Estates, as well as Toronto's early industrial era (Todmorden Mills and Evergreen Brick Works), Broadview Avenue's history provides a rich narrative. Qualitative attributes such as the sense-of-place and character should be maintained and enhanced to add to the character of the Study Area. Properties currently listed on the City of Toronto Heritage Register and those that are designated under either Parts IV or V of the Ontario Heritage Act will be

conserved in accordance with relevant legislation. Informal brochures and trail maps with information on the history of the area should be developed.

### 3. Create Quality Public Spaces and Parks

**Principle:** New public parks, locations with views into the Don Valley ravine (Natural Heritage Areas) and privately owned open spaces should create a connected and coherent system that enhances the identity of the Study Area.

Creating a green, comfortable and attractive public realm, exploring opportunities for new public and private (but publicly accessible) open spaces, and improving existing parks is paramount. New and existing parks and open spaces should be upgraded to serve a variety of users. Opportunities to create a network of parks and open spaces are a priority for the area.

The Study Area is located within a Parkland Acquisition Priority Area and is subject to the City's Alternative Parkland Dedication By-law. Redevelopment on large sites presents an opportunity to create new public parks that are safe, visible and accessible. The preservation of viewpoints into the Don Valley, POPS, and continuation of front yard landscaping are encouraged where possible.

### 4. Making Broadview Avenue a Place

**Principle:** Supporting a vibrant and dynamic community.

Broadview Avenue should reinforce its identity as a place in its own right shaped by its proximity to the Don Valley, its heritage, varied built form and public realm elements, supported by its residents and local shops. Support for local businesses with animated windows and entrances, sufficient building setbacks, safe pedestrian conditions, street level patios, open display of merchandise, trees and reorganized street furniture is encouraged. In segments of the Study Area where residential

uses are on the ground floor, and street activity is less intensive, there should be sufficient setback with expansive landscaping to buffer homes from street activity.

## 5. Maintain a Varied Built Form Character

**Principle:** *Redevelopment should include a mix of residential, commercial and institutional uses at various scales, with mid-rise buildings and modified mid-rise buildings, as well as low scale buildings.*

Mid-rise buildings are anticipated as the dominant building type for infill development, in keeping with the recommendations of the Avenues and Mid-rise Buildings Study. However, modified mid-rise which may result in shorter or taller buildings may be appropriate on certain sites and townhouses may provide a transition to adjacent neighbourhoods. The incorporation of additional setbacks at higher heights should be used to define the prevailing street wall. The built form guidelines will assist in minimizing impacts on local transportation capacity and achieve appropriate transition to the surrounding context. New infill development will co-exist with existing development in a coherent way.

## 6. Improve Mobility and Enhance Transportation Options

**Principle:** *Improve pedestrian and vehicular movement in the Study Area by enhancing walking, cycling and public transit use and connections through redeveloped parcels.*

New development creates opportunities for improving sidewalk conditions and bicycle facilities, and reorganization of street furniture that encourages walking and cycling. The entry and exit points to the Study Area, existing street uses, and limited supply of on-street parking has resulted in parking issues and conflicts. Transit oriented redevelopment with an appropriate

mix of uses provides opportunities for rebalancing modes of transportation and shared parking among uses with different peak characteristics.

## 7. Integrate Land Uses and Redevelopment with Supporting Infrastructure

**Principle:** *Redevelopment provides an opportunity to improve transportation and servicing infrastructure to support the existing and future community.*

On large lots, where intensification may occur on a larger scale than on a typical lot, servicing infrastructure may also need to be upgraded in order to support new development. Pedestrian connectivity should be improved and street addresses for development internal to main buildings fronting on the Avenue should also be provided. Where parks are provided, they should be connected to the existing local network, front onto the main street and designed to be publicly accessible.

## 8. Improve Community Services and Facilities through Redevelopment

**Principle:** *Community services and facilities serving the Study Area should be improved.*

Investment may be required to upgrade, improve and enhance access to community services and facilities. New developments, through Section 37 benefits where appropriate, should play a role in funding facilities as outlined in Section 5.1.1.6 of the Official Plan. A local library facility for example has been identified as a priority in the Study Area.

## 4.2 Public Realm

The public realm includes streets, sidewalks, parks, lookout points and other community amenities that are accessible to the public. The guidelines in this section are intended to guide development for the enhancement of the pedestrian environment and the public realm. This will also help to improve accessibility for all users.

One of the elements that characterizes this segment of Broadview is the proximity to the Don Valley Ravine. Views into this green space are only perceived sporadically at specific points. Therefore, the main objectives of the Public Realm guidelines are to raise awareness of the existence of these green spaces, to expand the visual and physical connections to these spaces, to add to the greenery of the Broadview streetscape and to define a comprehensive and cohesive system of public spaces.



Broadview Avenue Community Vision Diagram

### 4.2.1 PEDESTRIAN CONNECTIONS

Connections designed for pedestrians can create a pleasant and comfortable pedestrian experience and contribute to the walkability and sense of community by joining linear parks, parkettes, playing fields, natural areas, plazas and historic buildings. Pedestrian connections on Broadview can be improved and expanded through:

#### Sidewalk Improvements

Current sidewalk conditions along Broadview Avenue can be improved through new developments, creating wider sidewalks in some sections and adding more street trees on Broadview.

The City's Urban Design Streetscape Manual provides standards for the design of comfortable, aesthetically-pleasing sidewalks.

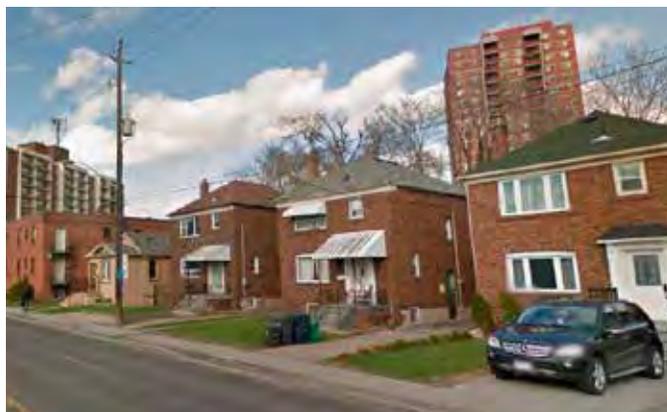
It also emphasizes design quality and amenity in the pedestrian realm with specifications for paving, trees, medians, lighting and street furniture. All new development should meet the Manual's requirements of a minimum pedestrian clearway of 2.1 metres and other public realm design specifications.

Opportunities for sidewalk improvements are more limited on the northern portion of Broadview Avenue, between Bater Avenue and O'Connor Drive. The prevailing built form in this area are house form buildings that front onto Broadview. Furthermore, this area is primarily designated as *Neighbourhoods*.

While it may not be feasible to widen the sidewalks, there are still opportunities to green the street through tree planting. The City's

Urban Forestry program for planting trees in the public portion of front yards should be promoted and implemented.

Opportunities to improve the sidewalks may also be feasible when the road is scheduled for major upgrades; however, an environmental assessment will be required. Further study should be undertaken to determine if additional sidewalk width can be gained at the time reconstruction is contemplated. Opportunities to add cycling infrastructure and to bury overhead wires should also be explored.

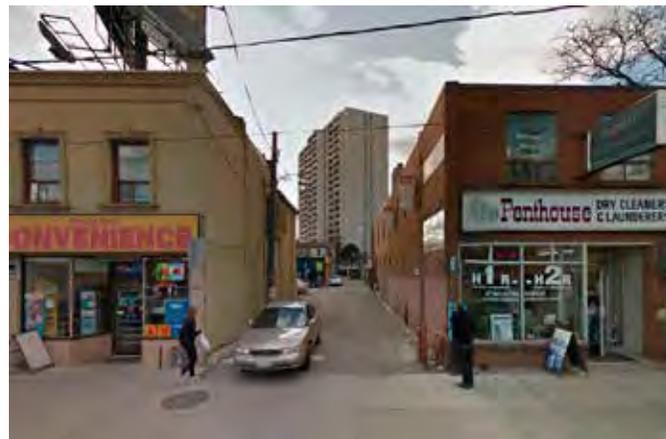


Opportunities for tree planting in the front yard of residential properties (east side of Broadview Avenue, north of Gowan Avenue)

## Off-street Linkages

Connection between open spaces can also be made available off-street, through laneways. Laneways are a potential open space resource in urban neighbourhoods that can provide a safe, calm, and comfortable refuge for pedestrians and cyclists. They can be transformed into shared spaces that prioritize the use of the space for pedestrians to create enhanced places for landscaping, seating and active transportation. Laneway beautification can be achieved through embellishments such as murals, enhanced paving materials, appropriate lighting, and green walls. Laneway improvements will be studied in consultation with Transportation Services staff.

A potential pedestrian linkage can be explored on the laneway located between 792 and 794 Broadview Avenue, directly across the street from the Broadview Subway Station Parkette. This laneway can provide a more pedestrian-friendly connection between the Playter Gardens and the Subway Station Parkette and an alternate access to Broadview Avenue from Cambridge Avenue.



View of laneway from Broadview Avenue looking West



The Green Alley Project in Detroit, USA

## 4.2.1 PEDESTRIAN CONNECTIONS (cont'd)

### Hillside Drive Green Street Project

On August 25, 2014, City Council directed City Planning and Transportation Services to report back to Toronto and East York Community Council regarding streetscape matters on Hillside Drive, south of Gamble Avenue. In particular, staff were directed to review potential green streetscape opportunities for the northern portion of Hillside Drive.

City Council adopted staff's preferred concept on February 3, 2016 and directed city staff to work on refining the adopted concept with the local Councillor and area residents. Some of the recommendations included the improvement of pedestrian access, reduction of paved surfaces, and the reinforcing of Hillside Drive as a green connection between Charles Sauriol Parkette and Gamble Avenue.



South end of Hillside Drive, with no sidewalks



Opportunity to extend green space in existing surface parking area

## 4.2.2 STREETScape

Streetscape improvements will reinforce existing character while seeking opportunities to expand the public realm. New developments should promote the pedestrian amenity area with landscaping, benches, bicycle racks, and other types of street furniture. Below are the four types of existing streetscape to be reinforced and improved through the development application process.



Sketch of proposed commercial frontage streetscape with a boulevard width of 4.8m

## Commercial Frontage

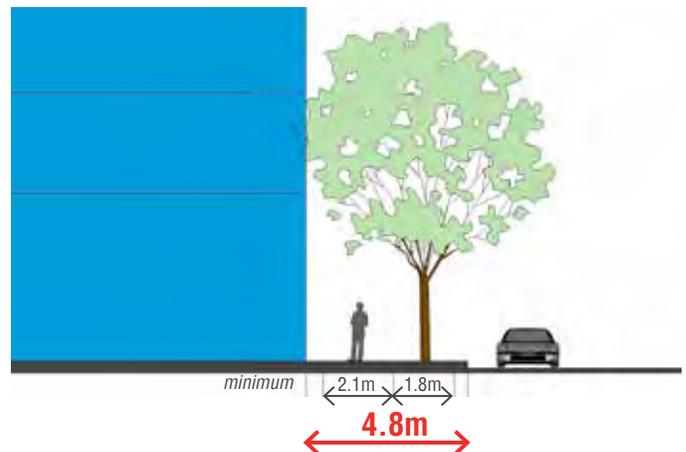
The southern portion of Broadview Avenue from Danforth Avenue to approximately Browning Avenue is characterized as a Main Street, with commercial, residential and mixed-use buildings which generate grade-related activities. These buildings create a direct or "storefront" relationship with the public realm, encouraging diverse types of social interaction at a pedestrian scale. For this reason, wider sidewalks and more pedestrian amenities are needed.

The existing boulevard width is narrow with limited opportunities for tree planting or other pedestrian amenities. This condition can be improved when new developments are proposed.

Where new development occurs, a minimum boulevard width of 4.8 metres is recommended, measured from the curb to the building face of a new development. This will include an edge zone along the curb, a tree planting zone of approximately 1.8 metres width and a minimum clear pedestrian walkway of 2.1 metres. Street trees should be planted except where they conflict with utilities or transit stops.



Existing boulevard width south of Pretoria Avenue



## Patios and Seating Areas

As mentioned above, the southern portion of Broadview Avenue is characterized as a Main Street. One of the character elements typically found in a Main Street is the presence of bustling meeting places for local neighbours and the wider community.

Currently, some of these types of meeting places already exist, especially on important street corners and intersections. Creating more of these places reinforces the community feel of the Study Area and the role of corners as social spaces where neighbours meet and congregate.

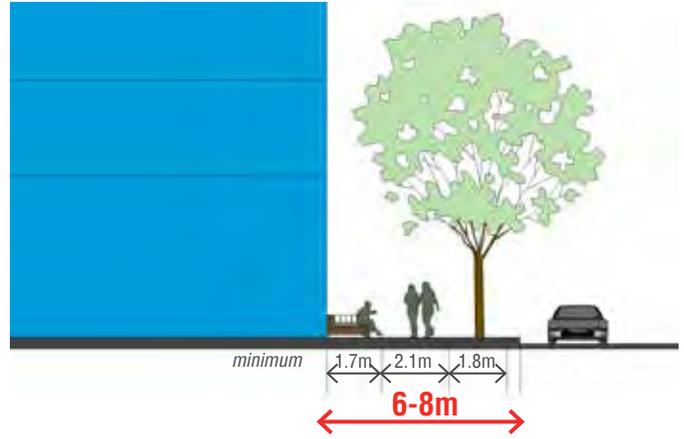


Existing outdoor patio at the intersection of Broadview Avenue and Pretoria Avenue

## 4.2.2 STREETSCAPE (cont'd)

Widening the boulevard width in strategic locations to accommodate patios and seating areas can help achieve this objective. A boulevard width of 6 to 8 metres is recommended, measured from the curb to the building face of a new development. This will include an edge zone along the curb, a tree planting zone of approximately 1.8 metres width, a clear pedestrian walkway of 2.1 metres, and a patio zone of approximately 2 metres wide.

The intersections that will be appropriate for the location of these meeting places are: Broadview Avenue and Pretoria Avenue, Broadview Avenue and Browning Avenue; and Broadview Avenue and Pottery Road.



### Residential Front Yard

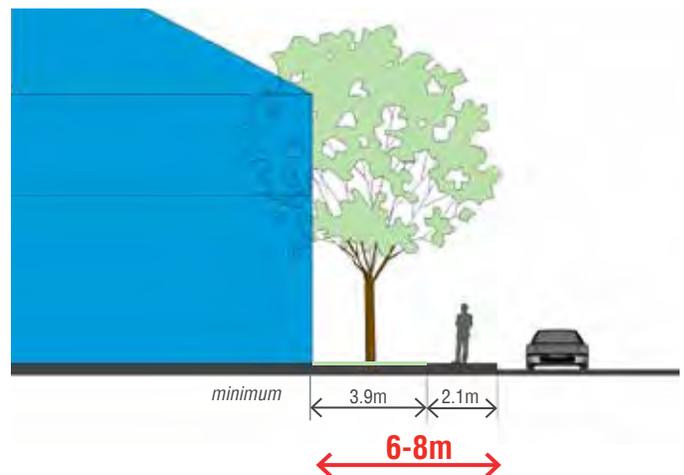
Existing low-rise residential areas along Broadview Avenue, mainly between Browning Avenue and Mortimer Avenue, and between Bater Avenue and O'Connor Drive, are characterized by deep, green front yard setbacks. These setbacks offer an appropriate buffer from the street and create a green and open pedestrian experience.

To reinforce existing residential character in these areas, a minimum boulevard width of 6 metres is recommended, measured from the curb to the building face of a new development. This will include a clear pedestrian walkway of 2.1 metres and a deep setback of approximately 4 metres for soft landscaping in front yards.

While new development opportunities are limited in these areas due to the size and configuration of these lots and its designation as *Neighbourhoods*, opportunities to add to the green feel of the street are possible through the City's Urban Forestry program for planting trees in the public portion of front yards. Participation in this program is voluntary, but it is highly recommended and should be promoted and implemented.



Low-rise buildings on the East side of Broadview Avenue, near Browning Avenue



## Expansive Front Yard Landscaping

In the middle portion of the Study Area along Broadview Avenue, approximately between Chester Hill Road and Bater Avenue, expansive front yard landscaping defines the existing character of the street, creating a very green and open feel streetscape. This segment of the Avenue is characterized by deep setbacks with high-rise buildings that are recessed from the public street. This is also an area that is mostly zoned as Residential Apartment and Residential Multiple Dwelling Zone.

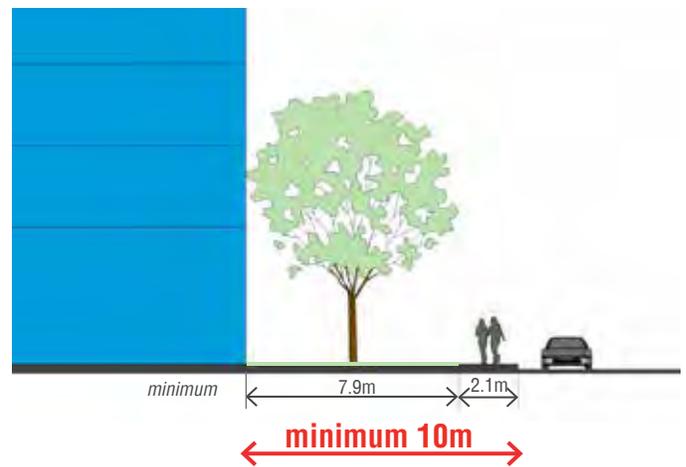
To reinforce this green, open feel of the street, a minimum boulevard width of 10 metres is recommended, measured from the curb to the building face of a new development. This will include a clear pedestrian walkway of 2.1 metres and a setback of approximately 8 metres for soft landscaping and tree planting.

## Other Improvements

Additional embellishments in the commercial section of the Study Area (south of Pretoria) will be encouraged to animate the public realm. These improvements will be reviewed in consultation with the Danforth BIA and may include coordinated street furniture, enhanced paving, and temporary planters.



High-rise building on the East side of Broadview Avenue, near Pottery Road



## Streetscape Recommendations Map



- 4.8m commercial frontage
- 6-8m patios and seating areas
- 6-8m residential front yard
- 10m expansive front yard landscaping

### 4.2.3 VIEWS AND VIEWPOINTS

Panoramic views into the Don Valley and the City's skyline are currently accessible from a number of locations along Broadview Avenue. The significant views of green space with the City's skyline as a backdrop accentuate Broadview's unique connection with natural heritage and the City. The availability and accessibility of these viewpoints should be protected and preserved for public enjoyment. The massing of new developments should complement these views and viewpoints.

Within the Study Area, views from the following viewpoints need to be protected and public access to these locations should be maintained:

1. Intersection of Broadview Ave. and Pottery Rd., from the vantage point of approaching from Mortimer Ave.
2. Rear of northwest corner of Broadview Ave. and Pottery Rd.
3. Rear of southwest corner of Broadview Ave. and Pottery Rd.
4. Rear of Charles Sauriol Parkette

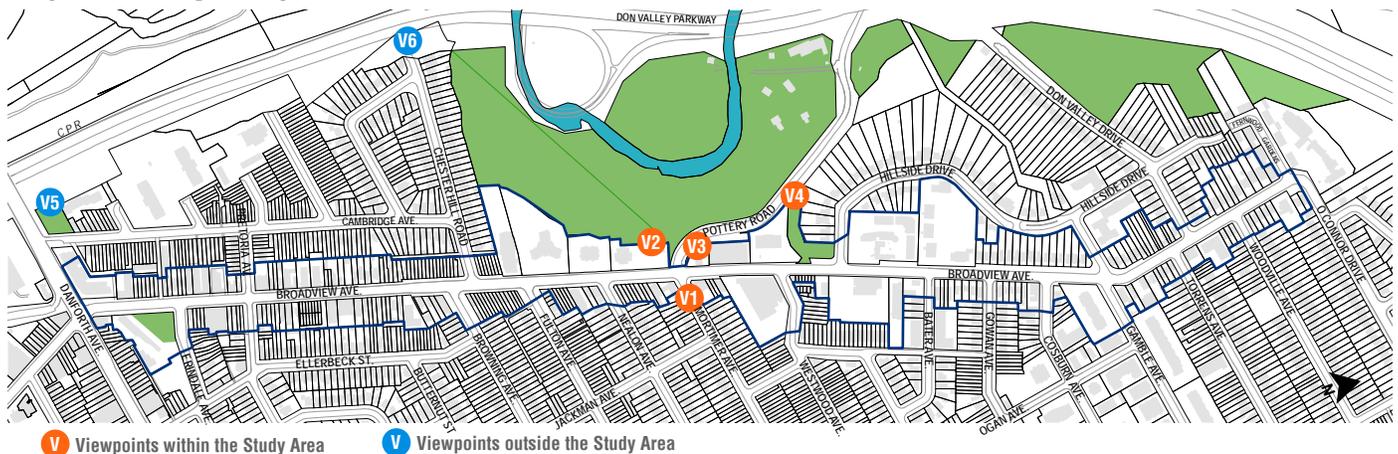
Future developments should be designed to ensure that the current viewpoints continue to exist for public use and enjoyment. One way of doing this is to create well-designed lookout points as part of the public space.

New developments in sites adjacent to these viewpoints should not negatively impact the existing views or viewpoints.



View towards the Don Valley and the City's skyline from Viewpoint #3

#### Map of Existing Viewpoints



## 4.2.4 PARKS AND OPEN SPACES

As Broadview continues to grow and intensify, there is an increasing need and demand to create new parks and open spaces as places of retreat, relaxation and recreation. Seeking opportunities to create and implement these open spaces is a priority. This can be achieved by creating new parks through development proposals, or improving existing open spaces with enhanced paving, furniture and playground facilities.

### On-site Parkland Dedication

Currently, two sites within the Study Area at 1015 Broadview and 1099 Broadview have been identified as sites for on-site parkland dedication. Future development on the above-noted sites will provide on-site parkland dedication of up to 15% of the lot area in accordance with the Alternative Rate By-law. This is anticipated to result in two new parks of approximately 850 to 950 m<sup>2</sup> each.

The design and location of these new open spaces within their respective sites will be decided in conjunction with the community and other stakeholders during the development application process. New parks should face a public street.

### Opportunities for Park Improvements

Improvements to the Charles Sauriol Parkette were completed in summer 2015, including a lookout area and additional seating areas. Improvements to Charles Sauriol Parkette were funded by Section 37 contributions from new development in the area. Opportunities for further improving existing parks and parkettes in the area funded by Section 37 contributions from new developments may be considered with public input and in consultation with Parks, Forestry and Recreation staff.

As part of the Hillside Drive Green Street Project, a comprehensive connection along Hillside Drive, between Charles Sauriol Parkette to the existing green space at the southwest corner of Gamble and Broadview Avenues, was proposed. A series of landscaping and paving options have been planned to improve this corner and incorporate it into the overall open space network in the Study Area. The Hillside Drive Green Street Project also recommends installing interpretive signage along the connection to create a "Charles Sauriol Walk" that will end at Gamble Parkette.

### Opportunity Map for New Parks and Open Spaces



## 4.3 Built Form

### 4.3.1 GENERAL BUILT FORM GUIDELINES

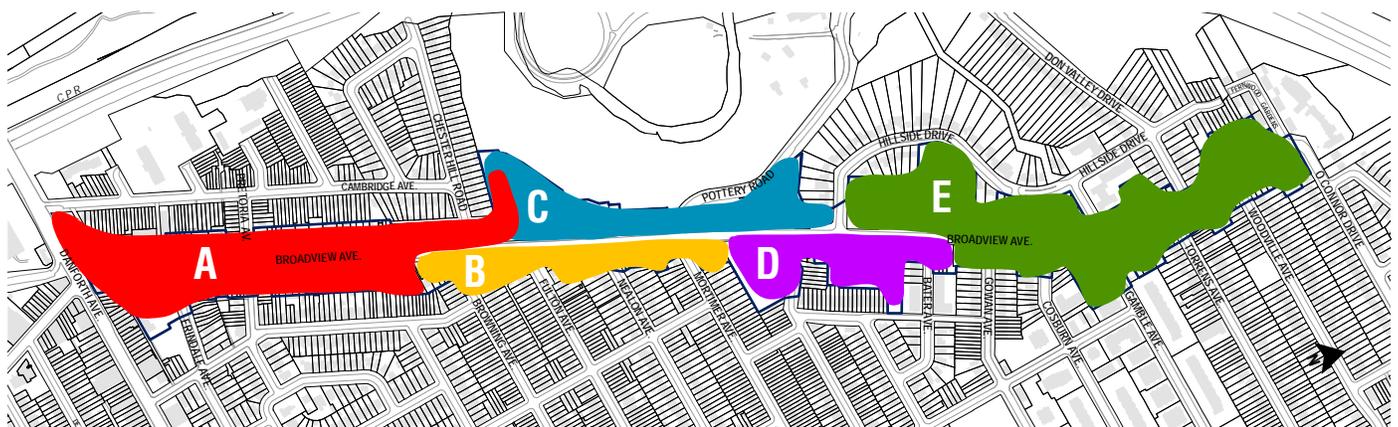
Built form guidelines for Broadview Avenue support the guiding principles of the Broadview Avenue Planning Study and the Broadview Avenue Vision Statement. These guidelines recognize and acknowledge the diverse range of building types along the street. Despite this diversity, certain building elements stand out as contributing to the character of the street: brick cladding, punched windows, vertical and horizontal articulation of building facades, pronounced storefronts, recessed entrances, and vertical rhythm. These building elements sit within the context of Broadview's cultural heritage and close physical relationship to the natural heritage of the Don Valley.

These guidelines put forward mid-rise buildings as an appropriate typology for incremental intensification along Broadview. Appropriately designed mid-rise buildings can complement the context of their immediate surroundings, provide a more cohesive massing and scale along the length of the Avenue, and incorporate building elements that strengthen the street's existing character. This is appropriate for Broadview Avenue, where new infill development will co-exist for the foreseeable future with a diverse range of pre-existing building types. The built form guidelines also complement the public realm guidelines, facilitating improvement and expansion of the public realm through building setbacks and tree planting achieved by way of new development.

The Broadview Avenue Planning Study has identified five Character Areas within the Study Area. The diversity of Broadview Avenue is part of its unique character and one single set of built form guidelines would not be sufficient for the entire area at a fine grain level. These five character areas have provided a part of a framework within which to customize the city-wide Avenues and Mid-rise Building Study with respect to the immediate context of each character area.

These guidelines supplement and build upon the city-wide Avenue and Mid-rise Building Study, which otherwise continue to apply to Broadview Avenue (see diagrams in next page illustrating key performance standards from the Mid-rise Guidelines). The built form guidelines for each Character Area highlight the recommendations that differ from the city-wide Mid-rise Guidelines. In these cases, the built form guidelines specified in each Character Area prevail over the city-wide Mid-rise performance standards.

These built form guidelines should be read and interpreted together with the Official Plan, Avenues and Mid-rise Building Study, and the Site and Area Specific Policies.



Character Areas Map

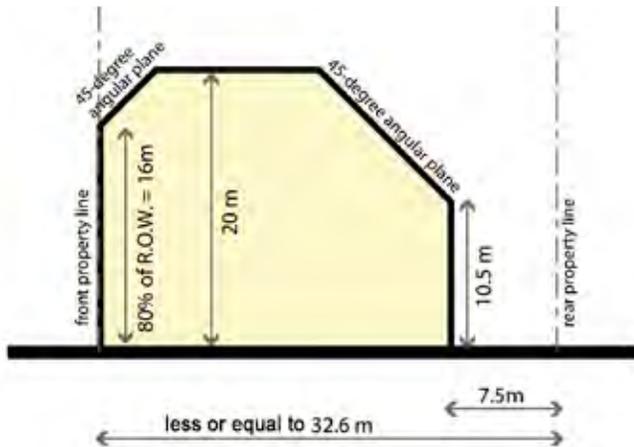


Illustration of the minimum setbacks and front and rear angular planes of a development in a shallow lot that conforms to the Performance Standards from the Mid-rise Guidelines

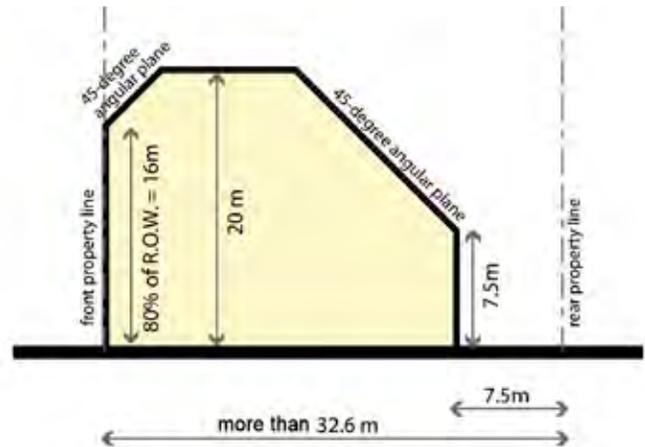


Illustration of the minimum setbacks and front and rear angular planes of a development in a deep lot that conforms to the Performance Standards from the Mid-rise Guidelines

## The Estonian House (958 Broadview Avenue)

Due to this site's unique conditions and dimensions, the Estonian House has been identified as being part of Character Area A and Character Area C. A more thorough review and community consultation is needed if a development application comes forward. Nonetheless, as part of the Broadview Avenue Study, a set of development principles have been identified by city staff through the community consultation process. These principles require any proposed new development on the site to address the following aspects:

- Conservation of the property's cultural heritage values and attributes
- Appropriate built form transition towards the adjacent *Neighbourhoods* area
- Adequate separation distances from adjacent properties
- Sensitivity to adjacent valleys and ravines by providing the required setbacks from TRCA top-of-bank lines
- Regard for existing context by continuing the expansive front yard landscaping in this segment of Broadview
- Explore potential of the property to serve as a cultural hub for the community



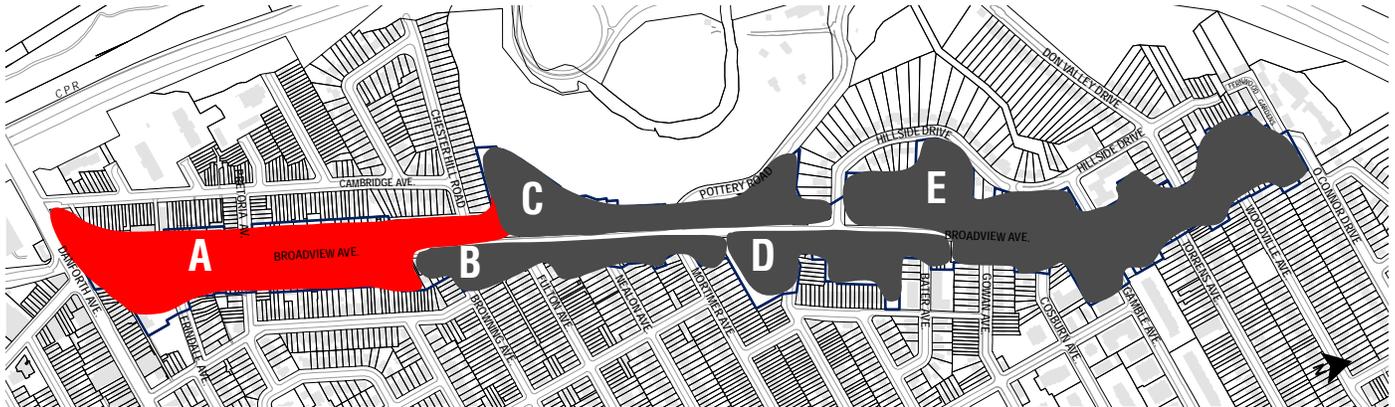
The Estonian House (Chester Public School) ca. 1905



The Estonian House today

## 4.3.2 BUILT FORM GUIDELINES FOR CHARACTER AREAS

### Character Area A



West of Broadview Avenue, from Danforth Avenue to Pretoria Avenue



East of Broadview Avenue, from Pretoria Avenue to Browning Avenue

Character Area A comprises lots on both sides of Broadview Avenue from Danforth Avenue to approximately a few lots north of Chester Hill Road to the west and to a few lots south of Browning Avenue to the east. This includes the following properties: 80 Danforth Avenue, 778 to 958 Broadview Avenue, 90 Danforth Avenue, and 769 to 883 Broadview Avenue.

This area displays a variety of lot sizes and land uses, with an equally diverse range of built forms, including single family dwellings, walk-up apartment buildings and office buildings.

The general feel of the street is commercial due to at-grade retail uses, with more commercial activity concentrated near Danforth Avenue and the TTC subway station. In this sense, this section of Broadview can be described as a main street and an entry point to the Broadview community. Intensification in the form of mixed-use, mid-rise buildings is anticipated in Character Area A.

### Built Form Guidelines

New development in this Character Area will conform to the Mid-rise Guidelines performance standards, including minimum setbacks and front and rear angular planes. Additional guidance is provided as follows.

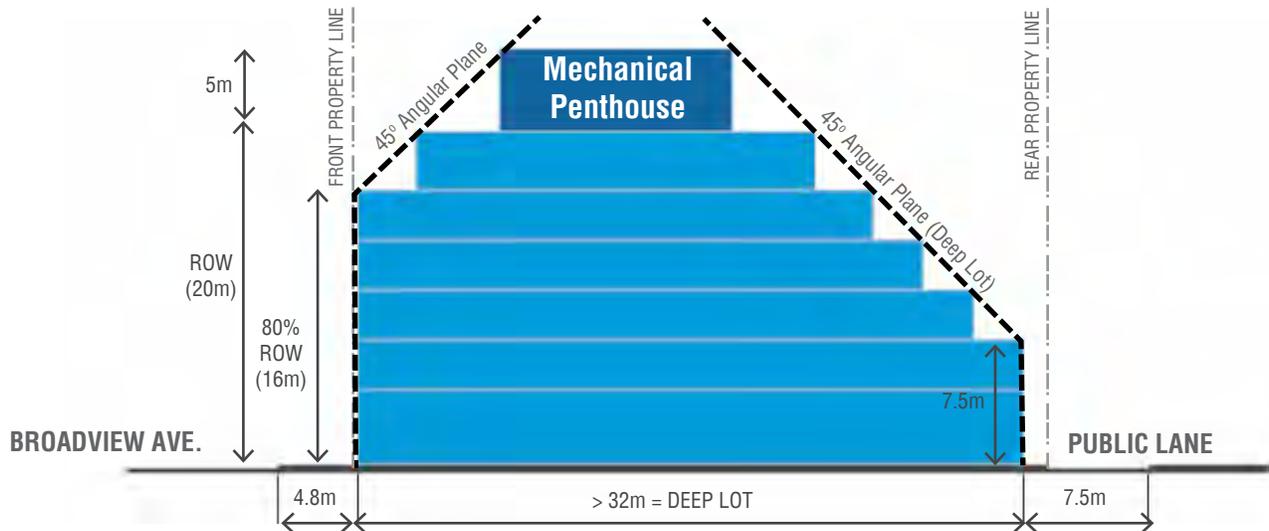
**Maximum Building Height:** The maximum height of the buildings will be 20 metres or 6 storeys. Some segments of Broadview's right-of-way within the Study Area are wider than 20 metres; however, the depth of lots in this segment is insufficient to support a building over 20 metres.

Similarly, the street wall for new developments will be at a maximum height of 16 metres irrespective of the street's right-of-way width at that location.

**Setback from the Street:** Sidewalk conditions in Character Area A were identified as poor and narrow, especially in an area where pedestrian traffic is heavy due to the ground level commercial use. These conditions can be improved through new developments.

New developments will set back to provide a minimum dimension of 4.8 metres between the curb edge and the front building face. This distance will allow for a 2.1 metre pedestrian clearway and a tree planting zone. In corner lots, especially at key intersections such as Broadview/Pretoria and Broadview/Browning, the building should set back to provide a minimum dimension of 6 metres between the curb edge and the front building face to accommodate patios and seating areas.

**Building Articulation:** Character Area A contains what was the historic Village of Chester (Doncaster). A number of buildings were identified as properties with heritage interest. The historic pattern of development in this area includes irregular building types, setbacks, and heights which add to the eclecticism of the street. New buildings should reflect this diversity through varying heights and both vertical and horizontal articulations to break up the building mass. The predominant condition of laneways and private walkways from the street to the rear of properties also creates gaps or reliefs along the street wall. New mid-rise buildings should express this street rhythm by breaking the mass of the building with deep vertical architectural articulation that creates visual gaps along the street edge (see diagram below).



Cross section of a development in a deep lot that conforms to the built form guidelines in Character Area A.



Sketch of recommended mid-rise form on Character Area A

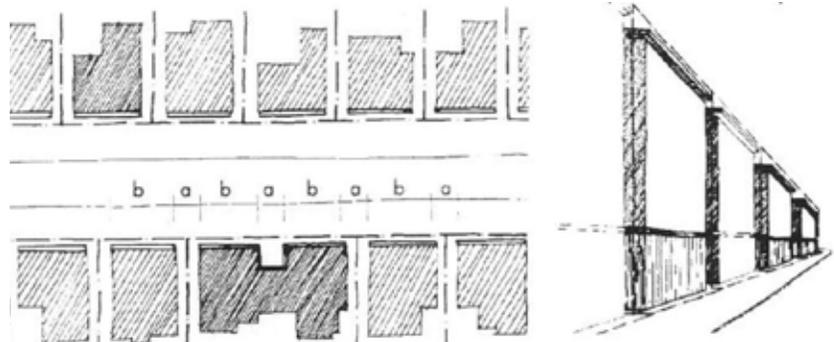
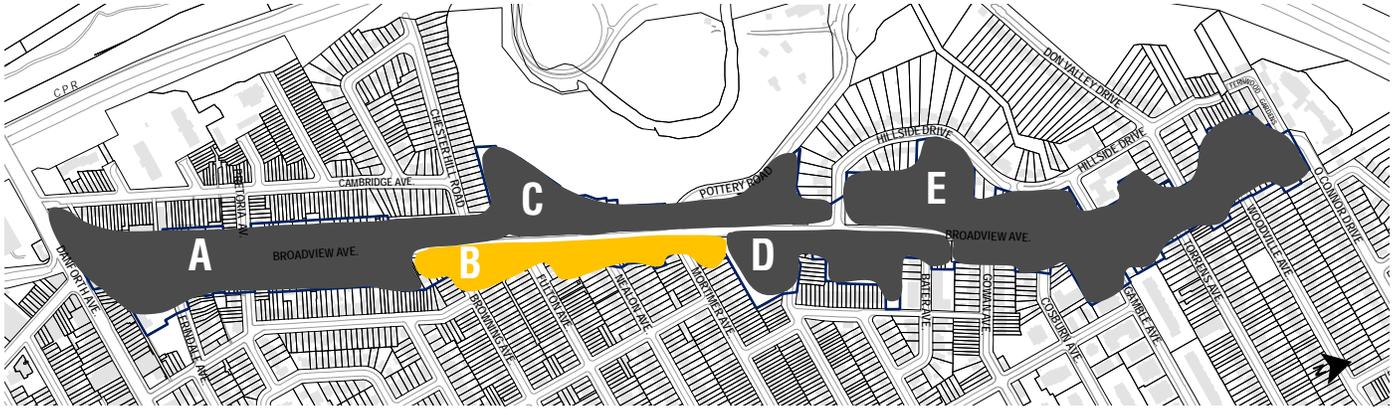


Diagram: Built form rhythm created by a regular pattern of building and open gaps along building frontages

## Character Area B



East of Broadview Avenue, from Browning Avenue to Nealon Avenue

Character Area B includes lots on the east side of Broadview Avenue, approximately from Browning Avenue to Mortimer Avenue. This comprises properties from 885 to 1007 Broadview Avenue. This area is a designated *Mixed Use Area* but is predominantly residential, with mostly 2-3 storey high buildings on small lots that front Broadview Avenue in a diagonal configuration. This condition creates a unique streetscape with landscaped "zig-zag" front yards.

This section of Broadview was also identified in the Avenue and Mid-rise Study for mid-rise intensification. However, due to the lot size and configuration, mid-rise buildings may only be possible in corners where the lots are wider and deeper. Also, due to the predominantly residential character of this section of Broadview, a more modest and sensitive mid-rise building is recommended.

### Built Form Guidelines

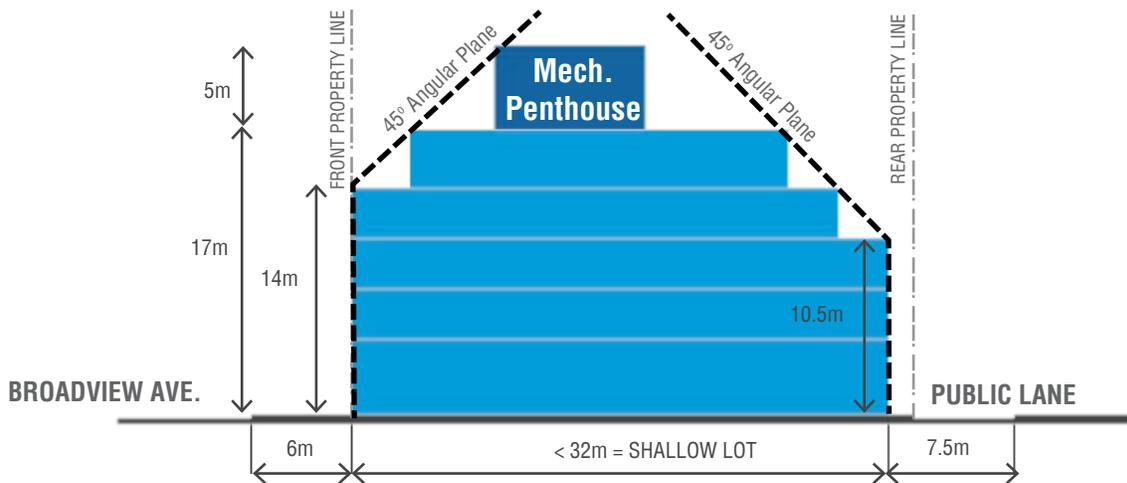
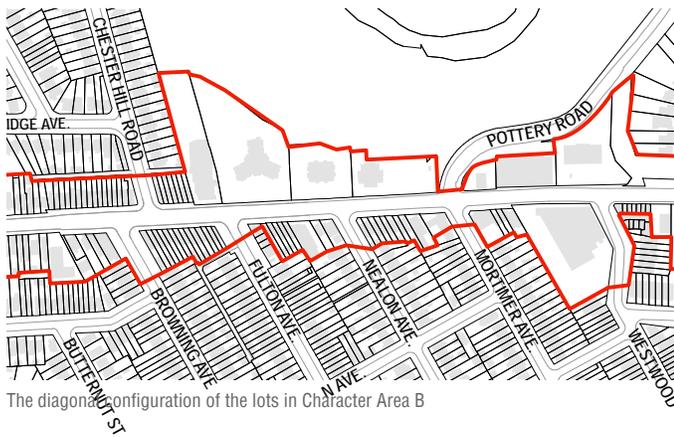
The recommended built form for Character Area B is a modified version of the Mid-rise Guidelines, particularly with a lower building height and street wall. The rest of the *Mid-rise Guidelines* performance standards applies, including minimum setbacks and rear angular planes. Additional guidance is provided as follows.

**Maximum Building Height:** The maximum height of the buildings will be 17 metres or 5 storeys. This height should only be achieved if the dimensions of the lot and the required angular planes are also complied with. Mechanical penthouses, elevator runs, stair towers or other equipment and servicing may exceed the maximum height limit by up to 5 metres but may not penetrate any angular plane.

**Front Angular Plane:** To create a street wall that complements the lower character of Area B, the front angular plane of 45 degrees will be measured from a height of 14 metres (4 storeys).

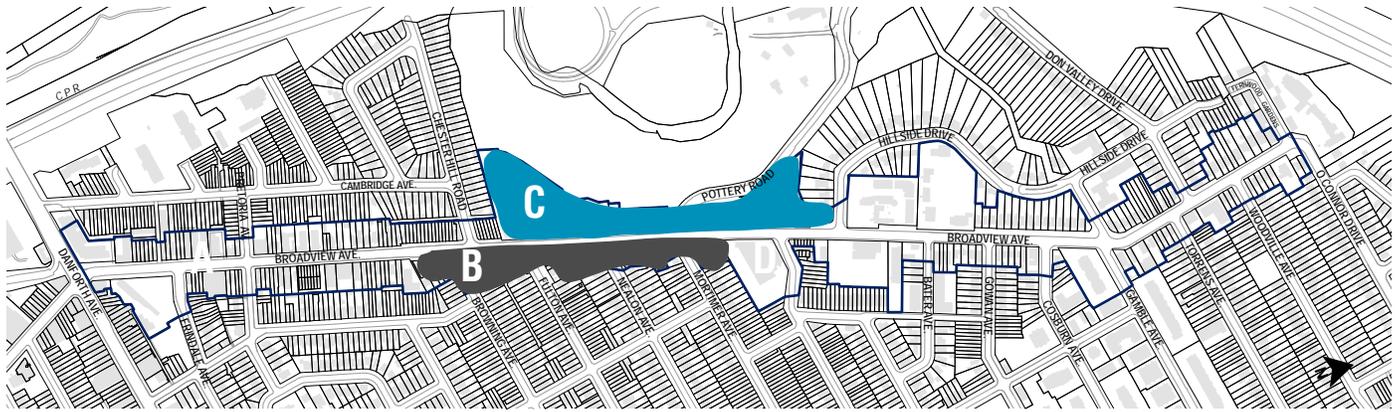
**Setback from the Street:** Wider sidewalk zones are needed in this area to reinforce existing residential character with deep setbacks for soft landscaping in front yards. New developments will set back to provide a minimum distance of 6 metres between the street curb edge and the front building face. This distance will allow for a 2.1 metre pedestrian clearway and a deep setback of approximately 4 metres for soft landscaping on the building's side. In corners, the 4 metres soft landscaping will be replaced by outdoor patio areas or seating areas. The specific setback from the property line that will be required to achieve the minimum 6 metres varies throughout the Study Area and will be determined on a site specific basis.

**Building Articulation:** The diagonal configuration of the lots in Character Area B reflects how new streets in the Study Area were integrated in Toronto's street grid during a wave of development after the construction of the Prince Edward Viaduct in 1918. New facades should as much as possible respond to this unique configuration.



Cross section of a development in a shallow lot that conforms to the built form guidelines in Character Area B.

## Character Area C



West of Broadview Avenue, from Chester Hill Road to Pottery Road



West of Broadview Avenue, from Pottery Road to Hillside Drive

Character Area C is located on the West side of Broadview Avenue where tall apartment buildings dominate the landscape. This area extends from Chester Hill Road to Hillside Drive and comprises properties from 958 Broadview Avenue to and including the Charles Sauriol Parkette. The majority of the lots are deep and have wide frontages, generally with expansive front yard landscaping. Buildings in this area back directly into the Don Valley, which is regulated by Toronto and Region Conservation Authority (TRCA), Ravine and Natural Feature Protection, and Parks, Forestry and Recreation (PFR). Because of this location, views towards the Don Valley are a unique character feature and precious asset of Character Area C.

The development lots in this area are not dimensioned to accommodate additional tall buildings. However, this character area has infill development potential for mid-rise or modified mid-rise buildings.

### Built Form Guidelines

New development in this Character Area will conform to the Mid-rise Guidelines performance standards, including minimum setbacks and front and rear angular planes. Additional guidance is provided as follows.

**Maximum Building Height:** The maximum height of the buildings will be 20 metres or 6 storeys. This height should only be achieved if the dimensions of the lot allows sufficient room to meet the required setbacks, angular planes and view protection policies.

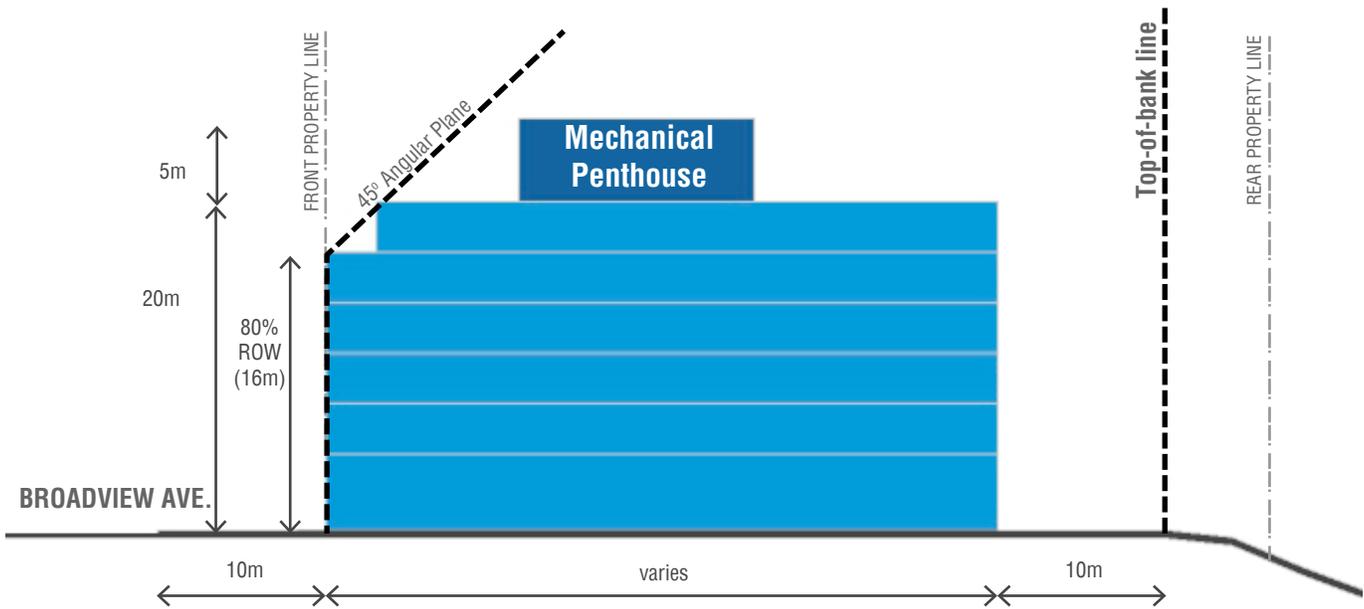
Mechanical penthouses, elevator runs, stair towers or other equipment or servicing may exceed the maximum height limit by up to 5 metres but may not penetrate any angular planes.

**Setback from the Street:** Wider sidewalk zones are needed in this area to reinforce the existing open feel character created by the expansive front yard landscaping of high-rise buildings. New developments will setback to provide a minimum dimension of 10 metres between the curb edge and the front building face. This distance will allow for a 2.1 metre pedestrian clearway and a deep front yard setback of approximately 8 metres for soft landscaping on the building's side. The specific setback from the property line that will be required to achieve the minimum 10 metres varies throughout Broadview and will be determined on a site specific basis.

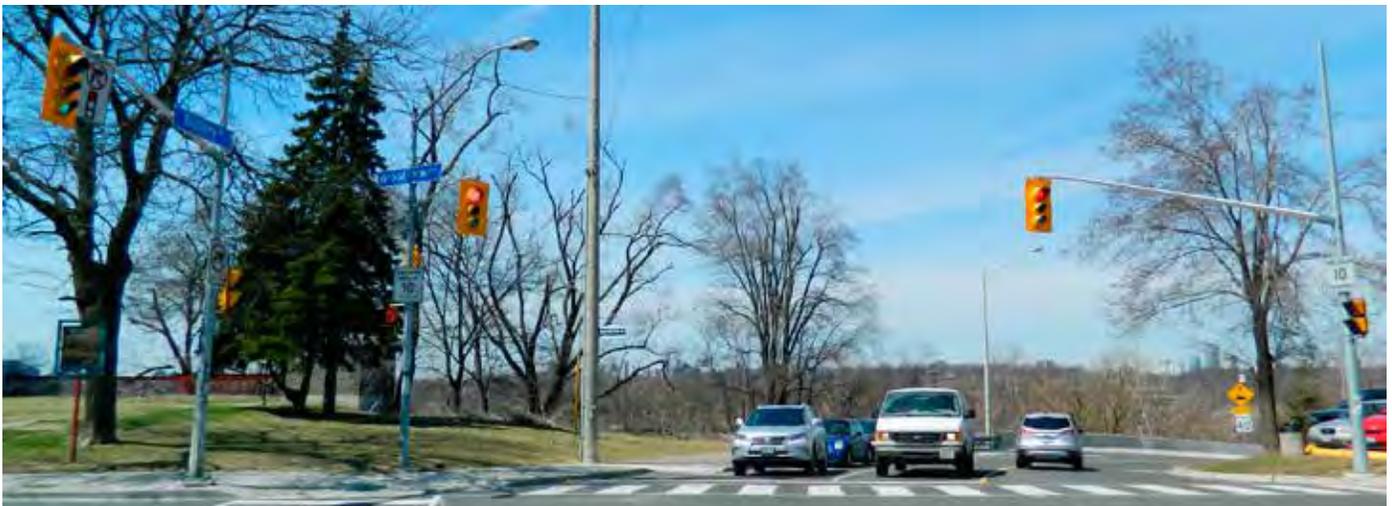
**View Protection:** Views towards the Don Valley are both a unique feature and asset of Character Area C. These views should be maintained for public access and enjoyment. Specifically, the view of Don Valley's greenery from the intersection of Broadview and Pottery Road; and the view of the city's skyline, the panorama of Don Valley and the silo of Todmorden Mills from existing viewpoints on corner properties of Broadview and Pottery Road, should be protected. New developments should reinforce the concept of a green entryway from Broadview by maintaining the visual and physical connections to the ravine from Broadview Avenue.

**Rear Transition:** The rear of the properties in Character Area C include or back onto TRCA-protected ravine lands. To ensure sufficient buffer to this green space, a minimum rear setback of 10 metres should be provided from the rear building face to the TRCA top-of-bank line.

This character area does not back onto *Neighbourhood* designated areas; therefore, a rear angular plane intended to provide transition to lower density areas is not required as long as the massing does not create any negative impact to the ravine, the protected views, or the integrity of the Don Valley's natural heritage.

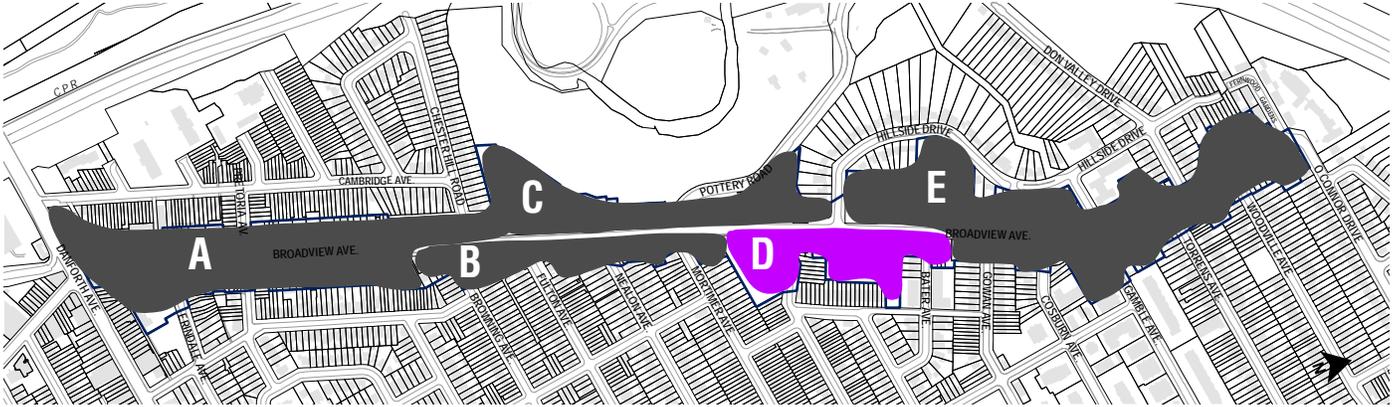


Cross section of a development that conforms to the built form guidelines in Character Area C. Angular planes and setbacks to be adjusted as needed to maintain views of the Don Valley



View of Don Valley from the intersection of Broadview and Pottery Road, taken from the opposite sidewalk

## Character Area D



East of Broadview Avenue, approximately from Mortimer Avenue to Bater Avenue

Character Area D is immediately north of Character Area B on the east side of Broadview Avenue up to Bater Avenue. This area comprises properties from 1015 to 1129 Broadview Avenue. The majority of the lots are generally bigger, wider and deeper, with existing large one-storey developments or 3-4 storey residential buildings. Buildings in this area back onto residential properties.

Due to the existing character, mix of uses, and lot sizes of Character Area D, the potential for intensification exists. Opportunities to add green spaces through on-site parkland dedication are also very favourable. Two sites are identified for on-site parkland dedication if a development application comes forward: 1015 Broadview Avenue and 1099 Broadview Avenue. This will result in two new parks of up to approximately 850 to 950 m<sup>2</sup>. The design and location of these new open spaces within their respective sites will be decided in conjunction with the community and other stakeholders during the development application approval process.

The massing of buildings on these two sites shall also be shaped in a way that provides gradual transition towards these open spaces and the *Neighbourhood* areas at the rear.

### Built Form Guidelines

New development in this Character Area will conform to the Mid-rise Guidelines performance standards, including minimum

setbacks and front and rear angular planes. Some properties may accommodate a slightly higher density due to the width and depth of the sites. However, the additional height can only be achieved provided that open spaces and the *Neighbourhood* areas at the rear are not impacted negatively. Additional guidance is provided further below.

**Maximum Building Height:** The maximum height of the buildings will be 20 metres or 6 storeys. This height should only be achieved if the required setbacks, stepbacks and angular planes are also complied with.

Mechanical penthouses, elevator runs, stair towers or other equipment or servicing may exceed the maximum height limit by up to 5 metres but may not penetrate any angular planes.

**Setback from the Street:** Wider sidewalk zones are needed in this area to complement the existing open feel created by the expansive front yard landscaping on the opposite side of the sidewalk and the Charles Sauriol Parkette. New developments will set back to provide a minimum dimension of 10 metres between the curb edge and the front building face. This distance will allow for a 2.1 metre pedestrian clearway and a deep front yard setback of approximately 8 metres for soft landscaping on the building's side. The specific setback from the property line

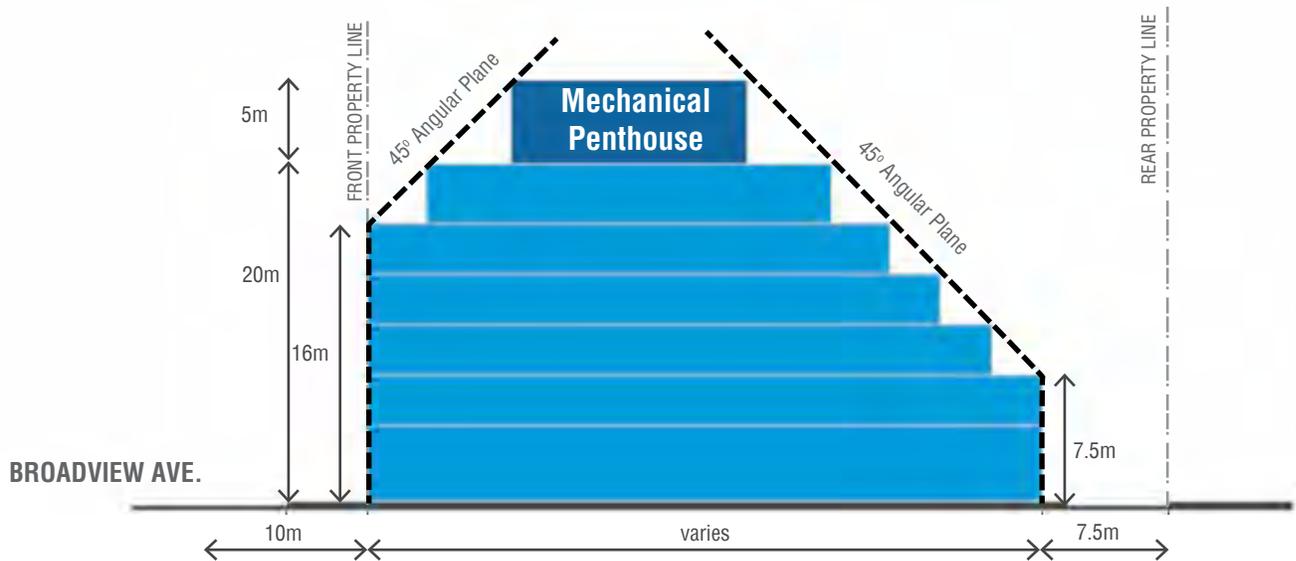
that will be required to achieve the minimum 10 metres varies throughout Broadview and will be determined on a site specific basis.

**Transition to Open Spaces:** The massing on the site should include appropriate transition in addition to a 7.5 metre setback from any open space areas or parks.

If low-rise development is proposed as part of a built form transition between the mid-rise building and open space areas, the low-rise portions of the development should be oriented towards the open space and set back a minimum of 3 metres from the open space. Having frontage on the open space will add safety and visibility onto the space. From there, a 45 degrees

angular plane should be taken above the third floor of the low-rise development (approximately 10.5 metres).

**Height Above 20 metres or 6 storeys:** Sites over 5,000m<sup>2</sup> may support additional height in the range of 7-8 storeys; provided privacy, overlook, shadows, wind conditions and transportation impacts are adequately addressed. Height above 6 storeys would need to step back sufficiently from Broadview Avenue to minimize the visual impact at street level. 3-storey townhouses should also be used to create a gradual transition from open spaces and *Neighbourhoods* towards the higher mid-rise building. This increase in density can only be achieved through an Official Plan Amendment process.



Cross section of a development that conforms to the built form guidelines in Character Area D.



Rear view of new open space showing townhouses transitioning to a mid-rise building



Bird eye view of new open space located at the rear of development

## Character Area E



View of Broadview Avenue to the North from Bater Avenue



View of Broadview Avenue to the South from O'Connor Drive

Character Area E is the area north of Hillside Drive and Bater Avenue up to O'Connor Drive. This area comprises the following properties: 1102 to 1276 Broadview Avenue, 1135 to 1277 Broadview Avenue, 87 Don Valley Drive, 1 Fernwood Gardens, and 2 Gamble Avenue. This area can be characterized as stable residential, with single detached houses being the main built form. This area is a designated *Neighbourhoods* under Toronto's Official Plan. Current zoning only allows residential uses with a maximum height limit of 8.5 metres (approximately 2-3 storeys). A small section within the area, between Gowan Avenue and few lots north Gamble Avenue, is zoned as *Residential Apartment Neighbourhood*. A higher density and height is allowed in this section, but existing lot sizes generally do not support such height and density.

Neither the Avenue and Mid-rise Study nor the Official Plan identify this area as an area for intensification. The majority of the lots are also small lots that cannot accommodate significant growth.

### Built Form Guidelines

As this is an area where major intensification is not anticipated, any new development will respect and reinforce the existing physical character in the form of single-detached house of 2 to 3 storeys in height.

Deep front yard soft landscaping is also recommended to complement the existing context. Section 4.2.2 of this document (Public Realm) speaks to the setbacks and streetscape improvements required to reinforce this existing character.

### 4.3.3 DESIGN EXCELLENCE & SUSTAINABILITY

New development should be compatible with the existing character of the Study Area, as well as achieving design excellence. Design excellence means that buildings will be designed with architectural details and materials that enhance the public realm and fit harmoniously in the existing and planned context. New developments should also achieve a balance of unit types, including larger units for families.

As well as achieving design excellence, new development should be modern, energy efficient and sustainable. All applications will be reviewed for their conformity to the Toronto Green Standards, a two-tiered set of performance measures with supporting guidelines related to sustainable site and building design for new public and private developments. The standards are designed to work with the regular development approvals and inspections process. All new planning applications are required to document compliance with Tier 1 environmental performance measures outlined in the Toronto Green Standards. Applicants who also choose to meet Tier 2, a voluntary higher level of environmental performance, may be eligible for reduction in development charges.

### 4.3.4 WOOD FRAME CONSTRUCTION

On January 1, 2015, the Ontario Building Code was revised to allow wood-frame construction for residential and office buildings up to 6 storeys. This revision is relevant to this Study Area (and other locations in the City that allow buildings up to 6 storeys), because it presents an opportunity for the development industry to build 6 storey buildings with a construction method that was previously limited to low-rise buildings. Wood frame construction is also often less expensive to build than other construction methods and may therefore be of interest to developers.

Consideration for a nominal increase in height (above the 20 metre maximum), will be given to proposed buildings to be constructed with this method, because there is anecdotal evidence that the depth of the structural frame is deeper than other construction methods and may directly influence the overall height of a 6 storey building (up to 20.75 - 21.50 metres). The increase above the 20 metre height limit will only be considered to allow for buildings of wood frame construction. Shadow studies will be required to demonstrate that the additional height does not create negative impacts on surrounding *Neighbourhoods* and public streets.

It is important to reiterate that wood frame buildings above 6 storeys would not be permitted by the Ontario Building Code.

## 4.4 Heritage

Broadview Avenue is rich in history, which is still legible in its built form including the natural topography, street layout and historic buildings. As the area continues to evolve in the future there is an opportunity to enhance its sense of place by strengthening the relationship with the area's heritage. There is opportunity to build greater awareness, commemorate the area's heritage and integrate the area's rich history into the design of new construction and streetscape improvements.

Based on the heritage analysis conducted as part of the Broadview Avenue Planning Study (see Appendix G for a detailed historic analysis of the evolution of the Study Area), urban design guidelines and the addition of individual properties to the City's Heritage Register was determined to be the most appropriate policy framework for heritage conservation in the Study Area. The heritage analysis has also provided a foundation for the Vision

of Broadview and has informed the recommendations for public realm and urban design guidelines contained in this document. The following recommendations are intended to further ensure that new development, as envisioned in this document, will respect the heritage context of the Study Area.

The objectives of these recommendations are to:

- Strengthen the relationship with the area's history and reinforce a sense of place;
- Strengthen the Study Area's relationship with its natural heritage;
- Conserve remaining heritage properties associated with Todmorden Village and Chester Village; and
- Ensure that future development responds to the area's historic context.



Sisters of St. Joseph building at 2 O'Connor Drive, included on the Heritage Register in 2006



Massey Centre for Women at 1102 Broadview Avenue, included on the Heritage Register in 2006

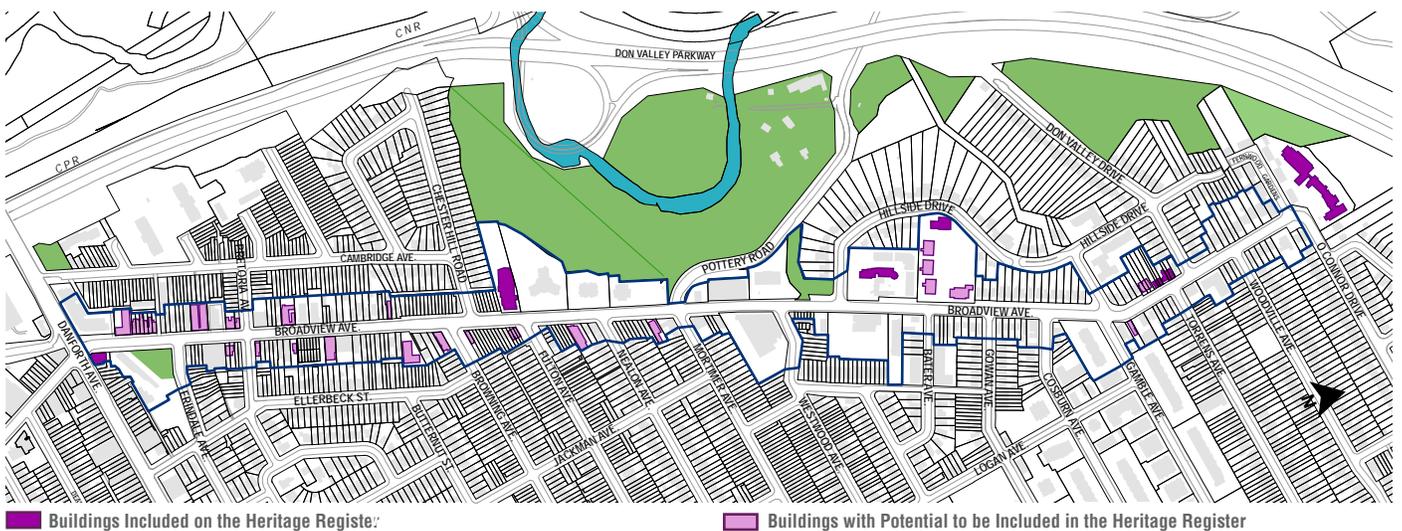
#### 4.4.1 OFFICIAL PLAN HERITAGE POLICIES

Heritage conservation policies within the Official Plan (Section 3.1.5) remain applicable in the Broadview Avenue Planning Study Area, with respect to properties that are included on the Heritage Register and the protection of areas of archaeological potential. As stated in the Official Plan's heritage conservation policies, the City may request a Heritage Impact Assessment for development proposals on any property that is included on, or is adjacent to a property included on the City's Heritage Register. A Heritage Impact Assessment is required for the proposed demolition of a property on the Heritage Register, and/or properties adjacent to a property on the Heritage Register. Archaeological Assessments are required prior to any soil disturbance activity in areas identified as having archaeological potential in the City's Archaeological Management Plan. See Appendix E for a map of Areas with Archaeological Potential in the Study Area.

#### 4.4.2 HERITAGE RECOMMENDATIONS

The further evaluation of remaining buildings associated with the historic Chester and Todmorden Villages for inclusion on the City of Toronto Heritage Register should be initiated by the City Planning Division. See Appendix F for the full list of recommended buildings and an enlarged map identifying these buildings.

Within the context of streetscape and public realm improvement initiatives or through the review of development applications, explore interpretation opportunities of the Study Area's history in collaboration with appropriate City divisions and agencies and in consultation with First Nations and Métis.



# 4.5 Transportation

## 4.5.1 TRANSPORTATION IMPACT

The changes in built form, scale, and intensity of development anticipated by this Study will have a minimal influence on all modes of travel in the area. The land use and transportation relationship as well as the findings are documented in this section. The Study also looked beyond this scope to address a range of short, medium and long-term issues, some of which arose during the course of the Study.

Based on the vision and recommendations for the built form, it is estimated that less than 100 net new automobile trips (A.M. peak hour) could be added to the area as a result of the Study's direction. These numbers are based on approximately 500 additional residential units related to the vision. These less than 100 trips would be spread over a distance of approximately one kilometre, and may gradually be built out over the next 10 to 20 years. This amount of traffic does not create a significant level of change to the Study Area.

The Study considered historical and current trends; automobile traffic on Broadview Avenue has been relatively stable for the past decade. Broadview Avenue is also typical of many main streets in Toronto which experience peak conditions during certain times of the day.

## 4.5.2 TRANSPORTATION IMPROVEMENTS

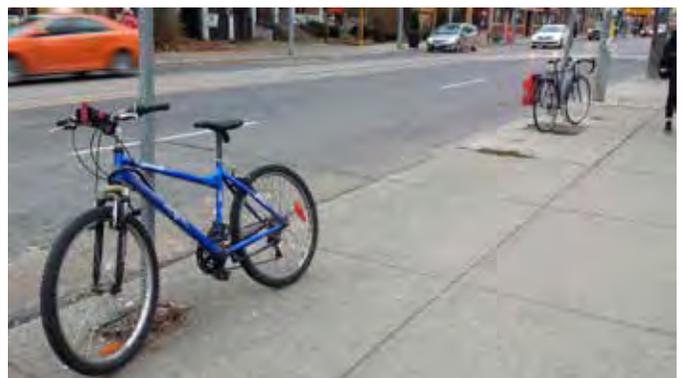
Transportation strategies that address all modes of mobility and congestion is a key issue that was identified by the community and the public. The City is continually investigating ways to address congestion through curbside management of illegal parking and loading activity, time-based strategies to better organize travel (eg., turn restrictions and parking restrictions), improvements to traffic signal timing to accommodate peak flows and transit vehicle priority, and physical and regulatory changes to improve safety for all road users. These are all tools aimed at improving the efficiency of the existing road network given the right-of-way conditions.



Broadview TTC subway station



Vehicular parking spaces at the rear of existing buildings with access from Broadview



Informal bike parking spaces along Broadview Avenue

It is recommended that a review be undertaken at Broadview and Chester Hill to determine the feasibility of introducing a pedestrian crossing on the north leg of the intersection. This will improve the wait time for pedestrians and improve the access to the TTC bus stop on the northeast corner, but will impact the eastbound flow of traffic to northbound Broadview Avenue.

Various cycling improvements have been identified during the Broadview Avenue Study, including on-street bicycle lanes, sharrows, separated bikeways within the street right-of-way and connections to adjacent streets such as Chester Hill Road. These potential improvements must all be considered within the broad network scope of the Cycling Plan.

Broadview is not scheduled for complete road reconstruction in the near future, but the study team presented a tool box of potential redesign including options for a mix of bus lanes, bike lanes, layby parking, on-street parking options, and sidewalk/ boulevard widening.

This toolbox of options is based on the core principle of the City's on-going work to develop a Complete Streets Guideline, which is to balance the essential needs of all road users, and includes potential directions 1, 2 and 3, ranging in short to long-term opportunities. These directions are general transportation ideas that, with consensus, can be implemented on Broadview when the opportunity is presented.

Additional studies may be required, however, in the case of a redesign. The Municipal Class Environmental Assessment takes effect when municipalities want to plan, design, construct, maintain, rehabilitate, and/or retire a municipal road. The Complete Streets Guidelines will inform any major redesign of Broadview.

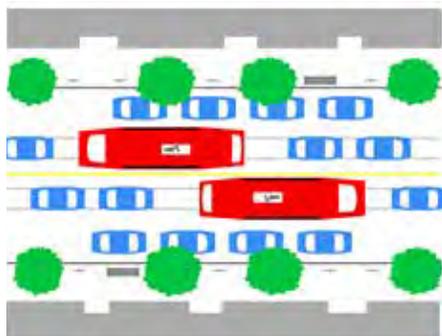
Potential Direction 1 maintains existing right-of-way and traffic configuration but proposes design and public realm improvements including pedestrian lighting and increase bicycling parking, and on-street off-peak parking may be considered.

Potential Direction 2 includes proposed options in direction 1, in addition to traffic configuration and operational changes through the use of signage or road paint while maintaining the existing curb of the sidewalk.

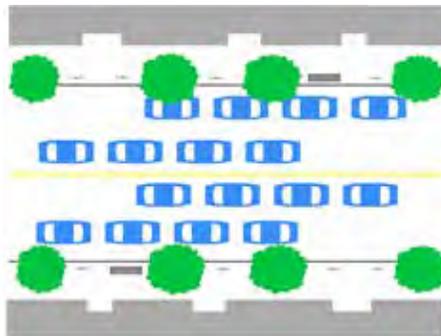
Similarly, Potential Direction 3 includes proposed options in Potential Direction 1, in addition to changes to sidewalk width and traffic configurations requiring major reconstruction of infrastructure.

Broadview Avenue is challenged, like many other streets, by the need to accommodate pedestrians, public realm space, cyclists, transit use and automobiles. The tool box of options is useful as a starting point for future discussions when Broadview Avenue is considered for reconstruction.

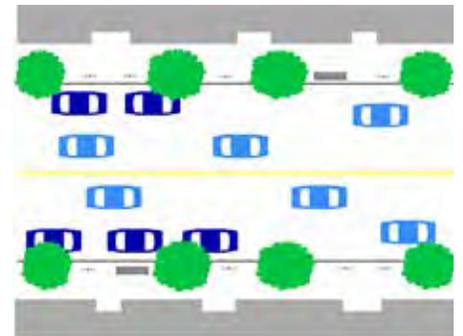
### Potential Direction 1



Streetcar Service - Mixed Traffic



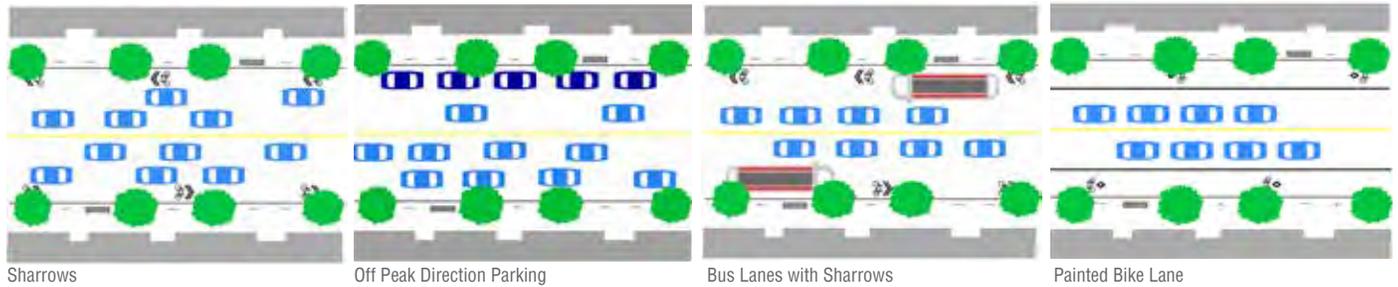
Two Lanes



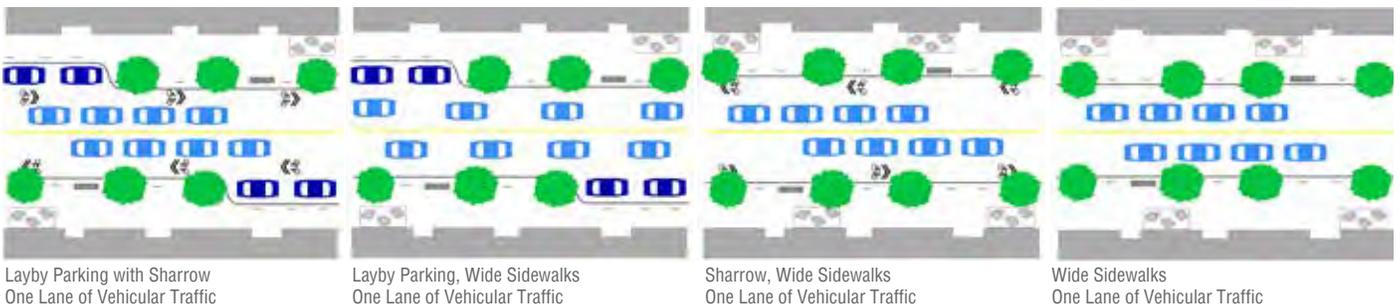
Off Peak Parking

## 4.5.2 TRANSPORTATION IMPROVEMENTS (cont'd)

### Potential Direction 2



### Potential Direction 3



## 4.5.3 TRANSIT

City staff are working jointly with the TTC on a project assessment study to determine a preferred alignment for a subway relief line connecting Downtown Toronto and the Danforth Subway east of the Don River. The transit team has studied a number of corridor connections to Line 2, and has proposed a recommended preferred corridor connecting Pape Station to the Downtown via Queen Street. Broadview Station is an important subway station and streetcar connection, and Broadview Avenue north of Danforth will continue to be an important bus corridor connection to the subway station. This will be a key consideration in any future plans for the redesign of the street.

## 4.5.4 PARKING

There are several types of parking in the Study Area serving the various land uses. In Character Area A (and parts of Character Area B and C) land use is mixed commercial-retail, and the on-street parking along Broadview Avenue in this area provides short term off-peak parking to those visiting its local restaurants, offices, and retail shops. Located immediately outside Character Area A, there is a Green P surface public parking lot containing 93 parking spaces at the corner of Erindale Avenue and Ellerbeck Street. Character Area B mainly consists of single family dwellings, with some properties having front-yard parking. Character Area C is predominantly multi-story residential buildings with parking available on-site to its residents and their visitors. Finally, Character Area D has a mix of residential and retail-commercial use, with some private parking and a few commercial properties providing customer parking on-site.

As redevelopment occurs along Broadview Avenue over time, demands for convenient public parking may increase to accommodate the increasing number of businesses, medical offices and retail spaces to the area. Partnerships with the Toronto Parking Authority (TPA) are encouraged to support integrating public parking in new buildings where needed.

The TPA has conducted studies which indicate that the Green P lot is well utilized. As demand for public parking increases, it is important to TPA that the current public parking supply is maintained, monitored and controlled such that the needs of short-term visitor parking are addressed.

Both on-street and off-street public parking supply and usage impact each other. In conjunction with any future street improvements and/or development applications, opportunities to improve public parking strategically (both on- and off-street) are necessary considerations as options are reviewed.

Existing surface lot owners are encouraged to apply the strategies and measures detailed in the Design Guidelines for "Greening Surface Parking Lots" to improve the design and reduce environmental impacts.

Opportunities for shared parking between various parking users are supported, with the appropriate mechanisms in place to facilitate each user.

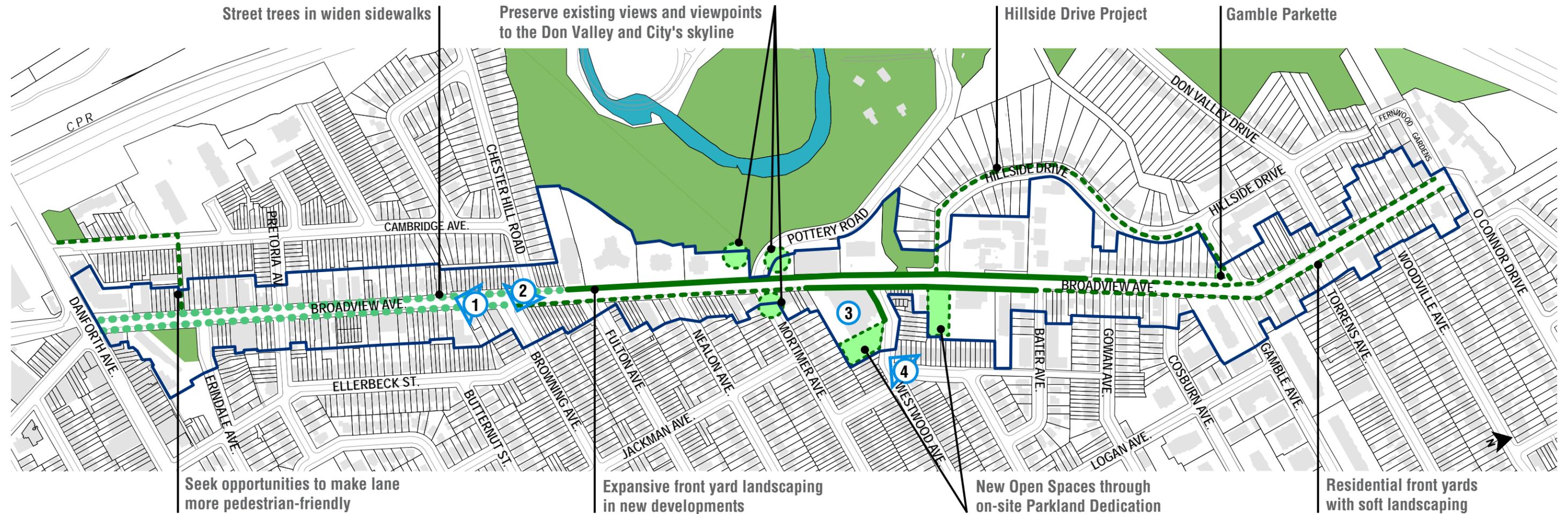
The transportation challenges faced by Broadview Avenue demonstrate the need to better accommodate pedestrians, cyclists, transit use and automobiles in the public realm space. Though there has been growth along Broadview, automobile use has remained stable over the past decade. As people use other modes of transportation, including walking, cycling and public transit, improved infrastructure should be in place to create a safer mobility environment and allow opportunities for a variety of transportation choices for this community.



# Appendices

# Appendix A

## SUMMARY OF PUBLIC REALM RECOMMENDATIONS



Street trees in widen sidewalks



Patios and seating areas on key corners as places to meet and socialize



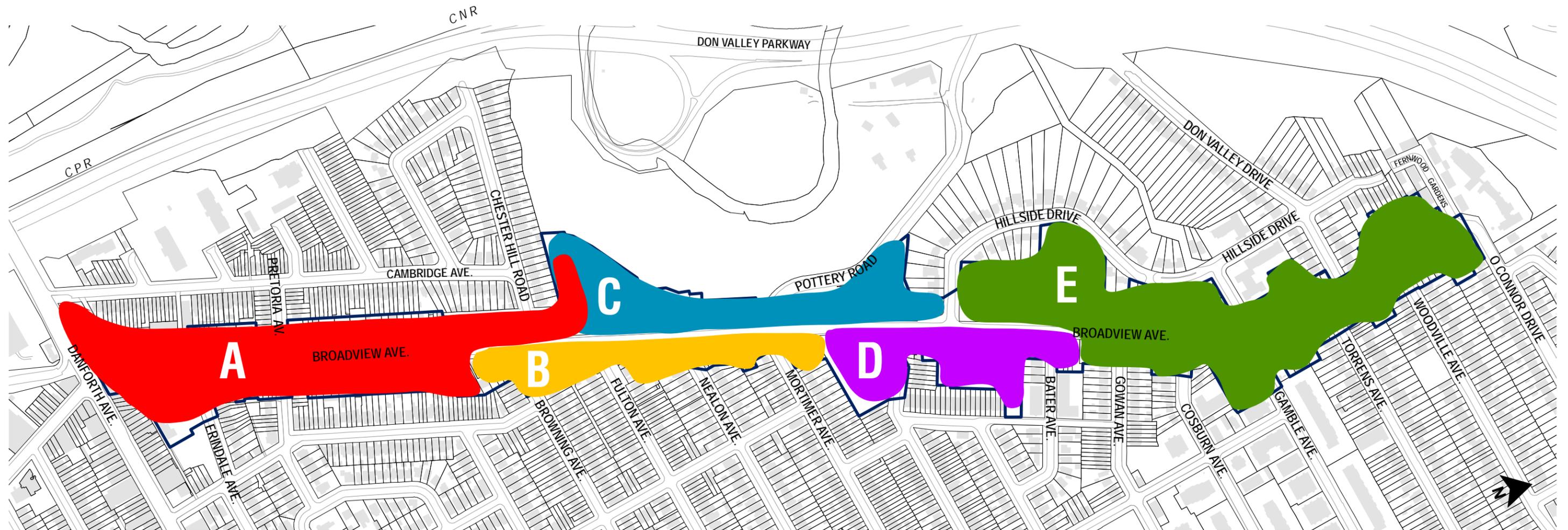
On-site parkland dedication on new development



View of new public park from the rear residential street

# Appendix B

## SUMMARY OF BUILT FORM RECOMMENDATIONS



### Character Area A

6 storey (20m)  
Mid-rise building

### Character Area B

5 storey (17m)  
Modified Mid-rise of  
5-storeys at corners

### Character Area C

6 storey (20m)  
Mid-rise building with  
appropriate setbacks to  
TRCA ravine area

### Character Area D

6 storey (20m)  
Mid-rise building  
Sites over 5,000m<sup>2</sup> may  
support additional density  
in the range of 7- 8 storeys;  
provided conditions specified  
in the Built Form guidelines  
are adequately addressed.

### Character Area E

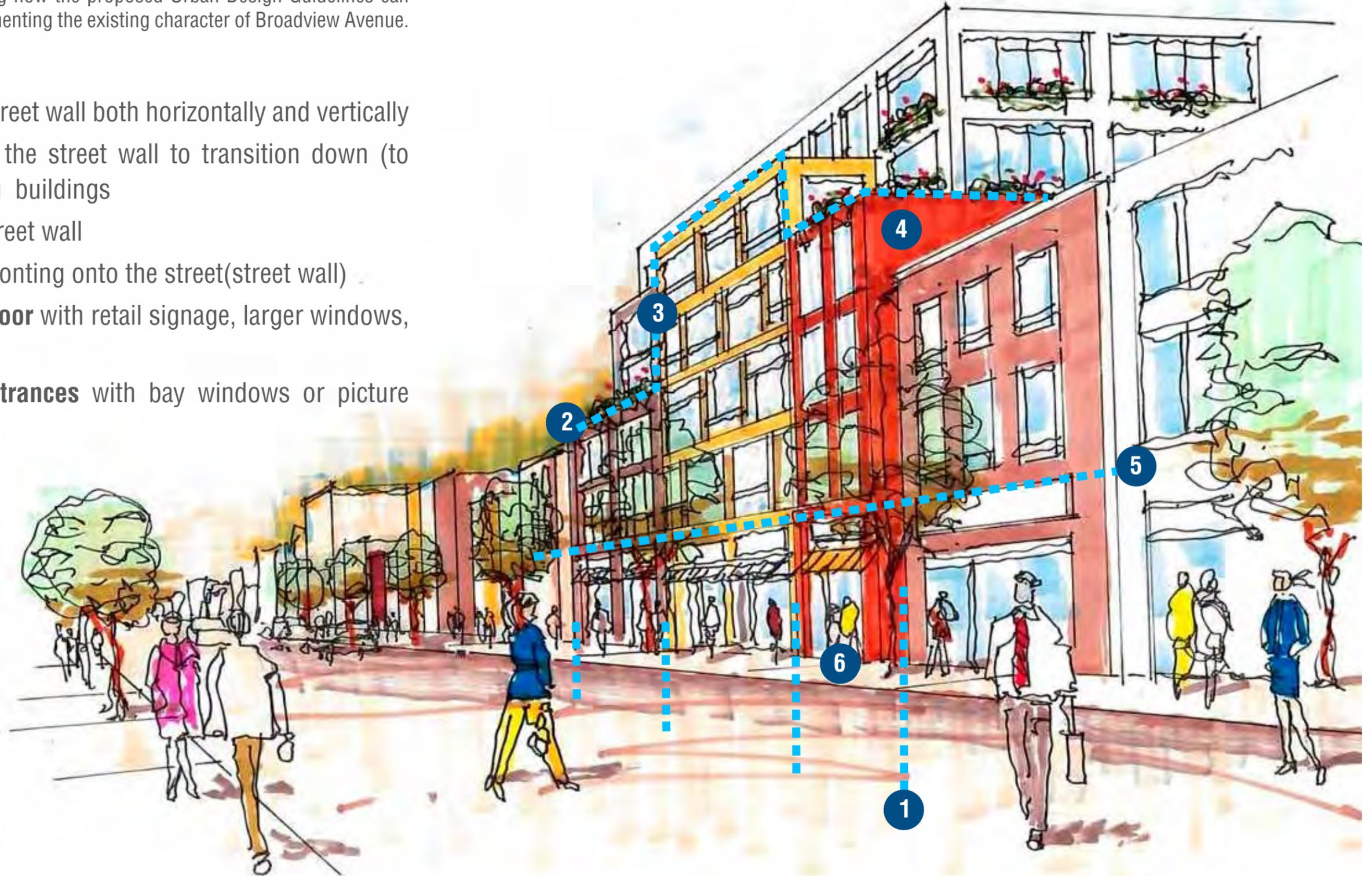
No changes to existing  
built form character

# Appendix C

## DEMONSTRATION OF THE DESIGN GUIDELINES IN CHARACTER AREA "A"

Below is an illustration demonstrating how the proposed Urban Design Guidelines can guide a new development by complementing the existing character of Broadview Avenue.

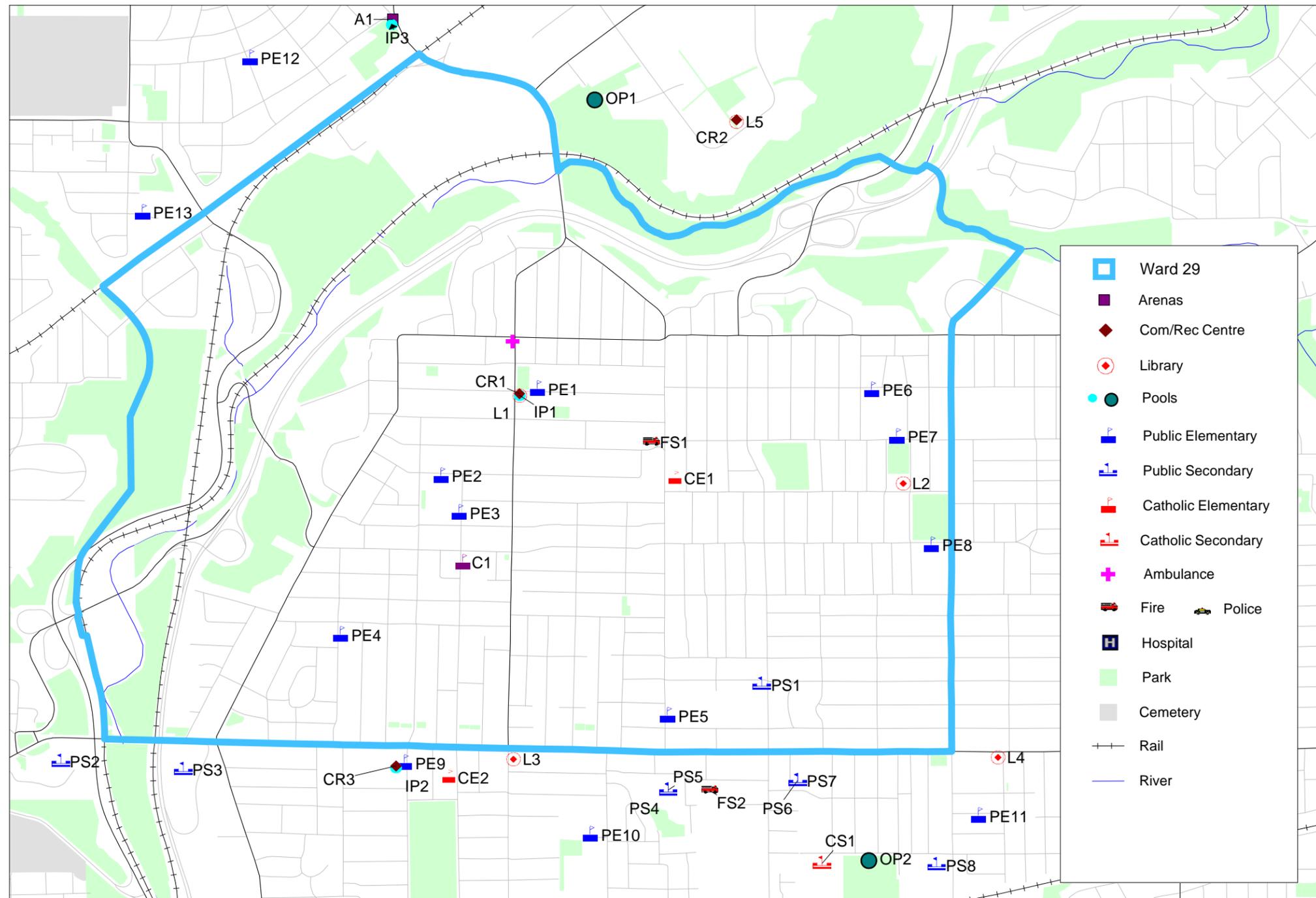
- 1 **Break up** the buildings street wall both horizontally and vertically
- 2 **Transition the scale** of the street wall to transition down (to blend) with neighbouring buildings
- 3 **Vary the height** of the street wall
- 4 Use of **brick materials** fronting onto the street (street wall)
- 5 **Emphasize the ground floor** with retail signage, larger windows, canopies, and awnings
- 6 **Recessed storefront entrances** with bay windows or picture windows



# Appendix D

## COMMUNITY SERVICES AND FACILITIES MAPS

### COMMUNITY SERVICES AND FACILITIES - Ward 29 and Surrounding Areas



Ward 29 Arenas			
MapID	NAME	ADDRESS	LOCATION
A1	LEASIDE GARDENS ARENA	1073 MILLWOOD AVE	Leaside Gardens Property
Ward 29 Community Centres			
MapID	NAME	ADDRESS	
CR1	EAST YORK CC	1081 PAPE AVE	
CR2	JENNER JEAN-MARIE CC	48 THORNCLIFFE PARK DR	
CR3	FRANKLAND CC	816 LOGAN AV	
Ward 29 Outdoor Pools			
MapID	NAME	ADDRESS	LOCATION
OP1	Leaside Park Outdoor Pool	5 Leaside Park Dr	Leaside Park
OP2	Monarch Park Outdoor Pool	115 Felstead Ave	Monarch Park
Ward 29 Indoor Pools			
MapID	NAME	BUILDING NAME	ADDRESS
IP1	East York Indoor Pool	East York Community Centre	1081 1/5 Pape Ave
IP2	Frankland Indoor Pool	Frankland Community Centre	816 Logan Ave
IP3	Leaside Memorial Indoor Pool	Leaside Memorial Indoor Pool	1073 Millwood Rd
Ward 29 Libraries			
MapID	NAME	ADDRESS	
L1	Tommoden Room	1081 Pape Ave	
L2	St. Walter Stewart	170 Memorial Park Ave	
L3	Pape Danforth	701 Pape Ave	
L4	Danforth Corwell	1675 Danforth Ave	
Ward 29 Fire Stations			
MapID	Station	ADDRESS	
FS1	322	296 Cosburn Ave	
FS2	323	153 Chatham Ave	
Ward 29 Ambulance Stations			
MapID	NAME	ADDRESS	
A51	Station #41	1300 Pape Ave	

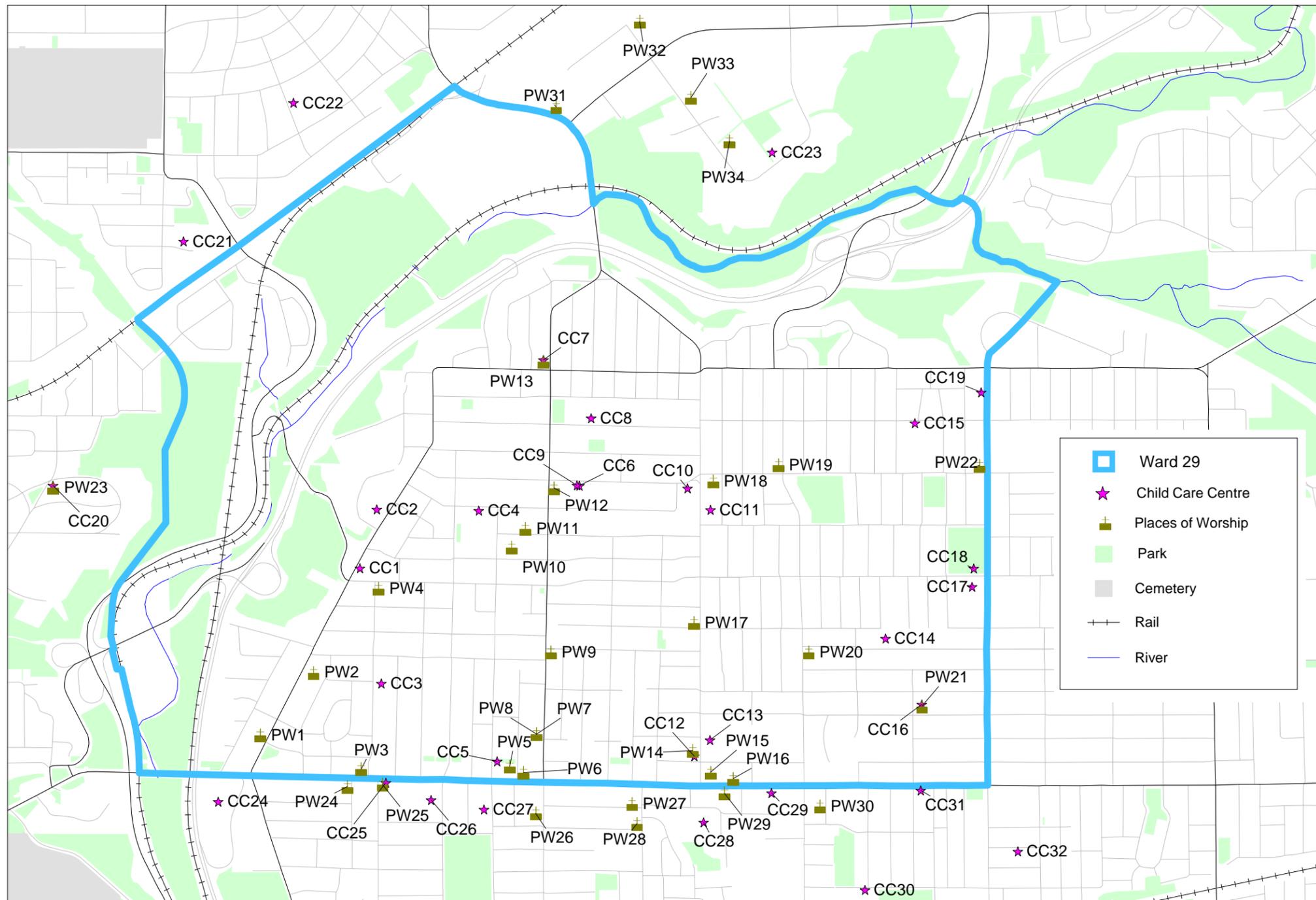
Ward 29 Colleges		
MapID	NAME	ADDRESS
C1	Centennial College	951 Carlaw Ave
Ward 29 Public Elementary Schools		
MapID	NAME	ADDRESS
PE1	William Burgess ES	100 Torrens Avenue
PE2	Chester ES	115 Gowan Avenue
PE3	Westwood Middle School	994 Carlaw Avenue
PE4	Jackman Avenue Jr PS	79 Jackman Avenue
PE5	Wilkinson Jr PS	53 Donlands Avenue
PE6	Diefenbaker ES	175 Plains Road
PE7	Cosburn MS	520 Cosburn Avenue
PE8	R H McGregor ES	555 Mortimer Avenue
PE9	Frankland Community JS & Community Centre	816 Logan Avenue
PE10	Earl Grey Sr PS	100 Strathcona Avenue
PE11	Earl Haig Jr PS	15 Earl Haig Avenue
PE12	Rolph Road ES	31 Rolph Road
PE13	Bennington Heights ES	76 Bennington Heights Drive
Ward 29 Public Secondary Schools		
MapID	NAME	ADDRESS
PS1	Danforth Collegiate & Technical Institute	840 Greenwood Avenue
PS2	Rosedale Heights SS	711 Bloor Street East
PS3	CALC Secondary	1 Danforth Ave
PS4	Subway Academy I / Eastern HS of Commerce	16 Phin Avenue
PS5	Eastern HS of Commerce / Subway Academy I	16 Phin Avenue
PS6	School of Life Experience (SOLE) / Greenwood SS	24 Mountjoy Avenue
PS7	Greenwood SS / School of Life Experience (SOLE)	24 Mountjoy Avenue
PS8	Monarch Park Collegiate	1 Hanson Street
Ward 29 Catholic Elementary Schools		
MapID	NAME	ADDRESS
CE1	Holy Cross	299A Donlands Ave
CE2	Holy Name	690 Carlaw Ave
Ward 29 Catholic Secondary Schools		
MapID	NAME	ADDRESS
CS1	St. Patrick's Secondary	45-49 Felstead Ave

Sources: TDSB, TCDSB, City of Toronto

Toronto City Planning Division, Research and Information - July 2015

# COMMUNITY SERVICES AND FACILITIES MAPS

## CHILDCARE CENTRES AND PLACES OF WORSHIP - Ward 29 and Surrounding Areas



MapID	NAME	ADDRESS	SCHOOL
CC1	Broad View French Montessori School	1007 BROADVIEW AVE	SCHOOL
CC2	Massey Centre Early Learning Centre	1102 BROADVIEW AVE	
CC3	Jacobsen Community Day Care	79 JACKMAN AVE	JACKMAN AVENUE
CC4	Chester Jr Ymca	115 GODWIN AVE	CHESTER
CC5	Our Happiness Day Nursery	901 CARLAW AVE	
CC6	Network Preschool - Bimminy	123 COSBURN AVE	
CC7	Petrie Mason Montessori	125 O CONNOR DR	
CC8	William Burgess Jr Ymca	100 TORRENS AVE	WILLIAM BURGESS
CC9	Child's Nest Infant Day Care Centre	121 COSBURN AVE	
CC10	Debbie Young Child Care Centre (Woodgrove)	249 COSBURN AVE	
CC11	Network Children Services - Holy Cross	299A DONLANDS AVE	HOLY CROSS
CC12	Melamorphosis Child Care Centre (Network)	48 DONLANDS AVE	
CC13	Diancyllon Childcare Centre	53 DONLANDS AVE	WILKINSON
CC14	Le Petit Chaperon Rouge - Queenston	80 QUEENSDALE AVE	LA MOSAÏQUE
CC15	Cannanap Cottage East York Day Care Centre (St)	175 FLAINS RD	DIFFENBAKER
CC16	Monarch Park Montessori	201 MONARCH PARK AVE	
CC17	McGregor Jr Ymca	555 MORTIMER AVE	RH MCGREGOR
CC18	Censusnaid Goleage Early Childhood Education Cent	503 MORTIMER AVE	RH MCGREGOR
CC19	Lily's House Day Care Centre	1032 COXWELL AVE	
CC20	Grassale Academy	159 ROXBOROUGH DR	
CC21	Brimington Heights Child Care Centre	70 BRIMINGTON HEIGHTS DR	BRIMINGTON HEIGHTS
CC22	Reign Road Day Care Centre	31 ROLPH RD	ROLPH ROAD
CC23	Thorncliffe Park Day Care Centre	48 THORNCLIFFE PARK DR	
CC24	Pat Schulz Child Care Centre	1 DANFORTH AVE	
CC25	Children's Circle Of St. Barnabas	361 DANFORTH AVE	
CC26	Frankland Day Care Centre	816 LOGAN AVE	FRANKLAND
CC27	Childspace 2	690 CARLAW AVE	HOLY NAME
CC28	Creative Preschool Of East Toronto	19 PHIN AVE	
CC29	Danforth Early Learning & Child Care Centre	1125 DANFORTH AVE	
CC30	Childspace 3	49 FELSTEAD AVE	
CC31	It's Playtime Childcare Centre	1423 DANFORTH AVE	EARL HAIG
CC32	Earl Haig Community Day Care	15 EARL HAIG AVE	EARL HAIG

MapID	NAME	ADDRESS	BUILDING NAME
PW1	IGLESIA EVANGELICA HISPANA	804 BROADVIEW AVE	IGLESIA EVANGELICA HISPANA
PW2	ESTONIAN BAPTIST CHURCH	883 BROADVIEW AVE	ESTONIAN BAPTIST CHURCH
PW3	EAST MINISTER UNITED CHURCH	310 DANFORTH AVE	EASTMINSTER CHURCH
PW4	TORONTO JAPANESE SEVENTH DAY	19 MORTIMER AVE	SEVENTH DAY ADVENTIST CHURCH
PW5	ST IRENE CHRISTOQUALANTOU GRE	86 GOUGH AVE	ST. IRENE CHRISTOQUALANTOU GREEK ORTHODOX
PW6	HOLY NAME CHURCH	808 DANFORTH AVE	HOLY NAME CHURCH
PW7	ETHIOPIAN EVANGELICAL CHURCH	740 PAPE AVE	CALVARY CHURCH
PW8	CALVARY CHURCH TORONTO	740 PAPE AVE	CALVARY CHURCH
PW9	GREEK GOSPEL CHURCH	871 PAPE AVE	PAPE AVE GOSPEL HALL
PW10	SEVENTH DAY ADVENTIST CHURCH	179 WESTWOOD AVE	TORONTO EAST 7TH DAY ADVENTIST CHURCH
PW11	WESTMINSTER PRESBYTERIAN CHURCH	152 FLOYD AVE	WESTMINSTER PRESBYTERIAN CHURCH
PW12	BETHANY BAPTIST CHURCH	1041 PAPE AVE	
PW13	DON MILLS UNITED CHURCH	126 O'CONNOR DR	DON MILLS UNITED CHURCH
PW14	METAMORPHOSIS CHURCH	42 DONLANDS AVE	METAMORPHOSIS CHURCH
PW15	ST DAVIDS ANGLICAN CHURCH	49 DONLANDS AVE	ST. DAVIDS ANGLICAN CHURCH
PW16	DANFORTH COMMUNITY CHURCH	1080 DANFORTH AVE	THE DANFORTH GOSPEL TEMPLE
PW17	JEHOVAH'S WITNESSES	172 DONLANDS AVE	KINGDOM HALL
PW18	CHURCH OF THE HOLY CROSS	291 COSBURN AVE	HOLY CROSS CHURCH
PW19	COSBURN UNITED CHURCH	324 COSBURN AVE	COSBURN UNITED CHURCH
PW20	GREENWOOD GOSPEL CHAPEL	949 GREENWOOD AVE	GREENWOOD GOSPEL CHAPEL
PW21	HOLY TRINITY	201 MONARCH PARK AVE	HOLY TRINITY
PW22	ST LUKE'S CHURCH	904 COXWELL AVE	ST LUKE'S CHURCH
PW23	ROSEDALE UNITED CHURCH	158 ROXBOROUGH DR	ROSEDALE UNITED CHURCH
PW24	DANFORTH BAPTIST CHURCH	285 DANFORTH AVE	DANFORTH BAPTIST CHURCH
PW25	ST BARNABAS ANGLICAN CHURCH	399 DANFORTH AVE	ST. BARNABAS ANGLICAN CHURCH
PW26	RIVERDALE PRESBYTERIAN CHURCH	902 PAPE AVE	
PW27	NEW APOSTOLIC CHURCH	152 HAZELWOOD AVE	NEW APOSTOLIC CHURCH
PW28	THE HOLY TRINITY CHRISTIAN MINISTRIES INC	692 JONES AVE	
PW29	JAMATUL MUSLEMIN OF TORONTO	1015 DANFORTH AVE	MADINA MASJID
PW30	MASJID UT TAZWA	14 LADYSMITH AVE	
PW31	MASONIC TEMPLE LTD	1108 MILLWOOD RD	
PW32	MASJID DARUS SALAM	4 THORNCLIFFE PARK DR	
PW33	THORNCLIFFE PARK UNITED CHURCH	18 THORNCLIFFE PARK DR	CHAPEL IN THE PARK
PW34	POLYMENAKION CULTURAL CENTRE	30 THORNCLIFFE PARK DR	GREEK COMMUNITY CENTRE

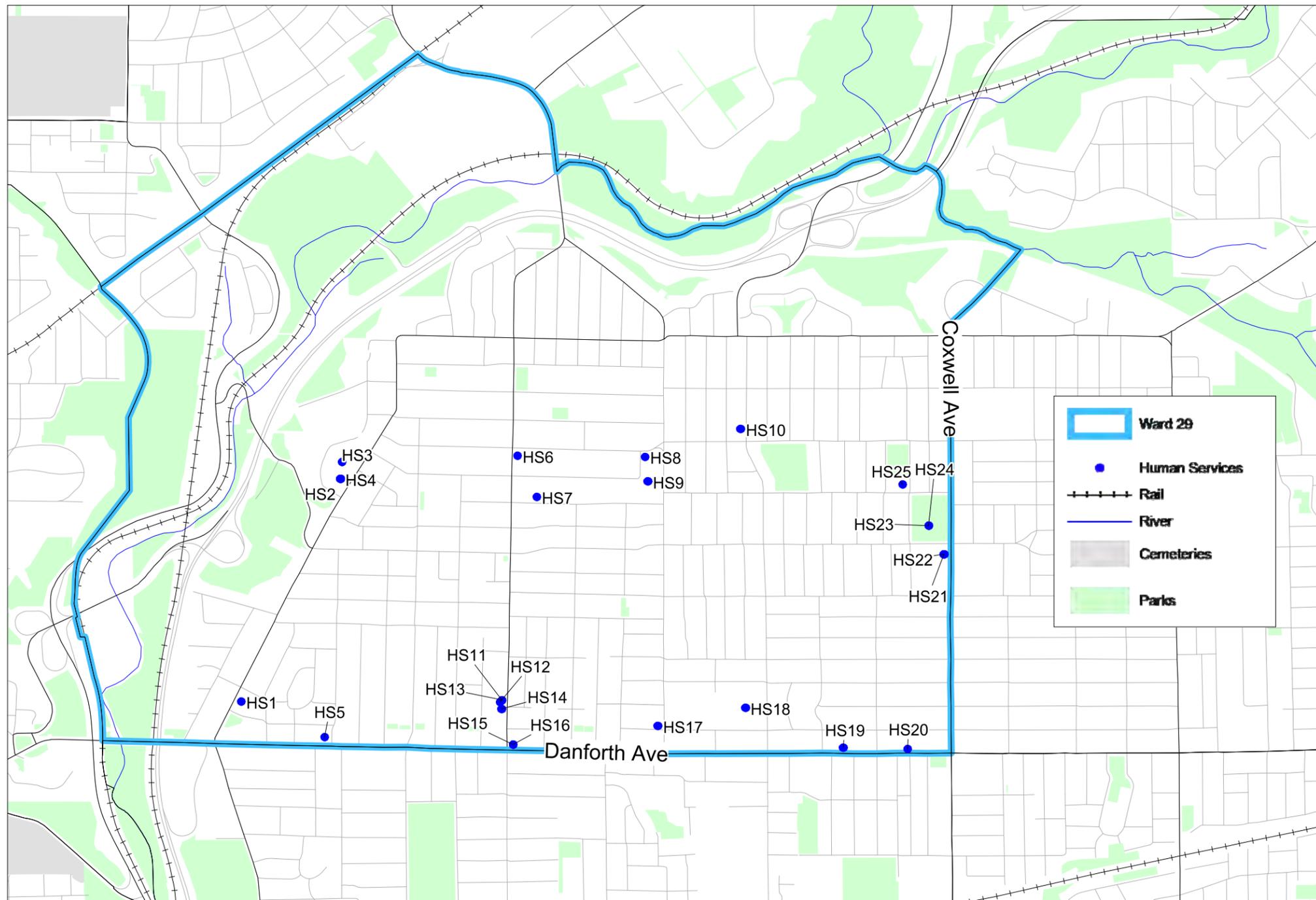
Sources: TDSB, CDSB, City of Toronto - Social Development, Finance & Administration

Toronto City Planning Division, Research and Information - July 2015



# COMMUNITY SERVICES AND FACILITIES MAPS

## HUMAN SERVICES - Ward 29 and Surrounding Areas

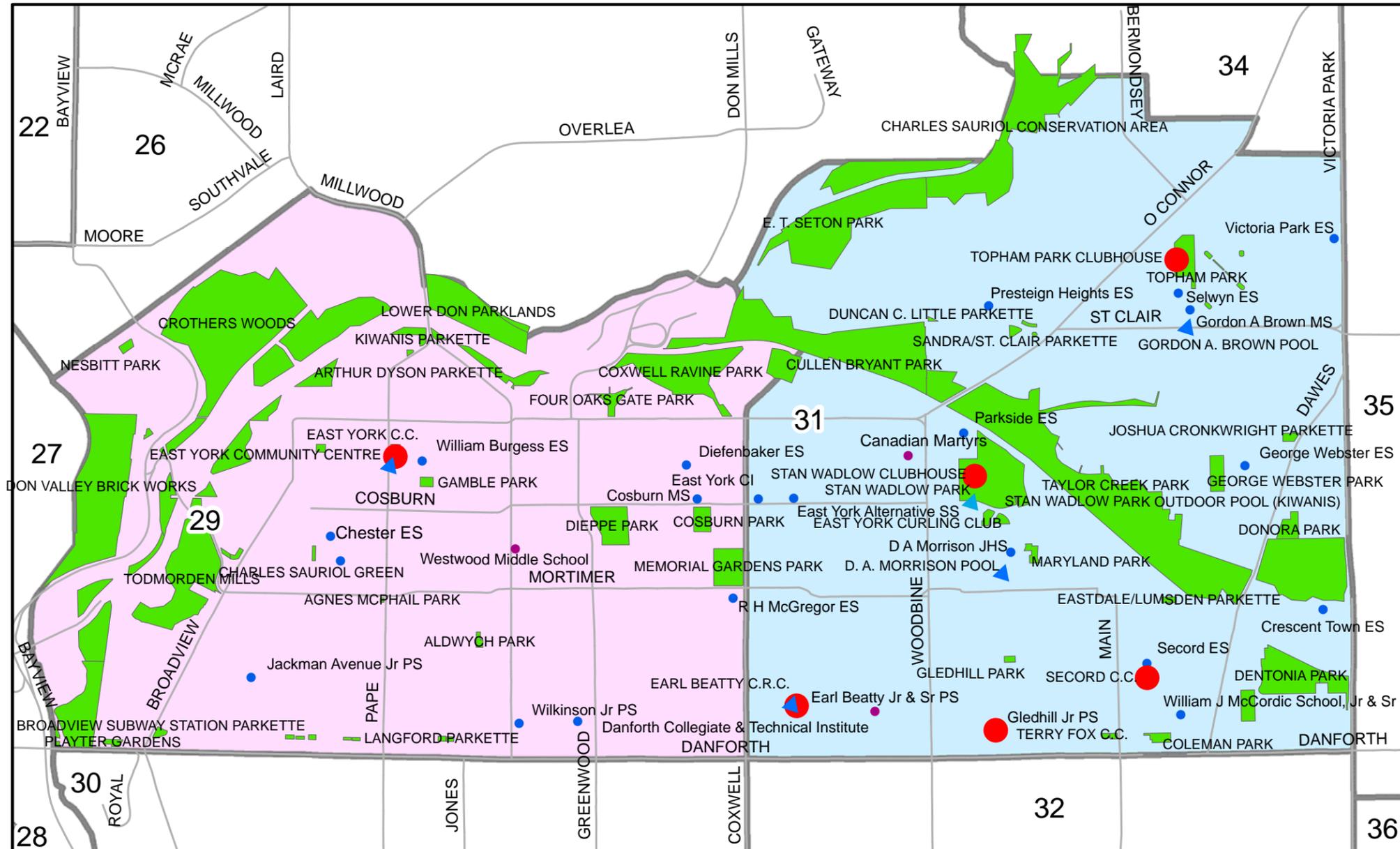


Ward 29: List of Human Services	
MapID	NAME
HS1	Albany Medical Clinic
HS2	Massey Centre
HS3	Salvation Army
HS4	Massey Centre
HS5	Out of the Cold. Overnight Shelter
HS6	Bethany Baptist Church
HS7	L'Arche Toronto
HS8	WoodGreen Community Services
HS9	East York Learning Experience
HS10	East York East Toronto Family Resources Organization
HS11	Out of the Cold. Meal Programs
HS12	Nisbet Lodge
HS13	Pape Food Bank
HS14	Nisbet Lodge
HS15	Credit Canada Debt Solutions
HS16	Co-operative Housing Federation of Toronto
HS17	Greek Orthodox Family Services and Counselling
HS18	Toronto Intergenerational Partnerships
HS19	East York East Toronto Family Resources Organization
HS20	West Scarborough Neighbourhood Community Centre
HS21	Woodgreen Community Services
HS22	Woodgreen Community Services
HS23	Toronto. Municipal Licensing and Standards
HS24	Toronto. Court Services
HS25	Toronto Public Library



COMMUNITY SERVICES AND FACILITIES MAPS

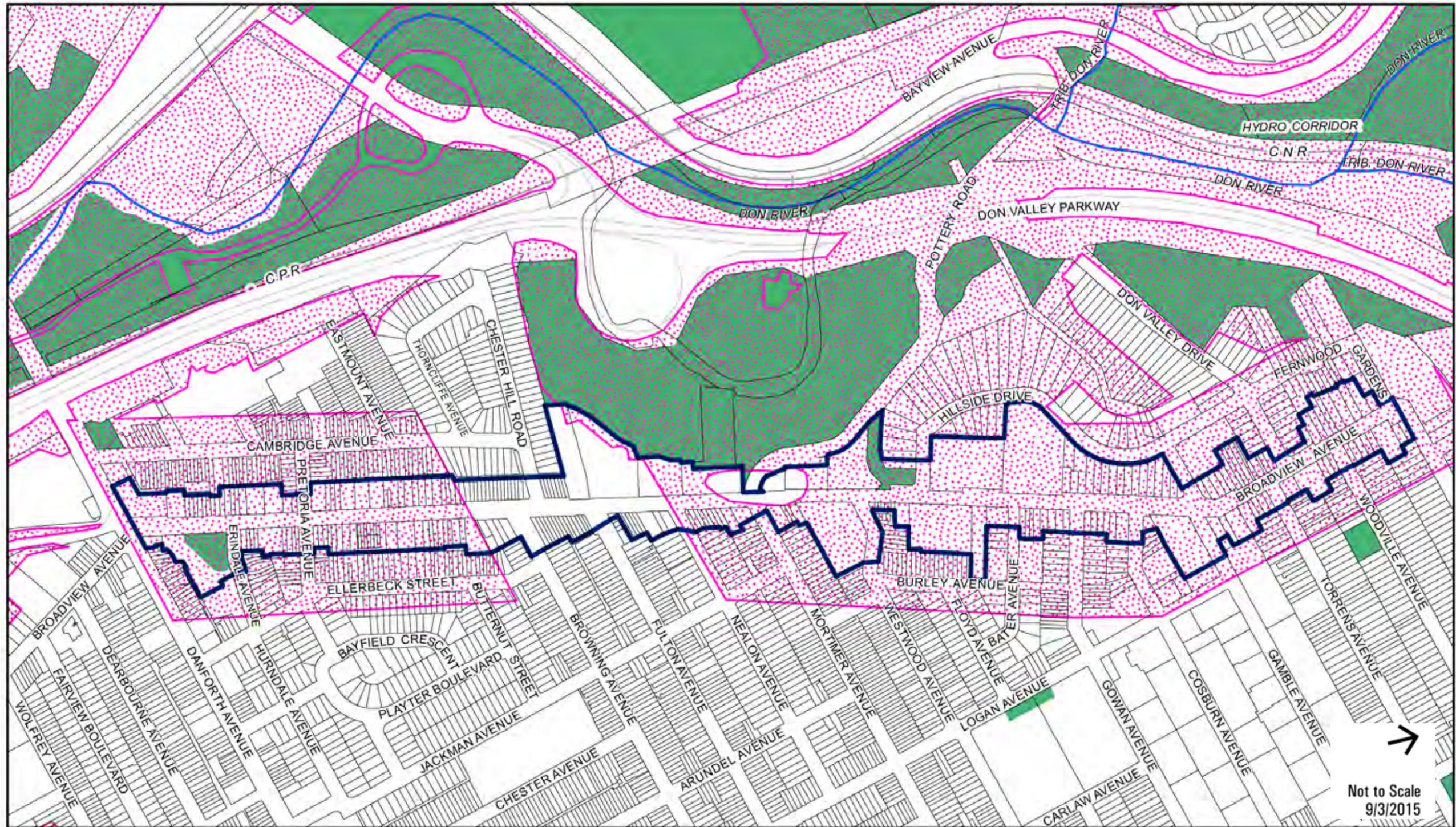
PARKS, FORESTRY AND RECREATION - Ward 29 and Surrounding Areas



Parks, Forestry and Recreation  
 Policy and Strategic Planning, Research  
 September, 2015 

# Appendix E

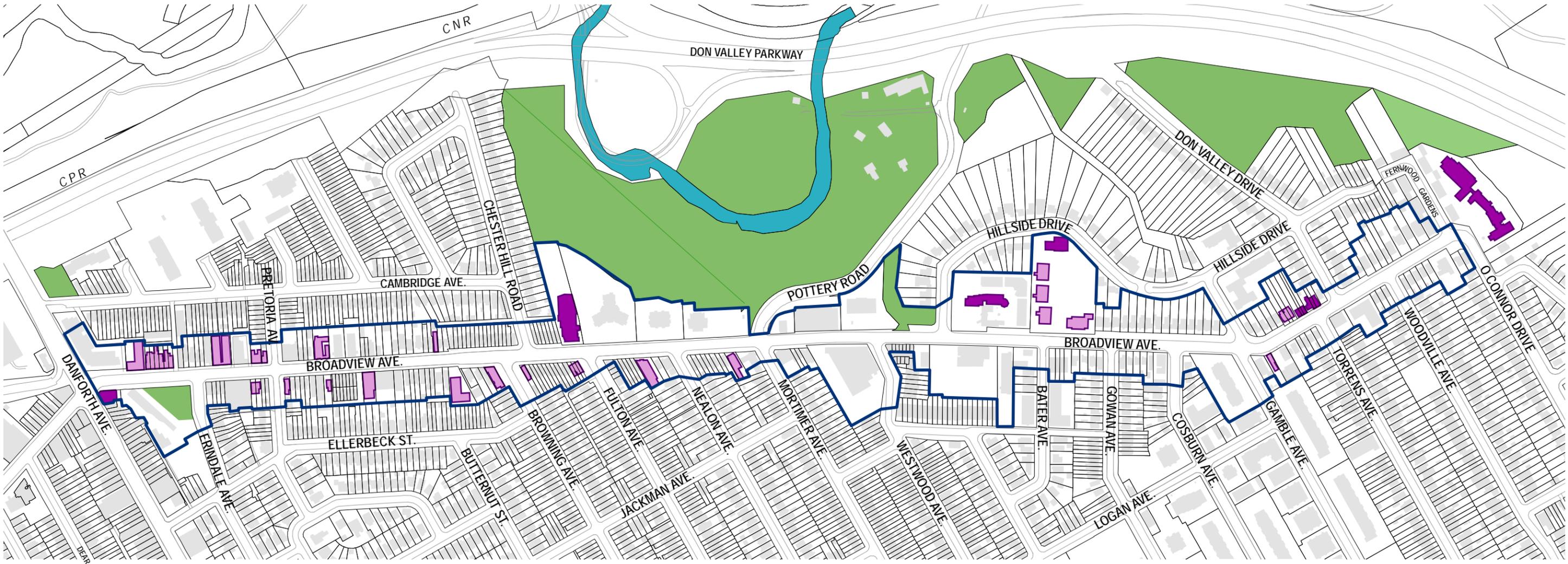
MAP OF AREAS WITH ARCHAEOLOGICAL POTENTIAL IN THE STUDY AREA



- Study Boundary
- Areas of Archaeological Potential
- Parks & Open Spaces

# Appendix F

## HERITAGE INVENTORY FOR THE BROADVIEW AVENUE STUDY AREA



■ Properties Included on the Heritage Register (HR)

■ Properties with Heritage Potential (HP)

## HERITAGE INVENTORY FOR THE BROADVIEW AVENUE STUDY AREA



"Looking North on Broadview Avenue from Danforth Avenue," 1908  
(*City of Toronto Archives, Fonds 1244, Item 7272*)

As part of the Broadview Avenue Study, a heritage built form survey has been undertaken to identify heritage properties as well as the heritage character of the Avenue as part of the general survey and study of the area so that heritage is a factor in the recommendations and principles being established for future development. This portion of the survey has identified individual properties and allocated them to two categories of heritage status and recommendation as indicated below.

The survey is divided into the east and west sides of Broadview Avenue and then by block, progressing numerically from south to north.

Unless otherwise indicated all photographs are by Heritage Preservation Services.

Two Categories:

**HR** - properties included on the Heritage Register or designated under Part IV of the Ontario Heritage Act

**HP - Potential for Inclusion** – these properties have not been previously included but from an initial survey have heritage value which would indicate they merit inclusion

**BROADVIEW AVE – EAST SIDE**

Photo	Status/ Recommen- -dation	Additional Information
<b>90 Danforth Avenue</b> Block 1: 769-775 Broadview Avenue (Danforth – Erindale Ave)		
	<p><b>HR</b></p>	<p>Date: 1918  <i>Included on the Heritage Register, 1976</i>                      Canadian Imperial Bank of Commerce, designed by the architect V. D Horsburgh, the property has cultural heritage value in the design, associative and contextual categories.</p>
<b>811-813 Broadview Avenue</b> Block 2: 791-815 Broadview Avenue (Erindale – Pretoria Ave)		
	<p><b>HP</b></p>	<p>Date: Pre-1890 (Goad's)                      This pair of semis appeared on the Goad's Atlas of 1890. They are located just south of the intersection of Broadview and Pretoria Ave (originally John St) the original intersection of Chester village (also known as Doncaster) Preliminary research and evaluation indicates that the pair of semis have design value as a rare example on Broadview Avenue of the 'Bay'n-Gable' house type. They have both associative and contextual value as they are two of the earliest surviving buildings within the study area and represent the 19<sup>th</sup> century history and building types of Broadview Avenue. Their contextual value is evident in their contribution to maintaining the historic character of the avenue and by being physically, functionally, visually and historically linked to its surroundings.</p>
<b>817 Broadview Avenue</b> Block 3: 817-897 Broadview Ave (Pretoria – Browning Ave)		
	<p><b>HP</b></p>	<p>Date: Prior to 1913 (Goad's)                      Located at the north-east corner of Broadview and Pretoria Avenues (originally John St) the intersection of the 19<sup>th</sup> century Chester Village. Preliminary research and evaluation indicates that this building has design value as an early 20<sup>th</sup> century example of the corner store building type which integrated residential accommodation. It has associative value as it yields information about the centre of the historic village. It has contextual value as the building is over 100 years old and has had a continuous presence contributing and maintaining the character of the area and is also physically, visually and historically linked to its surroundings.</p>

<b>835 Broadview Avenue</b> Block 3: 817-897 Broadview Ave (Pretoria – Browning Ave)		
	<b>HP</b>	Date: c. 1890 While this building lacks sufficient integrity to merit inclusion on the Heritage Register, it contributes to the character of Broadview Avenue as it dates from c 1890, and is set back from the street in a pattern typical for residential dwellings on Broadview Avenue. Its residential scale and form are characteristic of the late 19 <sup>th</sup> century. .
<b>849 Broadview Avenue</b> Block 3: 817-897 Broadview Ave (Pretoria – Browning Ave)		
	<b>HP</b>	Date: c. 1930 Known as the Bennett Apartments, this building has design value as a 3 ½ storey apartment building designed in a Neo-Classical Style. Preliminary research and evaluation indicates that it has associative value as it indicates the impact of the completion of the Bloor Danforth viaduct on the evolving social and built form history of Broadview Avenue. It has contextual value as its form, material, scale and building type contributes to the character of the Avenue where it is physically, visually, and historically linked to its surroundings.
<b>883 Broadview Avenue</b> Block 3: 817-897 Broadview Ave (Pretoria – Browning Ave)		
	<b>HP</b>	Date: c. 1950s Estonian Baptist Church Preliminary research and evaluation indicates this building has design value as an example of a mid-century Modern church, built after the Second World War, and merits further evaluation.
<b>895-897 Broadview Avenue</b> Block 3: 817-897 Broadview Ave (Pretoria – Browning Ave)		
	<b>HP</b>	Date: c. 1930 Located at the south-east corner of Broadview and Browning Avenue, this building combining commercial and residential functions displays design value in its composition and details which have been well-maintained. Preliminary research and evaluation indicates that it has associative value as it is a distinctive building type which was part of the evolution and growth of Broadview Avenue. Its contextual value is evident as it is important in defining and maintaining the character of the area and it is physically, visually and historically linked with its surrounding.

**905 Broadview Avenue** Block 4 905-931 Broadview Avenue (Browning Ave – Fulton Ave)



**HP**

Date: Prior to 1916

Located at the corner of Broadview and Browning avenues, preliminary research and evaluation indicates that this property has potential for inclusion on the Heritage Inventory on the basis that it has associative value as a corner store, identified on Goads Atlas as early as 1916 as a grocery store. Its design value lies in its example as a variation of the corner store type with a chamfered corner creating an entrance which addresses both streets with fine brick and stone details. It has contextual value as contributing to the characteristic pattern of corner stores located on Broadview Avenue which maintain and enhance the character of the street.

**931 Broadview Avenue** Block 4 905-931 Broadview Avenue (Browning Ave – Fulton Ave)



**HP**

Date: c. 1930

Located at the south-east corner of Broadview and Fulton Avenues, this building merits further investigation for inclusion on the inventory. Preliminary research and evaluation indicates that it has potential design value as an innovative type of semi-detached residence which maintains the integrity of its original features. It has contextual value as it contributes to the character of Broadview Avenue which is distinguished by a variety of building types responding to social change over more than 200 years.

**957 Broadview Avenue** Block 5 937-961 Broadview Avenue (Fulton Ave – Nealon Ave)



**HP**

Date: 1921-1923

Designed by the architect George Martel Miller, this building was originally a community centre for Toronto's Finnish community and was known as 'Don Hall' as well as the Toronto Finnish Hall" and the "Finnish Club House." It originally contained a theater for the production of Finnish plays, as well as a library. According to Barbara Myrvold, who compiled this research on this property, the Finnish community continued to use the property until the 1980s. The style of the building is a stripped Classicism typical of Scandinavian work at this period which also foreshadows Art Deco, especially with its stepped parapet and decorative triangular brick pendants. With a dark brick base and lighter upper stories the building possessed an elegant monumental grandeur.

Date: c 2010 – converted to condominiums



City of Toronto Archives Fonds 1266, Item 475

**981-983 Broadview Ave Block 6 965-985 Broadview Avenue (Nealon – Mortimer Ave)**



**HP**

Date: c: 1930

This property merits further evaluation As unique example of a quadriplex on Broadview Avenue, this property merits further evaluation for its potential design value as it has maintained the integrity of its original attributes, for its associative value as contributing to a history of residential accommodation and social evolution on the Avenue and for its contextual value as it maintains the character of the area, is integral to its history and its set-back on a diagonal to the street indicates the impact of the original historic lot ownerships and their particular development.

**1201 Broadview Avenue Block 12 1201-1217 Broadview Ave (Gamble – Torrens Ave)**



**HP**

Date 1914-1916

Preliminary research indicates that it has associative value as according to Goad's Atlas, this property was one of the earliest commercial properties to be developed on the east side of Don Mills Road as part of the Todmorden Village. This portion of Broadview Avenue was then known as Don Mills Road, it was numbered 315 and identified as a drugstore when it first appeared on Goads Atlas in 1916. It has contextual value as it supports and maintains the historic character of the village and is historically linked to its surroundings.



2255. Broadview North of Danforth June 9/23.

Broadview Avenue, looking north from Danforth Avenue, 1923  
*(City of Toronto Archives, Series 71, Item2255)*

**BROADVIEW AVE – WEST SIDE**

Photo	Status / Recommendation	Additional Information
	<p><b>HP</b></p>	<p>Date: c. 1930            Located in the first block north of Danforth Avenue, and originally known as Green Gables, this property has potential for inclusion on the Heritage Inventory. Preliminary research and evaluation indicates that its design value is evident in it being a well-designed example of low-rise multiple residential accommodation with fine details exhibiting both Classical and Spanish influences popular at this time. It has associative value as its building reflects the demands of the increased population following the completion of the Viaduct in 1919. Its contextual value is evident as it supports the historic character of the area as well as providing quality in scale and materials and details.  <i>(Photo Source: Google Maps, 2015)</i></p>

**782-792 Broadview Avenue** Block 1 778-828 Broadview Ave (Danforth – Pretoria Ave)



**HP**

Date: c.1920  
This commercial row of 6 properties with apartments above has potential for inclusion on the Heritage Inventory. Preliminary research and evaluation indicates that it has contextual value as it contributes to the character of the area and is physically and historically linked to its surroundings. It has associative value as it reflects the history of the development of the avenue in its built form. The design value is evident in the maintenance of original elements including the bay windows in four units and the brick details at the cornice.



**812-814 Broadview Avenue** Block 1 778-828 Broadview Ave (Danforth – Pretoria Ave)



**HP**

Date: c. 1930  
Located in the second block north of Danforth Avenue, and originally known as the Staveley Apartments, this property has potential for inclusion on the Heritage Inventory. Preliminary research and evaluation indicates that its design value is evident as it is a well-designed example of low-rise multiple residential accommodation with details reflecting a combination of the Gothic Revival and Streamlined Modern styles popular at this time. It has associative value as its reflects the demands for re-development on Broadview Avenue to accommodate the increased population following the completion of the Viaduct in 1919. Its contextual value is evident as it supports the historic character of the area as well as providing quality in scale and materials and details.

**822-826 Broadview Avenue** Block 1 778-828 Broadview Ave (Danforth – Pretoria Ave)



**HP**

Circa 1910 Stores with original storefronts  
This commercial row of 3 properties with apartments above has potential for inclusion on the Heritage Inventory. Preliminary research and evaluation indicates that it has contextual value as it contributes to the character of the area and is physically and historically linked to its surroundings. It has associative value as it reflects the history of the avenue in their built form. Design value is inherent in its second story fenestration, the brick details at the cornice but especially in the two original storefronts with their leaded glass transom lights at 824 and 826 Broadview Avenue.

**828 Broadview Avenue** Block 1 778-828 Broadview Ave (Danforth – Pretoria Ave)



**HP**

Date: c. 1884

Located at the intersection of the original Chester /Doncaster Village, this may be the original building indicated on Goads Atlas of 1884 which would make this building one of the earliest, if not the earliest building to survive on Broadview Avenue. Set back from the street it was originally built as a wood frame house but by 1916 was identified as a grocery store. The store front with the recessed entrance flanked by two large display windows may date from this time. It has associative and contextual value as it historically linked with the original village which occupied this intersection. Further evaluation is required to determine if it meets the criteria to merit inclusion on the Heritage Register.

**846-850 Broadview Avenue** Block 2 838-940 Broadview Ave (Pretoria Ave – Chester Hill Road )



**HP**

Date: c. 1930

Originally known as the Lenore Apartments, this building is identified as having potential for inclusion on the Heritage Register. Preliminary research and evaluation indicates that its design value includes an original roof garden, a carriage way entry which provided access and parking at the rear of the complex. It has associative value as part of the historic change brought to Broadview Avenue following the completion of the Bloor-Danforth viaduct. It contextual value is evident in its contribution to the historic character and scale of the neighbourhood and in its functional, visual and historic link to its surroundings.

**894 Broadview Avenue** Block 2 838-940 Broadview Ave (Pretoria Ave – Chester Hill Road )



**HP**

Date: c. 1920

Now known as the Richard Johnson Gallery, this building has potential for inclusion on the Heritage Registry as it is a typical example of a commercial building from this period set right at the edge of its lot frontage with residential accommodation above and including original storefront with a recessed entrance. Preliminary research and evaluation indicates that it has associative and contextual value as it is historically, physically and visually linked to its surroundings and contributes to the character of the Avenue.

**958 Broadview Avenue** Block 3 944-1010 Broadview Ave (Chester Hill – Pottery Road)



Chester School 1905 (Toronto Public Library)

**HR**

Date: 1890-1  
 Included on the Heritage Register in 2006  
 Formerly Chester School House from 1891-1959, the original building was designed by architects Gordon & Helliwell.  
 Date: post-1960 Addition  
 The property was sold to Estonian House Ltd in 1960 and after that date the four storey addition facing Broadview Avenue was done. The property is now known as Estonian House. The property is significant for its cultural heritage value in the architectural, associative and contextual categories.

**1102 Broadview Avenue** Block 5 1102 – 1168 Broadview Ave Dr – (Hillside - Gamble Ave)



**HR**

Date: 1919  
 Included on the Heritage Register in 2006  
 Originally the Wilfred Davies residence it now operates as the Massey Centre for Women. Although new buildings have been added to the property, the building retains its integrity and its view from the corner of Broadview Avenue and Hillside Drive remains intact. The property is significant for its cultural heritage value in the architectural, associative and contextual categories.

**1132 Broadview Avenue** Block 5 1102 – 1168 Broadview Ave Dr – (Hillside - Gamble Ave)



Chester Park, demolished (East York Historical Society)

**HR**

**HP**

Date: c. 1888 - Coach House  
 Date: 1967 - Salvation Army Children's Home  
 The Coach House, located at the rear of the property, was part of the original Chester Park estate built by Thomas Taylor and is included on the Heritage Register  
 The brick buildings viewed from Broadview Avenue are part of the Children's Home built by the Salvation Army and are recommended for further evaluation for inclusion on the Heritage Inventory for their design, associative and contextual value.

**1216-1220 Broadview Avenue Block 6 1206 – 1230 Broadview Ave (Gamble Ave – Don Valley Drive)**



**HP**

Date: Prior to 1913  
These three properties have been identified as having potential for inclusion on the Heritage Register. Preliminary research and evaluation indicates that they have design value as they represent an unusual set of three semi-detached houses with a fine design whose integrity and details such as the verandahs, gables and dormers remain in good repair. They are associated with the original Todmorden village. Contextually they support and maintain the character of this former village area and are visually and historically linked with their surroundings.

**1224-1228 Broadview Avenue Block 6 1206 – 1230 Broadview Ave (Gamble Ave – Don Valley Drive)**



**HR**

Date: c.1900  
Included on the Heritage Register in 2006  
This property which is already included on the Heritage Register was originally owned by the Howarth Family. The original purpose of the building is thought to have been a hotel or a store. As part of the original Todmorden village, the property represents an unusual building design. It meets the criteria by having design, associative and contextual values.

**1230 Broadview Avenue Block 6 1206 – 1230 Broadview Ave (Gamble Ave – Don Valley Drive)**



**HR**

Date: 1905  
Included on the Heritage Register in 2006  
This property which is included on the Heritage Register was built on land owned by the David Cramp family. The building is significant for its association with Todmorden Village and has contextual value as it maintains the character of the original village where is visually and historically linked to its surroundings.

**1232-1234 Broadview Avenue Block 6 1206 – 1230 Broadview Ave (Gamble Ave – Don Valley Drive)**



**HR**

Date: 1907  
Included on the Heritage Register in 2006  
Originally owned by Frederick Cramp, the son of David Cramp, the building is significant for its association with Todmorden Village and has contextual value as it maintains the character of the original village where is visually and historically linked to its surroundings.

**2 O'Connor Drive Block 8 (Fernwood Gardens – O'Connor Drive)**



**HR**

Date: 1885

Addition: 2010

Included on the Heritage Register in 2006,  
Designated Part IV in 2010

HEA 2010

Dating from 1885, the house was built to the designs of the architect D. B. Dick for John Frederick Taylor, the son of the mill owner, George Taylor.

The property was purchased by the Sisters of St. Joseph in 2010 and the extension, designed by the architects Shim Sutcliffe in 2010 was added. The property has significant architectural, associative and contextual value as it relates to the history of Broadview Avenue and the entire complex contributes significantly to maintaining the 19<sup>th</sup> century the character of the area by maintaining and supporting the historic qualities while introducing a contemporary structure which contributes to the design, associative and contextual values of the 21<sup>st</sup> century.

# Appendix G

## HISTORY AND EVOLUTION OF STUDY AREA

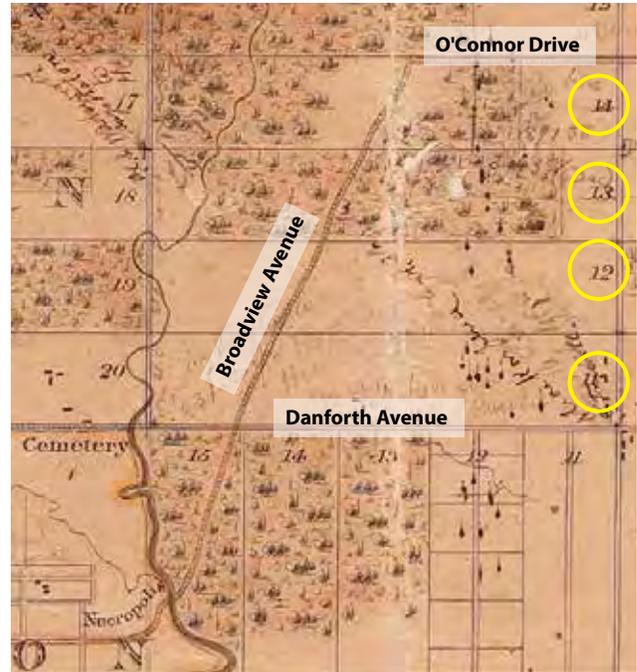
This section provides a summary of the historic evolution of the area for the purposes of describing the existing historic character of the Broadview Planning Study area. The research and analysis presented informs the heritage recommendations contained in this document, as well as the recommendations related to the public realm and urban design guidelines.

The history of the study area is characterized by the evolution of the mid-19<sup>th</sup> century hamlets of Todmorden and Chester<sup>1</sup> along a historic road on the high ground of the Don River Valley. Originally an aboriginal trail, it evolved to become the "Road to the Don Mills" in the late 18<sup>th</sup> century for its access to Toronto's first industrial site and finally came to be known as "Broadview Avenue" in the early 20<sup>th</sup> century.

The study area spans four farm lots – Lot 11, Lot 12, Lot 13 and Lot 14 (see image 1); land that was subdivided under the direction of Lieutenant John Graves Simcoe in the 1790s.

### *Acknowledgements:*

This historic overview relies greatly on primary research conducted by Barbara Myrvold, Senior Services Specialist, Local History, at Toronto Public Library. Barbara has written more than a dozen local histories include *The Danforth in Pictures* (1977), *Historical Walking Tour of the Danforth* (1992) and is presently working on a history of Broadview Avenue.



1851 J.O Browne Map of the Township of York in the County of York, Upper Canada. This map illustrates "Don Mills Road" as the diagonal road running south to today's Danforth Avenue, through what is now Riverdale Park and crossing the Don at the Necropolis. Local landowners built and planed this road in the 1840s as an alternative to Mill Road (Broadview Avenue) from their mills and farms into Toronto. Yellow circles highlight the historic farm lot division demarcations for Lot 11, 12, 13 and 14. Current street names have been added.



Don Valley at Todmorden Mills from north of Broadview Avenue, c1911 (City of Toronto Archives).

<sup>1</sup> Chester Village was sometimes called Doncaster, in reference to the Doncaster post office located within the village.

## Early Aboriginal Use

### *The Don River Valley*

The Don River Valley is an important component to the natural and cultural heritage character of the study area. Prior to being known as "Broadview Avenue", named after the spectacular "broad view" that one has at the top of the ridge overlooking the valley, various sources acknowledge that the east side of the Don River was part of an ancient aboriginal trail<sup>2</sup>. It could have been established as long as 4,000 years ago, contemporary to archaeological artifacts from the Archaic period (pre-3000 BC)<sup>3</sup> found south of the study area at the present Withrow Junior Public School site in 1886. Early settlers recalled that Mississauga first nations encamped along the east side of the Don in what is now Riverdale Park and , near Pottery Road, up until the 1830s<sup>4</sup>.

The aboriginal trail was a pre-cursor to Broadview Avenue. The road's deviation from the prevailing Toronto street grid speaks to the influence of the Don River Valley and its natural topography and its significant role in shaping the development of the area.

Prior to being called the Don, the Anishnaabe first nation called the river Wonscoteonach, meaning "the river coming down from the back burnt country"<sup>5</sup>. The river had once been deep and wide enough to accommodate boat travel all the way to Gerrard Street<sup>6</sup> and abounded with wildlife and trout<sup>7</sup>. The Don River was also an important source of water power to supply Toronto's first industrial community, which gave rise to the early Euro-Canadian settlement of the study area described in the next section.

2 Don Valley conservation report (Toronto: Ontario Department of Planning and Development, 1950), 13-14.; True Davidson, *The Golden years of East York* (East York, Ont.: Centennial College, 1976), 11.

3 "Bequeathed to us by the Indians," *Toronto Globe*, 21 March 1922, 13. *Toronto Evening Telegram*, 28 June 1886.

4 Charles Sauriol, *Pioneers of the Don* (Toronto: Charles Sauriol, 1995), 15.

5 Henry Scadding, *Toronto of old*, (Toronto: Adam Stevenson & Co., 1873), 233.

6 Archaeological Services Inc. "Archaeological Resource Management Plan, City of Toronto, Technical Report – Appendix C: Don River Corridor Historical Review". May 2014.

7 Elizabeth Muir, *Riverdale: East of the Don* (Toronto: Dundurn, 2014), 62.



Don River Valley. In Charles Sauriol's *Pioneers of the Don*.



Don Valley - from Don Mills Road Bridge. 1916 (City of Toronto Archives).



Opportunity to strengthen the study area's historic relationship with the Don Valley.

## Early Euro-Canadian Settlement (1790-1850)

The study area and surrounding lands were subdivided into approximately 200-acre farm lots in the 1790s as land grants for United Empire Loyalists, government officials and other landed settlers in the lands surrounding the Town of York (now Toronto). The study area spans four historic farm lots – Lot 11, Lot 12, Lot 13 and Lot 14, Concession 2 from the Bay. Lieutenant Governor John Graves Simcoe prioritized Lot 13, with the Don River flowing through it, for industrial use from the beginning of the city's history.

### *Todmorden Village*

Lot 13, Concession 2 from the Bay included lands bounded by today's Westwood Avenue, Donlands Avenue, Gowan Avenue and the historic route of the Don River. In 1795, Simcoe wrote to Christopher Robinson, a Loyalist, government official and original grantee of Lot 13, requesting Robinson to relinquish his land for the construction of a mill. To address the lumber shortage as the Town of York was growing, Simcoe stated that, "it is necessary that a mill should be built thereon. It is therefore I directed Skinners to build a mill".

Brothers Isaiah and Aaron Skinner were thus granted Lot 13 and built the first mill on the Don in 1795. In 1798, the Skinner's were permitted to erect a second mill, a grist mill, with the condition that they construct a road from today's Queen Street, on the east side of the river, to the mill site, turning left at Pottery Road. This road was initially called Mill Road, then Don Mills Road and eventually Broadview Avenue.

Thomas Helliwell purchased the Skinner's mills in 1829 and took over their operation with John Eastwood. The Helliwells also built a distillery, a brewery, a malt house, and their home<sup>2</sup>. The Helliwells would eventually own Lots 12, 13 and 14, Concession 2 from the Bay, comprising the study area from approximately Browning Avenue to O'Connor Drive. In 1855, the Helliwells sold their mill property

<sup>1</sup> Wolford Simcoe Papers, Toronto Public Library, III: 365. Quoted in Eleanor Darke, *A Mill shall be built thereon; an early history of the Todmorden Mills*, (Toronto: East York Historical Society, 1995) 27.

<sup>2</sup> Elizabeth Muir, *Riverdale: East of the Don* (Toronto: Dundurn, 2014), 38.



Todmorden Mills Museum site, 2015.



Helliwell house, 2015 with view of 1048 Broadview Avenue and 1050 Broadview Avenue in background.

to the Taylor brothers and subdivided portions of their land into village lots of one to five acres.

Industrial activity along the river would expand throughout the 19th century to include paper mills, distilleries, brick works and breweries – there were at least 22 mills on the Don River<sup>3</sup>. Today, the Todmorden Mill site, serves as a reminder of the study area's early beginnings. As the city grows around it, the Todmorden Mill site still retains a remarkable collection of intact, early 19th century structures from the industrial complex.

Although the industrial site lies outside of the study area, it is closely related to its early settlement. As industry expanded along the river, the adjacent land on the high ground of the Don Valley was populated by workers and development along the "Mill Road" expanded to include residences, hotels and shops. This budding community was called Todmorden Village, named in honour of the Helliwells who had emigrated from Todmorden, England.

In 1851, W.H Smith writes; "...the village of Todmorden. There are but a few houses on the upper bank, but on descending by a steep and circuitous road to the valley below, you reach a paper mill, grist mill and starch factory; with the residences of the owners and work-people employed<sup>4</sup>". Five years later, Todmorden Village had grown slightly, with 20 names listed at Todmorden on "Don Road" in the 1856 Brown's Toronto city directory. Village residents included labourers, carpenters, butchers, a shoe maker, a millwright, a paper maker and a shingle maker, a bricklayer, and a farmer<sup>5</sup>.

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3 Elizabeth Muir, *Riverdale: East of the Don* (Toronto: Dundurn, 2014), 47.

4 W. H. Smith, *Canada past, present and future* (Toronto, 1851), 1: 19-20.

5 1856 Brown's Toronto city directory information taken from Barbara Myrvold's research summarized in "Broadview Avenue – Danforth Avenue to Bater Avenue: An Historical Overview", August 6, 2015.



Buildings associated with the historic Todmorden Village (1216-1220 Broadview Avenue on the left, 1224-1228 Broadview Avenue on the right).

The intersection of Pottery Road and Mill Road (now Broadview Avenue) was once the heart of the historic Todmorden community. Pottery Road was the main path to the mill site from Mill Road. The Todmorden "Meeting House", Todmorden House Hotel, Central Hotel, and Reid's blacksmith shop, are known historic structures that stood near the four corners of this intersection in the 1850s<sup>6</sup>. Pottery Road originally connected to Broadview Avenue slightly north of Westwood Avenue. Pottery Road was re-aligned to its current location in the late 1960s<sup>7</sup>.



Pottery Road, 1900 in Charles Sauriol's *Remembering the Don*: "Smoke stack marks the approximate site of Skinner's Grist mill on the Don. The apple trees on the flats are now replaced with willows. White pine trees no longer grow on the denuded slopes of Hillside Drive" (Dundurn Press, 1981, p51).



Todmorden House c1892 (Toronto Public Library).  
 Todmorden House once stood at the south east corner of Broadview and Westwood avenues.

6 Archaeological Services Inc. "Archaeological Resource Management Plan, City of Toronto, Technical Report – Appendix C: Don River Corridor Historical Review". May 2014.

7 Initial streetscape changes to Pottery Road occurred in the early 1950s, establishing the Charles Sauriol Parkette at the southwest corner of Helliwell Drive and Broadview Avenue. Evolution of major realignment of Pottery Road is visible in aerial photos of the area from 1966, 1967 and 1968 from the City of Toronto Archives.

## Pottery Road Realignment



1950 aerial photograph of Pottery Road and Broadview Avenue intersection. Red arrow indicates the alignment of Pottery Road in the image (City of Toronto Archives).



1953 aerial photograph of Pottery Road and Broadview Avenue intersection. Red arrow indicates the alignment of Pottery Road in the image (City of Toronto Archives).



1966 aerial photograph of Pottery Road and Broadview Avenue intersection. Red arrow indicates the alignment of Pottery Road in the image (City of Toronto Archives).



1968 aerial photograph of Pottery Road and Broadview Avenue intersection. Red arrow indicates the alignment of Pottery Road in the image (City of Toronto Archives).



Location of the historic alignment of Pottery Road was just south of today's Hillside Drive.



Current alignment of Pottery Road was constructed in the 1960s.

## A Tale of Two Villages (1850-1900)

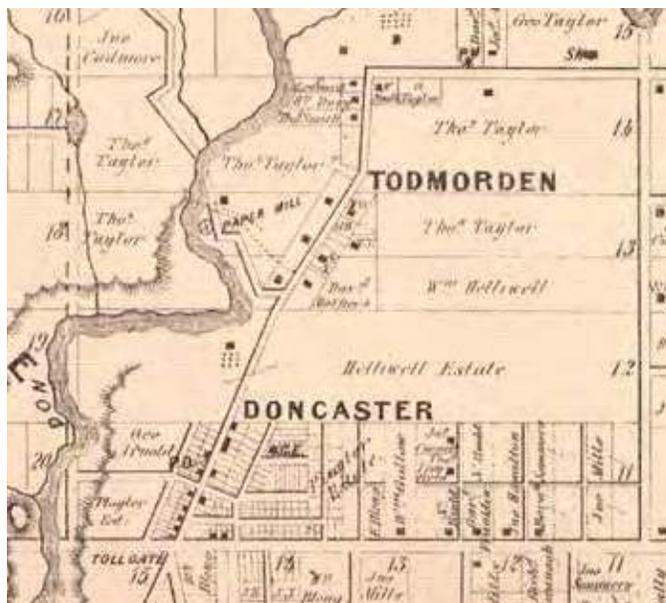
The study area comprises two historic villages: Todmorden and Chester. The Doncaster Postal Office, which opened in 1869, served both communities and was sometimes used interchangeably with the village names<sup>8</sup>. Todmorden expanded from the early industrial complex in the Don Valley to the road on the high ground above.

Chester Village was established in the late 1850s after the Playter family subdivided Lot 11, Concession 2 from the Bay. Today, more original buildings from Chester Village remain along Broadview Avenue than from Todmorden Village.

In 1855, the Playters made a plan subdivision of forty-five acres in the western part of their property in Lot 11. In 1859, the Playters commissioned architect John Tully to prepare a plan of subdivision for all 200 acres of Lot 11. The land was divided into village lots between five to fifteen acres. This plan established the existing street layout of the study area between Danforth and Browning avenues, although some of the original street names have changed. The smaller five acre village lots were aligned along Don Mills Road (now Broadview Avenue). An 1878 map of the area indicates that the majority of development in the subdivision occurred along Broadview Avenue (see image to the right).

In the mid 19th century, the villages of Todmorden and Chester were still small rural settlements outside the city limits. A description of the area in 1885 illustrates a picturesque rural river valley:

*The villages of Doncaster and Todmorden lie within a short distance of each other on the east bank of the Don; the former being about half a mile lower down. The scenery of the Don, in this neighbourhood and for miles further up, is extremely picturesque. The Don winds through a broad valley, the bottom lands immediately adjoining the river, which are usually flooded in the spring time, yielding rich pasturage. The banks, which are thickly wooded, rise abruptly, sometimes from the water, but more often at a considerable distance... The wildness and beauty of the ravines, glens, and stretches of woodland, present attractions for the lover of nature*



1878 Historical Atlas of York, Miles & Co showing historic building footprints.



Buildings constructed in the late 19<sup>th</sup> century in Chester Village and Todmorden Village.



Chester Village buildings

<sup>8</sup> Archaeological Services Inc. "Archaeological Resource Management Plan, City of Toronto, Technical Report – Appendix C: Don River Corridor Historical Review". May 2014.

*not readily surpassed in this part of Canada<sup>9</sup>.*

The 1880s marked a decade of economic prosperity and building development in the environs of the study area. In 1884, the City of Toronto annexed land east of the Don River bounded by Kingston Road (now Queen Street East), Greenwood Road, Danforth Avenue and the river. A few years after, the Don River was straightened in 1888 and a streetcar line opened along Broadview Avenue to downtown<sup>10</sup>. As the City of Toronto expanded east of the Don River and infrastructure improvements were made, enterprising landowners in north of Danforth road subdivided their lands in anticipation of development. In 1887, William P. Helliwell subdivided 120 acres of the eastern part of Lot 12, Concession 2 from the Bay. Churches and the Chester Public School (now Estonian House, constructed 1891) were planned for at this time in anticipation of a growing community.

At this time, residents of Todmorden and Chester unsuccessfully petitioned York Township in 1889 and 1890 to be incorporated in to a single village, Chester. They were seeking access to taxing powers to fund infrastructure improvements and other essential public services. However, each time they were unsuccessful because they failed to meet the minimum population of 750 to qualify for incorporation. Despite the growth occurring east of the Don River, south of Danforth, the population in Chester and Todmorden remained relatively stable between the 1870s and 1890s with city directories indicating approximately 100 people living in Chester and 150 in Todmorden<sup>11</sup>.

## Early 20<sup>th</sup> Century & Annexation (1900-1940)

Todmorden Village remained closely associated with Don River industry in the early 20<sup>th</sup> century. Historical accounts describe how, on pay day, the wives of workers in the Don Valley Brick

<sup>9</sup> History of Toronto, County of York (Toronto: Blackett-Robinson, 1885), 1: 94.  
<sup>10</sup> Barbara Myrvold, The Danforth in Pictures (Toronto: Toronto Public Library, 1979), 3.

<sup>11</sup> Directory information provided by Barbara Myrvold in "Broadview Avenue – Danforth to Bater Avenue: An Historical Overview", August 6 2014. In 1871, Chester (Doncaster) contained about 100 people and Todmorden about 150, according to Nason's directory of York County (James Randle Nason, Nason's East and West Ridings of the County of York, or Townships of Etobicoke, Markham, Scarboro', Vaughan & York Directory (Toronto, 1871), 96.) In 1893, the Toronto city directory provided 81 listings for Chester and 156 listings for Todmorden.



Chester Public School at 958 Broadview Avenue in 1903. The school was constructed in 1890 (Toronto Public Library).



Chester Public School at 958 Broadview Avenue was converted to the Estonian House in the 1960s and has remained a cultural hub for the Estonian Canadian community since. The front addition was added in the 1970s. The property was included on the Heritage Register in 2006.



Construction of Prince Edward Viaduct c1915 (National Archives of Canada PA-070098).

Works would wait at the top of the hill at Broadview Avenue and Pottery Road to intercept husbands before they spent their wage at the Todmorden Hotel, owned by Robert Davies in the early 20th century and located at Broadview and Pottery Road.

At the beginning of the 20th century, Todmorden and Chester villages were still regarded as suburbs on the outskirts of the city limits. Chester (Doncaster) Village was annexed by the City of Toronto in 1909. The southern half of the Helliwell Estate (Lot 12, Concession 2 from the Bay) was annexed in 1912.

The opening of the Prince Edward Viaduct in 1918 finally unlocked the study area for growth as landowners subdivided and sold their lands. The 1924 fire insurance plan for the area illustrates the extent of growth that occurred in comparison to the 1913 fire insurance plan. Ongoing growth in the 1920s resulted in the introduction of apartment buildings in the study area.

By 1924, Todmorden Village became a part of East York when it incorporated as a separate township.

Growth continued into the inter-war period, with the introduction of residential properties in Lot 14, the northern section of the study area, in the inter-war period.



Buildings constructed in Chester Village c1920.



1920s apartment building adjacent to a 1950s apartment building. Variances in architectural treatment, yet similar in form, massing, materials and scale.



The Finnish Club House at 957 Broadview Avenue was designed by George Martel Miller in 1921 and became a vibrant hub for the Finnish community in the study area (Toronto Public Library).



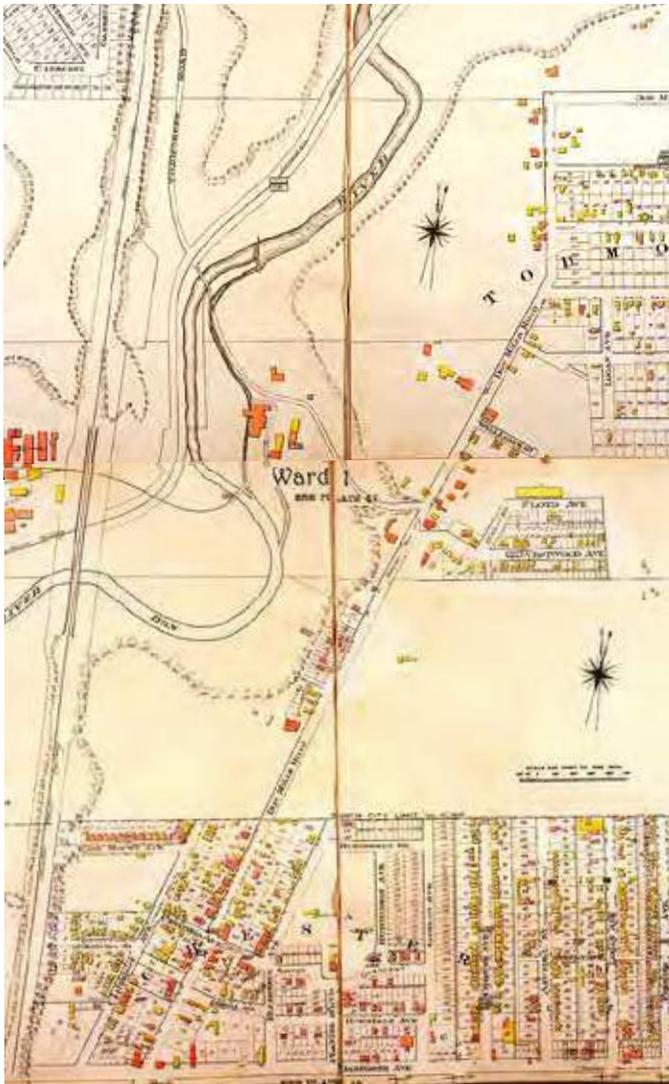
957 Broadview Avenue was converted into condominiums in 2010.



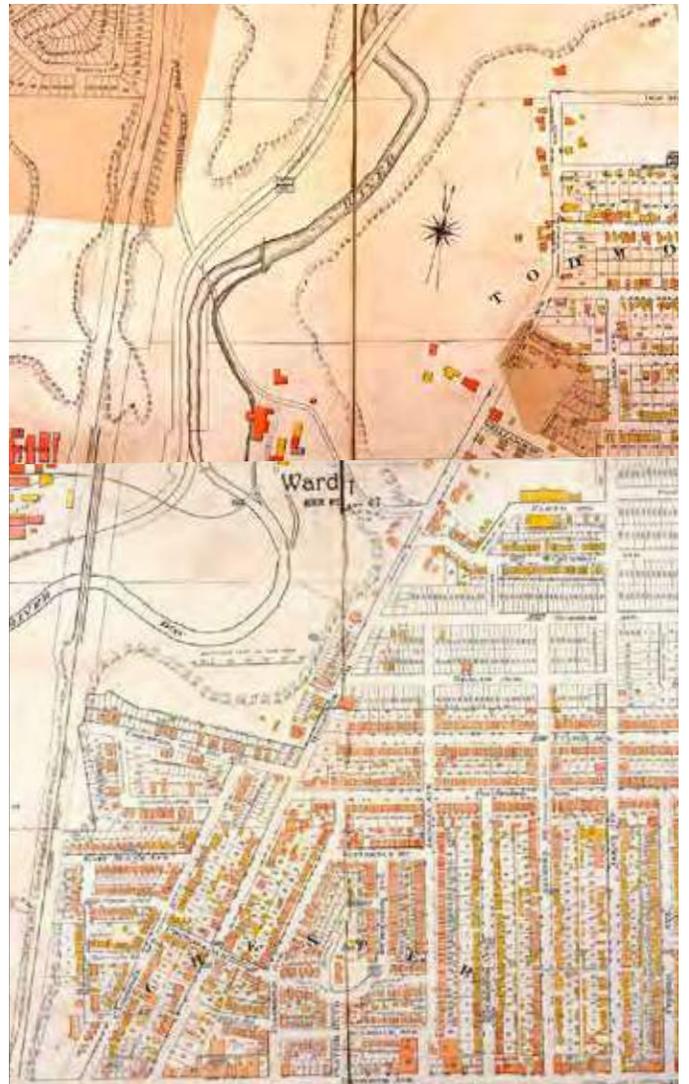
Broadview Avenue north of Gamble Avenue, 1930 (Todmorden Mills Heritage Museum and Arts Centre).



Broadview Avenue north of Gamble Avenue, 2015.



1913 Goads Fire Insurance Plan



1924 Goads Fire Insurance Plan

## 1940 - Present

In the 1940s, Toronto entrepreneur Rand Freeland purchased what was left of the original mill property, granted to the Skinners in the 1790s. The \$500 that Freeland paid for the property indicates the decline of the mill industry after the Great Depression and World War I and II. The area had become the location of a municipal dump and garbage was used to fill in the quarries of the nearby brickworks. In *Tales of the Don*, local conservationist Charles Sauriol describes the state of the area at the time;

*...threats against the natural beauty of the Valley were multiplying on every side. Pocket sewage disposal plants were so overloaded that raw effluent was being dumped into the Don River and Taylor Creek. There was a threat of factory development where Todmorden Mills now stands. Wooded slopes and wetlands disappeared under heaps of garbage when they were used as municipal dumps. Trees were felled to provide space for service lines spanning the Valley<sup>12</sup>.*

In response to these environmental threats, Charles Sauriol and Rand Freeland co-founded the Don Valley Conservation Authority (DVCA) to protect the Don Valley from further degradation. Freeland converted the Skinner property into his "Fantasy Farm" where he often hosted DVCA events and celebrations. The DVCA was successful in that many of its recommendations and objectives, including the protection of the floodplains from further misuse and the redirection of raw effluent to a central point in Lake Ontario were implemented<sup>13</sup>.

Sauriol and Freeland were trailblazers, with the DVCA pre-dating the Toronto and Region Conservation Authority (TRCA), which was established in 1954 after the devastation of Hurricane Hazel made the value of Toronto's ravines and the importance of protecting flood plains an obvious necessity.

In 1954, the TRCA declared that the Todmorden Mills site formed part of the flood plain. This put a stop to redevelopment plans of the site for new industrial and residential development, and conserved the four original mill buildings<sup>14</sup>. The Todmorden Mills

<sup>12</sup> Sauriol, Charles *Tales of the Don* (Toronto: Natural heritage/Natural History Inc., 1984), 23.

<sup>13</sup> *Ibid*, 23-27.

<sup>14</sup> *Ibid*.



"Stop: Don't Cut Trees", Don Valley Conservation Association, 1947 (with unidentified DVCA member). Charles Sauriol's cottage, located at the Forks of the Don, is seen in the background (City of Toronto Archives)..



Parkette at Hillside Drive is named after local conservationist, Charles Sauriol.

Museum opened in 1967. The prominent role Charles Sauriol had in conserving the Don Valley is recognized in the Study Area through the naming of Charles Sauriol Parkette at Hillside Drive.

Several historic buildings were demolished in the 1950s and 1960s. Todmorden Village in particular suffered major losses with many of its original buildings demolished during this period. Because original lot sizes in Todmorden Village tended to be larger than village lots in Chester Village, many of the replacement buildings were out of scale to the original historic buildings.

The realignment of Pottery Road was another major change that further altered the historic pattern of development in Todmorden Village. Pottery Road went through a slight re-alignment between 1950-1953, resulting in the Charles Sauriol Parkette on the southwest corner of Hillside Drive and Broadview Avenue. Major realignment of Pottery Road between 1966 to 1968 altered the road to its current location today.

Other significant changes in the study area in the post-war period included the introduction of the high-rise apartments in the 1960s and 1970s.

Changes in Chester Village evolved in a manner that was more consistent with the historic pattern of evolution of this portion of the study area. Mid-rise apartment buildings built in this period evolved from their 1920s equivalents with variances in architectural treatment, yet similar in form and massing.



Todmorden Hotel in 1955. Originally constructed in the 1850s. Demolished in 1965 (Toronto Public Library).



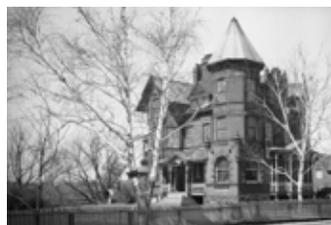
Sobeys replaced the historic Todmorden Hotel.



The Bater's General Store in 1966. Once stood on the east side of Broadview Avenue, north of Todmorden House Hotel (East York Historical Society).



Bater's General Store site at 1129 Broadview Avenue, 2015



William Taylor House in 1952. Also known as Bellehaven, this house once stood on Broadview Avenue on the original alignment of Pottery Road. It was demolished c1960 (Toronto Public Library).



The Minto Sky building at 1048 Broadview Ave and high-rise apartment at 1050 Broadview Ave was constructed on the former site of Bellehaven, William Taylor House.



Chester Park, built in 1888 at 1132 Broadview Avenue. In 1940, the Salvation Army acquired the property and it was used as an orphanage until it was demolished in 1967 and replaced by the Broadview Village complex (East York Historical Society).



The Chester Park coach house remains in situ on the Salvation Army Broadview Village complex.

**Broadview Avenue Planning Study  
Urban Design Guidelines**

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Danforth Avenue to O'Connor Drive