

# DECISION AND ORDER

**Decision Issue Date** Monday, September 09, 2019

PROCEEDING COMMENCED UNDER Section 45(12), subsection 45(1) of the Planning Act, R.S.O. 1990, c. P.13, as amended (the "Act")

Appellant(s): FERGUS BARNES

Applicant: MATTHEW RIBAU

Property Address/Description: 113 Hannaford Street

Committee of Adjustment Case File: 18 248452 STE 32 MV (A1018/18TEY)

**TLAB Case File Number: 19 143533 S45 19 TLAB**

**Hearing date:** Thursday, August 29, 2019

**DECISION DELIVERED BY JUSTIN LEUNG**

## APPEARANCES

NAME	ROLE	REPRESENTATIVE
FERGUS BARNES	Owner/Appellant	MATTHEW RIBAU
MATTHEW RIBAU	Applicant	
CARMEN HURTADO	Participant	

## INTRODUCTION

This is an appeal from a decision of the Toronto East York Committee of Adjustment (COA) pertaining to a request to permit 2 variances for 113 Hannaford Street (subject property).

The variances, if allowed by the Toronto Local Appeal Body (TLAB), would permit the construction of a rear laneway facing detached garage which is to replace an existing garage on the site.

This property is located in the East End-Danforth neighbourhood in the East York district of the City of Toronto (City) bounded by Malvern Avenue to the west and Pickering Street to the east. The property is located on Hannaford Street, south of Gerrard Street East and north of Swanwick Avenue.

At the beginning of the hearing, I informed all parties in attendance that I had performed a site visit of this subject property and the immediate neighbourhood and had reviewed all materials related to this appeal.

## **BACKGROUND**

The variances that had been requested are outlined as follows:

**1. Chapter 10.5.60.20.(6) (A), By-law 569-2013:** The minimum required side yard setback for an ancillary building or structure containing a parking space where a side lot line abuts a lane, and vehicle access to the parking space is from the lane is 1.0 m. In this case, the side yard setback for the ancillary building will be 0.04 m to the south lot line

**2. Chapter 10.5.60.20.(4), by-law 569-2013:** An ancillary building or structure may be no closer than 2.5 m from the original centre-line of a lane. In this case, the ancillary building or structure will be 1.71 m from the original centre-line of a lane on the south side.

These variances were heard and refused at the April 3, 2019 Toronto East York COA meeting. Subsequently, an appeal was filed by the property-owners of 113 Hannaford Street within the 20 day appeal period as outlined by the *Planning Act*. The TLAB received the appeal and scheduled a hearing on August 29, 2019 for all relevant parties to attend.

## **JURISDICTION**

### **Provincial Policy – S. 3**

A decision of the Toronto Local Appeal Body ('TLAB') must be consistent with the 2014 Provincial Policy Statement ('PPS') and conform to the Growth Plan for the Greater Golden Horseshoe for the subject area ('Growth Plan').

### **Minor Variance – S. 45(1)**

In considering the applications for variances from the Zoning By-laws, the TLAB Panel must be satisfied that the applications meet all of the four tests under s. 45(1) of the Act. The tests are whether the variances:

- maintain the general intent and purpose of the Official Plan;
- maintain the general intent and purpose of the Zoning By-laws;
- are desirable for the appropriate development or use of the land; and
- are minor.

## **MATTERS IN ISSUE**

The applicant argues that a City staff recommended condition is impractical and should be amended by the TLAB. A party to the appeal also contends that the proposed detached garage should be built to be consistent in terms of its siting and orientation with the other garage structures along the laneway. The TLAB must consider whether the variances requested would be appropriate in this instance and to amend City staff recommendations on a Planning application. Moreover, if there are factors to be considered in having a proposed structure in a defined space be constructed in a similar manner to other adjacent structures.

## **EVIDENCE**

The applicant, Matthew Ribau, a contractor with Perspective Views, provided information relating to his minor variance appeal. Mr. Ribau was acting on behalf of the owner Fergus Barnes, who was not in attendance at the hearing. Mr. Ribau described the rationale behind the owner's proposal. Initially, an Order to Comply from the City's Building Department had been issued to the owner due to the existing garage's structure being compromised resulting in the roof the structure overhanging onto the adjacent property of 115 Hannaford Street. The owner has decided to replace this structure with a new garage which would be slightly larger from the existing garage. The subject property straddles a laneway which is used to access several rear facing garages. The proposed garage siting will not be straddling the rear property line so as to leave space for vehicles to turn on the laneway as the subject property is at the corner of this laneway. Mr. Ribau indicated that Variance request #1 is the most significant in terms of potential impact.

He also referenced comments as provided by the City's Engineering Department which indicates that if the minor variance were approved by COA, that a proposed lane widening near the southerly portion of the subject property would be necessary. Mr. Ribau stated that there were concerns with the comments as provided by the Engineering Department but he had not been able to establish communication with requisite department staff on this matter. They contend that the only method to accomplish this widening is if the structure on the adjacent property were also taken down. Mr. Ribau requested the TLAB's indulgence to consider altering or removing Engineering Department's proposed condition of approval. He also referenced that research had been done of the area which found similar detached garage structures which have been constructed. On inquiry by the presiding TLAB member, Mr. Ribau confirmed that the laneway in question is legally registered.

The participant, Carmen Hurtado, of 115 Hannaford Street also presented to the TLAB. Ms. Hurtado raised concern on how the construction and maintenance of this proposed garage can occur if it would be abutting the property line. The Order to Comply has been outstanding for a year which has been concerning for her. She also indicates that the fence which she erected along the southerly property line is not encroaching onto 115 Hannaford Street in response to comments made by Mr. Ribau. She also inquired as to why they are proposing a driveway to the garage as it appears this may be an attempt to increase parking for the applicant's owner's property. She also raises the question as to whether the garage could potentially be converted into a laneway home in the near future. The soffit or eaves troughs to be placed on the garage could also be an issue due to encroachment. She cannot recall another similar garage design in the neighbourhood. She goes on to describe how the existing structure is causing structural issues for her property's shed. Ms. Hurtado would not object to the garage being rebuilt to the original siting; however, this new proposed siting intrudes on her enjoyment of her rear yard.

Mr. Ribau responded that several of the issues which Ms. Hurtado raised relate to the current structure and not to the proposal at hand. In addition, due to the lot pattern of this area, the proposed garage would have to be built to straddle the property line. In terms of the encroachment of the garage, Mr. Ribau responds that the material he has clearly shows it does not. The discrepancy could be attributed to the fence which had not been constructed to accurately follow the property line between the subject property and 115 Hannaford Street. In terms of the proposed driveway, this is being recommended so as to provide a greater turning radius for vehicles to access the laneway. In addition, this driveway would not be able to accommodate one standard sized vehicle. Parking will only occur in the garage. Mr. Ribau states that the property-owner has not indicated an intention to have a laneway home atop this proposed garage in future. Soffits and eaves troughs will also not be part of this proposal. The proposed garage is slightly larger to assist in storing some materials and for a vehicle as well. A building permit has been submitted but has not been fully processing pending the outcome of this TLAB appeal. In terms of siding, masonry block or brick has been proposed to address possible maintenance issues. Such material would be more weather resistant and require less maintenance. Mr. Ribau cited that in other municipalities such as the City of Hamilton there have been instances where the municipality would sell a portion of the laneway to adjacent property-owners. If this could be permitted in this instance the variance requests may not be necessary.

## **ANALYSIS, FINDINGS, REASONS**

Mr. Ribau outlined a principled discussion which clearly delineated the rationale for the property-owners proposal to construct a new garage in the orientation and manner as prescribed in the related appeal materials. In conducting a site visit prior to the hearing by the presiding TLAB member, it can be surmised that the laneway is a narrow access way which presents narrow and tight turning radius' for any vehicle traversing it. The image below, as accessed on internet resource Google Maps,

provides a clearer visual representation of the laneway which runs adjacent to the subject property.



Figure 1: location view (source:

<https://www.google.com/maps/place/115+Hannaford+St,+Toronto,+ON+M4E+3G9/@43.6844399,-79.2930417,3a,75y,78.91h,87.29t/data=!3m6!1e1!3m4!1sZOxfqztslwfnHbSWzfjaRw!2e0!7i16384!8i8192!4m5!3m4!1s0x89d4cc1e60f9df65:0xafb20ebb0721d172!8m2!3d43.684526!4d-79.2928395>)

This laneway configuration is a historical condition which the property-owner would have to contend with for any potential alteration to structures on the subject property. Mr. Ribau's explanation that their proposal to have the new detached garage constructed away from the rear property line and shifted further into the rear yard to allow for increased ease of access for vehicles into the laneway so as to minimize damage to their garage could be seen as plausible. However, it should be noted that this proposal would result in an increased visual impact for the neighbouring residents of 115 Hannaford Street when using their rear yard space. In assessing this, in a general overview of the garage and parking typology of the area, there are a variety of detached garages with varying siting and orientations. It is also noted that the adjacent property, 115 Hannaford Street, has no garage structure but a parking pad. These elements demonstrate that there isn't a consistent design for detached garages or parking pads in the area. As such, this proposal would not be introducing a new planning and design dynamic which residents of the area have not encountered previously. Moreover, in terms of constructing structures near or at property lines, there are established building practices which can allow for such structures to be constructed. The applicant would be liaising with the City Building Department to address such measures as per the *Ontario Building Code*. In terms of maintenance of this structure, mutual agreement could be reached between the two adjoining property-owners to ensure the upkeep of the structure is achieved to ensure the previous issues of the older detached garage do not emerge in future.

With respect to Mr. Ribau's comments recommending that the City's Building Department conditions of approval be rescinded, it is noted that the City of Toronto is comprised of several departments and agencies which contain nationally recognized expertise in a variety of disciplines, such as engineering. Bodies such as the TLAB, to a certain extent, rely on such knowledge and experience to inform them in their assessment and considerations of an appeal matter. The memo from the City's Engineering and Constructive Services Division, dated December 19, 2018, states the determinations which City staff have made after reviewing this minor variance proposal. For the TLAB to reverse such comments could result in issues of a municipal interest being compromised. As such, it would be inappropriate for the TLAB to provide an approval of this appeal without taking active consideration and deference to the professional opinion as critiqued by City staff without the satisfactory provision of alternative qualified opinion evidence.

Ms. Hurtado provided information relating to potential issues relating to the fence running along their property line which may not be running along the properly delineated property line, loss of privacy for the use of their rear yard by the proposed detached garage being located in an orientation closer to the main building on the subject property and to the intentions of now including a driveway on the rear of the subject property.

In terms of encroachment issues, the TLAB can only assess the proposal for the detached garage and associated driveway which has been presented to it. The participant Ms. Hurtado or any other interested party should contact the requisite City department if they have concerns about the location of the fence as it relates to the property line. Generally, questions which may arise about the location of the property line between two property-owners is most efficiently addressed by having a new survey prepared by a qualified surveyor. If this proposed garage and driveway were to be permitted, the applicant would need to apply for a building permit whereby related City regulation and rules would need to be complied with. This would include reviewing plans and a possible site visit to ensure the structure being constructed was located on the subject property only.

In terms of the orientation and siting of the proposed detached garage, earlier commentary has established that rear laneway driveway and parking configurations are of a varying nature with each property-owner designing their parking arrangement to meet individual needs, while also presumably acting to comply with related Planning and Engineering requirements. As such, the TLAB does not see a prevailing design character which would be pertinent to conserve.

Due to the dense lotting pattern of the neighbourhood, it is also surmised that residents must live in closer proximity to one another. While privacy is a planning issue to consider, it must also take into context of the location of an area. In a more urban environment, it is anticipated privacy will not be as equitably achieved in comparison to suburban or even rural landscapes.

In terms of the proposed driveway, the design as shown on the applicant's plans show a shorter-than average driveway length and a jog on the corner portion of the

driveway. This design, on *prima facie* review, appears to not be proposed for the parking of a vehicle. In addition, as the principal parking for this property would be in the proposed detached garage; parking on the driveway would not be practical for the property-owner. In addition, there is potential for interference to the rear laneway access if a vehicle were to park there. If this were to occur, City By-law Services or Toronto Police Parking Enforcement could be appropriate agencies to engage to address any concerns.

With the material that has been brought before me, I have chosen to accept the evidence of the applicant and the owner who advocated for the permitting of these variances. The proposal for consideration is a sensible development for the urban neighbourhood being assessed. The proposed detached garage with associated driveway will provide the property-owner with an improved garage structure from the existing one, while also acting to minimize damage to the garage by vehicles turning into the laneway and by setting the garage back from the rear property line.

In terms of privacy, while the neighbouring resident of 115 Hannaford Street will now encounter a new garage structure adjacent to her rear yard, the visual impact would not be as intrusive if it were perpendicular to her main dwelling. Furthermore, the detached garage will not be incompatible with other garage structures orientations in the area.

In terms of construction and maintenance of the garage and driveway, the TLAB invites and encourages both the property-owner and the participant to engage in a dialogue to address their concerns as opposed to relying solely on municipal enforcement services. Good neighbourly relations, while not codified in law, provide the underlying framework for vibrant neighbourhoods in the city.

## **DECISION AND ORDER**

The appeal is allowed, and the variances in Appendix 1 are approved subject to the condition therein and subject to the further condition that the garage building must be constructed substantially in accordance with the plans contained in Appendix 2.

**X** 

---

Justin Leung  
Panel Chair, Toronto Local Appeal Body  
Signed by: Leung, Justin

## Appendix 1

### *List of proposed variances*

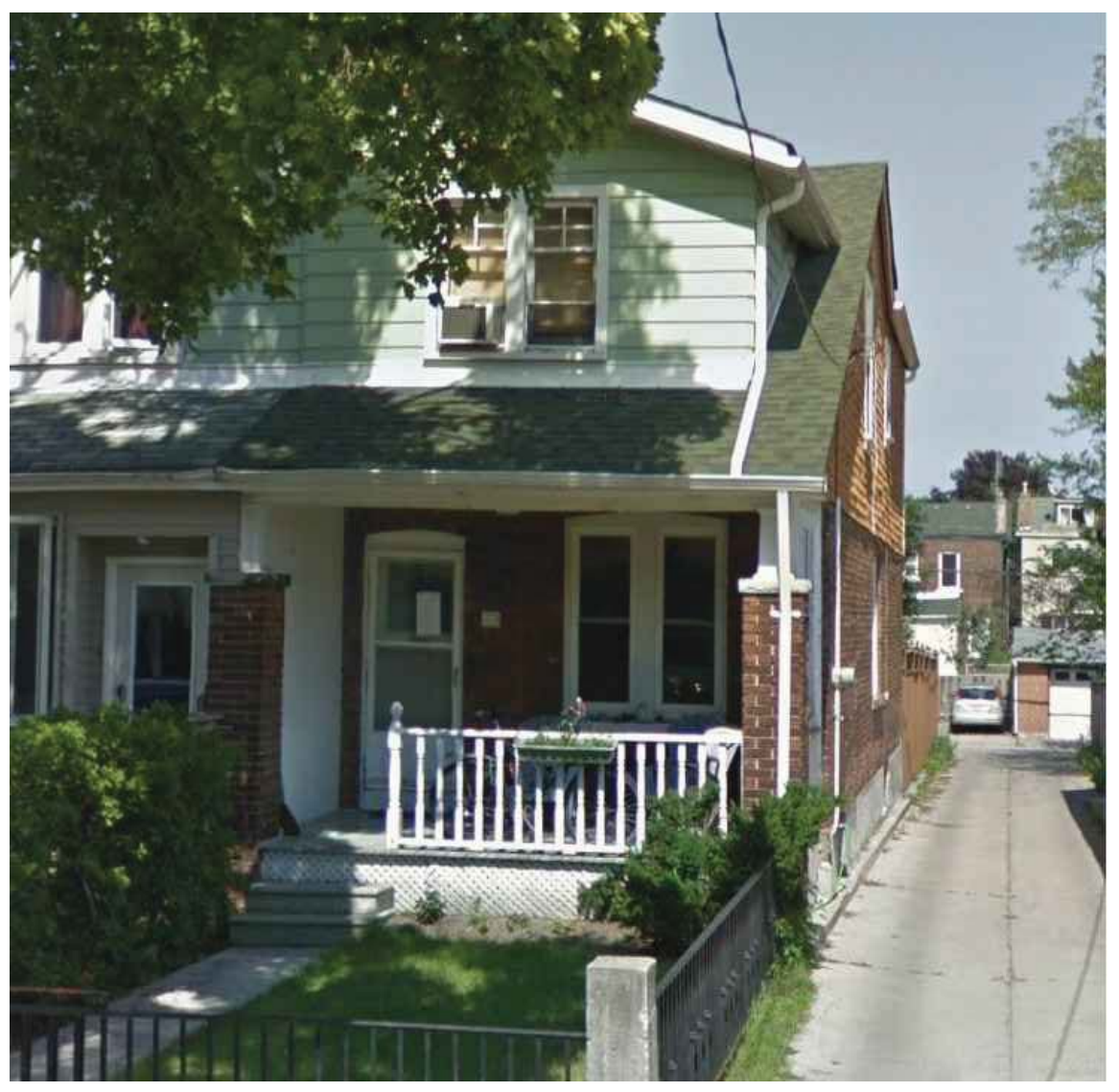
**1. Chapter 10.5.60.20.(6) (A), By-law 569-2013:** The minimum required side yard setback for an ancillary building or structure containing a parking space where a side lot line abuts a lane, and vehicle access to the parking space is from the lane is 1.0 m. In this case, the side yard setback for the ancillary building will be 0.04 m to the south lot line

**2. Chapter 10.5.60.20.(4), by-law 569-2013:** An ancillary building or structure may be no closer than 2.5 m from the original centre-line of a lane. In this case, the ancillary building or structure will be 1.71 m from the original centre-line of a lane on the south side.

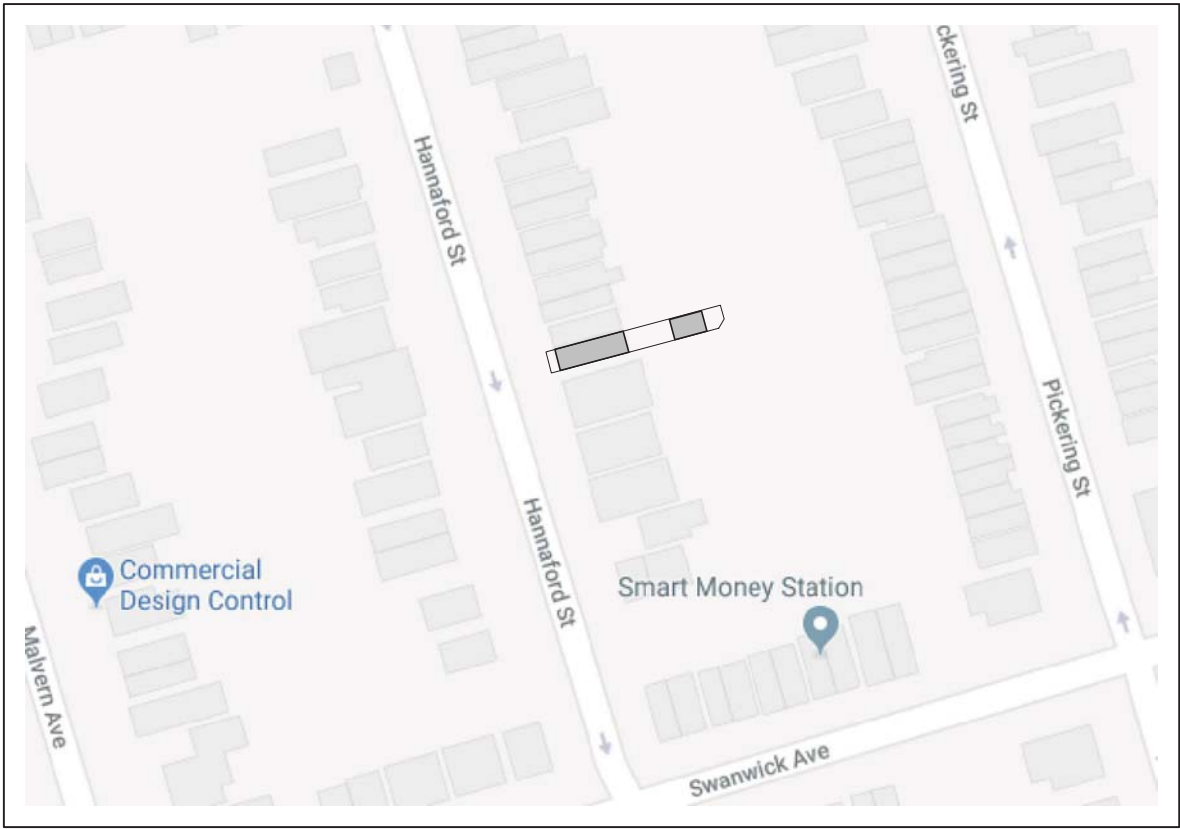
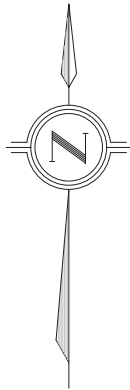
### *List of proposed conditions*

- 1) The proposed garage has to be modified and set back to protect for the 0.82m lane widening. A 0.82m lane widening is required along the southerly portion of this property not encumbered by any of the remaining structure to satisfy the requirement of a 5m wide lane right-of-way (the lane has an approximate width of 3.36m).

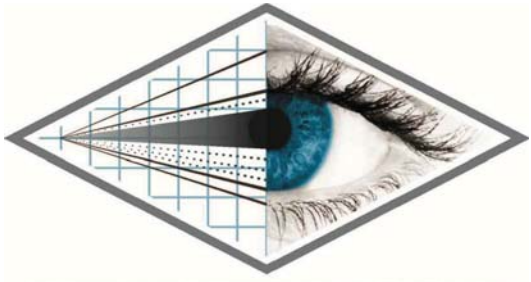




PROJECT STATISTICS					
ADDRESS: 113 HANNAFORD STREET, TORONTO					
LOT AREA		1726.37	ft²	160.39	m²
LOT FRONTAGE		14.46	ft	4.41	m
BUILDING HEIGHT					
HEIGHT TO MID-POINT		6.42	ft	1.96	m
HEIGHT TO HIGHEST RIDGE		12.83	ft	3.91	m
HEIGHT TO UNDERSIDE OF EAVES		8.25	ft	2.51	m
LOT COVERAGE					
		%		EXISTING	
DWELLING FOOTPRINT		33.6	579.34	ft²	53.82 m²
DETACHED GARAGE		13.5	232.54	ft²	21.60 m²
PORCH		6.1	105.86	ft²	9.83 m²
DECK		11.0	189.64	ft²	17.62 m²
TOTAL LOT COVERAGE		64.1	1107.38	ft²	102.9 m²
		%		PROPOSED	
DWELLING FOOTPRINT		33.6	579.34	ft²	53.82 m²
DETACHED GARAGE		17.8	307.08	ft²	28.53 m²
PORCH		6.1	105.86	ft²	9.83 m²
DECK		11.0	189.64	ft²	17.62 m²
TOTAL LOT COVERAGE		68.5	1181.92	ft²	109.80 m²
REAR YARD LANDSCAPING					
		%		AREA	
EXTERIOR REAR YARD AREA		100.0	952.48	ft²	88.49 m²
PROPOSED DETACHED GARAGE		32.2	307.08	ft²	28.53 m²
HARD SURFACED AREA		13.4	128.09	ft²	11.90 m²
DECK		19.9	189.64	ft²	17.62 m²
TOTAL SOFT LANDSCAPED AREA		34.4	327.67	ft²	30.44 m²



KEY PLAN

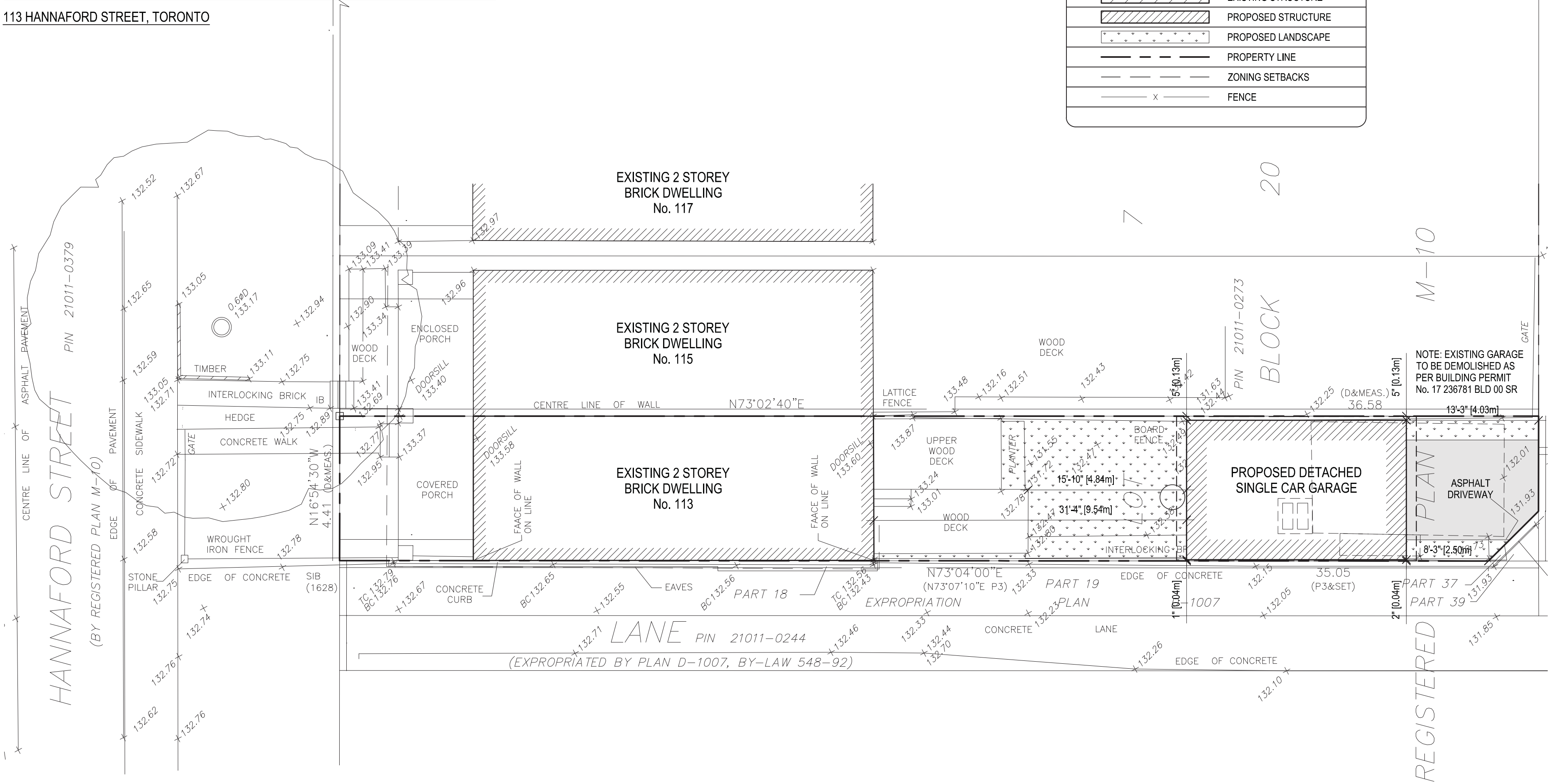


PERSPECTIVE VIEWS  
126 CATHARINE STREET N  
HAMILTON ON L8R 1J4

e. info@perspectiveviews.com p.289.389.4502

GENERAL NOTES:  
DRAWINGS MUST NOT BE SCALED  
DRAWINGS TO BE READ IN CONJUNCTION WITH THE GENERAL NOTES AND THE O.B.C.  
All drawings & information shown on these drawings must be checked & verified on site prior to construction or fabrication of its components.  
Unless noted otherwise, no provision has been made in the design for conditions occurring during construction. It shall be the responsibility of the contractor to provide all necessary bracing, shoring, sheet piling, or other temporary supports, to safeguard all existing or adjacent structures affected by construction.  
All dimensions are given in imperial units unless otherwise indicated.  
All drawings are the property of the Architect / Designer & must be returned upon request.

113 HANNAFORD STREET, TORONTO



The undersigned has reviewed and takes responsibility for this design, and has the qualifications and meets the requirements set out in the Ontario Building Code to be a designer.			
Qualification Information			
Required unless design is exempt under Div. C-3.6.5 of the Building Code			
Jason De Brum	Signature	34031	BCIN
Registration Information			
Required unless design is exempt under Div. C-3.2.5 of the Building Code			
Jason De Brum	Firm Name	39769	BCIN

No.	REVISION	DATE
1	ISSUED FOR DEMOLITION PERMIT	17/09/20
2	ISSUED FOR ZONING REVIEW	18/04/19
3	REISSUED FOR ZONING REVIEW	18/07/13
4	ISSUED FOR BUILDING PERMIT	18/07/17
5	REISSUED FOR BUILDING PERMIT	18/07/30
6	ISSUED FOR MINOR VARIANCE	18/08/13

PROJECT:	FERGUS BARNES DETACHED GARAGE REPLACEMENT 113 HANNAFORD STREET TORONTO, ON M4E 3G9
DRAWING:	SITE PLAN ARCHITECTURAL DRAWINGS
SCALE:	1/8" = 1'0"
DATE:	AUGUST 02, 2017
DRWN BY:	JOBERT SANTOS
CHKD BY:	MATTHEW RIBAU
PROJECT No:	PV17-043
DRAWING No:	SP1.1



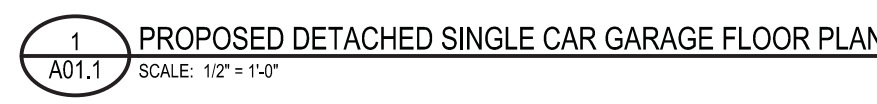


○ GENERAL NOTES:

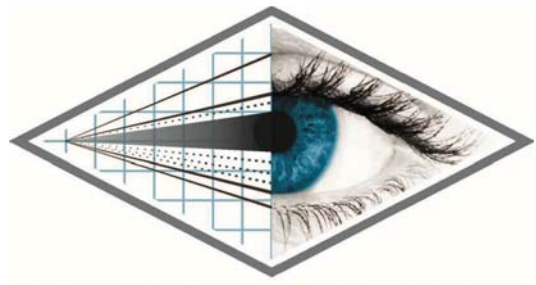
DRAWINGS MUST NOT BE SCALED  
DRAWINGS TO BE READ IN CONJUNCTION WITH THE GENERAL  
NOTES AND THE O.B.C.

Unless noted otherwise, no provision has been made in the design for conditions occurring during construction. It shall be the responsibility of the contractor to provide all necessary bracing, shoring, sheet piling, or other temporary supports, to safeguard all existing or adjacent structures affected by construction.

All drawings are the property of the Architect / Designer & must be returned upon request.

[illegible]

PROJECT:	FERGUS BARNES DETACHED GARAGE REPLACEMENT 113 HAINFORD STREET TORONTO, ON M4E 3G9
DRAWING:	FLOOR PLAN ARCHITECTURAL DRAWINGS
SCALE:	1/2" = 1'0"
DATE:	AUGUST 02, 2017
DRWN BY:	JOBERT SANTOS
CHK'D BY:	MATTHEW RIBAU
PROJECT No:	PV17-043
DRAWING No:	A01.1



**PERSPECTIVE VIEWS**  
126 CATHARINE STREET N  
HAMILTON ON L8R 1J4  
e. info@perspectiveviews.com p.289.389.4502

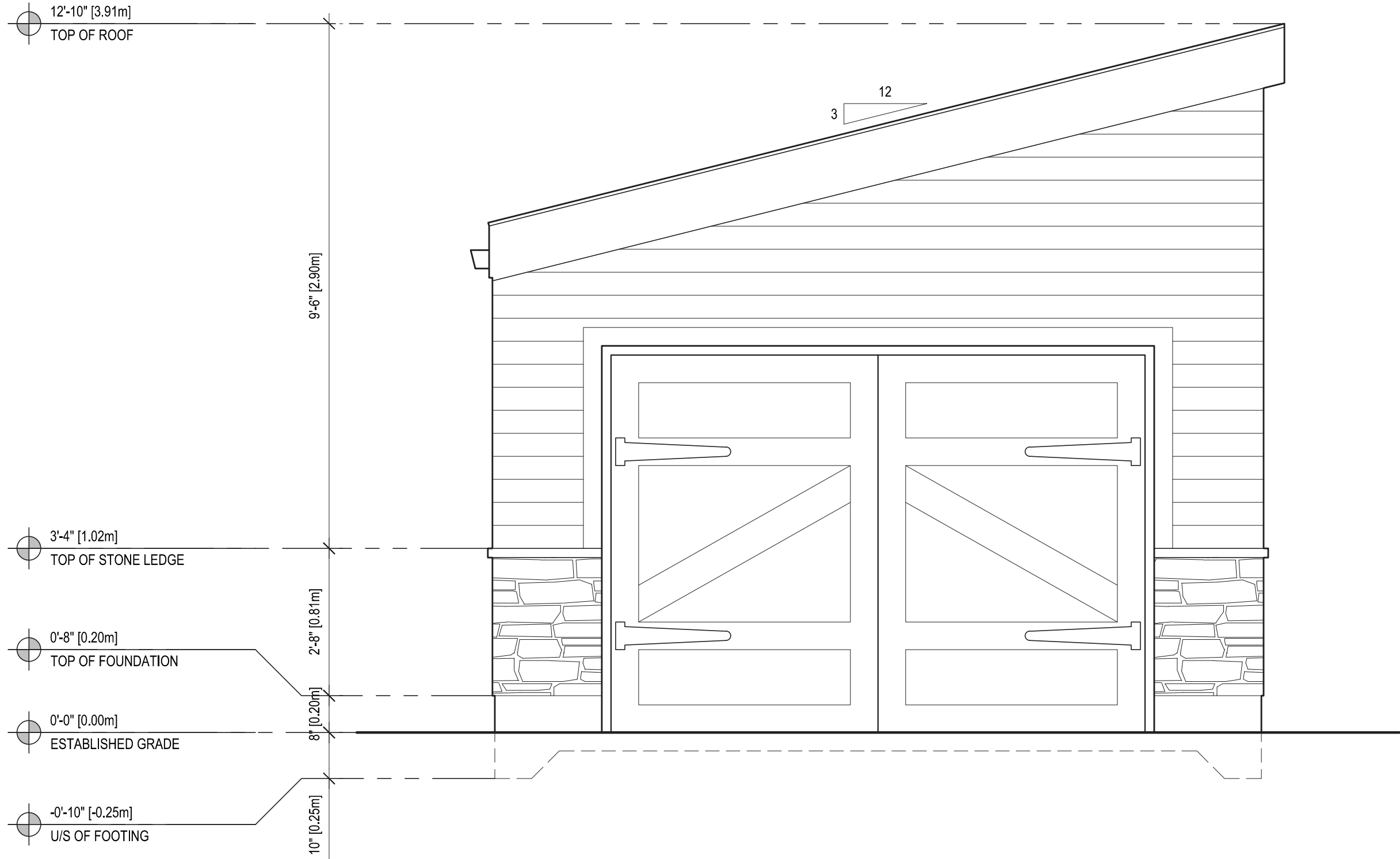
**GENERAL NOTES:**  
DRAWINGS MUST NOT BE SCALED  
DRAWINGS TO BE READ IN CONJUNCTION WITH THE GENERAL NOTES AND THE O.B.C.

All drawings & information shown on these drawings must be checked & verified on site prior to construction or fabrication of its components.

Unless noted otherwise, no provision has been made in the design for conditions occurring during construction. It shall be the responsibility of the contractor to provide all necessary bracing, shoring, sheet piling, or other temporary supports, to safeguard all existing or adjacent structures affected by construction.

All dimensions are given in imperial units unless otherwise indicated.

All drawings are the property of the Architect / Designer & must be returned upon request.



**1** PROPOSED FRONT ELEVATION  
A02.1 SCALE: 1/2" = 1'-0"

The undersigned has reviewed and takes responsibility for this design, and has the qualifications and meets the requirements set out in the Ontario Building Code to be a designer.	
Qualification Information	
Required unless design is exempt under Div. C - 3.6.5 of the Building Code	
Jason De Brum	34031
Name	BCIN
Signature	
Registration Information	
Required unless design is exempt under Div. C - 3.2.5 of the Building Code	
Jason De Brum	39769
Firm Name	BCIN

No.	REVISION	DATE
1	ISSUED FOR DEMOLITION PERMIT	17/09/20
2	ISSUED FOR ZONING REVIEW	18/04/19
3	REISSUED FOR ZONING REVIEW	18/07/13
4	ISSUED FOR BUILDING PERMIT	18/07/17
5	REISSUED FOR BUILDING PERMIT	18/07/30
6	ISSUED FOR MINOR VARIANCE	18/08/13

PROJECT:	FERGUS BARNES DETACHED GARAGE REPLACEMENT 113 HANNAFORD STREET TORONTO, ON M4E 3G9
DRAWING:	ELEVATION ARCHITECTURAL DRAWINGS
SCALE:	1/2" = 1'0"
DATE:	AUGUST 02, 2017
DRWN BY:	JOBERT SANTOS
CHKD BY:	MATTHEW RIBAU
PROJECT No:	PV17-043
DRAWING No:	A02.1



○ GENERAL NOTES:

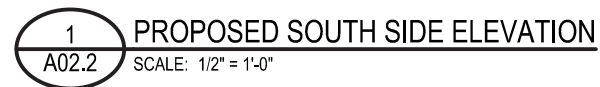
DRAWINGS MUST NOT BE SCALED  
DRAWINGS TO BE READ IN CONJUNCTION WITH THE GENERAL  
NOTES AND THE O.B.C.

All drawings & information shown on these drawings must be checked & verified on site prior to construction or fabrication of its components.

Unless noted otherwise, no provision has been made in the design for conditions occurring during construction. It shall be the responsibility of the contractor to provide all necessary bracing, shoring, sheet piling, or other temporary supports, to safeguard all existing or adjacent structures affected by construction.

All dimensions are given in imperial units unless otherwise indicated.

All drawings are the property of the Architect / Designer & must be returned upon request.

[illegible]

PROJECT: FERGUS BARNES  
DETACHED GARAGE REPLACEMENT  
113 HANNAFORD STREET  
TORONTO, ON M4E 3G9

○ DRAWING: ELEVATION  
ARCHITECTURAL DRAWINGS

○ SCALE:  $1/2" = 1'0"$

○ DATE: AUGUST 02, 2017

○ DRW'N BY: JOBERT SANTOS

○ CHK'D BY: MATTHEW RIBAU

PROJECT No: PV17-043

DRAWING No: A02.2

e. info@perspectiveviews.com p.289.389.4502

GENERAL NOTES:

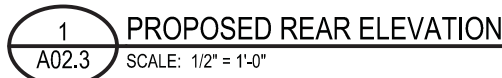
DRAWINGS MUST NOT BE SCALED  
DRAWINGS TO BE READ IN CONJUNCTION WITH THE GENERAL  
NOTES AND THE O.B.C.

All drawings & information shown on these drawings must be checked & verified on site prior to construction or fabrication of its components.

Unless noted otherwise, no provision has been made in the design for conditions occurring during construction. It shall be the responsibility of the contractor to provide all necessary bracing, shoring, sheet piling, or other temporary supports, to safeguard all existing or adjacent structures affected by construction.

All dimensions are given in imperial units unless otherwise indicated.

All drawings are the property of the Architect / Designer & must be returned upon request.

[illegible]

PROJECT: FERGUS BARNES  
DETACHED GARAGE REPLACEMENT  
113 HANNAFORD STREET  
TORONTO, ON M4E 3G9

 DRAWING: ELEVATION  
ARCHITECTURAL DRAWINGS

SCALE: 1/2" = 1'0"

○ DATE: AUGUST 02, 2017

○ DRW'N BY: JOBERT SANTOS

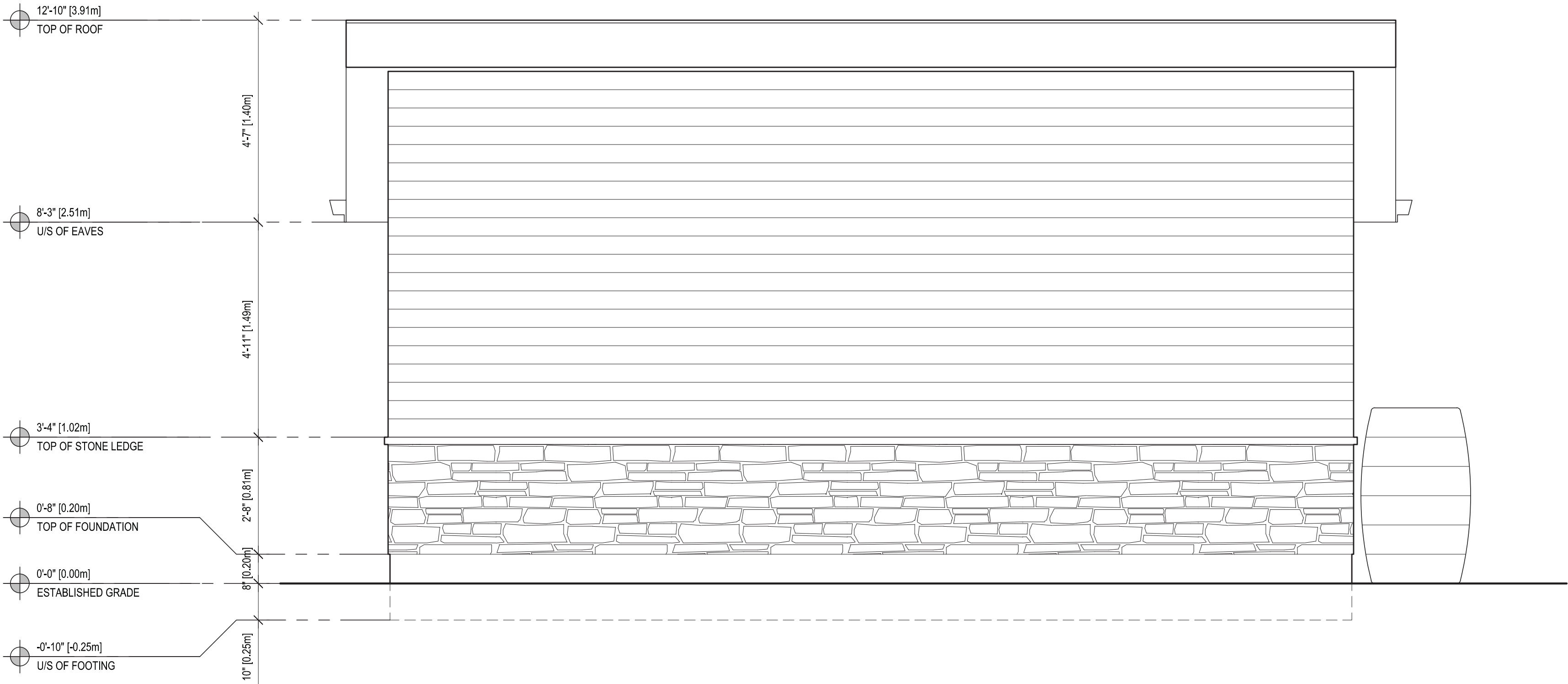
○ CHK'D BY: MATTHEW RIBAU

PROJECT No: PV17-043

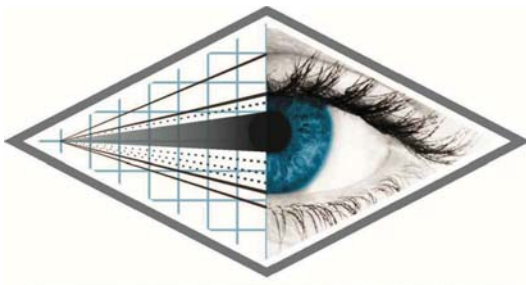
DRAWING No: A02.3

C:\Users\Matthew Riba\OneDrive - Perspective Views Inc\Projects\PV17-043 - Fergus Barnes - 113 Hannaford St\Drawings\113 Hannaford St (Building Permit).dwg, 2018-08-13 7:05:20 PM

FILE NAME:



1  
A02.4 PROPOSED NORTH SIDE ELEVATION  
SCALE: 1/2" = 1'-0"



**PERSPECTIVE VIEWS**  
126 CATHARINE STREET N  
HAMILTON ON L8R 1J4  
e. info@perspectiveviews.com p.289.389.4502

GENERAL NOTES:  
DRAWINGS MUST NOT BE SCALED  
DRAWINGS TO BE READ IN CONJUNCTION WITH THE GENERAL NOTES AND THE O.B.C.  
All drawings & information shown on these drawings must be checked & verified on site prior to construction or fabrication of its components.  
Unless noted otherwise, no provision has been made in the design for conditions occurring during construction. It shall be the responsibility of the contractor to provide all necessary bracing, shoring, sheet piling, or other temporary supports, to safeguard all existing or adjacent structures affected by construction.  
All dimensions are given in imperial units unless otherwise indicated.  
All drawings are the property of the Architect / Designer & must be returned upon request.

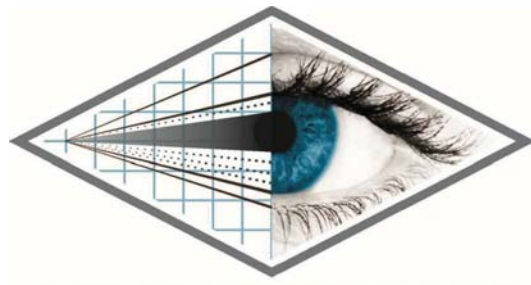
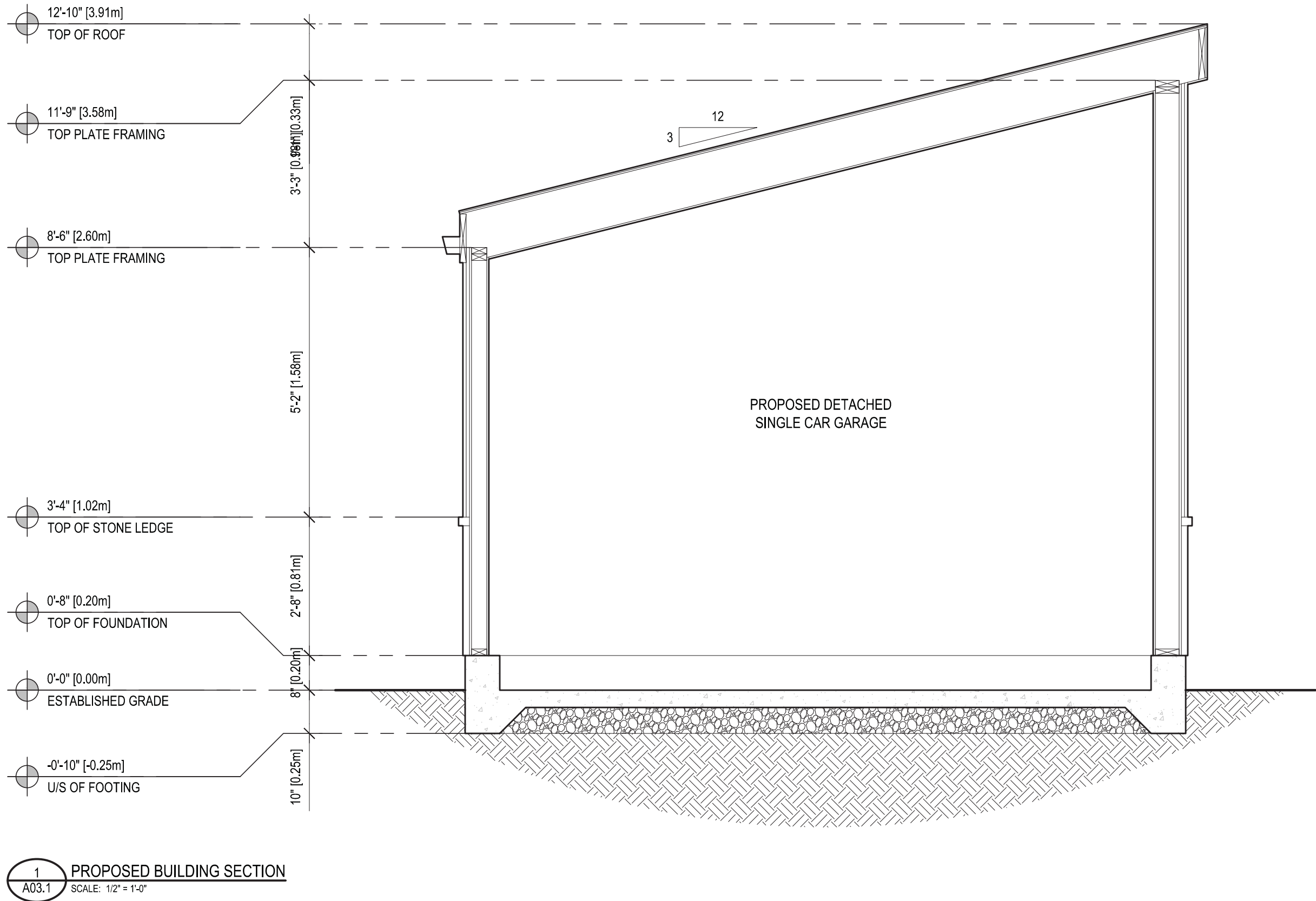
The undersigned has reviewed and takes responsibility for this design, and has the qualifications and meets the requirements set out in the Ontario Building Code to be a designer.

Qualification Information  
Required unless design is exempt under Div. C - 3.6.5 of the Building Code  
Name Jason De Brum 34031 BCIN  
Signature  
Registration Information  
Required unless design is exempt under Div. C - 3.2.5 of the Building Code  
Name Jason De Brum 39769 BCIN  
Firm Name

No.	REVISION	DATE
1	ISSUED FOR DEMOLITION PERMIT	17/09/20
2	ISSUED FOR ZONING REVIEW	18/04/19
3	REISSUED FOR ZONING REVIEW	18/07/13
4	ISSUED FOR BUILDING PERMIT	18/07/17
5	REISSUED FOR BUILDING PERMIT	18/07/30
6	ISSUED FOR MINOR VARIANCE	18/08/13

PROJECT: FERGUS BARNES  
DETACHED GARAGE REPLACEMENT  
113 HANNAFORD STREET  
TORONTO, ON M4E 3G9  
DRAWING: ELEVATION  
ARCHITECTURAL DRAWINGS  
SCALE: 1/2" = 1'0"  
DATE: AUGUST 02, 2017  
DRWN BY: JOBERT SANTOS  
CHKD BY: MATTHEW RIBAU  
PROJECT No: PV17-043  
DRAWING No: A02.4





**PERSPECTIVE VIEWS**  
126 CATHARINE STREET N  
HAMILTON ON L8R 1J4

e. info@perspectiveviews.com p.289.389.4502

GENERAL NOTES:

DRAWINGS MUST NOT BE SCALED  
DRAWINGS TO BE READ IN CONJUNCTION WITH THE GENERAL  
NOTES AND THE O.B.C.

All drawings & information shown on these drawings must be checked &  
verified on site prior to construction or fabrication of its components.

Unless noted otherwise, no provision has been made in the design for  
conditions occurring during construction. It shall be the responsibility of  
the contractor to provide all necessary bracing, shoring, sheet piling, or  
other temporary supports, to safeguard all existing or adjacent structures  
affected by construction.

All dimensions are given in imperial units unless otherwise indicated.

All drawings are the property of the Architect / Designer & must be  
returned upon request.

The undersigned has reviewed and takes responsibility for this design, and has the qualifications and meets the requirements set out in the Ontario Building Code to be a designer.		
Qualification Information		
Required unless design is exempt under Div. C - 3.2.5 of the Building Code		
Jason De Brum	Signature	34031
Name		BCIN
Registration Information		
Required unless design is exempt under Div. C - 3.2.5 of the Building Code		
Jason De Brum		39769
Firm Name		BCIN

No.	REVISION	DATE
1	ISSUED FOR DEMOLITION PERMIT	17/09/20
2	ISSUED FOR ZONING REVIEW	18/04/19
3	REISSUED FOR ZONING REVIEW	18/07/13
4	ISSUED FOR BUILDING PERMIT	18/07/17
5	REISSUED FOR BUILDING PERMIT	18/07/30
6	ISSUED FOR MINOR VARIANCE	18/08/13

PROJECT:	FERGUS BARNES DETACHED GARAGE REPLACEMENT 113 HANNAFORD STREET TORONTO, ON M4E 3G9
DRAWING:	BUILDING SECTION ARCHITECTURAL DRAWINGS
SCALE:	1/2" = 1'0"
DATE:	AUGUST 02, 2017
DRWN BY:	JOBERT SANTOS
CHKD BY:	MATTHEW RIBAU
PROJECT No:	PV17-043
DRAWING No:	A03.1