

---

# Section G.4:

## Area A Heritage Impact Assessment, Postal Delivery Building / Scotiabank Arena

Note: This appendix refers to Area A as Focus Area 1 and to Area B as Focus Area 2, a reflection of previous project nomenclature.



# **REP-060- WATERFRONT EAST LRT UNION STATION - QUEENS QUAY LINK**

**Heritage Impact Assessment, Postal Delivery  
Building/Scotiabank Arena, 40 Bay Street,  
City of Toronto, Ontario**

Project # OISO52004

Prepared for:

**Toronto Transit Commission**  
1900 Yonge Street, Toronto, ON M4S 1Z2

OISO52004-TEM-001 R0  
Waterfront East LRT Union Station  
- Queens Quay Link Concept  
Design Review Submission



### Document Revision History

Version	Date	Description
R0	July 23, 2021	Released for TTC review.

Prepared by:	<b>Luke Fischer, MA, CAHP</b> <b>Cultural Heritage Specialist</b>	Date:
		July 23, 2021
Reviewed by:	<b>Heidy Schopf, MES, CAHP</b> <b>Cultural Heritage Team Lead</b>	Date:
		July 23, 2021



## Executive Summary

The Toronto Transit Commission (TTC) is undertaking Preliminary Design and Engineering (PDE) to produce a Baseline Design (approximately 30% design completion) of a new TTC Waterfront East Light Rail Transit (WELRT) system (the Project). The WELRT, goes under Bay Street, from Front Street to Queens Quay West, and then heads easterly on Queens Quay West. A new portal on Queens Quay West between Bay Street and Yonge Street will be constructed to provide a transition from an underground Light Rail Transit (LRT) to an at-grade LRT, which will complement the existing west portal and at-grade LRT on Queens Quay West, west of Bay Street.

The WELRT will service Toronto's waterfront revitalization area by providing fast, reliable transit service in the East Bayfront (EBF) Area of the Waterfront. The expansion of the Union LRT and Queens Quay LRT Stations is required to accommodate the additional streetcar lines and passenger volume. This project is critical to the new waterfront transit plan in the EBF Precinct.

WSP E&I Canada Limited (WSP) (formerly Wood Environment & Infrastructure Solutions) was retained by TTC to complete a Heritage Impact Assessment (HIA) for the former Postal Delivery Building, now known as the Scotiabank Arena, located at 40 Bay Street, Toronto, Canada in support of the WELRT. This HIA represents one deliverable to support program delivery. This HIA is being carried out under the Transit Project Assessment Process (TPAP) and was prepared in accordance with the MCM guidance document titled *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties* (MCM 2017).

The purpose of this HIA is to establish the Statement of Cultural Heritage Value for Scotiabank Arena (40 Bay Street), assess the existing conditions of the property, describe the purpose of the proposed activity, complete an impact assessment and outline considered alternatives and mitigation measures, provide a summary of community engagement, and development recommendations for the conservation of the property.

Based on the above, the following recommendations are made:

1) Design Guidelines:

- a. The impacted area within 40 Bay Street functions as part of the public realm within the Union Station HCD. Accordingly, the design guidelines for the Union Station HCD pertaining to the public realm should be followed when considering replacement of this material (Section 2.2.1.2) (ERA Architects Inc. 2006: 64-65). Compatibility with the surrounding area should be considered.

2) Protection Measures:

- a. The proposed work for the WELRT project is largely located below grade and involves open cut excavation, the demolition of the existing streetcar loop, and construction of the new loop/platform along Bay Street from Front Street West to 40 Bay Street. Open cut construction will occur within





the former Postal Delivery Building/Scotiabank Arena property. Protection measures in the form of protective barriers and notes on construction drawings should be implemented during the construction phase of the project to protect this building. Protective fencing, film, or netting should be installed around the base of the historical north and east facades of the Postal Delivery Building in the vicinity of the proposed work to protect the exterior of the building from accidental damage during construction.

- b. The heritage status of the Scotiabank Arena should be noted on project drawings to communicate the status of this property to project personnel. The note should read:

“The former Postal Delivery Building (now Scotiabank Arena) (40 Bay Street) is a significant heritage property with protection under Parts IV and V of the *Ontario Heritage Act*. The north, east, and south facades of 40 Bay Street form the historical exterior of the Postal Delivery Building and are protected heritage attributes of the property. Use caution when conducting work in the vicinity of this building to avoid accidental damage.”
  - c. Construction work is proposed in the immediate vicinity of the historical building exterior of the former Postal Delivery Building that introduces vibration-related risk. Vibration monitoring should be carried out by a qualified geotechnical engineer and should consist of a pre-construction survey, vibration monitoring during construction, and a post-construction survey.
- 3) This report should be submitted to the following agencies for review and comment:
- a. City of Toronto Heritage Preservation Services
  - b. MCM

The above recommendations were prepared using the best available information regarding potential impacts at the time of writing. Should the proposed work change, then the preliminary impact assessment should be revisited to confirm identified impacts and proposed mitigation measures.



## Table of Contents

	Page
Executive Summary.....	i
Table of Contents .....	iii
List of Appendices .....	iv
<b>1.0 Introduction.....</b>	<b>1</b>
1.1 Description and Location of the Property .....	1
1.1.1 Heritage Recognition.....	1
1.1.2 Property Ownership .....	2
1.2 Proposed Undertaking and Purpose .....	2
1.3 Potential Impacts to Cultural Heritage Resources .....	6
1.4 Overview of Impacts and Mitigation Measures for the Dominion Public Building.....	6
1.4.1 Summary of Potential Impacts .....	6
1.4.2 Summary of Recommended Mitigation Measures.....	7
<b>2.0 Statement of Cultural Heritage Value .....</b>	<b>11</b>
2.1 Postal Delivery Building (40 Bay Street) .....	11
2.1.1 Statement of Cultural Heritage Value.....	11
2.1.2 Heritage Attributes .....	12
2.2 Union Station Heritage Conservation District.....	12
<b>3.0 Assessment of Existing Conditions .....</b>	<b>16</b>
3.1 Property Context.....	16
3.2 Postal Delivery Building/Scotiabank Arena .....	20
3.3 Area of Proposed Intervention .....	25
<b>4.0 Description and Purpose of Proposed Activity .....</b>	<b>30</b>
<b>5.0 Impact Assessment.....</b>	<b>33</b>
5.1 Potential Impacts to the Postal Delivery Building/Scotiabank Arena.....	33
5.2 Potential Impacts to Union Station HCD .....	36
5.3 Discussion of Anticipated Impacts .....	37
<b>6.0 Considered Alternatives and Mitigation Measures.....</b>	<b>38</b>
6.1 Discussion of Mitigation Measures .....	40
6.1.1 Conservation Guidance.....	40
6.1.2 Design Guidelines .....	41
6.1.3 Protection Measures .....	42
<b>7.0 Summary of Community Engagement.....</b>	<b>44</b>
<b>8.0 Recommendations .....</b>	<b>50</b>
<b>9.0 Assessor Qualifications.....</b>	<b>52</b>
<b>10.0 Closure .....</b>	<b>53</b>
<b>11.0 References .....</b>	<b>54</b>

## List of Figures

Figure 1: Location of the Study Area .....	9
Figure 2: Aerial Photograph Showing Location of the Study Area.....	10
Figure 3: Site Context of the Postal Delivery Building .....	28
Figure 4: Location of Heritage Attributes for the Postal Delivery Building .....	29



### **List of Tables**

Table 1: Assessment of Potential Direct Adverse Impacts .....	33
Table 2: Assessment of Potential Indirect Impacts.....	35
Table 3: Assessment of Potential Positive Impacts .....	36
Table 4: Mitigation Measures .....	38
Table 5: Summary of Community Engagement.....	45

### **List of Appendices**

Appendix A: Designation By-law for the Postal Delivery Building
Appendix B: Proposed Work
Appendix C: Conservation Guidelines
Appendix D: Record of Community Engagement
Appendix E: Assessor Qualifications
Appendix F: Limitations



### List of Acronyms and Abbreviations

BHR	Built Heritage Resource
CHL	Cultural Heritage Landscape
CHR	Cultural Heritage Report: Existing Conditions and Impact Assessment
CHVI	Cultural Heritage Value or Interest
HCD	Heritage Conservation District
HIA	Heritage Impact Assessment
MCM	Ministry of Citizenship and Multiculturalism
O. Reg.	Ontario Regulation
PPS	Provincial Policy Statement
TTC	Toronto Transit Commission

### Glossary

Adjacent lands	Those lands contiguous to a protected heritage property or as otherwise defined in the municipal official plan (Government of Ontario 2020).
Built Heritage Resource	Means a building, structure, monument, installation or any manufactured or constructed part or remnant that contributes to a property's cultural heritage value or interest as identified by a community, including an Indigenous community. Built heritage resources are located on property that may be designated under Parts IV or V of the <i>Ontario Heritage Act</i> , or that may be included on local, provincial, federal and/or international registers (Government of Ontario 2020).
Conserved	Means the identification, protection, management and use of built heritage resources, cultural heritage landscapes and archaeological resources in a manner that ensures their cultural heritage value or interest is retained. This may be achieved by the implementation of recommendations set out in a conservation plan, archaeological assessment, and/or heritage impact assessment that has been approved, accepted or adopted by the relevant planning authority and/or decision maker. Mitigative measures and/or alternative development approaches can be included in these plans and assessments (Government of Ontario 2020).
Cultural Heritage Landscape	Means a defined geographical area that may have been modified by human activity and is identified as having cultural heritage value or interest by a community, including an



## Glossary

	<p>Indigenous community. The area may include features such as buildings, structures, spaces, views, archaeological sites or natural elements that are valued together for their interrelationship, meaning or association. Cultural heritage landscapes may be properties that have been determined to have cultural heritage value or interest under the <i>Ontario Heritage Act</i>, or have been included on federal and/or international registers, and/or protected through official plan, zoning by-law, or other land use planning mechanisms (Government of Ontario 2020).</p>
Heritage Attributes	<p>Means the principal features or elements that contribute to a protected heritage property's cultural heritage value or interest, and may include the property's built, constructed, or manufactured elements, as well as natural landforms, vegetation, water features, and its visual setting (e.g. significant views or vistas to or from a protected heritage property) (Government of Ontario 2020).</p>
Protected Heritage Property	<p>Means property designated under Parts IV, V or VI of the <i>Ontario Heritage Act</i>; property subject to a heritage conservation easement under Parts II or IV of the <i>Ontario Heritage Act</i>; property identified by the Province and prescribed public bodies as provincial heritage property under the Standards and Guidelines for Conservation of Provincial Heritage Properties; property protected under federal legislation, and UNESCO World Heritage Sites (Government of Ontario 2020).</p>
Provincial Heritage Property	<p>Means real property, including buildings and structures on the property, that has cultural heritage value or interest and that is owned by the Crown in right of Ontario or by a prescribed body; or that is occupied by a ministry or prescribed body if the terms of the occupancy agreement are such that the ministry or public body is entitled to make the alterations to the property that may be required under these heritage standards and guidelines (MCM 2017).</p>
Provincial Heritage Property of Provincial Significance	<p>Means provincial heritage property that has been evaluated using the criteria found in Ontario Heritage Act O. Reg. 10/06 and has been found to have cultural heritage value or interest of provincial significance (MCM 2017).</p>
Significant	<p>In regard to cultural heritage and archaeology, resources that have been determined to have cultural heritage value or interest. Processes and criteria for determining cultural heritage value or interest are established by the Province under the authority of the <i>Ontario Heritage Act</i> (Government of Ontario 2020).</p>



## Glossary

Statement of Cultural Heritage Value	Means a concise statement explaining why a property is of heritage interest; this statement should reflect one or more of the criteria found in <i>Ontario Heritage Act</i> O. Regs. 9/06 and 10/06 (MCM 2017).
--------------------------------------	---



## 1.0 Introduction

### 1.1 Description and Location of the Property

WSP E&I Canada Limited (WSP) (formerly Wood Environment & Infrastructure Solutions) was retained by the Toronto Transit Commission (TTC) to conduct a Heritage Impact Assessment (HIA) for the former Postal Delivery Building, now Scotiabank Arena at 40 Bay Street (the Study Area) as part of Preliminary Design and Engineering (PDE) to produce a Baseline Design (approximately 30% design completion) of a new TTC Waterfront East Light Rail Transit (WELRT) system (the Project). For the purpose of this report, the Study Area will be referred to as the “Postal Delivery Building”. The Study Area is depicted in **Figure 1** and **Figure 2**.

The Scotiabank Arena at 40 Bay Street was originally known as the Postal Delivery Building and is designated on architectural and historical grounds. The Postal Delivery Building was constructed in 1939-40 according to the designs of Toronto architect Charles Dolphin and Public Works architect C.D. Sutherland. The building served as the central mail distribution centre in Toronto for the Canadian Postal Service.

The building, constructed of limestone and polished granites, is distinguished by its horizontal bands of fenestration and its bas relief sculpture, characteristic of the Art Moderne and Art Deco styles. Set on an irregular site at the intersection of Bay Street and Lake Shore Boulevard West, the original east and south elevations are treated in a similar manner and represent all the heritage attributes contributing to the Cultural Heritage Value or interest (CHVI) of the property. In the centre of each of these walls, multiple fenestration is inset between stone piers, while the rounded corners of the building feature two bands of wraparound metal windows. The principal entrances are located at the southeast northeast corner. Attention is focused on the east elevation with the name band and bronze Canadian coat-of-arms. The stylized sculptural program reflects the history of communication and transportation in Canada through a progressive series of corner panels. Architecture associated with the modern adaptive-reuse of the structure into a major arena characterises the north, west, and part of the south elevation of the current structure, this portion of the building is discussed in Section 2.1 and 3.0 of this report.

The Postal Delivery Building is a significant public commission, designed by a local architect in conjunction with the Department of Public Works. The sculptural program is one of the most extensive in the City of Toronto, demonstrating the role of the federal government as a patron of the arts.

#### 1.1.1 Heritage Recognition

The Postal Delivery Building has known CHVI at the municipal level:

- Municipal Recognition
  - Designated under Part V of the *Ontario Heritage Act* as a contributing building within the Union Station HCD under By-Law 634-2006
  - Individually Designated under Part IV of the *Ontario Heritage Act* under By-Law 360-90





### 1.1.2 Property Ownership

The former Postal Delivery Building, now Scotiabank Arena, is located at 40 Bay Street and partially owned by Metrolinx (PIN 213960127) and 3642968 Canada Inc (PIN 213960095). The property includes the Scotiabank Arena and associated areas that function as the public realm.

## 1.2 Proposed Undertaking and Purpose

The TTC is undertaking Preliminary Design and Engineering (PDE) to produce a Baseline Design (approximately 30% design completion) of a new TTC WELRT system (the Project). The WELRT, goes under Bay Street, from Front Street to Queens Quay West, and then heads easterly on Queens Quay West. A new portal on Queens Quay West between Bay Street and Yonge Street will be constructed to provide a transition from an underground Light Rail Transit (LRT) to an at-grade LRT, which will complement the existing west portal and at-grade LRT on Queens Quay West, west of Bay Street.

The WELRT will service Toronto's waterfront revitalization area by providing fast, reliable transit service in the East Bayfront (EBF) Area of the Waterfront (**Plate 1**). The expansion of the Union LRT and Queens Quay LRT Stations is required to accommodate the additional streetcar lines and passenger volume. This project is critical to the new waterfront transit plan in the EBF Precinct.



The planning for the Project began in 2010 when the East Bayfront Transit Class

### **Plate 1: Overview of the Waterfront Transit Network**

Please reach out to the Project Team should you require alternative text for this image.

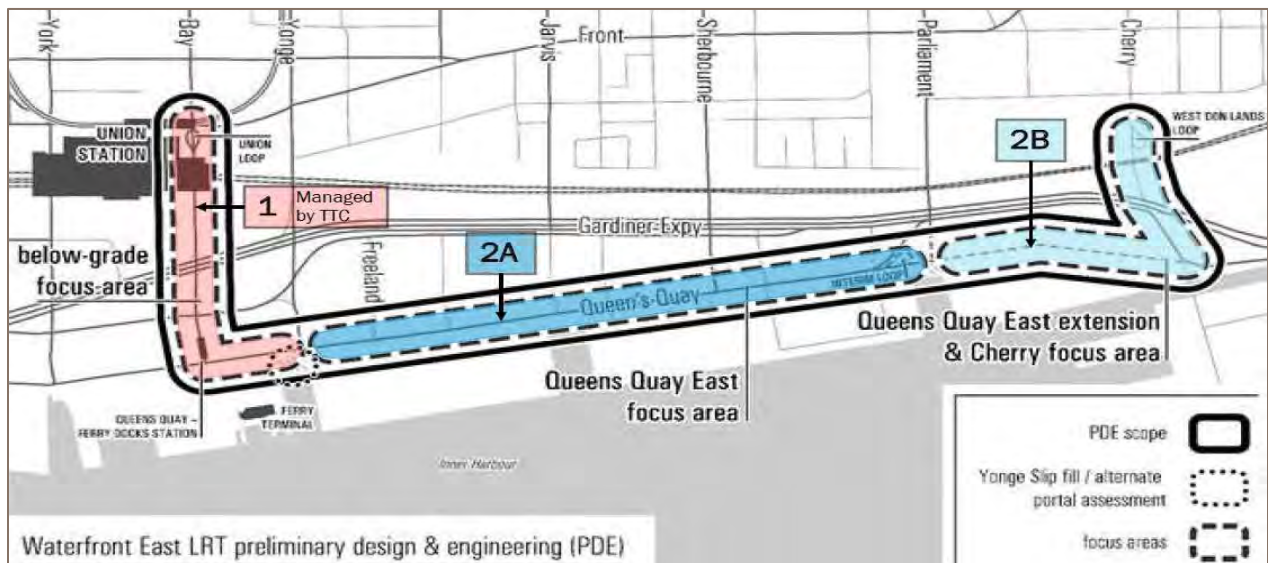
Environmental Assessment (EA) Study was carried by Waterfront Toronto, who is the proponent for all redevelopment activities in the East Bayfront Area. The Consultant, MRC, produced the draft Environmental Study Report in August 2009. The Engineering Department of TTC carried out the Conceptual 10% Design of the project, producing a final Conceptual Design Report in February 2010. The project has progressed to the current Preliminary Design and Engineering stage.





The overall scope of work to be completed for the Project includes, but is not limited to, Focus Area 1 and Focus Area 2. An overview of these Project Areas is provided in **Plate 2**.

1. Focus Area 1 - Managed by TTC - Below Grade (Union Station Loop to future Portal east of Bay Street on Queens Quay), which includes:
  - a. Union LRT Station Expansion, including new crossover tracks; Queen Quay LRT Station Expansion;
  - b. New Streetcar tunnel and portal structures along Queens Quay between Bay Street and Yonge Street; and
  - c. Track works within the tunnel and portal structures.
2. Focus Area 2 - Managed by Waterfront Toronto:
  - a. 2A: Queens Quay East (Future Portal to Parliament vicinity ancillary Queens Quay surface/public realm between Bay & future portal).
  - b. 2B (Provisional): Queens Quay East Extension & Cherry (Parliament vicinity to West Don Lands Loop).



**Plate 2: Focus Area 1 and Focus Area 2**

Please reach out to the Project Team should you require alternative text for this image.



WSP E&I Canada Limited (WSP) (formerly Wood Environment & Infrastructure Solutions) scope of work pertains to Focus Area 1 only and includes a collaborative effort among the City of Toronto, the TTC, and Waterfront Toronto. WSP's overall scope of work includes PDE services to provide a baseline design (30% design), a level 3 cost estimate for the expansion of the existing Union LRT and Queens Quay LRT Stations, and new running tunnel and portal as part of WELRT project. In particular, the main scope items include:

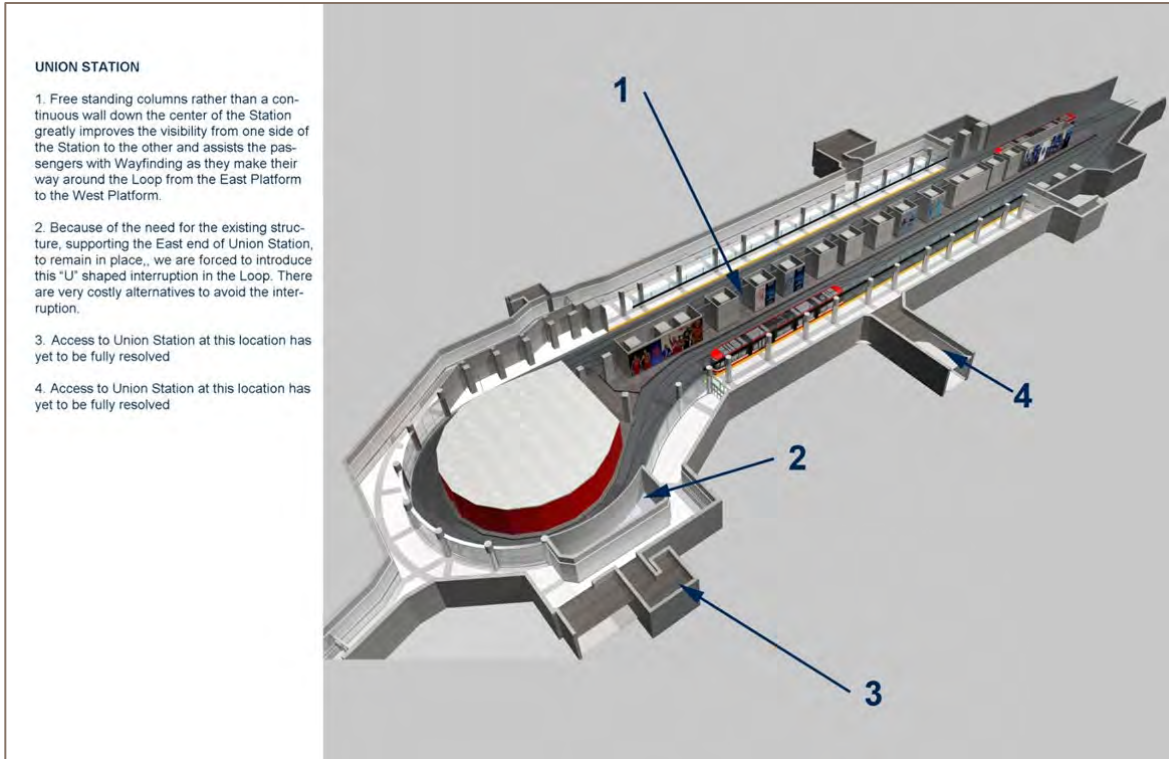
1. Union Station LRT Loop Expansion to accommodate up to four (4) new platforms, including new crossover tracks;
2. Queens Quay Station Expansion with up to two (2) extended platforms;
3. New streetcar tunnel and portal structures along Queens Quay between Bay Street and Yonge Street;
4. Track works within the tunnel and portal structures; and
5. Design interface and coordination with the work of Focus Area 2 and adjacent projects (public and private) along project limits.

Subject to further funding approval and a procurement options analysis, a contract amendment may be issued to extend the term of the contract and the consultant may be requested to carry out the detailed design and construction support services or develop Reference Concept Design (RCD) and Project Specific Output Specifications (PSOS) for this project.

The phases of WSP's overall scope of work are as follows:

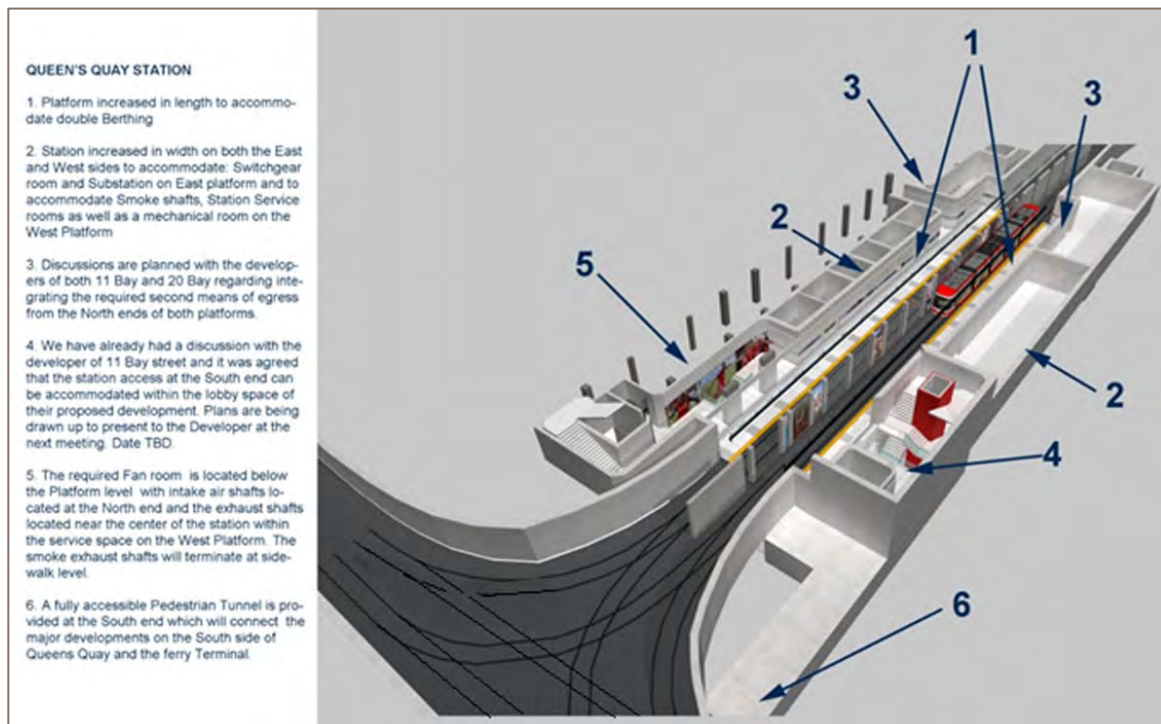
1. Phase 1 – Work Plan (OISO52004-PLN-001 Phase 2 Work Plan);
2. Phase 2a - Concept Design Review Submission (CDRS) (approximately 15%);  
and,
3. Phase 2b - Baseline Design Review Submission (BDRS) (approximately 30%).

From the layouts indicated in Appendix B of the *City of Toronto, Union Station - Queens Quay Transit Link Study Final Report* (April 2019), further developments were made to progress the design to consider the latest requirements and current standards. A summary of the current station layouts are provided in **Plate 3** and **Plate 4**.



**Plate 3: Union Station -CDRS Proposed Configuration**

Please reach out to the Project Team should you require alternative text for this image.



**Plate 4: Queens Quay Station - CDRS Proposed Configuration**

Please reach out to the Project Team should you require alternative text for this image.



### 1.3 Potential Impacts to Cultural Heritage Resources

As described above, the project consists of the design of a new TTC WELRT system that goes under Bay Street, from Front Street to Queens Quay West, and then heads easterly on Queens Quay West. A new portal on Queens Quay West between Bay Street and Yonge Street will be constructed to provide a transition from an underground LRT to an at-grade LRT, which will complement the existing west portal and at-grade LRT on Queens Quay West, west of Bay Street.

The proposed WELRT is located within the Union Station HCD and adjacent to, or in close proximity to, an additional 13 known and potential cultural heritage properties. A *Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment* (CHR) prepared for this project determined that direct impacts are anticipated to Union Station (65-71 Front Street West), the Dominion Public Building (1 Front Street West), the Postal Delivery Building (40 Bay Street), and the public realm of the Union Station HCD. Presently, Strategic Conservation Plans (SCP) are not available for these properties.

Given that there is proposed work within these heritage properties and SCPs are not available, HIAs are required. The purpose of the HIAs is to inform decisions that may affect the property while ensuring the conservation and protection of heritage attributes.

This report consists of the HIA for the Postal Delivery Building/Scotiabank Arena (40 Bay Street). HIAs have been prepared for Union Station (65-71 Front Street West), Dominion Public Building (1 Front Street West), and the Union Station HCD under separate covers. All four HIAs has been prepared in accordance with *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties* (MCM 2017).

### 1.4 Overview of Impacts and Mitigation Measures for the Dominion Public Building

A detailed impact assessment was prepared as part of this HIA. The impact assessment is presented in Section 5.0 and mitigation measures are presented in Section 6.0 of this report. An overview of the findings is presented below.

#### 1.4.1 Summary of Potential Impacts

The detailed impact assessment determined that the proposed development does not include extensive alterations or additions to the building itself. The work proposed at 40 Bay Street includes demolition of the existing sidewalk to facilitate excavation in the vicinity of the Scotiabank Arena Bay Street entrance and adjacent to the east elevation of the structure. This work will be undertaken in close proximity to the historical façade of the original Postal Delivery Building facade along Bay Street, which is a heritage attribute of this property.

This construction activity is anticipated to introduce vibration-related risk on the property. Access will be temporarily restricted access to the structure due to the presence of construction staging areas, open excavations, and fencing or hoarding limiting access to the area in the interest of public safety.





Drawings of the proposed work depict property acquisition in the vicinity of the Scotiabank Arena Bay Street entrance and work in close proximity to the structure along the entire Bay Street east elevation. Demolition is indicated in this area to accommodate excavation required for the subsurface construction of the WELRT streetcar loop along Bay Street.

#### 1.4.2 Summary of Recommended Mitigation Measures

Mitigation measures are proposed to address impacts resulting from the proposed work on within the Postal Delivery Building (40 Bay Street). Recommended mitigation measures include:

##### 1) Design Guidelines:

- a. The impacted area within 40 Bay Street functions as part of the public realm within the Union Station HCD. Accordingly, the design guidelines for the Union Station HCD pertaining to the public realm should be followed when considering replacement of this material (Section 2.2.1.2) (ERA Architects Inc. 2006: 64-65). Compatibility with the surrounding area should be considered.

##### 2) Protection Measures:

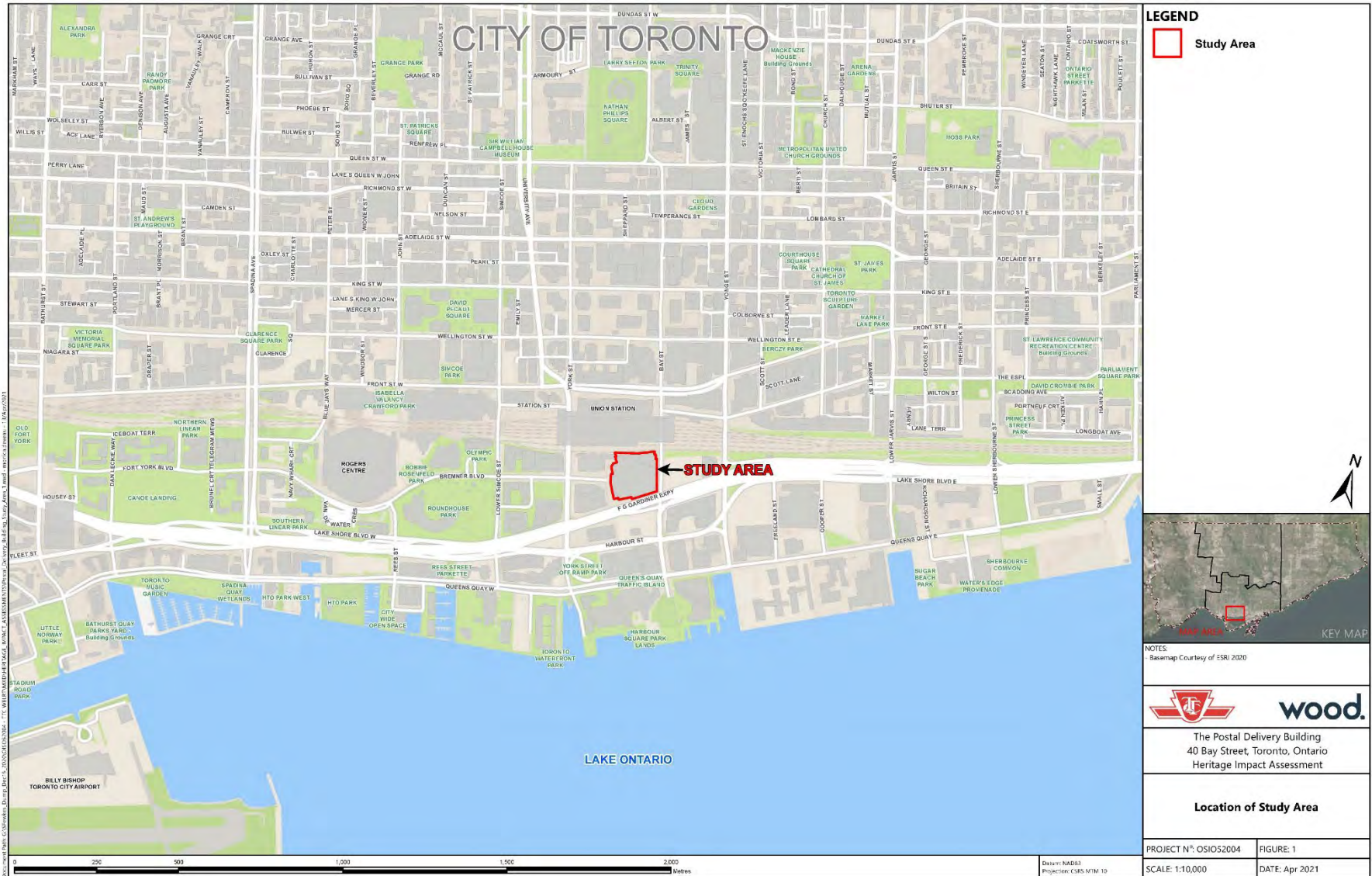
- a. The proposed work for the WELRT project is largely located below grade and involves open cut excavation, the demolition of the existing streetcar loop, and construction of the new loop/platform along Bay Street from Front Street West to 40 Bay Street. Open cut construction will occur within the former Postal Delivery Building/Scotiabank Arena property. Protection measures in the form of protective barriers and notes on construction drawings should be implemented during the construction phase of the project to protect this building. Protective fencing, film, or netting should be installed around the base of the historical north and east facades of the Postal Delivery Building in the vicinity of the proposed work to protect the exterior of the building from accidental damage during construction.
- b. The heritage status of the Dominion Public Building should be noted on project drawings to communicate the status of this property to project personnel. The note should read:

“The former Postal Delivery Building (now Scotiabank Arena) (40 Bay Street) is a significant heritage property with protection under Parts IV and V of the *Ontario Heritage Act*. The north, east, and south facades of 40 Bay Street form the historical exterior of the Postal Delivery Building and are protected heritage attributes of the property. Use caution when conducting work in the vicinity of this building to avoid accidental damage.”

- c. Construction work is proposed in the immediate vicinity of the historical building exterior of the former Postal Delivery Building that introduces vibration-related risk. Vibration monitoring should be carried out by a qualified geotechnical engineer and should consist of a pre-construction



survey, vibration monitoring during construction, and a post-construction survey.



**Figure 1: Location of the Study Area**

Please reach out to the Project Team should you require alternative text for this image.





**Figure 2: Aerial Photograph Showing Location of the Study Area**  
Please reach out to the Project Team should you require alternative text for this image.





## **2.0 Statement of Cultural Heritage Value**

The Postal Delivery Building (40 Bay Street) is designated under Part IV of the *Ontario Heritage Act* through [By-law 360-90](#) and under Part V of the *Ontario Heritage Act* as a 'Contributing Building' in the Union Station HCD through [By-law 634-2006](#) (ERA Architects Inc. 2006). The Statement of Cultural Heritage Value or interest is provided below in Section 2.1.1. A description of the property is provided below.

The Scotiabank Arena located at 40 Bay Street, formerly known as the Postal Delivery Building, is situated in a tight site thoroughly connected to the surrounding urban landscape. The property is bounded by the rail corridor on the north, Bay Street on the east, the Gardiner Expressway/Lakeshore Boulevard West on the south, and Bremner Boulevard/25 York Street on the west. The historical Postal Delivery Building has been rehabilitated and incorporated into the Scotiabank Area (formerly known as the Air Canada Centre), a multi-purpose area that is the home of the Toronto Raptors National Basketball Association (NBA) team and the Toronto Maple Leafs National Hockey League (NHL) team. The rehabilitation took place in 1996 and resulted in the demolition of the Postal Delivery Building with the exception of the east elevation, south elevation, and part of the north elevation, which were retained and incorporated into the new arena development. Effectively, the east and south elevations present as the historical Postal Delivery Building while the west and north elevations present as the Scotiabank Arena.

The north elevation of the structure once fronted Harbour Street, which has since been closed. Today the north elevation consists of an indoor concourse connecting to the Union Station Complex (65-71 Front Street West). The east elevation along Bay Street has multiple entrance gates and retains the original façade associated with the Postal Delivery Building. The south elevation fronts Lakeshore Boulevard West, its eastern portion retains the original facade while the west portion is a modern addition related to the adaptive re-use of the structure into a major sports arena which was undertaken in 1996. The west elevation of the structure once fronted Lorne Street, which has since been closed. This elevation now fronts Raptors Way and an open courtyard known as Maple Leaf Square. This elevation is built in a modern style exhibiting an irregular plan comprised mainly of glass, metal, and vibrant signage. The open space west of the structure is embellished with sculpture and has a cavern like quality created by the imposing modern structures located throughout the Union Station HCD and Financial District located north of the HCD. The public courtyard is accessible from Scotiabank Arena, York Street, Lakeshore Boulevard West, and the Union Station Complex.

### **2.1 Postal Delivery Building (40 Bay Street)**

#### **2.1.1 Statement of Cultural Heritage Value**

The property at 40 Bay Street (Postal Delivery Building) is designated on architectural and historical grounds. The Postal Delivery Building was constructed in 1939-40 according to the designs of Toronto architect Charles Dolphin and Public Works architect C.D. Sutherland. The building served as the central mail distribution centre in Toronto for the Canadian Postal Service.



The building, constructed of limestone and polished granite, is distinguished by its horizontal bands of fenestration and its bas relief sculpture, characteristic of the Art Moderne and Art Deco styles. Set on an irregular site at the intersection of Bay Street and Lake Shore Boulevard West, the elevations are treated in a similar manner. In the centre of each wall, multiple fenestration is inset between stone piers, while the rounded corners of the building feature two bands of wraparound metal windows. The principal entrances flank the southeast corner. Attention is focused on the east elevation with the name band and bronze Canadian coat-of-arms. The stylized sculptural program reflects the history of communication and transportation in Canada through a progressive series of corner panels.

The Postal Delivery Building is a significant public commission, designed by a local architect in conjunction with the Department of Public Works. The sculptural program is one of the most extensive in the City of Toronto, demonstrating the role of the federal government as a patron of the arts.

### **2.1.2 Heritage Attributes**

The designation By-law does not include a list of heritage attributes. However, based on the Statement of Cultural Heritage Value, it can be surmised that the heritage attributes include, but are not limited to:

- Limestone and polished granite building materials
- Horizontal bands of fenestration
- Bas relief sculpture, characteristic of Art Moderne and Art Deco styles
- Sculptural program that reflects the history of communication and transportation in Canada through a progressive series of corner panels
- East and south elevations with multiple fenestration between stone piers
- Rounded corners of the building
- Two bands of wraparound metal windows
- Principal entrance of the southeast corner
- Name band and Canadian coat-of-arms on the east elevation

### **2.2 Union Station Heritage Conservation District**

The Postal Delivery Building (40 Bay Street) falls within the Union Station HCD and is identified as a 'contributing building' of the district. The Union Station Heritage Conservation District (HCD) was designated in 2006 through [By-law 634-2006](#) (City of Toronto 2006; Ontario Heritage Trust 2020). The Union Station HCD is located between Toronto's Financial District, Entertainment District, historical St. Lawrence Neighborhood, and the post-industrial waterfront. The Union Station HCD Plan is available [online](#) and includes an in depth analysis of the history, heritage character, district policies, municipal policies, implementation guidance, and design guidelines (ERA Architects Inc 2006).



The boundaries of the Union Station HCD coincide with Wellington Street West to the north, Yonge Street to the east, and Lakeshore Boulevard West/Harbour Street to the south. The east boundary of the HCD is defined by Simcoe Street north of the rail corridor and Reese Street south of the rail corridor. The Union Station HCD consists of an assemblage of buildings, open spaces and streets that have a collective interdependent history. Intersecting development and planning initiatives associated with Toronto's railway lands, waterfront and central business district, historic and monumental architecture; as well as physical patterns of interrelated function are the key heritage attributes that comprise the heritage character of the HCD (ERA Architects Inc. 2006).

The Union Station HCD Plan contains design guidelines for contributing buildings, non-contributing buildings, new construction, adjacent properties, and the public realm (ERA Architects Inc. 2006: 57-65).

The HCD Plan describes contributing buildings as properties that contribute to the character of the district and/or are historically, architecturally or culturally significant as identified in the heritage evaluation or determined by further evaluation. Non-contributing buildings are defined as properties that do not contribute to the character of the district and/or are not historically, architecturally or culturally significant as identified in the Heritage Evaluation or determined by further evaluation (ERA Architects Inc. 2006: 58).

### 2.2.1.1 Union Station HCD Guidelines for Contributing Buildings

Design guidelines for contributing buildings in the Union station HCD are as follows:

**Additions and Alterations:** Additions and alterations may be approved, depending on their impact within the district. Additions that are not prominently visible – especially from Union Station, will generally be approved. Those that are visible will be evaluated on a case-by-case basis. Additions will be evaluated by the following criteria:

- The new structure respects the general size, shape and scale of features associated with the property of district.
- The site plan respects the general site characteristics associated with the property or district.
- The design respects the general historic and architectural characteristics associated with the property or district.
- The material choice respects the existing character of the property and district as a whole. Material choice not directly emulating what exists will be contextual and appropriate.
- Any addition is to be connected to the property in a way that does not alter, change, obscure, damage, or destroy any significant building features.
- Additions, renovations and alterations that enhance the character of the district, and are compatible with the overall planning goals of the district will be encouraged, yet subject to thorough review.



**Demolition:** The demolition involving any contributing building in the district will only be approved after thorough review in accordance with the *Ontario Heritage Act*.

(ERA Architects Inc. 2006: 60)

### 2.2.1.2 Guidelines for the Public Realm

Section 8.3.5 of the Union Station HCD Plan provides direction for the development of the public realm within the district. As outlined in the HCD Plan, the public realm provides a stage for the daily life of the city, comprising gathering spaces such as parks, public squares, streets, path networks, and the interior of malls. The design and functional aspects of the public realm, such as sidewalks, streetscapes, and boulevards, provide public spaces with both form and a sense of place (ERA Architects Inc. 2006: 64-65). The HCD Plan notes that enhancing connectivity and maintaining open spaces through public realm can promote the heritage character of the HCD. Similarly, the consolidation of and simplification of streetscape elements in the district, including paving, curbs, tree grates, signage, base plants, vertical elements, and lighting has a strong impact on the general quality and understanding of the public realm.

The design guidelines for the public realm in the Union Station HCD are as follows:

**Promote Heritage Character:** All aspects of the public realm need to recognize the heritage character of the district. Lighting should be used to emphasize building forms at night, in a manner representative of the grandeur of the architecture. Historical precedents in planning around Union Station, such as the John Lyle Plan of 1911, should be used to inform the importance of establishing connections to the south of the district. Historical connections, both visual and physical, should be maintained and enhanced, such as the view of the Royal York Hotel and the physical connections between the John Street Roundhouse and Union Station. The moats, teamways, and bridges of Union Station should be better utilized as important linkages between areas of the district.

**Aspire to Highest Standards of Design Excellence:** Streetscape elements should be of high-quality design and enduring materials that are appropriate to the district's historic character. The approach to design and materials used should reflect good contemporary design to emphasize the district's evolving character. Designs using inauthentic historical pastiche, for example mock-Victorian, should be avoided because they look backwards, and when executed using modern techniques do not have the quality of craft of the original.

**Streetscape furniture should fit into the landscape rather than be its focus:** Street furniture should have high standards of functionality, durability, environmental performance, and visual attractiveness. Materials such as stainless steel should be used because they wear well and do not require continual maintenance. Pedestrian comfort should be encouraged by considering and supporting pedestrian flow, needs of elderly, visually impaired, etc.



**Maintain open spaces:** Open spaces, such as Roundhouse Park, serve critical functions by helping to maintain the environmental quality of the district, in addition to providing a calm gathering space. Open spaces should be properly protected and maintained.

**Reduce clutter:** In order to reduce clutter on the streetscape, the size and number of objects like waste and recycling receptacles and newspaper boxes should be reduced. For example, the three-unit garbage receptacle should be reconsidered as three separate components. Consolidated newspaper boxes should be used to replace the banks of 10-20 boxes chained together.

**Coordinate Design Implementation and Maintenance:** The overall design of the public realm should be carried out in a consistent and well-coordinated manner to ensure that design measures complement each other and work towards enhancing the district's identity.

(ERA Architects Inc. 2006: 64-65)



## 3.0 Assessment of Existing Conditions

### 3.1 Property Context

The former Postal Delivery Building, now Scotiabank Arena, located at 40 Bay Street is situated within the Union Station HCD. Multiple heritage structures are located in the vicinity of the Postal Delivery Building, including the Union Station Complex (65-71 Front Street West), the Dominion Public Building (1 Front Street West), and the Toronto Harbour Commission Building (60 Harbour Street) (**Plate 5 to Plate 7**) (**Figure 3**). These three buildings are the closest contributing buildings within the Union Station HCD. These structures are all associated with the Beaux Arts aesthetic that characterizes much of the northern portion of the HCD. The east and south elevations of the Postal Delivery Building are constructed of limestone like these other nearby structures, although it is distinguished by its Art Deco and Art Moderne architectural characteristics and contemporary additions. The Union Station HCD is partially characterized by the eclectic nature of the built heritage within it that represents an evolving metropolitan environment. This is accentuated by its central location between the Financial District to the north, St. Lawrence HCD to the east, the Entertainment District to the west, and Toronto Harbour to the south. There are buildings in the area, outside of the HCD, with a similar Art Deco and Art Moderne aesthetic such as the Terminal Building located at 207-211 Queens Quay, although the uniqueness of the Postal Delivery Building within the HCD serves to illustrate how it helps characterize the varied metropolitan landscape of the Union Station HCD (**Plate 8**).

The former Postal Delivery Building is thoroughly connected to the surrounding landscape via street entrance gates, teamways from Union Station, raised teamways from other structures, and public spaces (**Plate 9**). The building shares an indoor concourse with Union Station which includes commercial space, an entry gate to the arena, access from Union Station, and access from a raised teamway from east of Bay Street (**Plate 10** and **Plate 11**). The south elevation of the building fronts Lakeshore Boulevard West on street level and the elevated Gardiner Expressway also runs parallel along this elevation (**Plate 13**). The west elevation of the building has a public courtyard known as Maple Leaf Square where the arena is accessible via York Street, Union Station, Raptors Way, and Lakeshore Boulevard West (**Plate 15**). The character of the Union Station HCD is defined in part by its interconnectedness and prevalence of transportation infrastructure. The concourse on the northern elevation of the structure connecting it with Union Station in addition to multiple teamways allowing entry and exit from the structure and a public courtyard facilitating circulation to and from the structure exemplifies this defining characteristic of the HCD.





**Plate 5: View of the Union Station Complex, facing southeast from Front Street West and York Street**



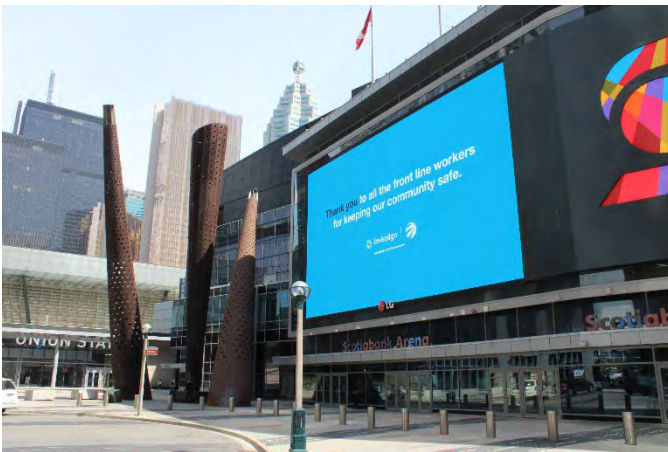
**Plate 6: View of the Dominion Public Building, facing southwest.**



**Plate 7: View of the Toronto Harbour Commission Building, facing north.**



**Plate 8: View of the Terminal Building, facing south.**



**Plate 9: View of Maple Leaf Square located along the west elevation of Scotiabank Arena, facing northeast.**



**Plate 10: View of concourse leading from Union Station into the commercial area at Scotiabank Arena, facing south.**





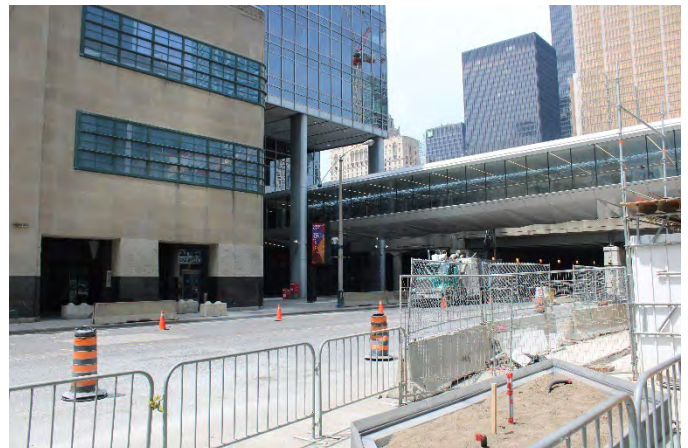
**Plate 11: View of the indoor concourse that links Union Station and Scotiabank Arena. Note that this wall represents the southern elevation of Union Station, but that artifacts salvaged from the former northern elevation of the Postal Delivery Building are displayed.**



**Plate 12: Oblique view of the Scotiabank Arena from a raised teamway leading to the concourse that connects the arena to Union Station. Note that the historical east and north elevations of the Postal Delivery Building dominate this view.**



**Plate 13: View of the Scotiabank Arena looking north from Bay Street. This view shows the proximity of the Gardiner Expressway to the structure.**



**Plate 14: View of the north corner of the east elevation of Scotiabank Arena showing the teamway leading to the concourse between Union Station and the arena, facing northwest. Note that high rises associated with the Financial District can be seen in the background.**



**Plate 15: View of Maple Leaf Square showing where an entrance to the Union Station Complex empties into the square, facing north.**



**Plate 16: View of the west elevation of Scotiabank Arena showing teamways that connect infrastructure in the area to the arena, facing southwest.**

### **3.2 Postal Delivery Building/Scotiabank Arena**

Presently, the former Postal Delivery Building, now Scotiabank Arena, is set on an irregular site at the intersection of Bay Street and Lakeshore Boulevard West. The building includes a combination of modern architectural components and a remnant facade of the original Postal Delivery Building located at this site. The Postal Delivery Building was constructed in 1939-40 in Art Deco and Art Moderne architectural style and the façade of this original structure remains intact along the east and south elevations of the structure. A section of the original north elevation remains in situ at the northeast corner of the building. The locations of these heritage attributes is provided in **Figure 4**. The remainder of the building has undergone considerable change and expansion relating to undergoing adaptive reuse as a major sports arena in 1996. An extended portion of the south façade, and the entire east façade is now composed primarily of glass with metal finishing and large vibrant modern signage, while the northern elevation consists of an indoor concourse shared with the Union Station Complex. The height of the entire building has been raised to accommodate stadium seating inside the structure.

The east and south elevations have similar characteristics and contain all the heritage attributes comprising the CHVI of this structure. A small section of the north elevation also includes part of the original building exterior. These elevations retain the original Art Deco and Art Moderne architectural characteristics associated with the Postal Delivery Building. Stylized curved corners are present along both elevations, which exhibit two bands of horizontal wraparound windows with metal finishing. Decorative carvings with Canadian themes are present above the wrap around windows along the curved corners accenting the top of the limestone façade (**Plate 17 to Plate 19**). These bas relief carvings feature beavers and other symbols of Canada (**Plate 19 and Plate 23**). The east and south wall both have a horizontal bronze fenestration running the majority of the length of the elevation symmetrically above tall prominent vertical windows with metal finishing. This bronze fenestration is currently topped with a





corresponding bronze name band reading “Scotiabank Arena”. (**Plate 17, Plate 18, and Plate 21**). Circular coats of arms set in raised limestone relief on either side of the fenestration on both elevations (**Plate 17, Plate 18, and Plate 23**). These original facades have dark marble applied to approximately the bottom 1 ½ metres of the elevations, which accents the limestone façade (**Plate 20, Plate 22, and Plate 23**). There is a recessed covered walkway along the east elevation of the structure, limestone piers mark the areas between the access openings to the walkway and under the large vertical windows. These limestone piers are adorned bas relief sculptures of a progressive motif featuring historical themes in transportation throughout Canada. These bas relief sculptures on the limestone piers continue throughout the entire length of the original east and south elevations (**Plate 20 and Plate 22**). It should be noted that the west half of the south elevation of the buildings is a modern extension and does not contain cultural heritage attributes relating to the CHVI of the structure (**Plate 24**).

The west portion of the south elevation and the entirety of the west elevation are modern additions to the original Postal Delivery Building. These modern additions relate to the adaptive re-use of the structure as an arena undertaken in 1996. The west portion of the south elevation is composed of a flat glass wall with metal finishing topped with vibrant modern signage (**Plate 24**). The eastern elevation of the structure has an irregular plan with an asymmetrical design that including a multitude of vertical and horizontal angles. It is composed of modern materials including glass and metal (**Plate 25 and Plate 26**). The west portion of the south elevation and the entirety of the west elevation exhibit large vibrant modern signage, this signage when coupled with the use of glass and modern asymmetrical design serves to create an obvious juxtaposition between the modern portions of the structure and the heritage portions of the structure. The drastic differentiation between the older sections of the building and the newer sections is tempered slightly by the restrained but noticeable use of limestone ashlar with marble trim in some areas of the east elevation (**Plate 16**). The inclusion of these materials within a predominately modern section helps to create a cohesive structure while maintaining a legible distinction between eras of construction.

The north elevation of 40 Bay Street, which once fronted Harbour Street, is now largely an indoor concourse shared with the Union Station Complex. Pieces from the original elevation have been salvaged and displayed in the indoor concourse and some limestone ashlar is used on the indoor walls hearkening back to the elevations original form as part of the Postal Delivery Building (**Plate 27 and Plate 29**). However, the concourse is now primarily composed of exposed concrete and is primarily a modern design with some Art Deco details providing an integration between the largely modern concourse and the heritage attributes of the east and south elevations of the structure (**Plate 30**).

It should be noted that the additional height added to the building due to its adaptation into an arena is visible from all elevations but is set back and legibly distinct from the heritage façade along the east elevation and the east portion of the south elevation of the structure (**Plate 17 and Plate 24**).



**Plate 17: View of the east elevation of 40 Bay Street, looking northwest.**



**Plate 18: View of the south elevation of 40 Bay Street, facing west-northwest. Note the proximity to the Gardiner Expressway (left).**



**Plate 19: Southwest view of the wrap around windows, bas relief carving, and a portion of the fenestration at the north corner of the east elevation.**



**Plate 20: View of bas relief sculpture on a limestone pier near the southern end of the east elevation**

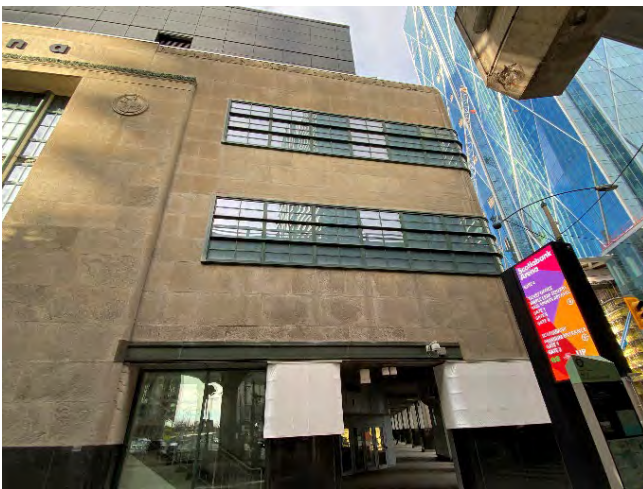




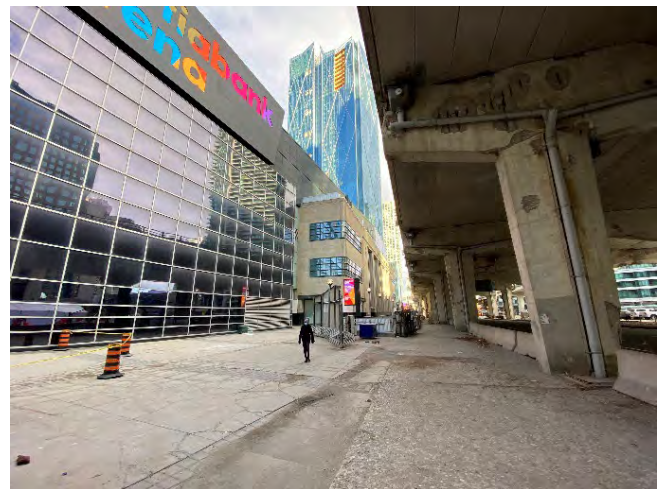
**Plate 21: View of the large vertical windows situated along the east and west elevation of the structure. Note the fenestration in the metal finishing of the windows.**



**Plate 22: View of bas relief sculpture on the south elevation of structure. Note that this sculpture marks the point along the south elevation where the original façade ends.**



**Plate 23: View of the southeast corner of the structure showing the warp around windows, coat of arms, and marble inlay along the bottom of the elevation.**



**Plate 24: View of south elevation of the structure facing east. This shows the modern structure in the foreground and the original façade in the background.**



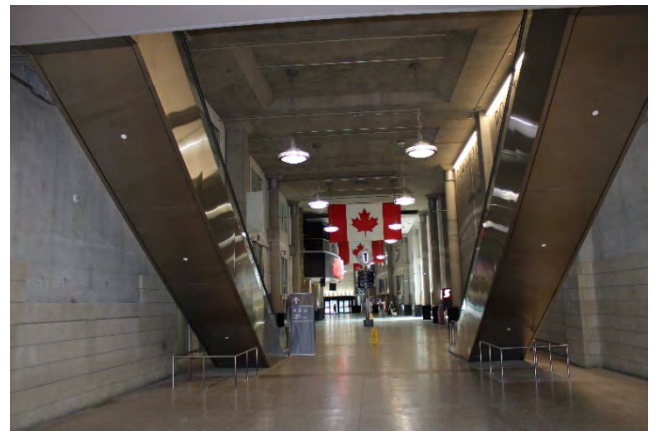
**Plate 25: View of the west elevation of 40 Bay Street, facing east. This shows Maple Leaf Square in the foreground.**



**Plate 26: View of the west elevation of 40 Bay Street, facing southeast. This shows the modern irregular design, vibrant signage, and sculptural embellishment located along the west elevation.**



**Plate 27: View of the concourse comprising the north elevation of 40 Bay Street, facing west. This shows the commercial space and public access.**



**Plate 28: View of the concourse that comprises the north elevation of 40 Bay Street, facing west. This shows escalators leading down from a raised teamway entrance. This also shows use of ashlar limestone in the concrete concourse.**





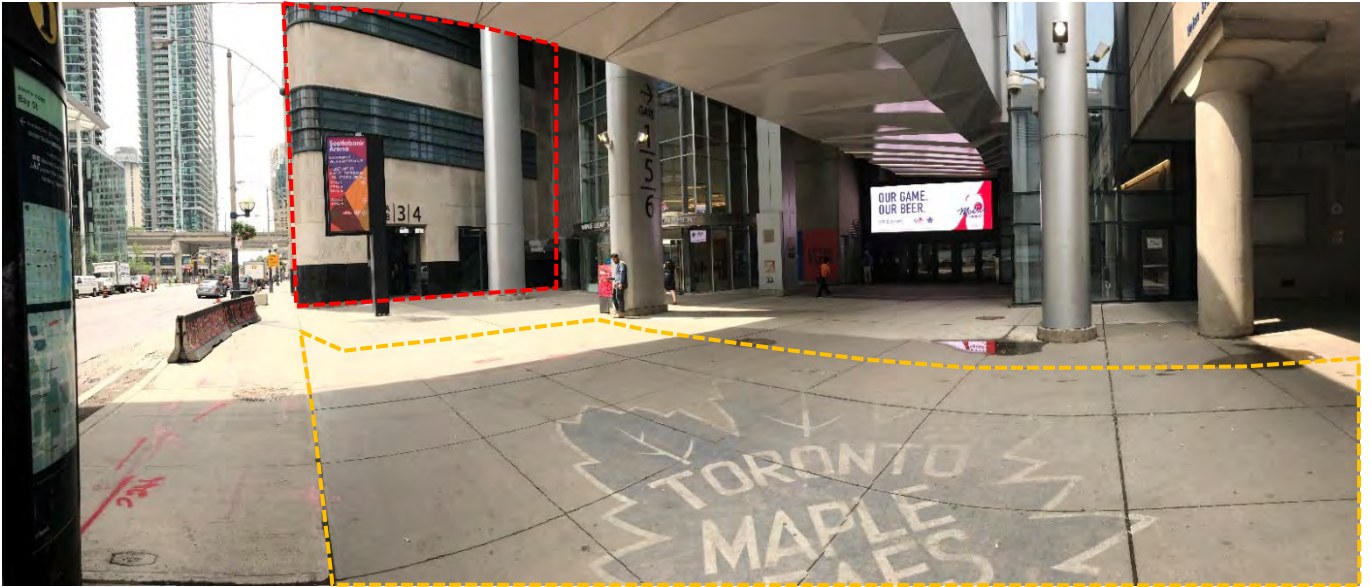
**Plate 29: View of pieces of the original façade Postal Delivery Building facade salvaged and included in the north concourse.**



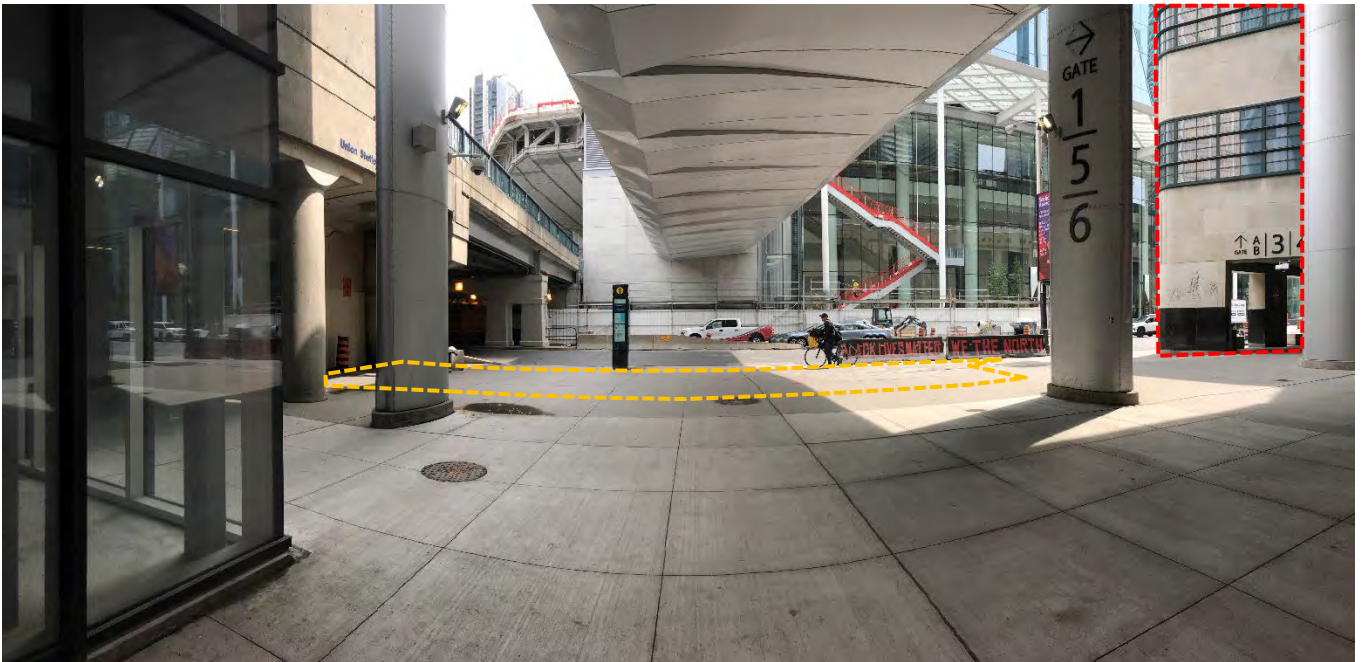
**Plate 30: View of a light fixture illustrating the Art Deco influences within the north concourse area.**

### **3.3 Area of Proposed Intervention**

The area of proposed intervention at the Postal Delivery Building/Scotiabank Arena (40 Bay Street) is situated along the east elevation of the property, adjacent to Bay Street (**Plate 31** and **Plate 32**). Proposed work in this location includes open cut excavation, demolition of the existing streetcar infrastructure, and construction of the new WELRT streetcar platform and tunnel. The façade of the north and east elevations of the structure along Bay Street is a remnant of the Postal Delivery Building. Features associated with this façade comprise the heritage attributes of the building (**Plate 32** to **Plate 35**).

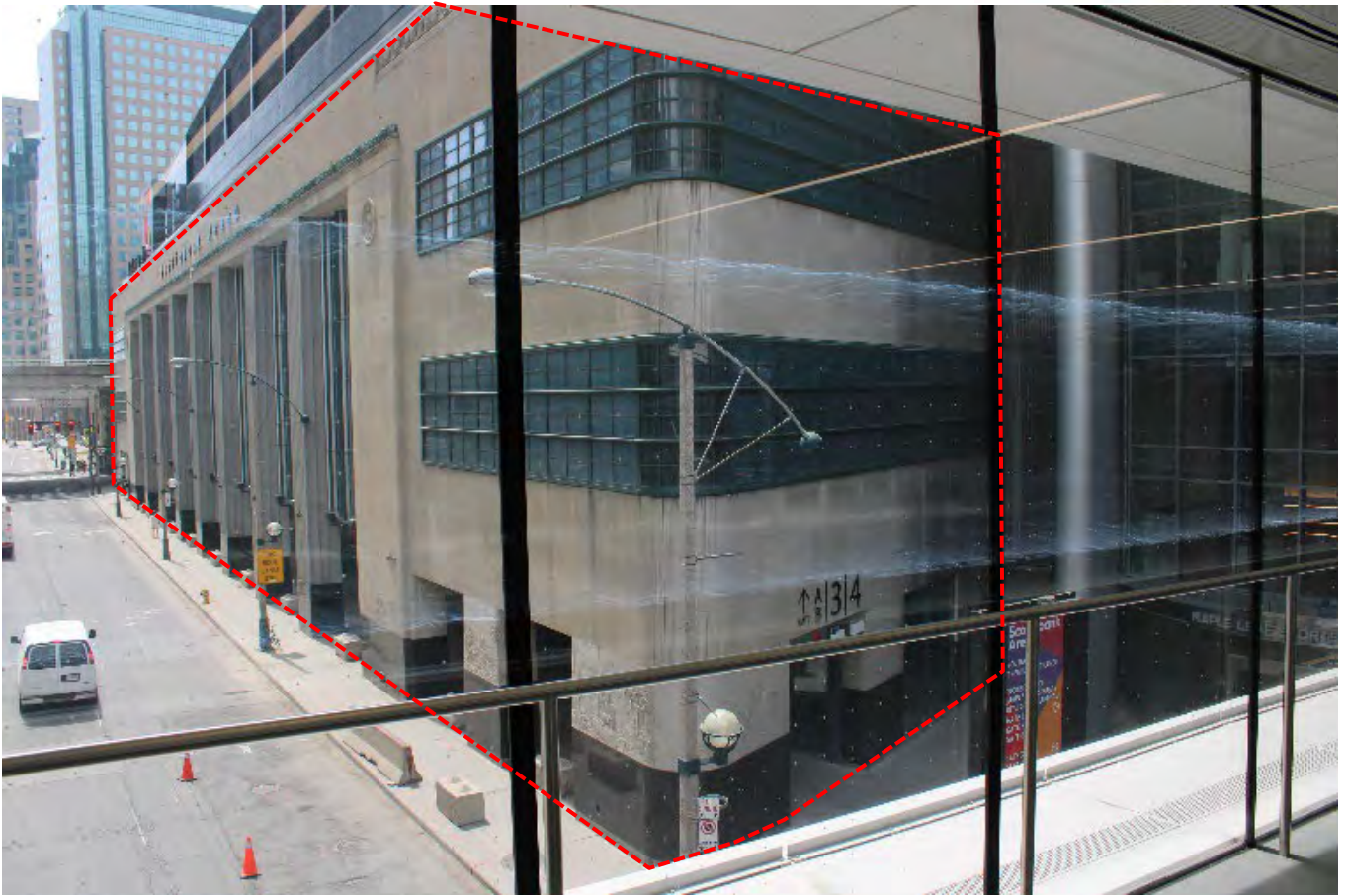


**Plate 31: Approximate location of the proposed intervention within 40 Bay Street (outlined in orange dashed line). Note the close proximity to the historical façade of the Postal Delivery Building (outlined in red dashed line).**



**Plate 32: Approximate area of proposed intervention within 40 Bay Street (outlined in an orange dashed line). Note the historical façade of the Postal Delivery Building on the right (outlined in red dashed line).**





**Plate 33: Location of the heritage attributes of 40 Bay Street (east and north elevations)**

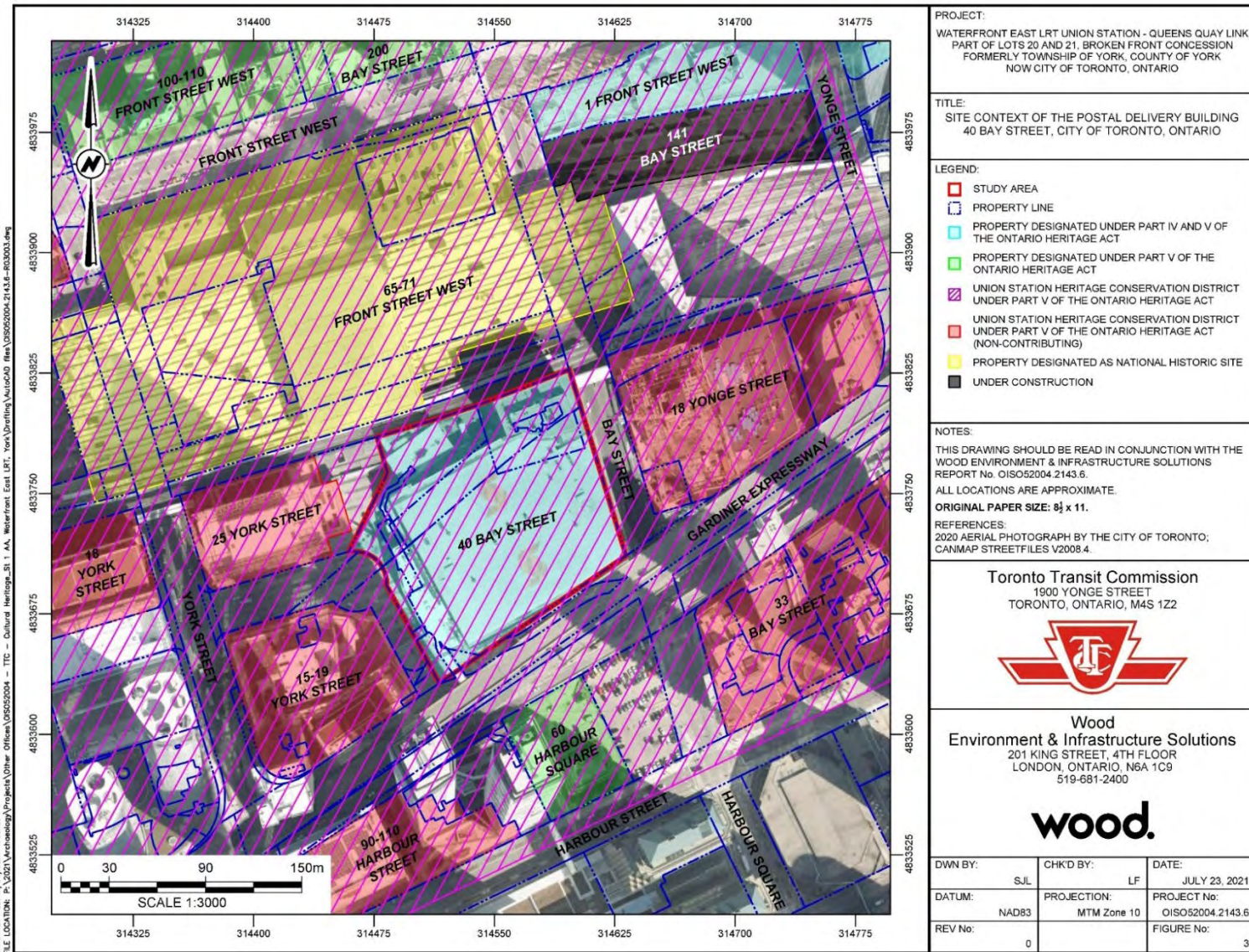


**Plate 34: View of the northern gate area on the east elevation of property, facing southwest. Note the bas relief sculpture in close proximity to the proposed works.**



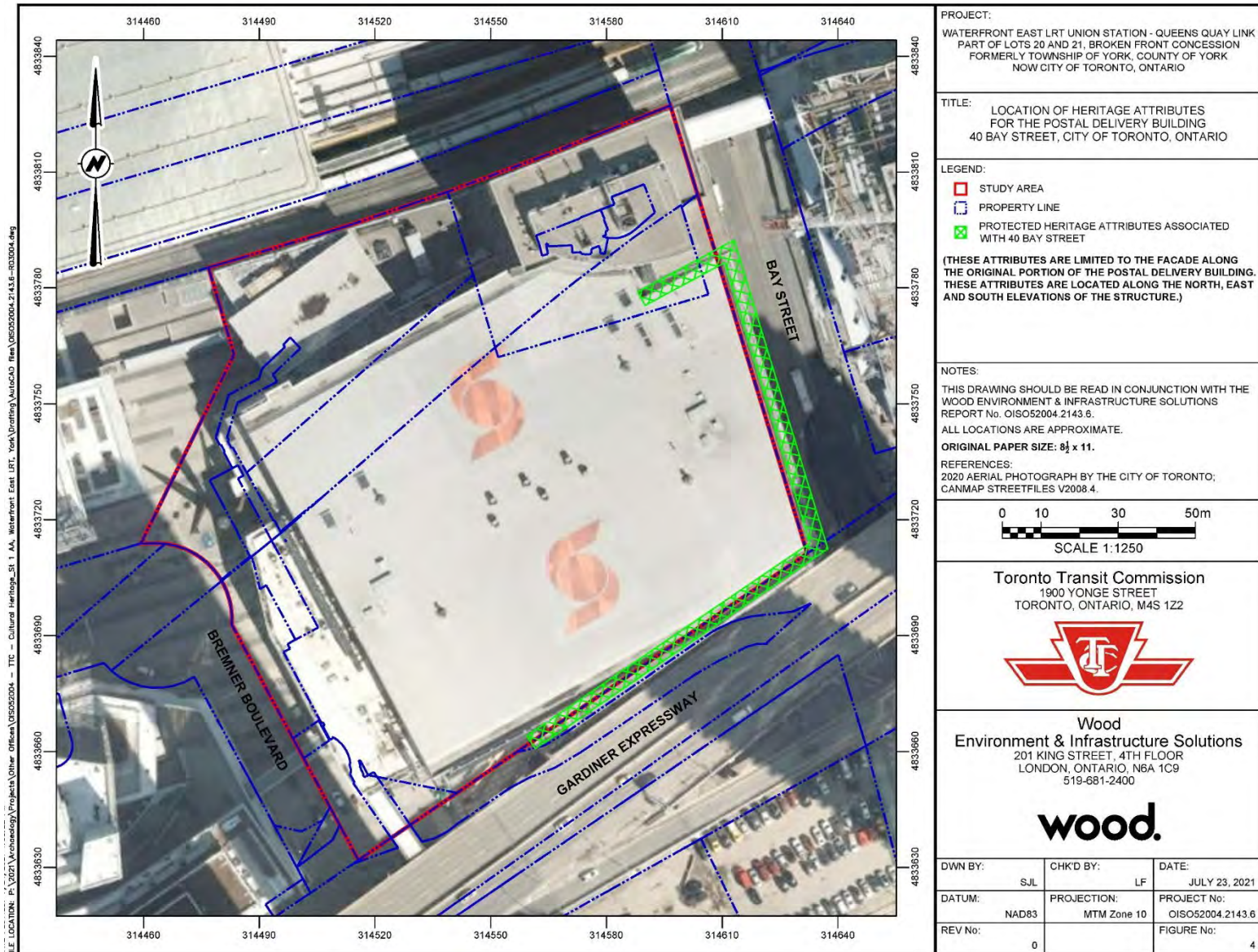
**Plate 35: View of the bas relief sculpture in close proximity to the proposed works along the east elevation of the structure, facing west.**





**Figure 3: Site Context of the Postal Delivery Building**  
Please reach out to the Project Team should you require alternative text for this image.





**Figure 4: Location of Heritage Attributes for the Postal Delivery Building**  
 Please reach out to the Project Team should you require alternative text for this image.



## 4.0 Description and Purpose of Proposed Activity

The proposed work at the Postal Delivery Building, now Scotiabank Arena (40 Bay Street) consists of property acquisition and street level demolition of a portion of the open area at the northeast entrance to the building. This work is located in the immediate vicinity of the historical north and east facades of the Postal Delivery Building, which form the heritage attributes of the property. Proposed work in this location includes open cut excavation, demolition of the existing below-grade streetcar line and associated infrastructure, and new WELRT station, platform, and tunnel. Details of the proposed work are provided below and drawings showing the proposed architectural plan and construction phasing plans are provided in **Appendix B**.

High-level description of potential impacts to the Postal Delivery Building (40 Bay Street):

- Open cut construction in the vicinity of the Scotiabank Area Bay Street entrance to accommodate the construction of the WELRT streetcar loop along Bay Street.
  - Permanent Impacts to the Postal Delivery Building:
    - Property acquisition in the vicinity of the Scotiabank Arena Bay Street entrance to accommodate excavation area required for the new streetcar loop. This work is in close proximity to the historical north and east façades of the Postal Delivery Building which are heritage attributes of this property.
  - Temporary Impacts:
    - Construction staging areas along Bay Street and within private property
    - Vibration related impacts due to open cut excavation, demolition and construction activities

Review of drawings of proposed work:

- Union Station Aerial View Renders, RE35-1-AA011 to RE35-1-AA013 (July 7, 2021)
  - Overview of the proposed work shown as 3D renders
- Union Station Platform Level Demolition Plan, RE35-1-AAD001 (July 7, 2021)
  - Demolition of Union Station Streetcar Loop at platform level
  - Basement work proposed at 141 Bay Street
  - Excavation area within 40 Bay Street and in the immediate vicinity of the historical north and east elevations of the Postal Delivery Building
  - Excavation within, and adjacent to, Union Station Complex
- Union Station Street Level Demolition Plan, RE35-1-AD002 (July 7, 2021)
  - Street level proposed demolition area depicted
  - Demolition activities shown with Union Station Complex (65-71 Front Street West), Dominion Public Building (1 Front Street West), Postal Delivery Building (40 Bay Street), and the public realm of the Union Station HCD along Bay Street





- Union Station Streetcar Platform Level Demolition Reflected Ceiling Plan, RE35-1-AD003 (July 7, 2021)
  - Street level demolitions depicted within Union Station (65-71 Front Street West), Postal Delivery Building (40 Bay Street), Dominion Public Building (1 Front Street West), and public realm of Union Station HCD
- Union Station Platform Level Staging (Plans 1 and 2), RE35-1-AS001 and RE35-1-AS002 (July 7, 2021)
  - Footprint of proposed work showing including alterations to the corridor/stairs in the 'USEP' Corridor, platform level mechanical rooms (northbound and southbound platforms), connections/alterations to stairs within Union Station (65-71 Front Street West), connection to existing TTC Union Station Subway Station, stairs and elevator within 141 Bay Street)
  - Retention of columns of Bay Street Bridge
- Union Station Site Plan, RE35-1-A1001 (July 7, 2021)
  - Site plan depicting the location of new construction shown
  - New construction shown within Union Station Complex (65-71 Front Street West) and 141 Bay Street, Dominion Public Building (1 Front Street West), and within public realm of Union Station HCD
  - New stairs showing within 'Existing Arcade' of Union Station and northwest portion of building
  - Vent shafts to street level shown
  - Existing columns of Bay Street Bridge to be retained
- Union Station Platform Level Plan, RE35-1-A2001 (July 7, 2021)
  - New track arrangement shown
  - Union Station Arcade: New Stair M, Elevator E3, Presto TVM/FSVM machines shown
  - New openings shown between existing columns of Bay Street Bridge
  - New West Platform shown
  - New East Platform shown
  - 141 Bay Street: Stair Q, Elevator E4, new corridor
  - Pedestrian Bridge Columns to remain
  - Existing Union Station Elevator 13 to remain
  - Stair N proposed within northwest portion of Union Station building (65-71 Front Street West)
  - Existing doors to TTC Union Station Subway to remain
  - New streetcar loop depicted including new handrails, new glazed wall, new structural walls and columns, and fan room
  - Existing columns of Bay Street Bridge to remain
- Union Station Street Level Plan, RE35-1-A2002 (July 7, 2021)
  - Union Station Arcade: Stair M and new Elevator E3
  - Northwest corner of Union Station building: Existing elevator 13 to remain and New Stair N



- Proposed vent shafts on west side of Bay Street, south of Front Street and within Bay Street right-of-way
- New curbs adjacent to Dominion Public Building (1 Front Street West)
- 141 Bay Street: Stair Q and new Elevator E4 shown
- Bay Street: Bay Street underpass (northbound and southbound shown) and associated new bike lanes shown



## 5.0 Impact Assessment

The Ministry of Citizenship and Multiculturalism’s (MCM) *Standards & Guidelines for Conservation of Provincial Heritage Properties – Information Bulletin 3: Heritage Impact Assessment for Provincial Heritage Properties* (MCM 2017) gives guidance on how to complete HIAs for public bodies prescribed under the *Ontario Heritage Act* (Government of Ontario 2014). The purpose of the HIA is to identify and assess the proposed activity to determine impacts (positive or negative, direct or indirect) that the proposed activity may have on the property’s cultural heritage value or interest. For the purpose of this HIA, the following definitions of direct, indirect, and positive impacts are used:

- **Direct Adverse Impact:** A permanent or irreversible negative affect on the cultural heritage value or interest of a property or result in the loss of a heritage attribute on all or part of the provincial heritage property.
- **Indirect Adverse Impact:** An impact that is the result of an activity on or near the property that may adversely affect its cultural heritage value or interest and/or heritage attributes.
- **Positive Impact:** An impact that may positively affect a property by conserving or enhancing its cultural heritage value or interest and/or heritage attributes.

(Government of Ontario 2017)

A detailed assessment of potential impacts resulting from the proposed TTC WELRT work is provided in Section 5.1. A discussion of the potential impacts resulting from the proposed work is provided in 5.3.

### 5.1 Potential Impacts to the Postal Delivery Building/Scotiabank Arena

As outlined in Section 4.0, the proposed work includes open cut excavation, demolition of the existing below-grade streetcar line and associated infrastructure, and new WELRT station, platform, and tunnel in the immediate vicinity of the historical north and east elevations of the Postal Delivery Building. A detailed assessment of the potential impacts resulting from the proposed work is provided in **Table 1** to **Table 3**.

**Table 1: Assessment of Potential Direct Adverse Impacts**

Potential Direct Adverse Impact	Y/N	Discussion
Removal or demolition of all or part of any heritage attribute.	N	No heritage attributes will be removed or demolished.
Removal or demolition of any building or structure on the provincial heritage property whether or not it contributes to the cultural heritage value or interest of the property (i.e. non-contributing properties).	N	No buildings or structures will be removed or demolished.





Potential Direct Adverse Impact	Y/N	Discussion
<p>Any land disturbance, such as change in grade and/or drainage patterns that may adversely affect a provincial heritage property, including archaeological resources.</p>	Y	<p>Land disturbance is anticipated. Property acquisition and open cut excavation is planned within a portion of 40 Bay Street adjacent to the historical east and north elevations of the structure and the northern entry gate along Bay Street. This disturbance will entail demolition of the existing sidewalk to facilitate open cut excavation in order to facilitate construction subgrade WELRT streetcar loop. This will be a temporary impact and will not result in permanent changes to grade and/or drainage patterns that will adversely affect the heritage property or any known archaeological sites. However, sidewalk will be removed in this area that acts as public space within the Union Station HCD.</p>
<p>Alterations to a property in a manner that is not sympathetic, or is incompatible, with cultural heritage value or interest of the property. This may include necessary alterations, such as new systems or materials to address health and safety requirements, energy-saving upgrades, building performance upgrades, security upgrades or servicing needs.</p>	N	<p>No significant alterations are anticipated to the heritage attributes of 40 Bay Street.</p>
<p>Alterations for access requirements or limitations to address factors as accessibility, emergency egress, public access, or security.</p>	N	<p>No permanent alterations to access at the Scotiabank Arena will be incurred. Temporary disruptions to building access may occurring during the construction phase of the project. However, no permanent alterations to the Postal Delivery Building are anticipated.</p>
<p>Introduction of new elements that diminish the integrity of the property, such as a new building, structure or addition, parking expansion or</p>	N	<p>No alterations to the Scotiabank Arena are anticipated.</p>



Potential Direct Adverse Impact	Y/N	Discussion
addition, access or circulation roads, or landscape features		
Changing the character of the property through removal or planting trees or other natural features, such as a garden, or that may result in the obstruction of significant views or vistas within, from, or of built and natural features.	N	No changes to the character of the property are proposed.
Change in use for the provincial heritage property that could result in permanent, irreversible damage or negates the property's cultural heritage value or interest.	N	No changes in land use are planned.
Continuation or intensification of a use of the provincial heritage property without conservation of heritage attributes.	N	No intensification of use is planned.

**Table 2: Assessment of Potential Indirect Impacts**

Potential Indirect Adverse Impact	Y/N	Discussion
Shadows that alter the appearance of a heritage attribute or change the visibility of an associated natural feature or plantings, such as a tree row, hedge, or garden.	N	No shadow-related impacts are anticipated.
Isolation of a heritage attribute from its surrounding environment, context, or a significant relationship	N	No permanent isolation-related impacts are anticipated. The open cut excavation and construction relating to the new streetcar loop may temporarily restrict access points to the building. However, the context of the structure within the HCD and its spatial relationship to its surroundings will remain intact.
Vibration damage to a structure due to construction or activities on adjacent to the property	Y	Construction activity that introduces vibration-related risk is planned on the property.



Potential Indirect Adverse Impact	Y/N	Discussion
Alteration or obstruction of a significant view of or from the provincial heritage property from a key advantage point	N	Temporary impacts to street level views of the heritage attributes of the property are anticipated due to safety fencing. However, this impact is temporary and the views that will be impacted are not significant protected heritage views.

**Table 3: Assessment of Potential Positive Impacts**

Potential Positive Impact	Y/N	Discussion
Changes or alterations that are consistent with accepted conservation principles, such as those articulated in MCM' <i>Eight Guiding Principles in the Conservation of Historic Properties, Heritage Conservation Principles for Land Use Planning, Parks Canada's Standards and Guidelines for the Conservation of Historic Places in Canada</i>	N	No alterations of the property are planned.
Adaptive re-use of a property – alteration of a provincial heritage property to fit new uses or circumstances of the property in a manner that retains its cultural heritage value or interest	N	No new use or re-use is planned as part of the proposed work.
Public interpretation or commemoration of the provincial heritage property	N	No public interpretation or commemoration is planned as part of the proposed work.

## 5.2 Potential Impacts to Union Station HCD

The Postal Delivery Building is a contributing building in the Union Station HCD. However, work associated with the WELRT is below grade and no exterior impacts to the Postal Delivery Building are anticipated. The HCD guidelines for contributing buildings address additions, alterations, or demolition of these properties with heritage significance within the district (ERA Architects Inc. 2006: 60). Given that no exterior demolition, additions, or alterations of the Postal Delivery Building are proposed, the HCD guidelines for contributing buildings are not applicable. The at-grade work within 40 Bay Street includes the rehabilitation of the sidewalk and public realm along the east





elevation of the building following construction of the WELRT. The proposed street level rehabilitation is directly tied to the public realm (i.e. sidewalk) along Bay Street.

The impacts to the Union Station HCD are related to the public realm of the district since the WELRT project will result in open cut excavation in the vicinity of Front Street West and Bay Street to accommodate the demolition of the existing streetcar loop and construction of the new streetcar loop and platform. Post-construction landscaping will be carried out to rehabilitate the sidewalk and disturbed sections of the public realm. A standalone HIA for the Union Station HCD to address impacts to the public realm has been prepared by WSP under a separate cover.

### **5.3 Discussion of Anticipated Impacts**

The detailed impact assessment determined that the proposed development poses one potential direct adverse impact and one potential indirect adverse impact to 40 Bay Street.

#### **5.3.1.1 Summary of Potential Direct Adverse Impacts**

There is one potential direct adverse impact to 40 Bay Street posed by the proposed WELRT. This includes land disturbance that may adversely impact the heritage resource as a result of open cut construction, demolition of the existing below grade streetcar infrastructure, and construction of the new WELRT.

Demolition of the existing sidewalk to facilitate excavation in the vicinity of the Scotiabank Arena Bay Street entrance and adjacent to the eastern elevation of the structure along Bay Street is anticipated. This demolition and excavation is required to facilitate construction of the streetcar loop. This work will take place in close proximity to the historical north and east façades of the Postal Delivery Building along Bay Street, which are heritage attributes of the property. This work will involve the demolition of sidewalk that acts as a public space within the Union Station HCD.

#### **5.3.1.2 Summary of Potential indirect Adverse Impacts**

There is one potential indirect adverse impact to 40 Bay Street posed by the proposed WELRT. Construction activities are anticipated in close proximity to the heritage attributes of the structure, which will introduce vibration-related risk.

#### **5.3.1.3 Summary of Potential Positive Impacts**

There are no potential positive impacts posed by the proposed development noted.



## 6.0 Considered Alternatives and Mitigation Measures

Alternative approaches and mitigation measures are required when impacts are anticipated to a property with CHVI. In the case of the Postal Delivery Building/Scotiabank Arena located at 40 Bay Street, one direct negative impact and one indirect negative impact have been identified. To address these impacts, the mitigation measures contained in MCM InfoSheet#5 were considered (Government of Ontario 2005). An evaluation of applicable mitigation measures is presented in **Table 4**.

**Table 4: Mitigation Measures**

Mitigation Measures	Relevance
Alternative development approaches	N/A
Isolating development and site alteration from significant built and natural features and vistas	N/A
Design guidelines that harmonize mass, setback, setting, and materials	<p>Demolition of the existing sidewalk to facilitate excavation in the vicinity of the Scotiabank Arena Bay Street entrance and along the east elevation of the structure is anticipated. This work is anticipated to be in the immediate vicinity of the east and north historical façades of the Postal Delivery Building, which are heritage attributes of this property.</p> <p>The project should be designed with a general avoidance approach to be in line with provincial and federal guidelines.</p> <p>The sidewalk area at 40 Bay Street functions as part of the public realm within the Union Station HCD, as such, the design guidelines for the Union Station HCD pertaining to the public realm should be followed when considering replacement of this material (Section 2.2.1.2) (ERA Architects Inc. 2006: 64-65). Compatibility with the surrounding area should be considered.</p>
Limiting height and density	N/A
Allowing only compatible infill and additions	N/A
Reversible alterations	N/A



Mitigation Measures	Relevance
<p>Buffer zones, protection measures, and other planning mechanisms</p>	<p>Demolition and excavation in proximity to the Scotiabank Arena Bay Street entrance and in the immediate vicinity of the historical east and north facades of the Postal Delivery Building is anticipated. To mitigate potential impacts, the following measures are recommended:</p> <p><i>Protective Barriers</i></p> <p>Protective fencing, film, or netting should be installed around the base of historical north and east elevations of the Postal Delivery Building in the vicinity of the proposed work to protect the exterior of the building from accidental damage during construction.</p> <p><i>Protection Measures</i></p> <p>The heritage status of the Dominion Public Building should be noted on project drawings to communicate the status of this property to project personnel. The note should read:</p> <p>“The former Postal Delivery Building (now Scotiabank Arena) (40 Bay Street) is a significant heritage property with protection under Parts IV and V of the <i>Ontario Heritage Act</i>. The north, east, and south facades of 40 Bay Street form the historical exterior of the Postal Delivery Building and are protected heritage attributes of the property. Use caution when conducting work in the vicinity of this building to avoid accidental damage.”</p> <p><i>Vibration Monitoring</i></p> <p>Work is proposed within the former Postal Delivery Building/Scotiabank Arena property, including open cut excavation, demolition activities, and construction of the new WELRT platform and tunnel.</p> <p>Vibration monitoring should be carried out by a qualified geotechnical engineer and should consist of a pre-construction survey, vibration monitoring during construction, and a post-construction survey.</p>





Mitigation Measures	Relevance
	Protection measures should endeavor to facilitate as much access to the heritage resource as possible while respecting pertinent safety requirements.

## 6.1 Discussion of Mitigation Measures

### 6.1.1 Conservation Guidance

The *Standards and Guidelines for the Conservation of Historic Places in Canada* (Standards and Guidelines) was reviewed to develop a conservation strategy for the former Postal Delivery Building, now the Scotiabank Arena (40 Bay Street). The Standards and Guidelines lay out the framework for the conservation of historic places, that can be defined as “all actions or processes aimed at safeguarding the character-defining elements [heritage attributes] of an historic place to retain the heritage value and extend its physical life” (Parks Canada 2011: 15). Per the Standards and Guidelines, there are three primary conservation treatments:

- **Preservation:** Involves protecting, maintaining, and stabilizing the existing form, material, and integrity of an historic place or individual component, while protecting its heritage value. Preservation should be considered as the primary treatment when, a) materials, features and spaces of the historic place are essentially intact and convey the historical significance without extensive repair or replacement, b) depiction during a particular period in its history is not appropriate, and, c) continuation or new use does not require extensive alterations or additions.
- **Rehabilitation:** Involves the sensitive adaptation of an historic place or individual component for a continuing or compatible contemporary use, while protecting its heritage value. Rehabilitation should be considered as the primary treatment when, a) repair or replacement of deteriorated features is necessary, b) alterations or additions to the historic place or planned for a new or continued use, and c) depiction during a particular period in its history is not appropriate.
- **Restoration:** Involves accurately revealing, recovering or representing the state of an historic place or individual component as it appeared at a particular period in its history, while protecting its heritage value. Restoration should be considered as the primary treatment when, a) an historic place’s significance during a particular period in its history significantly outweighs the potential loss of existing, non-character defining materials, features and spaces from other periods, b) substantial physical and documentary or oral evidence exists to accurately carry out the work, and, c) contemporary additions or alterations and are not planned.

*(Parks Canada 2011: 17)*



No direct impacts to heritage attributes of this structure are anticipated as a result of the proposed work, as such, it is recommended that *preservation* be considered the primary conservation treatment for the former Postal Delivery Building/Scotiabank Arena.

Accordingly, it is recommended that the 'General Standards' contained in Section 3 of the Standards and Guidelines are used when developing plans for the proposed work in the vicinity of this property.

Accordingly, it is recommended that the 'General Standards' and guidelines for Exterior Walls (Section 4.3.4), are used to guide the design and implementation of the WELRT project in the vicinity of the Postal Delivery Building. Full copies of these standards and guidelines are provided in **Appendix C**. Standards and guidelines of note are included below.

#### General Standards for Preservation, Rehabilitation, and Restoration (Parks Canada 2011: 22-23)

1. Conserve the heritage value of an historic place. Do not remove, replace or substantially alter its intact or repairable character defining elements. Do not move a part of an historic place if its current location is a character-defining element.
3. Conserve heritage value by adopting an approach calling for minimal intervention.

Section 4.3.4 *Exterior Walls* of the Standards and Guidelines should be used to guide work proposed within the Dominion Public Building property. Guidelines of note from this section include:

#### Guidelines for Exterior Walls (Section 4.3.4) (Parks Canada 2011: 146-152)

1. Understanding the exterior walls and how they contribute to the heritage value of the historic building
10. Protecting adjacent character-defining elements from accidental damage or exposure to damaging materials during maintenance or repair work.

### 6.1.2 Design Guidelines

The proposed development does not include extensive alterations or additions directly impacting the structure itself. The work proposed at 40 Bay Street includes demolition of the existing sidewalk to facilitate excavation in the vicinity of the Scotiabank Arena Bay Street entrance and adjacent to the historical north and east elevations of the structure. This work will be undertaken in close proximity to the historic façade of the Postal Delivery Building along Bay Street, which is a heritage attribute of this property. Additionally, the sidewalk area within 40 Bay Street property is anticipated to be demolished in order to facilitate construction. This sidewalk functions as part of the public realm within the Union Station HCD as it is physically, functionally, and visually linked to the HCD and facilitates circulation within the HCD.



To mitigate impact to the CHVI of this heritage resource and its contribution to the surrounding HCD the following design guidelines should be in place:

- The project should be designed with a general avoidance approach to be in line with provincial and federal guidelines.
- The sidewalk area at 40 Bay Street functions as part of the public realm within the Union Station HCD, as such, the design guidelines for the Union Station HCD pertaining to the public realm should be followed when considering replacement of this material (Section 2.2.1.2) (ERA Architects Inc. 2006: 64-65). Compatibility with the surrounding area should be considered.

### **6.1.3 Protection Measures**

Excavation, demolition, and construction activities in the immediate vicinity of the historical east and north elevations of the Postal Delivery Building are planned. This construction activity is anticipated to introduce vibration-related risk on the property. Additionally, this will cause temporarily restricted access to the structure due to the presence of construction staging areas, open excavations, and fencing or hoarding limiting access to the area in the interest of public safety.

In order to mitigate impact to the CHVI of the heritage resources the below controls are recommended.

#### **6.1.3.1 Protective Barriers**

Protective fencing, film, or netting should be installed around the base of historical north and east elevations of the Postal Delivery Building in the vicinity of the proposed work to protect the exterior of the building from accidental damage during construction

#### **6.1.3.2 Protection Measures**

The heritage status of the Dominion Public Building should be noted on project drawings to communicate the status of this property to project personnel. The note should read:

“The former Postal Delivery Building (now Scotiabank Arena) (40 Bay Street) is a significant heritage property with protection under Parts IV and V of the *Ontario Heritage Act*. The north, east, and south facades of 40 Bay Street form the historical exterior of the Postal Delivery Building and are protected heritage attributes of the property. Use caution when conducting work in the vicinity of this building to avoid accidental damage.”

#### **6.1.3.3 Vibration Monitoring**

Work is proposed within the former Postal Delivery Building/Scotiabank Arena property, including open cut excavation, demolition activities, and construction of the new WELRT platform and tunnel.

Vibration monitoring should be carried out by a qualified geotechnical engineer and should consist of a pre-construction survey, vibration monitoring during construction, and a post-construction survey.





Protection measures should endeavor to facilitate as much access to the heritage resource as possible while respecting pertinent safety requirements.



## 7.0 Summary of Community Engagement

In order to identify the existing heritage protections and requirements for the 40 Bay Street, the MCM, Ontario Heritage Trust, City of Toronto, Metrolinx, and Parks Canada were consulted. A summary of the community engagement results is presented in **Table 5** and records of correspondence are provided in **Appendix D**.



**Table 5: Summary of Community Engagement**

Individuals/Groups Engaged	Method	Results	Incorporation into HIA
<p>Yasmina Shamji, Urban Design/Heritage Planning, City of Toronto</p>	<p>-Email sent on December 16, 2020 -Response received on January 7, 2021</p>	<p>Yasmina Shamji from Urban Design and Heritage Planning at the City of Toronto was initially contacted as part of the CHR prepared for the TTC WELRT project. Ms. Shamji reported that there are a number of known/protected municipal heritage properties within the CHR Study Area. These included:</p> <ul style="list-style-type: none"> <li>-40 Bay Street (Designated under Part IV and V of the <i>Ontario Heritage Act</i>)</li> <li>-71 Front Street West (Union Station) (Designated under Part IV and V of the <i>Ontario Heritage Act</i>)</li> <li>-145 Queens Quay West (Designated under Part IV of the Ontario Heritage Act)</li> <li>-2 Cooper Street (Intention to Designate)</li> <li>-1 Front Street West (Designated under Part IV and V of the Ontario Heritage Act)</li> <li>-61 Front Street West (Listed Heritage Property)</li> </ul> <p>Ms. Shamji noted that property information can be found on the City of Toronto's <a href="#">Heritage Register Map</a> and By-law information is available on the <a href="#">By-laws and Municipal Codes</a> webpage. Information was requested on additional properties within the Union Station HCD. Ms. Shamji provided the contact information for Guy Zimmerman, Heritage Planner at the City of Toronto, and recommended that WSP contact Mr. Zimmerman regarding information related to the Union Station HCD.</p>	<p>Levels of heritage protection for Postal Delivery Building and nearby provincial heritage properties noted in the HIA. Guy Zimmerman contacted to obtain direction on the Union Station HCD.</p>





Individuals/Groups Engaged	Method	Results	Incorporation into HIA
<p>Guy Zimmerman, Heritage Planner, City of Toronto</p>	<p>-Series of email correspondence between March 26, 2021 and July 7, 2021            -Online meeting on Tuesday, July 7, 2021 regarding the Gardiner Expressway, PATH system, and Bay Street Bridge</p>	<p>Based on the recommendation of Yasmina Shamji, Guy Zimmerman, Heritage Planner with the City of Toronto, was contacted regarding properties within the Union Station HCD that are located within the TTC WELRT Study Area. Guy Zimmerman is noted as being the Heritage Planner for the Union Station HCD. Mr. Zimmerman provided HCD inventory sheets for a number of individual HCD properties/landscape features within the TTC WELRT Study Area.</p> <p>Subsequent conversations with Mr. Zimmerman determined that a demolition permit was issued for the Workmen’s Compensation Board Building (90 Harbour Street) in 2011 and that this building is no longer standing.</p> <p>An online meeting with Guy Zimmerman (City of Toronto), Brent Fairbairn (City of Toronto), and Heidi Schopf (WSP) was carried out on Tuesday, July 7, 2021 to discuss the role of the Gardiner Expressway, PATH System, and Bay Street Bridge in the Union Station HCD. Mr. Zimmerman discussed how the Gardiner Expressway and PATH System are contributing landscape elements of the HCD and that the public realm contributions of these resources should be considered. Ms. Schopf asked if the Bay Street Bridge (supporting the Union Station Trainshed over Bay Street) had heritage significance in its own right since this substructure was not noted as a heritage resource on the municipal register or by the MCM and OHT. Mr. Zimmerman expressed that Union Station is</p>	<p>Inventory sheets provided by Guy Zimmerman used as background information in the preparation of the CHR and this HIA. Information on the role of Parks Canada and Metrolinx used to expand the community engagement strategy for this HIA.</p>



Individuals/Groups Engaged	Method	Results	Incorporation into HIA
		run by Parks Canada and that this federal agency will have direction regarding the heritage status of individual components of the Union Station complex. Mr. Zimmerman suggested that Parks Canada and Metrolinx have previous cultural heritage studies that may be of relevance to the current Study Area.	
Kevin DeMille, Natural Heritage Coordinator, Ontario Heritage Trust	<p>-Emails sent on December 16, 2020, March 25, 2021, April 8, 2021, and July 8, 2021</p> <p>-Responses received on March 26, 2021, April 14, 2021, and July 13, 2021</p>	<p>Kevin DeMille, Natural Heritage Coordinator with the Ontario Heritage Trust was initially contacted as part of the CHR prepared for the TTC WELRT project. Mr. DeMille reported that the Ontario Heritage Trust does not have any conservation easements or Trust-owned properties within the WELRT Study Area. Mr. DeMille recommended reviewing the Ontario Heritage Trust's Plaques Database and Ontario Heritage Act register to obtain information on municipal/local heritage properties in the Study Area. Mr. DeMille further recommended direct communication with the City of Toronto regarding listed and designated municipal heritage properties.</p> <p>Kevin DeMille was subsequently contacted on April 8, 2021 and July 8, 2021 regarding three properties within the Study Area, including Union Station (65-71 Front Street West), the Dominion Public Building (1 Front Street West), and the Postal Delivery Building (40 Bay Street). Mr. DeMille provided the designation By-laws for these properties and also noted that Union Station (65-71 Front Street West) is a National Historic Site. Mr. DeMille recommended consultation with Parks</p>	Heritage protections noted by Kevin DeMille were incorporated into the CHR and this HIA. By-laws provided by Mr. DeMille were reviewed. Contact information for Kirushanth Gnanachandran at Parks Canada was used to consult this agency as part of the community engagement completed for this HIA.



Individuals/Groups Engaged	Method	Results	Incorporation into HIA
		<p>Canada given the federal status of this property. Mr. DeMille subsequently provided the contact information for Kirushanth Gnanachandran with Parks Canada at the request of WSP.</p>	
<p>Karla Barboza, (A) Team Lead, MCM</p>	<p>-Email sent on December 16, 2020            -Email response received on December 17, 2020            -Email sent on April 8, 2021</p>	<p>Karla Barboza, Acting Team Lead for Heritage at the MCM was initially contacted as part of the CHR prepared for the TTC WELRT project. Karla reported that there were no properties designated by the Minister in the TTC WELRT Study Area. Ms. Barboza did identify that Union Station is a Provincial Heritage Property of Provincial Significance. She also noted that there are two nearby provincial heritage properties including the Union Station Rail Corridor (USRC) Interlocking Tower – Scott Street (Provincial Heritage Property of Provincial Significance) and Yonge Street Railway Bridge (Provincial Heritage Property – Local Significance). Ms. Barboza also noted that Strategic Conservation Plans (SCP) have not been prepared for Union Station (65-71 Front Street), the URSC Interlocking Tower-Scott Street, or the Yonge Street Bridge. Ms. Barboza noted that lands owned or controlled by an Ontario Ministry or Prescribed Public Body on behalf of the Crown may have responsibilities under the Standards and Guidelines for the Conservation of Provincial Heritage Properties (MTC 2010).</p> <p>Communication with Karla Barboza continued in April 2021 when WSP contacted her to request input on the HIAs being prepared for this project.</p>	<p>Levels of heritage protection for properties in the Study Area noted in the HIA. Direction regarding obligations of prescribed public bodies under the Standards and Guidelines for the Conservation of Provincial Heritage Properties noted and used to guide the preparation of HIAs.</p>





Individuals/Groups Engaged	Method	Results	Incorporation into HIA
		Ms. Barboza requested to review the CHR before providing input on the HIAs.	
Laura Hatcher, Heritage Planner, MCM	-Response received on December 18, 2020	As part of the ongoing communication with Karla Barboza outlined above, Laura Hatcher, Heritage Planner, at the MCM provided the Statements of Cultural Heritage Value (SCHV) for Union Station (65-71 Front Street), the URSC Scott Street Interlocking Tower, and the Yonge Street Bridge. All three SCHV had been approved by the Metrolinx Heritage Committee.	n/a
Kirushanth Gnanachandran Project Coordinator, Rouge National Urban Park Parks Canada/Government of Canada	-Email sent on July 13, 2021 -Response received on July 14, 2021	An email was sent to Kirushanth Gnanachandran from Parks Canada on July 13, 2021 to request background information on Union Station (65-71 Front Street West) and the Dominion Public Building (1 Front Street West). A response was received on July 14, 2021 to notify WSP that the information gathering request had been passed on to others at FHBRO and the Cultural Resource Management group. To date, responses to fulfill the information gathering request have not been received.	n/a
Dan Beare, Metrolinx	-Email sent on July 13, 2021	To date, a response from Dan Beare at Metrolinx has not been received.	n/a
Tamkin Naghshbandi, Metrolinx	-Email sent on July 13, 2021	Tamkin Naghshbandi from Metrolinx responded and provided detailed information about the Union Station Complex. No information related to the Postal Delivery Building was provided by Metrolinx.	n/a



## 8.0 Recommendations

The TTC is undertaking PDE to produce a Baseline Design (approximately 30% design completion) of a new TTC WELRT system (the Project). The WELRT, goes under Bay Street, from Front Street to Queens Quay West, and then heads easterly on Queens Quay West. A new portal on Queens Quay West between Bay Street and Yonge Street will be constructed to provide a transition from an underground LRT to an at-grade LRT, which will complement the existing west portal and at-grade LRT on Queens Quay West, west of Bay Street.

The WELRT will service Toronto's waterfront revitalization area by providing fast, reliable transit service in the East Bayfront (EBF) Area of the Waterfront. The expansion of the Union LRT and Queens Quay LRT Stations is required to accommodate the additional streetcar lines and passenger volume. This project is critical to the new waterfront transit plan in the EBF Precinct.

WSP was retained by TTC to complete a HIA for the Postal Delivery Building (40 Bay Street) in support of the WELRT. This HIA represents one deliverable to support program delivery. This HIA is being carried out under the Transit Project Assessment Process (TPAP) and was prepared in accordance with the MCM guidance document titled *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties* (MCM 2017).

The purpose of this HIA is to establish the Statement of Cultural Heritage Value for the Postal Delivery Building now known as Scotiabank Arena located at 40 Bay Street, Toronto, Ontario, assess the existing conditions of the property, describe the purpose of the proposed activity, complete an impact assessment and outline considered alternatives and mitigation measures, provide a summary of community engagement, and development recommendations for the conservation of the property.

Based on the above, the following recommendations are made:

### 1) Design Guidelines:

- a. The impacted area within 40 Bay Street functions as part of the public realm within the Union Station HCD. Accordingly, the design guidelines for the Union Station HCD pertaining to the public realm should be followed when considering replacement of this material (Section 2.2.1.2) (ERA Architects Inc. 2006: 64-65). Compatibility with the surrounding area should be considered.

### 2) Protection Measures:

- a. The proposed work for the WELRT project is largely located below grade and involves open cut excavation, the demolition of the existing streetcar loop, and construction of the new loop/platform along Bay Street from Front Street West to 40 Bay Street. Open cut construction will occur within the former Postal Delivery Building/Scotiabank Arena property. Protection measures in the form of protective barriers and notes on construction drawings should be implemented during the construction phase of the project to protect this building. Protective fencing, film, or netting should be



installed around the base of the historical north and east facades of the Postal Delivery Building in the vicinity of the proposed work to protect the exterior of the building from accidental damage during construction.

- b. The heritage status of the Dominion Public Building should be noted on project drawings to communicate the status of this property to project personnel. The note should read:

“The former Postal Delivery Building (now Scotiabank Arena) (40 Bay Street) is a significant heritage property with protection under Parts IV and V of the *Ontario Heritage Act*. The north, east, and south facades of 40 Bay Street form the historical exterior of the Postal Delivery Building and are protected heritage attributes of the property. Use caution when conducting work in the vicinity of this building to avoid accidental damage.”
  - c. Construction work is proposed in the immediate vicinity of the historical building exterior of the former Postal Delivery Building that introduces vibration-related risk. Vibration monitoring should be carried out by a qualified geotechnical engineer and should consist of a pre-construction survey, vibration monitoring during construction, and a post-construction survey.
- 3) This report should be submitted to the following agencies for review and comment:
- a. City of Toronto Heritage Preservation Services
  - b. MCM

The above recommendations were prepared using the best available information regarding potential impacts at the time of writing. Should the proposed work change, then the preliminary impact assessment should be revisited to confirm identified impacts and proposed mitigation measures.



## 9.0 Assessor Qualifications

This report was prepared and reviewed by the undersigned, employees of WSP. WSP is one of North America's leading engineering firms, with more than 50 years of experience in the earth and environmental consulting industry. The qualifications of the assessors involved in the preparation of this report are provided in **Appendix E**.





## 10.0 Closure

This report was prepared for the exclusive use of the TTC and is intended to provide Heritage Impact Assessment for the Postal Delivery Building (40 Bay Street) (the Study Area).

Any use which a third party makes of this report, or any reliance on or decisions to be made based on it, are the responsibility of the third party. Should additional parties require reliance on this report, written authorization from WSP will be required. With respect to third parties, WSP has no liability or responsibility for losses of any kind whatsoever, including direct or consequential financial effects on transactions or property values, or requirements for follow-up actions and costs.

The report is based on data and information collected during the cultural heritage assessment conducted by WSP. It is based solely a review of historical information, a property reconnaissance conducted in April to July 2021 and data obtained by WSP as described in this report. Except as otherwise maybe specified, WSP disclaims any obligation to update this report for events taking place, or with respect to information that becomes available to WSP after the time during which WSP conducted the cultural heritage assessment. In evaluating the Study Area, WSP has relied in good faith on information provided by other individuals noted in this report. WSP has assumed that the information provided is factual and accurate. In addition, the findings in this report are based, to a large degree, upon information provided by the current owner/occupant. WSP accepts no responsibility for any deficiency, misstatement or inaccuracy contained in this report as a result of omissions, misinterpretations or fraudulent acts of persons interviewed or contacted.

WSP makes no other representations whatsoever, including those concerning the legal significance of its findings, or as to other legal matters touched on in this report, including, but not limited to, ownership of any property, or the application of any law to the facts set forth herein. With respect to regulatory compliance issues, regulatory statutes are subject to interpretation and change. Such interpretations and regulatory changes should be reviewed with legal counsel.

We trust that the information presented in this report meets your current requirements. Should you have any questions, or concerns, please do not hesitate to contact the undersigned.

Respectfully Submitted,

**WSP E&I Canada Limited**

Prepared by:

Reviewed by:

Luke Fischer, MA, CAHP  
Cultural Heritage Specialist

Heidy Schopf, MES, CAHP  
Cultural Heritage Team Lead



## 11.0 References

City of Toronto

2006 Union Station Heritage Conservation District Plan, Appendix 4. Available Online: [https://www.toronto.ca/wp-content/uploads/2018/01/8dfc-CityPlanning\\_Union-Station-HCD.pdf](https://www.toronto.ca/wp-content/uploads/2018/01/8dfc-CityPlanning_Union-Station-HCD.pdf). Retrieved 31 December 2020

ERA Architects Inc., Et Al

2006 *Union Station Heritage Conservation District Plan*. Document on file at WSP.

Government of Ontario

1990 *Ontario Heritage Act, R.S.O. 1990, Chapter O.18*. Last amendment: July 1, 2019. Electronic document: <https://www.ontario.ca/laws/statute/90o18>. Last accessed July 15, 2020.

2005 *InfoSheet #5, Heritage Resources in the Land Use Planning Process*. Available online: [http://www.mtc.gov.on.ca/en/publications/Heritage\\_Tool\\_Kit\\_Heritage\\_PPS\\_info\\_Sheet.pdf](http://www.mtc.gov.on.ca/en/publications/Heritage_Tool_Kit_Heritage_PPS_info_Sheet.pdf). Last accessed July 15, 2020.

2006a *Ontario Regulation 9/06: Criteria for Determining Cultural Heritage Value or Interest*. Available online: <https://www.ontario.ca/laws/regulation/060009>. Last accessed July 15, 2020.

2006b *Ontario Regulation 10/06: Criteria for Determining Cultural Heritage Value or Interest of Provincial Significance*. Available online: <https://www.ontario.ca/laws/regulation/060010>. Last accessed July 15, 2020.

2010 *Standards & Guidelines for Conservation of Provincial Heritage Properties*. Available online: [http://www.mtc.gov.on.ca/en/publications/Standards\\_Conservation.pdf](http://www.mtc.gov.on.ca/en/publications/Standards_Conservation.pdf). Last accessed July 15, 2020.

2014 *Ontario Regulation 157/10: Public Bodies – Part III.1 of the Act*. Available online: <https://www.ontario.ca/laws/regulation/100157>. Last accessed July 15, 2020.

2019a *Environmental Assessment Act, R.S.O. 1990, c. E.18*. Available online: <https://www.ontario.ca/laws/statute/90e18>. Last accessed July 15, 2020.

2020 *Provincial Policy Statement, Section 2.6 Cultural heritage and archaeology*. Electronic document: <https://files.ontario.ca/mmah-provincial-policy-statement-2020-accessible-final-en-2020-02-14.pdf>. Last accessed July 15, 2020.

2021 *Heritage Impact Assessment/Conservation Strategy Terms of Reference*. Available online: <https://www.toronto.ca/city-government/planning-development/application-forms-fees/building-toronto-together-a-development-guide/application-support-material-terms-of-reference/>. Last accessed July 15, 2020.

Ministry of Citizenship and Multiculturalism (MCM)

2005 *Heritage Resources in the Land Use Planning Process, InfoSheet#5*. Available online: [http://www.mtc.gov.on.ca/en/publications/Heritage\\_Tool\\_Kit\\_Heritage\\_PPS\\_info\\_Sheet.pdf](http://www.mtc.gov.on.ca/en/publications/Heritage_Tool_Kit_Heritage_PPS_info_Sheet.pdf). Last accessed July 22, 2021.



- 2007 Eight Guiding Principles in the Conservation of Built Heritage Properties.  
Available online:  
[http://www.mtc.gov.on.ca/en/publications/InfoSheet\\_8%20Guiding\\_Principles.pdf](http://www.mtc.gov.on.ca/en/publications/InfoSheet_8%20Guiding_Principles.pdf)  
. Last accessed July 15, 2020.
- 2017 *Standards and Guidelines for Conservation of Provincial Heritage Properties – Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties*. Ontario Ministry of Citizenship and Multiculturalism, Toronto.
- Parks Canada
- 2010 Standards and Guidelines for the Conservation of Historic Places in Canada.  
Available online: <https://www.historicplaces.ca/media/18072/81468-parks-s+g-eng-web2.pdf>. Last accessed September 21, 2020.

# **Appendix A: Designation By-law for the Postal Delivery Building**





An agency of the Government of Ontario



Un organisme du gouvernement de l'Ontario

This document was retrieved from the Ontario Heritage Act e-Register, which is accessible through the website of the Ontario Heritage Trust at **[www.heritagetrust.on.ca](http://www.heritagetrust.on.ca)**.

---

Ce document est tiré du registre électronique, tenu aux fins de la *Loi sur le patrimoine de l'Ontario*, accessible à partir du site Web de la Fiducie du patrimoine ontarien sur **[www.heritagetrust.on.ca](http://www.heritagetrust.on.ca)**.

IN THE MATTER OF THE ONTARIO HERITAGE ACT  
R.S.O. 1980, CHAPTER 337 AND  
40 BAY STREET, CITY OF TORONTO  
PROVINCE OF ONTARIO

NOTICE OF PASSING OF BY-LAW

To:



40 Bay Street Holdings Inc.  
100 King Street West  
Suite 6600  
1 First Canadian Place  
Toronto, Ontario  
M5X 1B8



✓ Ontario Heritage Foundation

Take notice that the Council of the Corporation of the City of Toronto has passed By-law No. 360-90 to designate the above-mentioned property to be of architectural and historical value or interest.

Dated at Toronto this 17th day of July, 1990.

*Barbara G. Caplan*  
Barbara G. Caplan  
City Clerk

RECEIVED  
IN THE OFFICE

AUG 3 1990  
ARCHITECTURE AND  
PLANNING  
HERITAGE BRANCH

~~RECEIVED~~

~~JUL 26 1990~~

~~MINISTRY OF TOURISM &  
RECREATION~~

ONTARIO HERITAGE  
FOUNDATION

AUG 2 1990

*Nancy Smith*

No. 360-90. A BY-LAW

*To designate the property at 40 Bay Street (Postal Delivery Building) of architectural and historical value or interest.*

(Passed June 25, 1990.)

Whereas by Clause 5 of Neighbourhoods Committee Report No. 8, adopted by Council at its meeting held on June 25, 1990, authority was granted to designate the property at 40 Bay Street (Postal Delivery Building) of architectural value or interest; and

Whereas the Ontario Heritage Act authorizes the Council of a municipality to enact by-laws to designate real property, including all the buildings and structures thereon, to be of historic or architectural value or interest; and

Whereas the Council of The Corporation of the City of Toronto has caused to be served upon the owners of the lands and premises known as 40 Bay Street (Postal Delivery Building) and upon the Ontario Heritage Foundation notice of intention to so designate the aforesaid real property and has caused such notice of intention to be published in a newspaper having a general circulation in the municipality once for each of three consecutive weeks; and

Whereas the reasons for designation are set out in Schedule "B" hereto; and

Whereas no notice of objection to the said proposed designation has been served upon the clerk of the municipality;

Therefore the Council of The Corporation of the City of Toronto enacts as follows:

1. There is designated as being of architectural and historical value or interest the real property more particularly described and shown on Schedules "A" and "C" hereto, known as 40 Bay Street (Postal Delivery Building).
2. The City Solicitor is hereby authorized to cause a copy of this by-law to be registered against the property described in Schedule "A" hereto in the proper land registry office.
3. The City Clerk is hereby authorized to cause a copy of this by-law to be served upon the owner of the aforesaid property and upon the Ontario Heritage Foundation and to cause notice of this by-law to be published in a newspaper having general circulation in the City of Toronto.

ARTHUR C. EGGLETON,  
*Mayor.*

BARBARA G. CAPLAN  
*City Clerk.*

Council Chamber,  
Toronto, June 25, 1990.  
(L.S.)

## SCHEDULE "A"

In the City of Toronto, in the Municipality of Metropolitan Toronto and Province of Ontario, being composed of Block 4 according to Plan 655E, part of Block 1 according to Plan 642E and part of Lake Street as shown on Plans 536E and 642E, all the said Plans being registered in the Land Registry Office for the Registry Division of Toronto (No. 63), the said part of Lake Street closed by City of Toronto By-law 14951 registered in the said Land Registry Office as Instrument 15889E.S., the boundaries of the said land being described as follows:

PREMISING that the bearings hereinafter mentioned are astronomic and are referred to the Central Meridian 79 degrees and 30 minutes West Longitude, through Zone 10 of the Ontario Coordinate System, then;

COMMENCING at the intersection of the westerly limit of Bay Street with the north-easterly production of the south-easterly limit of the said Block 4, being the south-easterly angle of PART 21 on a plan of survey deposited in the said Land Registry Office as 63R-3515;

THENCE South 55 degrees 40 minutes and 00 seconds West along the said production to and along the said south-easterly limit of Block 4, being along the north-westerly limit of Lake Shore Boulevard West, a distance of 131.13 metres more or less to the south-westerly angle of the said Block 4;

THENCE northerly along the westerly limit of the said Block 4, to and along the westerly limit of the said Block 1, in all a distance of 85.10 metres more or less to the north-westerly angle of the said Block 1;

THENCE North 48 degrees 53 minutes and 50 seconds East along the north-westerly limit of the said Block 1, being along the north-westerly limit of PART 21 a distance of 37.77 metres;

THENCE North 49 degrees 03 minutes and 00 seconds East along the said north-westerly limit of Block 1 being along the north-westerly limit of PART 21 a distance of 20.02 metres;

THENCE North 49 degrees 40 minutes and 00 seconds East along the said north-westerly limit of PART 21 a distance of 19.89 metres;

THENCE North 50 degrees 58 minutes and 40 second East along the said north-westerly limit of PART 21 a distance of 19.76 metres;

THENCE North 52 degrees 58 minutes and 30 seconds East along the said north-westerly limit of PART 21 a distance of 19.61 metres;

THENCE North 55 degrees 40 minutes and 20 seconds East along the said north-westerly limit of PART 21 a distance of 19.38 metres more or less to the north-easterly angle of the said PART 21, being a point in the westerly limit of Bay Street;

THENCE South 17 degrees 00 minutes and 20 seconds East along the easterly limit of the said PART 21, being the said westerly limit of Bay Street, 97.35 metres more or less to the point of commencement.

## SCHEDULE "B"

Reasons for the designation of the property at 40 Bay Street (Postal Delivery Building).

The property at 40 Bay Street (Postal Delivery Building) is designated on architectural and historical grounds. The Postal Delivery Building was constructed in 1939-40 according to the designs of Toronto architect Charles Dolphin and Public Works architect C.D. Sutherland. The building served as the central mail distribution centre in Toronto for the Canadian Postal Service.

The building, constructed of limestone and polished granite, is distinguished by its horizontal bands of fenestration and its bas relief sculpture, characteristic of the Art Moderne and Art Deco styles. Set on an irregular site at the intersection of Bay Street and Lake Shore Boulevard West, the elevations are treated in a similar manner. In the centre of each wall, multiple fenestration is

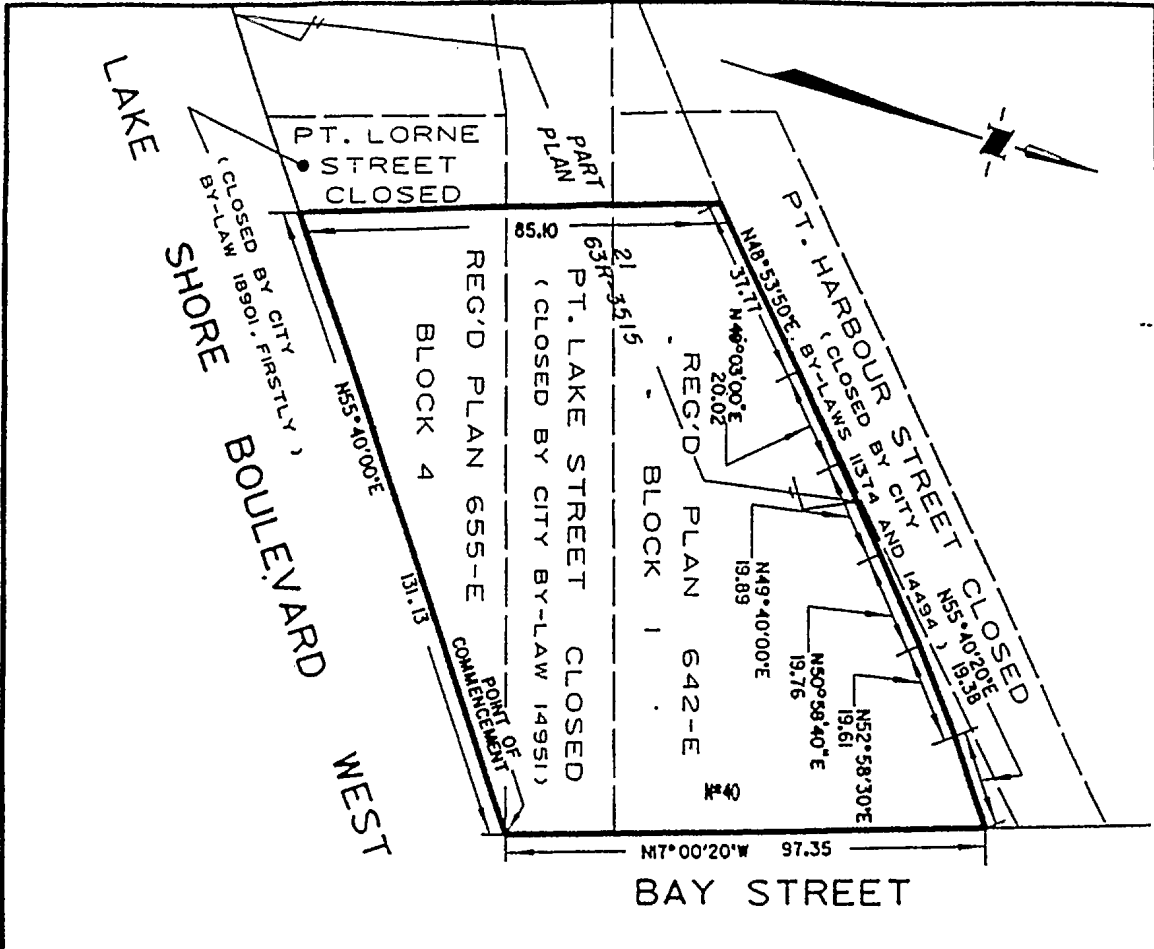


inset between stone piers, while the rounded corners of the building feature two bands of wraparound metal windows. The principal entrances flank the southeast corner. Attention is focused on the east elevation with the name band and the bronze Canadian coat-of-arms. The stylized sculptural program reflects the history of communication and transportation in Canada through a progressive series of corner panels.

The Postal Delivery Building is a significant public commission, designed by a local architect in conjunction with the Department of Public Works. The sculptural program is one of the most extensive in the City of Toronto, demonstrating the role of the federal government as a patron of the arts.

SCHEDULE "C"

MAP AREA 506 23



Bearings hereon are astronomic and are referred to the central meridian  $79^{\circ}30'$  west longitude in zone 10 of the Ontario Co-ordinate System.

DESIGNATED AS BEING OF ARCHITECTURAL AND HISTORICAL VALUE OR INTEREST CITY OF TORONTO BY-LAW

**NOTE**  
THIS IS NOT A PLAN OF SURVEY

DEPARTMENT OF PUBLIC WORKS  
CITY OF TORONTO

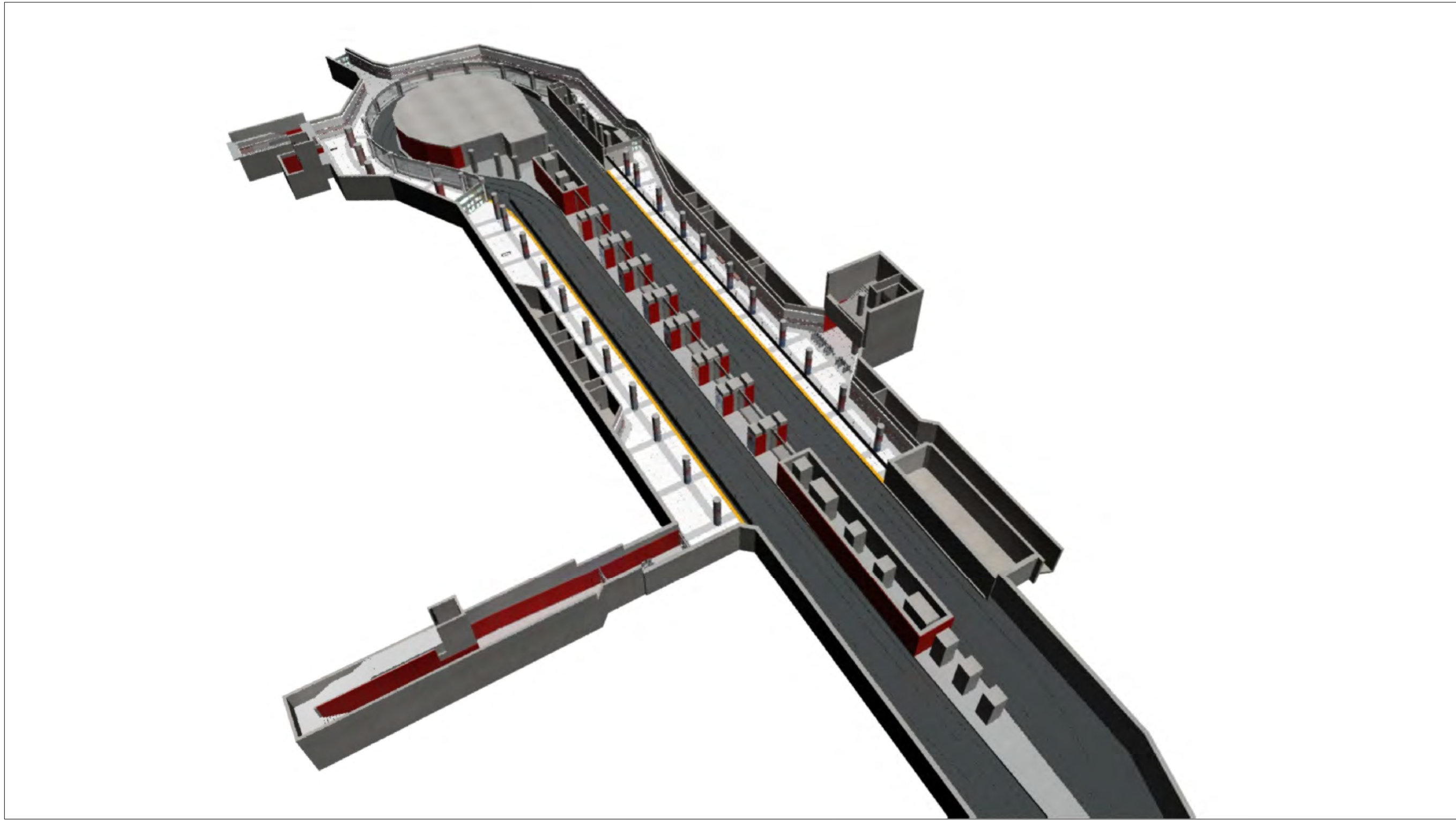
SKETCH TO ILLUSTRATE  
PART OF LAKE STREET (CLOSED BY CITY BY-LAW 149511)  
REGISTERED PLAN 656-E AND 642-E  
PART OF BLOCK 1, REGISTERED PLAN 642-E  
AND BLOCK 4, REGISTERED PLAN 655-E  
CITY OF TORONTO  
MUNICIPALITY OF METROPOLITAN TORONTO  
RATIO 1:1200

Scale bar: 0 to 40 metres

Drawn: G.B.  
Checked: R.H. Creve - City Surveyor  
Approved: [Signature] FILE: B7-16  
PLAN SYE2411  
DATE: MAY 1990  
D.L.S.

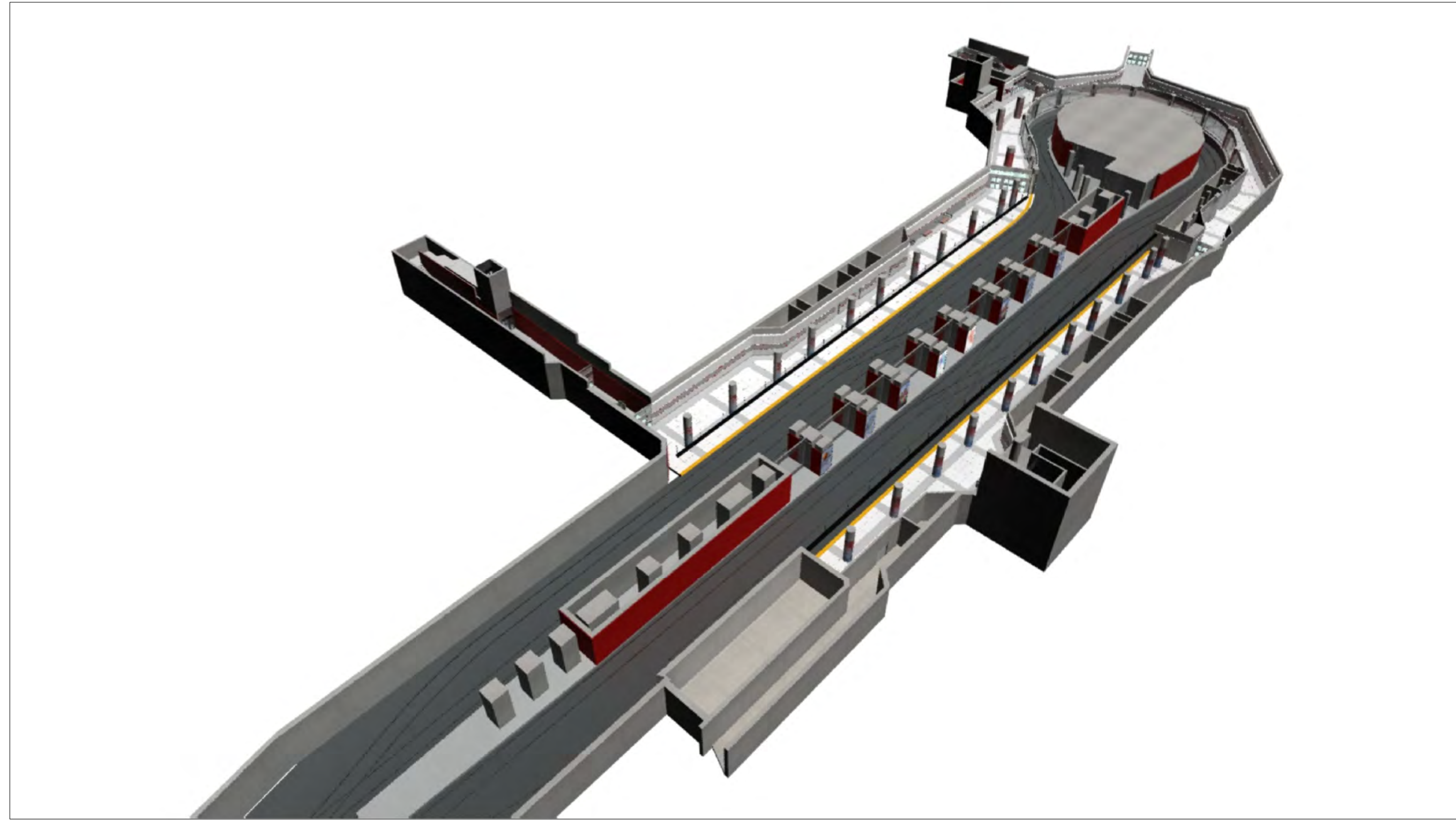
## **Appendix B: Proposed Work**





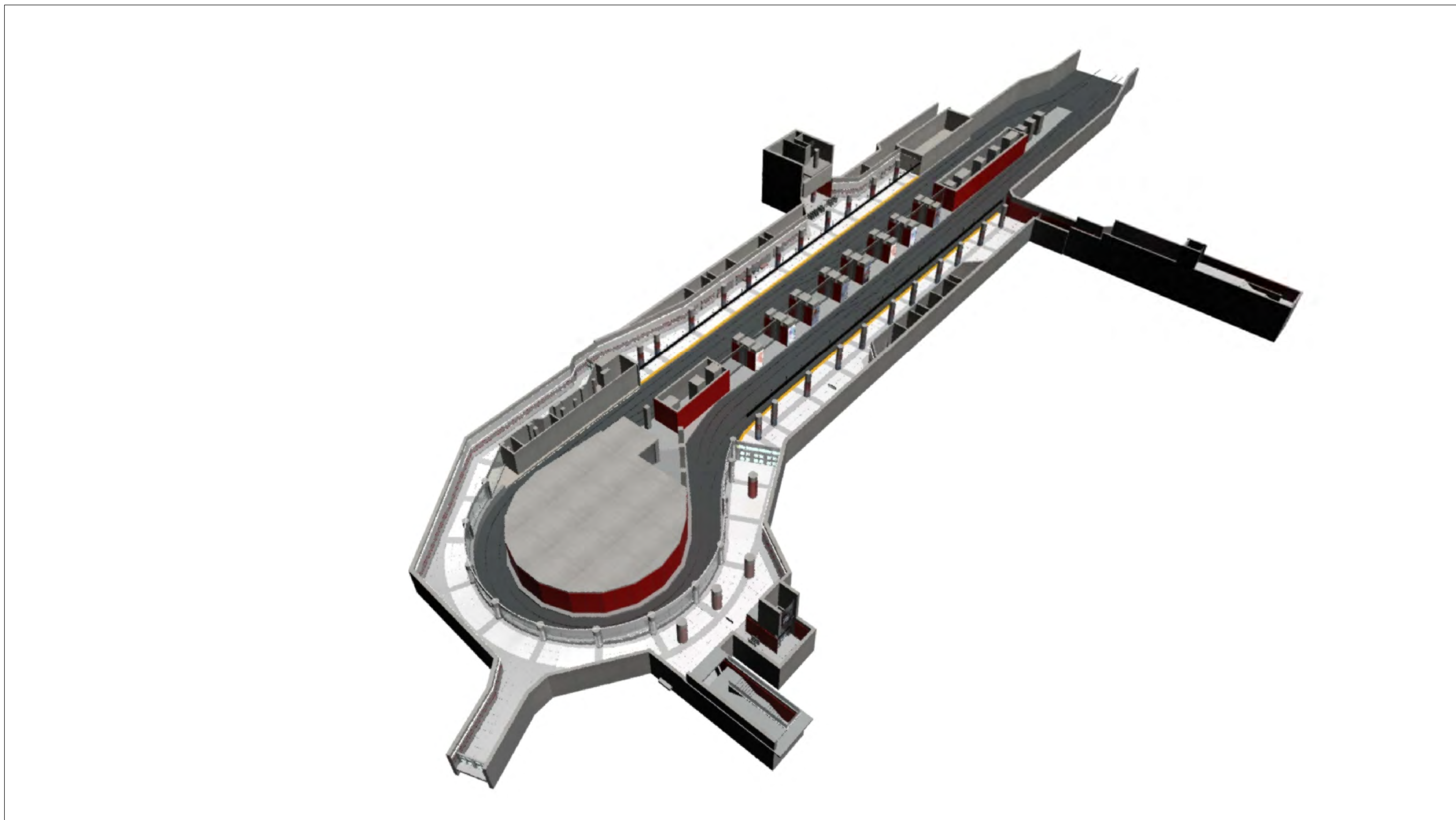
DETAIL  
3D RENDER

3  
AA011



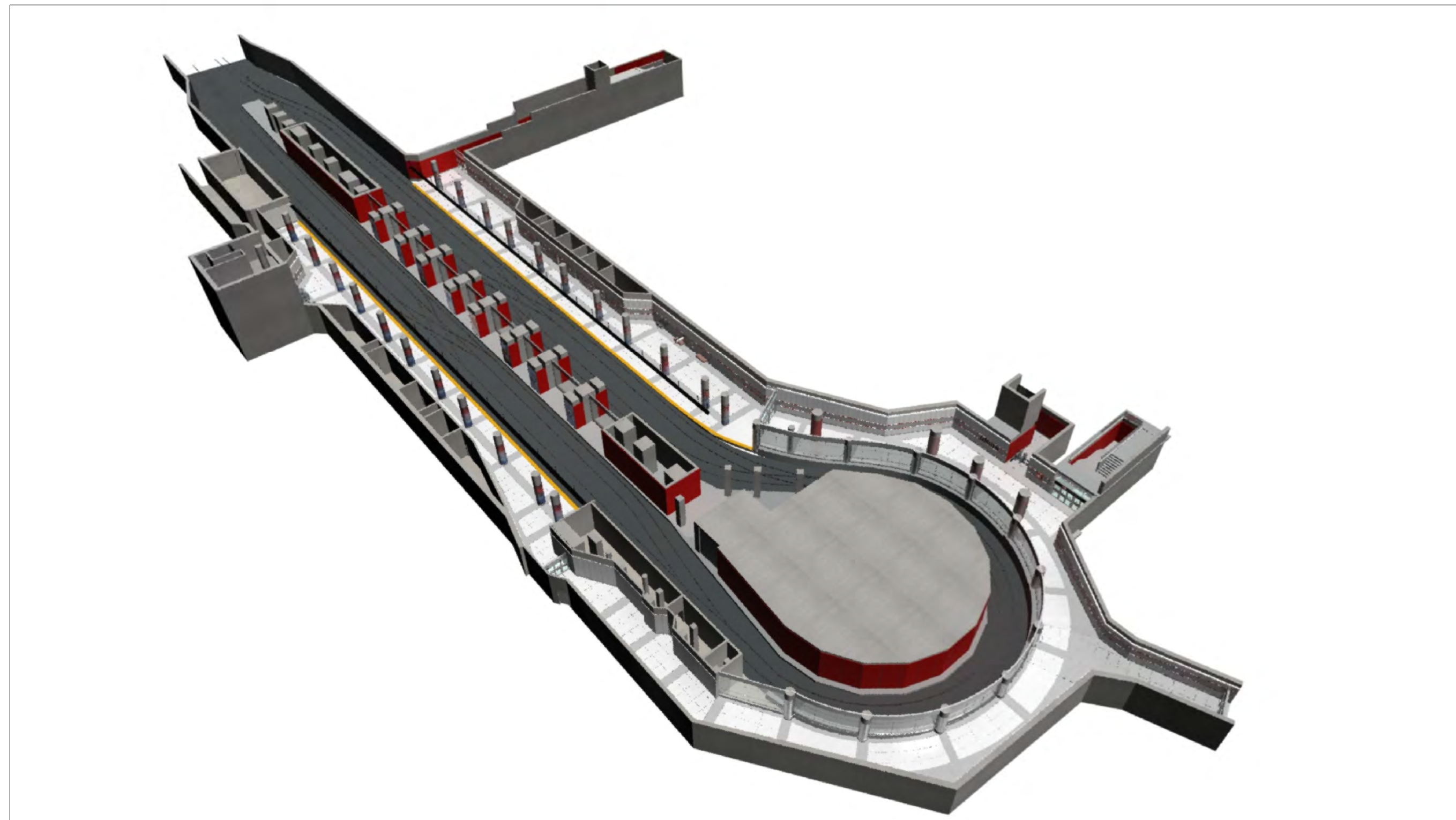
DETAIL  
3D RENDER

1  
AA011



DETAIL  
3D RENDER

4  
AA011



DETAIL  
3D RENDER

2  
AA011

REVISIONS	REVISIONS
△	△
△	△
△	△

30% REVIEW  
DRAWING  
NOT FOR CONSTRUCTION

**wood.**

WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS  
3450 HARVESTER ROAD, SUITE 100  
BURLINGTON, ONTARIO, CANADA L7N 3W5  
TEL: +1 905 336 2362

DRAWN	E. ENGLAND	YYYY/MM/DD	2020/10/23
CHECKED	O. PHELAN		
CORRECT	J. STRASMAN		
SCALE	N.T.S.		

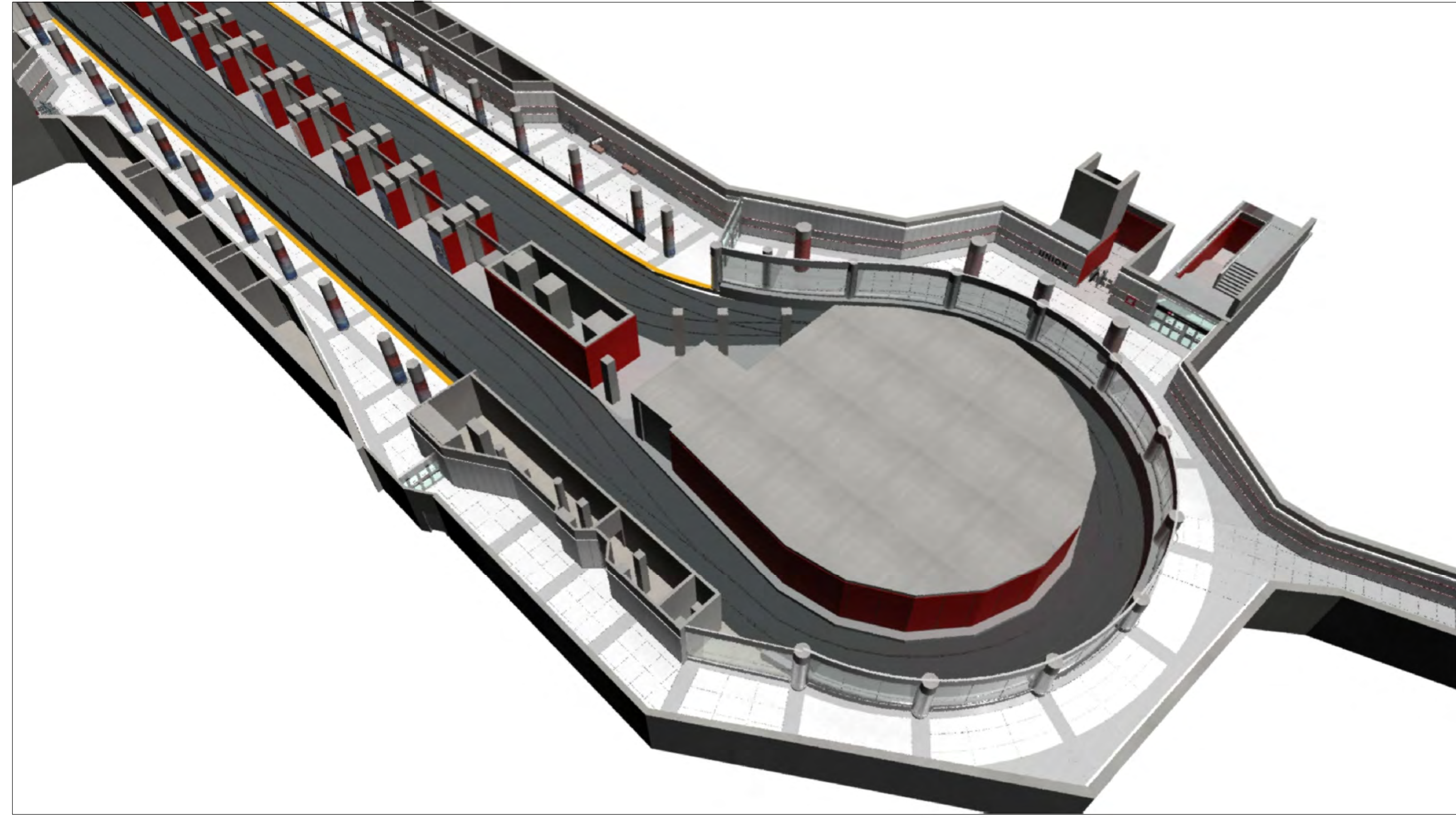
WATERFRONT EAST LRT  
UNION STATION QUEENS QUAY LINK

UNION STATION

AERIAL VIEW RENDERS

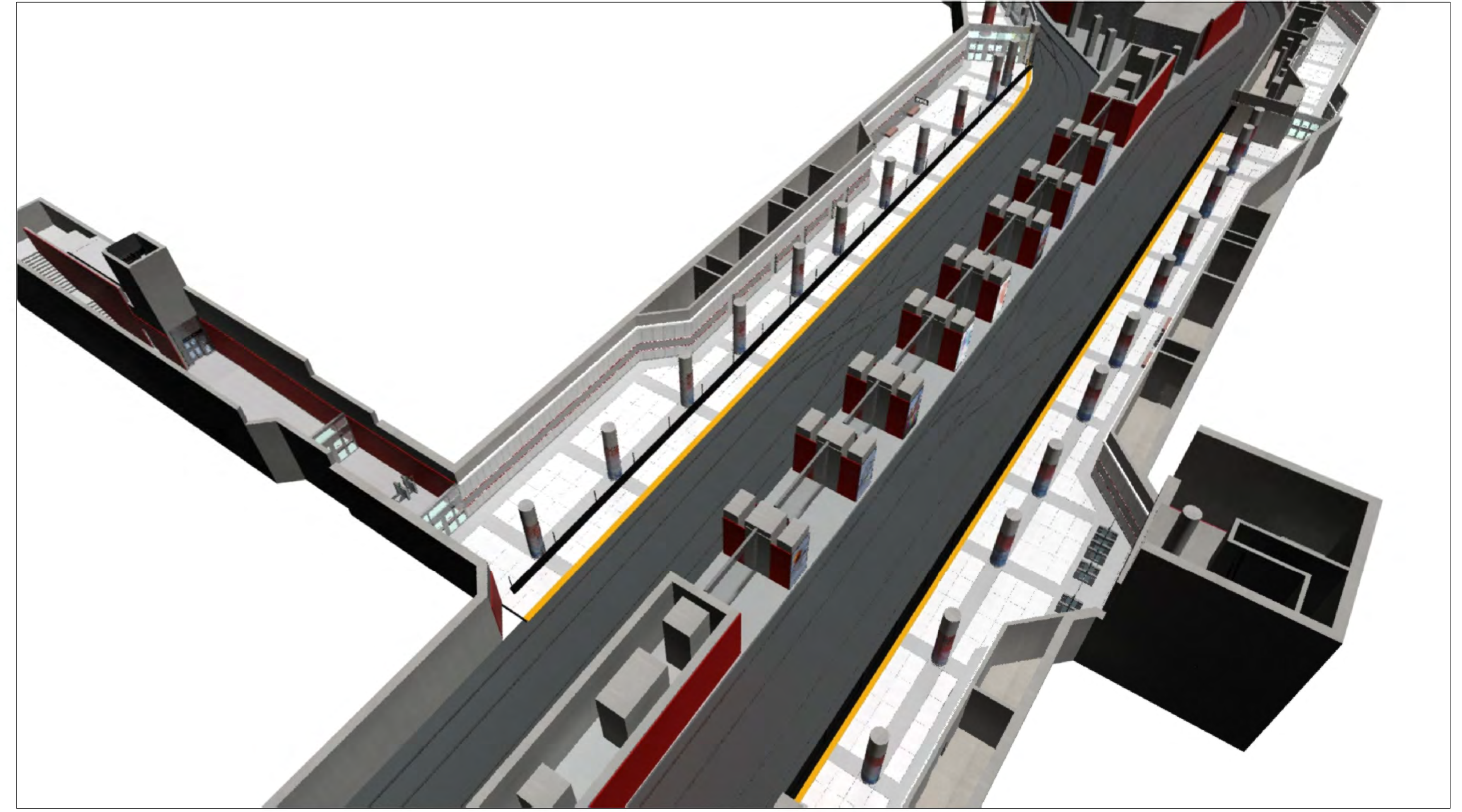
Contract: RE35-1		
TORONTO TRANSIT COMMISSION ENGINEERING DEPARTMENT		
Dwg. No.	Rev. No.	Sheet No.
RE35-1-AA011		





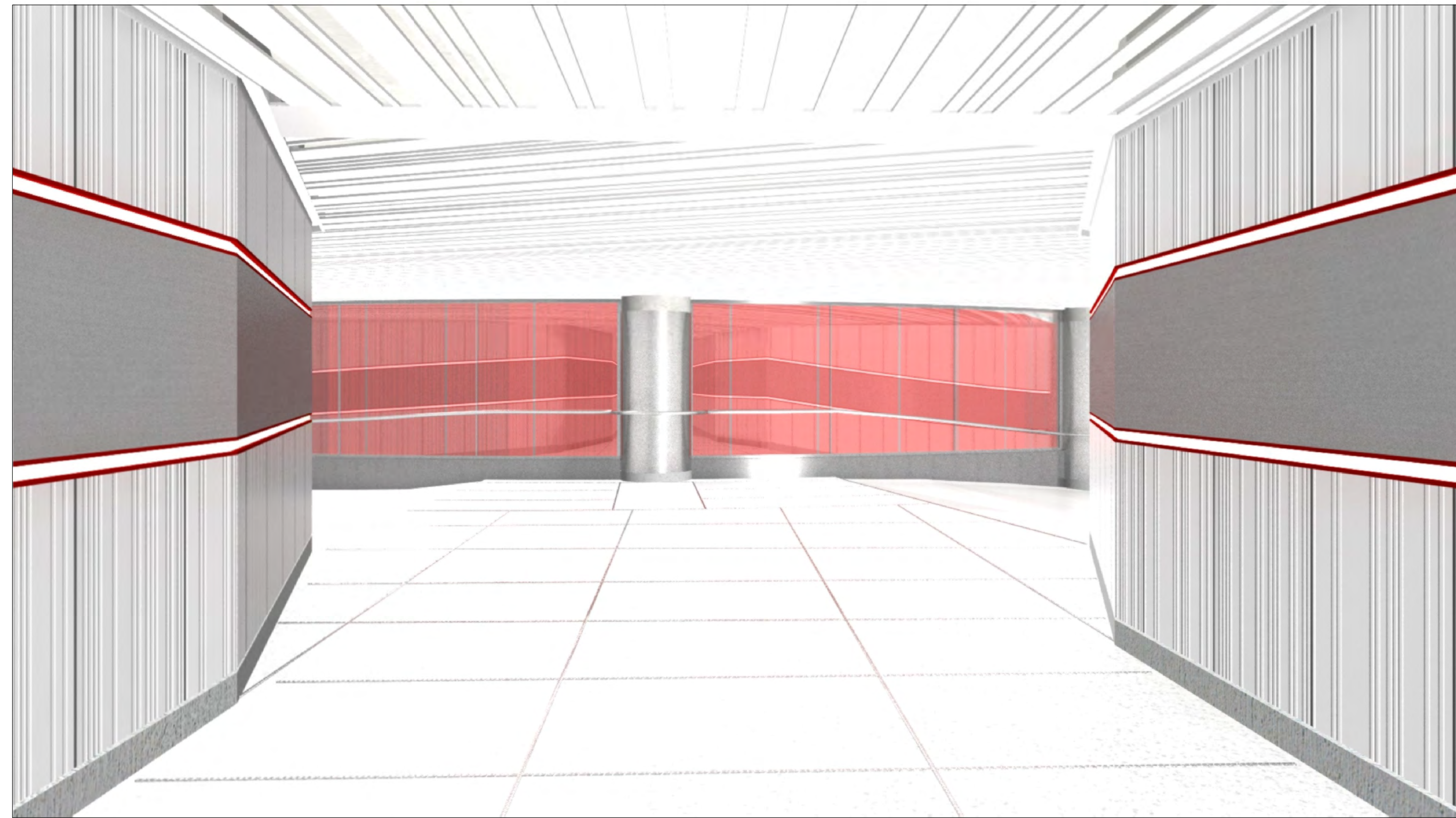
DETAIL  
3D RENDER

3  
AA012



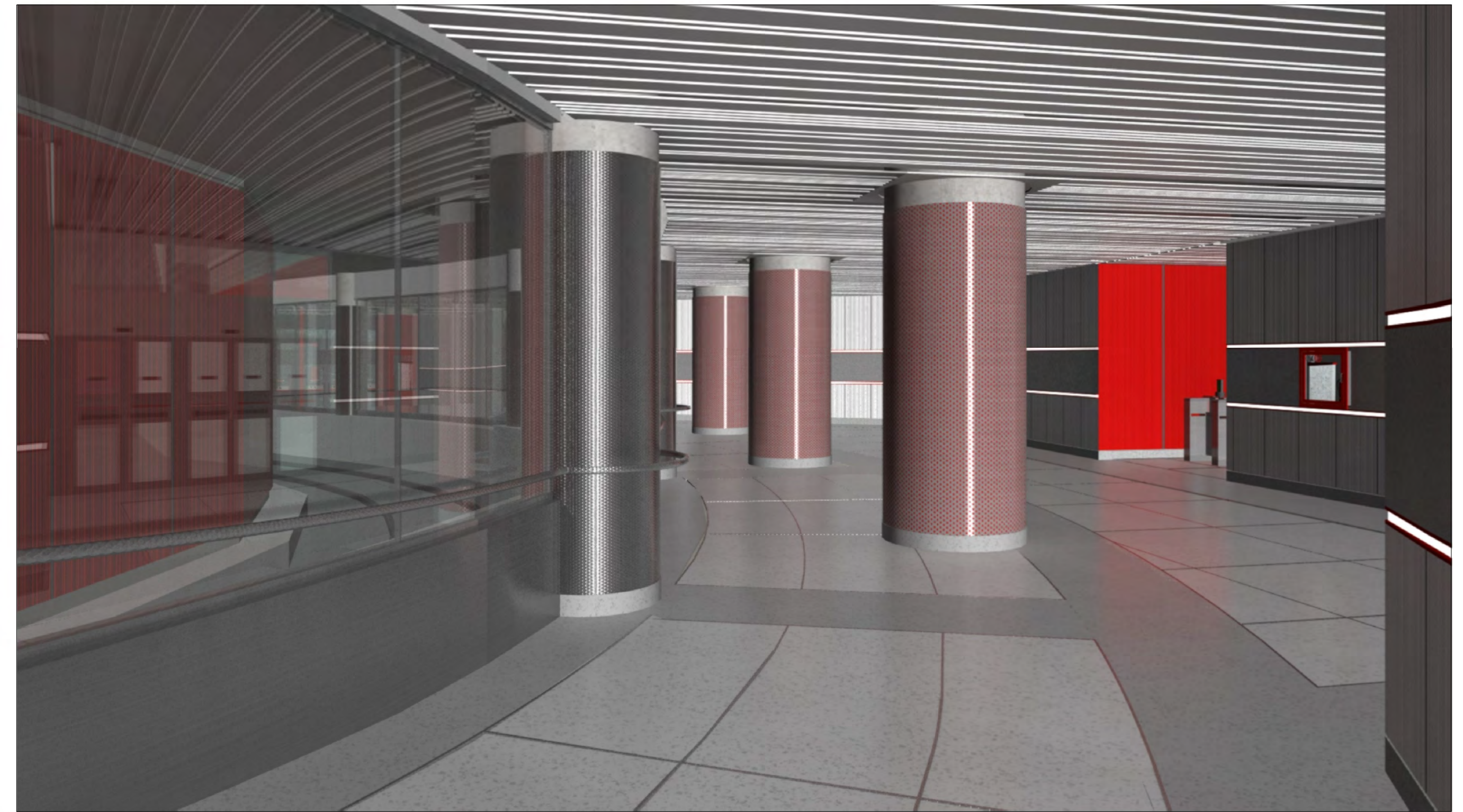
DETAIL  
3D RENDER

1  
AA012



DETAIL  
3D RENDER

4  
AA012



DETAIL  
3D RENDER

2  
AA012

REVISIONS	REVISIONS
△	△
△	△
△	△

30% REVIEW  
DRAWING  
NOT FOR CONSTRUCTION



WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS  
3450 HARVESTER ROAD, SUITE 100  
BURLINGTON, ONTARIO, CANADA L7N 3W5  
TEL: +1 905 336 2362

DRAWN	E. ENGLAND	YYYY/MM/DD	2020/10/23
CHECKED	O. PHELAN		
CORRECT	J. STRASMAN		
SCALE	N.T.S.		

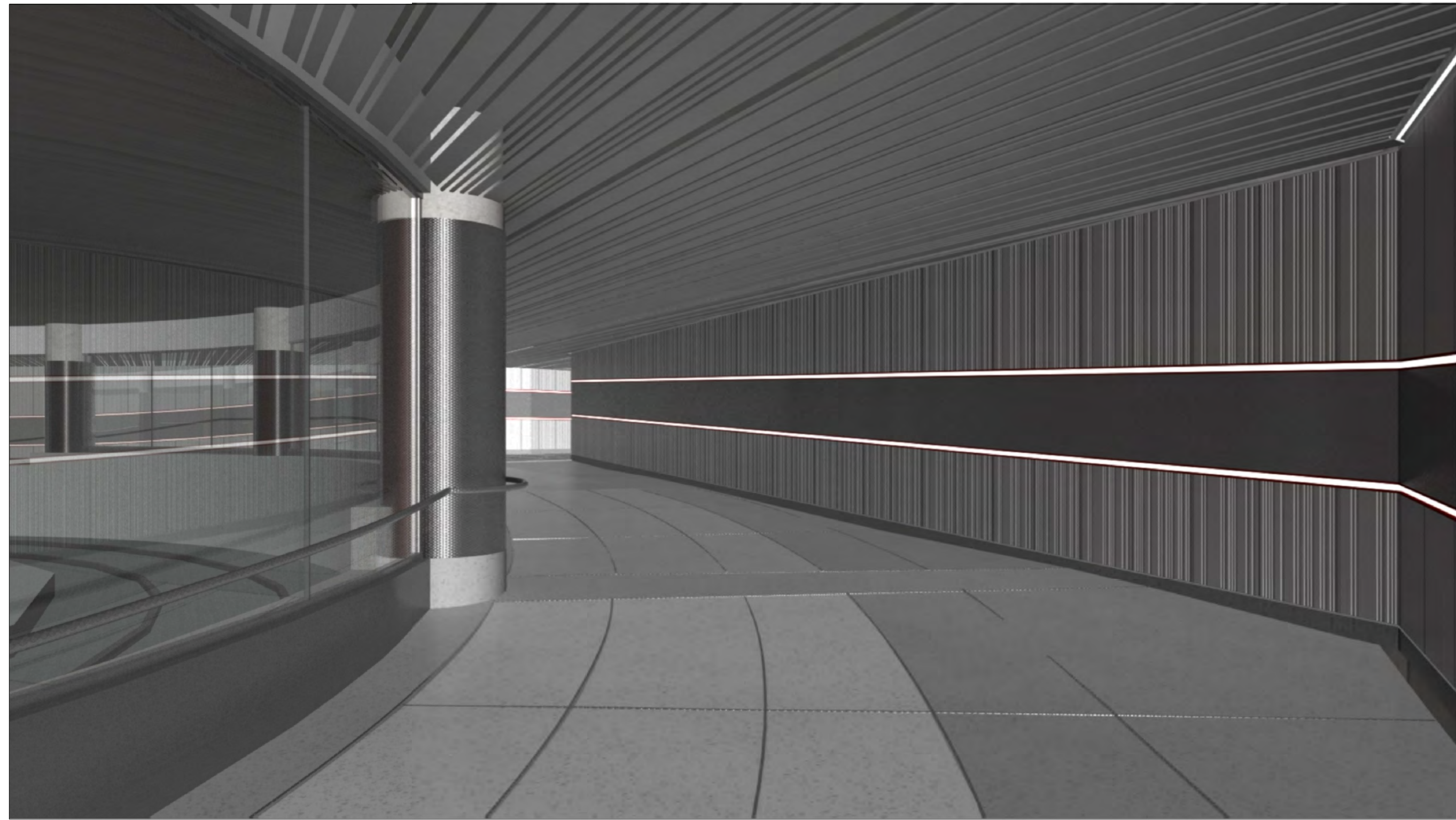
WATERFRONT EAST LRT  
UNION STATION QUEENS QUAY LINK

UNION STATION

PLATFORM LEVEL VIEWS  
3D AXONOMETRIC RENDERS

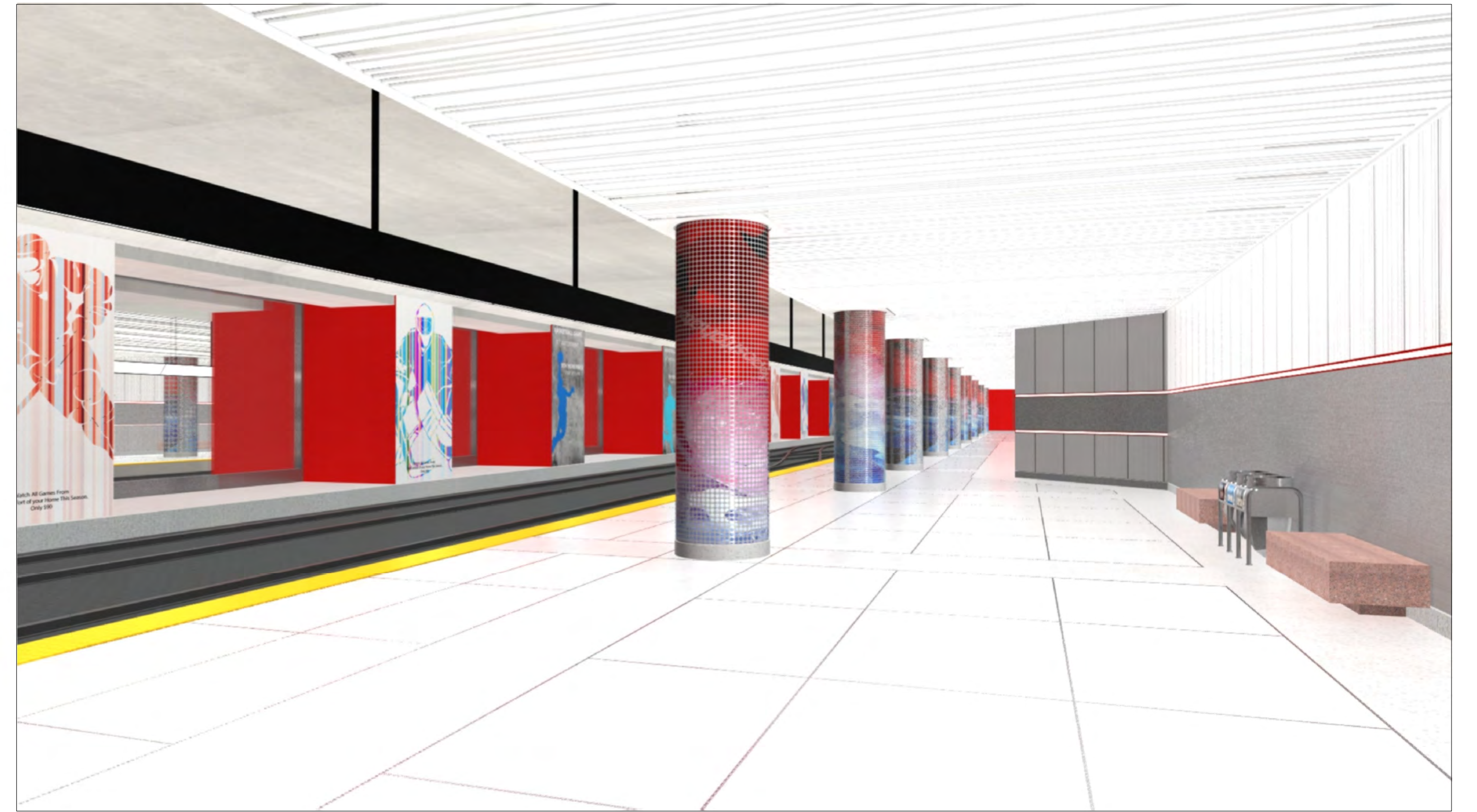
Contract: RE35-1		
TORONTO TRANSIT COMMISSION ENGINEERING DEPARTMENT		
Dwg. No.	Rev. No.	Sheet No.
RE35-1-AA012		
Plot Date: 7-JUL-2021		





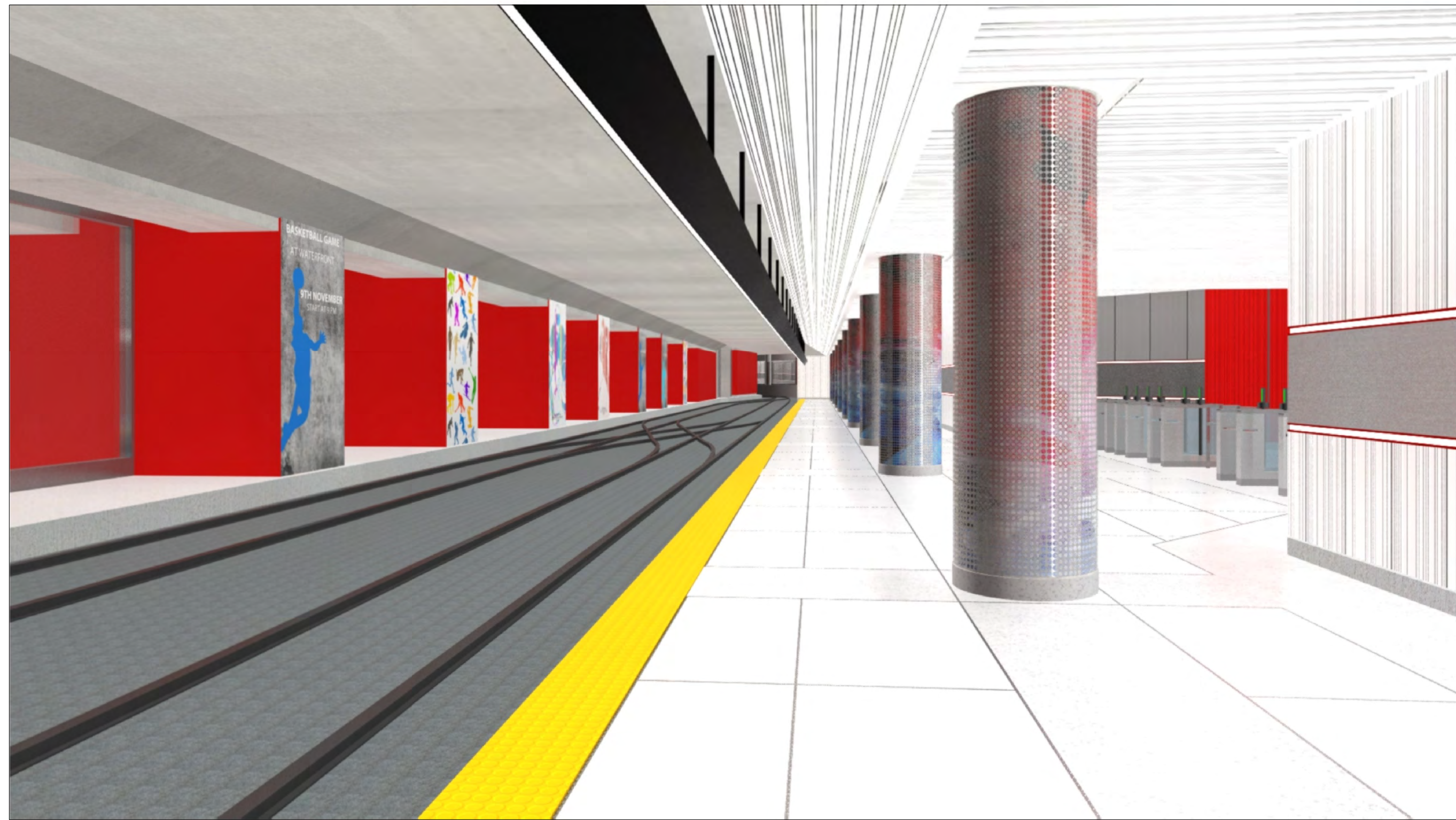
DETAIL  
3D RENDER

3  
AA013



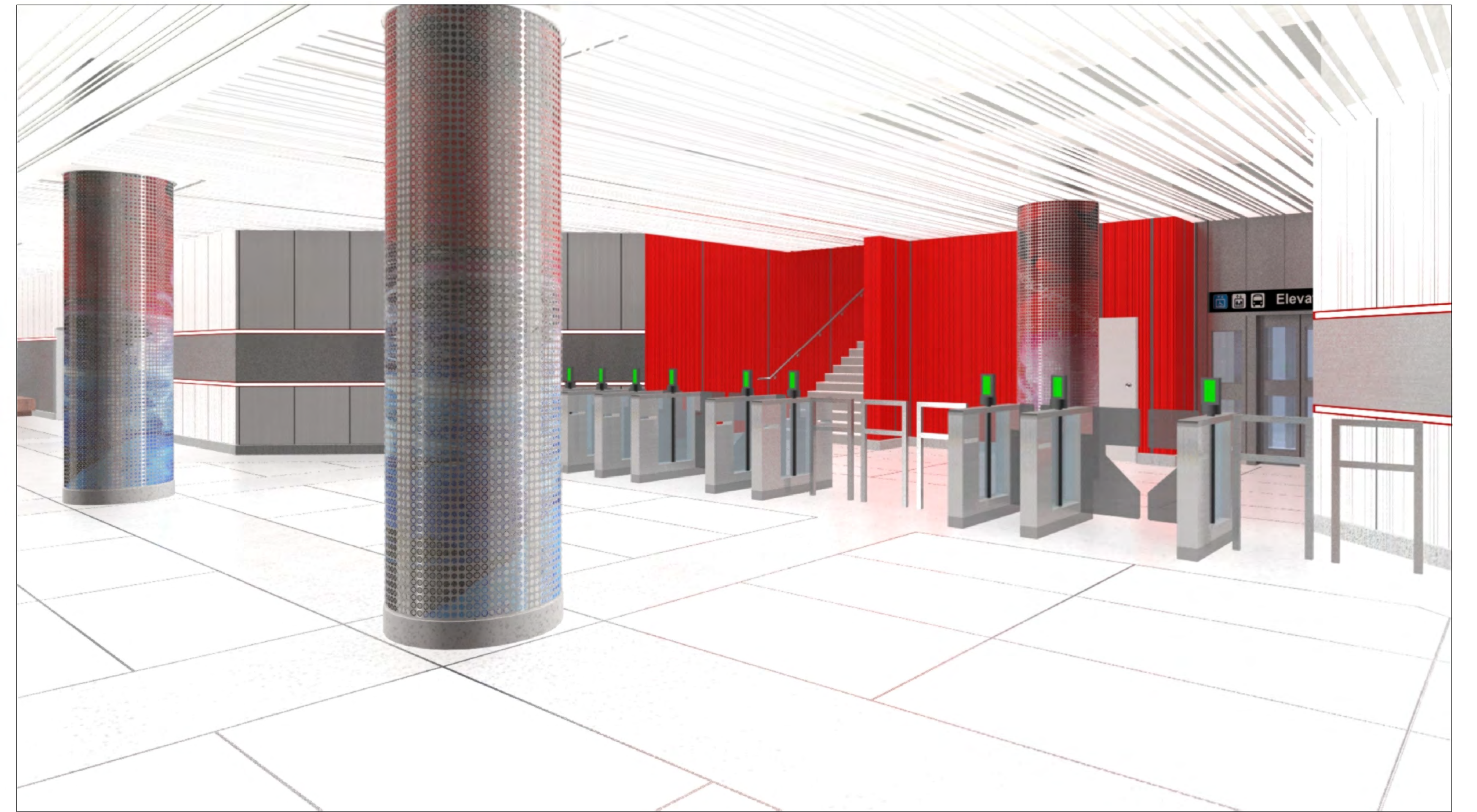
DETAIL  
3D RENDER

1  
AA013



DETAIL  
3D RENDER

4  
AA013



DETAIL  
3D RENDER

2  
AA013

REVISIONS	REVISIONS
△	△
△	△
△	△

30% REVIEW  
DRAWING  
NOT FOR CONSTRUCTION

**wood.**

WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS  
3450 HARVESTER ROAD, SUITE 100  
BURLINGTON, ONTARIO, CANADA L7N 3W5  
TEL: +1 905 335 2302

DRAWN	E.ENGLAND	YYYY/MM/DD	2020/10/23
CHECKED	O.PHELAN		
CORRECT	J.STRASMAN		
SCALE	N.T.S.		

WATERFRONT EAST LRT  
UNION STATION QUEENS QUAY LINK

UNION STATION

PLATFORM LEVEL VIEWS  
3D AXONOMETRIC RENDERS

Contract: RE35-1

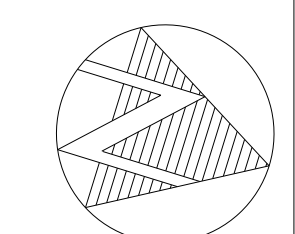


TORONTO TRANSIT COMMISSION  
ENGINEERING DEPARTMENT

Dwg. No. RE35-1-AA013 Rev. No. Sheet No.

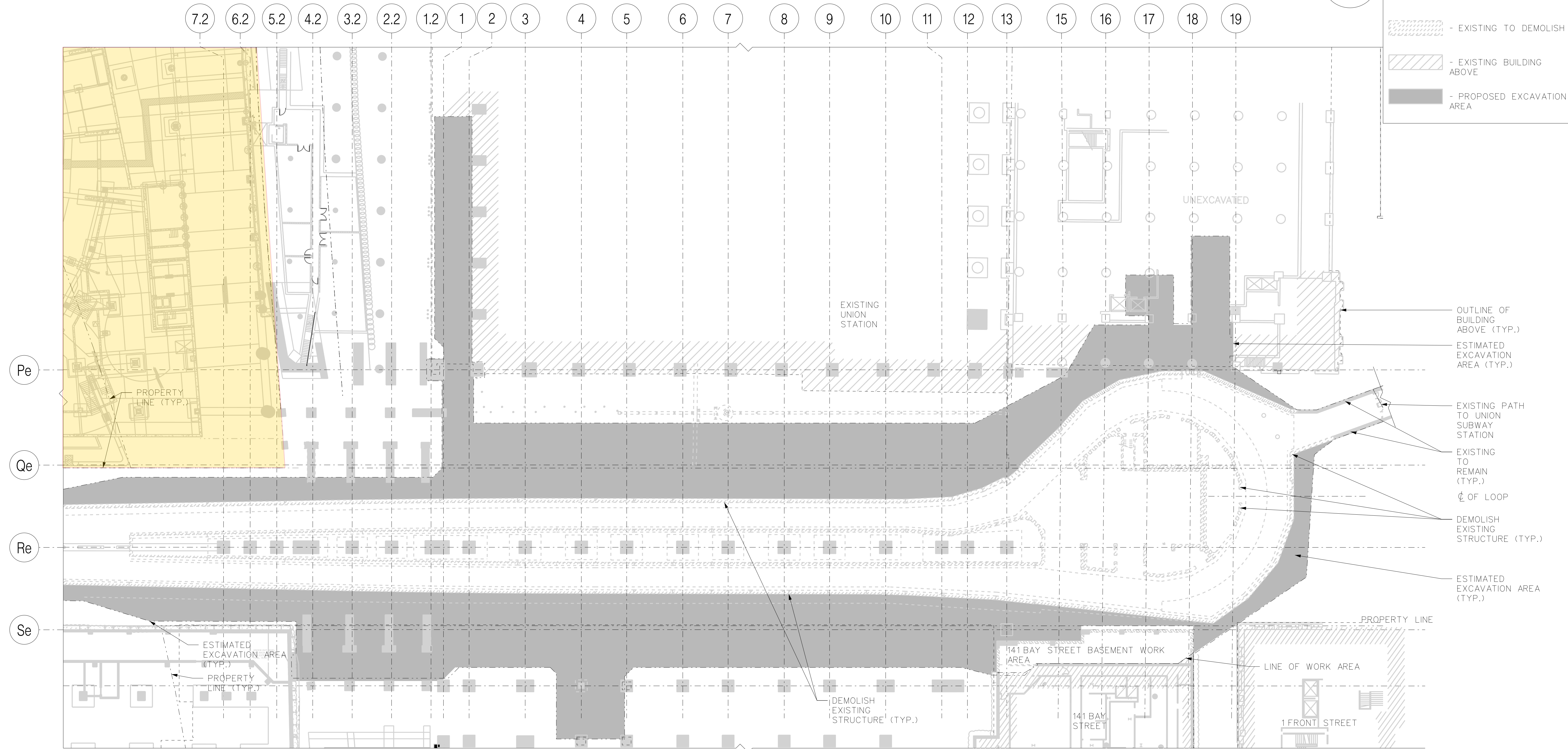
Plot Date: 7-JUL-2021





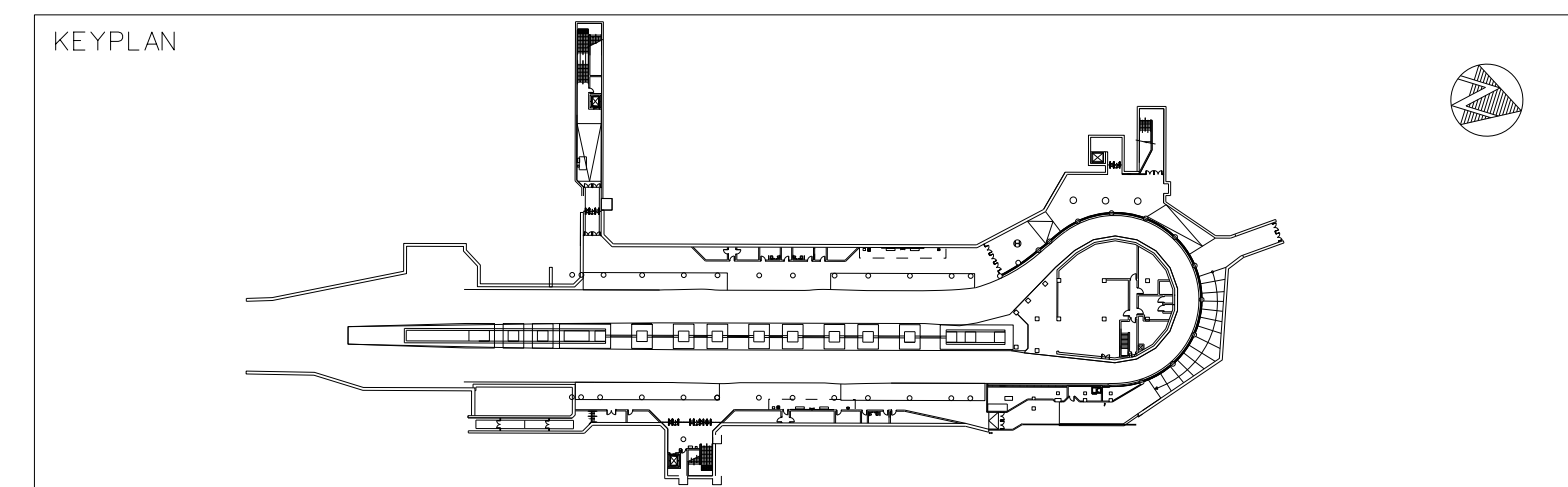
**LEGEND**

- EXISTING TO REMAIN
- EXISTING TO DEMOLISH
- EXISTING BUILDING ABOVE
- PROPOSED EXCAVATION AREA



**PLAN**  
UNION STATION STREETCAR LOOP PLATFORM LEVEL  
DEMOLITION PLAN

1  
AD001



REVISIONS	REVISIONS
△	△
△	△
△	△

**30% REVIEW DRAWING**  
NOT FOR CONSTRUCTION



WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS  
3450 HARVESTER ROAD, SUITE 100  
BURLINGTON, ONTARIO, CANADA L7N 3W5  
TEL: +1 905 336 2302

DRAWN ... E.ENGLAND ... YYYY/MM/DD  
2020/10/17  
CHECKED ... O.PHELAN ...  
CORRECT ... J.STRASMANN ...  
SCALE  
0 2 4 6 8 10 12m

**WATERFRONT EAST LRT  
UNION STATION QUEENS QUAY LINK**

**UNION STATION**

PLATFORM LEVEL  
DEMOLITION PLAN

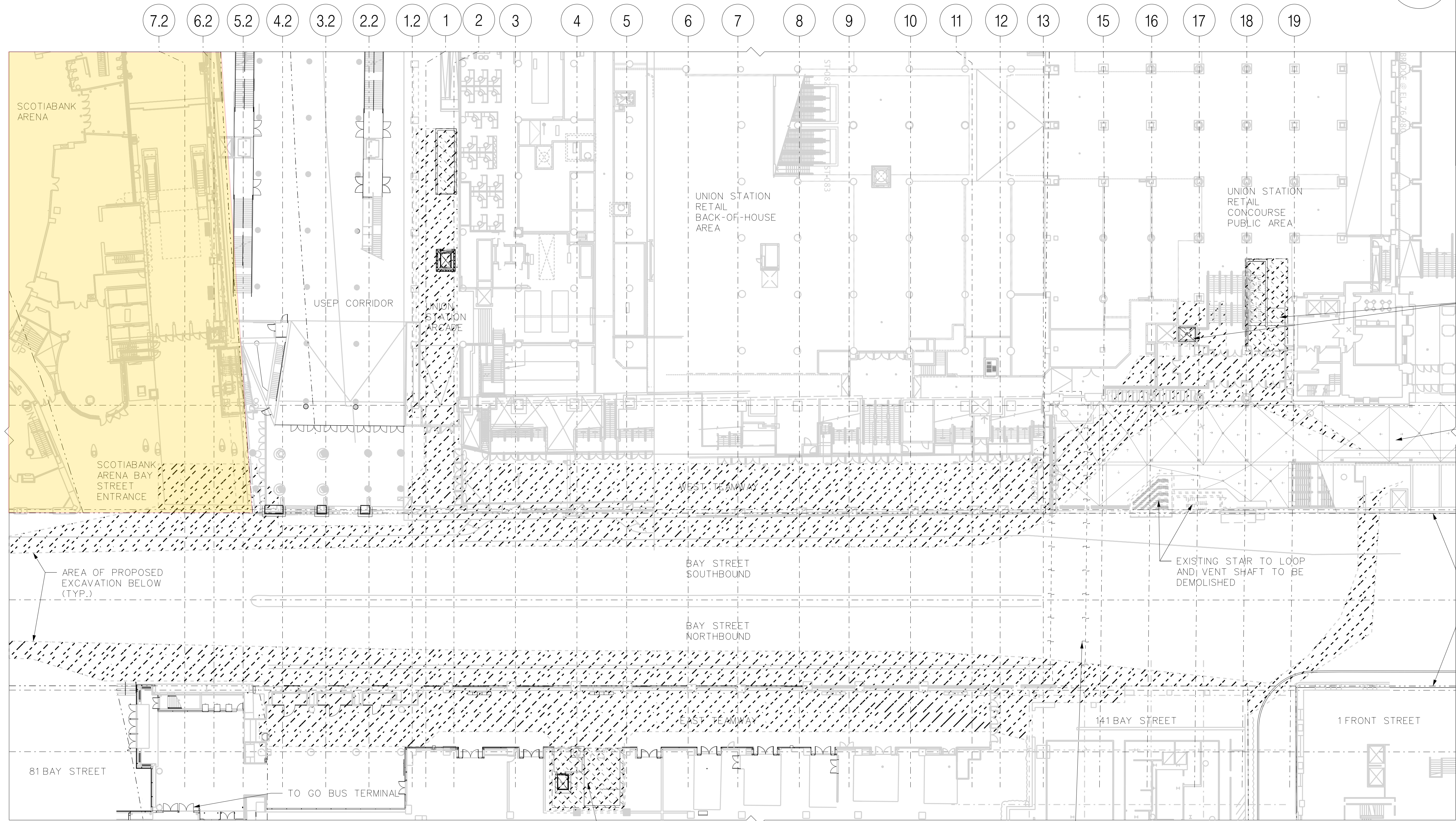
Contract: RE35-1

TORONTO TRANSIT COMMISSION  
ENGINEERING DEPARTMENT

Dwg. No. RE35-1-AD001 Rev. No. Sheet No.

Plot Date: 7-JUL-2021



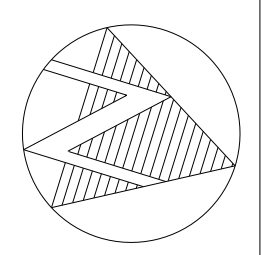
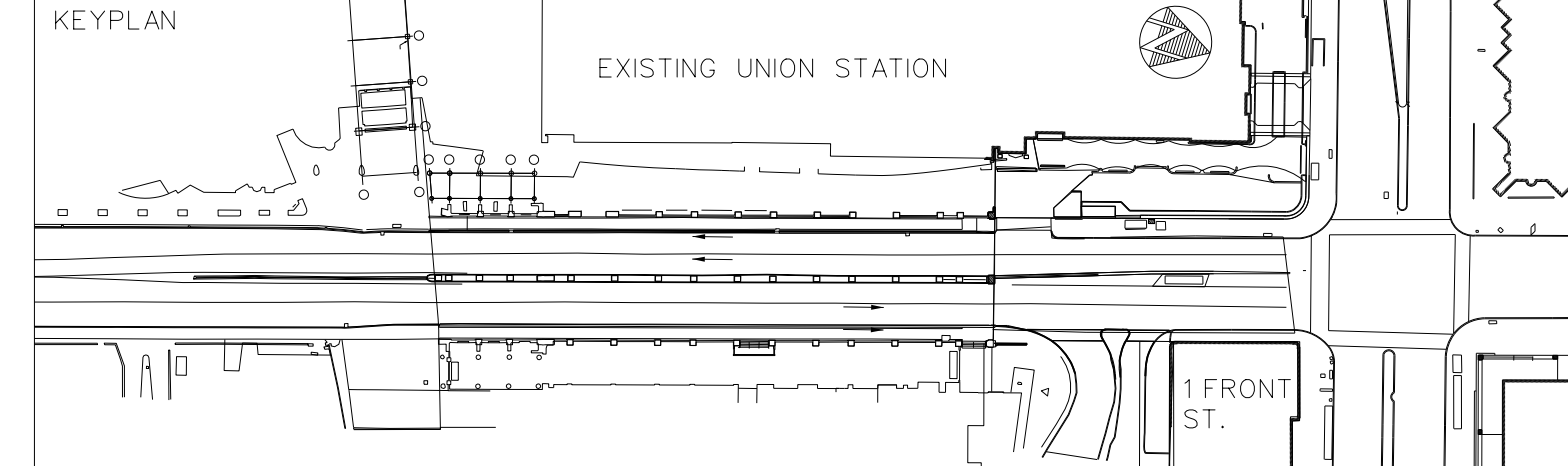


**PLAN**  
UNION STATION STREETCAR LOOP STREET LEVEL  
DEMOLITION PLAN

1  
AD002

PROPOSED EXTENT OF  
DEMOLITION AT STREET LEVEL  
(TYP.)

OUTLINE OF 141 BAY  
PEDESTRIAN BRIDGE  
ABOVE



**LEGEND**

- [Solid Grey Box] - EXISTING TO REMAIN
- [Hatched Box] - EXISTING TO DEMOLISH
- [Diagonal Line Box] - EXISTING BUILDING ABOVE
- [Dashed Line Box] - EXTENT OF EXCAVATION AREA BELOW
- [Dotted Line Box] - STREET LEV. PROPOSED DEMOLITION AREA

PROPOSED EXTENT OF  
DEMOLITION AT STREET LEVEL  
(TYP.)

BAY STREET MOAT

PROPERTY LINE (TYP.)

REVISIONS	REVISIONS

**30% REVIEW  
DRAWING**  
NOT FOR CONSTRUCTION



WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS  
3450 HARVESTER ROAD, SUITE 100  
BURLINGTON, ONTARIO, CANADA L7N 3W5  
TEL: +1 905 336 2323

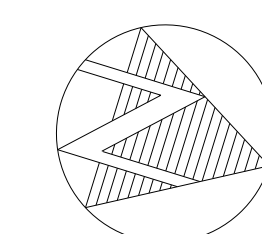
DRAWN ... E.ENGLAND ... YYYY/MM/DD ... 2021/05/05  
CHECKED ... O.PHELAN ...  
CORRECT ... J.STRASMAN ...  
SCALE  
0 2 4 6 8 10 12m

**WATERFRONT EAST LRT  
UNION STATION QUEENS QUAY LINK**

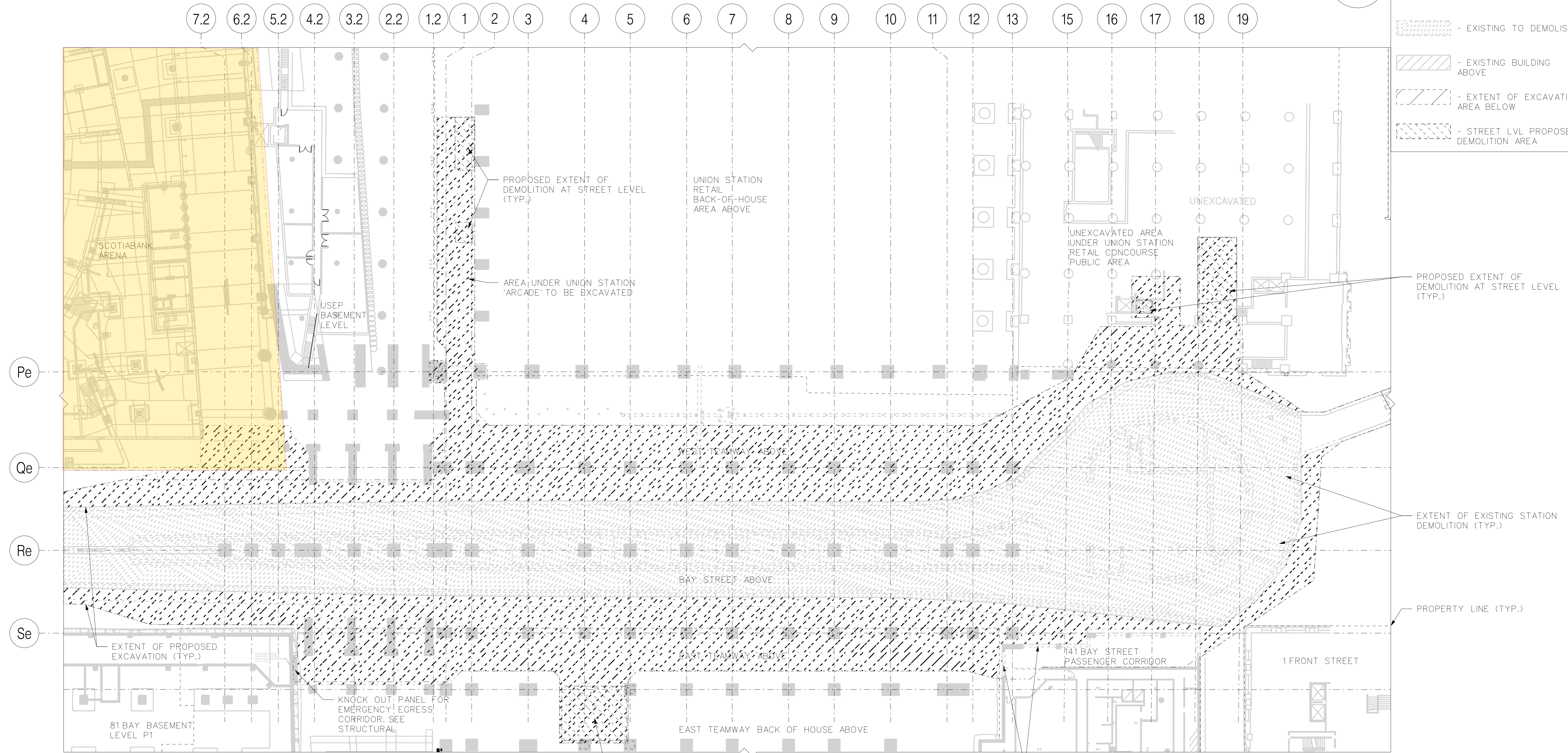
**UNION STATION**  
STREET LEVEL DEMOLITION  
PLAN

Contract: RE35-1  
TORONTO TRANSIT COMMISSION  
ENGINEERING DEPARTMENT  
Dwg. No. **RE35-1-AD002** Rev. No. Sheet No.  
Plot Date: 7-JUL-2021





LEGEND	
	- EXISTING TO REMAIN
	- EXISTING TO DEMOLISH
	- EXISTING BUILDING ABOVE
	- EXTENT OF EXCAVATION AREA BELOW
	- STREET LVL PROPOSED DEMOLITION AREA

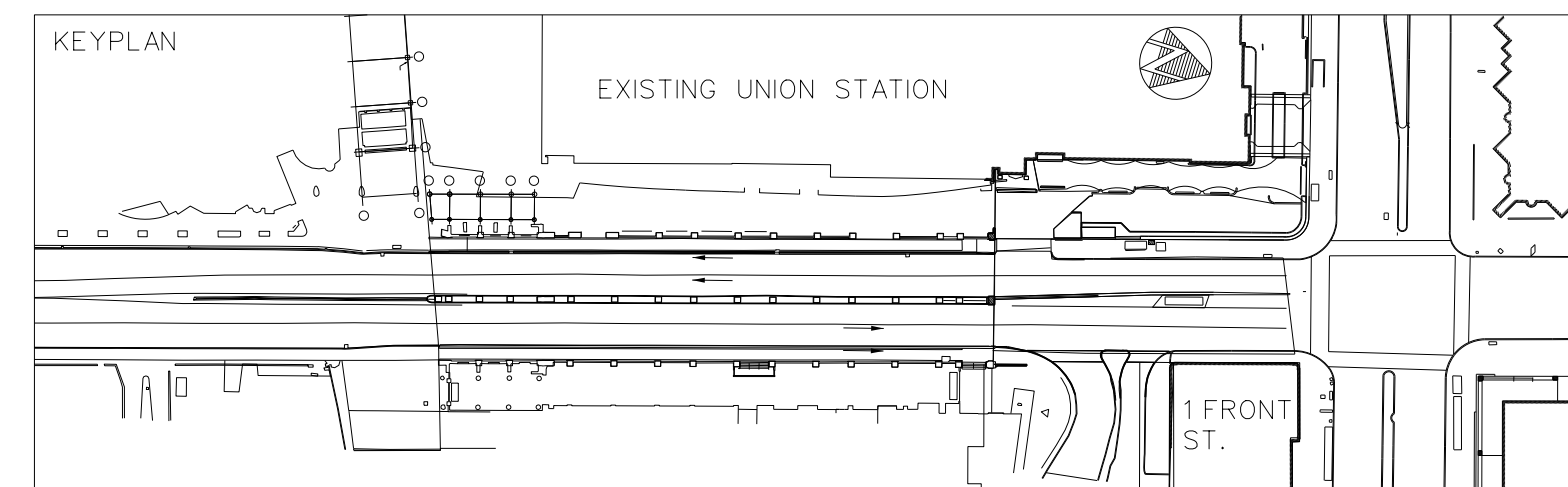


**PLAN**  
UNION STATION STREETCAR LOOP STREET LEVEL  
DEMOLITION REFLECTED CEILING PLAN

1  
AD003

PROPOSED EXTENT OF  
DEMOLITION AT STREET LEVEL  
(TYP.)

KNOCK OUT PANELS TO  
BE DEMOLISHED (TYP.)



REVISIONS	REVISIONS

**30% REVIEW  
DRAWING**  
NOT FOR CONSTRUCTION



WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS  
3450 HARVESTER ROAD, SUITE 100  
BURLINGTON, ONTARIO, CANADA L7N 3W5  
TEL: +1 905 336 2302

DRAWN ... E.ENGLAND ... YYYY/MM/DD  
2021/05/05  
CHECKED ... O.PHELAN  
CORRECT ... J.STRASMAN  
SCALE  
0 2 4 6 8 10 12m

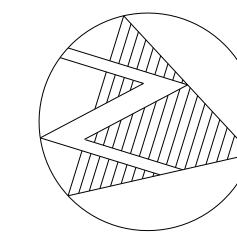
**WATERFRONT EAST LRT  
UNION STATION QUEENS QUAY LINK**

**UNION STATION**  
STREETCAR PLATFORM LEVEL  
DEMOLITION REFLECTED CEILING PLAN

Contract: RE35-1  
TORONTO TRANSIT COMMISSION  
ENGINEERING DEPARTMENT  
Dwg. No. RE35-1-AD003  
Rev. No. 03  
Sheet No.  

Plot Date: 7-JUL-2021

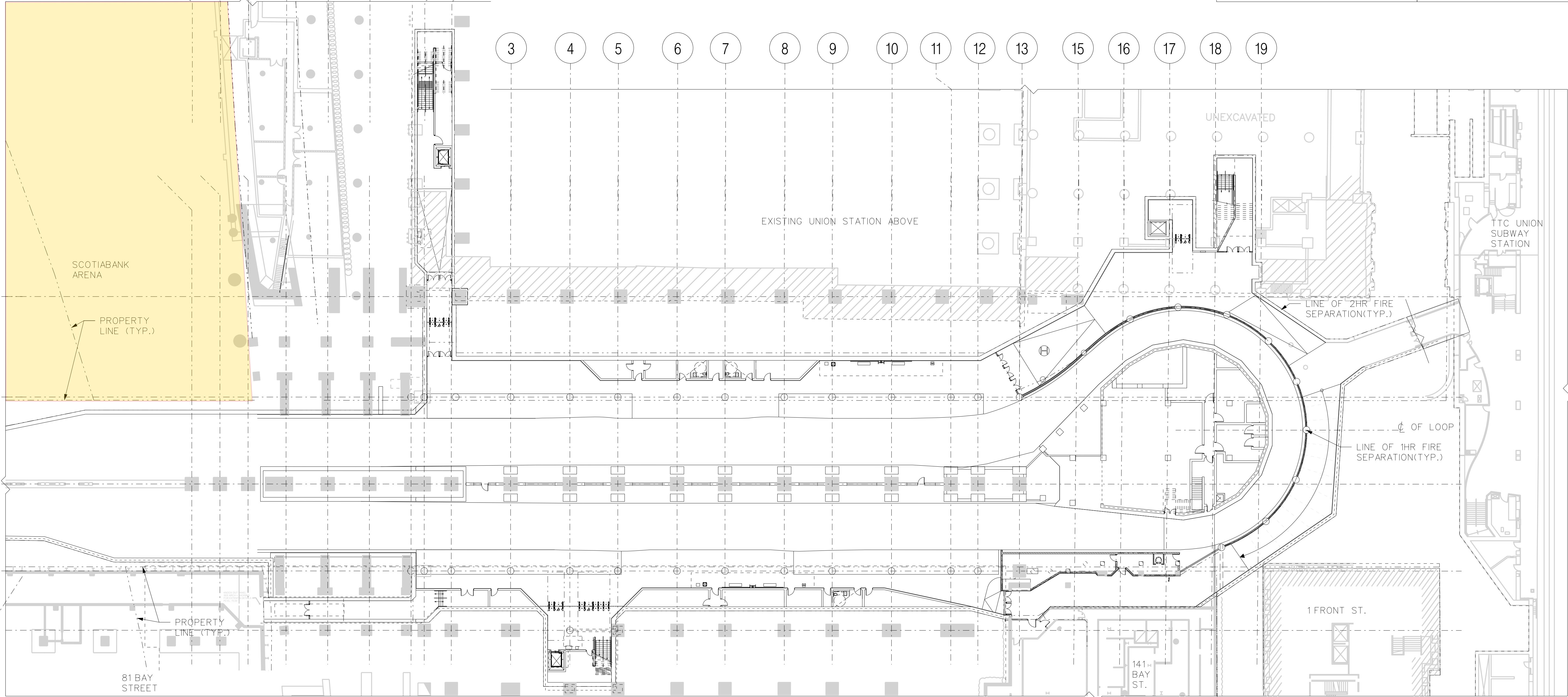




GENERAL NOTES

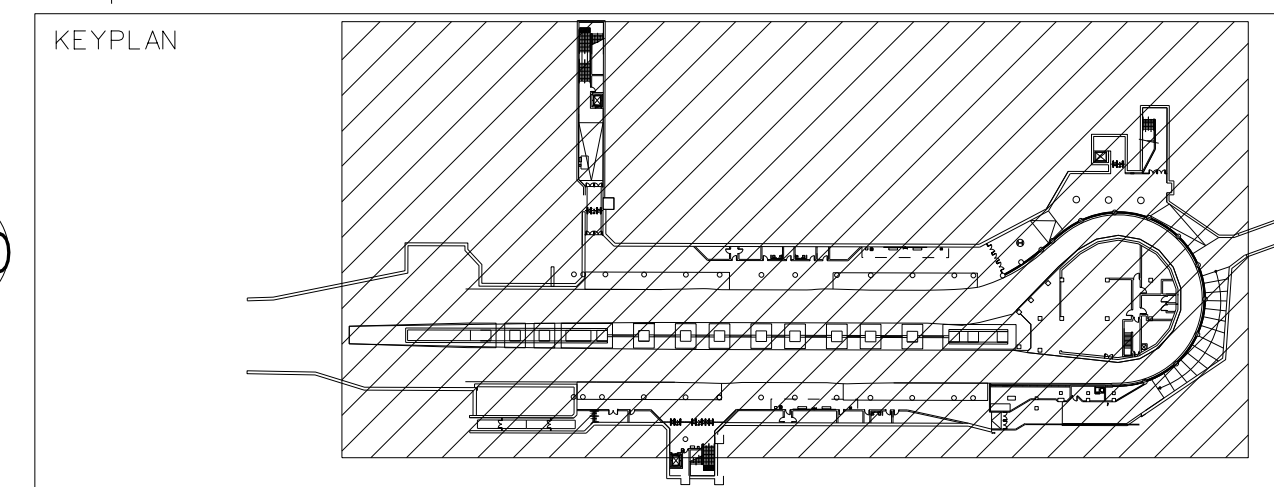
7.2 6.2 5.2 4.2 3.2 2.2 1.2 1 2

3 4 5 6 7 8 9 10 11 12 13 15 16 17 18 19



PLAN  
UNION STATION STREETCAR LOOP - PLATFORM STAGING  
AS001

NP-A NP-B NP-C NP-D



BLDG. REF. No.

FILENAME:

SHEET No.

DRAWING No.

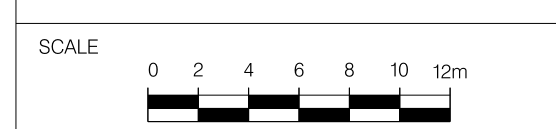
REVISIONS	REVISIONS
△	△
△	△
△	△

30% REVIEW  
DRAWING  
NOT FOR CONSTRUCTION



WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS  
3450 HARVESTER ROAD, SUITE 100  
BURLINGTON, ONTARIO, CANADA L7N 3W5  
TEL: +1 905 336 2363

DRAWN: E. ENGLAND  
CHECKED: O. PHELAN  
CORRECT: J. STRASMAN



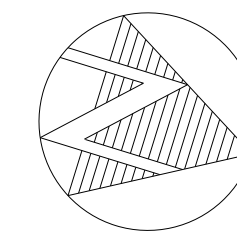
WATERFRONT EAST LRT  
UNION STATION QUEENS QUAY LINK  
  
UNION STATION  
PLATFORM LEVEL  
STAGING PLAN 1 OF 2

Contract: RE35-1  
TORONTO TRANSIT COMMISSION  
ENGINEERING DEPARTMENT  
Dwg. No. RE35-1-AS001  
Rev. No. 1  
Sheet No. 1  
Plot Date: 7-JUL-2021

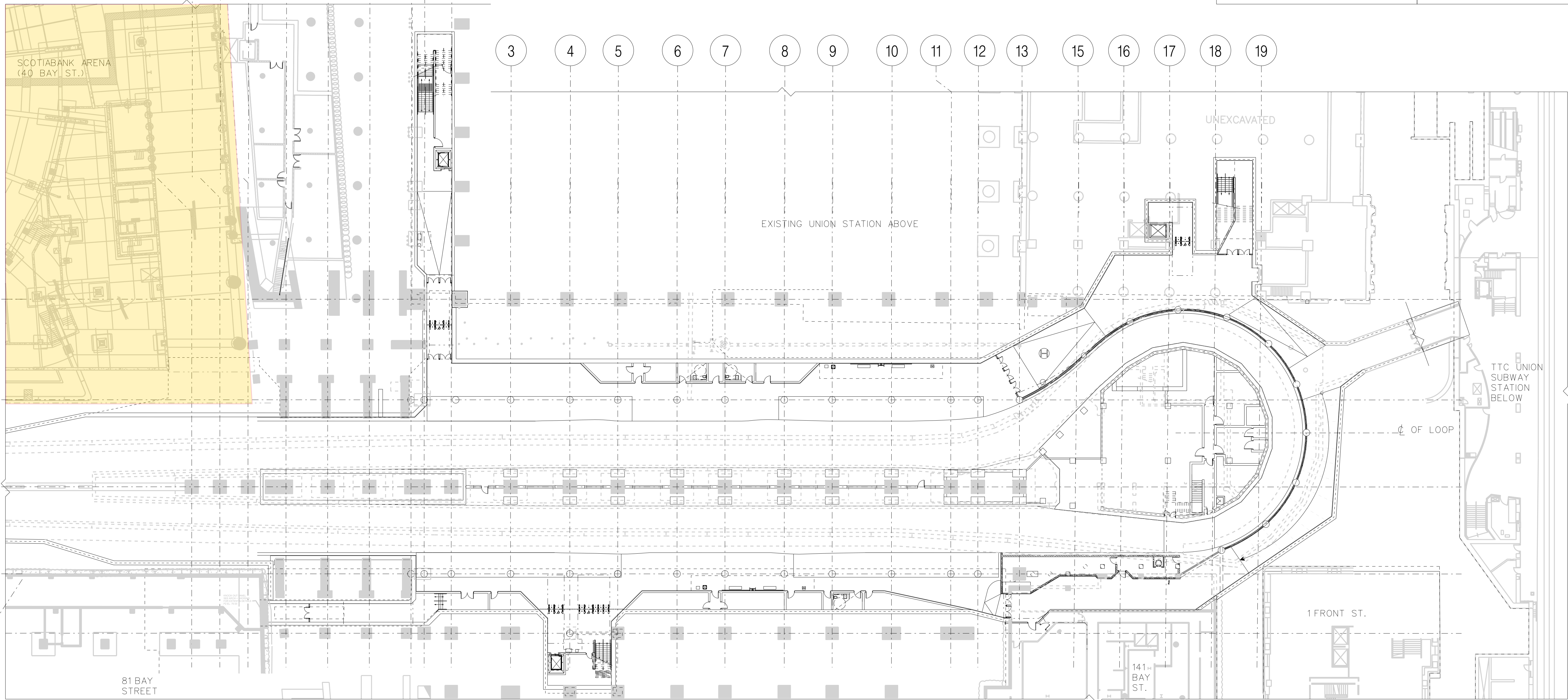


7.2 6.2 5.2 4.2 3.2 2.2 1.2 1 2

3 4 5 6 7 8 9 10 11 12 13 15 16 17 18 19



GENERAL NOTES



PLAN  
UNION STATION STREETCAR LOOP - STREET STAGING PLAN 1 AS002

NP-A NP-B NP-C NP-D

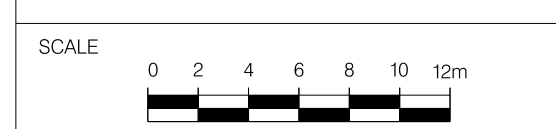
REVISIONS	REVISIONS
△	△
△	△
△	△

30% REVIEW DRAWING  
NOT FOR CONSTRUCTION



WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS  
3450 HARVESTER ROAD, SUITE 100  
BURLINGTON, ONTARIO, CANADA L7N 3W5  
TEL: +1 905 335 2352

DRAWN E.ENGLAND  
CHECKED O.PHELAN  
CORRECT J.STRASMAN



WATERFRONT EAST LRT  
UNION STATION QUEENS QUAY LINK

UNION STATION  
PLATFORM LEVEL  
STAGING PLAN 2 OF 2

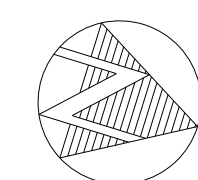
Contract: RE35-1



TORONTO TRANSIT COMMISSION  
ENGINEERING DEPARTMENT  
Dwg. No. RE35-1-AS002  
Rev. No. Sheet No.

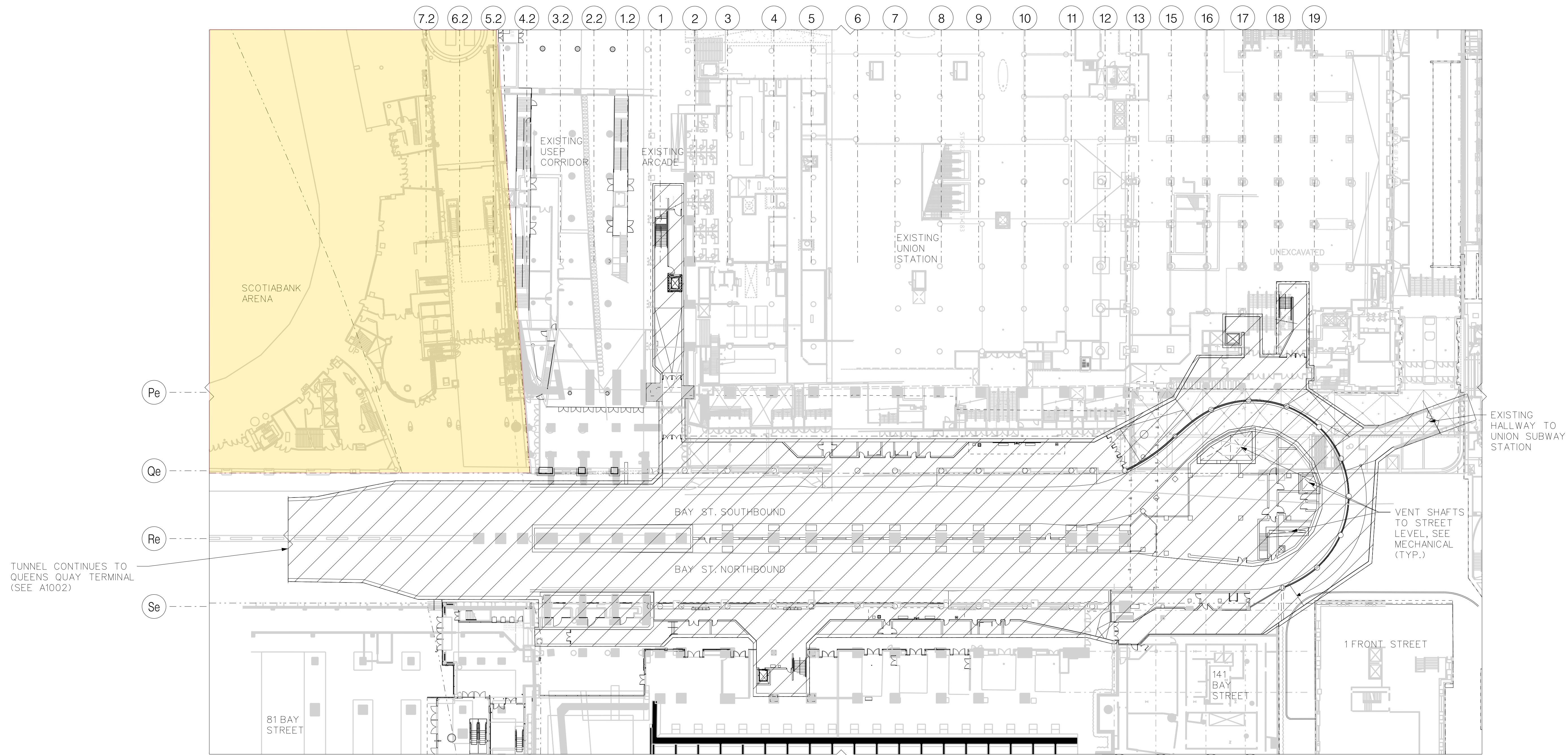
Plot Date: 7-JUL-2021





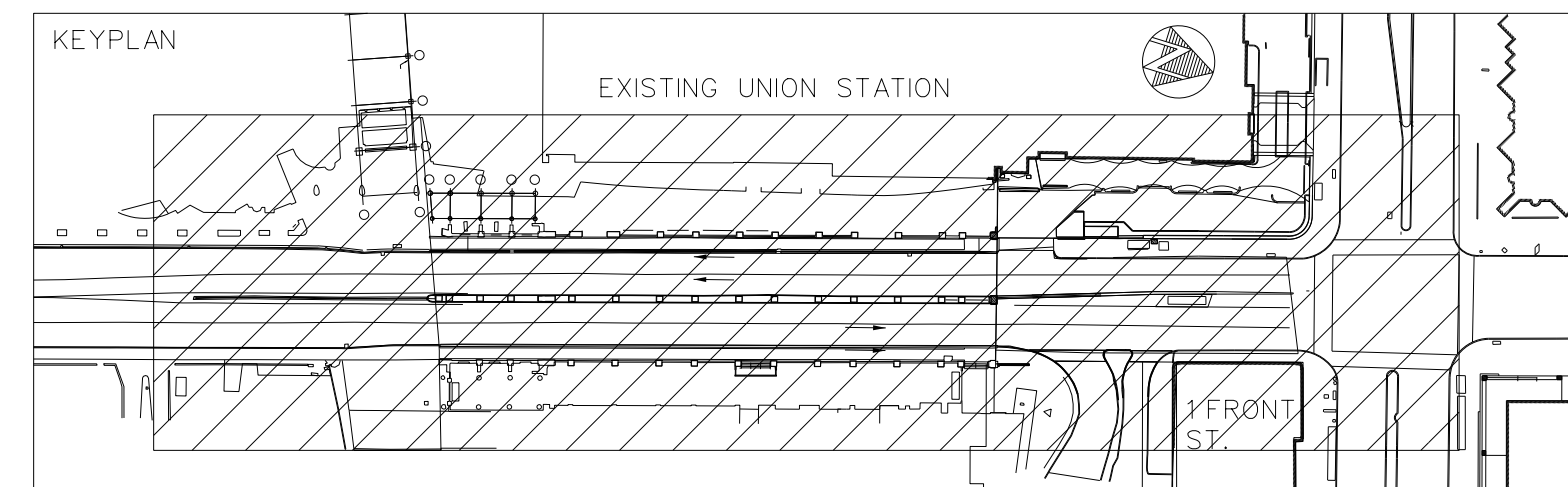
LEGEND

	- EXISTING TO REMAIN
	- NEW CONSTRUCTION



TUNNEL CONTINUES TO QUEENS QUAY TERMINAL (SEE A1002)

**PLAN**  
UNION STATION STREETCAR LOOP - STREET LEVEL



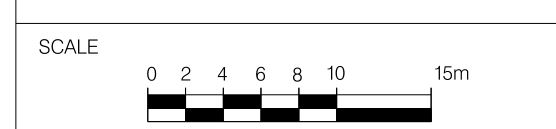
REVISIONS	REVISIONS

**30% REVIEW DRAWING**  
 NOT FOR CONSTRUCTION



WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS  
3450 HARVESTER ROAD, SUITE 100  
BURLINGTON, ONTARIO, CANADA L7N 3W5  
TEL: +1 905 336 2362

DRAWN ... E. ENGLAND ...  
CHECKED ... O. PHELAN ...  
CORRECT ... J. STRASMAN ...



**WATERFRONT EAST LRT  
UNION STATION QUEENS QUAY LINK**

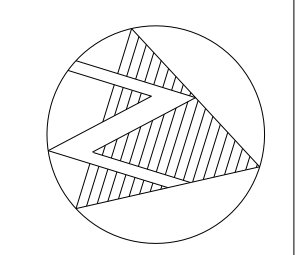
**UNION STATION**

**SITE PLAN**

Contract: RE35-1  
  
 TORONTO TRANSIT COMMISSION  
 ENGINEERING DEPARTMENT  
 Dwg. No. RE35-1-A1001  
 Rev. No. 1  
 Sheet No. 1

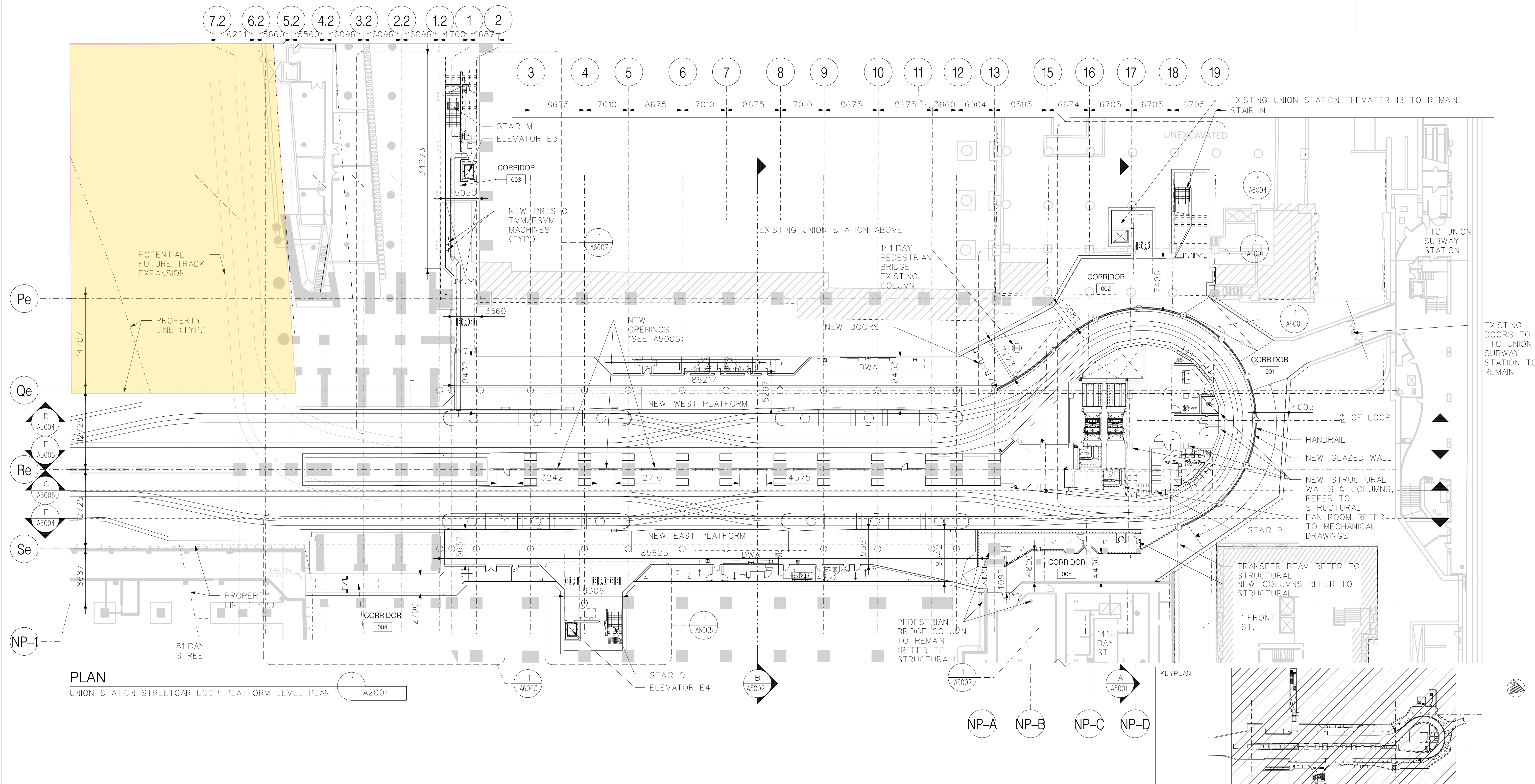
Plot Date: 7-JUL-2021





LEGEND

- EXISTING TO REMAIN
- NEW CONCRETE
- PROPERTY LINE



PLAN  
UNION STATION STREETCAR LOOP PLATFORM LEVEL PLAN A2001

REVISIONS	REVISIONS

30% REVIEW  
DRAWING  
NOT FOR CONSTRUCTION



WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS  
3450 HARVESTER ROAD, SUITE 100  
BURLINGTON, ONTARIO, CANADA L7N 3W5  
TEL: +1 905 336 2362

DRAWN: N.MANABAT  
CHECKED: O.PHELAN  
CORRECT: J.STRASMAN  
SCALE: 0 2 4 6 8 10 12m

WATERFRONT EAST LRT  
UNION STATION QUEENS QUAY LINK

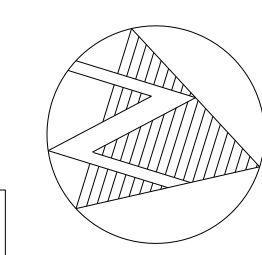
UNION STATION  
PLATFORM LEVEL  
PLAN

Contract: RE35-1  
TORONTO TRANSIT COMMISSION  
ENGINEERING DEPARTMENT  
Dwg. No. RE35-1-A2001  
Rev. No. 1  
Sheet No. 1  
Plot Date: 7-JUL-2021



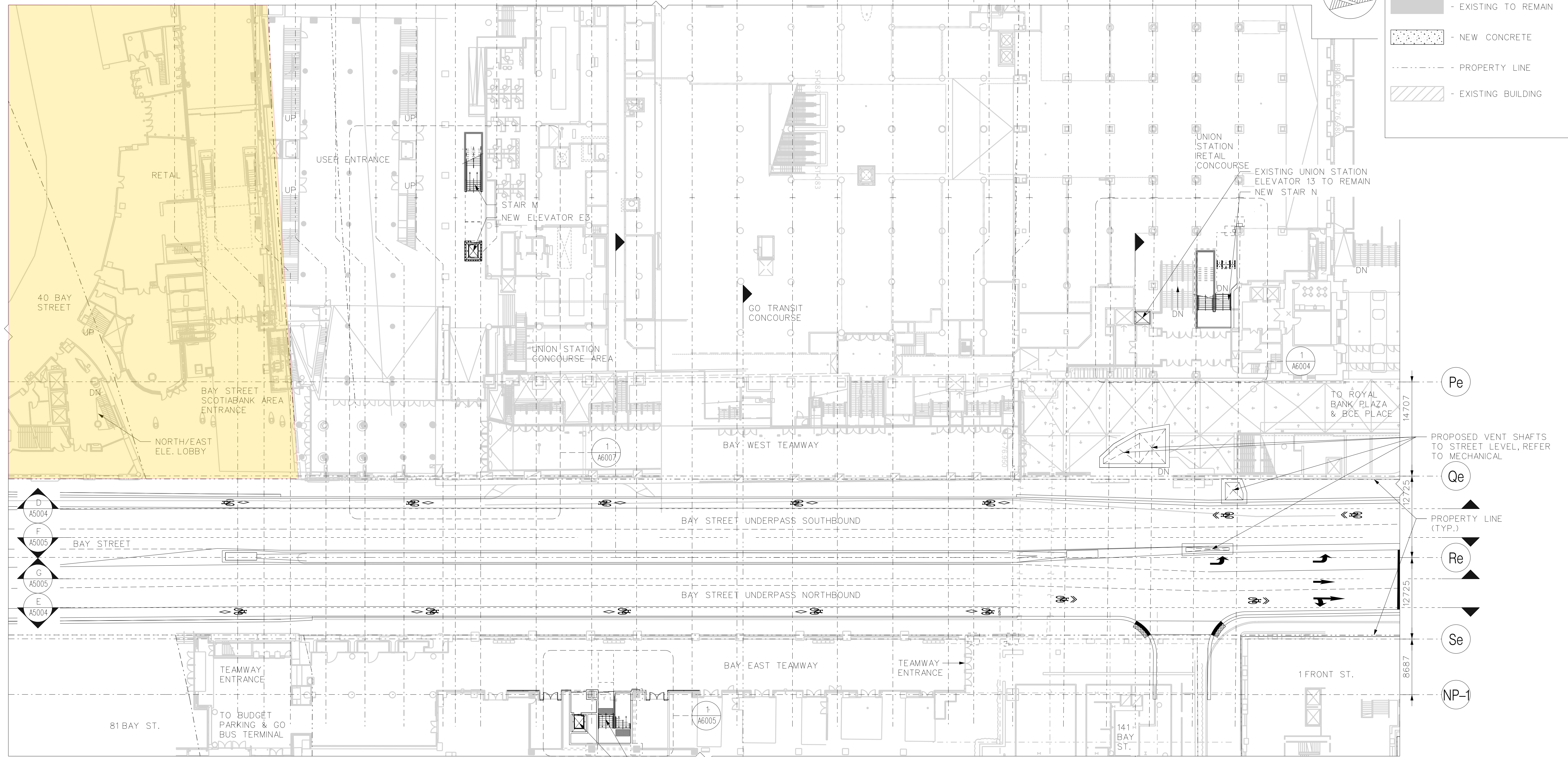
7.2 6.2 5.2 4.2 3.2 2.2 1.2 1 2 3 4 5 6 7 8 9 10 11 12 13 15 16 17 18 19

6221 6293 6292 6293 6293 6292 6120 6469 6166 8675 7010 8675 7010 8675 7010 8675 3960 6577 18221 6674 6705 6705 6705

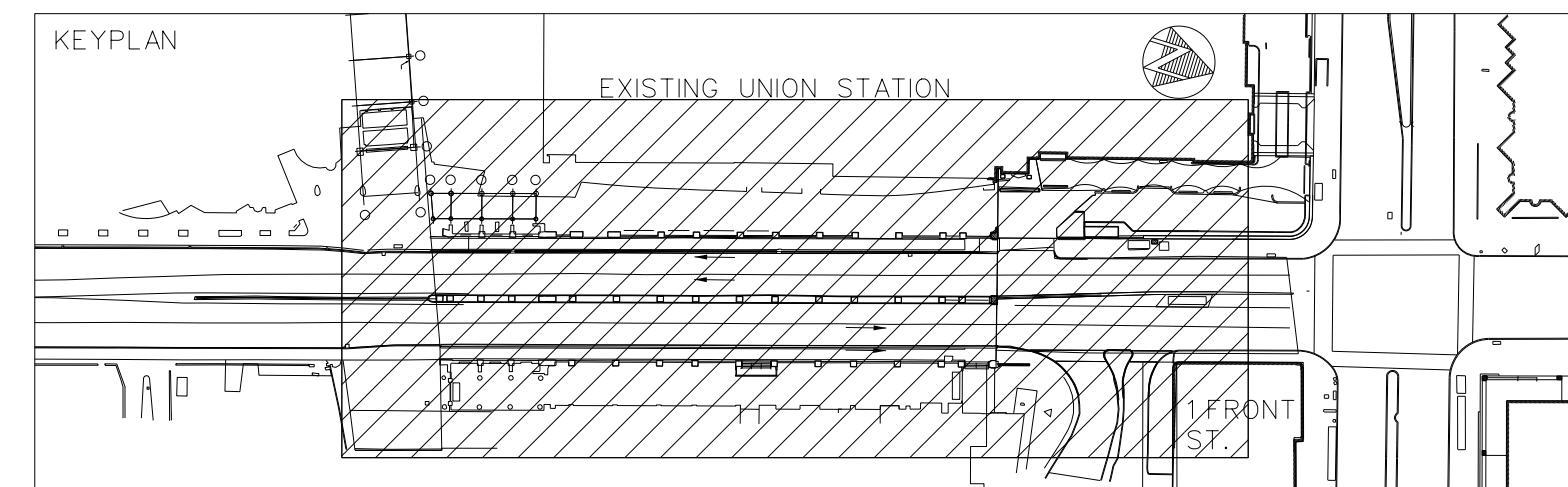


**LEGEND**

- EXISTING TO REMAIN
- NEW CONCRETE
- PROPERTY LINE
- EXISTING BUILDING



**PLAN**  
UNION STATION STREETCAR LOOP - STREET LEVEL



REVISIONS	REVISIONS

**30% REVIEW  
DRAWING**  
NOT FOR CONSTRUCTION



WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS  
3450 HARVESTER ROAD, SUITE 100  
BURLINGTON, ONTARIO, CANADA L7N 3W5  
TEL: +1 905 335 2323

DRAWN: E.ENGLAND  
CHECKED: O.PHELAN  
CORRECT: J.STRASMAN

SCALE: 0 2 4 6 8 10 12m

**WATERFRONT EAST LRT  
UNION STATION QUEENS QUAY LINK**

**UNION STATION**

STREET LEVEL PLAN

Contract: RE35-1

TORONTO TRANSIT COMMISSION  
ENGINEERING DEPARTMENT

Dwg. No. **RE35-1-A2002** Rev. No. **02** Sheet No. **02**

Plot Date: 7-JUL-2021



## **Appendix C: Conservation Guidelines**

# THE STANDARDS

The Standards are not presented in a hierarchical order. All standards for any given type of treatment must be considered, and applied where appropriate, to any conservation project.

## General Standards for Preservation, Rehabilitation and Restoration

1. Conserve the *heritage value* of an *historic place*. Do not remove, replace or substantially alter its intact or repairable *character-defining elements*. Do not move a part of an historic place if its current location is a character-defining element.
2. Conserve changes to an *historic place* that, over time, have become *character-defining elements* in their own right.
3. Conserve *heritage value* by adopting an approach calling for *minimal intervention*.
4. Recognize each *historic place* as a physical record of its time, place and use. Do not create a false sense of historical development by adding elements from other historic places or other properties, or by combining features of the same property that never coexisted.
5. Find a use for an *historic place* that requires minimal or no change to its *character-defining elements*.
6. Protect and, if necessary, stabilize an *historic place* until any subsequent *intervention* is undertaken. Protect and preserve archaeological resources in place. Where there is potential for disturbing archaeological resources, take mitigation measures to limit damage and loss of information.
7. Evaluate the existing condition of *character-defining elements* to determine the appropriate *intervention* needed. Use the gentlest means possible for any intervention. Respect *heritage value* when undertaking an intervention.
8. Maintain *character-defining elements* on an ongoing basis. Repair character-defining elements by reinforcing their materials using recognized conservation methods. Replace in kind any extensively deteriorated or missing parts of character-defining elements, where there are surviving *prototypes*.
9. Make any *intervention* needed to preserve *character-defining elements* physically and visually compatible with the *historic place* and identifiable on close inspection. Document any intervention for future reference.

## Additional Standards Relating to Rehabilitation

- 10.** Repair rather than replace *character-defining elements*. Where character-defining elements are too severely deteriorated to repair, and where sufficient physical evidence exists, replace them with new elements that match the forms, materials and detailing of sound versions of the same elements. Where there is insufficient physical evidence, make the form, material and detailing of the new elements compatible with the character of the *historic place*.
- 11.** Conserve the *heritage value* and *character-defining elements* when creating any new additions to an *historic place* or any related new construction. Make the new work physically and visually compatible with, subordinate to and distinguishable from the historic place.
- 12.** Create any new additions or related new construction so that the essential form and integrity of an *historic place* will not be impaired if the new work is removed in the future.

## Additional Standards Relating to Restoration

- 13.** Repair rather than replace *character-defining elements* from the *restoration* period. Where character-defining elements are too severely deteriorated to repair and where sufficient physical evidence exists, replace them with new elements that match the forms, materials and detailing of sound versions of the same elements.
- 14.** Replace missing features from the *restoration* period with new features whose forms, materials and detailing are based on sufficient physical, documentary and/or oral evidence.



## 4.3.4 EXTERIOR WALLS

These guidelines provide direction when exterior walls and their elements are identified as character-defining elements of an historic place. Exterior walls include foundation walls, structural masonry or log walls, and wood, concrete or steel framing with an exterior cladding, such as *curtain-wall systems*. The guidelines also provide direction on how to minimize the negative impact of additions or alterations on exterior walls.



Because they are largely below the surface, foundations are often dismissed as not contributing to a building's heritage character. However, it is important to remember that heritage value is not only found in what can be seen. Construction methods, materials and techniques can all be character defining, as is the case for the Grange Alexander-Salomon-Wallbridge in Quebec.

Log, stone and concrete foundations are visually and functionally very different. The decision to repair, replace in kind or rehabilitate a foundation should be determined by both the condition of the foundation and its compatibility with the heritage values of the place.

Exterior walls perform many functions, including those of structure, weatherproofing, thermal protection, daylight control and ventilation. Traditional load-bearing walls, such as log or masonry walls, perform all of these functions in a single composition. Later, the development of frame-based structural systems led to the separation of these functions. In modern buildings, components, such as cladding, air barriers and insulation, are combined to create a complex exterior wall assembly. These components act both independently and as a whole; consequently, there may be specific conservation issues associated with the materials concealed in the core or cavity of the assembly.



A modern building envelope includes multiple materials, such as a concrete structure, metal curtain wall mullions and glazed panels. Each of these materials ages differently and reacts in its own way to stresses, heat and cold. For a face-sealed curtain wall like this one, it is important to select sealants that make the assembly weathertight and that are compatible with all the materials with which they come into contact. Sealant materials generally have a shorter service life than adjacent materials and will eventually fail due to exposure to weather, stress and age. Regularly replacing appropriate sealants is an important part of building maintenance.

When frame-based cladding and curtain walls were developed, air and vapour barriers were introduced, as well as mechanical heating, ventilating and air conditioning (HVAC) systems that changed the thermal and moisture conditions in the walls. Changes to existing HVAC systems, or introducing mechanical systems in buildings where the walls were designed without insulation or air barriers, often led to the deterioration of exterior wall assemblies. It is, therefore, important to understand how the exterior wall relates to the building systems.

*Curtain wall systems* present a range of new conservation challenges, because they were the result of an era of experimentation in structures and materials, and predate higher standards for energy efficiency. Their conservation should be examined on a case-by-case basis, taking into account the heritage value of the design and the actual conditions and causes of deterioration, while planning for extended or improved performance.

These guidelines provide general recommendations appropriate to all types of exterior walls. For recommendations on associated issues related to walls, refer to the individual guidelines for Exterior Form, Windows, Doors and Storefronts; and Structural Systems. For specific materials that make up exterior walls, refer to the Guidelines for Materials.



New signs (top right) should be compatible with the building in terms of size, scale, material, style and colour. They should also not obscure, damage or destroy character-defining elements. In some cases, as Mel's Tea Room in Sackville, NB (left), signs added later become character-defining elements in their own right. Character defining signage should also be maintained despite a change in use, such as this painted sign for the Hartt Boot and She Factory in Fredericton (bottom right), which has been converted into apartments.



Responding to the dangers of arctic exploration and the loss of the Franklin Expedition, Kellett's Storehouse in Nunuvut was built in 1853 for sailors in distress. Constructed from local stone, it was filled with enough provisions to maintain a large group for several months. Conserving building remains or above-ground archaeological resources pose unique challenges. In this case, the walls and foundations were stabilized and an insulated floor was installed allowing remaining soil layers and associated artifacts to be left in place while being protected from theft and further exposure to the elements.



The impact of adding insulation to an exterior wall assembly that was not historically insulated should be carefully considered, including by measurement of the current performance of the wall and energy modelling the performance of different approaches. Adding insulation on the exterior or interior of solid masonry or log walls may impact on heritage character and lead to deterioration of the wall, if not based on a thorough understanding of the wall's physical characteristics and context, including its exposure to air, water and vapour pressures. Any changes to an exterior wall should be based on the building envelope science for the type of historic assembly.

## GENERAL GUIDELINES FOR PRESERVATION, REHABILITATION AND RESTORATION

	Recommended	Not Recommended
1	<b>Understanding</b> the exterior walls and how they contribute to the heritage value of the historic building.	
2	<b>Understanding</b> the properties and characteristics of the exterior walls as well as changes and previous maintenance practices.	Failing to consider the impact of previous changes to the exterior wall assembly, such as the addition of insulation and vapour barriers, or new heating or cooling systems.
3	<b>Documenting</b> the composition, form, materials, details, dimensions and condition of exterior wall assemblies before undertaking an intervention. This includes geometry, scale, proportions, openings, form and supporting frames or structures.	Undertaking an intervention that affects exterior wall assemblies without first documenting their existing character and condition.
4	<b>Assessing</b> the condition of wall assemblies and their materials early in the planning process so that the scope of work is based on current conditions.	
5	<b>Determining</b> the cause of distress, damage or deterioration of exterior walls through investigation, monitoring and minimally invasive or non-destructive testing techniques.	
6	<b>Protecting</b> and maintaining exterior walls by cleaning and repairing damaged materials, and checking exterior wall assemblies for moisture penetration and insect infestation, taking corrective action, as necessary and as soon as possible.	Failing to maintain exterior walls on a cyclical basis. Failing to correct causes of deterioration of the exterior wall assembly, such as failed sealants.
7	<b>Retaining</b> sound or deteriorated exterior wall assemblies that can be repaired.	
8	<b>Stabilizing</b> deteriorated exterior walls by using structural reinforcement, weather protection, or correcting unsafe conditions, as required, until repair work is undertaken.	Removing deteriorated exterior wall elements that could be stabilized or repaired.
9	<b>Repairing</b> parts of exterior walls by patching, piecing-in, consolidating, or otherwise reinforcing, using recognized conservation methods. Repair may also include the limited replacement in kind, or with a compatible substitute material, of extensively deteriorated or missing parts of the exterior wall assembly. Repairs should match the existing work as closely as possible, both physically and visually.	
10	<b>Protecting</b> adjacent character-defining elements from accidental damage or exposure to damaging materials during maintenance or repair work.	



## GENERAL GUIDELINES FOR PRESERVATION, REHABILITATION AND RESTORATION

	Recommended	Not Recommended
11	<b>Replacing</b> in kind extensively deteriorated or missing parts of exterior wall assemblies where there are surviving prototypes.	Replacing an entire exterior wall assembly when only limited replacement of deteriorated and missing parts is possible.  Using a substitute material for the replacement part that neither conveys the same appearance as the surviving parts of the element, nor is physically or visually compatible.
12	<b>Testing</b> proposed interventions to establish appropriate replacement materials, quality of workmanship and methodology. This can include reviewing samples, testing products, methods or assemblies, or creating a mock-up. Testing should be carried out under the same conditions as the proposed intervention.	
13	<b>Documenting</b> all interventions that affect the exterior walls, and ensuring that the documentation is available to those responsible for future interventions.	

## ADDITIONAL GUIDELINES FOR REHABILITATION PROJECTS

	Recommended	Not Recommended
14	<b>Repairing</b> an exterior wall assembly, including its functional and decorative elements, by using a minimal intervention approach. Such repairs might include the limited replacement in kind, or replacement using an appropriate substitute material of irreparable or missing elements, based on documentary or physical evidence. Repairs might also include dismantling and rebuilding a masonry or wood wall, if an evaluation of its overall condition determines that more than limited repair or replacement in kind is required.	Over-cladding a deteriorated or poorly insulated exterior wall with a new material or assembly, without considering the impact on heritage value or the condition of underlying materials.  Replacing an entire exterior wall assembly when the repair and limited replacement of deteriorated or missing elements is feasible.  Failing to reuse intact cladding when only the internal parts of the wall assembly need replacement.
15	<b>Improving</b> the drying ability of exterior wall assemblies through suitable heating and/or ventilation measures.	Damaging the masonry of an exterior wall by drilling drainage holes into the masonry units or into the joints, with a drill bit wider than the mortar joints.  Introducing a vapour barrier in an exterior wall that was constructed to be permeable or breathable.

## ADDITIONAL GUIDELINES FOR REHABILITATION PROJECTS

	Recommended	Not Recommended
16	<b>Accommodating</b> the thermal expansion and contraction of masonry, concrete and curtain wall assemblies, by introducing expansion or control joints, and incorporating those joints into existing crack patterns, where feasible, to minimize impact on character-defining elements.	Filling moving cracks or expansion joints in exterior wall assemblies with materials that inhibit or prevent thermal expansion and contraction.
17	<b>Replacing</b> in kind an irreparable exterior wall assembly, based on documentary and physical evidence. If using the same kind of material is not environmentally sound, or technically or economically feasible, then a compatible substitute material may be considered.	Removing an irreparable exterior wall assembly, such as a cornice or <i>brise-soleil</i> , and not replacing it, or replacing it with a new element that does not convey the same appearance or serve the same function.  Replacing deteriorated elements and materials in curtain wall assemblies that are no longer available, with physically and visually incompatible substitutes.
18	<b>Replacing</b> missing historic features by designing and constructing a new portion of the exterior wall assembly, based on physical and documentary evidence, or one that is compatible in size, scale, material, style and colour.	Creating a false historical appearance, because the replicated feature is incompatible or based on insufficient physical and documentary evidence.

### ADDITIONS OR ALTERATIONS TO EXTERIOR WALLS

19	<b>Modifying</b> exterior walls to accommodate an expanded program, a new use, or applicable codes and regulations, in a manner that respects the building's heritage value.	
20	<b>Designing</b> a new addition in a manner that preserves the character-defining exterior walls of the historic building.	Constructing an addition that requires the removal of character-defining exterior walls.

### HEALTH, SAFETY AND SECURITY CONSIDERATIONS

21	<b>Complying</b> with health, safety and security requirements in a manner that conserves the heritage value of the exterior wall assembly and minimizes impact on its character-defining elements.	Damaging or destroying elements while making modifications to comply with health, safety or security requirements.
22	<b>Working</b> with code specialists to determine the most appropriate solution to health, safety and security requirements with the least impact on the character-defining elements and overall heritage value of the historic building.	Making changes to exterior walls, without first exploring equivalent systems, methods or devices that may be less damaging to character-defining elements and the heritage value of the historic building.
23	<b>Removing</b> or encapsulating toxic materials, using the least-invasive abatement methods possible, and only after thorough testing has been conducted.	
24	<b>Protecting</b> exterior walls against loss or damage by identifying and assessing specific risks, and by implementing an appropriate fire-protection and blast protection strategy that addresses those risks.	Covering flammable character-defining walls with fire-resistant sheathing or coatings that alter their appearance.

## ADDITIONAL GUIDELINES FOR REHABILITATION PROJECTS

	Recommended	Not Recommended
<b>SUSTAINABILITY CONSIDERATIONS</b>		
<b>25</b>	<b>Complying</b> with energy efficiency objectives in upgrades to exterior wall assemblies in a manner that respects the building's character-defining elements, and considers the energy efficiency of the building envelope and systems as a whole.	<p>Changing the composition or materials of the exterior wall assembly in a manner that compromises the building's character-defining elements and the durability of its materials.</p> <p>Replacing single pane glazing with sealed thermal units, without considering the impact on interrelated elements, such as curtain wall connections.</p>
<b>26</b>	<b>Assessing</b> the potential impacts of adding insulation to the building envelope, such as displacing the <i>dew point</i> and creating <i>thermal bridges</i> .	<p>Inserting thermal insulation in exterior wall cavities, in attics, and in unheated cellars and crawl spaces, that might adversely affect the building's envelope and character-defining elements.</p> <p>Installing insulation on the inside of exterior walls without considering the effect on character-defining interior mouldings or detailing.</p>
<b>27</b>	<b>Working</b> with energy efficiency specialists to determine the most appropriate solution to energy efficiency requirements with the least impact on the character-defining elements and overall heritage value of the historic building.	Making changes to the exterior walls, without first exploring alternative energy efficiency solutions that may be less damaging to the character-defining elements and overall heritage value of the historic building.



## ADDITIONAL GUIDELINES FOR RESTORATION PROJECTS

	Recommended	Not Recommended
28	<b>Repairing</b> an exterior wall assembly from the restoration period by reinforcing its materials; for example, using heavier gauge metal to reinforce a mullion in a curtain wall.	Replacing an entire exterior wall assembly from the restoration period when the repair of materials and limited replacement of deteriorated or missing parts is possible.
29	<b>Replacing</b> in kind an entire exterior wall assembly from the restoration period that is too deteriorated to repair, using the physical evidence as a model to reproduce the assembly. The new work should be well documented and unobtrusively dated to guide future research and treatment.	Removing an irreparable exterior wall assembly from the restoration period and not replacing it, or replacing it with an inappropriate exterior wall assembly.  Reinstating an exterior wall detail that is damaging to adjacent character-defining elements.

### REMOVING EXISTING FEATURES FROM OTHER PERIODS

30	<b>Removing</b> or altering a non character-defining exterior wall assembly or element from a period other than the restoration period.	Failing to remove a non character-defining exterior wall assembly or element from another period that confuses the depiction of the building's chosen restoration period.
31	<b>Retaining</b> alterations to exterior wall assemblies that address problems with the original design, if those alterations do not have a negative impact on the building's heritage value.	Removing an exterior wall assembly or element from a later period that serves an important function in the building's ongoing use.

### RECREATING MISSING FEATURES FROM THE RESTORATION PERIOD

32	<b>Recreating</b> a missing exterior wall assembly from the restoration period, based on physical or documentary evidence.	Constructing an exterior wall assembly that was part of the building's original design, but was never actually built, or constructing a feature thought to have existed during the restoration period, but for which there is insufficient documentation.
----	--	---

## **Appendix D: Record of Community Engagement**

**From:** [Yasmina Shamji](#)  
**To:** [Schopf, Heidi](#)  
**Subject:** FW: Information Gathering Request  
**Date:** Thursday, March 25, 2021 11:37:01 AM  
**Attachments:** [image001.png](#)  
[image006.png](#)  
[image007.png](#)  
[OSIO52004 TTC Waterfront East LRT Information Gathering Request 20201216....pdf](#)

---

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy

Sorry, I didn't realize I hadn't copied you on the email. Here is my response to Luke.

Thank you

Yasmina Shamji  
Urban Design | Heritage Planning  
Toronto City Hall | 100 Queen Street West, 17<sup>th</sup> Floor East Tower  
Toronto, ON M5H 2N2  
416-392-1975  
[City of Toronto](#)



---

**From:** Yasmina Shamji  
**Sent:** January 7, 2021 4:27 PM  
**To:** 'Fischer, Luke' <luke.fischer@woodplc.com>  
**Cc:** Guy Zimmerman <Guy.Zimmerman@toronto.ca>; Joe Muller <Joe.Muller@toronto.ca>  
**Subject:** RE: Information Gathering Request

Luke

Happy New Year!

I have reviewed your report and compiled the information as requested. I don't have information for the properties noted in red below – it's possible details for these properties doesn't exist. In that case, I am deferring your request to Guy Zimmerman (also copied on this email), who is the Heritage Planner for the Union Station Heritage Conservation District.

According to our [Heritage Register Map](#), the following properties fall within the area study areas indicated in your report:

- Bay Street from Front Street West to Queens Quay East
- Queens Quay East from York Street to Cooper Street



(I am attaching the link to the property details from our online register as well as the by-laws wherever applicable)

40 Bay St: Part IV Designation (Part IV By-law 360-90) as well as Union Station HCD (By-law 634-2006)  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433226&propertyRsn=206956>  
<https://www.toronto.ca/legdocs/pre1998bylaws/toronto%20-%20former%20city%20of/1990-0360.pdf>

71 Front St W (Union Station) aka 140 & 142 Bay St & 55 Front St W: Part IV Designation (Part IV By-law 948-2005) as well as Union Station HCD  
And 61 Front St W - <http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2440090&propertyRsn=206955>  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2435414&propertyRsn=705027>  
<https://www.toronto.ca/legdocs/bylaws/2005/law0948.pdf>

145 Queens Quay W aka 77 Harbour Sq: Part IV Designation (By-law 1249-2007)  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2439065&propertyRsn=206198>  
<https://www.toronto.ca/legdocs/bylaws/2007/law1249.pdf>

2 Cooper St (entrance address for 55 Lake Shore Blvd E) – Under Intention to Designate  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2438226&propertyRsn=719130>  
<http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2018.TE30.17>

Further to the above, you also requested information sheets for the following properties:

**Gardiner Expressway – no information available – deferred to Guy**

1 Front St W  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2435336&propertyRsn=578987>  
<https://www.toronto.ca/legdocs/bylaws/2017/law0423.pdf>

61 Front St W – Listed – no additional information is available  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2440090&propertyRsn=206955>

65 Front St W - refer to details provide above

**18 Yonge St – no property info sheets available – deferred to Guy**  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2437048&propertyRsn=615674>

**33 Bay St - no property info sheets available – deferred to Guy**  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433225&propertyRsn=751123>

40 Bay St – refer to details provide above

**161 Bay St - no property info sheets available – deferred to Guy**  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=3246951&propertyRsn=206967>

**200 Bay St - no property info sheets available – deferred to Guy**

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433240&propertyRsn=206973>

60 Harbour St - no property info sheets available – deferred to Guy

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2434664&propertyRsn=206938>

88-100 Harbour St - no property info sheets available – deferred to Guy

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=3607011&propertyRsn=826634>

15-19 York St - no property info sheets available – deferred to Guy

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2436569&propertyRsn=750799>

Also, please note that most of the information provided above can be found on our website via the links below:

<https://www.toronto.ca/city-government/planning-development/heritage-preservation/heritage-register/> - using both the Heritage Register Map as well as the Heritage Search Tool.

By-law information can be found here: <https://www.toronto.ca/legdocs/bylaws/lawhome.htm>

Thank you

Yasmina Shamji

Urban Design | Heritage Planning

Toronto City Hall | 100 Queen Street West, 17<sup>th</sup> Floor East Tower

Toronto, ON M5H 2N2

[City of Toronto](#)

416-392-1975



---

**From:** Fischer, Luke [<mailto:luke.fischer@woodplc.com>]

**Sent:** December 16, 2020 3:13 PM

**To:** Yasmina Shamji <[Yasmina.Shamji@toronto.ca](mailto:Yasmina.Shamji@toronto.ca)>

**Cc:** Schopf, Heidy <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>

**Subject:** Information Gathering Request

Hello Yasmina,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,

Luke

**Luke Fischer, M.A., CAHP**

Senior Archaeologist, Built Heritage Analyst

Direct: 1 (519) 681-2400

Mobile: 1 (226) 376-2968

[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)

[www.woodplc.com](http://www.woodplc.com)

**wood.**

---

This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: [unsubscribe@woodplc.com](mailto:unsubscribe@woodplc.com) and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.

Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at <https://www.woodplc.com/policies/privacy-notice>

---



**From:** [Guy Zimmerman](#)  
**To:** [Schopf, Heidy](#)  
**Cc:** [Fischer, Luke](#); [Brent Fairbairn](#)  
**Subject:** RE: DRAFT Response from HP with contact names and document references  
**Date:** Wednesday, July 7, 2021 11:55:10 AM  
**Attachments:** [image020.png](#)  
[image021.png](#)  
[image022.png](#)  
[image002.png](#)  
[image005.png](#)  
[image006.png](#)

---

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy,

I was glad to be able to share my experience with you and Brent regarding my stewardship of the Union Station HCD and provide that contact information to source background regarding the Bay Street Bridge (that you apparently may not have been familiar with)

Regarding any additional feedback that you were fishing for... beyond that please let me know if there is missing information that I can source for you as my expectation was that I was providing pretty specific insight into the Gardiner Expressway and Path System (relative to the Union Station HCD).

**Guy Zimmerman** - BEDS, CAHP  
Heritage Planner  
Heritage Planning /Urban Design  
City Planning • City of Toronto  
[guy.zimmerman@toronto.ca](mailto:guy.zimmerman@toronto.ca)  
416.338.1094

---

**From:** Schopf, Heidy [mailto:heidy.schopf@woodplc.com]  
**Sent:** July 6, 2021 3:51 PM  
**To:** Guy Zimmerman <Guy.Zimmerman@toronto.ca>  
**Cc:** Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>  
**Subject:** RE: DRAFT Response from HP with contact names and document references

Thank you both once again for your time today. Your insight on the Gardiner, PATH, and Bay Street bridge is appreciated!

Sincerely,  
-Heidy

---

**From:** Guy Zimmerman <[Guy.Zimmerman@toronto.ca](mailto:Guy.Zimmerman@toronto.ca)>  
**Sent:** Tuesday, July 6, 2021 2:59 PM  
**To:** Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Cc:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>; Brent Fairbairn <[Brent.Fairbairn@toronto.ca](mailto:Brent.Fairbairn@toronto.ca)>  
**Subject:** DRAFT Response from HP with contact names and document references

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy et al,

Please disregard my DRAFT RESPONSE that I had been preparing if you have that random assortment of materials and contacts names. I had included Dima's contact information at the bottom for your reference should there be any confusion with the consultant indicated on the image of the cover of the report that is included as to who would be the first point of contact.

**Guy Zimmerman** - BEDS, CAHP  
Heritage Planner  
Heritage Planning /Urban Design  
City Planning • City of Toronto  
[guy.zimmerman@toronto.ca](mailto:guy.zimmerman@toronto.ca)  
416.338.1094

---

**From:** Guy Zimmerman  
**Sent:** July 6, 2021 7:59 AM  
**To:** 'Schopf, Heidi' <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Cc:** 'Fischer, Luke' <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>; Brent Fairbairn <[Brent.Fairbairn@toronto.ca](mailto:Brent.Fairbairn@toronto.ca)>  
**Subject:** RE: Union Station HCD Background Request Cultural Heritage Report for the Waterfront East LRT - Outstanding question RE:Bridge supporting the Union Station Trainshed Inquiry

Good morning Heidi,

I thought that it was funny when you first asked about this that you wouldn't have access to this material through Metrolinx.

I won't bother completing the material that I was assembling below as we can discuss it during our 3:00 Union Station/TTC WELRT Discussion (catchy name!)

**Guy Zimmerman** - BEDS, CAHP  
Heritage Planner  
Heritage Planning /Urban Design  
City Planning • City of Toronto  
[guy.zimmerman@toronto.ca](mailto:guy.zimmerman@toronto.ca)  
416.338.1094

DRAFT RESPONSE 7/6/2021

I dredged up this report from the work associated with the Bay Street Railway Bridge.

I don't know if I am at liberty to share this material but I think you would do better to deal directly with the consultants as they may have additional research that you would benefit from.

I'd be surprised if you have not dealt with Dima Cook in one capacity or another over the years.

Dima has been involved in heritage related work in Toronto for likely 15 years.

ERA was also involved and I don't know who has what... Dima will.

#### Bay Street PATH Connection (North)

##### 1.1 Project Overview

The proposed development of 45 (81) & 141 Bay Street is part of a contemporary large-scale city building initiative incorporating transportation and is located within the Union Station Heritage Conservation District (HCD) Plan and adjacent to a number of prominent Beaux-Arts buildings associated with Toronto's development.

CHER

Bay Street railway Bridge  
Union Station rail Corridor  
Project # 16-118-01  
Prepared by GS / JN (ERA)  
Cultural Heritage Evaluation Report - Part 2

#### PREPARED FOR:

Metrolinx  
20 Bay Street, Suite 600  
Toronto Ontario M5J2W3  
Rodyney Yee, Project Coordinator, GO Transit  
[rodyney.yee@gotransit.com](mailto:rodyney.yee@gotransit.com)  
416-202-4516

#### PREPARED BY:

ERA Architects Inc.  
10 St. Mary Street, Suite 801  
Toronto, Ontario M4Y 1P9



416-963-4497

**FGMD**a  
ARCHITECTES

STAGE TWO  
HERITAGE IMPACT ASSESSMENT  
**Bay Street PATH  
Connection (North)**  
IVANHOÉ CAMBRIDGE C/O HINES  
PROJECT NO. EVOQ. 9019-13  
ISSUE DATE: NOVEMBER 2019



## Dima Cook

OAQ OAA AAA AANB RAIC LEED AP CAHP

ARCHITECT, PRINCIPAL

NEW OFFICE ADDRESS

75 SHERBOURNE STREET, SUITE 503

TORONTO, ON M5A 2P9

D./C. 416. 873.2018

T. 647.723.2030 / 1001

EVOQ ARCHITECTURE

@evoqarchitecture

Instagram / Facebook / LinkedIn

---

**From:** Guy Zimmerman

**Sent:** July 5, 2021 4:23 PM

**To:** 'Schopf, Heidy' <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Cc:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>; Brent Fairbairn <[Brent.Fairbairn@toronto.ca](mailto:Brent.Fairbairn@toronto.ca)>  
**Subject:** RE: Union Station HCD Background Request Cultural Heritage Report for the Waterfront East LRT - Outstanding question RE: Bridge supporting the Union Station Trainshed Inquiry

RE: Bridge supporting the Union Station Train shed Inquiry

Sorry Heidy... "I dropped the ball" on that aspect of my response/your request.

I don't see a problem sourcing the material that I previously referenced in time for our call (although there is nothing really to discuss in that regard... as it is really just providing the consultant's background associated with the integration of the new passage forward of the metal elevation)

**Guy Zimmerman - BEDS, CAHP**  
Heritage Planner  
Heritage Planning/Urban Design  
City Planning • City of Toronto  
[guy.zimmerman@toronto.ca](mailto:guy.zimmerman@toronto.ca)  
416.338.1094

---

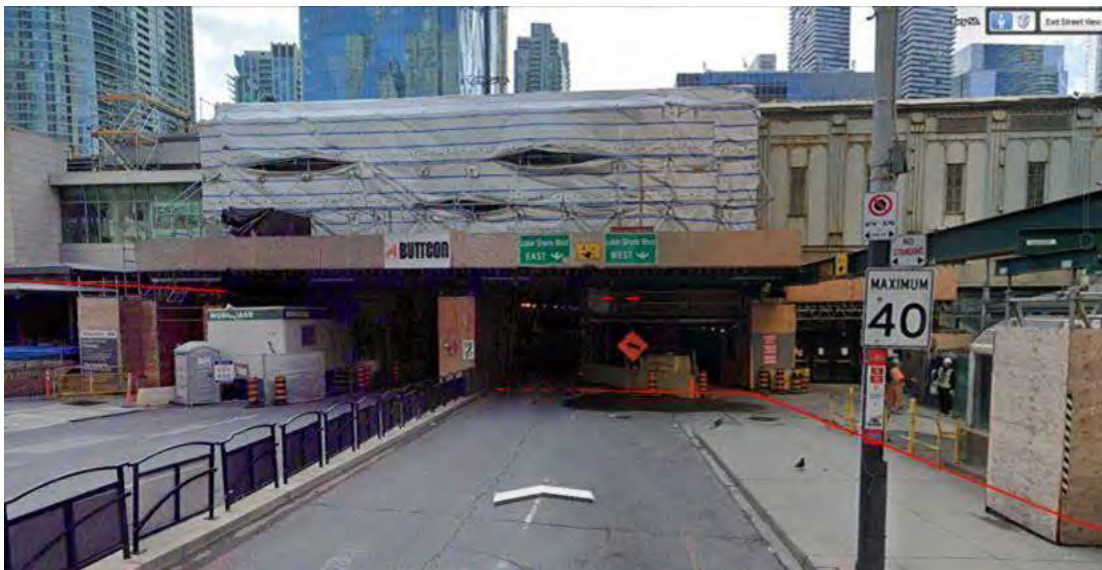
**From:** Schopf, Heidy [<mailto:heidy.schopf@woodplc.com>]  
**Sent:** July 5, 2021 3:06 PM  
**To:** Guy Zimmerman <[Guy.Zimmerman@toronto.ca](mailto:Guy.Zimmerman@toronto.ca)>  
**Cc:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>; Brent Fairbairn <[Brent.Fairbairn@toronto.ca](mailto:Brent.Fairbairn@toronto.ca)>  
**Subject:** RE: Union Station HCD Background Request Cultural Heritage Report for the Waterfront East LRT - Outstanding question RE: Gardiner and Path

Thanks Guy!

The only outstanding item is the heritage status of the bridge associated with the Union Station trainshed (see pictures below).

To date, we have assumed that this bridge is part of the Union Station complex. However, I would like to double check with you if this structure has any heritage status independent of Union Station. The bridge itself did not come up on our searches of the municipal register or in our information gathering exercises.

-Heidy



---

**From:** Guy Zimmerman <[Guy.Zimmerman@toronto.ca](mailto:Guy.Zimmerman@toronto.ca)>

**Sent:** Monday, July 5, 2021 9:30 AM

**To:** Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>

**Cc:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>; Brent Fairbairn <[Brent.Fairbairn@toronto.ca](mailto:Brent.Fairbairn@toronto.ca)>

**Subject:** RE: Union Station HCD Background Request Cultural Heritage Report for the Waterfront East LRT - Outstanding question RE: Gardiner and Path

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy et al,

Please confirm that the only outstanding information that you are seeking (to include in



your Cultural Heritage Report for the Waterfront East LRT) is clarification of the HCD direction regarding the PATH system and Gardiner Expressway.

(Between the material that I had provide and the additional information that Yasmina referenced I believe all other aspects of your request have been satisfied)

I have copied below excerpts from the 2006 Union Station HCD Appendix that can underpin our Teams discussion tomorrow at 3:00.

I had mentioned that I would send you a summary of what I intend to share regarding these two subjects and I believe that these excerpts serve that purpose.



### Gardiner Expressway

Era: Post-War  
Style: Modern Infrastructure  
Contributing: Yes

The development of this expressway was part of Fred Gardiner's vision of a modern Toronto. As Metropolitan Toronto's first Chairman, he was instrumental in the development of the Gardiner Expressway beginning in 1955. Built in an industrial zone at the foot of the city, the expressway modernized access to the centre of the city and from the city to its suburbs during a period of rapid growth. It was a nascent project in the process of creating regional infrastructure for the newly created Metropolitan region. The expressway was planned in conjunction with the rapid transit network as part of the comprehensive transportation planning of the region. The Gardiner was the first of several highways planned for the Toronto area, most of which were never realized.

The Skywalk was completed in 1989 to coincide with the completion of the Rogers Centre. Designed in the arcade style, the Skywalk consists of a large glazed passageway enclosed by a semi-circular arched roof reminiscent of early century European pedestrian areas. The Skywalk begins at the western most side of Union Station, passes through the second and third stories of the Canadian National Express building and crosses the Toronto rail viaduct at Simcoe Street. South of the railway the Skywalk traverses through the south end of the Metro Convention Centre and terminates in the plaza adjacent to the CN Tower, Roundhouse and Rogers Centre. The skywalk is a piece of urban infrastructure of growing importance; connecting the city to the increasingly developed rail lands. Furthermore, the skywalk offers unique views of Toronto's existing rail activity and provides a physical connection between to the District's two national historic sites: the Roundhouse and Union Station.



#### PATH System

Era: Post-War  
Style: Various  
Contributing: Yes

The PATH system is the subgrade pedestrian network which connects 6 subway stations and over 4 million square feet of retail space through 27 km of passages. It is currently the largest network of this type in the world.

The system developed as a result municipal policy of the 1960's encouraging sub-grade connections between new large office developments to one another and to the subway. The system has continued to grow incrementally, as new developments make connections with adjacent buildings. An underground tunnel of 1929 between the Fairmont Royal York Hotel and Union Station has been incorporated into the system.

Heritage Conservation Districts Plan Appendix L

141

Currently the city has a contentious relationship with the Gardiner as a result of a change in use of the central waterfront. As new solutions are realized in managing transportation in an increasingly post-industrial mixed-use waterfront and urban core, the Gardiner remains as an important piece of the City's early modern heritage and contributing element to the function and character of the Union Station Heritage Conservation District.

**Guy Zimmerman** - BEDS, CAHP  
Heritage Planner  
Heritage Planning/Urban Design  
City Planning • City of Toronto  
[guy.zimmerman@toronto.ca](mailto:guy.zimmerman@toronto.ca)

416.338.1094

---

**From:** Guy Zimmerman

**Sent:** June 18, 2021 11:20 AM

**To:** 'Schopf, Heidy' <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>

**Cc:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>; Brent Fairbairn <[Brent.Fairbairn@toronto.ca](mailto:Brent.Fairbairn@toronto.ca)>;

Yasmina Shamji <[Yasmina.Shamji@toronto.ca](mailto:Yasmina.Shamji@toronto.ca)>

**Subject:** RE: Union Station HCD Background Request - HP Response re 90 Harbour Street



Heidy,

Here is the low hanging fruit:

1) Workmen's Compensation Board Building

i) Withdrawal of Notice of Intention to Designate, Part IV, Section 29, Ontario Heritage Act - 90 Harbour Street

<http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2008.TE12.15>

ii) Demolition permit issued February 14 2011 (11 110475)

2) Gardiner Expressway or the PATH System

I need to have "a think" about this one and articulate their role in the HCD... and get back to you.

**Guy Zimmerman** - BEDS, CAHP  
Heritage Planner  
Heritage Planning/Urban Design  
City Planning • City of Toronto  
[guy.zimmerman@toronto.ca](mailto:guy.zimmerman@toronto.ca)  
416.338.1094

---

**From:** Schopf, Heidy [<mailto:heidyschopf@woodplc.com>]

**Sent:** June 18, 2021 10:21 AM

**To:** Guy Zimmerman <[Guy.Zimmerman@toronto.ca](mailto:Guy.Zimmerman@toronto.ca)>

**Cc:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>; Brent Fairbairn <[Brent.Fairbairn@toronto.ca](mailto:Brent.Fairbairn@toronto.ca)>;  
Yasmina Shamji <[Yasmina.Shamji@toronto.ca](mailto:Yasmina.Shamji@toronto.ca)>

**Subject:** RE: Union Station HCD Background Request

Hi Guy,

Following up on this. Do you have any direction on the two items below?

-Heidy

---

**From:** Schopf, Heidy

**Sent:** Friday, April 9, 2021 9:54 AM

**To:** Guy Zimmerman <[Guy.Zimmerman@toronto.ca](mailto:Guy.Zimmerman@toronto.ca)>

**Cc:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>; Brent Fairbairn <[Brent.Fairbairn@toronto.ca](mailto:Brent.Fairbairn@toronto.ca)>; Yasmina Shamji <[Yasmina.Shamji@toronto.ca](mailto:Yasmina.Shamji@toronto.ca)>

**Subject:** RE: Union Station HCD Background Request

Good morning Guy,

We received the HCD inventory information earlier this week. Thank you! The information was very helpful.

I have a couple questions:

- 1) The Workmen's Compenstaion Board Building (90 Harbour Street) appears to have been demolished between 2009 and 2012 based on Google Earth imagery. Can you confirm?
- 2) The Union Station HCD Plan does not depict the Gardiner Expressway or the PATH System as contributing buildings in the HCD (see below). Yet, the inventory sheets note that these resources are "contributing". What is the heritage status of these two resources? Our approach has been to address them as elements of the HCD in general but not as individual resources. It would be great to get some direction here to confirm if the City views the Gardiner and the PATH System as standalone heritage resources (or not).

Thanks,  
Heidy



### Contributing Buildings

**From:** Guy Zimmerman <[Guy.Zimmerman@toronto.ca](mailto:Guy.Zimmerman@toronto.ca)>

**Sent:** Friday, March 26, 2021 9:00 AM

**To:** Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>

**Cc:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>; Brent Fairbairn <[Brent.Fairbairn@toronto.ca](mailto:Brent.Fairbairn@toronto.ca)>;

Yasmina Shamji <[Yasmina.Shamji@toronto.ca](mailto:Yasmina.Shamji@toronto.ca)>

**Subject:** Union Station HCD Background Request

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy/Luke,

I will be working on this today.

The HCD material that you are referencing dates back to 2006.



Brent Fairbairn has made an inquiry as well and I will determine what additional material we have relative to your requests... beyond what Yasmina has already provided.

**Guy Zimmerman** - BEDS, CAHP  
Heritage Planner  
Heritage Planning/Urban Design  
City Planning • City of Toronto  
[guy.zimmerman@toronto.ca](mailto:guy.zimmerman@toronto.ca)  
416.338.1094

**From:**

---

**From:** Schopf, Heidi [<mailto:heidy.schopf@woodplc.com>]  
**Sent:** March 26, 2021 6:53 AM  
**To:** Guy Zimmerman <[Guy.Zimmerman@toronto.ca](mailto:Guy.Zimmerman@toronto.ca)>  
**Cc:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>  
**Subject:** FW: Information Gathering Request

Hi Guy,

I am following up on an information request submitted by Luke this past January. Apologies if you already replied to him!

We are completing a Cultural Heritage Report for the Waterfront East LRT and part of the Study Area falls within the Union Station HCD. Any additional information you can provide on the heritage attributes of the district and property sheets for addresses in the Study Area would be hugely helpful! Happy to have a call sometime if that works better for you.

Thank you,  
Heidy

---

**From:** Yasmina Shamji <[Yasmina.Shamji@toronto.ca](mailto:Yasmina.Shamji@toronto.ca)>  
**Sent:** Thursday, March 25, 2021 11:36 AM  
**To:** Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Subject:** FW: Information Gathering Request

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy

Sorry, I didn't realize I hadn't copied you on the email. Here is my response to Luke.

Thank you

Yasmina Shamji  
Urban Design | Heritage Planning  
Toronto City Hall | 100 Queen Street West, 17<sup>th</sup> Floor East Tower  
Toronto, ON M5H 2N2  
416-392-1975  
[City of Toronto](#)



---

**From:** Yasmina Shamji  
**Sent:** January 7, 2021 4:27 PM  
**To:** 'Fischer, Luke' <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>  
**Cc:** Guy Zimmerman <[Guy.Zimmerman@toronto.ca](mailto:Guy.Zimmerman@toronto.ca)>; Joe Muller <[Joe.Muller@toronto.ca](mailto:Joe.Muller@toronto.ca)>  
**Subject:** RE: Information Gathering Request

Luke

Happy New Year!

I have reviewed your report and compiled the information as requested. I don't have information for the properties noted in red below – it's possible details for these properties doesn't exist. In that case, I am deferring your request to Guy Zimmerman (also copied on this email), who is the Heritage Planner for the Union Station Heritage Conservation District.

According to our [Heritage Register Map](#), the following properties fall within the area study areas indicated in your report:

- Bay Street from Front Street West to Queens Quay East
- Queens Quay East from York Street to Cooper Street

(I am attaching the link to the property details from our online register as well as the by-laws wherever applicable)

40 Bay St: Part IV Designation (Part IV By-law 360-90) as well as Union Station HCD (By-law 634-2006)  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433226&propertyRsn=206956>  
<https://www.toronto.ca/legdocs/pre1998bylaws/toronto%20-%20former%20city%20of/1990-0360.pdf>

71 Front St W (Union Station) aka 140 & 142 Bay St & 55 Front St W: Part IV Designation (Part IV By-law 948-2005) as well as Union Station HCD  
And 61 Front St W - <http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2440090&propertyRsn=206955>  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2435414&propertyRsn=705027>  
<https://www.toronto.ca/legdocs/bylaws/2005/law0948.pdf>

145 Queens Quay W aka 77 Harbour Sq: Part IV Designation (By-law 1249-2007)  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2439065&propertyRsn=206198>  
<https://www.toronto.ca/legdocs/bylaws/2007/law1249.pdf>

2 Cooper St (entrance address for 55 Lake Shore Blvd E) – Under Intention to Designate  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2438226&propertyRsn=719130>  
<http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2018.TE30.17>

Further to the above, you also requested information sheets for the following properties:

**Gardiner Expressway – no information available – deferred to Guy**

1 Front St W  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2435336&propertyRsn=578987>  
<https://www.toronto.ca/legdocs/bylaws/2017/law0423.pdf>

61 Front St W – Listed – no additional information is available  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2440090&propertyRsn=206955>

65 Front St W - refer to details provide above

**18 Yonge St – no property info sheets available – deferred to Guy**  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2437048&propertyRsn=615674>

**33 Bay St - no property info sheets available – deferred to Guy**  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433225&propertyRsn=751123>

40 Bay St – refer to details provide above

**161 Bay St - no property info sheets available – deferred to Guy**  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=3246951&propertyRsn=206967>

**200 Bay St - no property info sheets available – deferred to Guy**  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433240&propertyRsn=206973>

**60 Harbour St - no property info sheets available – deferred to Guy**  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2434664&propertyRsn=206938>

**88-100 Harbour St - no property info sheets available – deferred to Guy**  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=3607011&propertyRsn=826634>

**15-19 York St - no property info sheets available – deferred to Guy**  
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2436569&propertyRsn=750799>

Also, please note that most of the information provided above can be found on our website via the links below:

<https://www.toronto.ca/city-government/planning-development/heritage-preservation/heritage-register/> -



using both the Heritage Register Map as well as the Heritage Search Tool.

By-law information can be found here: <https://www.toronto.ca/legdocs/bylaws/lawhome.htm>

Thank you

Yasmina Shamji  
Urban Design | Heritage Planning  
Toronto City Hall | 100 Queen Street West, 17<sup>th</sup> Floor East Tower  
Toronto, ON M5H 2N2  
[City of Toronto](#)

416-392-1975



---

**From:** Fischer, Luke [<mailto:luke.fischer@woodplc.com>]  
**Sent:** December 16, 2020 3:13 PM  
**To:** Yasmina Shamji <[Yasmina.Shamji@toronto.ca](mailto:Yasmina.Shamji@toronto.ca)>  
**Cc:** Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Subject:** Information Gathering Request

Hello Yasmina,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,  
Luke

**Luke Fischer, M.A., CAHP**  
Senior Archaeologist, Built Heritage Analyst  
Direct: 1 (519) 681-2400  
Mobile: 1 (226) 376-2968  
[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)  
[www.woodplc.com](http://www.woodplc.com)

**wood.**

---

This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: [unsubscribe@woodplc.com](mailto:unsubscribe@woodplc.com) and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.

Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at <https://www.woodplc.com/policies/privacy-notice>

---

**From:** [Kevin DeMille](#)  
**To:** [Schopf, Heidi](#)  
**Subject:** Re: Information Gathering Request  
**Date:** Tuesday, July 13, 2021 8:07:01 AM  
**Attachments:** [image001.png](#)

---

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Good morning Heidi,

Please feel free to contact:

Kirushanth Gnanachandran  
Project Coordinator, Rouge National Urban Park  
Parks Canada / Government of Canada  
[kirushanth.gnanachandran@canada.ca](mailto:kirushanth.gnanachandran@canada.ca) /Tel: 647-205-6150

They were notified and should be able to help you or forward you on to the correct person.

Kind regards,

Kevin De Mille

Kevin De Mille  
Natural Heritage Coordinator  
Telephone: 437-246-5854\*NEW



---

**From:** Schopf, Heidi <heidy.schopf@woodplc.com>  
**Sent:** July 8, 2021 10:50 AM  
**To:** Kevin DeMille <Kevin.DeMille@heritagetrust.on.ca>; Fischer, Luke <luke.fischer@woodplc.com>  
**Subject:** RE: Information Gathering Request

**CAUTION:** External mail. Do not click on links or open attachments unless you recognize the sender and know the content.

Hi Kevin,

It's been a while but following up on this. Is there a contact at Parks Canada that you could recommend? We are trying to track down the correct person/department to contact regarding Union Station.



Thanks,  
Heidy

---

**From:** Kevin DeMille <Kevin.DeMille@heritagetrust.on.ca>  
**Sent:** Wednesday, April 14, 2021 11:15 AM  
**To:** Fischer, Luke <luke.fischer@woodplc.com>  
**Cc:** Schopf, Heidy <heidy.schopf@woodplc.com>  
**Subject:** Re: Information Gathering Request

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Good morning Luke,

You may have these already but the Registrar provided the following information:

- 1 Front Street West: <https://www.heritagetrust.on.ca/en/oha/details?id=3781&backlinkslug=search-results&fields%5Baddress%5D=1+Front+Street>
- Union Station (note I have it as 71 Front Street West) and includes HCD info: [https://www.heritagetrust.on.ca/en/oha/details?id=2024&backlinkslug=search-results&fields%5Bproperty\\_name%5D=Union+Station](https://www.heritagetrust.on.ca/en/oha/details?id=2024&backlinkslug=search-results&fields%5Bproperty_name%5D=Union+Station)
- 40 Bay Street: <https://www.heritagetrust.on.ca/en/oha/details?id=2522&backlinkslug=search-results&fields%5Baddress%5D=40+bay+street>

There is a National Historic Site, so we recommend you reach out to Parks Canada.

Kind regards,

Kevin De Mille

Kevin De Mille  
Natural Heritage Coordinator  
Telephone: 437-246-5854\*NEW



**From:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>  
**Sent:** April 8, 2021 11:04 AM  
**To:** Kevin DeMille <[Kevin.DeMille@heritagetrust.on.ca](mailto:Kevin.DeMille@heritagetrust.on.ca)>  
**Cc:** Schopf, Heidy <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Subject:** RE: Information Gathering Request

**CAUTION: External mail. Do not click on links or open attachments unless you recognize the sender and know the content.**

Hi Kevin,

Thank you for your feedback for the TTC Waterfront East LRT CHR. We have completed a draft report for this project. For the next stage of our work, we will prepare four HIAs for the following properties:

- CHR 1: Union Station Heritage Conservation District → designated under Part V of the OHA
- CHR 2: Union Station, 65 Front Street → designated under Parts IV and V of the OHA, National Historic Site, Provincial Property of Provincial Significance
- CHR 3: Dominion Public Building, 1 Front Street → designated under Parts IV and V of the OHA, Classified Federal Heritage Building (FHBRO)
- CHR 4: Postal Delivery Building, 40 Bay Street → designated under Parts IV and V of the OHA

Do you have any further information or guidance regarding these properties? We plan to prepare the HIAs using *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties* (2017) but can use other guidance documents if you recommend a different approach.

In addition, we are carrying out community engagement for these HIAs via email and telephone. We have ongoing dialogue with MHSTCI and the City of Toronto but also plan to reach out to the Toronto Preservation Board, GO Transit, Metrolinx, and the property owners (i.e. Canada Revenue and Maple Leaf Sports). Do you have any further groups that you feel we should contact? We are committed to providing an early and ongoing dialogue regarding the potential impacts of the project to these properties so that we can incorporate feedback into our reports.

Thanks ,  
Luke

---

**From:** Schopf, Heidy <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Sent:** Wednesday, April 7, 2021 9:44 AM  
**To:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>  
**Subject:** FW: Information Gathering Request

---

**From:** Kevin DeMille <[Kevin.DeMille@heritagetrust.on.ca](mailto:Kevin.DeMille@heritagetrust.on.ca)>

**Sent:** Friday, March 26, 2021 3:46 PM  
**To:** Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Subject:** Re: Information Gathering Request

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Good afternoon Heidi,

I'm sorry for the confusion. I was away in December and January and it seems your inquiry was received during my absence and unfortunately the response was not sent.

Thank you for your information request related to a Cultural Heritage Evaluation Report relating to the TTC Waterfront East LRT study area including Bay Street from Front Street West to Queens Quay East, and Queens Quay East from York Street to Cooper Street in Toronto. Your request to verify the presence of OHT heritage properties or easements within or adjacent to the study area has been processed. I've reviewed the study area against our database of OHT easements and properties. We can confirm that the OHT does not have any conservation easements or Trust-owned properties within or adjacent to the study area provided in your map.

If you have not already done so, I recommend you check the Trust's Plaque Database (available online) <https://www.heritagetrust.on.ca/en/online-plaque-guide> to verify the presence of plaques. Additionally, I recommend you check the Trust's register (available online) <http://www.heritagetrust.on.ca/en/index.php/pages/tools/ontario-heritage-act-register> and contact the local municipality to verify no local heritage properties are present within the identified study area.

As described in Section 23 of the Ontario Heritage Act, the Trust holds and maintains the provincial Ontario Heritage Act Register of properties that have been designated by municipalities under sections 29 and 41 of the Act as well as properties designated under the Act by the Minister. We rely on municipalities to send us information and it is advisable to check with the clerk's office to verify information.

Under Section 27 of the Act (OHA) the clerk of a municipality is required to maintain a local register of all designated properties. Section 27 also states that municipalities may keep a register of property that has not been designated, but that the municipality has determined to be of cultural heritage value or interest. These are often referred to as "listed" properties. These non-designated heritage properties are not reflected in the OHA Register.

Kind regards,

Kevin De Mille

Kevin De Mille  
Natural Heritage Coordinator  
Telephone: 437-246-5854\*NEW





---

**From:** Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Sent:** March 25, 2021 10:02 AM  
**To:** registrar <[registrar@heritagetrust.on.ca](mailto:registrar@heritagetrust.on.ca)>  
**Cc:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>  
**Subject:** FW: Information Gathering Request

**CAUTION:** External mail. Do not click on links or open attachments unless you recognize the sender and know the content.

Good morning,

Following up on this request. Do you have any information regarding trust-owned properties or heritage easements in this Study Area? Apologies if this information was already sent. I can find a record in my email.

Sincerely,  
Heidy

---

**From:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>  
**Sent:** Wednesday, December 16, 2020 3:10 PM  
**To:** [registrar@heritagetrust.on.ca](mailto:registrar@heritagetrust.on.ca)  
**Cc:** Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Subject:** Information Gathering Request

Hello,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,  
Luke

**Luke Fischer, M.A., CAHP**  
Senior Archaeologist, Built Heritage Analyst  
Direct: 1 (519) 681-2400

Mobile: 1 (226) 376-2968  
[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)  
[www.woodplc.com](http://www.woodplc.com)

# wood.

---

This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: [unsubscribe@woodplc.com](mailto:unsubscribe@woodplc.com) and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.

Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at <https://www.woodplc.com/policies/privacy-notice>

---

**From:** [Barboza, Karla \(MHSTCI\)](#)  
**To:** [Fischer, Luke](#)  
**Cc:** [Hatcher, Laura \(MHSTCI\)](#); [Registrar \(MHSTCI\)](#); [Schopf, Heidi](#)  
**Subject:** RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)  
**Date:** Thursday, April 8, 2021 4:27:49 PM  
**Attachments:** [image001.png](#)

---

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Hi Luke,

I hope this email finds you well.

It may be prudent for us to review the draft Cultural Heritage Report before we provide any advice on the approach for the Heritage Impact Assessment. Would you be able to share the draft report for our review?

Regards,  
Karla

---

**From:** Fischer, Luke <luke.fischer@woodplc.com>  
**Sent:** April-08-21 9:11 AM  
**To:** Hatcher, Laura (MHSTCI) <Laura.E.Hatcher@ontario.ca>; Schopf, Heidi <heidy.schopf@woodplc.com>; Registrar (MHSTCI) <Registrar@ontario.ca>  
**Subject:** RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

**CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.**

Hi Laura,

Thank you for your feedback for the TTC Waterfront East LRT CHR. We have completed a draft report for this project and we will circulate this to you for review shortly. For the next stage of our work, we will prepare four HIAs for the following properties:

- CHR 1: Union Station Heritage Conservation District → designated under Part V of the OHA
- CHR 2: Union Station, 65 Front Street → designated under Parts IV and V of the OHA, National Historic Site, Provincial Property of Provincial Significance
- CHR 3: Dominion Public Building, 1 Front Street → designated under Parts IV and V of the OHA, Classified Federal Heritage Building (FHBRO)
- CHR 4: Postal Delivery Building, 40 Bay Street → designated under Parts IV and V of the OHA

Do you have any further information or guidance regarding these properties? We plan to prepare the HIAs using *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties*



(2017) but can use other guidance documents if you recommend a different approach.

In addition, we are carrying out community engagement for these HIAs via email and telephone. We have ongoing dialogue with the City of Toronto but also plan to reach out to the Toronto Preservation Board, GO Transit, Metrolinx, and the property owners (i.e. Canada Revenue and Maple Leaf Sports). Do you have any further groups that you feel we should contact? We are committed to providing an early and ongoing dialogue regarding the potential impacts of the project to these properties so that we can incorporate feedback into our reports.

Thanks,  
Luke

---

**From:** Hatcher, Laura (MHSTCI) <[Laura.E.Hatcher@ontario.ca](mailto:Laura.E.Hatcher@ontario.ca)>  
**Sent:** Friday, December 18, 2020 12:56 PM  
**To:** Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>; Registrar (MHSTCI) <[Registrar@ontario.ca](mailto:Registrar@ontario.ca)>; Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>  
**Subject:** RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Hi Heidi and Luke,

Karla is away today so I am following up with the statements of Cultural Heritage Value for the three properties. Please let us know if you need anything else.

Sincerely,  
Laura

**Laura Hatcher, MCIP, RPP**

Heritage Planner

Heritage Planning Unit | Programs and Services Branch | Heritage, Tourism and Culture Division

Ministry of Heritage, Sport, Tourism and Culture Industries

401 Bay Street Suite 1700 Toronto ON M7A 0A7

Tel. 437-239-3404 **New** | email: [laura.e.hatcher@ontario.ca](mailto:laura.e.hatcher@ontario.ca)

---

**From:** Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Sent:** Friday, December 18, 2020 7:24 AM  
**To:** Registrar (MHSTCI) <[Registrar@ontario.ca](mailto:Registrar@ontario.ca)>; Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>  
**Cc:** Hatcher, Laura (MHSTCI) <[Laura.E.Hatcher@ontario.ca](mailto:Laura.E.Hatcher@ontario.ca)>  
**Subject:** RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

**CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.**

Hi Karla,

Thank you so much for the quick reply. Yes, please send along the 'Statements of Cultural Heritage Value' for all three properties. That would be much appreciated!

I will double check on the process as well and get back to you.

Happy Holidays

-Heidy

---

**From:** Barboza, Karla (MHSTCI) <[Karla.Barboza@ontario.ca](mailto:Karla.Barboza@ontario.ca)> **On Behalf Of** Registrar (MHSTCI)  
**Sent:** Thursday, December 17, 2020 5:02 PM  
**To:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>  
**Cc:** Registrar (MHSTCI) <[Registrar@ontario.ca](mailto:Registrar@ontario.ca)>; Hatcher, Laura (MHSTCI) <[Laura.E.Hatcher@ontario.ca](mailto:Laura.E.Hatcher@ontario.ca)>; Schopf, Heidy <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Subject:** MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

### **File 0013552 – TTC Waterfront East Light Rail Transit (LRT)**

Hi Luke,

I hope this email finds you well.

As you may know, the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI) developed screening checklists to assist property owners, developers, consultants and others to identify known and potential cultural heritage resources:

- [Criteria for Evaluating Archaeological Potential](#)
- [Criteria for Evaluating Marine Archaeological Potential](#)
- [Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes](#)

I have used the document above (Built Heritage and Cultural Heritage Landscapes) in order to respond to your question:

- Question 3a. i. Is the property (or project area) identified, designated or otherwise protected under the Ontario Heritage Act as being of cultural heritage value e.g. a property that is designated by order of the Minister of Heritage, Sport, Tourism and Culture Industries as being of cultural heritage value or interest of provincial significance [s.34.5]?

*MHSTCI Response:* To date, no properties have been designated by the Minister.

- Question 3a.v. Is the property (or project area) identified, designated or otherwise protected under the Ontario Heritage Act as being of cultural heritage value included in the Ministry of Heritage, Sport, Tourism and Culture Industries' list of provincial heritage properties?

*MHSTCI Response:* The following provincial heritage properties are within or adjacent to the study area:

- Union Station – provincial heritage property of provincial significance
- Union Station Rail Corridor (USRC) Interlocking Tower - Scott Street - provincial heritage property of provincial significance
- Yonge Street Railway Bridge – provincial heritage property (of local significance)

Let me know if you would like a copy of the Statement of Cultural Heritage Value for any of the properties above. None of these properties have a Strategic Conservation Plan in place. Please note that if the subject lands or parts of the subject lands are owned or controlled by an Ontario Ministry or Prescribed Public Body (PPB) on behalf of the Crown (the list of PPBs is available as O. Reg. 157/10), a Ministry or PPB may have responsibilities under the [Standards and Guidelines for Conservation of Provincial Heritage Properties](#).

Regarding your request about other protected heritage properties within or adjacent to the study area, you should contact the Ontario Heritage Trust, Provincial Heritage Registrar at [registrar@heritagetrust.on.ca](mailto:registrar@heritagetrust.on.ca) or 416-212-7104 and the City of Toronto Heritage Preservation Services.

Please let us know which process will this project fall under (e.g. Ontario Regulation 231/08 – Transit Project Assessment Process) as we may have additional advice.

I hope this helps. Let me know if you have any questions.

Regards,  
Karla

Karla Barboza MCIP, RPP, CAHP | (A) Team Lead, Heritage  
Ministry of Heritage, Sport, Tourism and Culture Industries  
Heritage, Tourism and Culture Division | Programs and Services Branch | Heritage Planning Unit  
T. 416.314.7120 | Email: [karla.barboza@ontario.ca](mailto:karla.barboza@ontario.ca)

---

**From:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>

**Sent:** December-16-20 3:08 PM

**To:** Registrar (MHSTCI) <[Registrar@ontario.ca](mailto:Registrar@ontario.ca)>

**Cc:** Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>

**Subject:** Information Gathering Request

**CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.**

Hello,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,  
Luke

**Luke Fischer, M.A., CAHP**

Senior Archaeologist, Built Heritage Analyst

Direct: 1 (519) 681-2400

Mobile: 1 (226) 376-2968

[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)

[www.woodplc.com](http://www.woodplc.com)

The logo for Wood, featuring the word "wood." in a bold, dark blue, lowercase sans-serif font. The period is a solid dot.

---

This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: [unsubscribe@woodplc.com](mailto:unsubscribe@woodplc.com) and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.

Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at



<https://www.woodplc.com/policies/privacy-notice>

---

**From:** [Hatcher, Laura \(MHSTCI\)](#)  
**To:** [Schopf, Heidy](#); [Registrar \(MHSTCI\)](#); [Fischer, Luke](#)  
**Subject:** RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)  
**Date:** Friday, December 18, 2020 12:56:19 PM  
**Attachments:** [image001.png](#)  
[MHC SCHV - Union Station FINAL.pdf](#)  
[4. USRC Scott Street Interlocking Tower - MHC Decision Form & SCHV \(PHPPS\).pdf](#)  
[Metrolinx Heritage Committee - Yonge Street Bridge.pdf](#)

---

**CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.**

Hi Heidy and Luke,

Karla is away today so I am following up with the statements of Cultural Heritage Value for the three properties. Please let us know if you need anything else.

Sincerely,  
Laura

**Laura Hatcher, MCIP, RPP**

Heritage Planner

Heritage Planning Unit | Programs and Services Branch | Heritage, Tourism and Culture Division

Ministry of Heritage, Sport, Tourism and Culture Industries

401 Bay Street Suite 1700 Toronto ON M7A 0A7

Tel. 437-239-3404 [New](#) | email: [laura.e.hatcher@ontario.ca](mailto:laura.e.hatcher@ontario.ca)

---

**From:** Schopf, Heidy <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Sent:** Friday, December 18, 2020 7:24 AM  
**To:** Registrar (MHSTCI) <[Registrar@ontario.ca](mailto:Registrar@ontario.ca)>; Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>  
**Cc:** Hatcher, Laura (MHSTCI) <[Laura.E.Hatcher@ontario.ca](mailto:Laura.E.Hatcher@ontario.ca)>  
**Subject:** RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

**CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.**

Hi Karla,

Thank you so much for the quick reply. Yes, please send along the 'Statements of Cultural Heritage Value' for all three properties. That would be much appreciated!

I will double check on the process as well and get back to you.

Happy Holidays

-Heidy

---

**From:** Barboza, Karla (MHSTCI) <[Karla.Barboza@ontario.ca](mailto:Karla.Barboza@ontario.ca)> **On Behalf Of** Registrar (MHSTCI)  
**Sent:** Thursday, December 17, 2020 5:02 PM  
**To:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>  
**Cc:** Registrar (MHSTCI) <[Registrar@ontario.ca](mailto:Registrar@ontario.ca)>; Hatcher, Laura (MHSTCI) <[Laura.E.Hatcher@ontario.ca](mailto:Laura.E.Hatcher@ontario.ca)>; Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Subject:** MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

### File 0013552 – TTC Waterfront East Light Rail Transit (LRT)

Hi Luke,

I hope this email finds you well.

As you may know, the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI) developed screening checklists to assist property owners, developers, consultants and others to identify known and potential cultural heritage resources:

- [Criteria for Evaluating Archaeological Potential](#)
- [Criteria for Evaluating Marine Archaeological Potential](#)
- [Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes](#)

I have used the document above (Built Heritage and Cultural Heritage Landscapes) in order to respond to your question:

- Question 3a. i. Is the property (or project area) identified, designated or otherwise protected under the Ontario Heritage Act as being of cultural heritage value e.g. a property that is designated by order of the Minister of Heritage, Sport, Tourism and Culture Industries as being of cultural heritage value or interest of provincial significance [s.34.5]?  
*MHSTCI Response:* To date, no properties have been designated by the Minister.
- Question 3a.v. Is the property (or project area) identified, designated or otherwise protected under the Ontario Heritage Act as being of cultural heritage value included in the Ministry of Heritage, Sport, Tourism and Culture Industries' list of provincial heritage properties?  
*MHSTCI Response:* The following provincial heritage properties are within or adjacent to the study area:
  - Union Station – provincial heritage property of provincial significance
  - Union Station Rail Corridor (USRC) Interlocking Tower - Scott Street - provincial heritage property of provincial significance
  - Yonge Street Railway Bridge – provincial heritage property (of local significance)Let me know if you would like a copy of the Statement of Cultural Heritage Value for any of the properties above. None of these properties have a Strategic Conservation Plan in place.

Please note that if the subject lands or parts of the subject lands are owned or controlled by an Ontario Ministry or Prescribed Public Body (PPB) on behalf of the Crown (the list of PPBs is available as O. Reg. 157/10), a Ministry or PPB may have responsibilities under the [Standards and Guidelines for Conservation of Provincial Heritage Properties](#).

Regarding your request about other protected heritage properties within or adjacent to the study area, you should contact the Ontario Heritage Trust, Provincial Heritage Registrar at [registrar@heritagetrust.on.ca](mailto:registrar@heritagetrust.on.ca) or 416-212-7104 and the City of Toronto Heritage Preservation Services.

Please let us know which process will this project fall under (e.g. Ontario Regulation 231/08 – Transit Project Assessment Process) as we may have additional advice.

I hope this helps. Let me know if you have any questions.

Regards,  
Karla

Karla Barboza MCIP, RPP, CAHP | (A) Team Lead, Heritage  
Ministry of Heritage, Sport, Tourism and Culture Industries  
Heritage, Tourism and Culture Division | Programs and Services Branch | Heritage Planning Unit  
T. 416.314.7120 | Email: [karla.barboza@ontario.ca](mailto:karla.barboza@ontario.ca)

---

**From:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>  
**Sent:** December-16-20 3:08 PM  
**To:** Registrar (MHSTCI) <[Registrar@ontario.ca](mailto:Registrar@ontario.ca)>  
**Cc:** Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Subject:** Information Gathering Request

**CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.**

Hello,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,  
Luke

**Luke Fischer, M.A., CAHP**  
Senior Archaeologist, Built Heritage Analyst  
Direct: 1 (519) 681-2400  
Mobile: 1 (226) 376-2968



[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)

[www.woodplc.com](http://www.woodplc.com)

# wood.

---

This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: [unsubscribe@woodplc.com](mailto:unsubscribe@woodplc.com) and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.

Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at <https://www.woodplc.com/policies/privacy-notice>

---

**From:** [Gnanachandran, Kirushanth \(PC\)](#)  
**To:** [Schopf, Heidi](#)  
**Cc:** [Fischer, Luke](#); [Dickinson, Chelsea](#)  
**Subject:** RE: Information Request: Union Station (65-71 Front St W) and Dominion Public Building (1 Front St W)  
**Date:** Tuesday, July 13, 2021 5:46:06 PM  
**Attachments:** [image002.png](#)

---

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Hello Heidi,

Thank you for your email. I have forwarded on your information request to a colleague at FHBRO, and another colleague that is with Cultural Resource Management. They may be reaching out to you to help you with your request.

If you don't hear back from them soon, please let me know.

Thanks and have a great day,

Kirushanth Gnanachandran

Project Coordinator, Rouge National Urban Park  
Parks Canada / Government of Canada  
[kirushanth.gnanachandran@canada.ca](mailto:kirushanth.gnanachandran@canada.ca) /Tel: 647-205-6150

Coordonnateur de projet, Parc urbain national de la Rouge  
Parcs Canada / Gouvernement du Canada  
[kirushanth.gnanachandran@canada.ca](mailto:kirushanth.gnanachandran@canada.ca) / Tél: 647-205-6150

**Parks Canada - 450 000 km<sup>2</sup> of memories / Parcs Canada - 450 000 km<sup>2</sup> de souvenirs**

---

**From:** Schopf, Heidi <heidy.schopf@woodplc.com>  
**Sent:** July 13, 2021 12:04 PM  
**To:** Gnanachandran, Kirushanth (PC) <kirushanth.gnanachandran@canada.ca>  
**Cc:** Fischer, Luke <luke.fischer@woodplc.com>; Dickinson, Chelsea <chelsea.dickinson@woodplc.com>  
**Subject:** Information Request: Union Station (65-71 Front St W) and Dominion Public Building (1 Front St W)

Good afternoon Kirushanth,

Kevin DeMille from the Ontario Heritage Trust passed your contact information to me.

Wood has been retained by the Toronto Transit Commission (TTC) to complete a Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment (CHR) and subsequent Heritage Impact Assessments (HIAs) to support the Waterfront East Light Rail Transit (WELRT) project in the City of Toronto, Ontario. To date, our work has identified that there are two properties with federal protection within the Study Area. These include:

- Union Station (65-71 Front Street West): Designated as a National Historic Site of Canada under the Historic Sites and Monuments Act by Parks Canada on 1975-11-28 (R.S.C., 1985, c. H-4)
- Dominion Public Building (1 Front Street West): Classified as a Federal Heritage Building by Parks Canada's Federal Heritage Review Office (FHBRO) in 1983

Wood has completed information gathering with the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI), the Ontario Heritage Trust, and the City of Toronto. These groups identified that Parks Canada should be consulted as part of this work given the presence of federal heritage properties within the Study Area. Accordingly, Wood is seeking background information on both Union Station (65-71 Front Street West) and the Dominion Public Building (1 Front Street West) and direction on any permits or approvals required for these properties.

Any information you can provide would be greatly appreciated.

Sincerely,

**Heidy Schopf, MES, CAHP (she/her)**

Built and Landscape Heritage Team Lead

Mobile: +1 (416) 518 0145

[www.woodplc.com](http://www.woodplc.com)

The logo for Wood, featuring the word "wood." in a bold, lowercase, sans-serif font. The period is a solid black dot.

---

This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us

immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: [unsubscribe@woodplc.com](mailto:unsubscribe@woodplc.com) and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.

Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at <https://www.woodplc.com/policies/privacy-notice>

---



**From:** [Tamkin Naghshbandi](#)  
**To:** [Schopf, Heidy](#); [Dan Beare](#)  
**Cc:** [Fischer, Luke](#); [Dickinson, Chelsea](#)  
**Subject:** RE: Information Request: Union Station (65-71 Front Street West), Provincial Heritage Property of Provincial Significance  
**Date:** Friday, July 16, 2021 4:27:32 PM  
**Attachments:** [image001.png](#)

---

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Glad to be of help! Sounds good, thanks Heidy. Have a great weekend.

Kindly,

**Tamkin Naghshbandi**

T: 416.202.1804 | M: 416.271.1546

---

**From:** Schopf, Heidy <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>  
**Sent:** Friday, July 16, 2021 4:21 PM  
**To:** Tamkin Naghshbandi <[Tamkin.Naghshbandi@metrolinx.com](mailto:Tamkin.Naghshbandi@metrolinx.com)>; Dan Beare <[Dan.Beare@metrolinx.com](mailto:Dan.Beare@metrolinx.com)>  
**Cc:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>; Dickinson, Chelsea <[chelsea.dickinson@woodplc.com](mailto:chelsea.dickinson@woodplc.com)>  
**Subject:** RE: Information Request: Union Station (65-71 Front Street West), Provincial Heritage Property of Provincial Significance

**EXTERNAL SENDER:** Do not click any links or open any attachments unless you trust the sender and know the content is safe.  
**EXPÉDITEUR EXTERNE:** Ne cliquez sur aucun lien et n'ouvrez aucune pièce jointe à moins qu'ils ne proviennent d'un expéditeur fiable, ou que vous ayez l'assurance que le contenu provient d'une source sûre.

Thank you so much Tamkin – This is hugely helpful!

I will review the documents and may be in touch again next week if I have questions.

Have a wonderful weekend,  
Heidy

---

**From:** Tamkin Naghshbandi <[Tamkin.Naghshbandi@metrolinx.com](mailto:Tamkin.Naghshbandi@metrolinx.com)>  
**Sent:** Friday, July 16, 2021 1:51 PM  
**To:** Schopf, Heidy <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>; Dan Beare <[Dan.Beare@metrolinx.com](mailto:Dan.Beare@metrolinx.com)>  
**Cc:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>; Dickinson, Chelsea <[chelsea.dickinson@woodplc.com](mailto:chelsea.dickinson@woodplc.com)>  
**Subject:** RE: Information Request: Union Station (65-71 Front Street West), Provincial Heritage Property of Provincial Significance

**CAUTION:** External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Hi Heidi,

Thanks for reaching out and providing the background on this ask. We've put pulled some documents we hope you will find of assistance. The reference documents listed below are available at the following link: <https://we.tl/t-wZ0zwDr5al>. Please note this link will expire in 2 weeks.

- SCHV for Union Station
- Heritage Statement Report, Union Station Complex, Toronto, 2016
- Union Station Electrification HIA, 2017
- Heritage Easement Agreement, 2000
- Collateral Agreement, 2006
- Standards and Guidelines for Conservation of Provincial Heritage Properties (Ontario Heritage Act), 2010

For details related to direction on permits or approvals required for work proposed at Union Station, please see below:

- It's important to note that Union Station is owned by a number of parties.
- In 2016, Union Station Complex was identified by Metrolinx as a Provincial Heritage Property of Provincial Significance. Prescribed public bodies prescribed under Ontario Regulation 157/10, must comply with the Standards and Guidelines for Conservation of Provincial Heritage Properties (Ontario Heritage Act). Ministries and prescribed public bodies are required to use best efforts to mitigate loss of cultural heritage value or interest. The Standards and Guidelines, established by MHSTCI, set out the criteria and process for identifying provincial heritage properties and set the standards for their protection, maintenance, use and disposal. The Standards and Guidelines require all provincial ministries and prescribed public bodies to identify, protect and care for provincial heritage properties they own and manage.
- The Metrolinx owned heritage areas of the Union Station Complex are subject to a Heritage Easement Agreement dated June 30, 2000 between The Toronto Terminals Railway Company Limited and the City of Toronto and a Collateral Agreement dated May 1, 2006 between Parks Canada (Approval Authority), City of Toronto and Greater Toronto Transit Authority (GO Transit now Metrolinx) which establishes the approval process for certain alterations.
  - The Collateral Agreement notes, "the City, GO Transit and the Approval Authority acknowledge that Union Station is an operating multi-modal transportation centre and, as such, the Station Complex is subjected to intensive use by a number of parties who require a clear understanding of the approval process within the Station Complex; and ... wish to agree on a process for how the approvals required under the Easement Agreement will be carried out when there is a request for approval to undertake or permit any demolition, construction, reconstruction, alteration, remodelling or any other thing or act that would materially affect the appearance or construction of the Heritage Elements so that the Approval Authority can effectively deal with requests for approvals" (2006, p.2).
- There are additional heritage recognitions and agreements that apply to varying extents, including:

- under the Heritage Railway Stations Protection Act a heritage railway station designation in 1989 with a heritage character statement in 1992, and statement of significance in 2007;
- under the Historic Sites and Monuments Act a national historic site designation in 1975 with a commemorative integrity statement in 2002, and statement of significance in 2006;
- under the Ontario Heritage Act a Part IV designation in 2005 with reasons for designation; and
- under the Ontario Heritage Act a Part V designation in 2006 with a heritage conservation district plan.

I hope this helps and feel free to let me know if you have any questions.

Kindly,

**Tamkin Naghshbandi**

T: 416.202.1804 | M: 416.271.1546

---

**From:** Schopf, Heidi <[heidy.schopf@woodplc.com](mailto:heidy.schopf@woodplc.com)>

**Sent:** Tuesday, July 13, 2021 4:32 PM

**To:** Dan Beare <[Dan.Beare@metrolinx.com](mailto:Dan.Beare@metrolinx.com)>; Tamkin Naghshbandi <[Tamkin.Naghshbandi@metrolinx.com](mailto:Tamkin.Naghshbandi@metrolinx.com)>

**Cc:** Fischer, Luke <[luke.fischer@woodplc.com](mailto:luke.fischer@woodplc.com)>; Dickinson, Chelsea <[chelsea.dickinson@woodplc.com](mailto:chelsea.dickinson@woodplc.com)>

**Subject:** Information Request: Union Station (65-71 Front Street West), Provincial Heritage Property of Provincial Significance

**EXTERNAL SENDER:** Do not click any links or open any attachments unless you trust the sender and know the content is safe.  
**EXPÉDITEUR EXTERNE:** Ne cliquez sur aucun lien et n'ouvrez aucune pièce jointe à moins qu'ils ne proviennent d'un expéditeur fiable, ou que vous ayez l'assurance que le contenu provient d'une source sûre.

Good afternoon,

Wood has been retained by the Toronto Transit Commission (TTC) to complete a Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment (CHR) and subsequent Heritage Impact Assessments (HIAs) to support the Waterfront East Light Rail Transit (WELRT) project in the City of Toronto, Ontario. To date, our work has identified that the Study Area contains a Metrolinx Provincial Heritage Property of Provincial Significance (Union Station, 65-71 Front Street West).

Wood has completed information gathering with the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI), the Ontario Heritage Trust, and the City of Toronto. Wood has also contacted Parks Canada to gather information on heritage properties within the Study Area that have federal heritage protection.

Presently, Wood is seeking background information from Metrolinx on Union Station (65-71 Front Street West). Wood respectfully requests that Metrolinx consider sharing the following background

documents with Wood:

- Statement of Cultural Heritage Value (SCHV) for Union Station (65-71 Front Street West) as approved by the Metrolinx Heritage Committee
- Previously completed cultural heritage reports related to the Union Station Complex. Reports that are of relevance to the TTC WELRT work include:
  - CHR, CHERs, or HIAs related to the Bay Street Bridge/Union Station Trainshed
  - CHR, CHERs, or HIAs related to work on the east elevation/Bay Street side of the Union Station building
- Direction on permits or approvals required for work proposed at Union Station (65-71 Front Street West)

Any information you can provide would be greatly appreciated.

Sincerely,

**Heidy Schopf, MES, CAHP** (she/her)

Built and Landscape Heritage Team Lead

Mobile: +1 (416) 518 0145

[www.woodplc.com](http://www.woodplc.com)

The logo for Wood Group PLC, featuring the word "wood." in a bold, lowercase, sans-serif font. The period is a solid dot.

---

This message is the property of John Wood Group PLC and/or its subsidiaries and/or affiliates and is intended only for the named recipient(s). Its contents (including any attachments) may be confidential, legally privileged or otherwise protected from disclosure by law. Unauthorized use, copying, distribution or disclosure of any of it may be unlawful and is strictly prohibited. We assume no responsibility to persons other than the intended named recipient(s) and do not accept liability for any errors or omissions which are a result of email transmission. If you have received this message in error, please notify us immediately by reply email to the sender and confirm that the original message and any attachments and copies have been destroyed and deleted from your system.

If you do not wish to receive future unsolicited commercial electronic messages from us, please forward this email to: [unsubscribe@woodplc.com](mailto:unsubscribe@woodplc.com) and include "Unsubscribe" in the subject line. If applicable, you will continue to receive invoices, project communications and similar factual, non-commercial electronic communications.



Please click <http://www.woodplc.com/email-disclaimer> for notices and company information in relation to emails originating in the UK, Italy or France.

As a recipient of an email from a John Wood Group Plc company, your contact information will be on our systems and we may hold other personal data about you such as identification information, CVs, financial information and information contained in correspondence. For more information on our privacy practices and your data protection rights, please see our privacy notice at <https://www.woodplc.com/policies/privacy-notice>

---

This e-mail is intended only for the person or entity to which it is addressed. If you received this in error, please contact the sender and delete all copies of the e-mail together with any attachments. **This e-mail is intended only for the person or entity to which it is addressed. If you received this in error, please contact the sender and delete all copies of the e-mail together with any attachments.**

# **Appendix E: Assessor Qualifications**

### **Heidy Schopf, MES, CAHP – Built and Landscape Heritage Team Lead**

Heidy Schopf is the Cultural Heritage Team Lead for WSP Canada Inc. She is a Senior Cultural Heritage Specialist and has worked in the field of cultural resource management since 2007. Ms. Schopf is a Professional Member of the Canadian Association of Heritage Professionals (CAHP).

Ms. Schopf has worked on hundreds of cultural heritage projects in Ontario, including Cultural Heritage Reports, Cultural Heritage Evaluation Reports (CHERs), Heritage Impact Assessments (HIAs), Strategic Conservation Plans (SCP), heritage documentation (photography, photogrammetry, and LiDAR), Heritage Conservation District (HCD) Studies and Plans, and heritage peer review. She regularly provides cultural heritage conservation guidance to public and private sector clients. Heidy is a Senior Project Manager and has managed and delivered cultural heritage work under a variety of processes, including: Environmental Assessment Act, Planning Act, Transit Project Assessment Process (TPAP), and the Ontario Heritage Act. She has extensive and applied knowledge of Ministry of Citizenship and Multiculturalism (MCM) guidance documents for heritage properties.

Ms. Schopf has had the privilege of working with Indigenous Nations on several projects to gather Indigenous perspectives on cultural heritage and integrate this shared learning into WSP's heritage work.

### **Luke Fischer, MA, CAHP – Cultural Heritage Specialist**

Mr. Fischer is a Senior Archaeologist who has been working in the cultural heritage field since 2002. Mr. Fischer has successfully managed projects, field directed, analysed artifact assemblages, and authored reports for Stage 1 to 4 archaeological assessments. Mr. Fischer is knowledgeable in the legislative processes and policies pertinent to cultural resource management in Ontario and has completed archeological assessments for both public and private development proponents. Mr. Fischer has extensive experience completing archaeological investigations in support of large-scale corridor projects as well as smaller site-specific investigations with stringent timelines. Mr. Fischer's experience includes archaeological investigation in landscapes varying from the remote boreal forest and rural agricultural fields to deeply buried urban deposits. Mr. Fischer has experience facilitating Indigenous engagement, including coordination of field monitors. In addition to his main experience in Ontario Mr. Fischer has worked as an archaeologist on linear corridor studies in Alberta, British Columbia, and Illinois. Mr. Fischer also has experience acting as a Cultural Heritage Specialist conducting various types of studies relating to built heritage landscape. Mr. Fischer holds a Professional Archaeology License (P219) issued by the Ontario Ministry of Citizenship and Multiculturalism (MCM), is a member of

the Association of Professional Archaeologists (APA), the Canadian Association of Heritage Professionals (CAHP), and is MTO RAQs certified in Archaeology and Heritage.





## **Appendix F: Limitations**

## **Limitations**

1. The work performed in the preparation of the Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment (CHR) and the conclusions presented are subject to the following:
  - (a) The Standard Terms and Conditions which form a part of our Professional Services Contract;
  - (b) The Scope of Services;
  - (c) Time and Budgetary limitations as described in our Contract; and,
  - (d) The Limitations stated herein.
2. No other warranties or representations, either expressed or implied, are made as to the professional services provided under the terms of our Contract, or the conclusions presented.
3. The conclusions presented in the report are based, in part, on visual observations of the Study Area. Our conclusions cannot and are not extended to include those portions of the Study Area which were not reasonably available, in WSP Environment & Infrastructure's opinion, for direct observation.
4. The potential and protected cultural heritage resources encountered at the Study Area were assessed, within the limitations set out above, having due regard for applicable heritage regulations as of the date of the inspection.
5. Services including a background study and fieldwork were performed. WSP E&I Canada Limited's work, including archival studies and fieldwork, were completed in a professional manner and in accordance with the Ministry of Citizenship and Multiculturalism's guidelines. It is possible that Cultural heritage resources not visible from the public realm may be within, or adjacent to the Study Area.
6. The utilization of WSP E&I Canada Limited's services during the implementation of any further cultural heritage work recommended will allow WSP E&I Canada Limited to observe compliance with the conclusions and recommendations contained the CHR. WSP E&I Canada Limited's involvement will also allow for changes to be made as necessary to suit field conditions as they are encountered.
7. This report is for the sole use of the parties to whom it is addressed unless expressly stated otherwise in the report or contract. Any use which any third party makes of the report, in whole or in part, or any reliance thereon, or decisions made based on any information of conclusions in the report, is the sole responsibility of such third party. WSP E&I Canada Limited accepts no responsibility whatsoever for damages or loss of any nature or kind suffered by any such third party as a result of actions taken or not taken or decisions made in reliance on the report or anything set out therein.
8. This report is not to be given over to any third-party other than a governmental entity, for any purpose whatsoever without the written permission of WSP E&I Canada Limited, which shall not be unreasonably withheld.