
Section G.5:

Area A Heritage Impact Assessment, Union Station Heritage Conservation District

Note: This appendix refers to Area A as Focus Area 1 and to Area B as Focus Area 2, a reflection of previous project nomenclature.



REP-061- WATERFRONT EAST LRT UNION STATION - QUEENS QUAY LINK

**Heritage Impact Assessment, Union Station Heritage
Conservation District, City of Toronto, Ontario**

Project # OISO52004

Prepared for:

Toronto Transit Commission
1900 Yonge Street, Toronto, ON M4S 1Z2

OISO52004-TEM-001 R0 Waterfront East
LRT Union Station - Queens Quay Link
Concept Design Review Submission



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Executive Summary

The Toronto Transit Commission (TTC) is undertaking Preliminary Design and Engineering (PDE) to produce a Baseline Design (approximately 30% design completion) of a new TTC Waterfront East Light Rail Transit (WELRT) system (the Project). The WELRT, goes under Bay Street, from Front Street to Queens Quay West, and then heads easterly on Queens Quay West. A new portal on Queens Quay West between Bay Street and Yonge Street will be constructed to provide a transition from an underground Light Rail Transit (LRT) to an at-grade LRT, which will complement the existing west portal and at-grade LRT on Queens Quay West, west of Bay Street.

The WELRT will service Toronto's waterfront revitalization area by providing fast, reliable transit service in the East Bayfront (EBF) Area of the Waterfront. The expansion of the Union LRT and Queens Quay LRT Stations is required to accommodate the additional streetcar lines and passenger volume. This project is critical to the new waterfront transit plan in the EBF Precinct.

WSP was retained by TTC to complete a Heritage Impact Assessment (HIA) for the Union Station Heritage Conservation District (HCD) in support of the WELRT. This HIA represents one deliverable to support program delivery. This HIA is being carried out under the Transit Project Assessment Process (TPAP) and was prepared in accordance with the MCM guidance document titled *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties* (MCM 2017).

The purpose of this HIA is to establish the Statement of Cultural Heritage Value for the Union Station, assess the existing conditions of the property, describe the purpose of the proposed activity, complete an impact assessment and outline considered alternatives and mitigation measures, provide a summary of community engagement, and development recommendations for the conservation of the property.

Based on the above, the following recommendations are made:

- 1) Street level/open cut excavation along Bay Street from Front Street West to Harbour Street within the Union Station HCD will directly impact the Bay Street right-of-way and the public realm immediately adjacent to it. Design guidelines within Section 8.3.5 of the [Union Station HCD Plan](#) should be adhered to when re-establishing the impacted public realm. Relevant design guidelines from the HCD Plan include:
 - a. Replacement lighting should be designed in a manner that accentuates or does not diminish the integrity of the heritage architecture within the HCD. Examples of successful lighting currently within the HCD should be considered in the design of replacement lighting. Replacement fixtures should be consistent throughout the Study Area.
 - b. Walking surface treatments should be consistent within the Study Area. The design that is currently most prominent within the HCD includes large concrete pavers flanked by smaller stone pavers lining the street edge and edge of structures. This should be emulated consistently throughout the



Study Area. Interruptions to the cohesiveness of the walking surface in the form of high-quality finishing such as granite and tree planters are acceptable interruptions to the cohesive streetscape that illustrate the significance of the HCD.

- c. Clutter should be limited throughout the HCD. Examples of successful street furniture currently within the HCD should be considered in the design of replacement street furniture. Replacement street furniture should be consistent throughout the Study Area. The street furniture should be composed of high-quality material and should be as inconspicuous and multi-use as possible to reduce clutter and diminishment of the surrounding of the heritage quality of the HCD.
 - d. The design and finishes of the teamway along the west side of Bay Street should be consistent with either the teamway on the east side of Bay Street or the teamway on the east side of York Street. Both areas are examples of finished teamways in good repair and can be used as design references to select the finishes, surface materials, and layout of the teamway on the west side of Bay Street.
- 2) This report should be submitted to the following agencies for review and comment:
- a. City of Toronto Heritage Preservation Services
 - b. MCM

The above recommendations were prepared using the best available information regarding potential impacts at the time of writing. Should the proposed work change, then the preliminary impact assessment should be revisited to confirm identified impacts and proposed mitigation measures.



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List of Acronyms and Abbreviations

BHR	Built Heritage Resource
CHL	Cultural Heritage Landscape
CHR	Cultural Heritage Report: Existing Conditions and Impact Assessment
CHVI	Cultural Heritage Value or Interest
HCD	Heritage Conservation District
HIA	Heritage Impact Assessment
MCM	Ministry of Citizenship and Multiculturalism
O. Reg.	Ontario Regulation
PPS	Provincial Policy Statement
TTC	Toronto Transit Commission

Glossary

Adjacent lands	Those lands contiguous to a protected heritage property or as otherwise defined in the municipal official plan (Government of Ontario 2020).
Built Heritage Resource	Means a building, structure, monument, installation or any manufactured or constructed part or remnant that contributes to a property's cultural heritage value or interest as identified by a community, including an Indigenous community. Built heritage resources are located on property that may be designated under Parts IV or V of the <i>Ontario Heritage Act</i> , or that may be included on local, provincial, federal and/or international registers (Government of Ontario 2020).
Conserved	Means the identification, protection, management and use of built heritage resources, cultural heritage landscapes and archaeological resources in a manner that ensures their cultural heritage value or interest is retained. This may be achieved by the implementation of recommendations set out in a conservation plan, archaeological assessment, and/or heritage impact assessment that has been approved, accepted or adopted by the relevant planning authority and/or decision maker. Mitigative measures and/or alternative development approaches can be included in these plans and assessments (Government of Ontario 2020).
Cultural Heritage Landscape	Means a defined geographical area that may have been modified by human activity and is identified as having cultural heritage value or interest by a community, including an



Glossary

	Indigenous community. The area may include features such as buildings, structures, spaces, views, archaeological sites or natural elements that are valued together for their interrelationship, meaning or association. Cultural heritage landscapes may be properties that have been determined to have cultural heritage value or interest under the <i>Ontario Heritage Act</i> , or have been included on federal and/or international registers, and/or protected through official plan, zoning by-law, or other land use planning mechanisms (Government of Ontario 2020).
Heritage Attributes	Means the principal features or elements that contribute to a protected heritage property's cultural heritage value or interest, and may include the property's built, constructed, or manufactured elements, as well as natural landforms, vegetation, water features, and its visual setting (e.g. significant views or vistas to or from a protected heritage property) (Government of Ontario 2020).
Protected Heritage Property	Means property designated under Parts IV, V or VI of the <i>Ontario Heritage Act</i> ; property subject to a heritage conservation easement under Parts II or IV of the <i>Ontario Heritage Act</i> ; property identified by the Province and prescribed public bodies as provincial heritage property under the Standards and Guidelines for Conservation of Provincial Heritage Properties; property protected under federal legislation, and UNESCO World Heritage Sites (Government of Ontario 2020).
Provincial Heritage Property	Means real property, including buildings and structures on the property, that has cultural heritage value or interest and that is owned by the Crown in right of Ontario or by a prescribed body; or that is occupied by a ministry or prescribed body if the terms of the occupancy agreement are such that the ministry or public body is entitled to make the alterations to the property that may be required under these heritage standards and guidelines (MCM 2017).
Provincial Heritage Property of Provincial Significance	Means provincial heritage property that has been evaluated using the criteria found in Ontario Heritage Act O. Reg. 10/06 and has been found to have cultural heritage value or interest of provincial significance (MCM 2017).
Significant	In regard to cultural heritage and archaeology, resources that have been determined to have cultural heritage value or interest. Processes and criteria for determining cultural heritage value or interest are established by the Province under the authority of the <i>Ontario Heritage Act</i> (Government of Ontario 2020).



Glossary

Statement of Cultural Heritage Value	Means a concise statement explaining why a property is of heritage interest; this statement should reflect one or more of the criteria found in <i>Ontario Heritage Act</i> O. Regs. 9/06 and 10/06 (MCM 2017).
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1.0 Introduction

1.1 Description and Location of the Property

WSP E&I Canada Limited (WSP) (formerly Wood Environment & Infrastructure) was retained by the Toronto Transit Commission (TTC) to conduct a Heritage Impact Assessment (HIA) for the Union Station Heritage Conservation District (HCD) (the Study Area) as part of Preliminary Design and Engineering (PDE) to produce a Baseline Design (approximately 30% design completion) of a new TTC Waterfront East Light Rail Transit (WELRT) system (the Project). The Study Area is depicted in **Figure 1** and **Figure 2**.

The Union Station Heritage Conservation District (HCD) was designated in 2006 through [By-law 634-2006](#) (City of Toronto 2006; Ontario Heritage Trust 2020). The Union Station HCD is located between Toronto's Financial District, Entertainment District, historical St. Lawrence Neighborhood, and the post-industrial waterfront. The Union Station HCD Plan is available [online](#) and includes an in depth analysis of the history, heritage character, district policies, municipal policies, implementation guidance, and design guidelines (ERA Architects Inc 2006).

The boundaries of the Union Station HCD coincide with Wellington Street West to the north, Yonge Street to the east, and Lakeshore Boulevard West/Harbour Street to the south. The east boundary of the HCD is defined by Simcoe Street north of the rail corridor and Reese Street south of the rail corridor. The Union Station HCD consists of an assemblage of buildings, open spaces and streets that have a collective interdependent history. The intersecting development and planning initiatives associated with Toronto's railway lands, waterfront and central business district, historic and monumental architecture; as well as physical patterns of interrelated function are key heritage components that comprise the heritage character of the HCD (ERA Architects Inc. 2006). Every property within the Union Station HCD is designed under Part V of the Ontario Heritage Act and is subject to the guidelines contained HCD plan, including the public realm.

1.1.1 Heritage Recognition

The Union Station HCD has known heritage value and is recognized at the municipal level. Existing protections include:

- Municipal Recognition
 - Designated under Part V of the *Ontario Heritage Act* under By-Law 634-2006
- Provincial Recognition
 - The Union Station HCD does not have provincial recognition. However, individual properties within the HCD have provincial recognition, including the Union Station Complex (65-71 Front Street West; Metrolinx Provincial Heritage Property of Provincial Significance). An HIA for this property has been prepared under a separate cover.



- Federal
 - The Union Station HCD does not have federal recognition. However, individual properties within the HCD have federal recognition, including the Union Station Complex (65-71 Front Street West; National Historic Site). An HIA for this property has been prepared in a separate cover.

1.1.2 Property Ownership

The Union Station HCD consists of a conglomerate of built heritage resources owned by many different parties. This report assesses the public realm within the HCD that is owned by the City of Toronto.

1.2 Proposed Undertaking and Purpose

The TTC is undertaking Preliminary Design and Engineering (PDE) to produce a Baseline Design (approximately 30% design completion) of a new TTC WELRT system (the Project). The WELRT, goes under Bay Street, from Front Street to Queens Quay West, and then heads easterly on Queens Quay West. A new portal on Queens Quay West between Bay Street and Yonge Street will be constructed to provide a transition from an underground Light Rail Transit (LRT) to an at-grade LRT, which will complement the existing west portal and at-grade LRT on Queens Quay West, west of Bay Street.

The WELRT will service Toronto's waterfront revitalization area by providing fast, reliable transit service in the East Bayfront (EBF) Area of the Waterfront (**Plate 1**). The expansion of the Union LRT and Queens Quay LRT Stations is required to accommodate the additional streetcar lines and passenger volume. This project is critical to the new waterfront transit plan in the EBF Precinct.



Plate 1: Overview of the Waterfront Transit Network

Please reach out to the Project Team should you require alternative text for this image.



The planning for the Project began in 2010 when the East Bayfront Transit Class Environmental Assessment (EA) Study was carried by Waterfront Toronto, who is the proponent for all redevelopment activities in the East Bayfront Area. The Consultant, MRC, produced the draft Environmental Study Report in August 2009. The Engineering Department of TTC carried out the Conceptual 10% Design of the project, producing a final Conceptual Design Report in February 2010. The project has progressed to the current Preliminary Design and Engineering stage.

The overall scope of work to be completed for the Project includes, but is not limited to, Focus Area 1 and Focus Area 2. An overview of these Project Areas is provided in **Plate 2**.

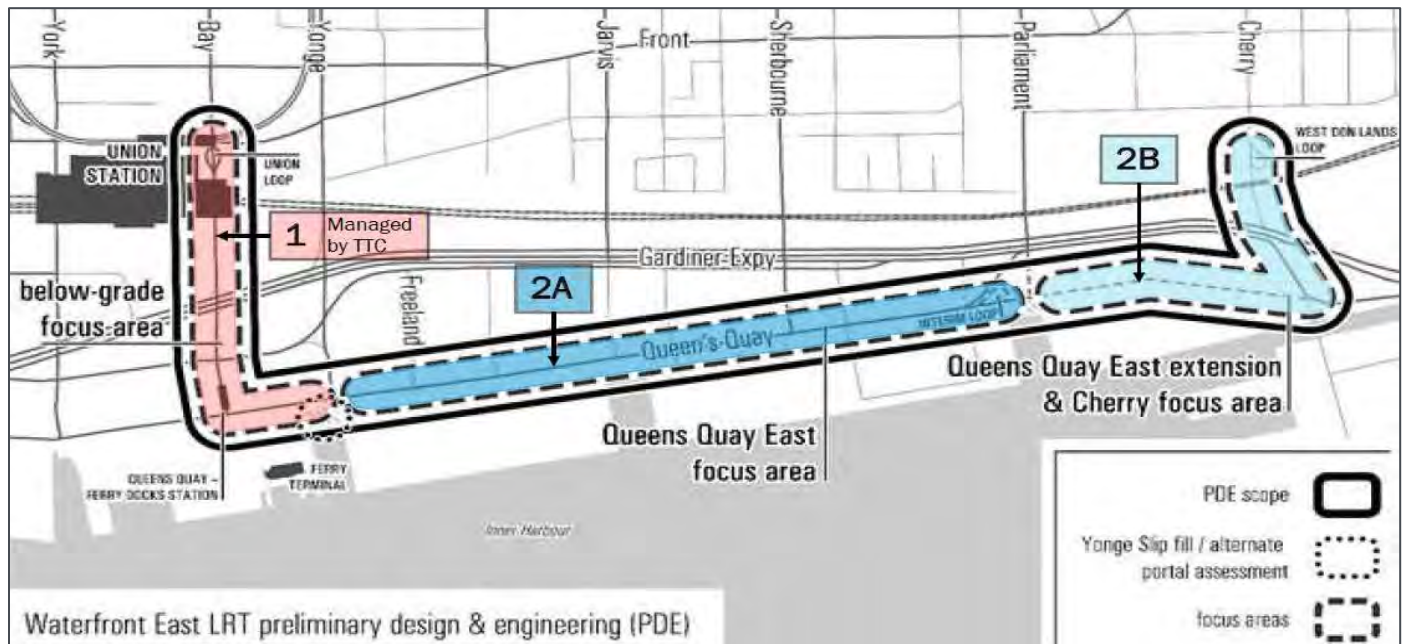


Plate 2: Focus Area 1 and Focus Area 2

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1. Focus Area 1 - Managed by TTC - Below Grade (Union Station Loop to future Portal east of Bay Street on Queens Quay), which includes:
 - a. Union LRT Station Expansion, including new crossover tracks; Queen Quay LRT Station Expansion;
 - b. New Streetcar tunnel and portal structures along Queens Quay between Bay Street and Yonge Street; and
 - c. Track works within the tunnel and portal structures.
2. Focus Area 2 - Managed by Waterfront Toronto:
 - a. 2A: Queens Quay East (Future Portal to Parliament vicinity ancillary Queens Quay surface/public realm between Bay & future portal).



- b. 2B (Provisional): Queens Quay East Extension & Cherry (Parliament vicinity to West Don Lands Loop).

WSP E&I Canada Limited, (WSP) scope of work pertains to Focus Area 1 only and includes a collaborative effort among the City of Toronto, the TTC, and Waterfront Toronto. WSP's overall scope of work includes Preliminary Design and Engineering (PDE) services to provide a baseline design (30% design), a level 3 cost estimate for the expansion of the existing Union LRT and Queens Quay LRT Stations, and new running tunnel and portal as part of WELRT project. In particular, the main scope items include:

1. Union Station LRT Loop Expansion to accommodate up to four (4) new platforms, including new crossover tracks;
2. Queens Quay Station Expansion with up to two (2) extended platforms;
3. New streetcar tunnel and portal structures along Queens Quay between Bay Street and Yonge Street;
4. Track works within the tunnel and portal structures; and
5. Design interface and coordination with the work of Focus Area 2 and adjacent projects (public and private) along project limits.

Subject to further funding approval and a procurement options analysis, a contract amendment may be issued to extend the term of the contract and the consultant may be requested to carry out the detailed design and construction support services or develop Reference Concept Design (RCD) and Project Specific Output Specifications (PSOS) for this project.

The phases of WSP's overall scope of work are as follows:

6. Phase 1 – Work Plan (OISO52004-PLN-001 Phase 2 Work Plan);
7. Phase 2a - Concept Design Review Submission (CDRS) (approximately 15%); and,
8. Phase 2b - Baseline Design Review Submission (BDRS) (approximately 30%).

From the layouts indicated in Appendix B of the *City of Toronto, Union Station - Queens Quay Transit Link Study Final Report* (April 2019), further developments were made to progress the design to consider the latest requirements and current standards. A summary of the current station layouts are provided in **Plate 3** and **Plate 4**.



UNION STATION

1. Free standing columns rather than a continuous wall down the center of the Station greatly improves the visibility from one side of the Station to the other and assists the passengers with Wayfinding as they make their way around the Loop from the East Platform to the West Platform.
2. Because of the need for the existing structure, supporting the East end of Union Station, to remain in place, we are forced to introduce this "U" shaped interruption in the Loop. There are very costly alternatives to avoid the interruption.
3. Access to Union Station at this location has yet to be fully resolved
4. Access to Union Station at this location has yet to be fully resolved

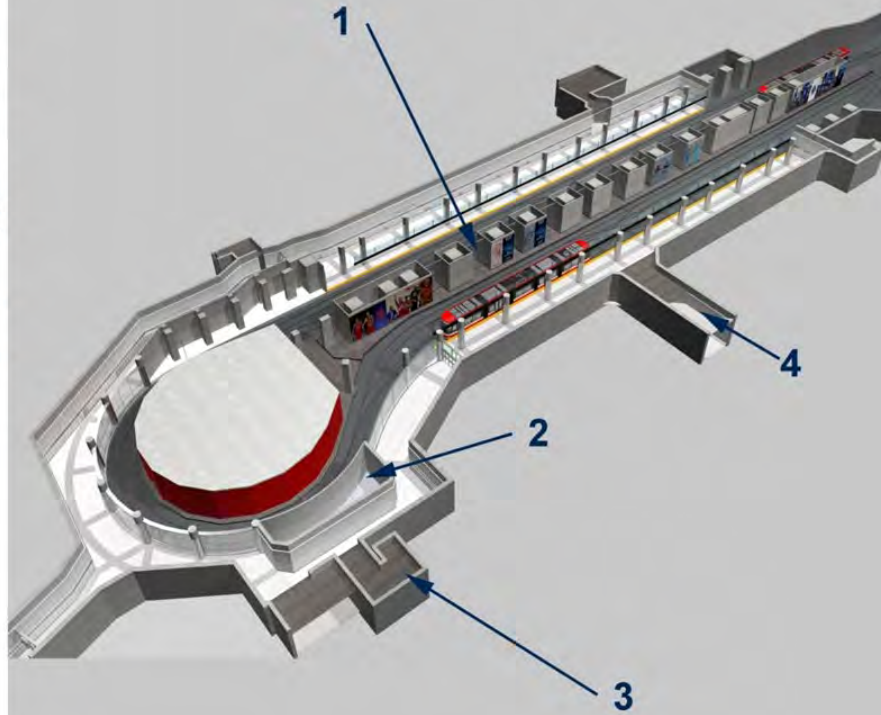


Plate 3: Union Station -CDRS Proposed Configuration

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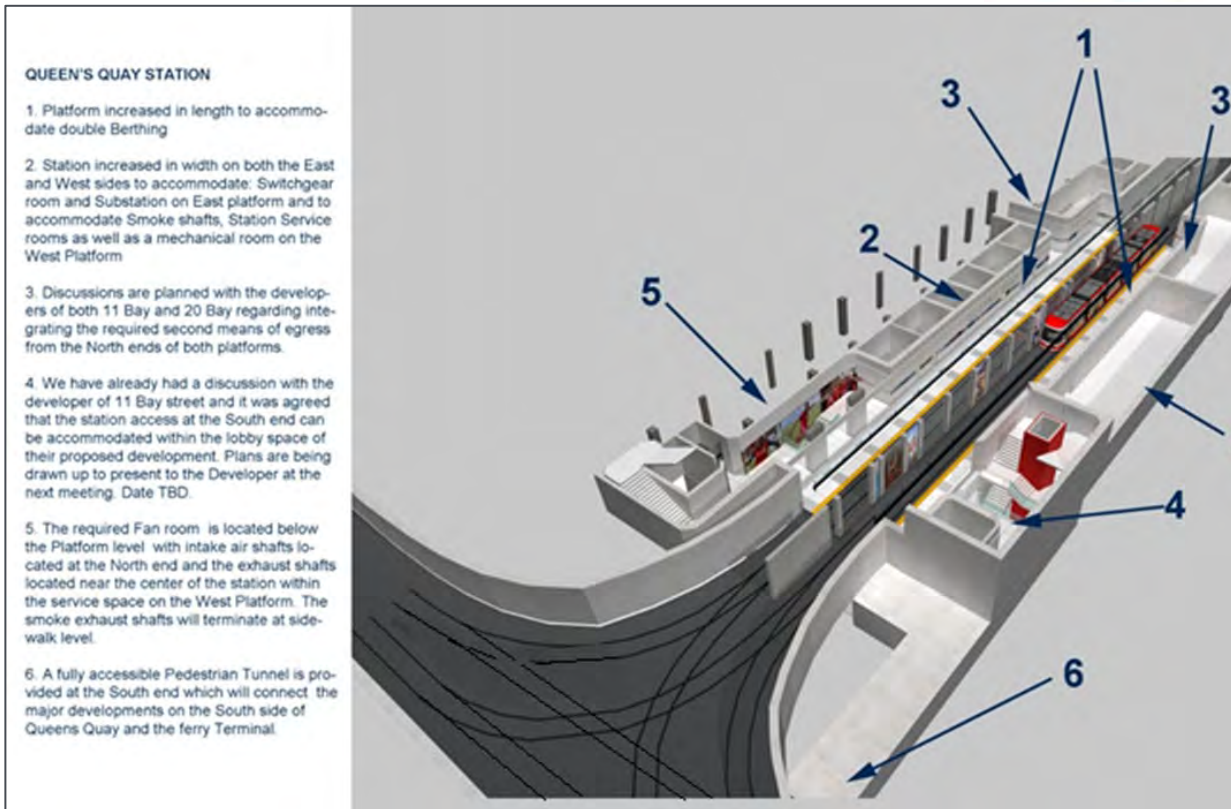


Plate 4: Queens Quay Station - CDRS Proposed Configuration

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1.3 Potential Impacts to Cultural Heritage Resources

As described above, the project consists of the design of a new TTC WELRT system that goes under Bay Street, from Front Street to Queens Quay West, and then heads easterly on Queens Quay West. A new portal on Queens Quay West between Bay Street and Yonge Street will be constructed to provide a transition from an underground LRT to an at-grade LRT, which will complement the existing west portal and at-grade LRT on Queens Quay West, west of Bay Street.

The proposed WELRT is located within the Union Station HCD and adjacent to, or in close proximity to, an additional 13 known and potential cultural heritage properties. A *Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment* (CHR) prepared for this project determined that direct impacts are anticipated to Union Station (65-71 Front Street West), the Dominion Public Building (1 Front Street West), the Postal Delivery Building (40 Bay Street), and the public realm of the Union Station HCD. Presently, Strategic Conservation Plans (SCP) are not available for these properties.

Given that there is proposed work within these heritage properties and SCPs are not available, HIAs are required. The purpose of the HIAs is to inform decisions that may affect the property while ensuring the conservation and protection of heritage attributes.



This report consists of the HIA for the Union Station HCD. HIAs have been prepared for the Union Station Complex (65-71 Front Street West), Dominion Public Building (1 Front Street West), and the Postal Delivery Building (40 Bay Street) under separate covers. All four HIAs has been prepared in accordance with *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties* (MCM 2017).

1.4 Overview of Impacts and Mitigation Measures for the Dominion Public Building

A detailed impact assessment was prepared as part of this HIA. The impact assessment is presented in Section 5.0 and mitigation measures are presented in Section 6.0. Drawings of the proposed work are provided in **Appendix B**. An overview of the findings is presented below.

1.4.1 Summary of Potential Impacts

The proposed work within the Union Station HCD consists of the installation of the new WELRT streetcar loop and associated platform. This includes the installation of major infrastructure including the expansion of the current subway tunnel to be deeper and the installation of a new platform with access to the TTC Union Station Subway Station. Potential impacts to the Union Station HCD include:

- Open cut excavation to install the WELRT streetcar loop along Bay Street and Front Street West involving demolition of existing infrastructure, property acquisition, and construction of new loop, including the associated platform and connections to existing transit infrastructure. Post-construction landscaping is proposed to restore the public realm.

1.4.2 Summary of Recommended Mitigation Measures

Mitigation Measures are proposed to address impacts resulting from the proposed work within the Union Station HCD public realm. These include:

- 1) Street level/open cut excavation along Bay Street from Front Street West to Harbour Street within the Union Station HCD will directly impact the Bay Street right-of-way and the public realm immediately adjacent to it. Design guidelines within Section 8.3.5 of the [Union Station HCD Plan](#) should be adhered to when re-establishing the impacted public realm; and,
- 2) This report should be submitted to the following agencies for review and comment:
 - a. City of Toronto Heritage Preservation Services
 - b. MCM

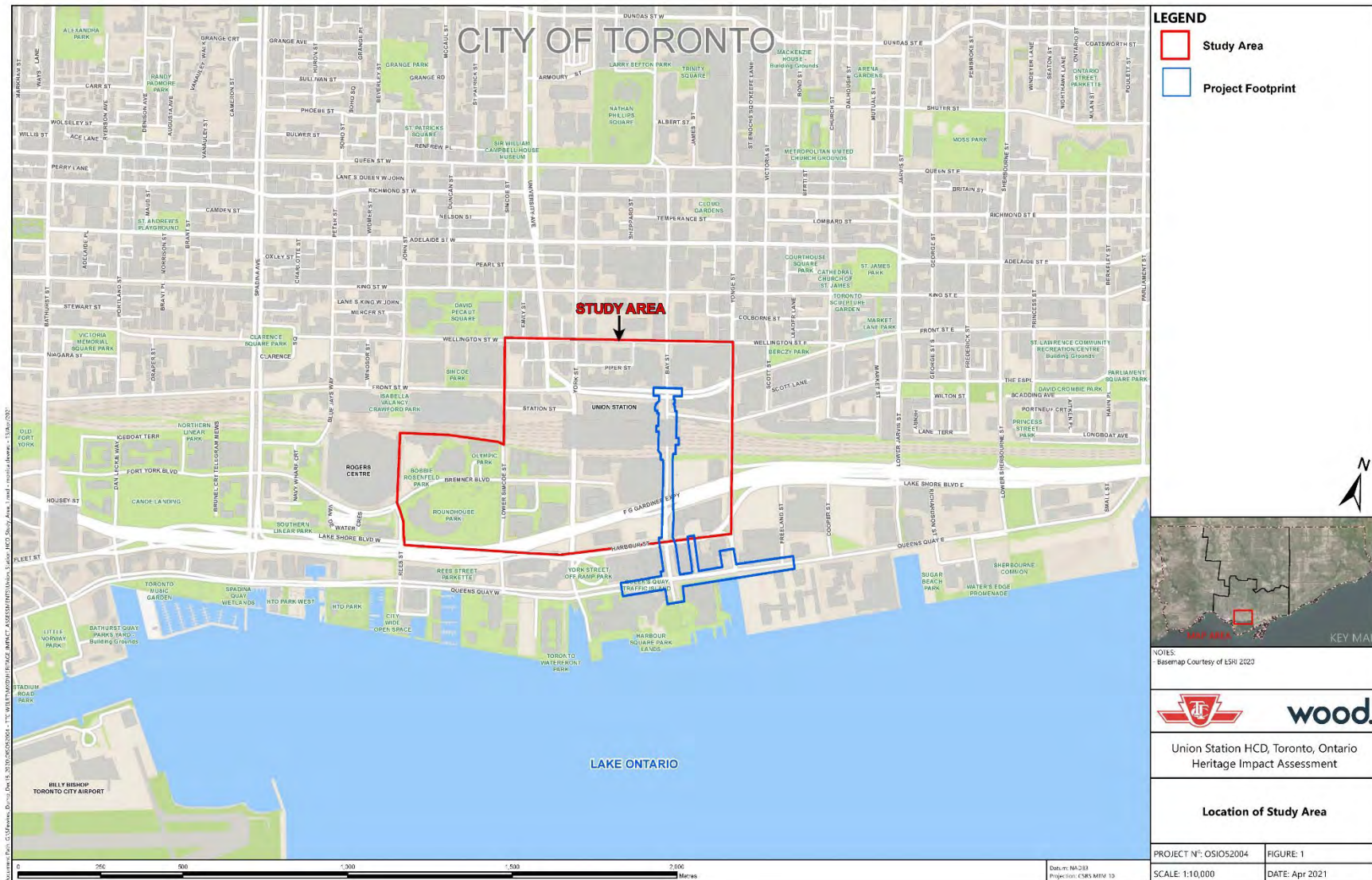


Figure 1: Location of the Study Area

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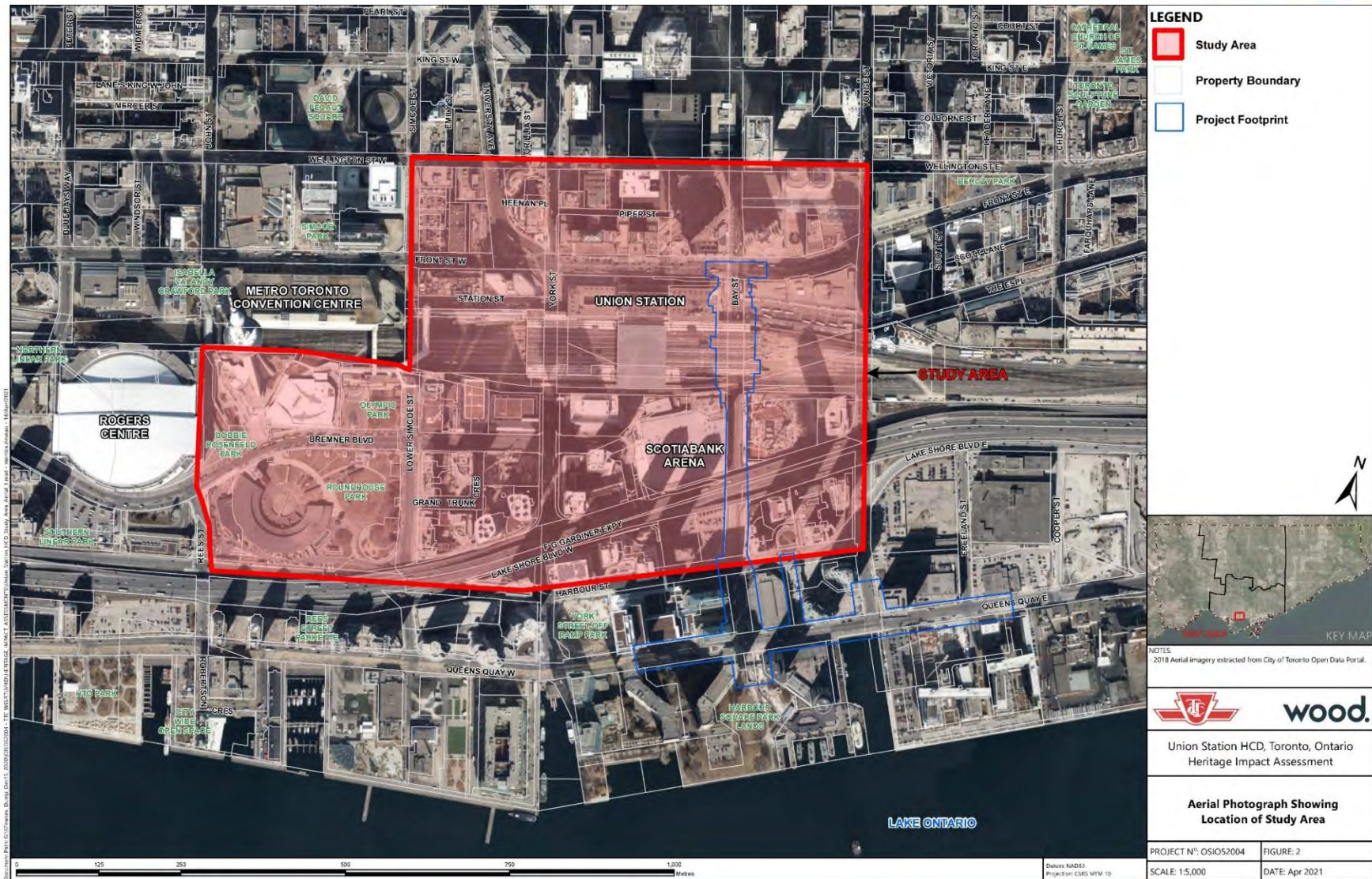


Figure 2: Aerial Photograph Showing Location of the Study Area
Please reach out to the Project Team should you require alternative text for this image.



2.0 Statement of Cultural Heritage Value

The Union Station Heritage Conservation District (HCD) was designated in 2006 through [By-law 634-2006](#) (City of Toronto 2006; Ontario Heritage Trust 2020). The Union Station HCD is located between Toronto's Financial District, Entertainment District, historical St. Lawrence Neighborhood, and the post-industrial waterfront.

2.1 Union Station Heritage Conservation District

2.1.1 Description of Property

The Union Station Heritage Conservation District (HCD) was designated in 2006 through [By-law 634-2006](#) (City of Toronto 2006; Ontario Heritage Trust 2020). The Union Station HCD is located between Toronto's Financial District, Entertainment District, historical St. Lawrence Neighborhood, and the post-industrial waterfront. The Union Station HCD Plan is available [online](#) and includes an in depth analysis of the history, heritage character, district policies, municipal policies, implementation guidance, and design guidelines (ERA Architects Inc 2006).

The boundaries of the Union Station HCD coincide with Wellington Street West to the north, Yonge Street to the east, and Lakeshore Boulevard West/Harbour Street to the south. The east boundary of the HCD is defined by Simcoe Street north of the rail corridor and Reese Street south of the rail corridor. The Union Station HCD consists of an assemblage of buildings, open spaces and streets that have a collective interdependent history. Intersecting development and planning initiatives associated with Toronto's railway lands, waterfront and central business district, historic and monumental architecture; as well as physical patterns of interrelated function are the key heritage attributes that comprise the heritage character of the HCD (ERA Architects Inc. 2006).

The Union Station HCD Plan contains design guidelines for contributing buildings, non-contributing buildings, new construction, adjacent properties, and the public realm (ERA Architects Inc. 2006: 57-65).

The HCD Plan describes contributing buildings as properties that contribute to the character of the district and/or are historically, architecturally or culturally significant as identified in the heritage evaluation or determined by further evaluation. Non-contributing buildings are defined as properties that do not contribute to the character of the district and/or are not historically, architecturally or culturally significant as identified in the heritage evaluation or determined by further evaluation (ERA Architects Inc. 2006: 58).

2.1.2 Statement of Cultural Heritage Value

The heritage character of the Union Station District illustrates several periods of development. The architectural legacies and development patterns underline the prominence of Union Station as a node of urban activity.

Since the opening of the station, the district has remained a focus of pedestrian activity in downtown Toronto. Different phases of development have resulted in varied streetscapes. These open space patterns describe the district's historical relationship to



adjacent downtown districts and its important role as a multimodal transportation hub. Today the district's significant public space provides an opportunity to celebrate its important historical identity.

A strong Beaux-Arts presence around Union Station creates one of the most stylistically cohesive areas in the City of Toronto. The civic-minded architecture speaks strongly to the prominence of Union Station as a centre of urban activity. As a transportation hub linked to the TTC and the PATH system, Union Station has catalyzed the development of some of the largest examples of modern architecture and urban design in the world.

Post-war office towers such as BCE Place and modernist developments like the CN tower represent a distinct shift in built form. The John Street Roundhouse and other red brick industrial buildings are interspersed throughout the district and act as reminders of an era in which the district played a substantially different role within the city. Many architectural eras and styles coexist within the Union Station Heritage Conservation District. One does not predominate – yet they are unified in their monumentality.

(ERA Architects Inc. 2006: 56)

2.1.3 Heritage Attributes

The Union Station HCD Plan describes contributing buildings and non-contributing buildings as well as view sheds associated with the HCD. However, there is no comprehensive list of heritage attributes within the HCD included in [By-law 634-2006](#) or in the HCD Plan (City of Toronto 2006; Ontario Heritage Trust 2020).

2.1.4 Union Station HCD Guidelines for Contributing Buildings

Design guidelines for contributing heritage buildings in the Union station HCD are as follows:

Additions and Alterations: Additions and alterations may be approved, depending on their impact within the district. Additions that are not prominently visible – especially from Union Station, will generally be approved. Those that are visible will be evaluated on a case-by-case basis. Additions will be evaluated by the following criteria:

- The new structure respects the general size, shape and scale of features associated with the property of district.
- The site plan respects the general site characteristics associated with the property or district.
- The design respects the general historic and architectural characteristics associated with the property or district.
- The material choice respects the existing character of the property and district as a whole. Material choice not directly emulating what exists will be contextual and appropriate.
- Any addition is to be connected to the property in a way that does not alter, change, obscure, damage, or destroy any significant building features.



- Additions, renovations and alterations that enhance the character of the district, and are compatible with the overall planning goals of the district will be encouraged, yet subject to thorough review.

Demolition: The demolition involving any contributing building in the district will only be approved after thorough review in accordance with the *Ontario Heritage Act*.

(ERA Architects Inc. 2006: 60)

2.1.4.1 Guidelines for the Public Realm

Section 8.3.5 of the Union Station HCD Plan provides direction for the development of the public realm within the district. As outlined in the HCD Plan, the public realm provides a stage for the daily life of the city, comprising gathering spaces such as parks, public squares, streets, path networks, and the interior of malls. The design and functional aspects of the public realm, such as sidewalks, streetscapes, and boulevards, provide public spaces with both form and a sense of place (ERA Architects Inc. 2006: 64-65). The HCD Plan notes that enhancing connectivity and maintaining open spaces through public realm can promote the heritage character of the HCD. Similarly, the consolidation of and simplification of streetscape elements in the district, including paving, curbs, tree grates, signage, base plants, vertical elements, and lighting has a strong impact on the general quality and understanding of the public realm.

The design guidelines for the public realm of the Union Station HCD are as follows:

Promote Heritage Character: All aspects of the public realm need to recognize the heritage character of the district. Lighting should be used to emphasize building forms at night, in a manner representative of the grandeur of the architecture. Historical precedents in planning around Union Station, such as the John Lyle Plan of 1911, should be used to inform the importance of establishing connections to the south of the district. Historical connections, both visual and physical, should be maintained and enhanced, such as the view of the Royal York Hotel and the physical connections between the John Street Roundhouse and Union Station. The moats, teamways, and bridges of Union Station should be better utilized as important linkages between areas of the district.

Aspire to Highest Standards of Design Excellence: Streetscape elements should be of high-quality design and enduring materials that are appropriate to the district's historic character. The approach to design and materials used should reflect good contemporary design to emphasize the district's evolving character. Designs using inauthentic historical pastiche, for example mock-Victorian, should be avoided because they look backwards, and when executed using modern techniques do not have the quality of craft of the original.

Streetscape furniture should fit into the landscape rather than be its focus: Street furniture should have high standards of functionality, durability, environmental performance, and visual attractiveness. Materials



such as stainless steel should be used because they wear well and do not require continual maintenance. Pedestrian comfort should be encouraged by considering and supporting pedestrian flow, needs of elderly, visually impaired, etc.

Maintain open spaces: Open spaces, such as Roundhouse Park, serve critical functions by helping to maintain the environmental quality of the district, in addition to providing a calm gathering space. Open spaces should be properly protected and maintained.

Reduce clutter: In order to reduce clutter on the streetscape, the size and number of objects like waste and recycling receptacles and newspaper boxes should be reduced, for example, the three-unit garbage receptacle should be reconsidered as three separate components. Consolidated newspaper boxes should be used to replace the banks of 10-20 boxes chained together.

Coordinate Design Implementation and Maintenance: The overall design of the public realm should be carried out in a consistent and well-coordinated manner to ensure that design measures complement each other and work towards enhancing the district's identity.

(ERA Architects Inc. 2006: 64-65)



3.0 Assessment of Existing Conditions

3.1 Union Station HCD

The Union Station HCD is a defined area that is designated under Part V of the *Ontario Heritage Act* that contains of buildings, open spaces, streets, rail infrastructure, and pedestrian circulation infrastructure that have an eclectic, yet intersecting history. Many development and planning initiatives associated with Toronto's railway lands, waterfront, central business district, and historic monumental architecture form physical patterns of form and function characterizing the heritage significance of the HCD.

The boundaries of the Union Station HCD coincide with Wellington Street West to the north, Yonge Street to the east, and Lakeshore Boulevard West/Harbour Street to the south. The east boundary of the HCD is defined by Simcoe Street north of the rail corridor and Reese Street south of the rail corridor. (ERA Architects Inc. 2006).

Heritage properties adjacent to the HCD include:

- The Saint Lawrence HCD (currently under appeal), By-Law 1328-2015. This HCD is located one block east of the Union Station HCD along Scott Street.
- The King Spadina HCD (currently under appeal). This HCD is located one block north of the Union Station HCD at King Street West.
- 120 Wellington Street West, this address includes the façade formerly known as 74-76 York Street, By-Law 1063-2009
- 66 Wellington Street West, this address includes the Toronto-Dominion Bank Tower (1967), Banking Pavilion (1968), and the Royal Trust Tower (1969), By-Law 871-2003
- 222 Bay Street, this address includes the Former Toronto Stock Exchange (1937), By-Law 570-78
- 187 Bay Street and 18 Wellington Street, this address includes Commerce Court (1968-72), By-Law 467-91, 539-91
- 56 Yonge Street, his address includes Hotel Mossop (1907-08), adopted by City Council on 05 March 1984
- 49 Yonge Street, this address includes the Bank of British North America, now the Canadian Imperial Bank of Commerce (1872-73), By-Law 53-80
- 3 Wellington, this address includes the BCE Warehouse, By-Law 602-83
- 1 Front Street East, this property includes the O'Keefe Centre (1959-60), By-Law 1156-2008

Heritage Resources located within the HCD include buildings and infrastructure relating to a wide variety of historical periods and representing an eclectic mix of architectural styles and social influences. The WELRT Study Area is located in the eastern portion of the HCD, which is characterized most prominently by monumental Beaux arts architecture including the Union Station Complex (65-71 Front Street West) and the Dominion Public Building (1 Front Street West). The Study Area runs north-south from the corner of Front Street West and Bay Street through the south end of the HCD. This Study Area is situated within or adjacent to properties containing modern skyscraper



architecture, 20th century Art Deco and Arts Moderne motifs as well as monumental Beaux Arts streetscape. The architecture contributing to the area character of HCD within or adjacent to the WELRT Study Area within the Union Station HCD is accompanied by modern buildings that are identified as non-contributing properties.

Heritage resources within the HCD and within or adjacent to the Study Area include:

- Union Station (65-71 Front Street) (Designated under Part IV and V of the *Ontario Heritage Act*, Provincial Heritage Property of Provincial Significance, designated as a National Historic Site under the *Historic Sites and Monuments Act*, and a Heritage Railway Station under the *Heritage Railway Stations Protection Act*)
- Dominion Public Building (1 Front Street West) (Designated under Part IV and V of the *Ontario Heritage Act*)
- Brookfield Place (161-181 Bay Street; Designated under Part V of the *Ontario Heritage Act* as a contributing property in the Union Station HCD)
- Royal Bank Plaza (200 Bay Street; Designated under Part V of the *Ontario Heritage Act* as a contributing building in the Union Station HCD)
- Building under construction (18 Yonge Street), Designated under Part V of the *Ontario Heritage Act* as a non-contributing part in the Union Station HCD)
- Commercial Building (31, 35, 39 Bay Street), Designated under Part V of the *Ontario Heritage Act* as a non-contributing part in the Union Station HCD)

Properties within the Union Station HCD that fall within the WELRT Study Area are depicted in **Figure 3**.



Plate 5: View of Brookfield Place located at the northeast corner of Bay Street and Front Street West at 161-181 Bay Street, facing north.



Plate 6: View of Royal Bank Plaza located on the northwest corner of Bay Street and Front Street West at 200 Bay Street, facing north.



Plate 7: View of the Union Station Complex located at 65-71 Front Street West between Bay Street and York Street, facing southeast.



Plate 8: View of the Dominion Public Building located at 1 Front Street West on the northwest corner of Bay Street and Yonge Street, facing southwest.



Plate 9: View of Scotiabank Arena, formerly the Postal Delivery Building located 40 Bay Street, facing northwest.



Plate 10: View of the Harbour Commission Building located at 1 Harbour Square, facing north.

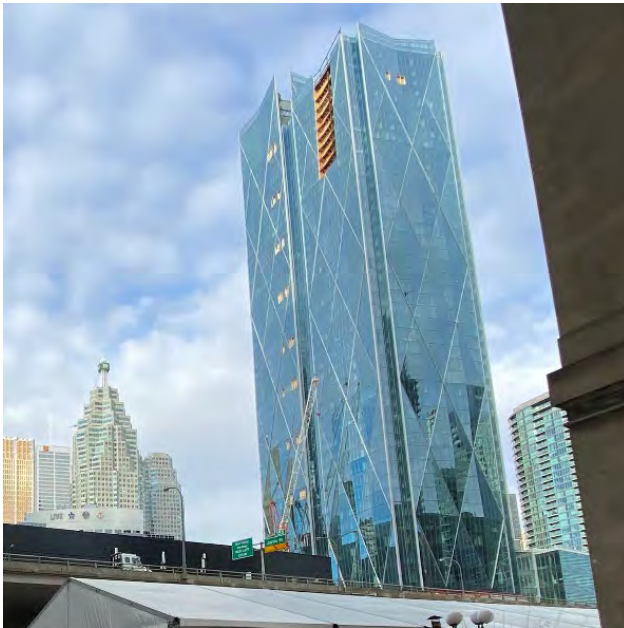


Plate 11: View of new construction located along Bay Street between the railway and Lakeshore Boulevard, facing northeast.



Plate 12: View of commercial residential structure located at 31, 35, 39 Bay Street, facing east.

3.2 Union Station HCD Public Realm

The project footprint of the WELRT runs through the Union Station HCD along Bay Street and is situated primarily within the public realm. The public realm within this area is comprised of functional spaces, such as streets, sidewalks, boulevards, an above grade teamway, and an underpass. These spaces provide a facilitate daily life and connect the current form and function of the HCD to its historical past. Many of the buildings in this area are home to public transit and recreational activities and as such



the permeability and amenity of the public realm facilitates a relationship between the streetscape and the buildings. The design and condition of the public realm within the Study Area is not cohesive as currently constituted; some areas are newly refurbished while others are in various states of disrepair. While there is some cohesion in the types of surface treatments, lighting, and style of street furniture, these elements are not uniform within the Study Area that falls within the Union Station HCD.

The north end of the Study Area is located at the intersection of Bay Street and Front Street West. Brookfield Place is located on the northeast corner, the Royal Bank Plaza is on the northwest corner, the Dominion Public Building is located at the southeast corner, and the Union Station Complex is situated at the southwest corner. This streetscape generally typical of a high-density urban neighborhood with streetlights, painted crosswalks, and sidewalks (**Plate 13**). The area has signage relating to traffic laws and street names as well as signage directing pedestrians to mass transit and tourist destinations. Some signage is posted to metal light posts and some signage is set with modern tablet like fixtures (**Plate 15 to Plate 16**). Non standardized street furniture including waste and recycling receptacles as well as newspaper boxes are situated throughout the streetscape (**Plate 15 to Plate 16**). The sidewalk area surrounding Union Station is wider than normal and, as such, the open space associated with Union Station melds into the public realm (**Plate 15**). There is a concrete centre partition located in the middle of Bay Street starting south of Front Street West leading to the Bay Street underpass that has a cast iron partition. The presence of the Union Station moat and underground circulation paths necessitates the inclusion of entry points and ventilation infrastructure of different heights. The southwest corner of the intersection has a large quantity of concrete highway dividers situated in a formation limiting vehicular access to the flow of pedestrian traffic around Union Station while allowing pedestrians to move through the area with as little restriction as possible. The paving stone and concrete paving material utilized in this area has some unifying characteristics including the use of dark grey brick pavers as an accent, although for the most part the materials are not cohesive. There are large concrete pavers that comprise most of the walking surface of different ages and patina. In some areas it appears construction and repair has led to the replacement of some concrete paving material with asphalt or concrete that is not the same colour or texture as the surrounding area.

The WELRT project footprint proceeds south from Front Street West along Bay Street where through an underpass under the Union Station trainshed. This underpass allows vehicular and pedestrian traffic to flow under the trainshed along Bay Street. Pedestrian traffic flows under Bay Street via teamways located immediately parallel to the street and through indoor teamways connecting to Union Station (west) and the bus terminal at 141 Bay Street (east) (**Plate 21 to Plate 23 and Plate 26 to Plate 29**). The underpass is supported by formed concrete columns with segmentally arched openings running the length of the underpass down the centre of Bay Street and the length of both pedestrian teamways along Bay Street (**Plate 21 to Plate 23**). The finishing of the arched piers within this underpass is variable, the middle row of concrete columns and the row along the east teamway have undergone refurbishment in the form of a pigmented white sealer while the concrete columns along the west side of Bay Street have not (**Plate 22 and Plate 23**). A concrete centre median forms a base for the concrete columns located



in the centre span of the road, this is accented by a stainless-steel partition located intermittently throughout the spaces between the centre arches. The concrete columns located along the east and west teamways have windows set within the arches. The window finishing is more uniform and refined along the west teamway. The underpass is lit by square light fixtures located along the top of the arched pier walls as appose to the ceiling of the underpass. Beams and girders relating to the steel construction of the trainshed above can be seen throughout the ceiling of the underpass (**Plate 21**). Both exterior teamways are flanked by low concrete partitions protecting pedestrians from traffic (**Plate 24**). The west outdoor teamway has a surface composed primarily of red brick at the northern end of the underpass, although this material is not consistent and is comprised of large concrete pavers flanked by stone paver accents at the southern end of the underpass (**Plate 23** and **Plate 24**). The walking surface of the eastern outdoor teamway is comprised mainly of large concrete pavers, although the area was under construction at the time of the site assessment and much of the surface was covered (**Plate 22**).

The indoor teamways along Bay Street are wide corridors facilitating pedestrian flow north-south through the HCD and allowing entry into the mass transit amenities flanking the underpass. These teamways exhibit the concrete columns with segmentally arched openings on the elevations fronting the underpass and have access points to mass transit along the opposite length of the corridors (**Plate 26** and **Plate 27**). The indoor teamway corridor along the west side of Bay Street is composed primarily of exposed concrete and degradation can be seen on the columns and arched openings (**Plate 25** and **Plate 26**). The windows between the arches separating the indoor teamway from the outdoor teamway are clear and the walking surface is concrete. In contrast, the east indoor teamway corridor accessing the bus terminal at 141 Bay Street has undergone recent refurbishment and has a clean white appearance. The arches along this teamway have uniform frosted glass fronting Bay Street reflecting a warm orange light, the walking surface is uniform tile, and the street furniture is uniform and modern (**Plate 28** and **Plate 29**). The teamway on the east side of York Street includes much simpler design finished (concrete walking surface and bare concrete columns) but is in good condition (**Plate 30**).

The project footprint continues south of the Bay Street underpass where there is a modern raised teamway running east-west crossing Bay Street (**Plate 31**). Scotiabank Arena is located immediately at the southwest entrance to the Bay Street underpass while construction hoarding is present at the southeast entrance to the underpass due to ongoing construction (**Plate 32** and **Plate 33**). Beyond the construction hoarding on the east side of Bay Street there is a new modern high-rise building that is fronted by planters and a high-quality walking surface with dark pavers (**Plate 34**). The west side of Bay Street from the underpass to Lakeshore Boulevard West generally consists of a narrow concrete sidewalk lined with stoned pavers (**Plate 35**). This area is characterized by signage and street furniture comparable to that north of the underpass. The Gardiner Expressway crosses east-west over the intersection of Lakeshore Boulevard West and Bay Street above Lakeshore Boulevard where there is ongoing construction located at the northeast corner of the intersection (**Plate 36**). The concrete



piers supporting the expressway are in disrepair and the deck of the expressway shades areas of the intersection.

The Study Area continues south through the HCD along Bay Street from Lakeshore Boulevard West to Harbour Street. The sidewalk along the west side of Bay Street from Lakeshore Boulevard to Harbour Street is generally formed by large concrete pavers and is partially lined by decorative stone pavers, as is the general theme throughout the HCD. The concrete sidewalk exhibits intermittent evidence of repair and modifications as is generally the case throughout the Study Area (**Plate 37**). There is a mixture of globe shaped streetlights attached to concrete utility poles along the sidewalk that have green metal finishing and lighting that is cantilevered off of concrete and metal utility poles (**Plate 38**). The sidewalk along the west side of Bay Street is lined by interspersed modern metal framed structures with curved glass overhangs sheltering pedestrians from the elements along the sidewalk (**Plate 39**). The east side of Bay Street from Lakeshore Boulevard West to Harbour Street consists of a walking surface composed of primarily large concrete pavers with stone pavers utilized sporadically along the road edge. The cantilevered type of streetlight is utilized on the concrete utility poles and there is permeable access to the west elevation of the structure located along Bay Street (**Plate 40**).



Plate 13: View of the intersection of Front Street West and Bay Street facing east.



Plate 14: View of the intersection of Bay Street and Front Street West facing west. Note the modern signage and the large concrete block that is likely associate with underground infrastructure.

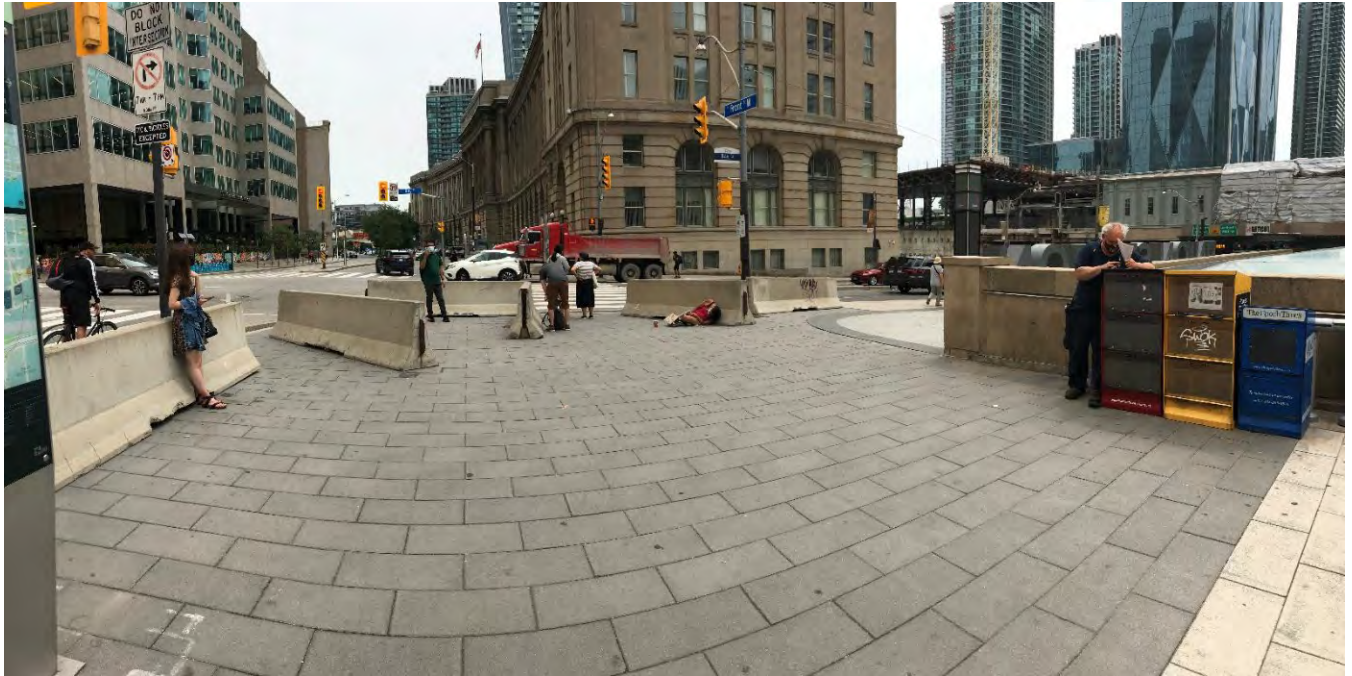


Plate 15: View of the corner of Front Street West and Bay Street facing east showing the expanded sidewalk area of Union Station that melds into the public realm. Note the use of large stone pavers used within the area associated with Union Station.

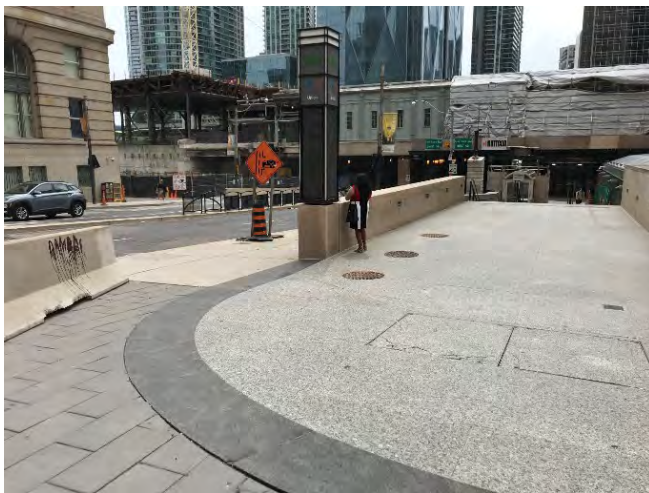


Plate 16: View of the southwest corner of Bay Street and Front Street West facing south. Note the concrete partition between the Union Station open space and the concrete sidewalk closer to Bay Street.

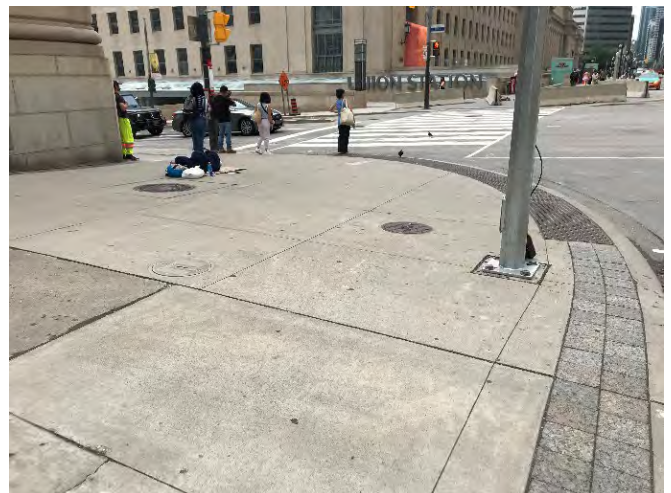


Plate 17: View of the southeast corner of Bay Street and Front Street West facing west. Note the large concrete paving stones that are accented by decorative paving stone inlay along the curb.



Plate 18: View of Bay Street from Front Street West facing south. Note the stone paver inlay flanking the curb and the large concrete paving with interspersed pavers of different ages and texture.



Plate 19: View of Bay Street facing south. Note the metal partition in the middle of Bay Street.



Plate 20: View of walking surface interface between Bay Street and the Union Station Complex property.



Plate 21: View of the Bay Street underpass facing southeast.



Plate 22: View of the east teamway in the Bay Street underpass facing south.



Plate 23: View of the west teamway in the Bay Street underpass facing south.



Plate 24: View of the concrete partition and brick walking surface at the north end of the west teamway in the Bay Street underpass.

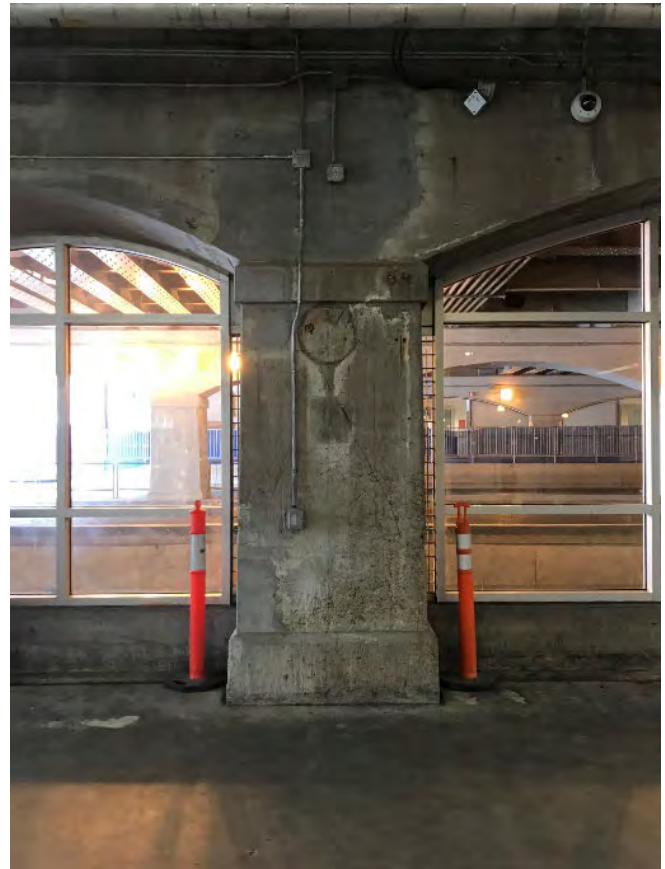


Plate 25: View of a concrete pier in the west indoor teamway



Plate 26: Indoor teamway running along the west side of Bay Street.



Plate 27: Indoor teamway running along the east side of Bay Street



Plate 28: View showing the condition of the refurbished segmentally arched columns in the indoor teamway on the east of Bay Street.

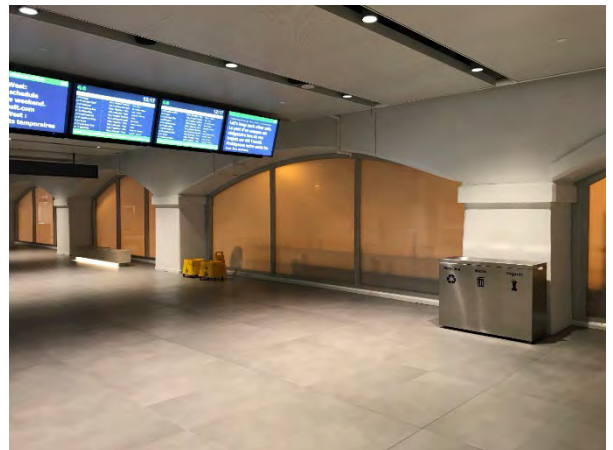


Plate 29: View showing the refurbished modern white interior of the indoor teamway on the east of Bay Street. Note the frosted glass reflecting orange light and the modern street furniture.



Plate 30: Indoor teamway on the east side of York Street



Plate 31: View of Bay Street facing north across from the Postal Delivery Building (left). Note the modern raised teamway crossing Bay Street.



Plate 32: View of construction hoarding south of the southeast entrance to the Bay Street underpass, facing north.



Plate 33: View of the west side of Bay Street facing the Union Station trainshed, looking north.



Plate 34: North view of the east side of Bay Street south of the Bay Street underpass, looking towards the Union Station trainshed.



Plate 35: South view of the west side of Bay Street, looking towards the Gardiner Expressway.



Plate 36: East view of the Gardiner Expressway from Bay Street



Plate 37: View of the west side of Bay Street south of Lakeshore Boulevard West, looking south.



Plate 38: View showing an example of lighting throughout the Study Area, facing south.



Plate 39: View showing pedestrian shelters located on the west side of Bay Street between Lakeshore Boulevard West and Harbour Street, facing north.



Plate 40: View showing the east side of Bay Street between Lakeshore Boulevard West and Harbour Street, facing north.

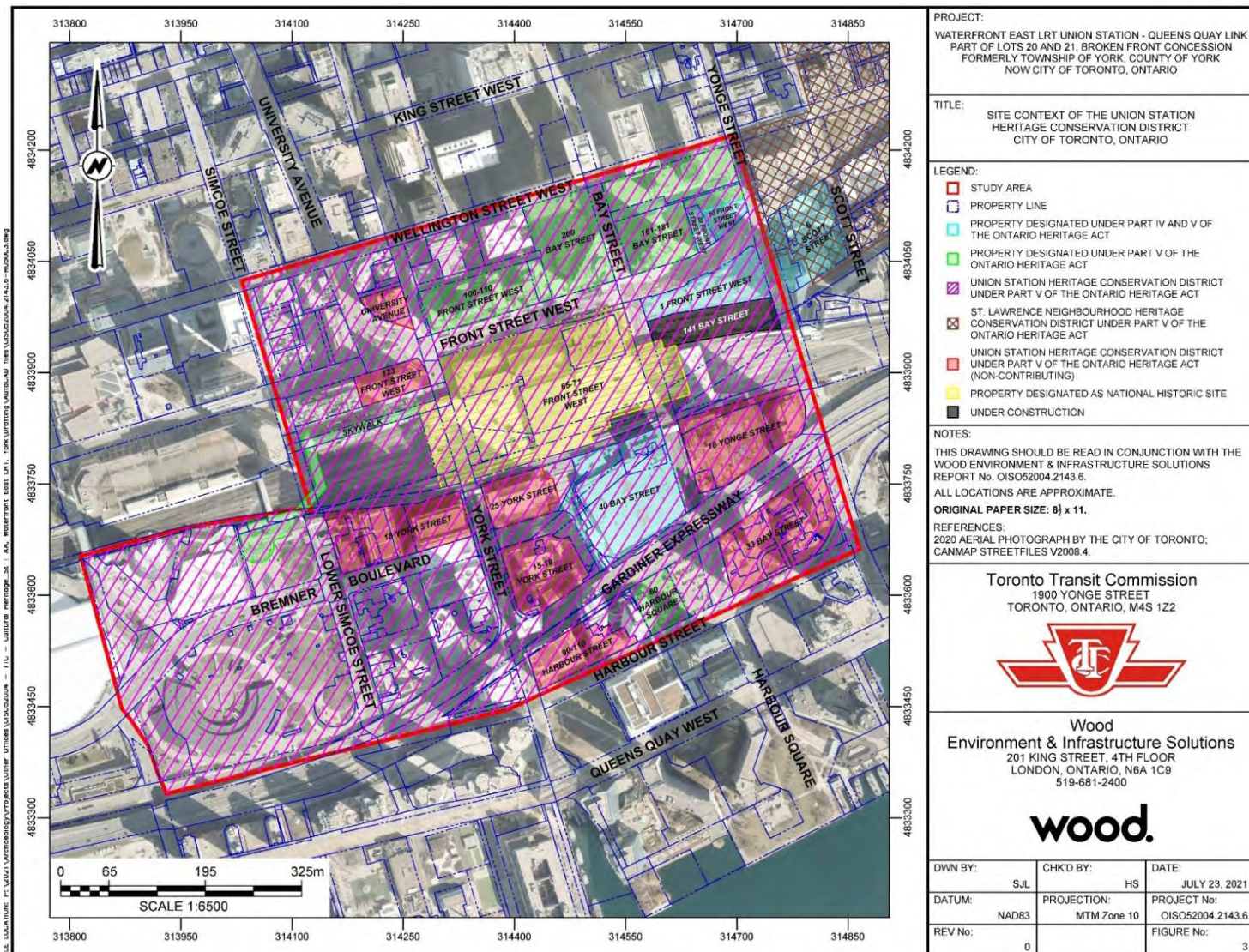


Figure 3: Site Context of the Union Station Heritage Conservation District
Please reach out to the Project Team should you require alternative text for this image.



4.0 Description and Purpose of Proposed Activity

The proposed work within the Union Station HCD consists of the installation of the new WELRT streetcar loop and associated platform. This includes the installation of major infrastructure including the expansion of the current subway tunnel to be deeper and the installation of a new platform with access to the TTC Union Station Subway Station. Details of the proposed work are provided below and drawings showing the proposed architectural plan, construction phasing plan, and landscape plan are provided in **Appendix B**.

High-level description of potential impacts to the Union Station HCD:

- Open cut excavation to install the WELRT streetcar loop along Bay Street and Front Street West involving demolition of existing infrastructure, property acquisition, and construction of new loop, including the associated platform and connections to existing transit infrastructure. Post-construction landscaping is proposed to restore the public realm.

Review of drawings of proposed work:

- Union Station Aerial View Renders, RE35-1-AA011 to RE35-1-AA013 (July 7, 2021)
 - Overview of the proposed work shown as 3D renders
- Union Station Platform Level Demolition Plan, RE35-1-AAD001 (July 7, 2021)
 - Demolition of Union Station Streetcar Loop at platform level
 - Basement work proposed at 141 Bay Street
 - Excavation area within 1 Front Street West and immediately adjacent to southwest corner of heritage building
 - Excavation within, and adjacent to, Union Station Complex
- Union Station Street Level Demolition Plan, RE35-1-AD002 (July 7, 2021)
 - Street level proposed demolition area depicted
 - Demolition activities shown with Union Station Complex (65-71 Front Street West), Dominion Public Building (1 Front Street West), Postal Delivery Building (40 Bay Street), and the public realm of the Union Station HCD along Bay Street
- Union Station Site Plan, RE35-1-A1001 (July 7, 2021)
 - Site plan depicting the location of new construction shown
 - New construction shown within Union Station Complex (65-71 Front Street West) and 141 Bay Street, Dominion Public Building (1 Front Street West), and within public realm of Union Station HCD
 - New stairs showing within 'Existing Arcade' of Union Station and northwest portion of building
 - Vent shafts to street level shown
 - Existing columns of Bay Street Bridge to be retained



- Union Station Street Level Plan, RE35-1-A2002 (July 7, 2021)
 - Union Station Arcade: Stair M and new Elevator E3
 - Northwest corner of Union Station building: Existing elevator 13 to remain and New Stair N
 - Proposed vent shafts on west side of Bay Street, south of Front Street and within Bay Street right-of-way
 - New curbs adjacent to Dominion Public Building (1 Front Street West)
 - 141 Bay Street: Stair Q and new Elevator E4 shown
 - Bay Street: Bay Street underpass (northbound and southbound shown) and associated new bike lanes shown

Review of Landscape Plan Drawings (G85-395-AL100; G85-395-AL200; G85-395-AL500; G85-395-AL800; G85-395-AL900; and G85-395-AL1200):

- Unit pavers will be restored to original condition at the southeast and southwest corners of Bay Street and Front Street West within the public realm.
- The signal wall, stairwells, and shade structure canopy at the lower level of Union Station will be restored.
- Cast in place concrete paving will be installed south of the corner of Front Street West along the west and east sides of Bay Street to the Bay Street underpass within the public realm.
- Cast in place concrete paving will be installed on the west and east sides of Bay Street within the public realm between the underpass and Lakeshore Boulevard West.
- Cast in place concrete paving will be installed on the west and east sides of Bay Street within the public realm between the Lakeshore Boulevard West and Harbour Street.
- Unit pavers immediately adjacent to the entrance into the concourse between Scotiabank Arena and Union Station will be restored to the original condition.
- Existing trees and tree planters will remain along Bay Street within the public realm.
- Positive drainage toward Bay Street will be implemented on newly installed pavers.
- Limited planting of additional trees indicated along Bay Street south of the underpass.
- Drawing G85-395-AL1200 shows details of landscape and paving including tree planting and sidewalk construction.
- Sidewalk detail indicates cast in place pavers will have a strip of precast unit pavers adjacent to the curb.
- Tree planting will be conducted as per manual City of Toronto streetscape manual T-34



5.0 Impact Assessment

The MCM *Standards & Guidelines for Conservation of Provincial Heritage Properties – Information Bulletin 3: Heritage Impact Assessment for Provincial Heritage Properties* (MCM 2017) gives guidance on how to complete HIAs for public bodies prescribed under the *Ontario Heritage Act*, (Government of Ontario 2014). The purpose of the HIA is to identify and assess the proposed activity to determine impacts (positive or negative, direct or indirect) that the proposed activity may have on the property's cultural heritage value or interest. For the purpose of this HIA, the following definitions of direct, indirect, and positive impacts are used:

- **Direct Adverse Impact:** A permanent or irreversible negative affect on the cultural heritage value or interest of a property or result in the loss of a heritage attribute on all or part of the provincial heritage property.
- **Indirect Adverse Impact:** An impact that is the result of an activity on or near the property that may adversely affect its cultural heritage value or interest and/or heritage attributes.
- **Positive Impact:** An impact that may positively affect a property by conserving or enhancing its cultural heritage value or interest and/or heritage attributes.

(Government of Ontario 2017)

A detailed assessment of potential impacts resulting from the proposed TTC WELRT work is provided in Section 5.1. A discussion of the potential impacts resulting from the proposed work is provided in Section 5.2.

5.1 Potential Impacts to the Union Station HCD Public Realm

As outlined in Section 4.0, the proposed work includes the demolition of the existing streetcar loop and construction of the new WELRT streetcar loop in the same location. A detailed assessment of the potential impacts resulting from the proposed work is provided in **Appendix B**.

Table 1: Assessment of Potential Direct Adverse Impacts to the Union Station HCD

Potential Direct Adverse Impact	Y/N	Discussion
Removal or demolition of all or part of any heritage attribute.	N	No heritage attributes associated with the Union Station HCD public realm will be removed or demolished as part of the proposed work. The Bay Street right-of-way within the Union Station HCD and the public realm immediately adjacent to it will be demolished and excavated to accommodate the removal of the existing below grade streetcar loop and construction of the new streetcar loop and platform. While this



Potential Direct Adverse Impact	Y/N	Discussion
		will result in removal and demolition of the Bay Street right-of-way and the public realm immediately adjacent to it, this impact will not result in the permanent removal or demolition of any protected heritage attributes within the public realm of the Union Station HCD.
Removal or demolition of any building or structure on the provincial heritage property whether or not it contributes to the cultural heritage value or interest of the property (i.e. non-contributing properties).	Y	The demolition of the existing streetcar loop is proposed as part of the WELRT project. While the streetcar loop is below grade, proposed work will result in the demolition of parts of the existing public realm within the Union Station HCD. Accordingly, mitigation measures must be prepared.
Any land disturbance, such as change in grade and/or drainage patterns that may adversely affect a provincial heritage property, including archaeological resources.	Y	Open cut excavation is planned within the Bay Street right-of-way and the public realm immediately adjacent to it. Accordingly, mitigation measures must be prepared.
Alterations to a property in a manner that is not sympathetic, or is incompatible, with cultural heritage value or interest of the property. This may include necessary alterations, such as new systems or materials to address health and safety requirements, energy-saving upgrades, building performance upgrades, security upgrades or servicing needs.	N	<p>The Bay Street right-of-way within the Union Station HCD and public realm immediately adjacent to it will be demolished and excavated to accommodate the removal of the existing below grade streetcar loop and construction of the new streetcar loop and platform.</p> <p>Installation of a new streetcar loop is not incompatible with the CHVI of the HCD. The existing streetscape will be demolished and permanently altered, however the proposed alterations are in keeping with the spirit of the HCD.</p>
Alterations for access requirements or limitations to address factors as accessibility, emergency egress, public access, or security.	N	No new access requirements or limitations to address factors as accessibility, emergency egress, public access, or security are anticipated within the public realm of the HCD



Potential Direct Adverse Impact	Y/N	Discussion
Introduction of new elements that diminish the integrity of the property, such as a new building, structure or addition, parking expansion or addition, access or circulation roads, or landscape features	N	The WELRT project involves the reconstruction of the existing streetcar loop in the same location. While existing elements of the streetscape will be demolished and replaced, this will not diminish the integrity of the HCD. Accordingly, no impacts related to new elements are anticipated.
Changing the character of the property through removal or planting trees or other natural features, such as a garden, or that may result in the obstruction of significant views or vistas within, from, or of built and natural features.	N	No changes to the character of the HCD are proposed. The scope of work for this project is largely below grade and changes to the streetscape at ground level are anticipated to at a scale that would result in impacts to views or vistas within the HCD.
Change in use for the provincial heritage property that could result in permanent, irreversible damage or negates the property's cultural heritage value or interest.	N	No changes in land use are planned.
Continuation or intensification of a use of the provincial heritage property without conservation of heritage attributes.	N	The proposed improved streetcar loop may result in the intensification of use within the HCD. However, this intensification in use is in line with the evolving character of the HCD and will not have an impact on the heritage attributes of the public realm within the HCD that is already a well-used area.

Table 2: Assessment of Potential Indirect Impacts Union Station (65-71 Front Street West)

Potential Indirect Adverse Impact	Y/N	Discussion
Shadows that alter the appearance of a heritage attribute or change the visibility of an associated natural feature or plantings, such as a tree row, hedge, or garden.	N	No shadow-related impacts are anticipated.



Potential Indirect Adverse Impact	Y/N	Discussion
Isolation of a heritage attribute from its surrounding environment, context, or a significant relationship	N	No isolation-related impacts are anticipated.
Vibration damage to a structure due to construction or activities on adjacent to the property	N	Open cut excavation and construction is proposed within the Union Station HCD. Vibration related impacts are not anticipated to have an effect on the public realm of the HCD and mitigation measures are not required.
Alteration or obstruction of a significant view of or from the provincial heritage property from a key advantage point	N	No impacts to significant views are anticipated since the proposed work is largely located below grade.

Table 3: Assessment of Potential Positive Impacts to Union Station (65-71 Front Street West)

Potential Positive Impact	Y/N	Discussion
Changes or alterations that are consistent with accepted conservation principles, such as those articulated in MCM' <i>Eight Guiding Principles in the Conservation of Historic Properties, Heritage Conservation Principles for Land Use Planning</i> , Parks Canada's <i>Standards and Guidelines for the Conservation of Historic Places in Canada</i>	Y	<p>The Bay Street right-of-way within the Union Station HCD and public realm immediately adjacent to it will be demolished and rehabilitated to accommodate the removal of the existing below grade streetcar loop and construction of the new streetcar loop.</p> <p>The type of improvement to transportation infrastructure proposed will encourage continued and increased use of the HCD in a manner consistent with the CHVI of the HCD and its evolving character. The proposed intervention also provides the opportunity to create a more cohesive public realm within the HCD corresponding more closely to the guidelines pertaining to the Union Station Public Realm outlined in the Section 8.3.5 of the Union Station HCD Plan.</p>



Potential Positive Impact	Y/N	Discussion
Adaptive re-use of a property – alteration of a provincial heritage property to fit new uses or circumstances of the property in a manner that retains its cultural heritage value or interest	N	No new use is planned as part of the proposed work.
Public interpretation or commemoration of the provincial heritage property	N	No public interpretation or commemoration is planned as part of the proposed work.

5.2 Discussion of Anticipated Impacts

The detailed impact assessment determined that two (2) direct adverse impacts and one (1) positive impact are anticipated as a result of the proposed work.

5.2.1.1 Summary of Potential Direct Adverse Impacts

Drawings of the proposed work depict the demolition of the existing streetcar loop and street level/open cut excavation along Bay Street from Front Street West to Harbour Street within the Union Station HCD. The Bay Street right-of-way and the public realm immediately adjacent to it will be demolished and replaced as part of this work. Open cut excavation is planned within the Bay Street right-of-way and the public realm immediately adjacent to it. Accordingly, mitigation measures must be prepared.

Impacts of the WLERT project are anticipated within the Union Station Complex, the Dominion Public Building, and Scotia Bank Arena. These impacts have been identified and assessed by WSP under separate covers for this project.

Alternatives and mitigation measures to avoid potential adverse impacts to the Union Station Complex are provided in Section 6.0.

5.2.1.2 Summary of Potential indirect Adverse Impacts

No potential indirect impacts to the Union station HCD public realm are anticipated.

5.2.1.3 Summary of Potential Positive Impacts

The proposed work will result in permanent changes to the public realm within the Union Station HCD. However, the type of improvement to transportation infrastructure proposed will encourage continued and increased use of the HCD in a manner consistent with its CHVI and evolving character of the district. Additionally, the proposed intervention provides the opportunity to create a more cohesive public realm within the HCD corresponding more closely to the guidelines pertaining to the public realm outlined in the Section 8.3.5 of the Union Station HCD Plan (ERA Architects Inc. 2006).



6.0 Considered Alternatives and Mitigation Measures

Alternative approaches and mitigation measures are required when impacts are anticipated to a property with CHVI. In the case of the Union Station HCD two direct impacts and one positive impact are anticipated due to demolition activities, open cut excavation, and replacement of the demolished portions of the public realm. To address these anticipated impacts, the mitigation measures contained in MCM InfoSheet#5 were considered (Government of Ontario 2005). An evaluation of applicable mitigation measures is presented in **Table 4**.

Table 4: Mitigation Measures

Mitigation Measures		Relevance	
Alternative development approaches		n/a	
Isolating development and site alteration from significant built and natural features and vistas		n/a	
Design guidelines that harmonize mass, setback, setting, and materials		<p>The public realm of the Union Station HCD will be permanently altered along Bay Street between Front Street West and Lakeshore Boulevard West. The existing landscape treatments in the public realm of this section of the HCD are variable and in poor condition in some areas. The proposed work has the potential to rehabilitate the public realm of this section of the HCD to be in line with the guidelines of the Union Station HCD Plan. High quality materials and standards should be used to link this section of Bay Street with other areas of the Union Station HCD realm that have already been rehabilitated.</p> <p>Guidelines pertaining to the public realm within Section 8.3.5 of the Union Station HCD Plan should applied when considering the design of the public realm once excavation is complete. Based on the HCD Plan, the following design guidance is offered:</p> <p>Replacement of the infrastructure within the public realm should promote the heritage character of the HCD:</p> <ul style="list-style-type: none">• Lighting should emphasize building form at night in a manner representative of the grandeur of the architecture• Historical precedents in planning around Union Station, such as the John Lyle Plan of 1911,	



Mitigation Measures	Relevance
	<p>should be used to inform the importance of establishing connections to the south of the district.</p> <ul style="list-style-type: none">• Historical connections, both visual and physical, should be maintained and enhanced, such as the view of the Royal York Hotel and the physical connections between the John Street Roundhouse and Union Station.• The utilization of teamways and bridges should be emphasised as important linkages between areas of the district. <p>Design should aspire to the highest standards of design excellence:</p> <ul style="list-style-type: none">• Streetscape elements should be of high-quality design and enduring materials that are appropriate to the district's historic character.• The approach to design and materials used should reflect good contemporary design to emphasize the district's evolving character.• Designs using inauthentic historical pastiche, for example mock-Victorian, should be avoided because they look backwards, and when executed using modern techniques do not have the quality of craft of the original. <p>Streetscape furniture should fit into the landscape rather than be its focus:</p> <ul style="list-style-type: none">• Street furniture should have high standards of functionality, durability, environmental performance, and visual attractiveness.• Materials such as stainless steel should be used because they wear well and do not require continual maintenance.• Pedestrian comfort should be encouraged by considering and supporting pedestrian flow, needs of elderly, visually impaired, etc.



Mitigation Measures	Relevance
	<p>Maintain open space:</p> <ul style="list-style-type: none">• Open space helps maintain the environmental quality of the district, in addition to providing a calm gathering space. Open spaces should be properly protected and maintained. <p>Reduce Clutter:</p> <ul style="list-style-type: none">• In order to reduce clutter on the streetscape, the size and number of objects like waste and recycling receptacles and newspaper boxes should be reduced. For example, the three-unit garbage receptacle should be reconsidered as three separate components. Consolidated newspaper boxes should be used to replace the banks of 10-20 boxes chained together. <p>Coordinate Design implementation and maintenance:</p> <ul style="list-style-type: none">• The overall design of the public realm should be carried out in a consistent and well-coordinated manner to ensure that design measures complement each other and work towards enhancing the district's identity.
Limiting height and density	n/a
Allowing only compatible infill and additions	n/a
Reversible alterations	n/a
Buffer zones, protection measures, and other planning mechanisms	n/a

6.1 Discussion of Mitigation Measures

The evaluation of mitigation measures contained in InfoSheet#5 determined that design guidelines are appropriate mitigation measure to protect the cultural heritage value of the Union Station HCD public realm.



6.1.1 Design Guidelines

Design guidelines are required to guide the proposed work within the Union Station HCD public realm. The Union Station HCD has an evolving identity and the streetscape should reflect contemporary advances in technology and design that encourage accessibility for the elderly and visually impaired, promote ease of pedestrian flow, create a cohesive sense of place, and extenuate the monumental heritage structures within the HCD. Design of the public realm should promote the heritage character of the HCD by utilizing existing examples of materials and forms that currently within the HCD that create a sense of place and promote the heritage character of the HCD.

The HCD currently has multiple types of lighting fixtures within it. When replacing the lighting fixtures within the public realm materials and forms should be used that accentuate the heritage architecture in the area and promote. Lighting that is too bright or is constructed in a way that obscures the surrounding architecture should be avoided. Examples of existing lighting within the HCD should be consulted when considering the form of new fixtures.

Walking surfaces currently within the HCD have some continuity in form, although there is variation in design and material throughout. Walking surface material and alignment should be designed to be as accessible as possible from all intersecting access points including teamways and sidewalks, as connectivity is an important defining attribute of the HCD. Large concrete paving stones accented by smaller stone pavers along the road edge and the edge of structures is general theme throughout the HCD. This cohesive theme should be maintained and improved upon in order to increase the sense of place and cohesiveness of the HCD. Some areas of the HCD have high quality granite walking surfaces accented by planters. While these outlier areas create interruptions to a cohesive design throughout the HCD, they contribute to the HCDs sense of grandeur and are welcome outliers that should be maintained or added to.

Currently the HCD has a variety of street furniture including garbage cans, recycling bins, signage, and benches. Design of the replacement streetscape should include street furniture with a consistent and contemporary design throughout the HCD. The materials and forms selected should be enduring and high-quality materials such as stainless steel that reflect the importance of the HCD and are relatively low maintenance. An effort should be made to de-clutter the public realm by selecting inconspicuous multi-purpose street furniture. As with the lighting and walking surfaces, successful examples throughout the HCD should be consulted and added to or adapted to create a consistent design, not just throughout the Study Area but throughout the HCD as a whole. While tree planters and street furniture add to the amenity of the public realm and are consistent with the heritage tenants of the HCD, effort should be made to keep the public realm as spacious as possible. The size and number of objects should be limited to maintain the environmental quality of the HCD. Overall, design throughout the HCD should be as consistent as possible to create a sense of place and be complementary to the identity of the district. Using inauthentic historical pastiche is not consistent with the character of the HCD.

The Study Area includes the Bay Street railway underpass. Currently this area is under construction and refurbishment of the east teamway has occurred. This refurbishment



includes a white finish of the arched piers and a modern white aesthetic accented by frosted windows set between the arches creating an orange light. The walking surface is comprised of large tiles. This design could be emulated in the west teamway along Bay Street in order to create a cohesive and amenable environment within this part of the HCD.



7.0 Summary of Community Engagement

In order to identify the existing heritage protections and requirements for the Union Station HCD, the MCM, Ontario Heritage Trust, and the City of Toronto were consulted. A summary of the community engagement results is presented in **Table 5** and records of correspondence are provided in **Appendix D**.



Table 5: Summary of Community Engagement

Individuals/Groups Engaged	Method	Results	Incorporation into HIA
Yasmina Shamji, Urban Design/Heritage Planning, City of Toronto	-Email sent on December 16, 2020 -Response received on January 7, 2021	<p>Yasmina Shamji from Urban Design and Heritage Planning at the City of Toronto was initially contacted as part of the CHR prepared for the TTC WELRT project. Ms. Shamji reported that there are a number of known/protected municipal heritage properties within the CHR Study Area. These included:</p> <ul style="list-style-type: none">-40 Bay Street (Designated under Part IV and V of the <i>Ontario Heritage Act</i>)-71 Front Street West (Union Station) (Designated under Part IV and V of the <i>Ontario Heritage Act</i>)-145 Queens Quay West (Designated under Part IV of the Ontario Heritage Act)-2 Cooper Street (Intention to Designate)-1 Front Street West (Designated under Part IV and V of the Ontario Heritage Act)-61 Front Street West (Listed Heritage Property) <p>Ms. Shamji noted that property information can be found on the City of Toronto's Heritage Register Map and By-law information is available on the By-laws and Municipal Codes webpage.</p> <p>Information was requested on additional properties within the Union Station HCD. Ms. Shamji provided the contact information for Guy Zimmerman, Heritage Planner at the City of Toronto, and recommended that WSP contact</p>	Levels of heritage protection for Union Station (65-71 Front Street) and nearby provincial heritage properties noted in the HIA. Guy Zimmerman contacted to obtain direction on the Union Station HCD.



Individuals/Groups Engaged	Method	Results	Incorporation into HIA
		Mr. Zimmerman regarding information related to the Union Station HCD.	
Guy Zimmerman, Heritage Planner, City of Toronto	-Series of email correspondence between March 26, 2021 and July 7, 2021 -Online meeting on Tuesday, July 7, 2021 regarding the Gardiner Expressway, PATH system, and Bay Street Bridge	Based on the recommendation of Yasmina Shamji, Guy Zimmerman, Heritage Planner with the City of Toronto, was contacted regarding properties within the Union Station HCD that are located within the TTC WELRT Study Area. Guy Zimmerman is noted as being the Heritage Planner for the Union Station HCD. Mr. Zimmerman provided HCD inventory sheets for a number of individual HCD properties/landscape features within the TTC WELRT Study Area. Subsequent conversations with Mr. Zimmerman determined that a demolition permit was issued for the Workmen's Compensation Board Building (90 Harbour Street) in 2011 and that this building is no longer standing. An online meeting with Guy Zimmerman (City of Toronto), Brent Fairbairn (City of Toronto), and Heidi Schopf (WSP) was carried out on Tuesday, July 7, 2021 to discuss the role of the Gardiner Expressway, PATH System, and Bay Street Bridge in the Union Station HCD. Mr. Zimmerman discussed how the Gardiner Expressway and PATH System are contributing landscape elements of the HCD and that the public realm contributions of these resources should be considered. Ms. Schopf asked if the Bay Street Bridge (supporting the Union Station Trainshed over Bay Street) had heritage	Inventory sheets provided by Guy Zimmerman used as background information in the preparation of the CHR and this HIA. Information on the role of Parks Canada and Metrolinx used to expand the community engagement strategy for this HIA.



Individuals/Groups Engaged	Method	Results	Incorporation into HIA
		significance in its own right since this substructure was not noted as a heritage resource on the municipal register or by the MCM and OHT. Mr. Zimmerman expressed that Union Station is run by Parks Canada and that this federal agency will have direction regarding the heritage status of individual components of the Union Station complex. Mr. Zimmerman suggested that Parks Canada and Metrolinx have previous cultural heritage studies that may be of relevance to the current Study Area.	
Kevin DeMille, Natural Heritage Coordinator, Ontario Heritage Trust	-Emails sent on December 16, 2020, March 25, 2021, April 8, 2021, and July 8, 2021 -Responses received on March 26, 2021, April 14, 2021, and July 13, 2021	Kevin DeMille, Natural Heritage Coordinator with the Ontario Heritage Trust was initially contacted as part of the CHR prepared for the TTC WELRT project. Mr. DeMille reported that the Ontario Heritage Trust does not have any conservation easements or Trust-owned properties within the WELRT Study Area. Mr. DeMille recommended reviewing the Ontario Heritage Trust's Plaques Database and Ontario Heritage Act register to obtain information on municipal/local heritage properties in the Study Area. Mr. DeMille further recommended direct communication with the City of Toronto regarding listed and designated municipal heritage properties. Kevin DeMille was subsequently contacted on April 8, 2021 and July 8, 2021 regarding three properties within the Study Area, including Union Station (65-71 Front Street West), the Dominion Public Building (1 Front Street West),	Heritage protections noted by Kevin DeMille were incorporated into the CHR and this HIA. By-laws provided by Mr. DeMille were reviewed. Contact information for Kirushanth Gnanachandran at Parks Canada was used to consult this agency as part of the community engagement completed for this HIA.



Individuals/Groups Engaged	Method	Results	Incorporation into HIA
		and the Postal Delivery Building (40 Bay Street). Mr. DeMille provided the designation By-laws for these properties and also noted that Union Station (65-71 Front Street West) is a National Historic Site. Mr. DeMille recommended consultation with Parks Canada given the federal status of this property. Mr. DeMille subsequently provided the contact information for Kirushanth Gnanachandran with Parks Canada at the request of WSP.	
Karla Barboza, (A) Team Lead, MCM	-Email sent on December 16, 2020 -Email response received on December 17, 2020 -Email sent on April 8, 2021	Karla Barboza, Acting Team Lead for Heritage at the MCM was initially contacted as part of the CHR prepared for the TTC WELRT project. Karla reported that there were no properties designated by the Minister in the TTC WELRT Study Area. Ms. Barboza did identify that Union Station is a Provincial Heritage Property of Provincial Significance. She also noted that there are two nearby provincial heritage properties including the Union Station Rail Corridor (USRC) Interlocking Tower – Scott Street (Provincial Heritage Property of Provincial Significance) and Yonge Street Railway Bridge (Provincial Heritage Property – Local Significance). Ms. Barboza also noted that Strategic Conservation Plans (SCP) have not been prepared for Union Station (65-71 Front Street), the USRC Interlocking Tower-Scott Street, or the Yonge Street Bridge. Ms. Barboza noted that lands owned or controlled by an Ontario Ministry or Prescribed Public Body on	Levels of heritage protection for Union Station (65-71 Front Street) and nearby provincial heritage properties noted in the HIA. Direction regarding obligations of prescribed public bodies under the Standards and Guidelines for the Conservation of Provincial Heritage Properties noted and used to guide the preparation of HIAs.



Individuals/Groups Engaged	Method	Results	Incorporation into HIA
		<p>behalf of the Crown may have responsibilities under the Standards and Guidelines for the Conservation of Provincial Heritage Properties (MTC 2010).</p> <p>Communication with Karla Barboza continued in April 2021 when WSP contacted her to request input on the HIAs being prepared for this project. Ms. Barboza requested to review the CHR before providing input on the HIAs.</p>	
Laura Hatcher, Heritage Planner, MCM	-Response received on December 18, 2020	As part of the ongoing communication with Karla Barboza outlined above, Laura Hatcher, Heritage Planner, at the MCM provided the Statements of Cultural Heritage Value (SCHV) for Union Station (65-71 Front Street), the URSC Scott Street Interlocking Tower, and the Yonge Street Bridge. All three SCHV had been approved by the Metrolinx Heritage Committee.	SCHV for Union Station (65-71 Front Street) used to form WSP's understanding of the Union Station property.



8.0 Recommendations

The TTC is undertaking PDE to produce a Baseline Design (approximately 30% design completion) of a new TTC WELRT system (the Project). The WELRT, goes under Bay Street, from Front Street to Queens Quay West, and then heads easterly on Queens Quay West. A new portal on Queens Quay West between Bay Street and Yonge Street will be constructed to provide a transition from an underground LRT to an at-grade LRT, which will complement the existing west portal and at-grade LRT on Queens Quay West, west of Bay Street.

The WELRT will service Toronto's waterfront revitalization area by providing fast, reliable transit service in the EBF Area of the Waterfront. The expansion of the Union LRT and Queens Quay LRT Stations is required to accommodate the additional streetcar lines and passenger volume. This project is critical to the new waterfront transit plan in the EBF Precinct.

WSP was retained by TTC to complete an HIA for the Union Station HCD in support of the WELRT. This HIA represents one deliverable to support program delivery. This HIA is being carried out under the Transit Project Assessment Process (TPAP) and was prepared in accordance with the MCM guidance document titled *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties* (MCM 2017).

The purpose of this HIA is to establish the Statement of Cultural Heritage Value for the Union Station HCD, assess the existing conditions of the district, describe the purpose of the proposed activity, complete an impact assessment and outline considered alternatives and mitigation measures, provide a summary of community engagement, and development recommendations for the conservation of the property.

Based on the above, the following recommendations are made:

- 1) Street level/open cut excavation along Bay Street from Front Street West to Harbour Street within the Union Station HCD will directly impact the Bay Street right-of-way and the public realm immediately adjacent to it. Design guidelines within Section 8.3.5 of the [Union Station HCD Plan](#) should be adhered to when re-establishing the impacted public realm. Relevant design guidelines from the HCD Plan include:
 - a. Replacement lighting should be designed in a manner that accentuates or does not diminish the integrity of the heritage architecture within the HCD. Examples of successful lighting currently within the HCD should be considered in the design of replacement lighting. Replacement fixtures should be consistent throughout the Study Area.
 - b. Walking surface treatments should be consistent within the Study Area. The design that is currently most prominent within the HCD includes large concrete pavers flanked by smaller stone pavers lining the street edge and edge of structures. This should be emulated consistently throughout the Study Area. Interruptions to the cohesiveness of the walking surface in the form of high-quality finishing such as granite and tree planters are



acceptable interruptions to the cohesive streetscape that illustrate the significance of the HCD.

- c. Clutter should be limited throughout the HCD. Examples of successful street furniture currently within the HCD should be considered in the design of replacement street furniture. Replacement street furniture should be consistent throughout the Study Area. The street furniture should be composed of high-quality material and should be as inconspicuous and multi-use as possible to reduce clutter and diminishment of the surrounding of the heritage quality of the HCD.
 - d. The design and finishes of the teamway along the west side of Bay Street should be consistent with either the teamway on the east side of Bay Street or the teamway on the east side of York Street. Both areas are examples of finished teamways in good repair and can be used as design references to select the finishes, surface materials, and layout of the teamway on the west side of Bay Street.
- 2) This report should be submitted to the following agencies for review and comment:
- a. City of Toronto Heritage Preservation Services
 - b. MCM

The above recommendations were prepared using the best available information regarding potential impacts at the time of writing. Should the proposed work change, then the preliminary impact assessment should be revisited to confirm identified impacts and proposed mitigation measures.



9.0 Assessor Qualifications

This report was prepared and reviewed by the undersigned, employees of WSP. WSP is one of North America's leading engineering firms, with more than 50 years of experience in the earth and environmental consulting industry. The qualifications of the assessors involved in the preparation of this report are provided in **Appendix D**.



10.0 Closure

This report was prepared for the exclusive use of the TTC and is intended to provide Heritage Impact Assessment for the public realm of the Union Station HCD. The Study Area includes public realm of the Union Station HCD along Bay Street from Front Street West to Harbour Street.

Any use which a third party makes of this report, or any reliance on or decisions to be made based on it, are the responsibility of the third party. Should additional parties require reliance on this report, written authorization from WSP will be required. With respect to third parties, WSP has no liability or responsibility for losses of any kind whatsoever, including direct or consequential financial effects on transactions or property values, or requirements for follow-up actions and costs.

The report is based on data and information collected during the cultural heritage assessment conducted by WSP. It is based solely a review of historical information, a property reconnaissance conducted in December 2019 and data obtained by WSP as described in this report. Except as otherwise maybe specified, WSP disclaims any obligation to update this report for events taking place, or with respect to information that becomes available to WSP after the time during which WSP conducted the cultural heritage assessment. In evaluating the Study Area, WSP has relied in good faith on information provided by other individuals noted in this report. WSP has assumed that the information provided is factual and accurate. In addition, the findings in this report are based, to a large degree, upon information provided by the current owner/occupant. WSP accepts no responsibility for any deficiency, misstatement or inaccuracy contained in this report as a result of omissions, misinterpretations or fraudulent acts of persons interviewed or contacted.

WSP makes no other representations whatsoever, including those concerning the legal significance of its findings, or as to other legal matters touched on in this report, including, but not limited to, ownership of any property, or the application of any law to the facts set forth herein. With respect to regulatory compliance issues, regulatory statutes are subject to interpretation and change. Such interpretations and regulatory changes should be reviewed with legal counsel.

We trust that the information presented in this report meets your current requirements. Should you have any questions, or concerns, please do not hesitate to contact the undersigned.



Respectfully Submitted,
WSP E&I Canada Limited,

Prepared by:

Reviewed by:

Luke Fischer, MA, CAHP
Cultural Heritage Specialist

Heidy Schopf, MES, CAHP
Cultural Heritage Team Lead



11.0 References

City of Toronto

- 2006 *Union Station Heritage Conservation District Plan, Appendix 4*. Available Online: https://www.toronto.ca/wp-content/uploads/2018/01/8dfc-CityPlanning_Union-Station-HCD.pdf. Retrieved 31 December 2020
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Government of Ontario

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WSP

2020 *OISO52004-REP-019 R2 CDRS Property Impacts Report, CDRS Property Impacts Report, Union Station – Queen’s Quay Station, Project # OISO52004*. Document on file at WSP.

Appendix A: Designation By-law for the Union Station Heritage Conservation District



An agency of the Government of Ontario



Un organisme du gouvernement de l'Ontario

This document was retrieved from the Ontario Heritage Act Register, which is accessible through the website of the Ontario Heritage Trust at **www.heritagetrust.on.ca**.

Ce document est tiré du registre aux fins de la *Loi sur le patrimoine de l'Ontario*, accessible à partir du site Web de la Fiducie du patrimoine ontarien sur **www.heritagetrust.on.ca**.

Authority: Toronto and East York Community Council Report 5, Clause 2, as adopted by City of Toronto Council on June 27, 28 and 29, 2006, and Notice of Motion J(47), moved by Councillor Milczyn, seconded by Councillor Rae, as adopted by City of Toronto Council on July 25, 26 and 27, 2006

Enacted by Council: July 27, 2006

CITY OF TORONTO

BY-LAW No. 634-2006

To designate the Union Station area as a heritage conservation district.

WHEREAS City Council has authorized the establishment of the Union Station Heritage Conservation District and the adoption of the Union Station Heritage Conservation District Plan;

The Council of the City of Toronto HEREBY ENACTS as follows:

1. The area outlined by the heavy lines shown on the map attached as Schedule "A" to this by-law is designated as a heritage conservation district under Part V of the *Ontario Heritage Act*, R.S.O. 1990, chapter O.18.
2. The Union Station Heritage Conservation District Plan, dated April, 2006 and prepared by ERA Architects Inc is adopted.
3. A copy of the heritage district conservation plan is available for inspection at the offices of the City of Toronto Planning Division, Heritage Preservation Services, City Hall, 100 Queen Street West, Toronto, Ontario.

ENACTED AND PASSED this 27th day of July, A.D. 2006.

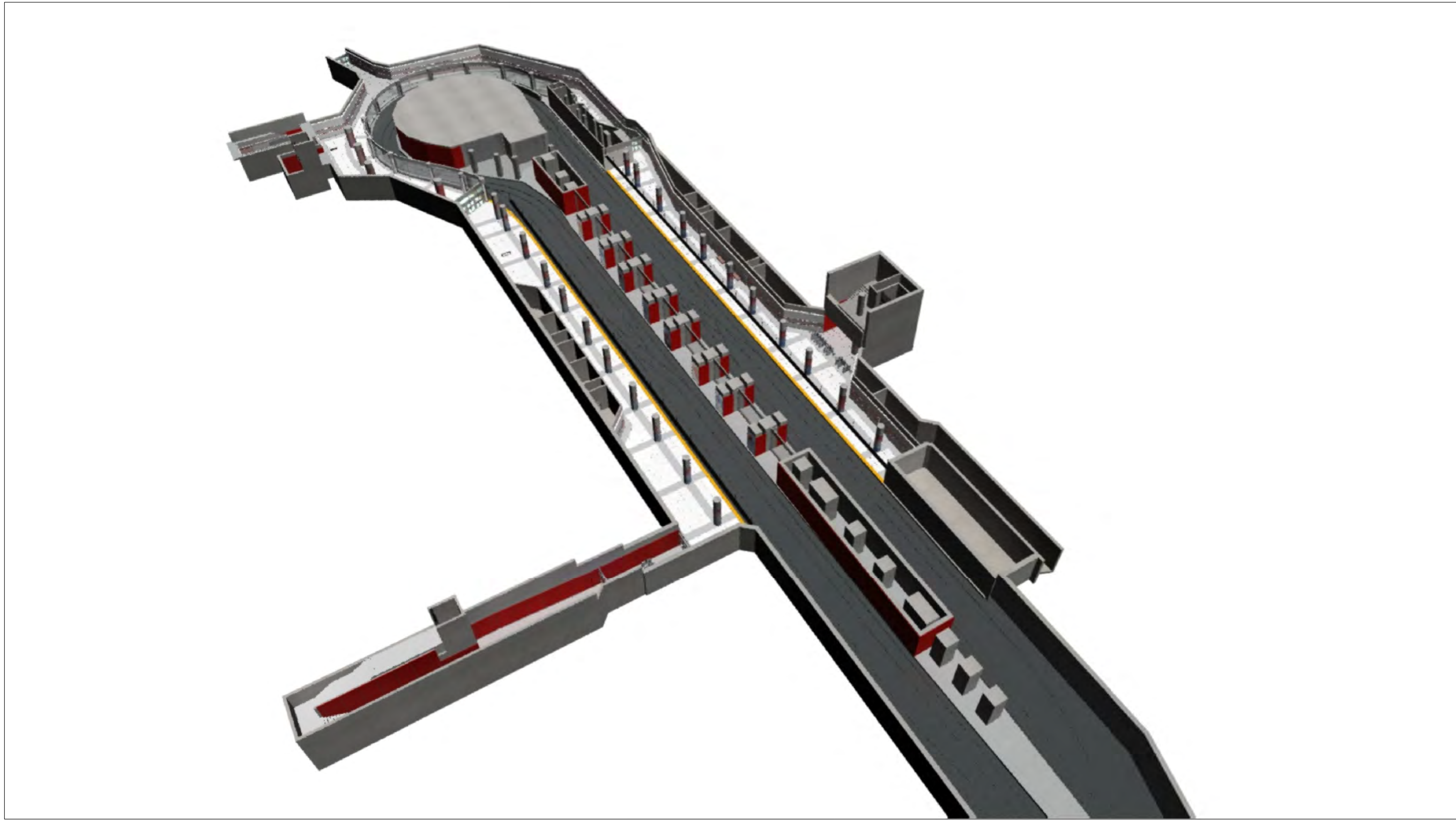
DAVID R. MILLER,
Mayor

ULLI S. WATKISS
City Clerk

(Corporate Seal)

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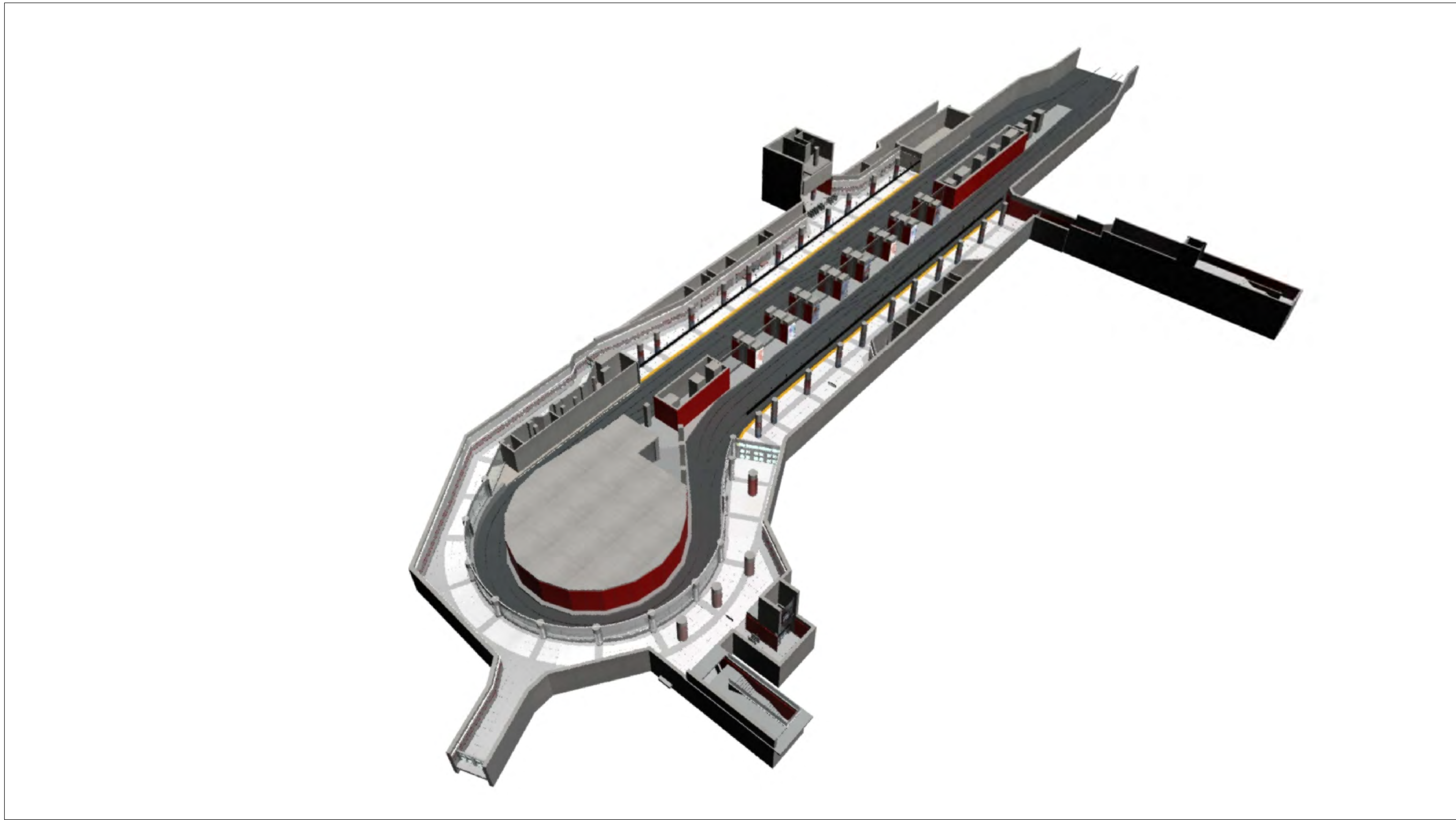
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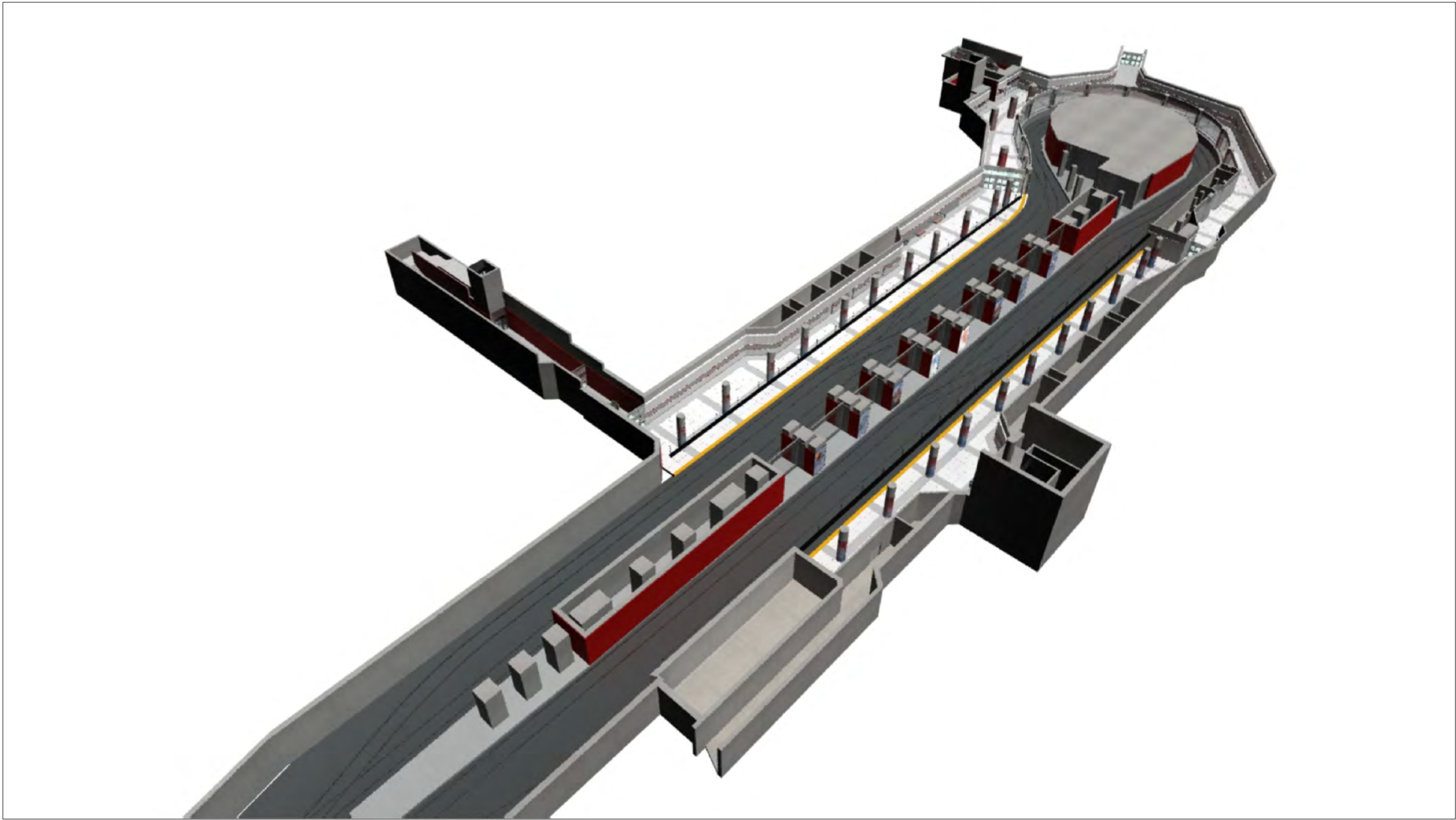
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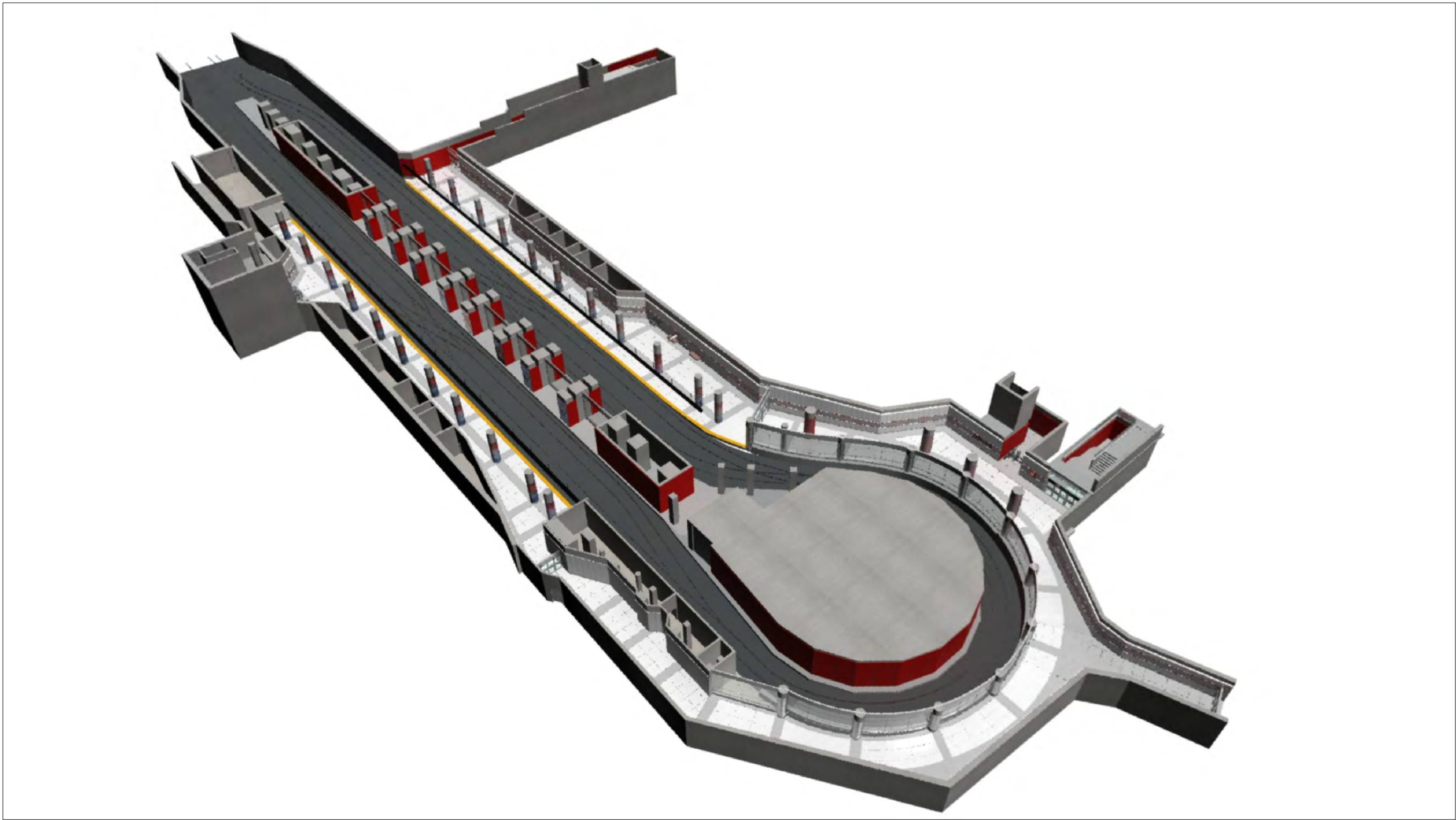
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WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS
3450 HARVESTER ROAD, SUITE 100
BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 336 2263

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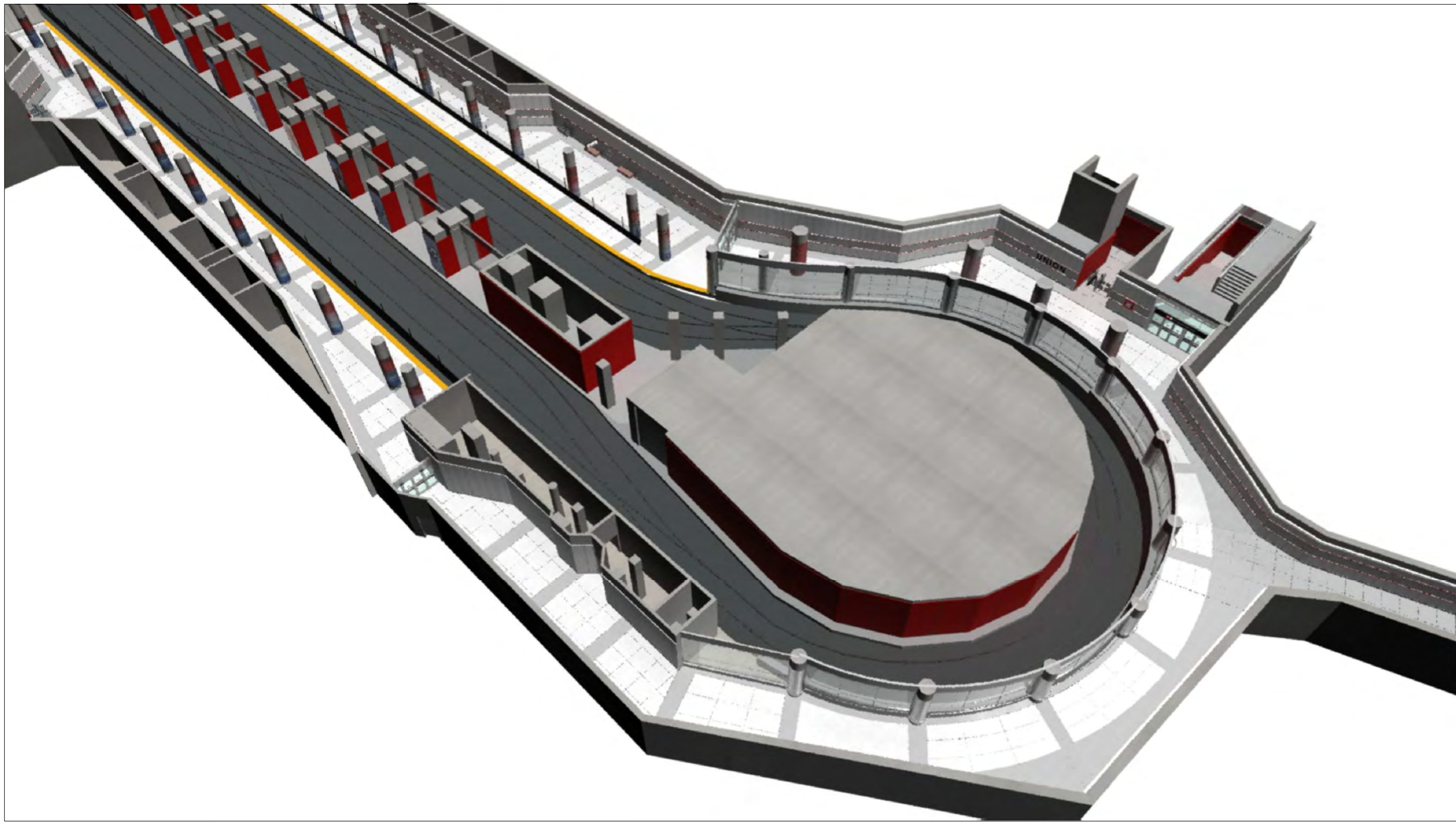
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AERIAL VIEW RENDERS

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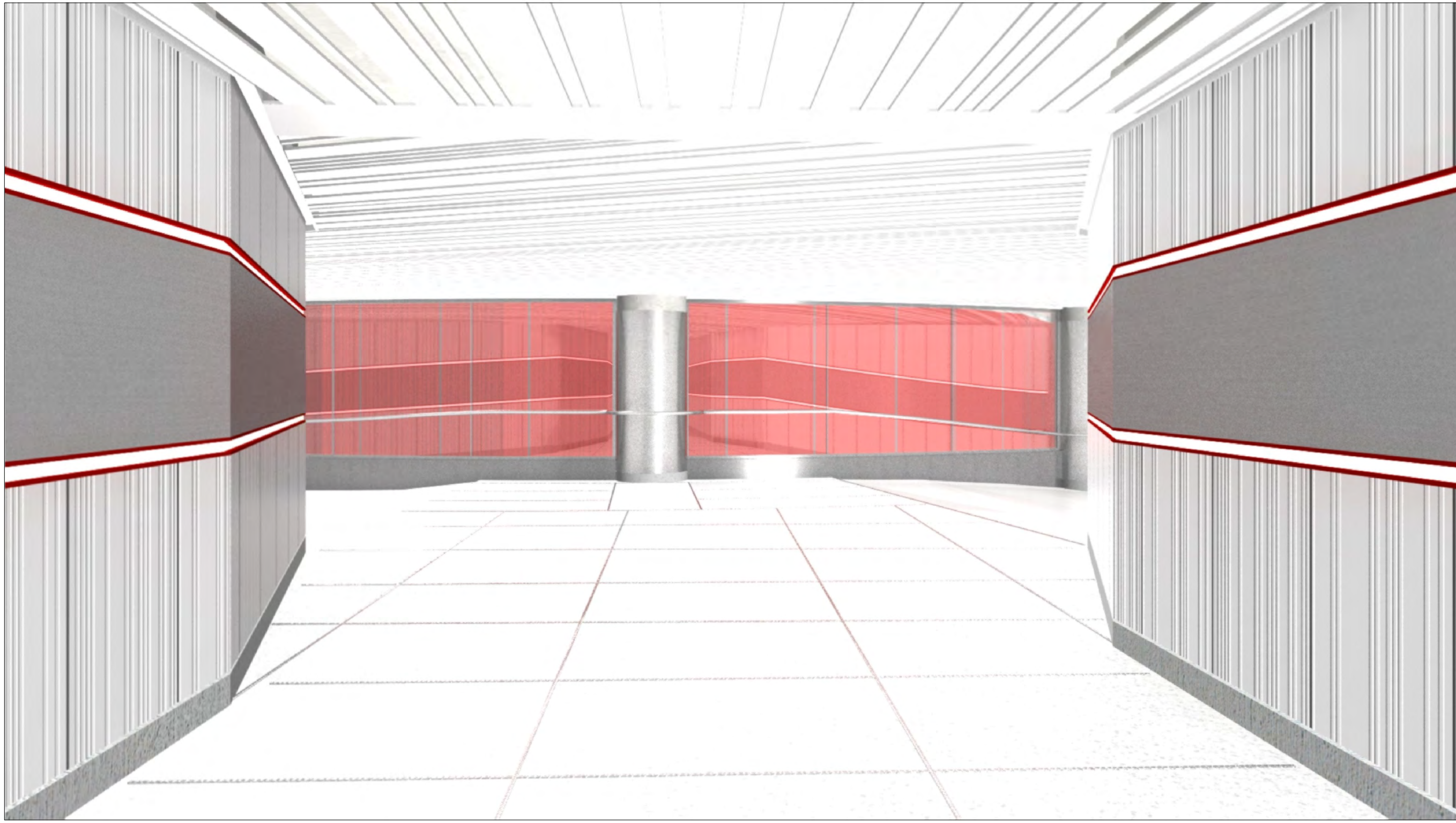
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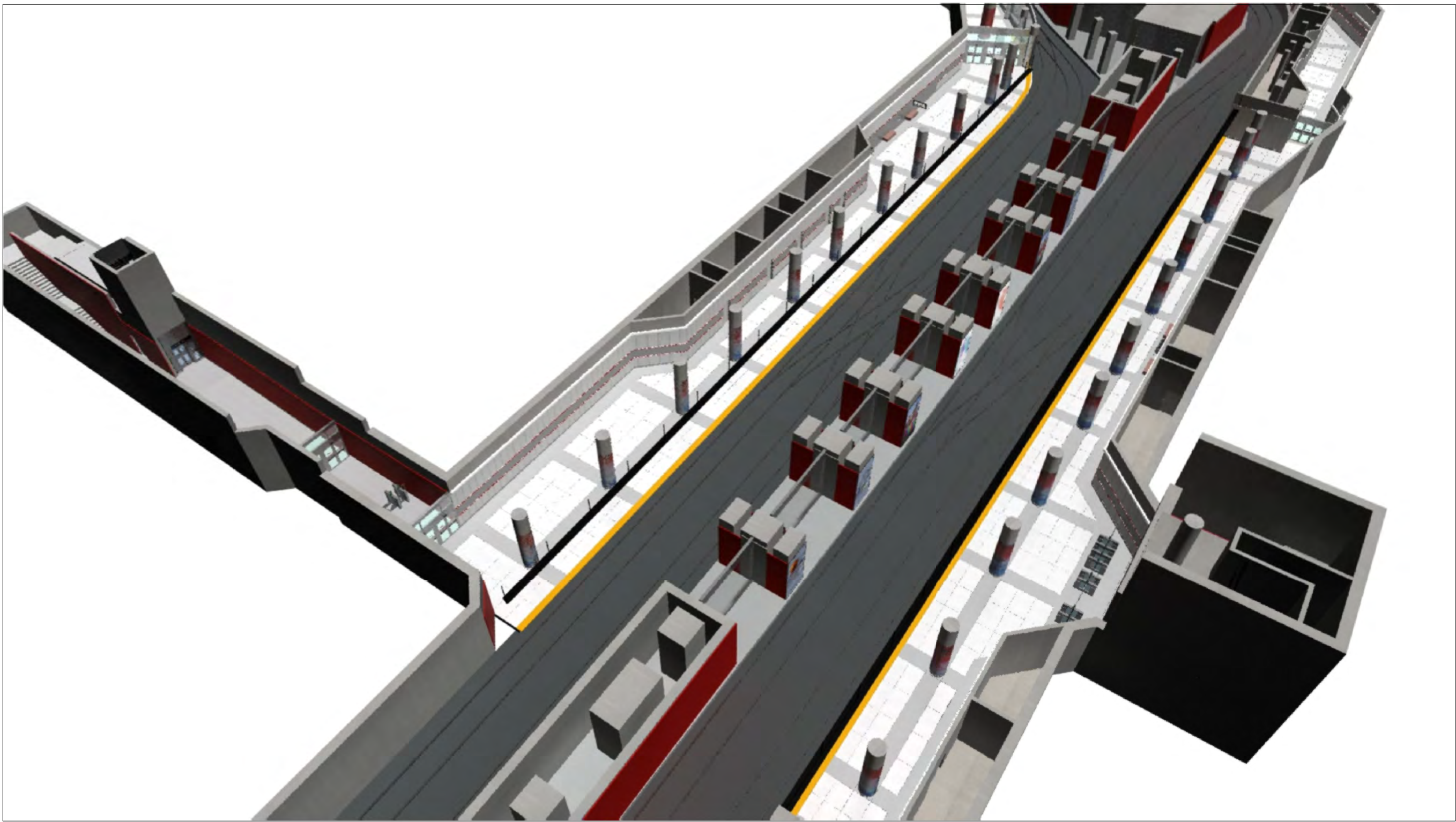
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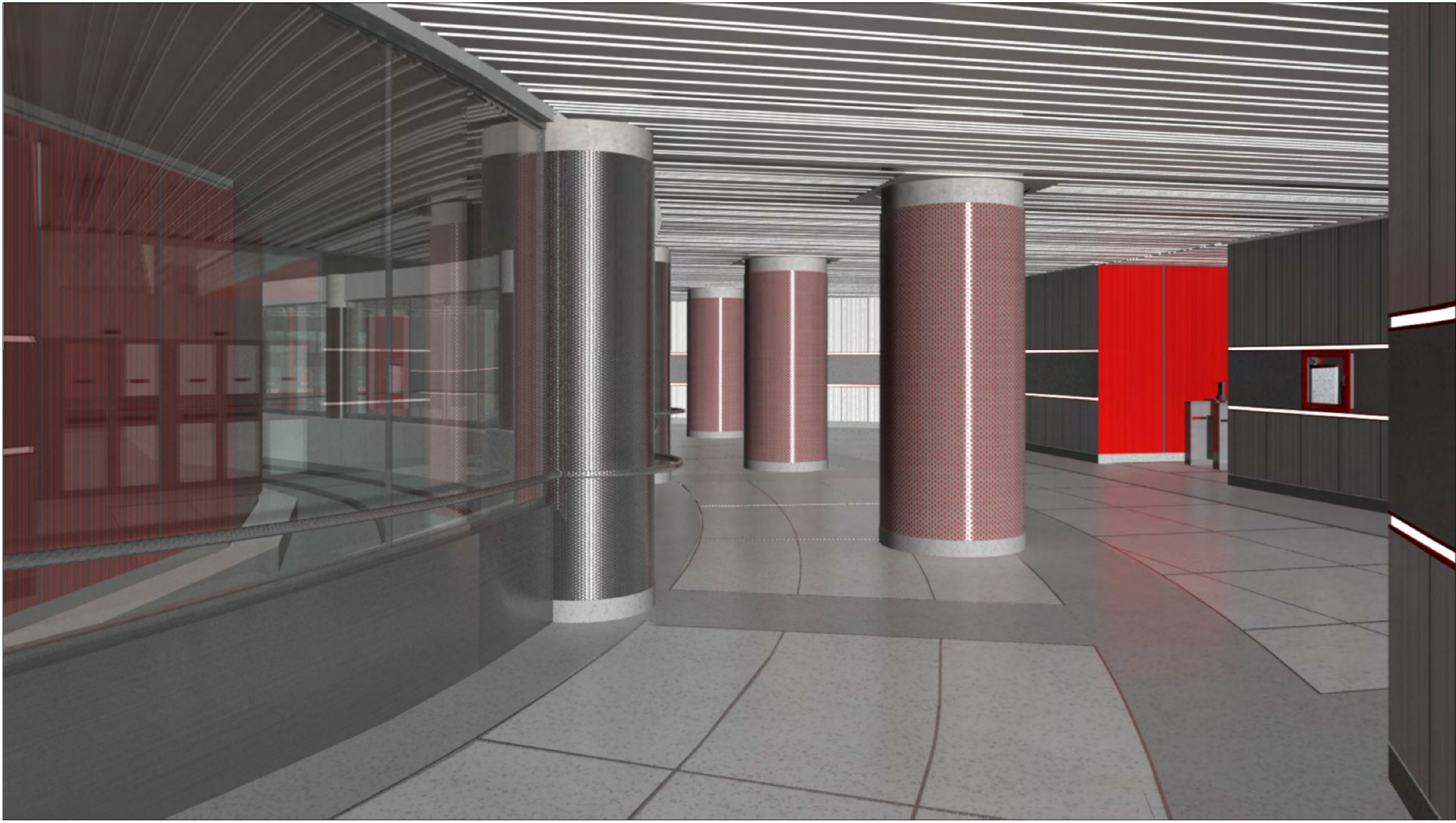
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WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS
3450 HARVESTER ROAD, SUITE 100
BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 336 2263

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UNION STATION

PLATFORM LEVEL VIEWS
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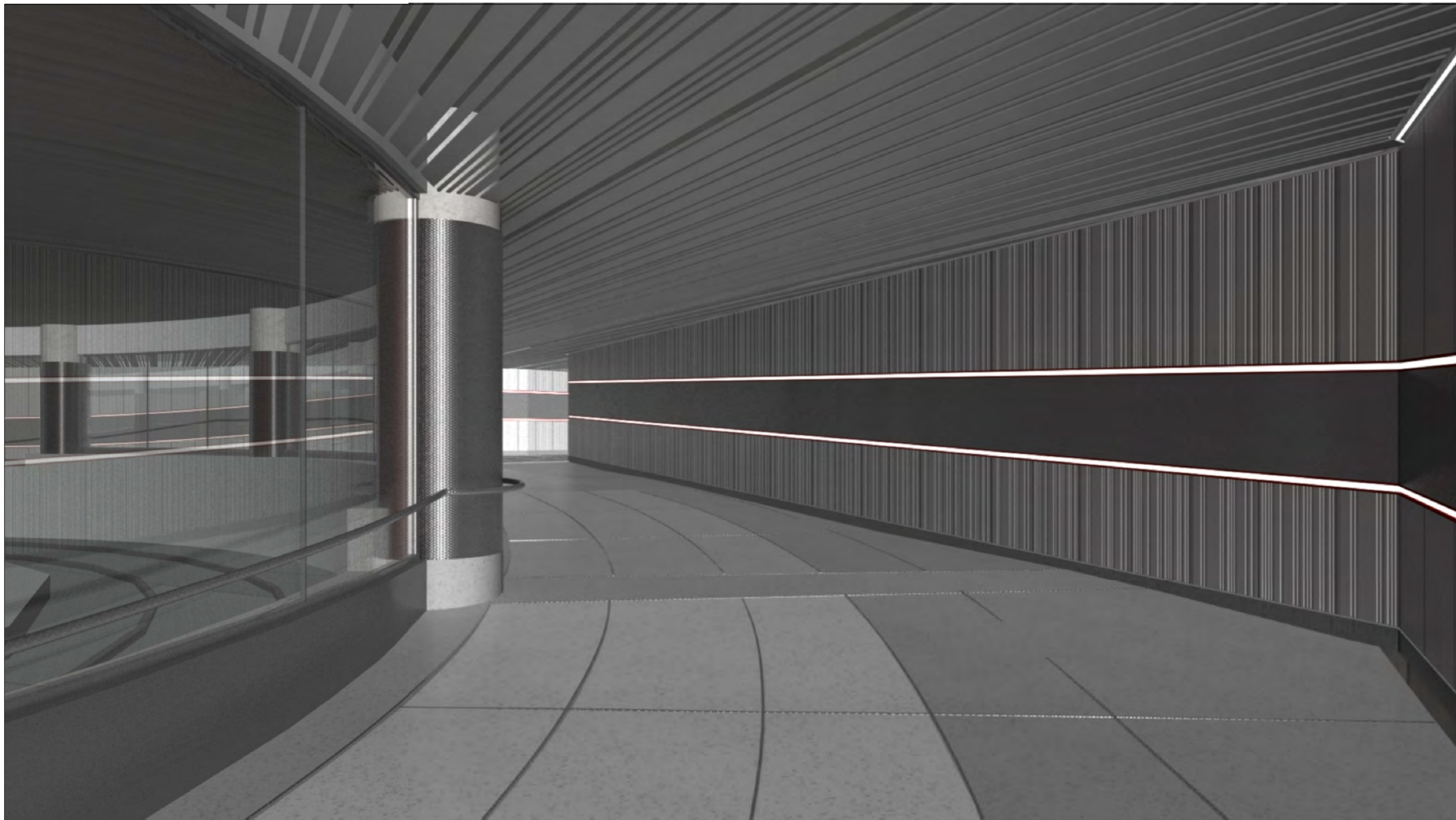
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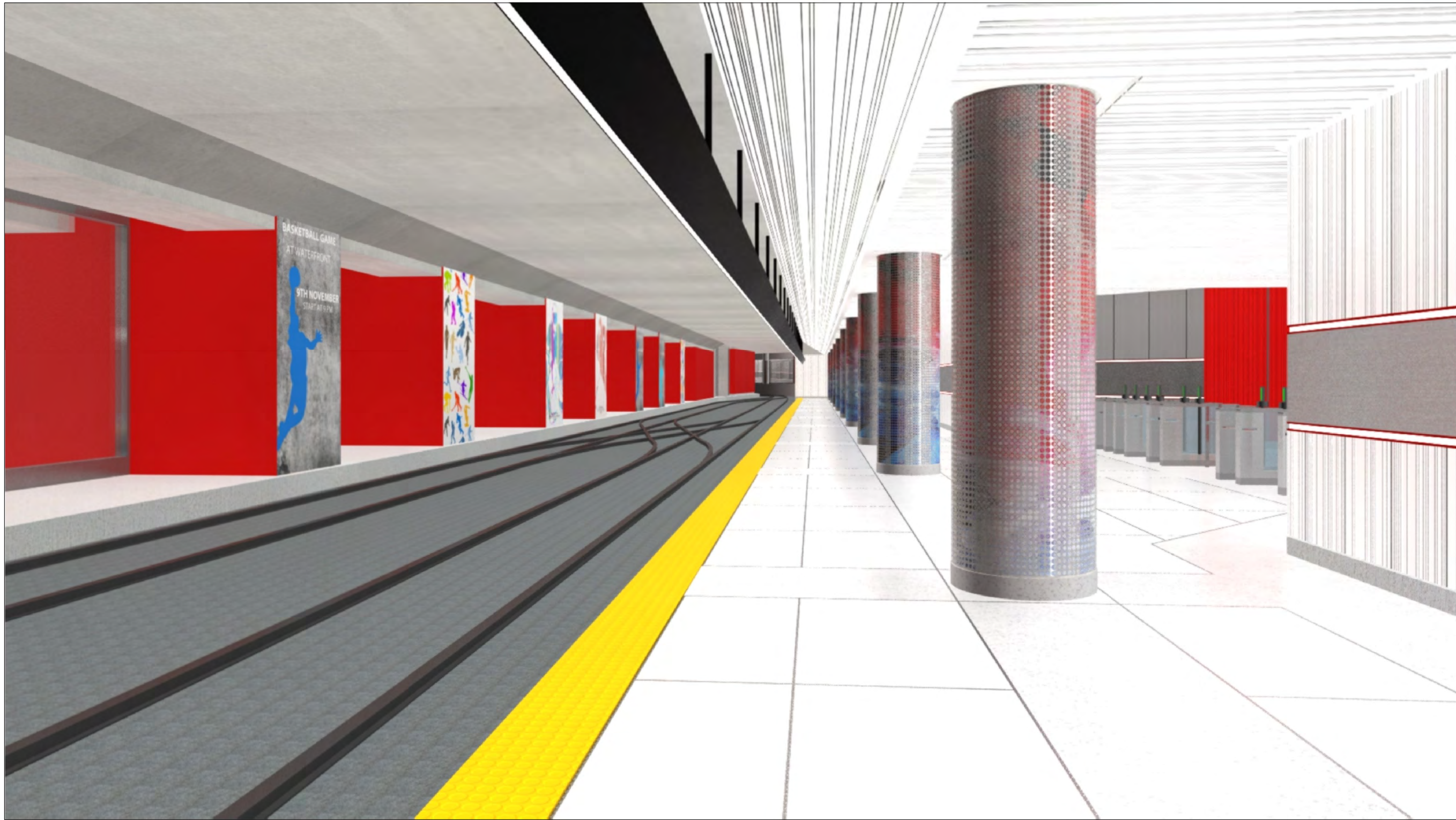
DRAWING No.



DETAIL

3D RENDER

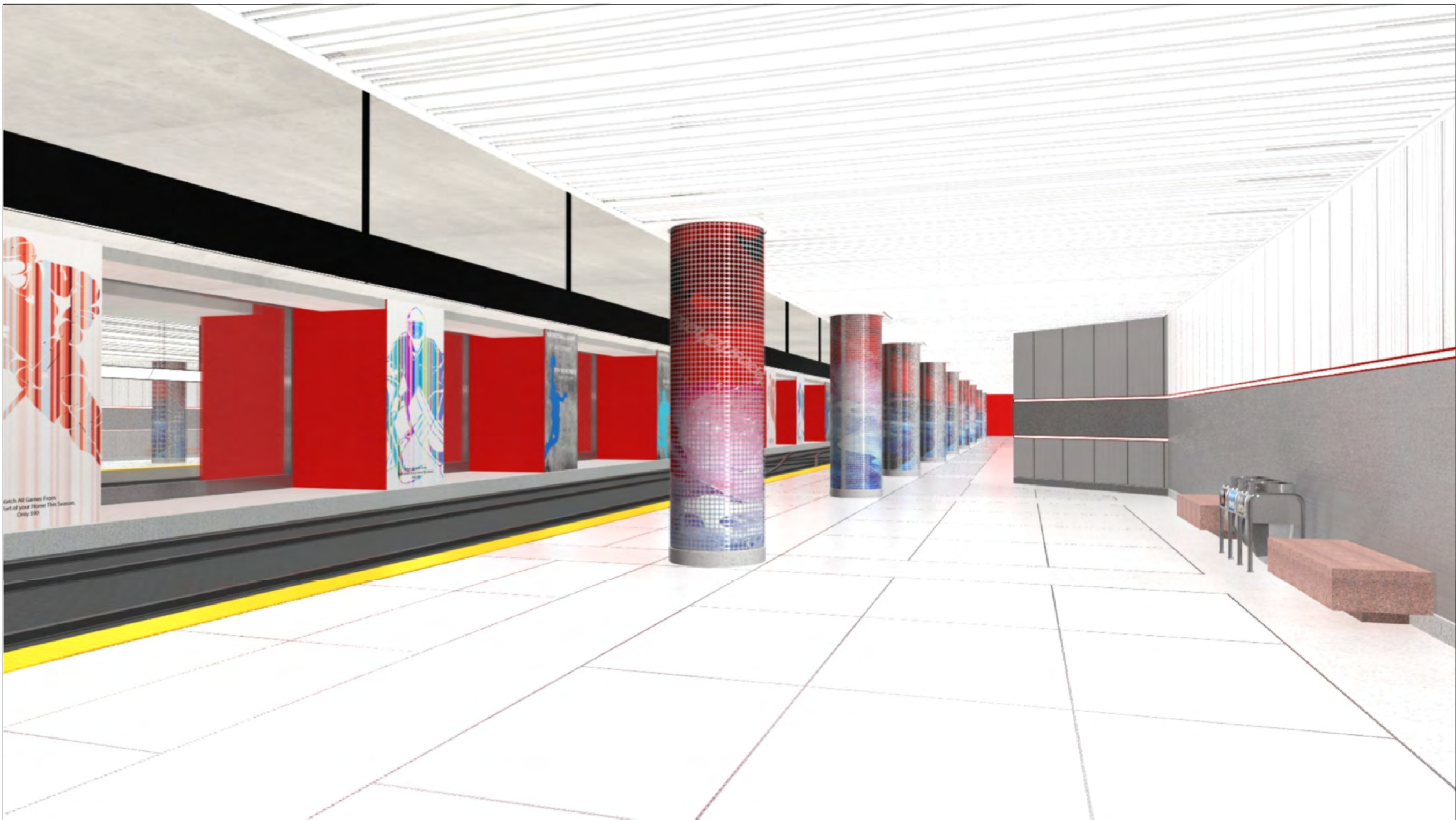
3
AA013



DETAIL

3D RENDER

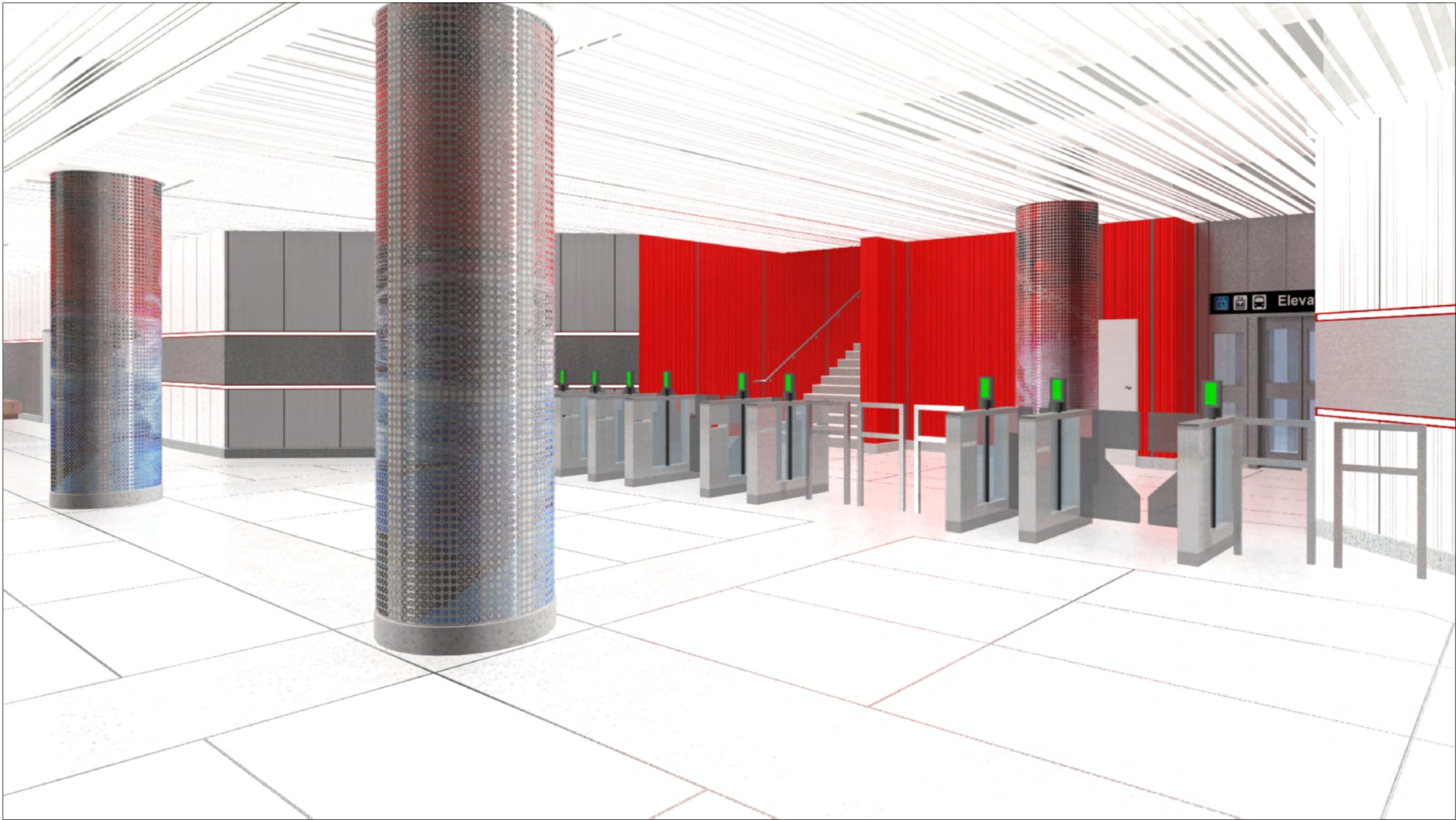
4
AA013



DETAIL

3D RENDER

1
AA013



DETAIL

3D RENDER

2
AA013

REVISIONS	REVISIONS
△	△
△	△
△	△

30% REVIEW
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NOT FOR CONSTRUCTION

wood.

WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS
3450 HARVESTER ROAD, SUITE 100
BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 336 2263

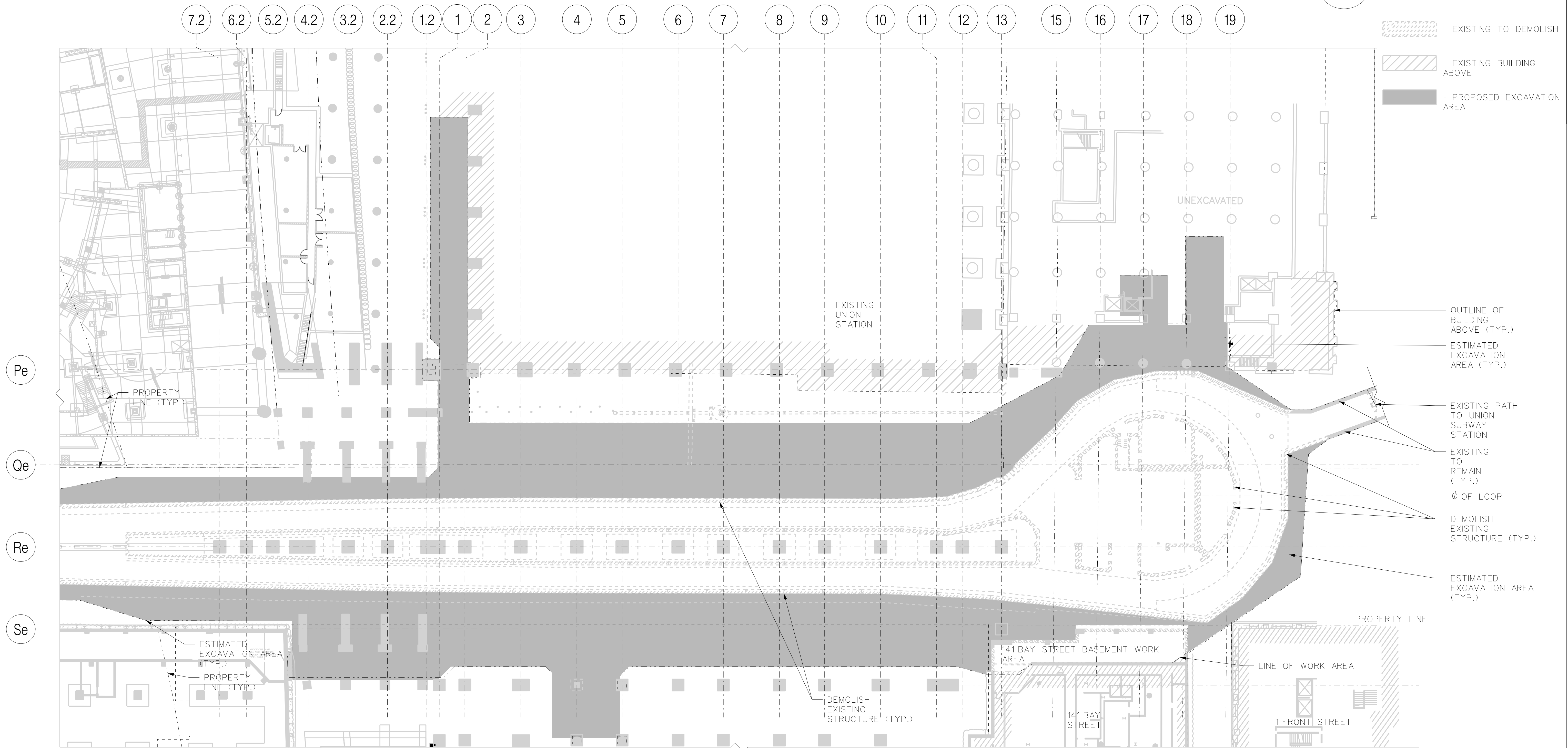
DRAWN	E.ENGLAND	YYYY/MM/DD
CHECKED	O.PHELAN	2020/10/23
CORRECT	J.STRASMAN	
SCALE	N.T.S.	

WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK

UNION STATION

PLATFORM LEVEL VIEWS
3D AXONOMETRIC RENDERERS

Contract: RE35-1		
TORONTO TRANSIT COMMISSION ENGINEERING DEPARTMENT		
Dwg. No.	Rev. No.	Sheet No.
RE35-1-AA013		
Plot Date: 7-JUL-2021		



PLAN

UNION STATION STREETCAR LOOP PLATFORM LEVEL
DEMOLITION PLAN

1

AD001

REVISIONS	REVISIONS
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△	△
△	△

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BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 335 2353

DRAWN	E.ENGLAND	YYYY/MM/DD
CHECKED	O.PHELAN	2020/10/17
CORRECT	J.STRASMAN	

SCALE



WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK

UNION STATION

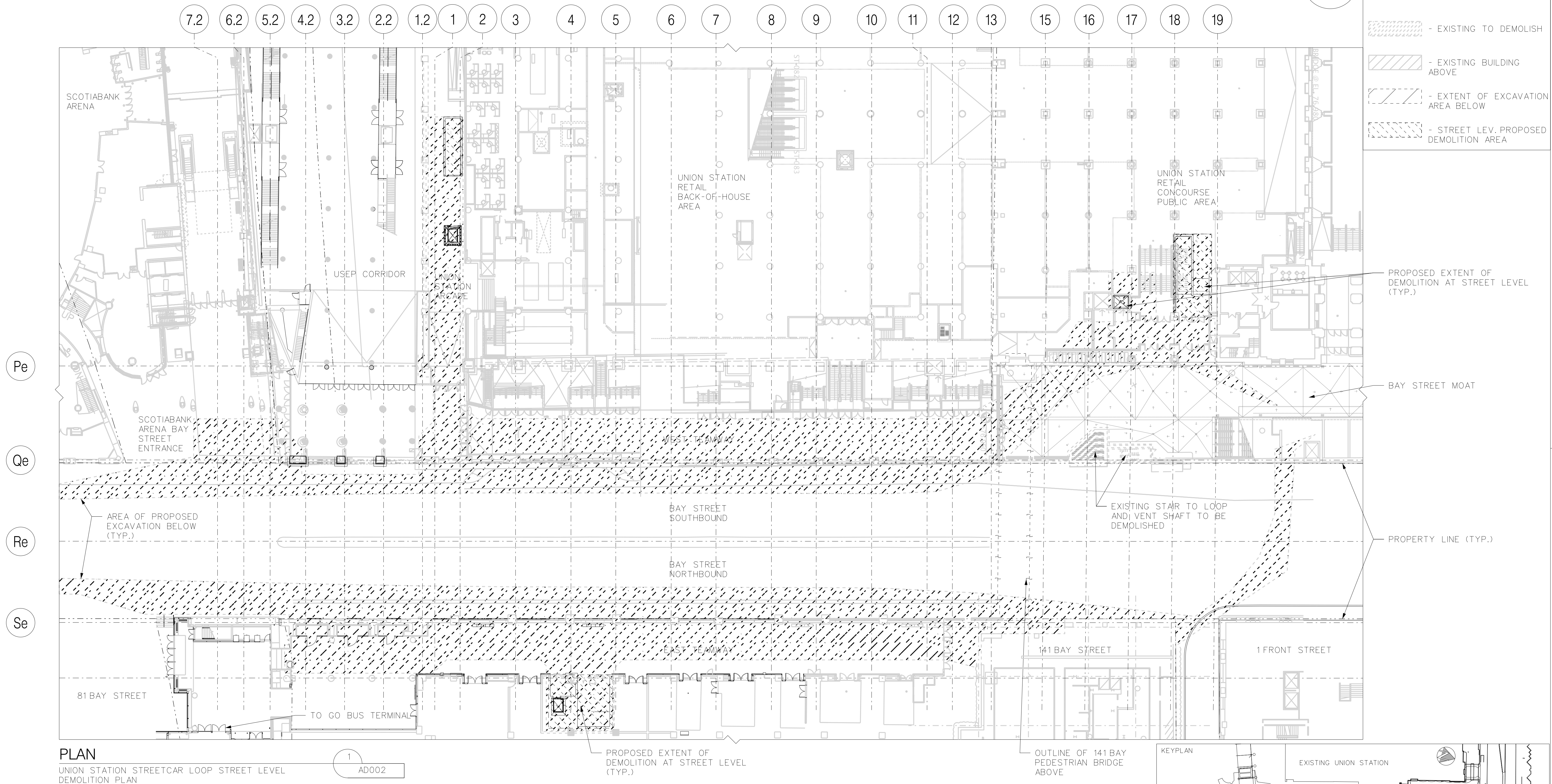
PLATFORM LEVEL DEMOLITION PLAN

Contract: RE35-1

TORONTO TRANSIT COMMISSION
ENGINEERING DEPARTMENT

Dwg. No.	Rev. No.
RE35-1-AD001	

Plot Date: 7-JUL-2021



REVISIONS	REVISIONS
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BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 335 2353

DRAWN E.ENGLAND
 CHECKED O.PHELAN
 CORRECT J.STRASMAN

SCALE



0 2 4 6 8 10 12m

WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK

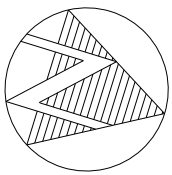
UNION STATION
STREET LEVEL DEMOLITION
PLAN

Contract: RE35-1

TORONTO TRANSIT COMMISSION
ENGINEERING DEPARTMENT

Dwg. No.	Rev. No.	
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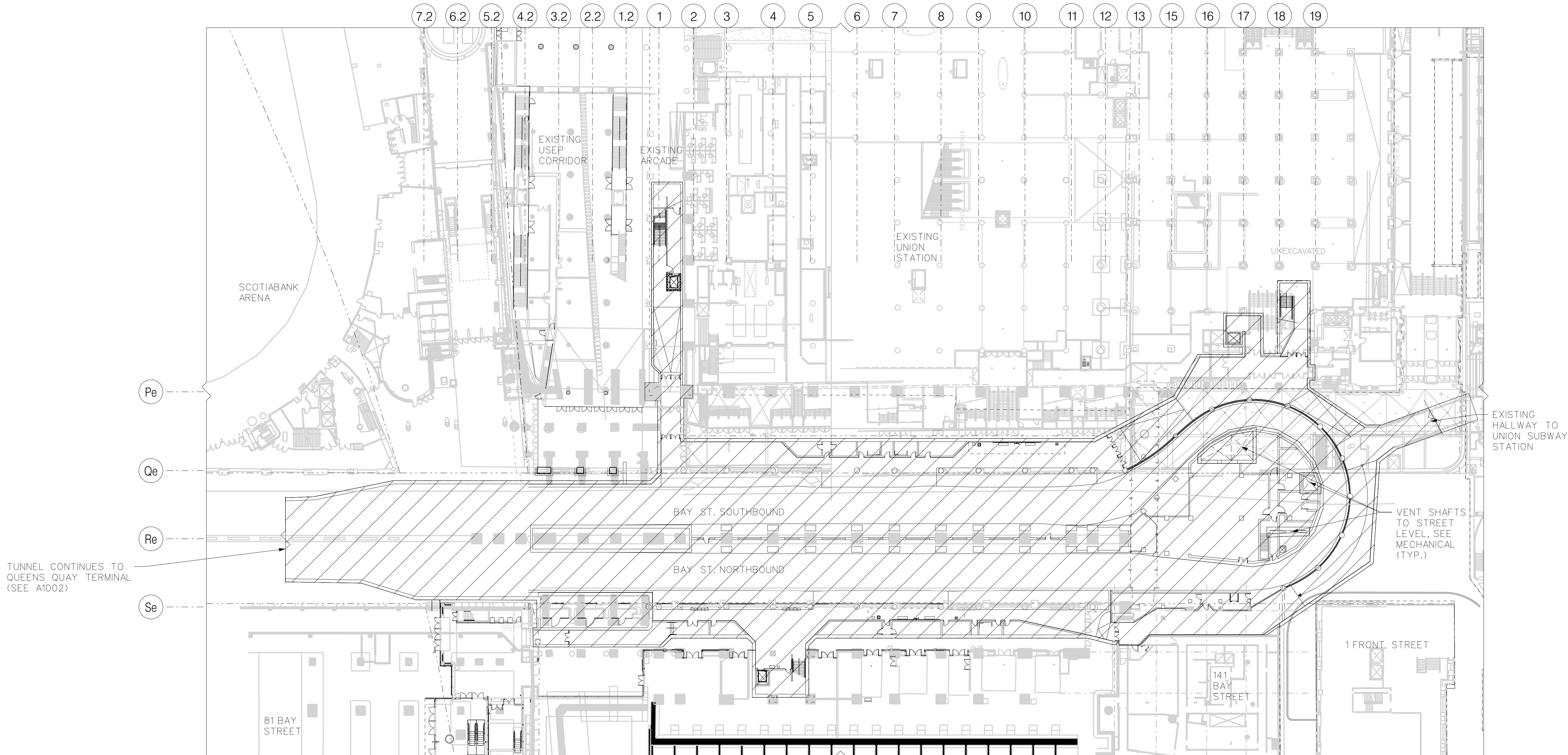
Plot Date: 7-JUL-2021



LEGEND

- EXISTING TO REMAIN

- NEW CONSTRUCTION

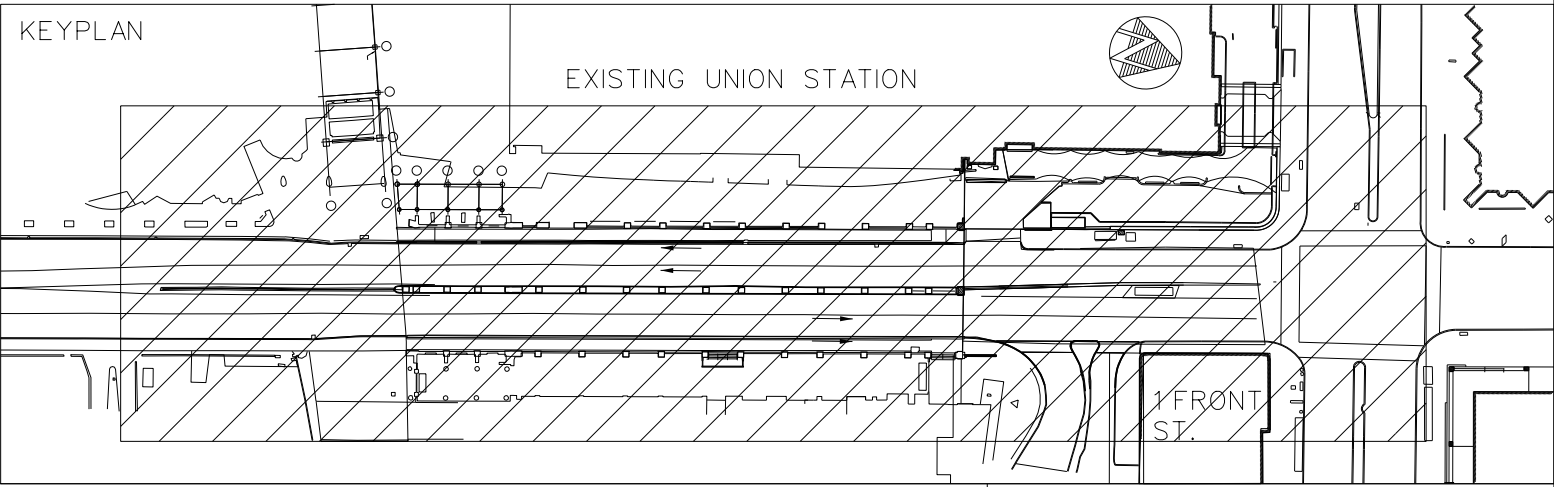


PLAN

UNION STATION STREETCAR LOOP - STREET LEVEL

1
A1001

NP-A NP-B NP-C NP-D



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△	△
△	△

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wood.

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BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 336 2263

DRAWN E. ENGLAND
CHECKED O. PHELAN
CORRECT J. STRASMAN

SCALE
0 2 4 6 8 10 15m

WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK

UNION STATION

SITE PLAN

Contract: RE35-1

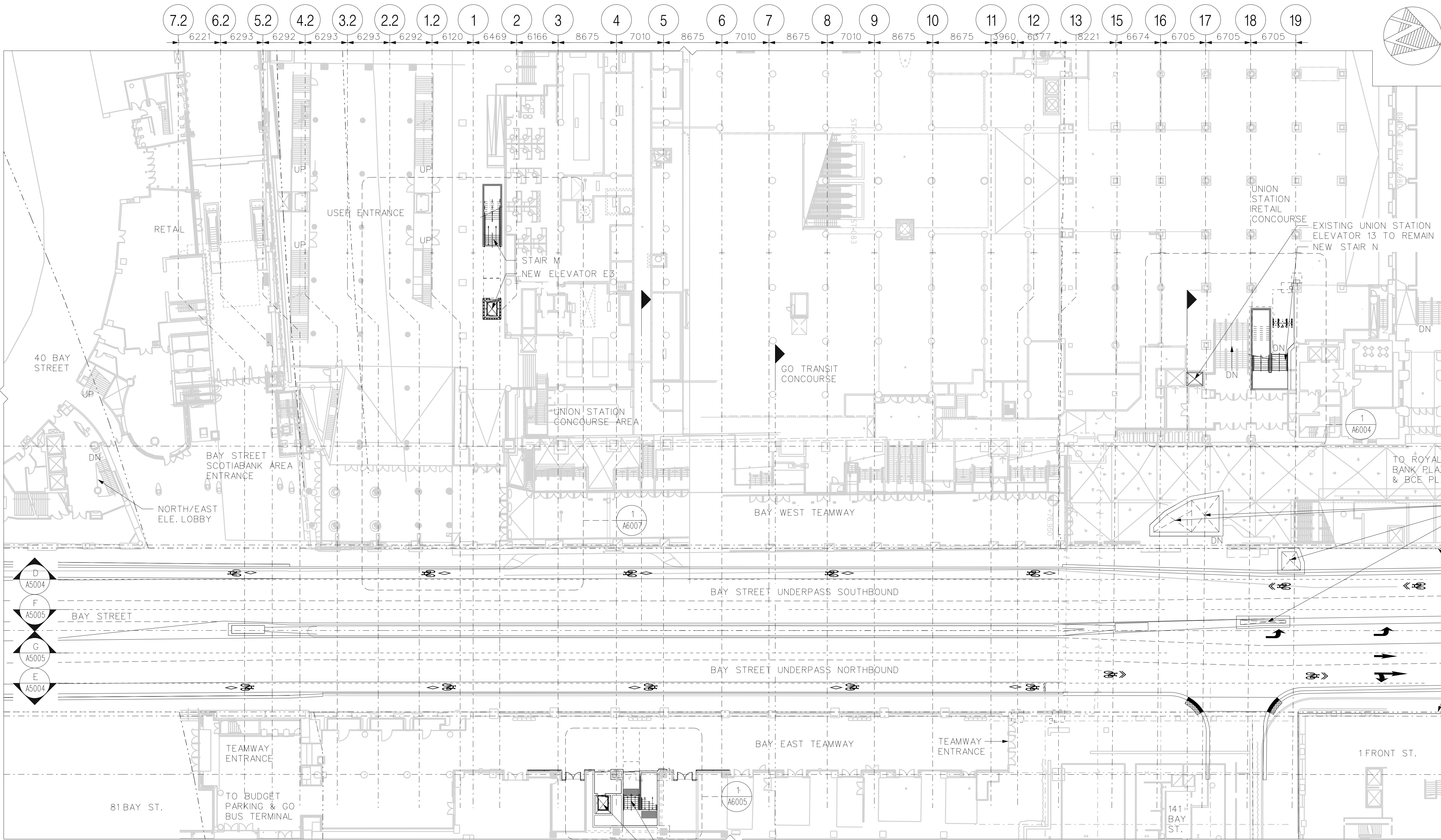


TORONTO TRANSIT COMMISSION
ENGINEERING DEPARTMENT

Dwg. No. Rev. No. Sheet No.
RE35-1-A1001

Plot Date: 7-JUL-2021

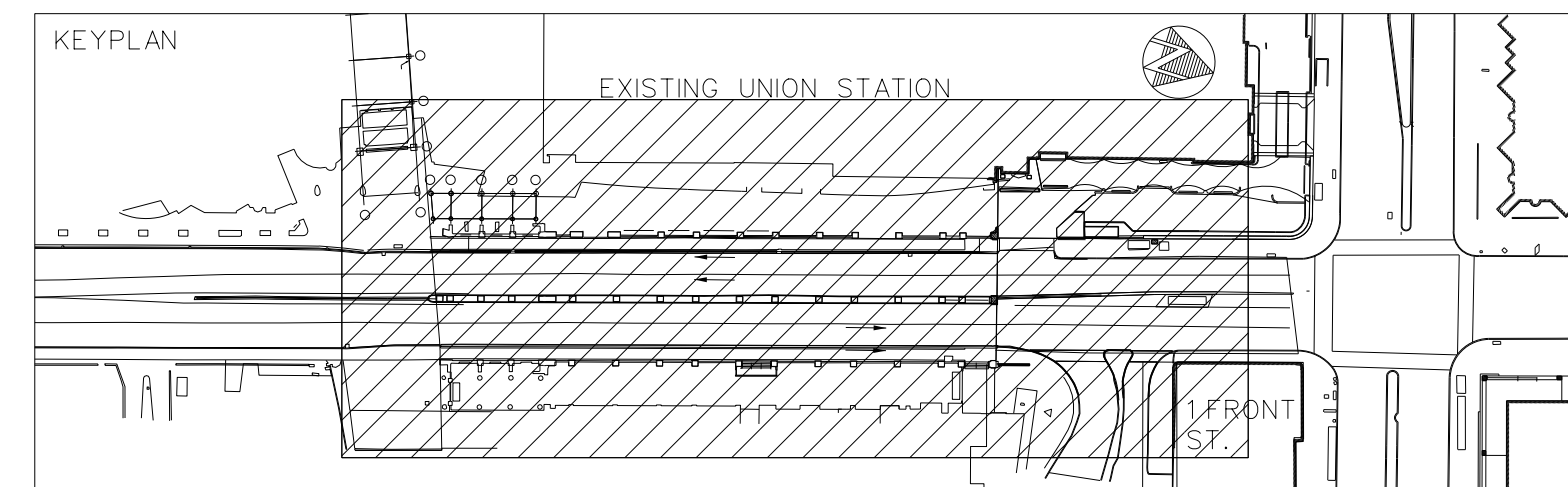
CADD FILE NAME: SAI_2s_pln_st_01.dgn
engborc3.dgn (BORDER) FEBRUARY 2, 2020
BLDG. REF. No.
FILENAME:
SHEET No.
DRAWING No.



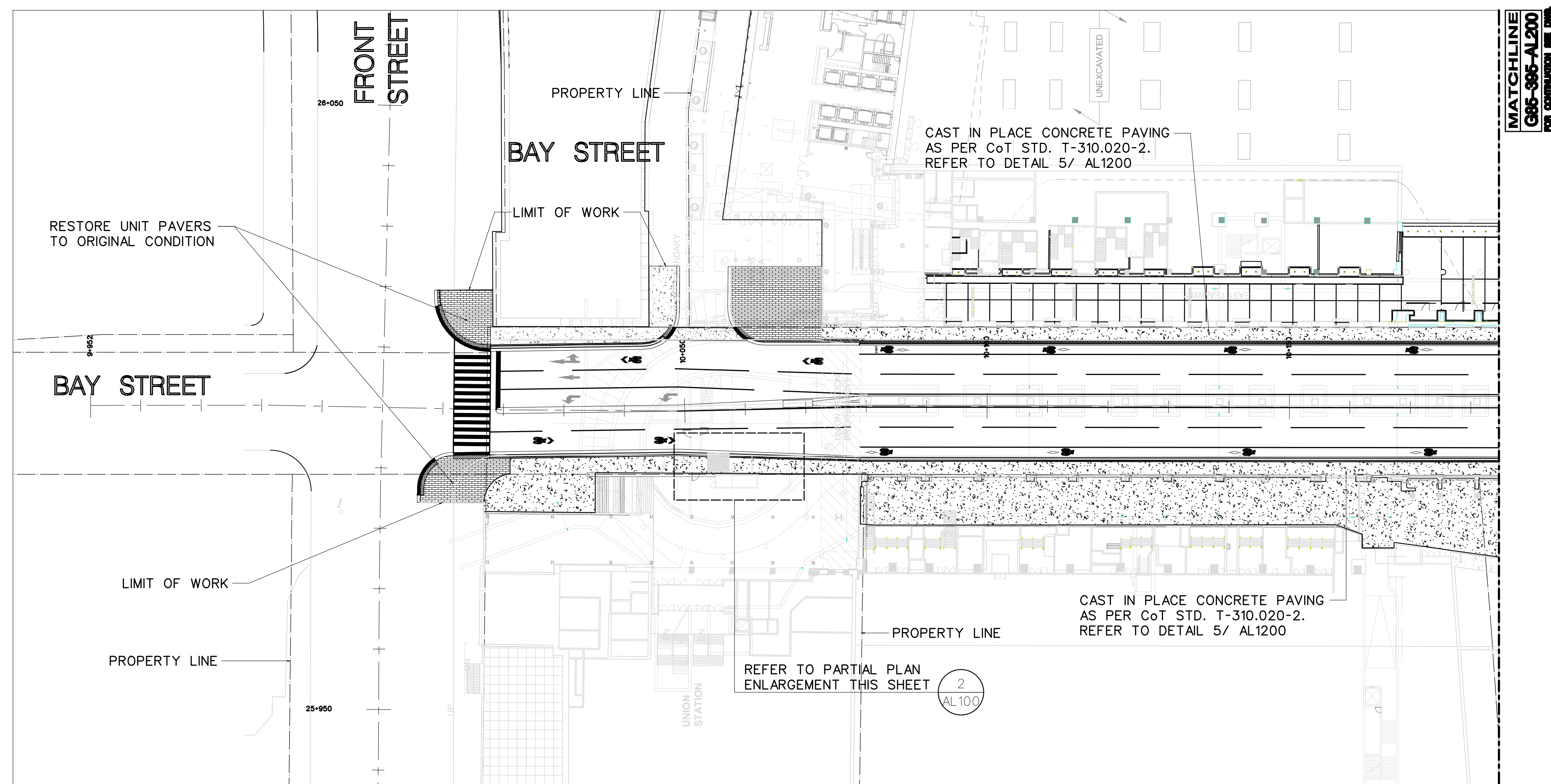
LEGEND

- EXISTING TO REMAIN
- NEW CONCRETE
- PROPERTY LINE
- EXISTING BUILDING

PLAN
UNION STATION STREETCAR LOOP - STREET LEVEL
1 A2002



REVISIONS	REVISIONS	30% REVIEW DRAWING NOT FOR CONSTRUCTION	wood. WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS 3450 HARVESTER ROAD, SUITE 100 BURLINGTON, ONTARIO, CANADA L7N 3W5 TEL: +1 905 336 2583	DRAWN E. ENGLAND CHECKED O. PHELAN CORRECT J. STRASMAN SCALE 0 2 4 6 8 10 12m	WATERFRONT EAST LRT UNION STATION QUEENS QUAY LINK UNION STATION STREET LEVEL PLAN	Contract: RE35-1 TORONTO TRANSIT COMMISSION ENGINEERING DEPARTMENT Dwg. No. RE35-1-A2002 Rev. No. Sheet No. Plot Date: 7-JUL-2021



BAY STREET & FRONT STREET WEST

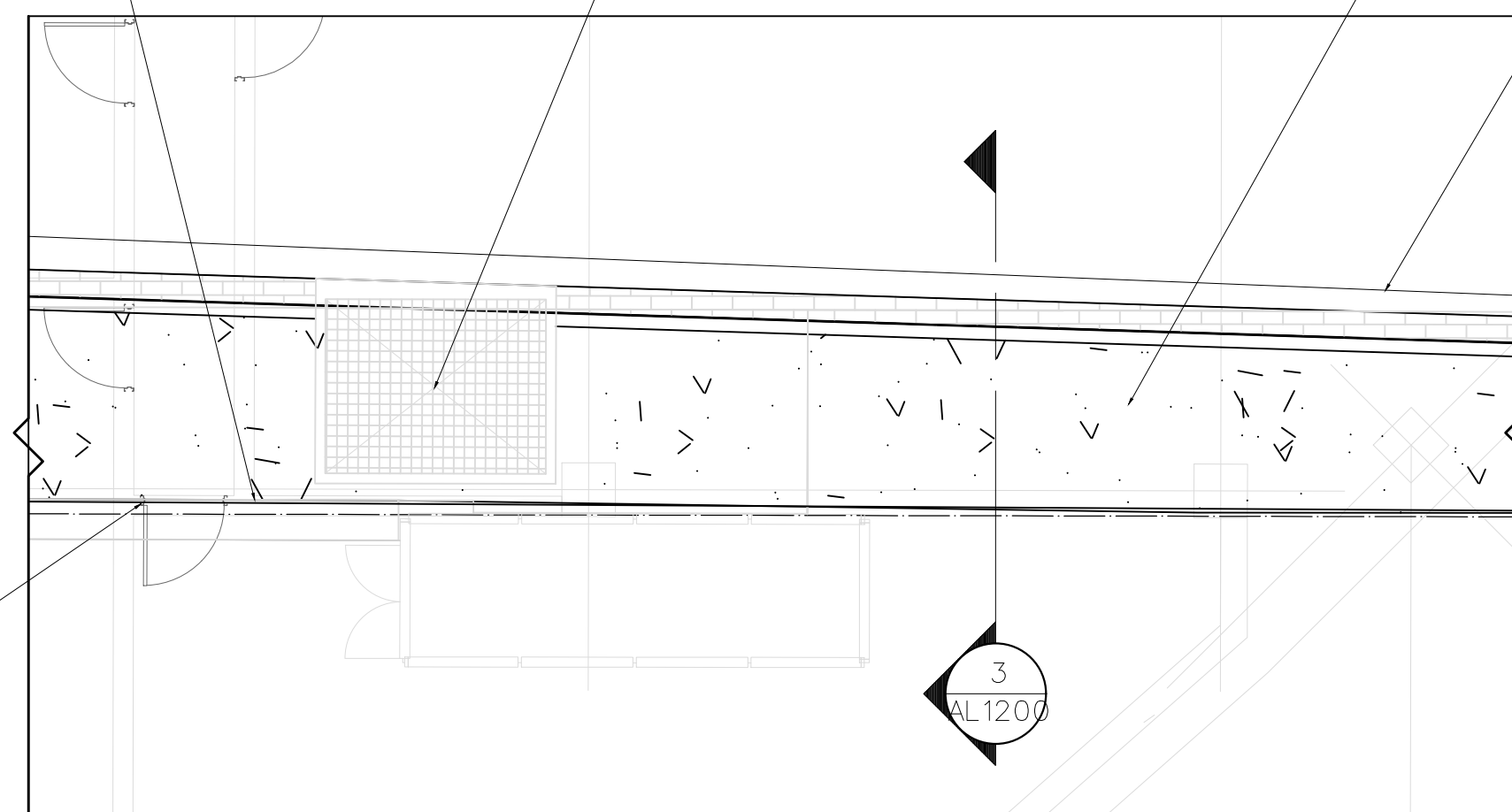


RESTORE ALL PAVING, SIGNAL
WALLS, STAIRWELLS AND
SHADE STRUCTURE CANOPY
AT LOWER LEVEL

— METAL GRATE AIR SHAFT
FLUSH WITH GRADE
REFER TO ARCH. DWGS.

CAST IN PLACE
CONCRETE PAVING
AS PER CoT STD.
T-310.020-2. REFER
TO DETAIL 5/ AL1200

— CONCRETE CURB
REFER TO CIVIL
ROAD DWGS.



BAY STREET TYPICAL SIDEWALK TREATMENT



2
AL 100

REVISIONS	
△	
△	
△	

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wood.
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BURLINGTON, ONTARIO, CANADA, L7N 3W5
TEL: +1 905 336 2353

PMA

Landscape Architects

355 Keele St. Toronto, Ontario M6P 2Y6
t. 416.239.9816 f. 416.239.1310 www.pmaarch.com

DRAWN	<u>K.CAGADAS</u>	YYYYMMDD 2021/07/07
CHECKED	<u>A. MEZA</u>	2021/07/07
CORRECT	<u>E.LEE</u>	2021/07/07
SCALE		

WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK

BAY STREET

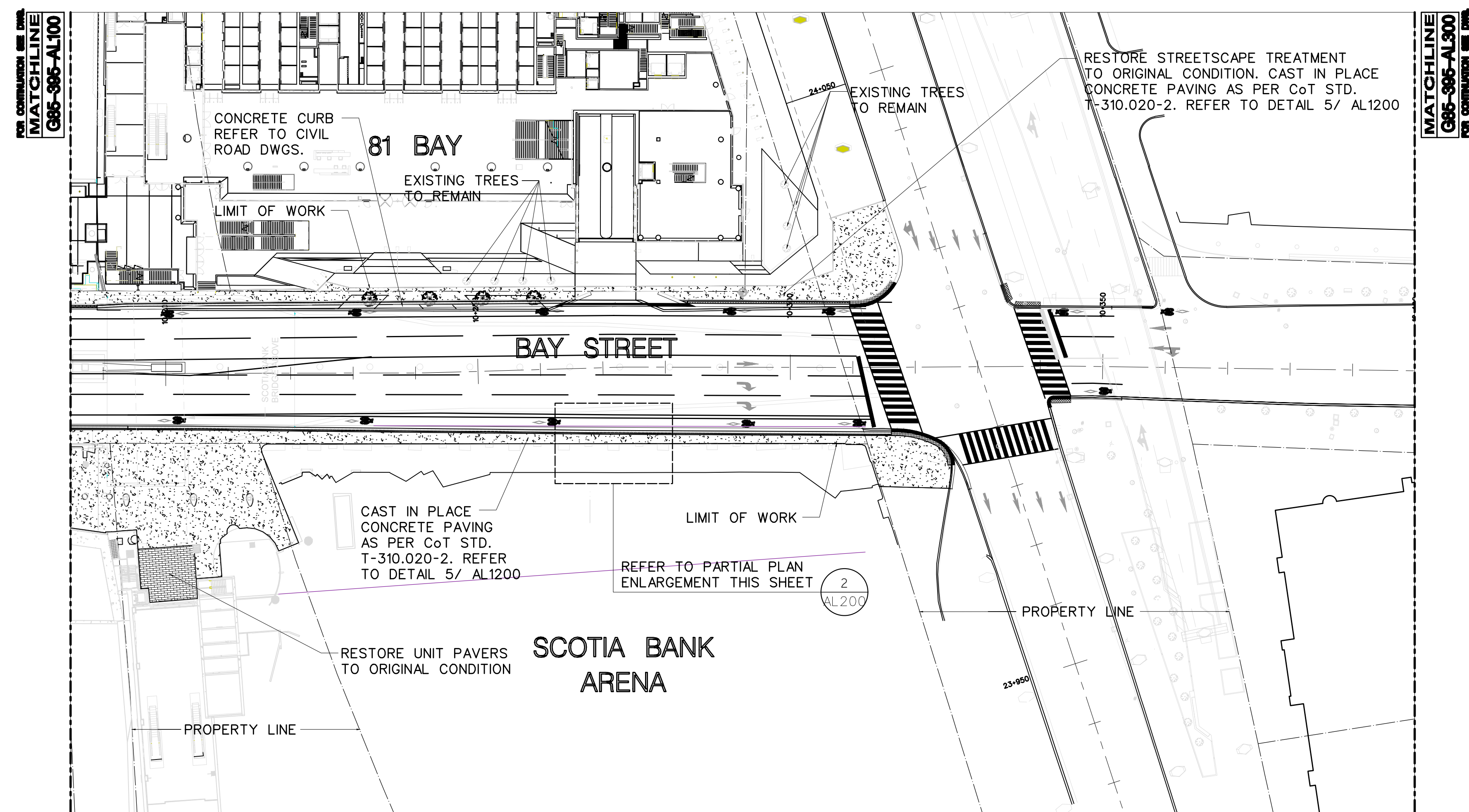
SURFACE, MATERIALS AND LAYOUT
STA. 9 + 952 TO STA. 10 + 185 PLAN

Contract: G85-395 Package:



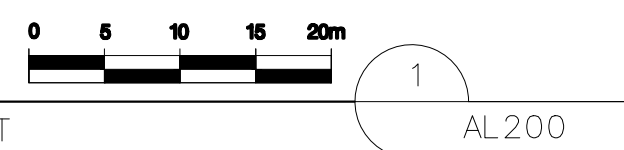
TORONTO TRANSIT COMMISSION ENGINEERING DEPARTMENT		
Dwg. No. G85-395-AL100	Rev. No.	Sheet No.

Plot Date: 2021-07-07



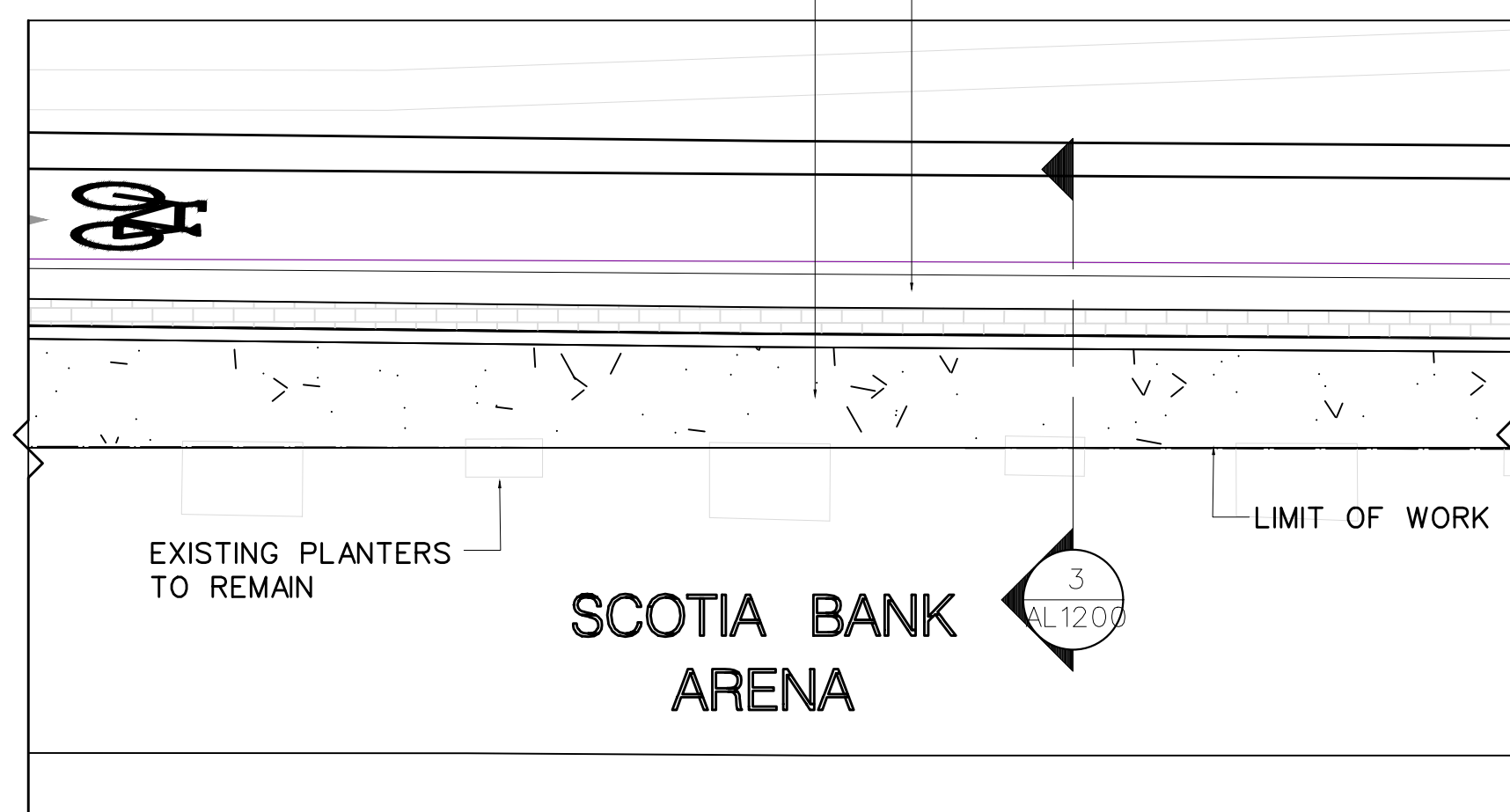
PLAN

BAY STREET & LAKE SHORE BLVD. WEST



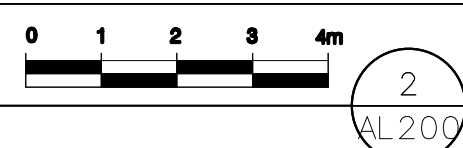
CAST IN PLACE CONCRETE PAVING
AS PER CoT STD. T-310.020-2

— CONCRETE CURB
REFER TO CIVIL
ROAD DWGS.



PARTIAL PLAN

BAY STREET



DRAWN	<u>K.CAGADAS</u>	YYYYMMDD 2021/07/07
CHECKED	<u>A. MEZA</u>	2021/07/07
CORRECT	<u>E.LEE</u>	2021/07/07
SCALE		

WATERFRONT EAST LRT
UNION STATION QUEENS QUAY LINK

BAY STREET

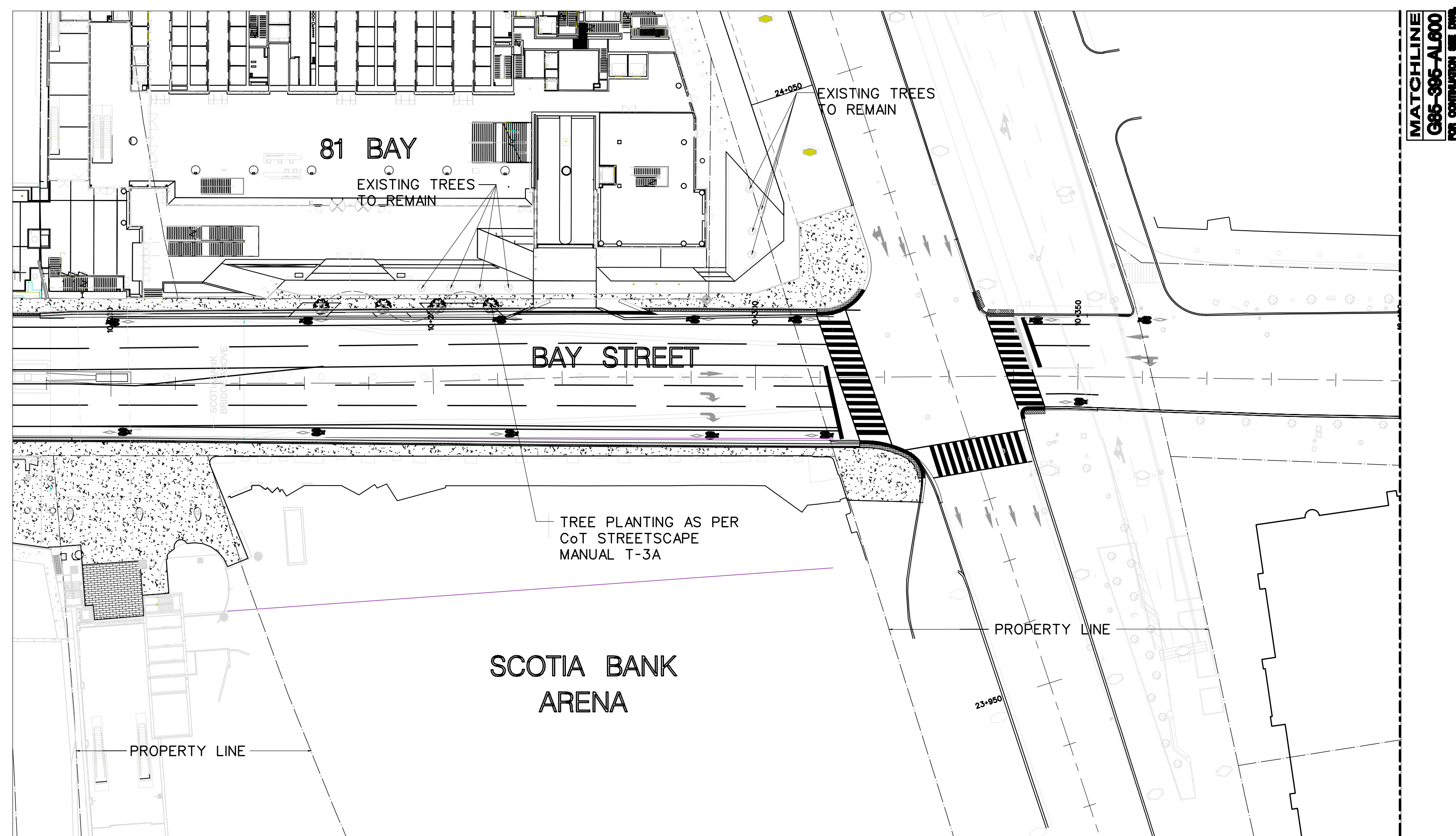
SURFACE, MATERIALS AND LAYOUT
STA. 10 + 185 TO STA. 10 + 400 PLAN

Contract: G85-395 Package:

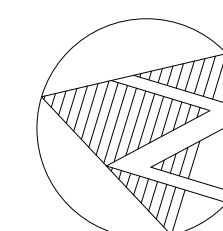
TORONTO TRANSIT COMMISSION
ENGINEERING DEPARTMENT

Dwg. No.	Rev. No.	Sheet No.
G85-395-AL200		

Plot Date: 2021-07-07

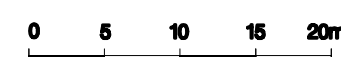


MATCHLINE
G85-385-AL600
FOR CONTINUATION SEE DWG.

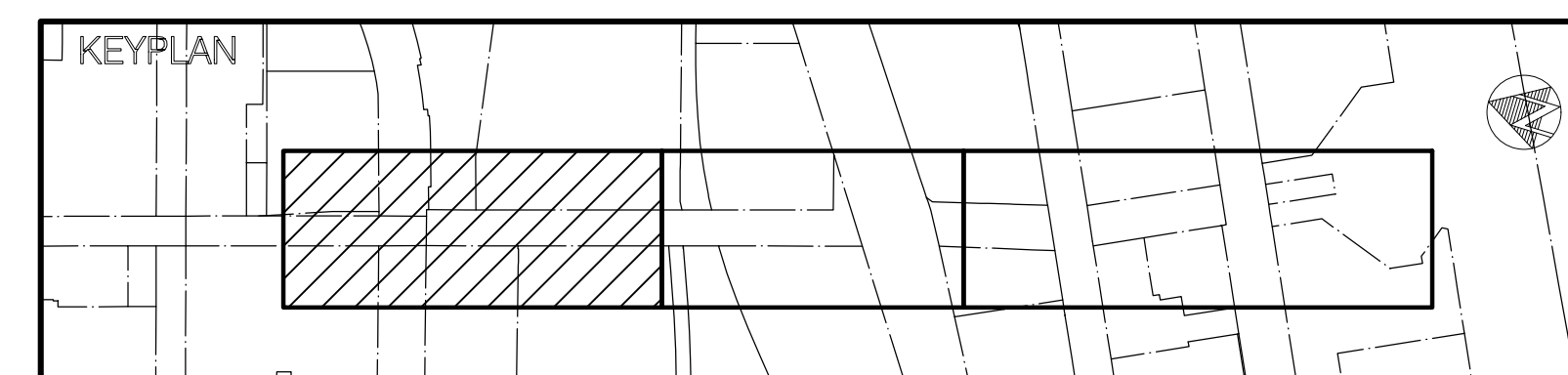


PLAN

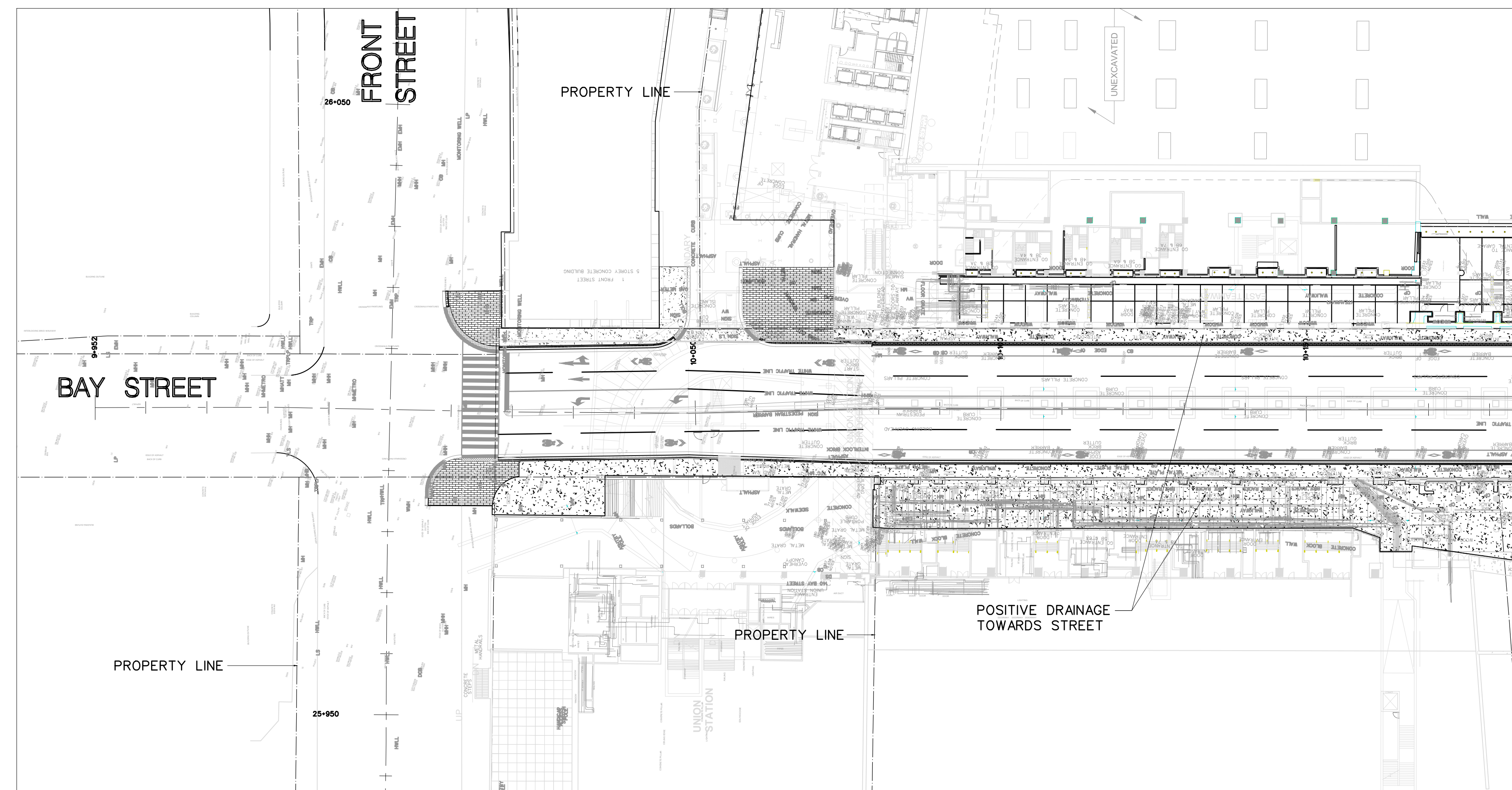
BAY STREET & LAKE SHORE BLVD. WEST



AL500

[illegible]

Plot Date: 2021-07-07



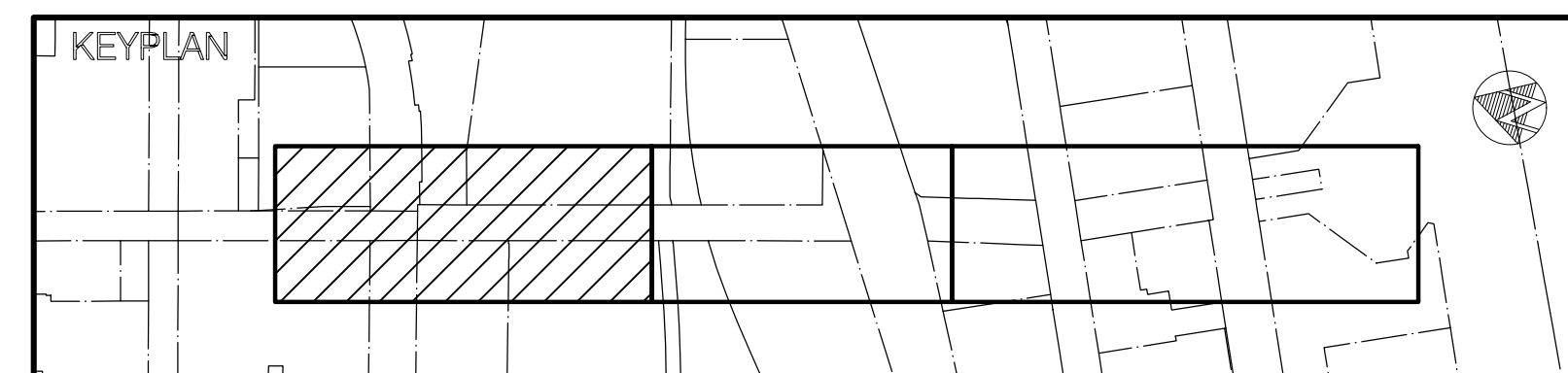
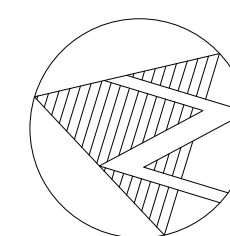
BAY STREET & FRONT STREET WEST



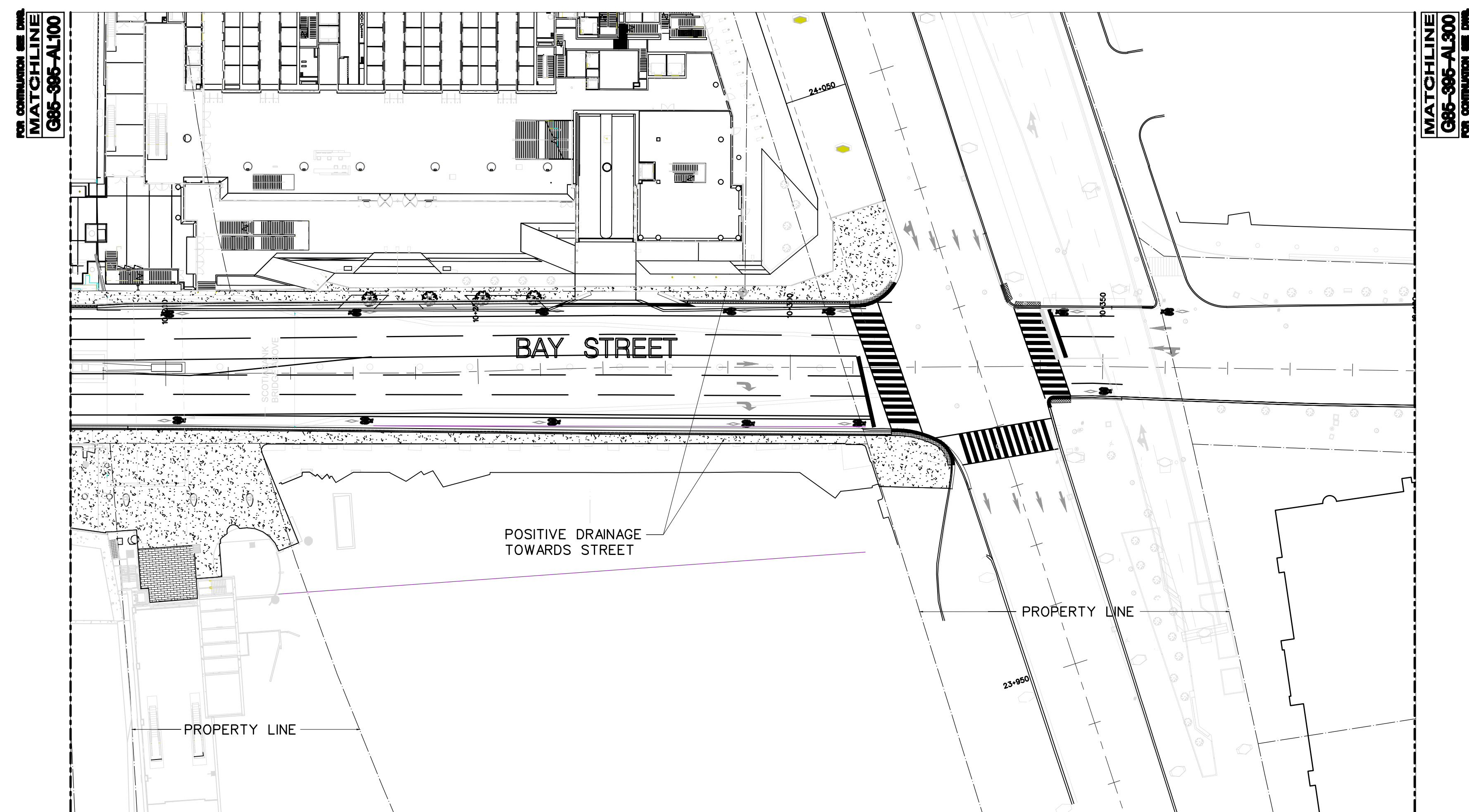
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AL800

MATCHLINE
G85-395-AL200
FOR CONTINUATION SEE DWG.

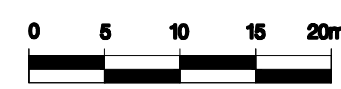
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Plot Date: 2021-07-07

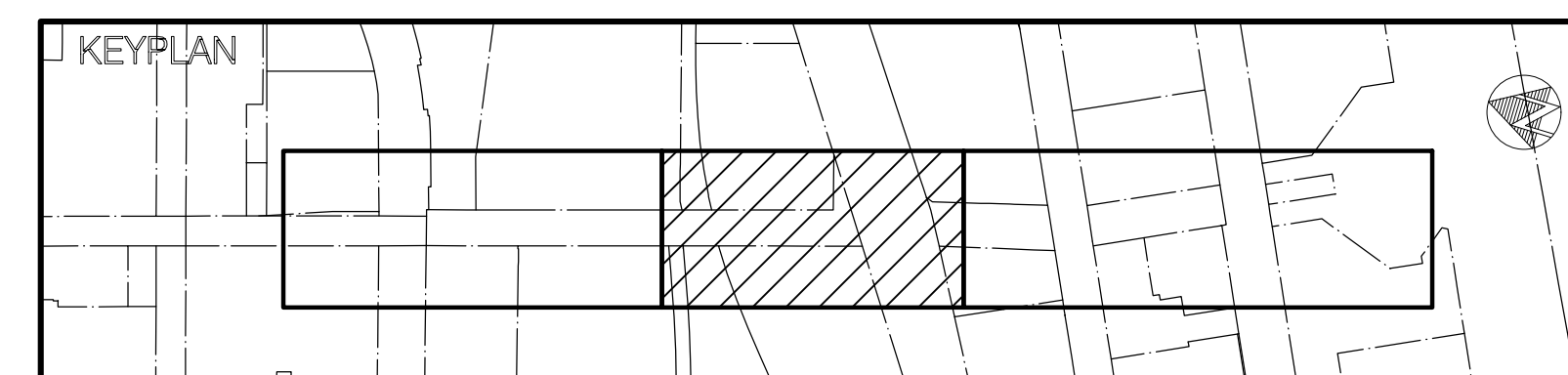
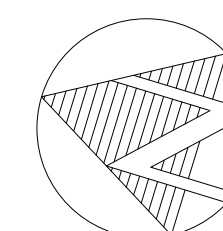


PLAN

BAY STREET & LAKE SHORE BLVD. WEST



AL900

[illegible]

Plot Date: 2021-07-07

Appendix C: Record of Community Engagement

From: [Yasmina Shamji](#)
To: [Schopf, Heidi](#)
Subject: FW: Information Gathering Request
Date: Thursday, March 25, 2021 11:37:01 AM
Attachments: [image001.png](#)
[image006.png](#)
[image007.png](#)
[OSIO52004 TTC Waterfront East LRT Information Gathering Request 20201216....pdf](#)

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidi

Sorry, I didn't realize I hadn't copied you on the email. Here is my response to Luke.

Thank you

Yasmina Shamji
Urban Design | Heritage Planning
Toronto City Hall | 100 Queen Street West, 17th Floor East Tower
Toronto, ON M5H 2N2
416-392-1975
[City of Toronto](#)



From: Yasmina Shamji
Sent: January 7, 2021 4:27 PM
To: 'Fischer, Luke' <luke.fischer@woodplc.com>
Cc: Guy Zimmerman <Guy.Zimmerman@toronto.ca>; Joe Muller <Joe.Muller@toronto.ca>
Subject: RE: Information Gathering Request

Luke

Happy New Year!

I have reviewed your report and compiled the information as requested. I don't have information for the properties noted in red below – it's possible details for these properties doesn't exist. In that case, I am deferring your request to Guy Zimmerman (also copied on this email), who is the Heritage Planner for the Union Station Heritage Conservation District.

According to our [Heritage Register Map](#), the following properties fall within the area study areas indicated in your report:

- Bay Street from Front Street West to Queens Quay East
- Queens Quay East from York Street to Cooper Street

(I am attaching the link to the property details from our online register as well as the by-laws wherever applicable)

40 Bay St: Part IV Designation (Part IV By-law 360-90) as well as Union Station HCD (By-law 634-2006)
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433226&propertyRsn=206956>
<https://www.toronto.ca/legdocs/pre1998bylaws/toronto%20-%20former%20city%20of/1990-0360.pdf>

71 Front St W (Union Station) aka 140 & 142 Bay St & 55 Front St W: Part IV Designation (Part IV By-law 948-2005) as well as Union Station HCD
And 61 Front St W - <http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2440090&propertyRsn=206955>
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2435414&propertyRsn=705027>
<https://www.toronto.ca/legdocs/bylaws/2005/law0948.pdf>

145 Queens Quay W aka 77 Harbour Sq: Part IV Designation (By-law 1249-2007)
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2439065&propertyRsn=206198>
<https://www.toronto.ca/legdocs/bylaws/2007/law1249.pdf>

2 Cooper St (entrance address for 55 Lake Shore Blvd E) – Under Intention to Designate
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2438226&propertyRsn=719130>
<http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2018.TE30.17>

Further to the above, you also requested information sheets for the following properties:

Gardiner Expressway – no information available – deferred to Guy

1 Front St W
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2435336&propertyRsn=578987>
<https://www.toronto.ca/legdocs/bylaws/2017/law0423.pdf>

61 Front St W – Listed – no additional information is available
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2440090&propertyRsn=206955>

65 Front St W - refer to details provide above

18 Yonge St – no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2437048&propertyRsn=615674>

33 Bay St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433225&propertyRsn=751123>

40 Bay St – refer to details provide above

161 Bay St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=3246951&propertyRsn=206967>

200 Bay St - no property info sheets available – deferred to Guy

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433240&propertyRsn=206973>

60 Harbour St - no property info sheets available – deferred to Guy

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2434664&propertyRsn=206938>

88-100 Harbour St - no property info sheets available – deferred to Guy

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=3607011&propertyRsn=826634>

15-19 York St - no property info sheets available – deferred to Guy

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2436569&propertyRsn=750799>

Also, please note that most of the information provided above can be found on our website via the links below:

<https://www.toronto.ca/city-government/planning-development/heritage-preservation/heritage-register/> - using both the Heritage Register Map as well as the Heritage Search Tool.

By-law information can be found here: <https://www.toronto.ca/legdocs/bylaws/lawhome.htm>

Thank you

Yasmina Shamji

Urban Design | Heritage Planning

Toronto City Hall | 100 Queen Street West, 17th Floor East Tower

Toronto, ON M5H 2N2

[City of Toronto](#)

416-392-1975



From: Fischer, Luke [<mailto:luke.fischer@woodplc.com>]

Sent: December 16, 2020 3:13 PM

To: Yasmina Shamji <Yasmina.Shamji@toronto.ca>

Cc: Schopf, Heidi <heidy.schopf@woodplc.com>

Subject: Information Gathering Request

Hello Yasmina,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,

Luke

Luke Fischer, M.A., CAHP

Senior Archaeologist, Built Heritage Analyst

Direct: 1 (519) 681-2400

Mobile: 1 (226) 376-2968

luke.fischer@woodplc.com

www.woodplc.com



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From: [Guy Zimmerman](#)
To: [Schopf, Heidy](#)
Cc: [Fischer, Luke](#); [Brent Fairbairn](#)
Subject: RE: DRAFT Response from HP with contact names and document references
Date: Wednesday, July 7, 2021 11:55:10 AM
Attachments: [image020.png](#)
[image021.png](#)
[image022.png](#)
[image002.png](#)
[image005.png](#)
[image006.png](#)

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy,

I was glad to be able to share my experience with you and Brent regarding my stewardship of the Union Station HCD and provide that contact information to source background regarding the Bay Street Bridge (that you apparently may not have been familiar with)

Regarding any additional feedback that you were fishing for... beyond that please let me know if there is missing information that I can source for you as my expectation was that I was providing pretty specific insight into the Gardiner Expressway and Path System (relative to the Union Station HCD).

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning /Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca
416.338.1094

From: Schopf, Heidy [mailto:heidy.schopf@woodplc.com]
Sent: July 6, 2021 3:51 PM
To: Guy Zimmerman <Guy.Zimmerman@toronto.ca>
Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>
Subject: RE: DRAFT Response from HP with contact names and document references

Thank you both once again for your time today. Your insight on the Gardiner, PATH, and Bay Street bridge is appreciated!

Sincerely,
-Heidy

From: Guy Zimmerman <Guy.Zimmerman@toronto.ca>
Sent: Tuesday, July 6, 2021 2:59 PM
To: Schopf, Heidi <heidy.schopf@woodplc.com>
Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>
Subject: DRAFT Response from HP with contact names and document references

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy et al,

Please disregard my DRAFT RESPONSE that I had been preparing if you have that random assortment of materials and contacts names. I had included Dima's contact information at the bottom for your reference should there be any confusion with the consultant indicated on the image of the cover of the report that is included as to who would be the first point of contact.

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning /Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca
416.338.1094

From: Guy Zimmerman
Sent: July 6, 2021 7:59 AM
To: 'Schopf, Heidi' <heidy.schopf@woodplc.com>
Cc: 'Fischer, Luke' <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>
Subject: RE: Union Station HCD Background Request Cultural Heritage Report for the Waterfront East LRT - Outstanding question RE:Bridge supporting the Union Station Trainshed Inquiry

Good morning Heidy,

I thought that it was funny when you first asked about this that you wouldn't have access to this material through Metrolinx.

I won't bother completing the material that I was assembling below as we can discuss it during our 3:00 Union Station/TTC WELRT Discussion (catchy name!)

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning /Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca
416.338.1094

DRAFT RESPONSE 7/6/2021

I dredged up this report from the work associated with the Bay Street Railway Bridge.

I don't know if I am at liberty to share this material but I think you would do better to deal directly with the consultants as they may have additional research that you would benefit from.

I'd be surprised if you have not dealt with Dima Cook in one capacity or another over the years.

Dima has been involved in heritage related work in Toronto for likely 15 years.

ERA was also involved and I don't know who has what... Dima will.

Bay Street PATH Connection (North)

1.1 Project Overview

The proposed development of 45 (81) & 141 Bay Street is part of a contemporary large-scale city building initiative incorporating transportation and is located within the Union Station Heritage Conservation District (HCD) Plan and adjacent to a number of prominent Beaux-Arts buildings associated with Toronto's development.

CHER

Bay Street railway Bridge

Union Station rail Corridor

Project # 16-118-01

Prepared by GS / JN (ERA)

Cultural Heritage Evaluation Report - Part 2

PREPARED FOR:

Metrolinx

20 Bay Street, Suite 600

Toronto Ontario M5J2W3

Rodyney Yee, Project Coordinator, GO Transit

rodney.yee@gotransit.com

416-202-4516

PREPARED BY:

ERA Architects Inc.

10 St. Mary Street, Suite 801

Toronto, Ontario M4Y 1P9

416-963-4497



STAGE TWO
HERITAGE IMPACT ASSESSMENT

**Bay Street PATH
Connection (North)**

IVANHOÉ CAMBRIDGE C/O HINES

PROJECT NO. EVOQ. 9019-13

ISSUE DATE: NOVEMBER 2019



Dima Cook

OAQ OAA AAA AANB RAIC LEED AP CAHP

ARCHITECT, PRINCIPAL

NEW OFFICE ADDRESS

75 SHERBOURNE STREET, SUITE 503

TORONTO, ON M5A 2P9

D./C. 416. 873.2018

T. 647.723.2030 / 1001

EVOQ ARCHITECTURE

@evoqarchitecture

Instagram / Facebook / LinkedIn

From: Guy Zimmerman

Sent: July 5, 2021 4:23 PM

To: 'Schopf, Heidy' <heidy.schopf@woodplc.com>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>

Subject: RE: Union Station HCD Background Request Cultural Heritage Report for the Waterfront East LRT - Outstanding question RE: Bridge supporting the Union Station Trainshed Inquiry

RE: Bridge supporting the Union Station Train shed Inquiry

Sorry Heidy... "I dropped the ball" on that aspect of my response/your request.

I don't see a problem sourcing the material that I previously referenced in time for our call (although there is nothing really to discuss in that regard... as it is really just providing the consultant's background associated with the integration of the new passage forward of the metal elevation)

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning/Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca
416.338.1094

From: Schopf, Heidy [mailto:heidy.schopf@woodplc.com]

Sent: July 5, 2021 3:06 PM

To: Guy Zimmerman <Guy.Zimmerman@toronto.ca>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>

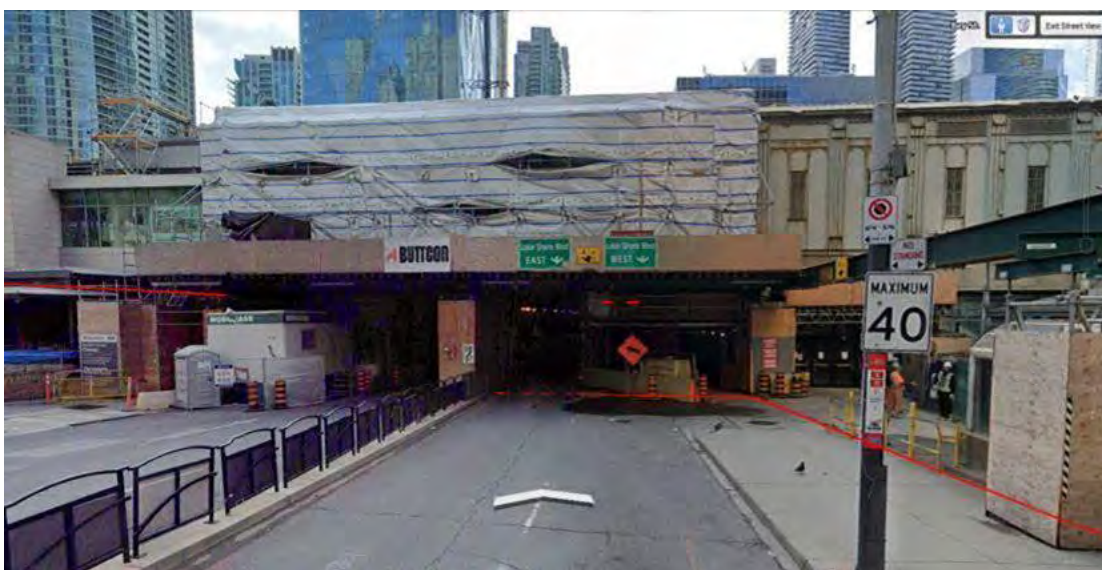
Subject: RE: Union Station HCD Background Request Cultural Heritage Report for the Waterfront East LRT - Outstanding question RE: Gardiner and Path

Thanks Guy!

The only outstanding item is the heritage status of the bridge associated with the Union Station trainshed (see pictures below).

To date, we have assumed that this bridge is part of the Union Station complex. However, I would like to double check with you if this structure has any heritage status independent of Union Station. The bridge itself did not come up on our searches of the municipal register or in our information gathering exercises.

-Heidy



From: Guy Zimmerman <Guy.Zimmerman@toronto.ca>

Sent: Monday, July 5, 2021 9:30 AM

To: Schopf, Heidi <heidy.schopf@woodplc.com>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>

Subject: RE: Union Station HCD Background Request Cultural Heritage Report for the Waterfront East LRT - Outstanding question RE: Gardiner and Path

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Heidy et al,

Please confirm that the only outstanding information that you are seeking (to include in

your Cultural Heritage Report for the Waterfront East LRT) is clarification of the HCD direction regarding the PATH system and Gardiner Expressway.

(Between the material that I had provide and the additional information that Yasmina referenced I believe all other aspects of your request have been satisfied)

I have copied below excerpts from the 2006 Union Station HCD Appendix that can underpin our Teams discussion tomorrow at 3:00.

I had mentioned that I would send you a summary of what I intend to share regarding these two subjects and I believe that these excerpts serve that purpose.



Gardiner Expressway

Era: Post-War
Style: Modern Infrastructure
Contributing: Yes

The development of this expressway was part of Fred Gardiner's vision of a modern Toronto. As Metropolitan Toronto's first Chairman, he was instrumental in the development of the Gardiner Expressway beginning in 1955. Built in an industrial zone at the foot of the city, the expressway modernized access to the centre of the city and from the city to its suburbs during a period of rapid growth. It was a nascent project in the process of creating regional infrastructure for the newly created Metropolitan region. The expressway was planned in conjunction with the rapid transit network as part of the comprehensive transportation planning of the region. The Gardiner was the first of several highways planned for the Toronto area, most of which were never realized.

The Skywalk was completed in 1989 to coincide with the completion of the Rogers Centre. Designed in the arcade style, the Skywalk consists of a large glazed passageway enclosed by a semi-circular arched roof reminiscent of early century European pedestrian areas. The Skywalk begins at the western most side of Union Station, passes through the second and third stories of the Canadian National Express building and crosses the Toronto rail viaduct at Simcoe Street. South of the railway the Skywalk traverses through the south end of the Metro Convention Centre and terminates in the plaza adjacent to the CN Tower, Roundhouse and Rogers Centre. The skywalk is a piece of urban infrastructure of growing importance; connecting the city to the increasingly developed rail lands. Furthermore, the skywalk offers unique views of Toronto's existing rail activity and provides a physical connection between to the District's two national historic sites: the Roundhouse and Union Station.



PATH System

Era: Post-War
Style: Various
Contributing: Yes

The PATH system is the subgrade pedestrian network which connects 6 subway stations and over 4 million square feet of retail space through 27 km of passages. It is currently the largest network of this type in the world.

The system developed as a result municipal policy of the 1960's encouraging sub-grade connections between new large office developments to one another and to the subway. The system has continued to grow incrementally, as new developments make connections with adjacent buildings. An underground tunnel of 1929 between the Fairmont Royal York Hotel and Union Station has been incorporated into the system.

Heritage Conservation District Plan Appendix E

141

Currently the city has a contentious relationship with the Gardiner as a result of a change in use of the central waterfront. As new solutions are realized in managing transportation in an increasingly post-industrial mixed-use waterfront and urban core, the Gardiner remains as an important piece of the City's early modern heritage and contributing element to the function and character of the Union Station Heritage Conservation District.

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning/Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca

416.338.1094

From: Guy Zimmerman

Sent: June 18, 2021 11:20 AM

To: 'Schopf, Heidy' <heidy.schopf@woodplc.com>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>;

Yasmina Shamji <Yasmina.Shamji@toronto.ca>

Subject: RE: Union Station HCD Background Request - HP Response re 90 Harbour Street

Heidy,

Here is the low hanging fruit:

1) Workmen's Compensation Board Building

i) Withdrawal of Notice of Intention to Designate, Part IV, Section 29, Ontario Heritage Act - 90 Harbour Street

<http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2008.TE12.15>

ii) Demolition permit issued February 14 2011 (11 110475)

2) Gardiner Expressway or the PATH System

I need to have "a think" about this one and articulate their role in the HCD... and get back to you.

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning/Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca
416.338.1094

From: Schopf, Heidy [<mailto:heidyschopf@woodplc.com>]

Sent: June 18, 2021 10:21 AM

To: Guy Zimmerman <Guy.Zimmerman@toronto.ca>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>;
Yasmina Shamji <Yasmina.Shamji@toronto.ca>

Subject: RE: Union Station HCD Background Request

Hi Guy,

Following up on this. Do you have any direction on the two items below?

-Heidy

From: Schopf, Heidy

Sent: Friday, April 9, 2021 9:54 AM

To: Guy Zimmerman <Guy.Zimmerman@toronto.ca>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>; Yasmina Shamji <Yasmina.Shamji@toronto.ca>

Subject: RE: Union Station HCD Background Request

Good morning Guy,

We received the HCD inventory information earlier this week. Thank you! The information was very helpful.

I have a couple questions:

- 1) The Workmen's Compenstaion Board Building (90 Harbour Street) appears to have been demolished between 2009 and 2012 based on Google Earth imagery. Can you confirm?
- 2) The Union Station HCD Plan does not depict the Gardiner Expressway or the PATH System as contributing buildings in the HCD (see below). Yet, the inventory sheets note that these resources are "contributing". What is the heritage status of these two resources? Our approach has been to address them as elements of the HCD in general but not as individual resources. It would be great to get some direction here to confirm if the City views the Gardiner and the PATH System as standalone heritage resources (or not).

Thanks,
Heidy



From: Guy Zimmerman <Guy.Zimmerman@toronto.ca>

Sent: Friday, March 26, 2021 9:00 AM

To: Schopf, Heidi <heidy.schopf@woodplc.com>

Cc: Fischer, Luke <luke.fischer@woodplc.com>; Brent Fairbairn <Brent.Fairbairn@toronto.ca>;

Yasmina Shamji <Yasmina.Shamji@toronto.ca>

Subject: Union Station HCD Background Request

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Heidy/Luke,

I will be working on this today.

The HCD material that you are referencing dates back to 2006.

Brent Fairbairn has made an inquiry as well and I will determine what additional material we have relative to your requests... beyond what Yasmina has already provided.

Guy Zimmerman - BEDS, CAHP
Heritage Planner
Heritage Planning/Urban Design
City Planning • City of Toronto
guy.zimmerman@toronto.ca
416.338.1094

From:

From: Schopf, Heidy [<mailto:heidy.schopf@woodplc.com>]
Sent: March 26, 2021 6:53 AM
To: Guy Zimmerman <Guy.Zimmerman@toronto.ca>
Cc: Fischer, Luke <luke.fischer@woodplc.com>
Subject: FW: Information Gathering Request

Hi Guy,

I am following up on an information request submitted by Luke this past January. Apologies if you already replied to him!

We are completing a Cultural Heritage Report for the Waterfront East LRT and part of the Study Area falls within the Union Station HCD. Any additional information you can provide on the heritage attributes of the district and property sheets for addresses in the Study Area would be hugely helpful! Happy to have a call sometime if that works better for you.

Thank you,
Heidy


From: Yasmina Shamji <Yasmina.Shamji@toronto.ca>
Sent: Thursday, March 25, 2021 11:36 AM
To: Schopf, Heidy <heidy.schopf@woodplc.com>
Subject: FW: Information Gathering Request

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Heidy

Sorry, I didn't realize I hadn't copied you on the email. Here is my response to Luke.

Thank you

Yasmina Shamji
Urban Design | Heritage Planning
Toronto City Hall | 100 Queen Street West, 17th Floor East Tower
Toronto, ON M5H 2N2
416-392-1975
[City of Toronto](#)


From: Yasmina Shamji

Sent: January 7, 2021 4:27 PM

To: 'Fischer, Luke' <luke.fischer@woodplc.com>

Cc: Guy Zimmerman <Guy.Zimmerman@toronto.ca>; Joe Muller <Joe.Muller@toronto.ca>

Subject: RE: Information Gathering Request

Luke

Happy New Year!

I have reviewed your report and compiled the information as requested. I don't have information for the properties noted in red below – it's possible details for these properties doesn't exist. In that case, I am deferring your request to Guy Zimmerman (also copied on this email), who is the Heritage Planner for the Union Station Heritage Conservation District.

According to our [Heritage Register Map](#), the following properties fall within the area study areas indicated in your report:

- Bay Street from Front Street West to Queens Quay East
- Queens Quay East from York Street to Cooper Street

(I am attaching the link to the property details from our online register as well as the by-laws wherever applicable)

40 Bay St: Part IV Designation (Part IV By-law 360-90) as well as Union Station HCD (By-law 634-2006)
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433226&propertyRsn=206956>
<https://www.toronto.ca/legdocs/pre1998bylaws/toronto%20-%20former%20city%20of/1990-0360.pdf>

71 Front St W (Union Station) aka 140 & 142 Bay St & 55 Front St W: Part IV Designation (Part IV By-law 948-2005) as well as Union Station HCD

And 61 Front St W - <http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2440090&propertyRsn=206955>

<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2435414&propertyRsn=705027>
<https://www.toronto.ca/legdocs/bylaws/2005/law0948.pdf>

145 Queens Quay W aka 77 Harbour Sq: Part IV Designation (By-law 1249-2007)
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2439065&propertyRsn=206198>
<https://www.toronto.ca/legdocs/bylaws/2007/law1249.pdf>

2 Cooper St (entrance address for 55 Lake Shore Blvd E) – Under Intention to Designate
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2438226&propertyRsn=719130>
<http://app.toronto.ca/tmmis/viewAgendaItemHistory.do?item=2018.TE30.17>

Further to the above, you also requested information sheets for the following properties:

Gardiner Expressway – no information available – deferred to Guy

1 Front St W
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2435336&propertyRsn=578987>
<https://www.toronto.ca/legdocs/bylaws/2017/law0423.pdf>

61 Front St W – Listed – no additional information is available
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2440090&propertyRsn=206955>

65 Front St W - refer to details provide above

18 Yonge St – no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2437048&propertyRsn=615674>

33 Bay St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433225&propertyRsn=751123>

40 Bay St – refer to details provide above

161 Bay St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=3246951&propertyRsn=206967>

200 Bay St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2433240&propertyRsn=206973>

60 Harbour St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2434664&propertyRsn=206938>

88-100 Harbour St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=3607011&propertyRsn=826634>

15-19 York St - no property info sheets available – deferred to Guy
<http://app.toronto.ca/HeritagePreservation/details.do?folderRsn=2436569&propertyRsn=750799>

Also, please note that most of the information provided above can be found on our website via the links below:

<https://www.toronto.ca/city-government/planning-development/heritage-preservation/heritage-register/> -

using both the Heritage Register Map as well as the Heritage Search Tool.

By-law information can be found here: <https://www.toronto.ca/legdocs/bylaws/lawhome.htm>

Thank you

Yasmina Shamji
Urban Design | Heritage Planning
Toronto City Hall | 100 Queen Street West, 17th Floor East Tower
Toronto, ON M5H 2N2
[City of Toronto](#)

416-392-1975



From: Fischer, Luke [<mailto:luke.fischer@woodplc.com>]
Sent: December 16, 2020 3:13 PM
To: Yasmina Shamji <Yasmina.Shamji@toronto.ca>
Cc: Schopf, Heidy <heidy.schopf@woodplc.com>
Subject: Information Gathering Request

Hello Yasmina,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,
Luke

Luke Fischer, M.A., CAHP
Senior Archaeologist, Built Heritage Analyst
Direct: 1 (519) 681-2400
Mobile: 1 (226) 376-2968
luke.fischer@woodplc.com
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From: [Kevin DeMille](#)
To: [Schopf, Heidy](#)
Subject: Re: Information Gathering Request
Date: Tuesday, July 13, 2021 8:07:01 AM
Attachments: [image001.png](#)

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Good morning Heidi,

Please feel free to contact:

Kirushanth Gnanachandran
Project Coordinator, Rouge National Urban Park
Parks Canada / Government of Canada
kirushanth.gnanachandran@canada.ca /Tel: 647-205-6150

They were notified and should be able to help you or forward you on to the correct person.

Kind regards,

Kevin De Mille

Kevin De Mille
Natural Heritage Coordinator
Telephone: 437-246-5854*NEW



From: Schopf, Heidy <heidy.schopf@woodplc.com>
Sent: July 8, 2021 10:50 AM
To: Kevin DeMille <Kevin.DeMille@heritagetrust.on.ca>; Fischer, Luke <luke.fischer@woodplc.com>
Subject: RE: Information Gathering Request

CAUTION: External mail. Do not click on links or open attachments unless you recognize the sender and know the content.

Hi Kevin,

It's been a while but following up on this. Is there a contact at Parks Canada that you could recommend? We are trying to track down the correct person/department to contact regarding Union Station.

Thanks,
Heidy

From: Kevin DeMille <Kevin.DeMille@heritagetrust.on.ca>

Sent: Wednesday, April 14, 2021 11:15 AM

To: Fischer, Luke <luke.fischer@woodplc.com>

Cc: Schopf, Heidy <heidy.schopf@woodplc.com>

Subject: Re: Information Gathering Request

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Good morning Luke,

You may have these already but the Registrar provided the following information:

- 1 Front Street West: <https://www.heritagetrust.on.ca/en/oha/details?id=3781&backlinkslug=search-results&fields%5Baddress%5D=1+Front+Street>
- Union Station (note I have it as 71 Front Street West) and includes HCD info: https://www.heritagetrust.on.ca/en/oha/details?id=2024&backlinkslug=search-results&fields%5Bproperty_name%5D=Union+Station
- 40 Bay Street: <https://www.heritagetrust.on.ca/en/oha/details?id=2522&backlinkslug=search-results&fields%5Baddress%5D=40+bay+street>

There is a National Historic Site, so we recommend you reach out to Parks Canada.

Kind regards,

Kevin De Mille

Kevin De Mille

Natural Heritage Coordinator

Telephone: 437-246-5854*NEW



From: Fischer, Luke <luke.fischer@woodplc.com>
Sent: April 8, 2021 11:04 AM
To: Kevin DeMille <Kevin.DeMille@heritagetrust.on.ca>
Cc: Schopf, Heidy <heidy.schopf@woodplc.com>
Subject: RE: Information Gathering Request

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Hi Kevin,

Thank you for your feedback for the TTC Waterfront East LRT CHR. We have completed a draft report for this project. For the next stage of our work, we will prepare four HIAs for the following properties:

- CHR 1: Union Station Heritage Conservation District → designated under Part V of the OHA
- CHR 2: Union Station, 65 Front Street → designated under Parts IV and V of the OHA, National Historic Site, Provincial Property of Provincial Significance
- CHR 3: Dominion Public Building, 1 Front Street → designated under Parts IV and V of the OHA, Classified Federal Heritage Building (FHBRO)
- CHR 4: Postal Delivery Building, 40 Bay Street → designated under Parts IV and V of the OHA

Do you have any further information or guidance regarding these properties? We plan to prepare the HIAs using *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties* (2017) but can use other guidance documents if you recommend a different approach.

In addition, we are carrying out community engagement for these HIAs via email and telephone. We have ongoing dialogue with MHSTCI and the City of Toronto but also plan to reach out to the Toronto Preservation Board, GO Transit, Metrolinx, and the property owners (i.e. Canada Revenue and Maple Leaf Sports). Do you have any further groups that you feel we should contact? We are committed to providing an early and ongoing dialogue regarding the potential impacts of the project to these properties so that we can incorporate feedback into our reports.

Thanks ,
Luke

From: Schopf, Heidy <heidy.schopf@woodplc.com>
Sent: Wednesday, April 7, 2021 9:44 AM
To: Fischer, Luke <luke.fischer@woodplc.com>
Subject: FW: Information Gathering Request

From: Kevin DeMille <Kevin.DeMille@heritagetrust.on.ca>

Sent: Friday, March 26, 2021 3:46 PM
To: Schopf, Heidi <heidy.schopf@woodplc.com>
Subject: Re: Information Gathering Request

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Good afternoon Heidi,

I'm sorry for the confusion. I was away in December and January and it seems your inquiry was received during my absence and unfortunately the response was not sent.

Thank you for your information request related to a Cultural Heritage Evaluation Report relating to the TTC Waterfront East LRT study area including Bay Street from Front Street West to Queens Quay East, and Queens Quay East from York Street to Cooper Street in Toronto. Your request to verify the presence of OHT heritage properties or easements within or adjacent to the study area has been processed. I've reviewed the study area against our database of OHT easements and properties. We can confirm that the OHT does not have any conservation easements or Trust-owned properties within or adjacent to the study area provided in your map.

If you have not already done so, I recommend you check the Trust's Plaque Database (available online) <https://www.heritagetrust.on.ca/en/online-plaque-guide> to verify the presence of plaques. Additionally, I recommend you check the Trust's register (available online) <http://www.heritagetrust.on.ca/en/index.php/pages/tools/ontario-heritage-act-register> and contact the local municipality to verify no local heritage properties are present within the identified study area.

As described in Section 23 of the Ontario Heritage Act, the Trust holds and maintains the provincial Ontario Heritage Act Register of properties that have been designated by municipalities under sections 29 and 41 of the Act as well as properties designated under the Act by the Minister. We rely on municipalities to send us information and it is advisable to check with the clerk's office to verify information.

Under Section 27 of the Act (OHA) the clerk of a municipality is required to maintain a local register of all designated properties. Section 27 also states that municipalities may keep a register of property that has not been designated, but that the municipality has determined to be of cultural heritage value or interest. These are often referred to as "listed" properties. These non-designated heritage properties are not reflected in the OHA Register.

Kind regards,

Kevin De Mille

Kevin De Mille
Natural Heritage Coordinator
Telephone: 437-246-5854*NEW



From: Schopf, Heidy <heidyschopf@woodplc.com>

Sent: March 25, 2021 10:02 AM

To: registrar <registrar@heritagetrust.on.ca>

Cc: Fischer, Luke <luke.fischer@woodplc.com>

Subject: FW: Information Gathering Request

CAUTION: External mail. Do not click on links or open attachments unless you recognize the sender and know the content.

Good morning,

Following up on this request. Do you have any information regarding trust-owned properties or heritage easements in this Study Area? Apologies if this information was already sent. I can find a record in my email.

Sincerely,
Heidy

From: Fischer, Luke <luke.fischer@woodplc.com>

Sent: Wednesday, December 16, 2020 3:10 PM

To: registrar@heritagetrust.on.ca

Cc: Schopf, Heidy <heidyschopf@woodplc.com>

Subject: Information Gathering Request

Hello,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,
Luke

Luke Fischer, M.A., CAHP

Senior Archaeologist, Built Heritage Analyst

Direct: 1 (519) 681-2400

Mobile: 1 (226) 376-2968
luke.fischer@woodplc.com
www.woodplc.com



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From: [Barboza, Karla \(MHSTCI\)](#)
To: [Fischer, Luke](#)
Cc: [Hatcher, Laura \(MHSTCI\)](#); [Registrar \(MHSTCI\)](#); [Schopf, Heidy](#)
Subject: RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)
Date: Thursday, April 8, 2021 4:27:49 PM
Attachments: [image001.png](#)

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Hi Luke,

I hope this email finds you well.

It may be prudent for us to review the draft Cultural Heritage Report before we provide any advice on the approach for the Heritage Impact Assessment. Would you be able to share the draft report for our review?

Regards,
Karla

From: Fischer, Luke <luke.fischer@woodplc.com>
Sent: April-08-21 9:11 AM
To: Hatcher, Laura (MHSTCI) <Laura.E.Hatcher@ontario.ca>; Schopf, Heidy <heidy.schopf@woodplc.com>; Registrar (MHSTCI) <Registrar@ontario.ca>
Subject: RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

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Hi Laura,

Thank you for your feedback for the TTC Waterfront East LRT CHR. We have completed a draft report for this project and we will circulate this to you for review shortly. For the next stage of our work, we will prepare four HIAs for the following properties:

- CHR 1: Union Station Heritage Conservation District → designated under Part V of the OHA
- CHR 2: Union Station, 65 Front Street → designated under Parts IV and V of the OHA, National Historic Site, Provincial Property of Provincial Significance
- CHR 3: Dominion Public Building, 1 Front Street → designated under Parts IV and V of the OHA, Classified Federal Heritage Building (FHBRO)
- CHR 4: Postal Delivery Building, 40 Bay Street → designated under Parts IV and V of the OHA

Do you have any further information or guidance regarding these properties? We plan to prepare the HIAs using *Information Bulletin 3: Heritage Impact Assessments for Provincial Heritage Properties*

(2017) but can use other guidance documents if you recommend a different approach.

In addition, we are carrying out community engagement for these HIAs via email and telephone. We have ongoing dialogue with the City of Toronto but also plan to reach out to the Toronto Preservation Board, GO Transit, Metrolinx, and the property owners (i.e. Canada Revenue and Maple Leaf Sports). Do you have any further groups that you feel we should contact? We are committed to providing an early and ongoing dialogue regarding the potential impacts of the project to these properties so that we can incorporate feedback into our reports.

Thanks,
Luke

From: Hatcher, Laura (MHSTCI) <Laura.E.Hatcher@ontario.ca>
Sent: Friday, December 18, 2020 12:56 PM
To: Schopf, Heidi <heidy.schopf@woodplc.com>; Registrar (MHSTCI) <Registrar@ontario.ca>; Fischer, Luke <luke.fischer@woodplc.com>
Subject: RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

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Hi Heidi and Luke,

Karla is away today so I am following up with the statements of Cultural Heritage Value for the three properties. Please let us know if you need anything else.

Sincerely,
Laura

Laura Hatcher, MCIP, RPP

Heritage Planner

Heritage Planning Unit | Programs and Services Branch | Heritage, Tourism and Culture Division

Ministry of Heritage, Sport, Tourism and Culture Industries

401 Bay Street Suite 1700 Toronto ON M7A 0A7

Tel. 437-239-3404 **New** | email: laura.e.hatcher@ontario.ca

From: Schopf, Heidi <heidy.schopf@woodplc.com>
Sent: Friday, December 18, 2020 7:24 AM
To: Registrar (MHSTCI) <Registrar@ontario.ca>; Fischer, Luke <luke.fischer@woodplc.com>
Cc: Hatcher, Laura (MHSTCI) <Laura.E.Hatcher@ontario.ca>
Subject: RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

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Hi Karla,

Thank you so much for the quick reply. Yes, please send along the 'Statements of Cultural Heritage Value' for all three properties. That would be much appreciated!

I will double check on the process as well and get back to you.

Happy Holidays

-Heidy

From: Barboza, Karla (MHSTCI) <Karla.Barboza@ontario.ca> **On Behalf Of** Registrar (MHSTCI)
Sent: Thursday, December 17, 2020 5:02 PM
To: Fischer, Luke <luke.fischer@woodplc.com>
Cc: Registrar (MHSTCI) <Registrar@ontario.ca>; Hatcher, Laura (MHSTCI) <Laura.E.Hatcher@ontario.ca>; Schopf, Heidi <heidyschopf@woodplc.com>
Subject: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)

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File 0013552 – TTC Waterfront East Light Rail Transit (LRT)

Hi Luke,

I hope this email finds you well.

As you may know, the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI) developed screening checklists to assist property owners, developers, consultants and others to identify known and potential cultural heritage resources:

- [Criteria for Evaluating Archaeological Potential](#)
- [Criteria for Evaluating Marine Archaeological Potential](#)
- [Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes](#)

I have used the document above (Built Heritage and Cultural Heritage Landscapes) in order to respond to your question:

- Question 3a. i. Is the property (or project area) identified, designated or otherwise protected under the Ontario Heritage Act as being of cultural heritage value e.g. a property that is designated by order of the Minister of Heritage, Sport, Tourism and Culture Industries as being of cultural heritage value or interest of provincial significance [s.34.5]?

MHSTCI Response: To date, no properties have been designated by the Minister.

- Question 3a.v. Is the property (or project area) identified, designated or otherwise protected under the Ontario Heritage Act as being of cultural heritage value included in the Ministry of Heritage, Sport, Tourism and Culture Industries' list of provincial heritage properties?

MHSTCI Response: The following provincial heritage properties are within or adjacent to the study area:

- Union Station – provincial heritage property of provincial significance
- Union Station Rail Corridor (USRC) Interlocking Tower - Scott Street - provincial heritage property of provincial significance
- Yonge Street Railway Bridge – provincial heritage property (of local significance)

Let me know if you would like a copy of the Statement of Cultural Heritage Value for any of the properties above. None of these properties have a Strategic Conservation Plan in place. Please note that if the subject lands or parts of the subject lands are owned or controlled by an Ontario Ministry or Prescribed Public Body (PPB) on behalf of the Crown (the list of PPBs is available as O. Reg. 157/10), a Ministry or PPB may have responsibilities under the [Standards and Guidelines for Conservation of Provincial Heritage Properties](#).

Regarding your request about other protected heritage properties within or adjacent to the study area, you should contact the Ontario Heritage Trust, Provincial Heritage Registrar at registrar@heritagetrust.on.ca or 416-212-7104 and the City of Toronto Heritage Preservation Services.

Please let us know which process will this project fall under (e.g. Ontario Regulation 231/08 – Transit Project Assessment Process) as we may have additional advice.

I hope this helps. Let me know if you have any questions.

Regards,
Karla

Karla Barboza MCIP, RPP, CAHP | (A) Team Lead, Heritage
Ministry of Heritage, Sport, Tourism and Culture Industries
Heritage, Tourism and Culture Division | Programs and Services Branch | Heritage Planning Unit
T. 416.314.7120 | Email: karla.barboza@ontario.ca

From: Fischer, Luke <luke.fischer@woodplc.com>

Sent: December-16-20 3:08 PM

To: Registrar (MHSTCI) <Registrar@ontario.ca>

Cc: Schopf, Heidy <heidy.schopf@woodplc.com>

Subject: Information Gathering Request

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Hello,

Wood is carrying out a *Cultural heritage Report: Exiting Conditions and Preliminary Impact Assessment* (CHR) for the Waterfront East Light Rail Transit (LRT) project in Toronto. Please find the attached letter indicating the study area and outlining the information we are seeking to inform our report.

Kind Regards,
Luke

Luke Fischer, M.A., CAHP

Senior Archaeologist, Built Heritage Analyst

Direct: 1 (519) 681-2400

Mobile: 1 (226) 376-2968

luke.fischer@woodplc.com

www.woodplc.com



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<https://www.woodplc.com/policies/privacy-notice>

From: [Hatcher, Laura \(MHSTCI\)](#)
To: [Schopf, Heidy](#); [Registrar \(MHSTCI\)](#); [Fischer, Luke](#)
Subject: RE: MHSTCI Response: Information Gathering Request - Waterfront East Light Rail Transit (LRT)
Date: Friday, December 18, 2020 12:56:19 PM
Attachments: [image001.png](#)
[MHC SCHV - Union Station FINAL.pdf](#)
[4. USRC Scott Street Interlocking Tower - MHC Decision Form & SCHV \(PHPPS\).pdf](#)
[Metrolinx Heritage Committee - Yonge Street Bridge.pdf](#)

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Heritage Planner

Heritage Planning Unit | Programs and Services Branch | Heritage, Tourism and Culture Division

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401 Bay Street Suite 1700 Toronto ON M7A 0A7

Tel. 437-239-3404 [New](#) | email: laura.e.hatcher@ontario.ca

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Karla Barboza MCIP, RPP, CAHP | (A) Team Lead, Heritage
Ministry of Heritage, Sport, Tourism and Culture Industries
Heritage, Tourism and Culture Division | Programs and Services Branch | Heritage Planning Unit
T. 416.314.7120 | Email: karla.barboza@ontario.ca

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Kind Regards,
Luke

Luke Fischer, M.A., CAHP
Senior Archaeologist, Built Heritage Analyst
Direct: 1 (519) 681-2400
Mobile: 1 (226) 376-2968

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www.woodplc.com



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Appendix D: Assessor Qualification

Heidy Schopf, MES, CAHP – Cultural Heritage Team Lead

Heidy Schopf is the Cultural Heritage Team Lead for WSP Canada Inc. She is a Senior Cultural Heritage Specialist and has worked in the field of cultural resource management since 2007. Ms. Schopf is a Professional Member of the Canadian Association of Heritage Professionals (CAHP).

Ms. Schopf has worked on hundreds of cultural heritage projects in Ontario, including Cultural Heritage Reports, Cultural Heritage Evaluation Reports (CHERs), Heritage Impact Assessments (HIAs), Strategic Conservation Plans (SCP), heritage documentation (photography, photogrammetry, and LiDAR), Heritage Conservation District (HCD) Studies and Plans, and heritage peer review. She regularly provides cultural heritage conservation guidance to public and private sector clients. Heidy is a Senior Project Manager and has managed and delivered cultural heritage work under a variety of processes, including: Environmental Assessment Act, Planning Act, Transit Project Assessment Process (TPAP), and the Ontario Heritage Act. She has extensive and applied knowledge of Ministry of Citizenship and Multiculturalism (MCM) guidance documents for heritage properties.

Ms. Schopf has had the privilege of working with Indigenous Nations on several projects to gather Indigenous perspectives on cultural heritage and integrate this shared learning into WSP's heritage work.

Luke Fischer, MA, CAHP – Cultural Heritage Specialist

Mr. Fischer is a Senior Archaeologist who has been working in the cultural heritage field since 2002. Mr. Fischer has successfully managed projects, field directed, analysed artifact assemblages, and authored reports for Stage 1 to 4 archaeological assessments. Mr. Fischer is knowledgeable in the legislative processes and policies pertinent to cultural resource management in Ontario and has completed archeological assessments for both public and private development proponents. Mr. Fischer has extensive experience completing archaeological investigations in support of large-scale corridor projects as well as smaller site-specific investigations with stringent timelines. Mr. Fischer's experience includes archaeological investigation in landscapes varying from the remote boreal forest and rural agricultural fields to deeply buried urban deposits. Mr. Fischer has experience facilitating Indigenous engagement, including coordination of field monitors. In addition to his main experience in Ontario Mr. Fischer has worked as an archaeologist on linear corridor studies in Alberta, British Columbia, and Illinois. Mr. Fischer also has experience acting as a Cultural Heritage Specialist conducting various types of studies relating to built heritage landscape. Mr. Fischer holds a Professional Archaeology License (P219) issued by the Ontario Ministry of Citizenship and Multiculturalism (MCM), is a member of the Association of Professional Archaeologists (APA), the Canadian Association of Heritage Professionals (CAHP), and is MTO RAQs certified in Archaeology and Heritage.

Appendix E: Limitations

Limitations

1. The work performed in the preparation of the Heritage Impact Assessment (HIA) and the conclusions presented are subject to the following:
 - (a) The Standard Terms and Conditions which form a part of our Professional Services Contract;
 - (b) The Scope of Services;
 - (c) Time and Budgetary limitations as described in our Contract; and,
 - (d) The Limitations stated herein.
2. No other warranties or representations, either expressed or implied, are made as to the professional services provided under the terms of our Contract, or the conclusions presented.
3. The conclusions presented in the report are based, in part, on visual observations of the Study Area. Our conclusions cannot and are not extended to include those portions of the Study Area which were not reasonably available, in WSP Environment & Infrastructure's opinion, for direct observation.
4. The potential and protected cultural heritage resources encountered at the Study Area were assessed, within the limitations set out above, having due regard for applicable heritage regulations as of the date of the inspection.
5. Services including a background study and fieldwork were performed. WSP Environment & Infrastructure's work, including archival studies and fieldwork, were completed in a professional manner and in accordance with the Ministry of Citizenship and Multiculturalism's guidelines. It is possible that Cultural heritage resources not visible from the public realm may be within, or adjacent to the Study Area.
6. The utilization of WSP Environment & Infrastructure's services during the implementation of any further cultural heritage work recommended will allow WSP Environment & Infrastructure to observe compliance with the conclusions and recommendations contained the HIA. WSP Environment & Infrastructure's involvement will also allow for changes to be made as necessary to suit field conditions as they are encountered.
7. This report is for the sole use of the parties to whom it is addressed unless expressly stated otherwise in the report or contract. Any use which any third party makes of the report, in whole or in part, or any reliance thereon, or decisions made based on any information of conclusions in the report, is the sole responsibility of such third party. WSP Environment & Infrastructure accepts no responsibility whatsoever for damages or loss of any nature or kind suffered by any such third party as a result of actions taken or not taken or decisions made in reliance on the report or anything set out therein.
8. This report is not to be given over to any third-party other than a governmental entity, for any purpose whatsoever without the written permission of WSP Environment & Infrastructure, which shall not be unreasonably withheld.