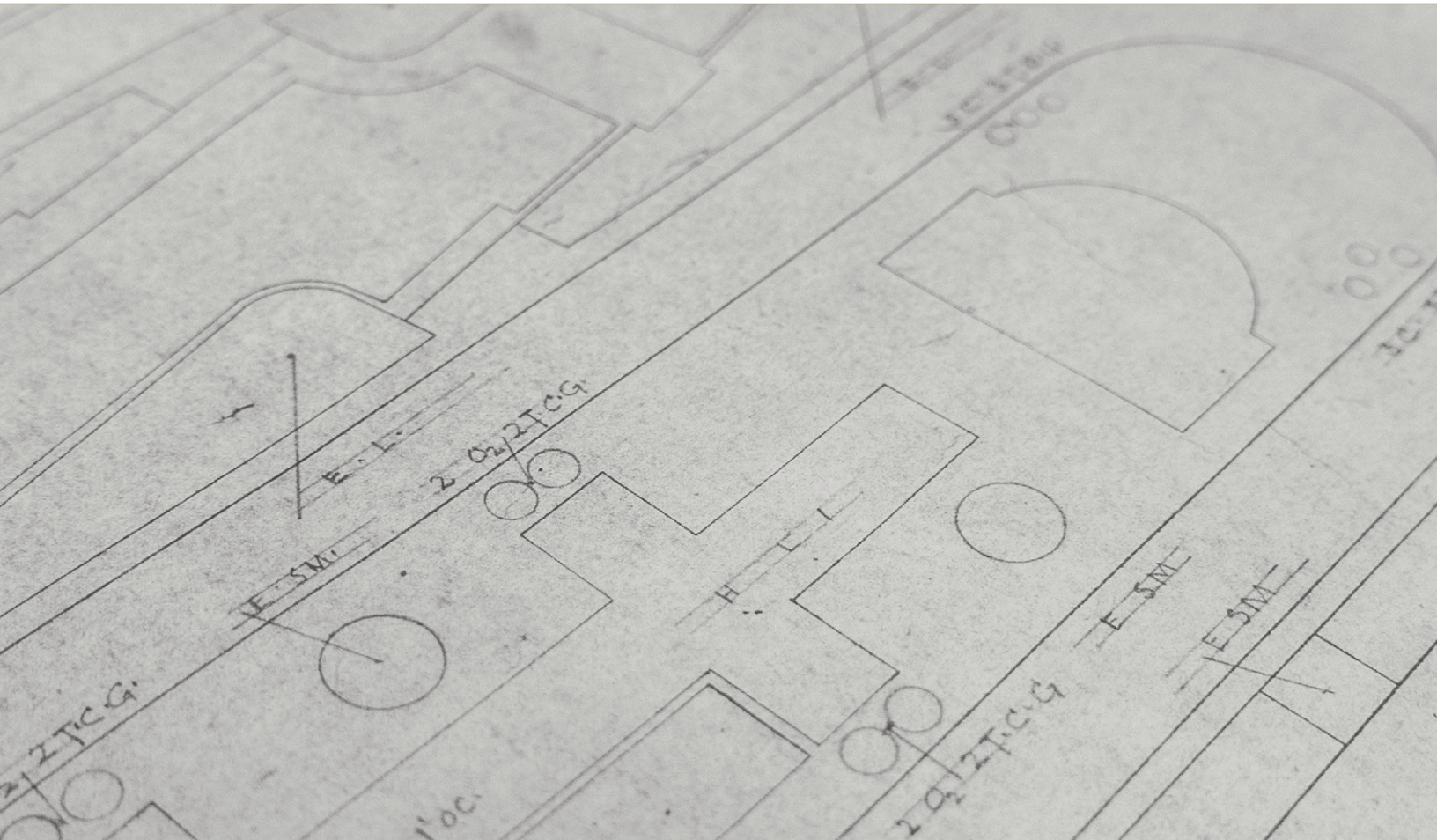


UNIVERSITY AVENUE CULTURAL HERITAGE LANDSCAPE STUDY

VOLUME B: REFERENCE MATERIALS

NOVEMBER 21, 2025



PREPARED FOR
CITY OF TORONTO

CONTACT
COMMON BOND COLLECTIVE
340 KING ST E #239
TORONTO ON
M5A 1K8

COMMON
BOND
COLLECTIVE

dtah

UNIVERSITY AVENUE CHL STUDY

VOLUME B: REFERENCE MATERIALS

TABLE OF CONTENTS

Appendix A: 1948 Plans	B1
Appendix B: 1961 Plan	B4
Appendix C: 1962 Plans	B6
Appendix D: Archival Maps, Plans and Aerial Images of the Study Area	B22
Appendix E: Island Description Sheets	B26
Appendix F: Evolution of University Avenue	B39
Appendix G: Chronology	B41

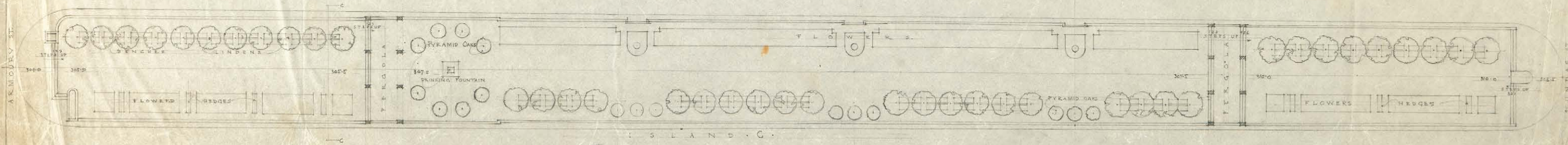
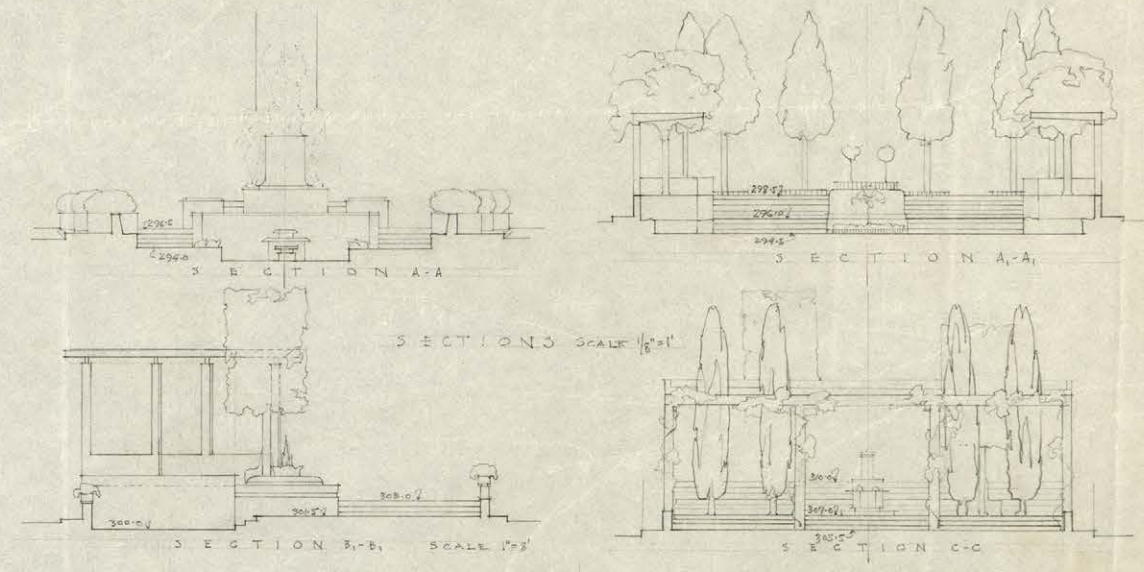
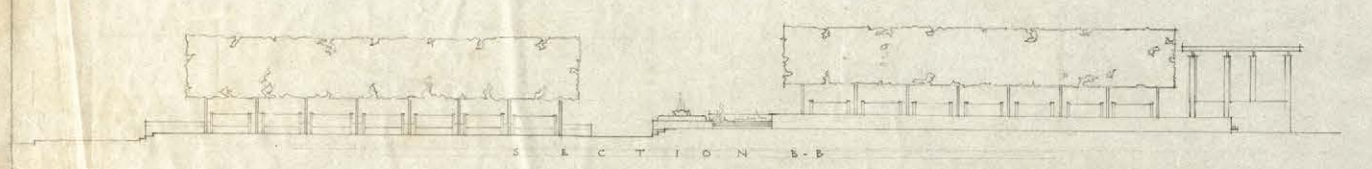
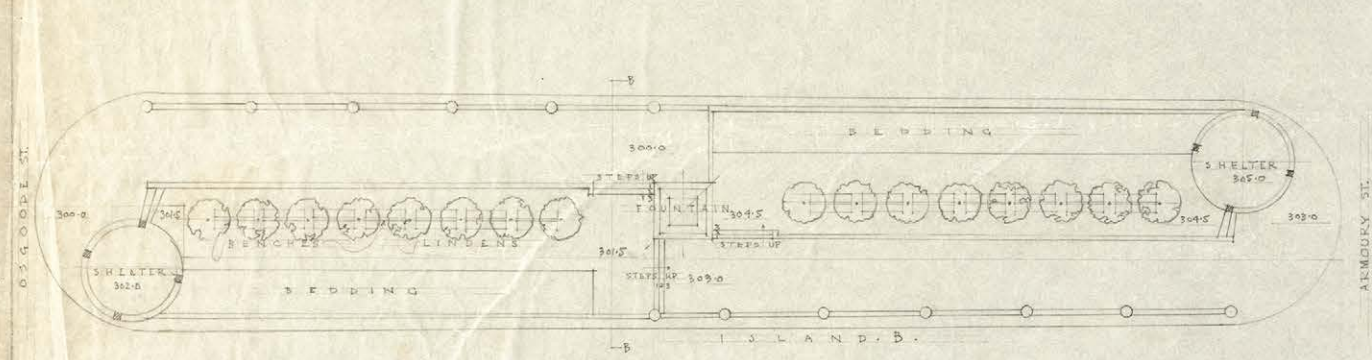
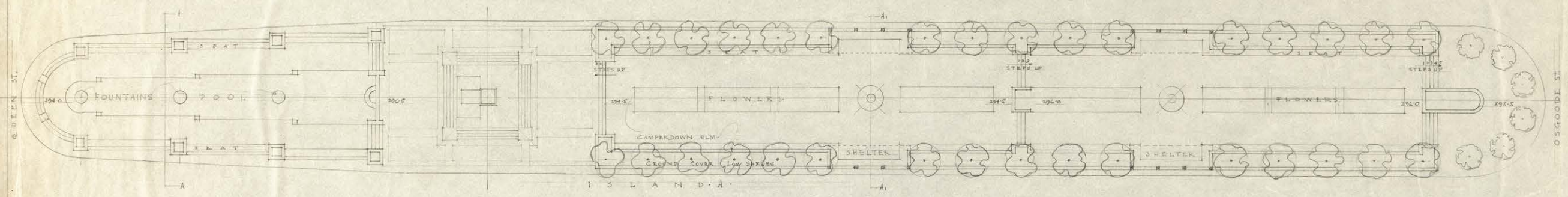
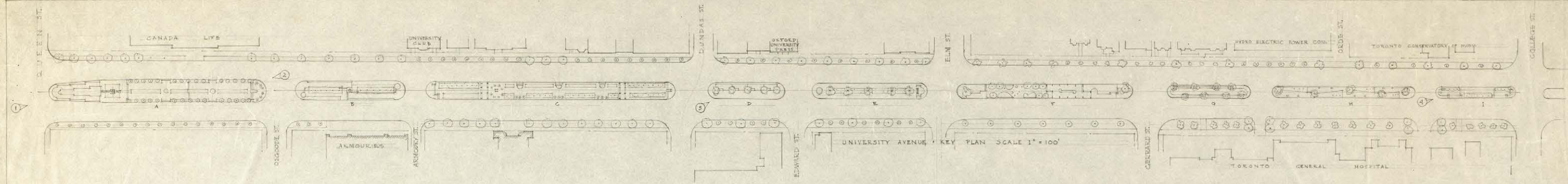
Front Cover: 1962 Dunington-Grubb & Stensson drawing set (City of Toronto Archives, Fonds 416, File 119).

APPENDIX A: 1948 PLANS

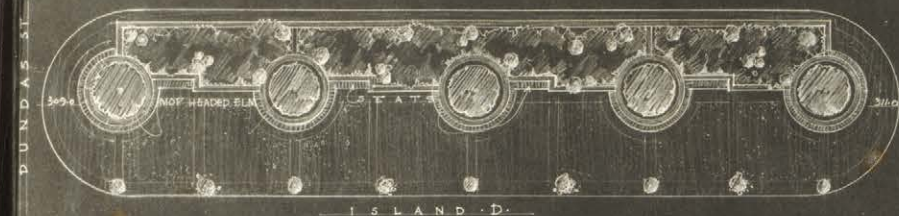
DUNINGTON-GRUBB & STENSSON, "TORONTO CITY PLANNING BOARD - UNIVERSITY AVENUE" JANUARY 20, 1948 - SHEET 1

DUNINGTON-GRUBB & STENSSON, "TORONTO CITY PLANNING BOARD - UNIVERSITY AVENUE" JANUARY 20, 1948 - SHEET 2

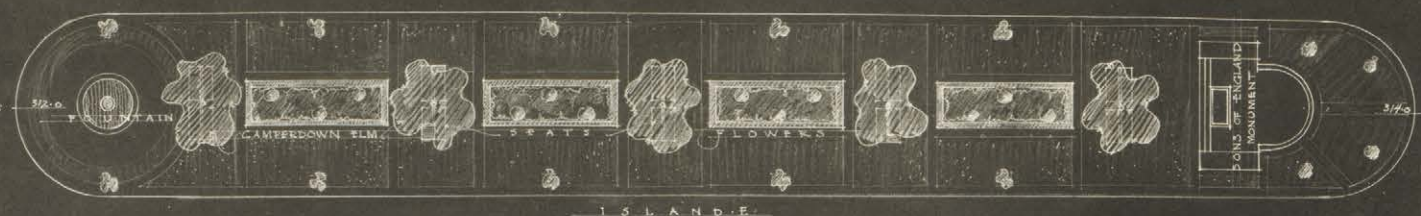
City of Toronto Archives: Fonds 416, File 216, Items 1 & 2



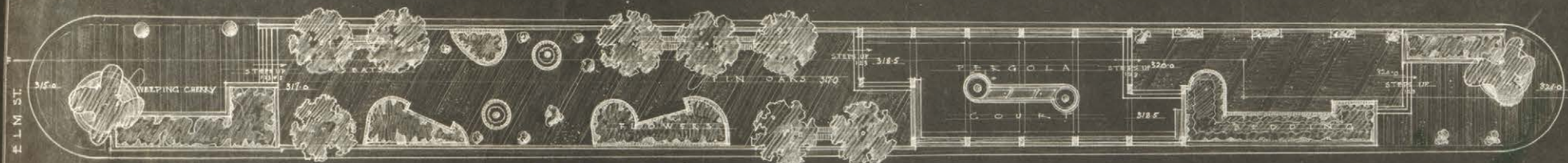
TORONTO CITY PLANNING BOARD • UNIVERSITY AVENUE
 SCALE ONE INCH EQUALS SIXTEEN FEET JANUARY 20th 1948
 Dringrup-Grubb & Sturges Landscape Architects



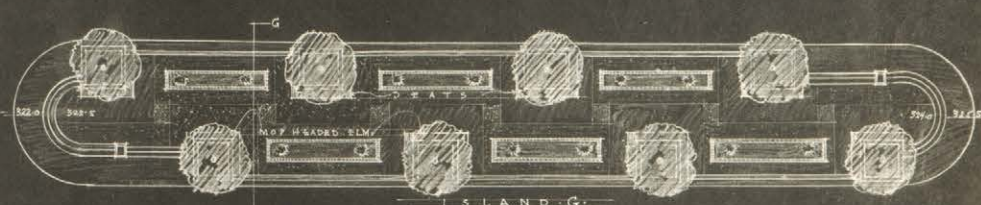
ISLAND D



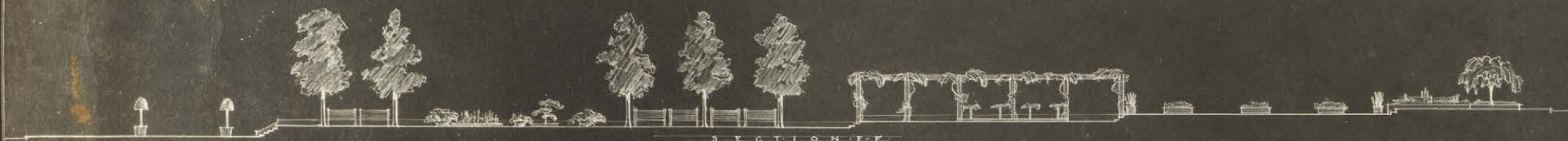
ISLAND E



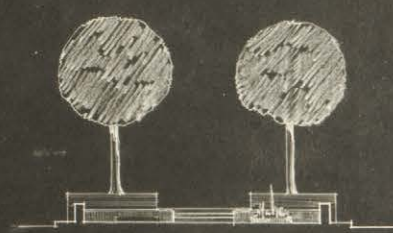
ISLAND F



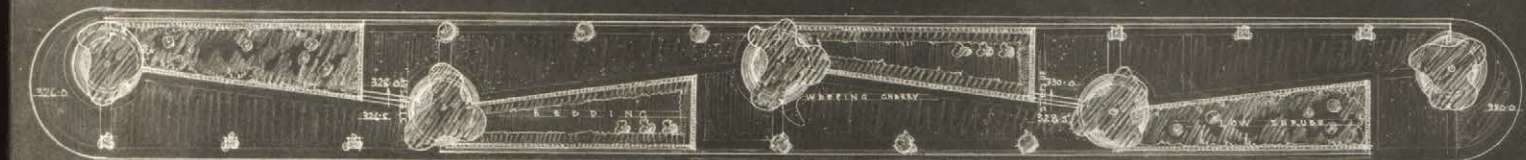
ISLAND G



SECTION F-F



SECTION G-G



ISLAND H



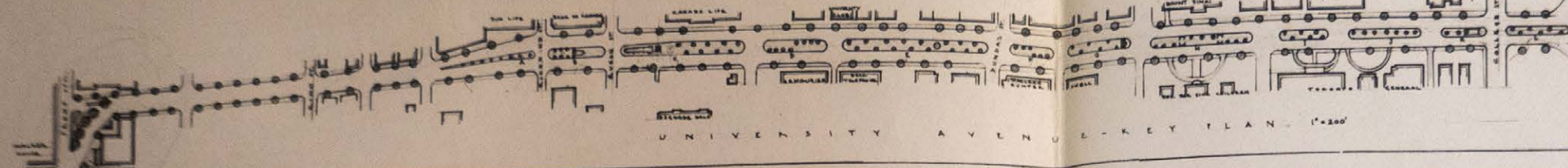
ISLAND I

TORONTO CITY PLANNING BOARD • UNIVERSITY AVENUE
 SCALE ONE INCH EQUALS SIXTEEN FEET. PROPOSED ELEVATIONS THUS: 33.0' JANUARY 20, 1948
 WALLS PAVING WATER BENCHES FLOWERS LOW SHRUBS CAMPERDOWN ELM MOYHEAD ELM TIN OAK LINDEN WEeping CHERRY
 Dringington Grubb & Sanson Landscape Architects

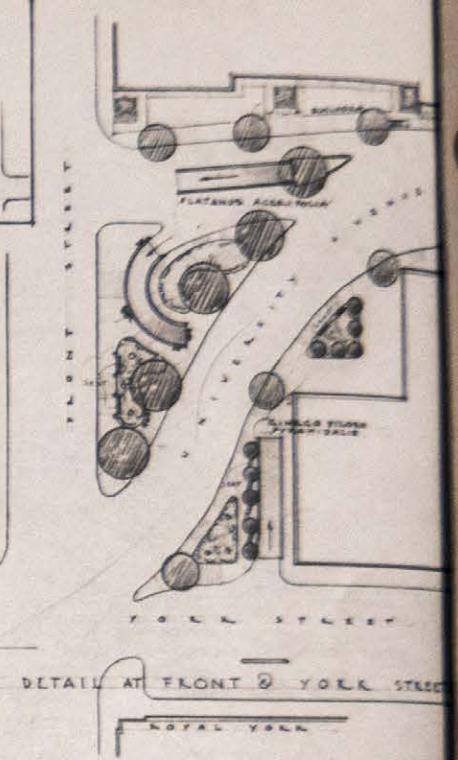
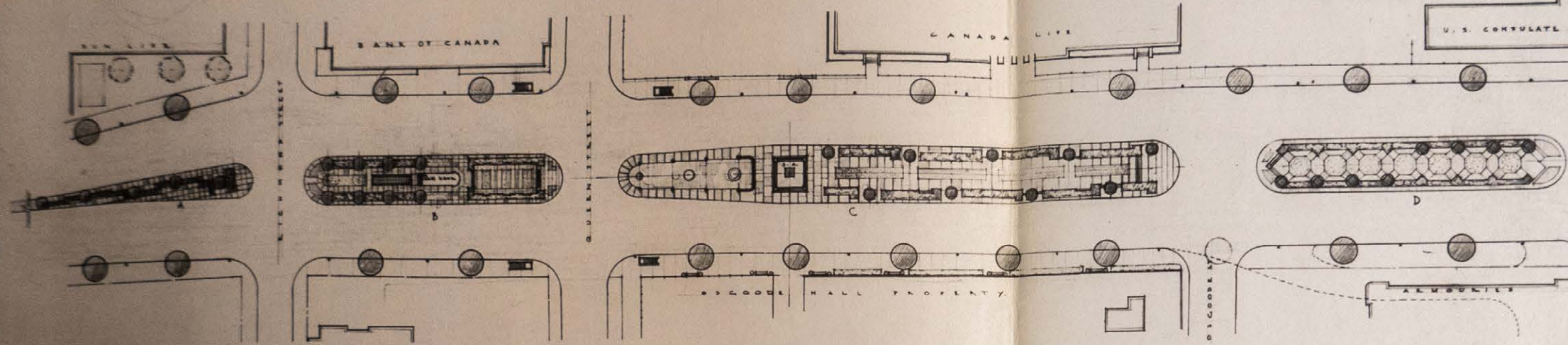
APPENDIX B: 1961 PLAN

DUNINGTON-GRUBB & STENSSON, “UNIVERSITY AVENUE MUNICIPALITY OF METROPOLITAN TORONTO A PLAN FOR ITS LANDSCAPE TREATMENT,” REVISED SEPTEMBER 1, 1961 - SHEET A

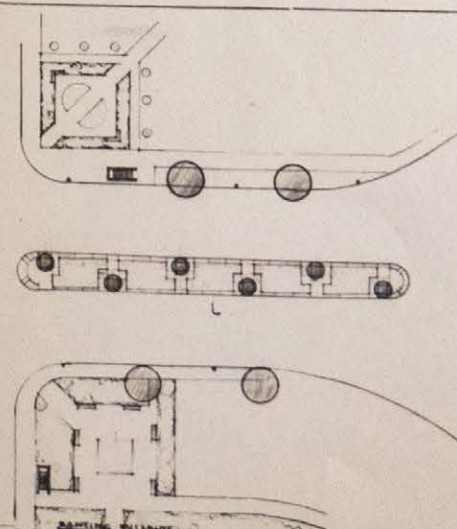
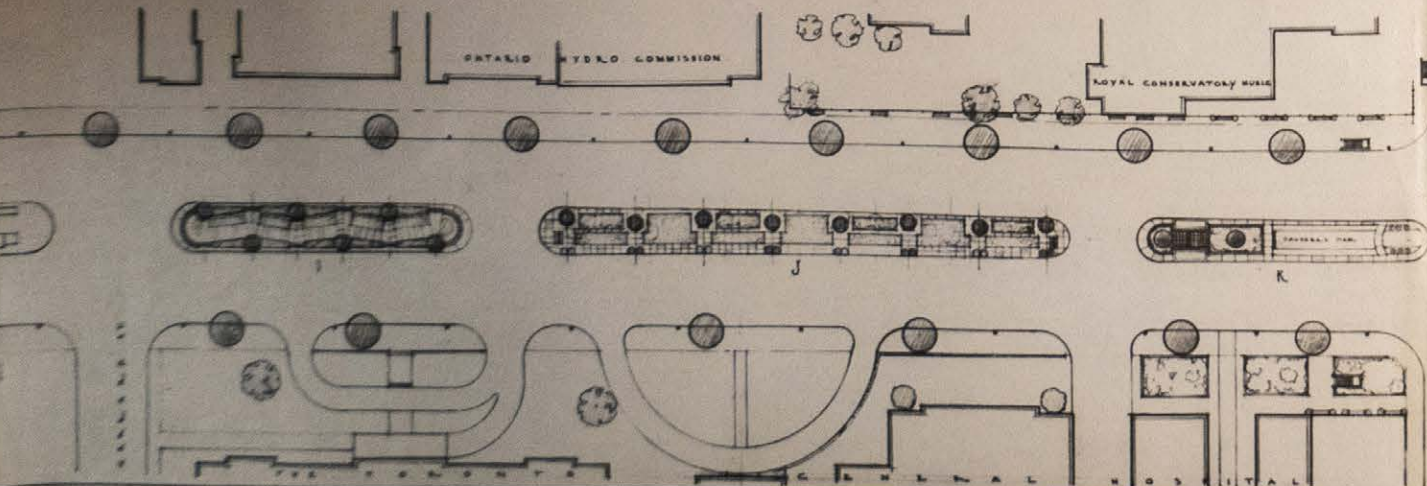
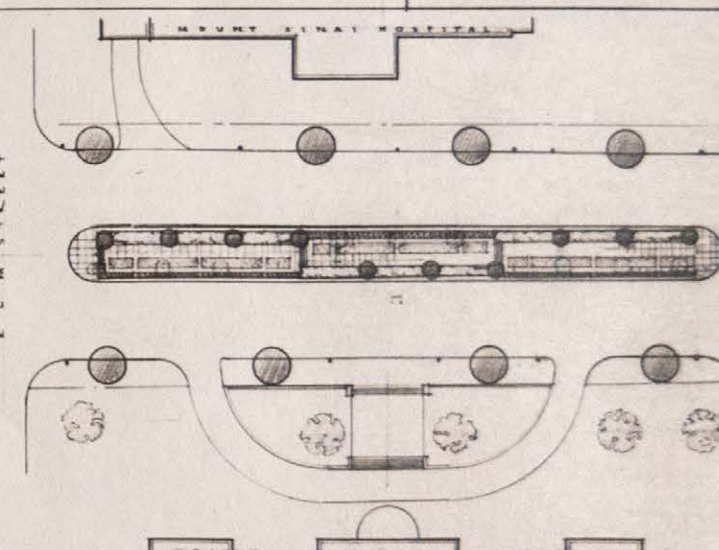
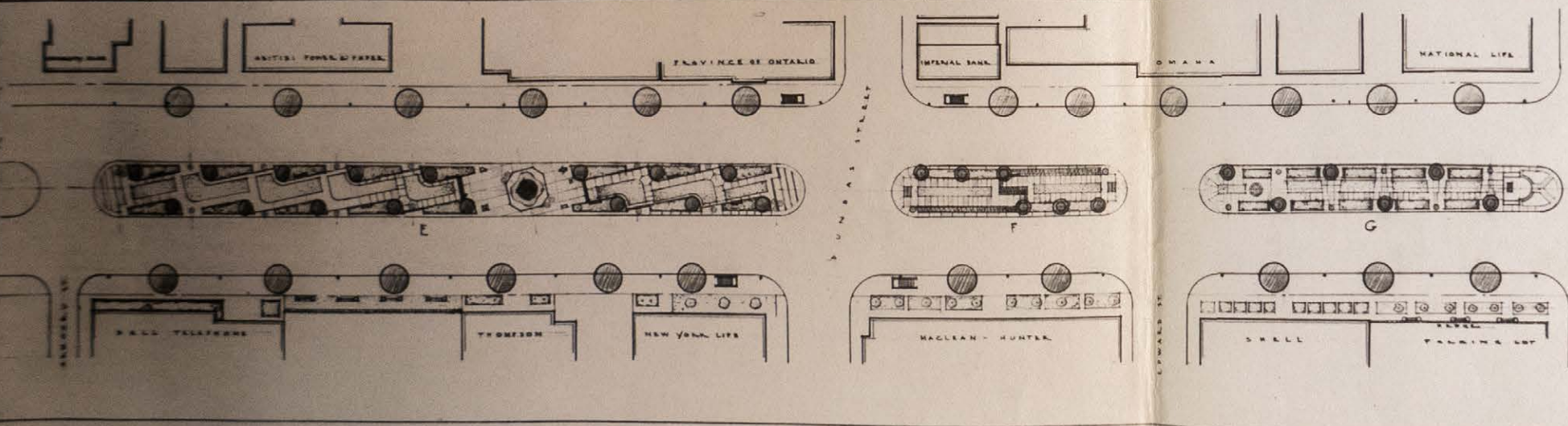
City of Toronto Archives: Fonds 416, File 33



UNIVERSITY AVENUE - KEY PLAN 1"=200'



DETAIL AT FRONT OF YORK STREET

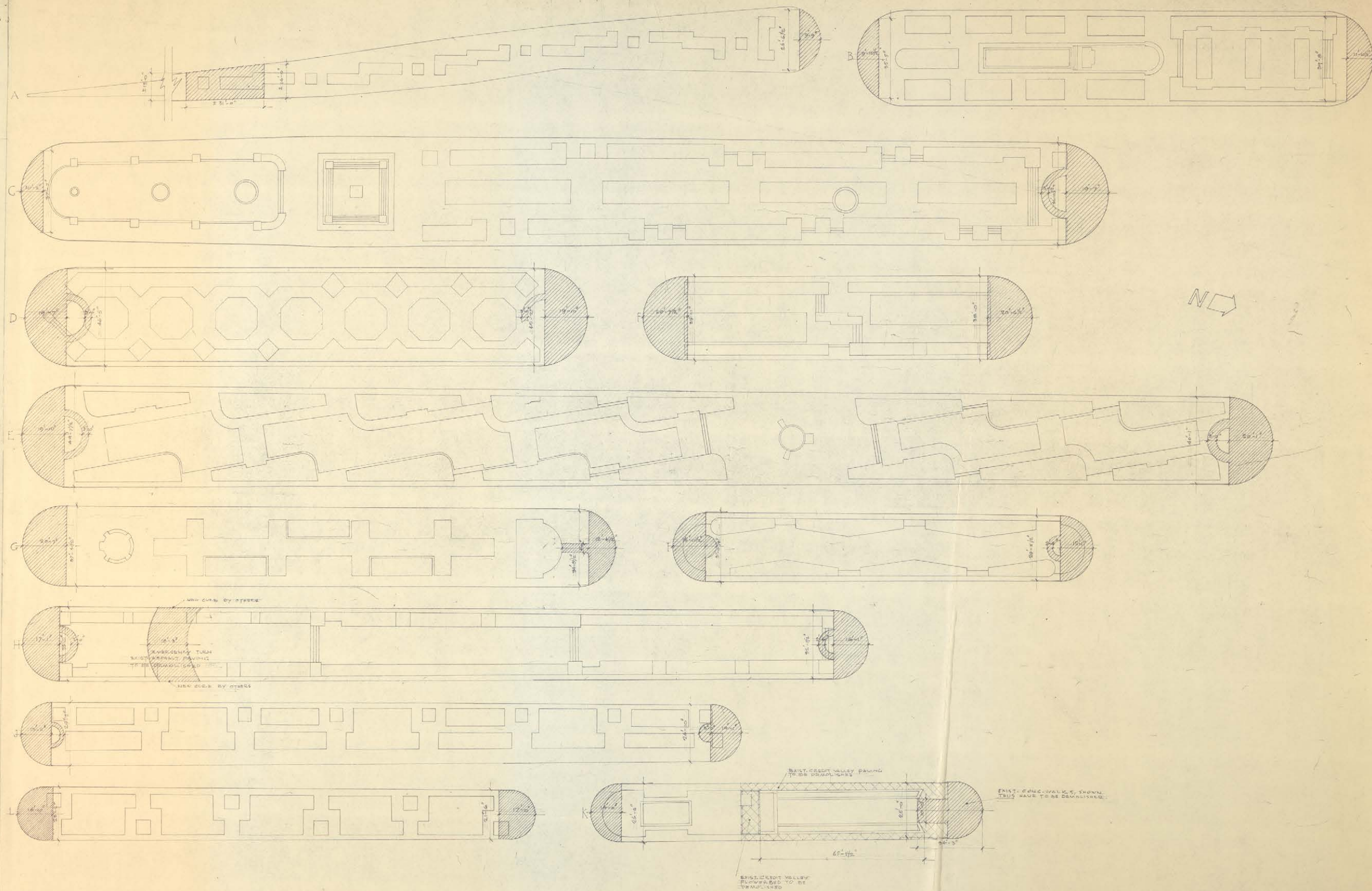


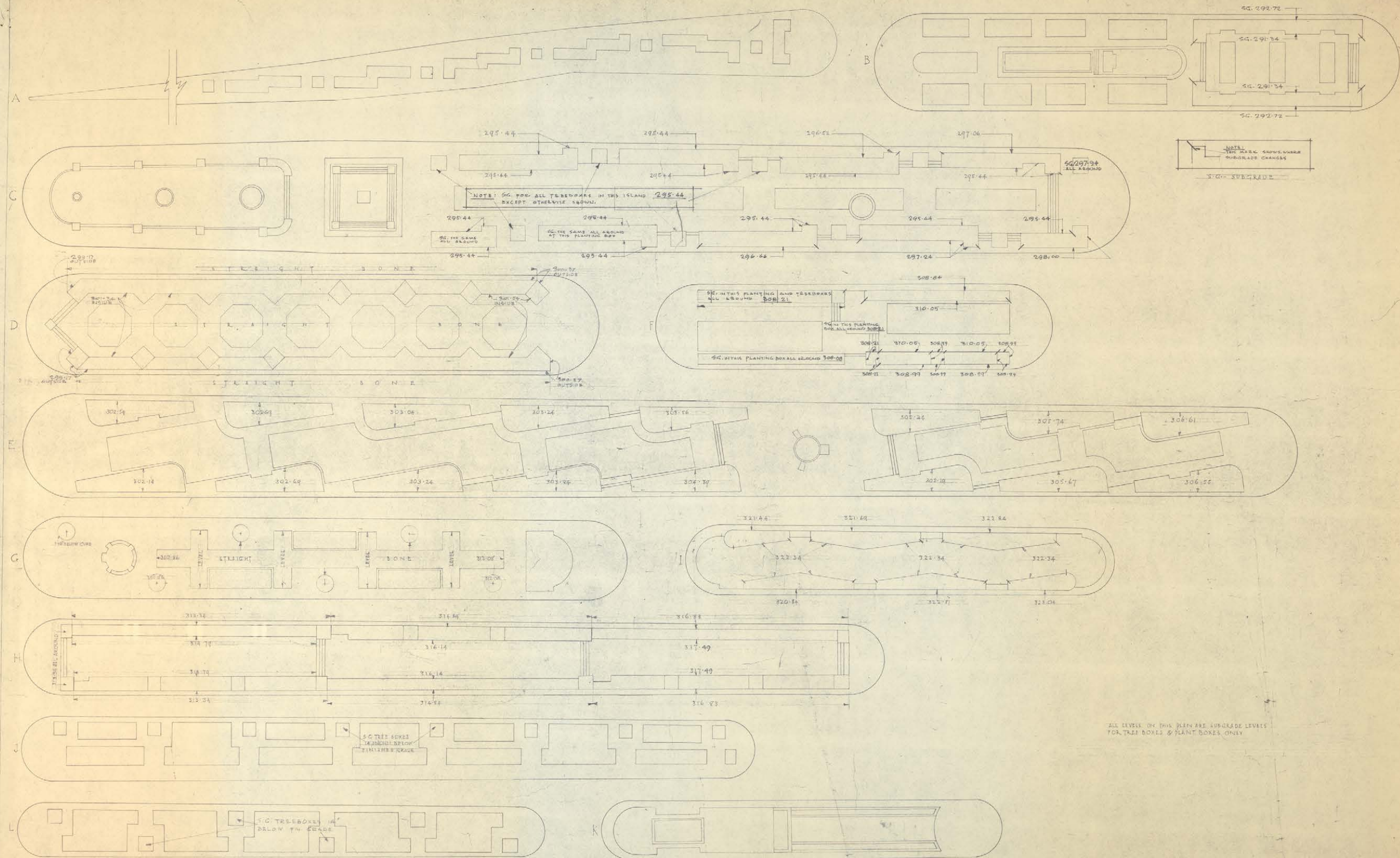
UNIVERSITY AVENUE
MUNICIPALITY OF METROPOLITAN TORONTO
PARKS DEPARTMENT
SCALE ONE INCH EQUALS FORTY FEET
BRIMINGTON CRISP & STENTON LANDSCAPE ARCHITECTS
REVISED AUG. 27, 1941
SEPT. 14, 1941

APPENDIX C: 1962 PLANS

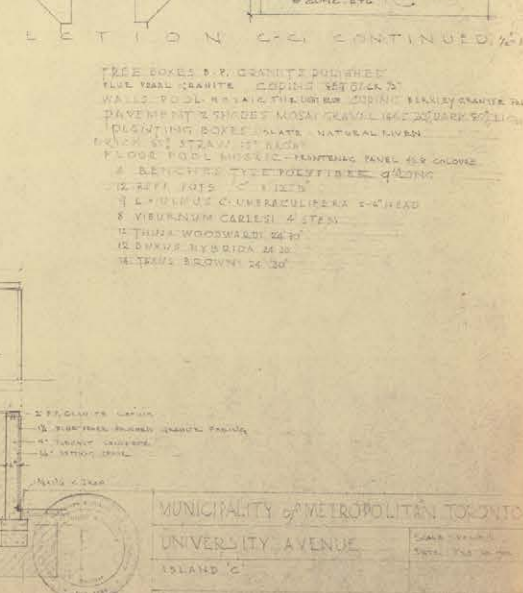
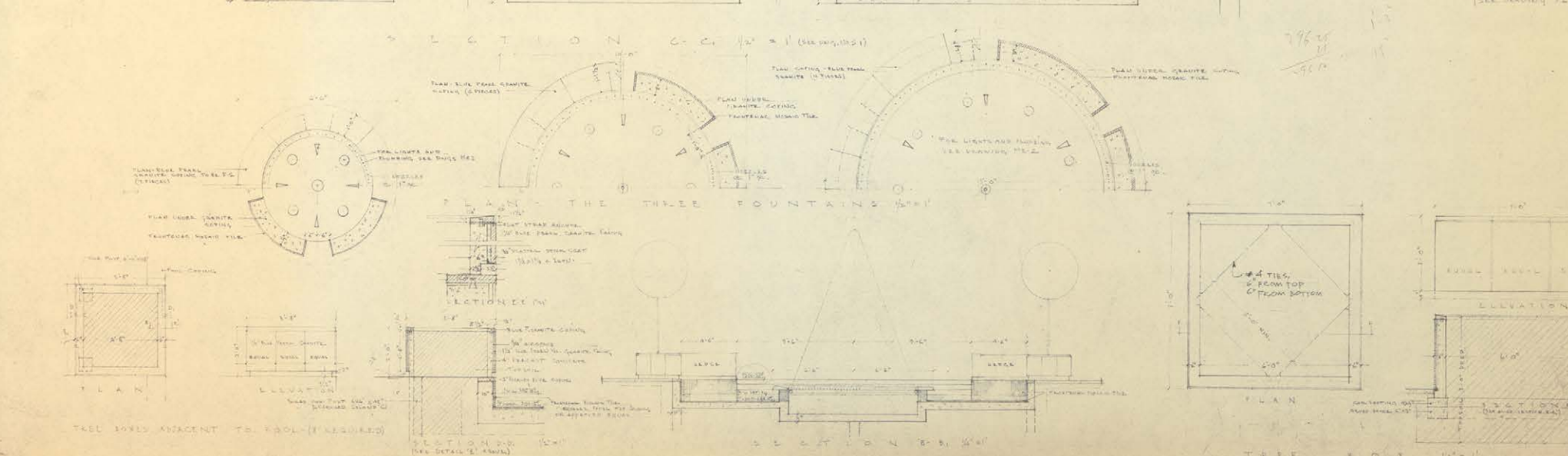
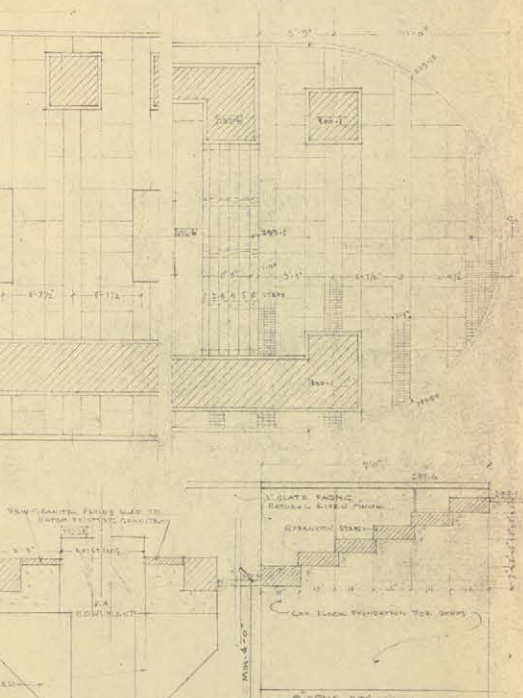
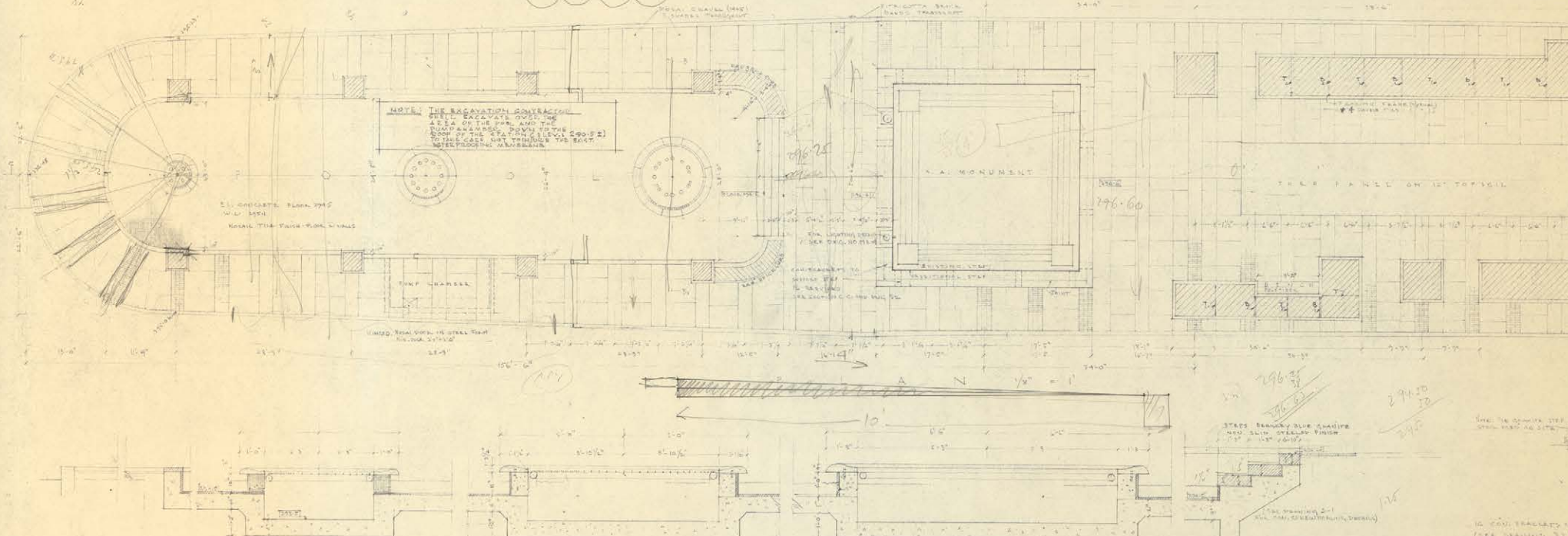
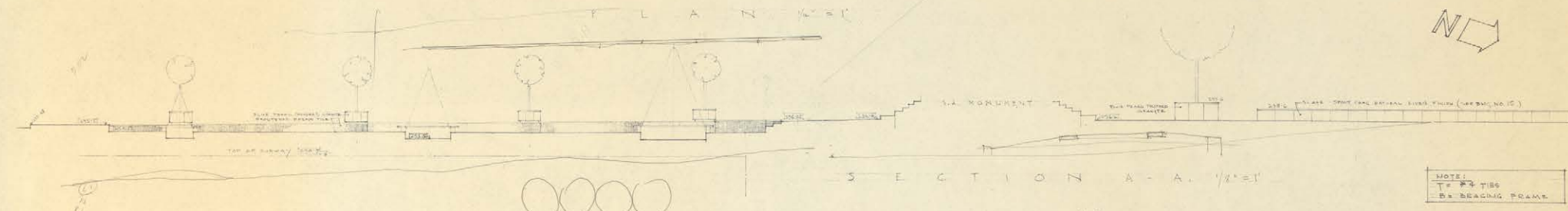
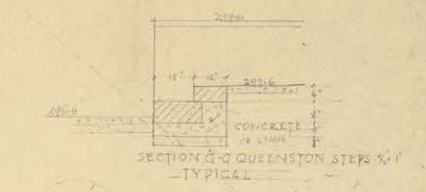
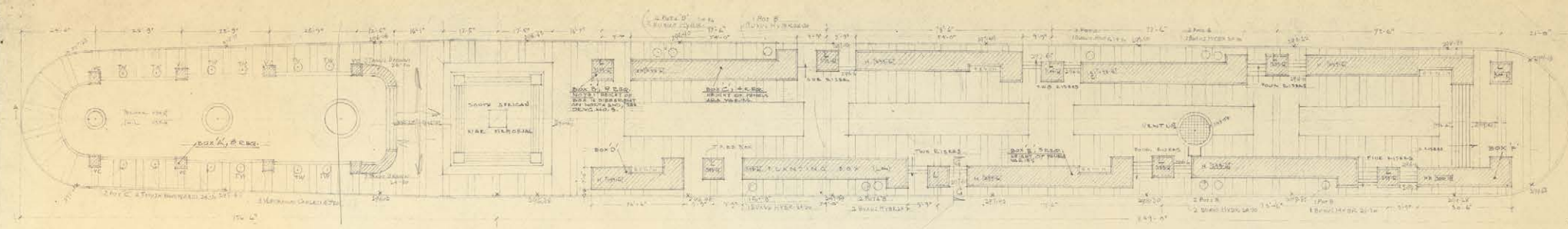
DUNINGTON-GRUBB & STENSSON, "MUNICIPALITY OF METROPOLITAN TORONTO UNIVERSITY AVENUE," FEBRUARY & JUNE 1962, VARIOUS SHEETS

City of Toronto Archives: Fonds 416, File 119



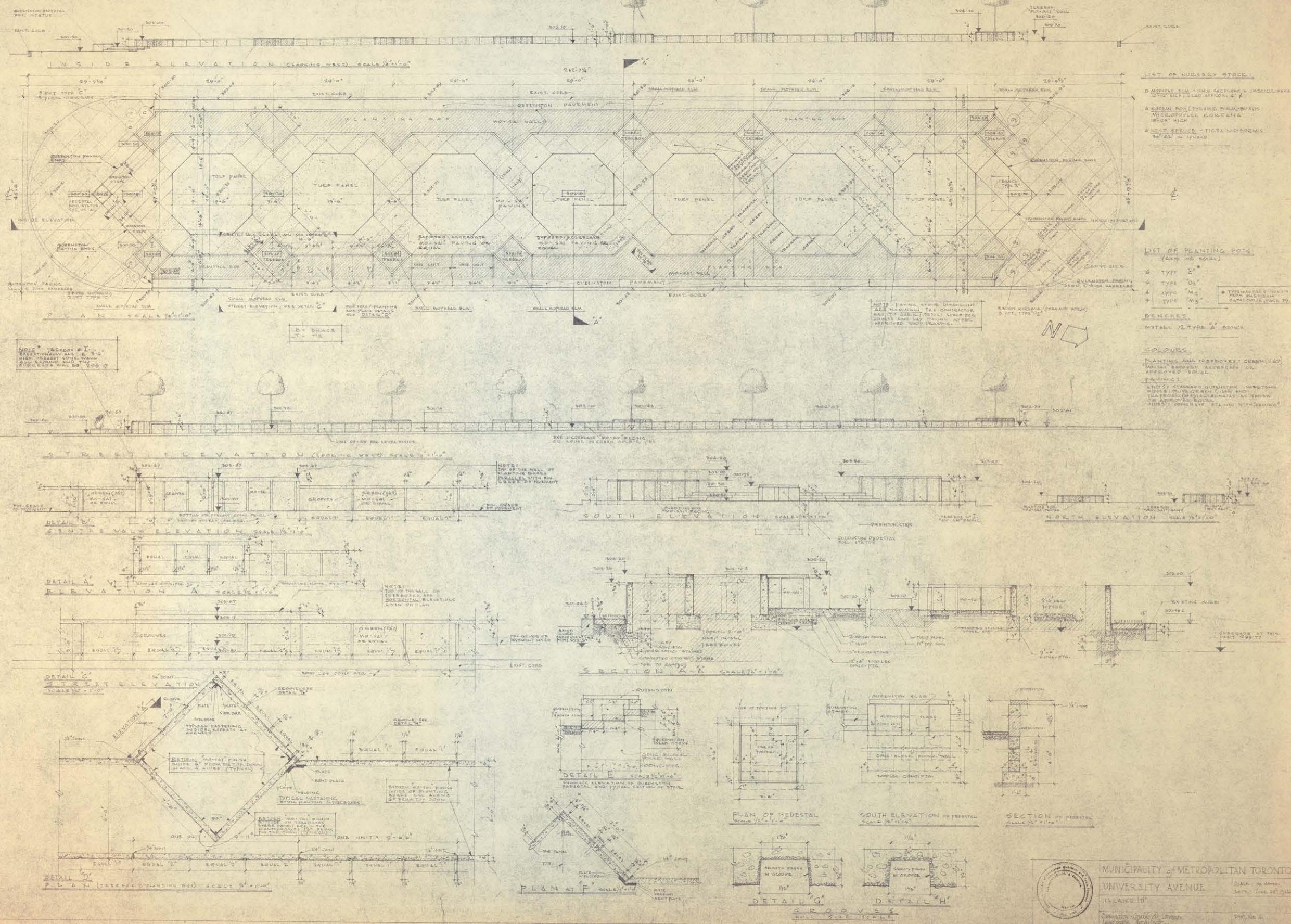


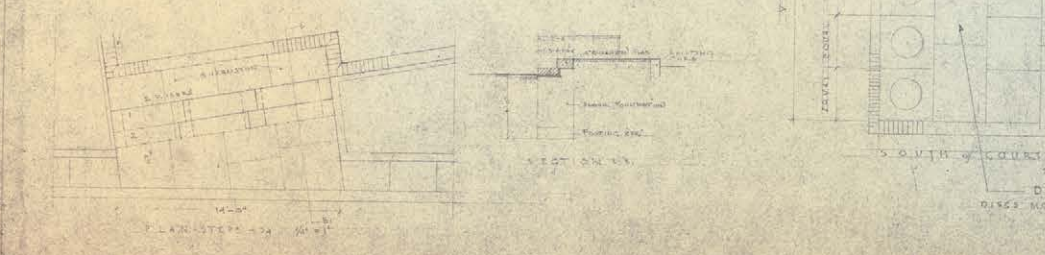
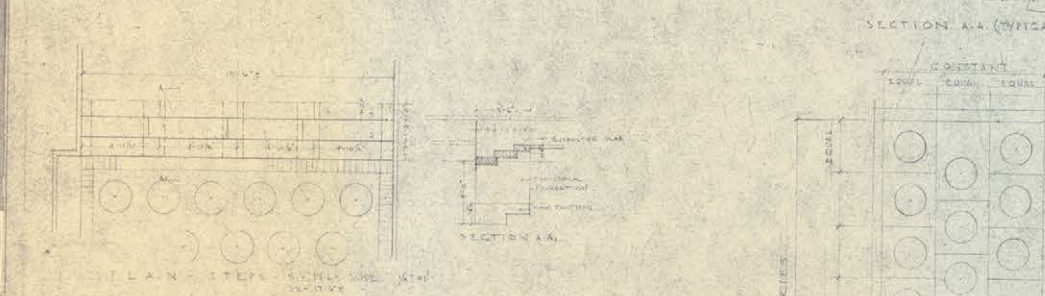
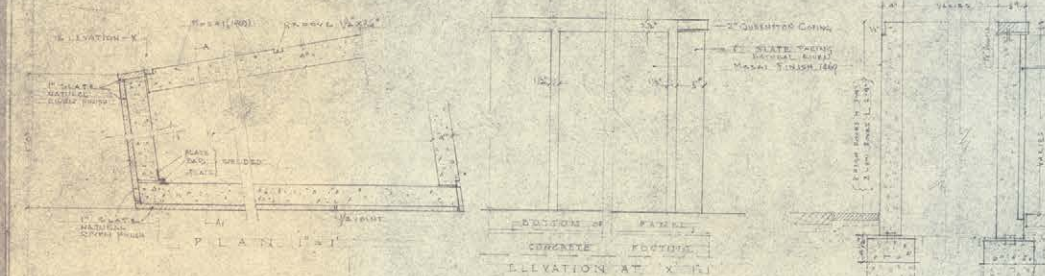
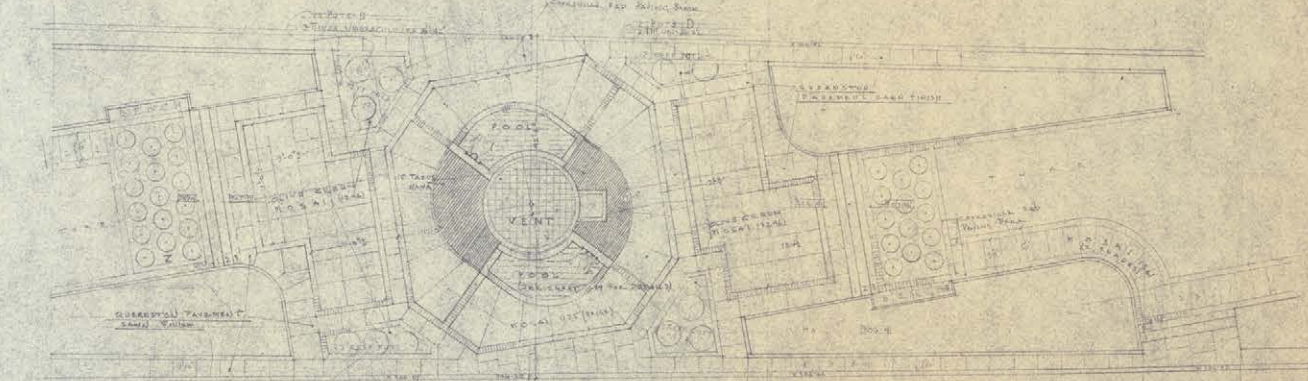
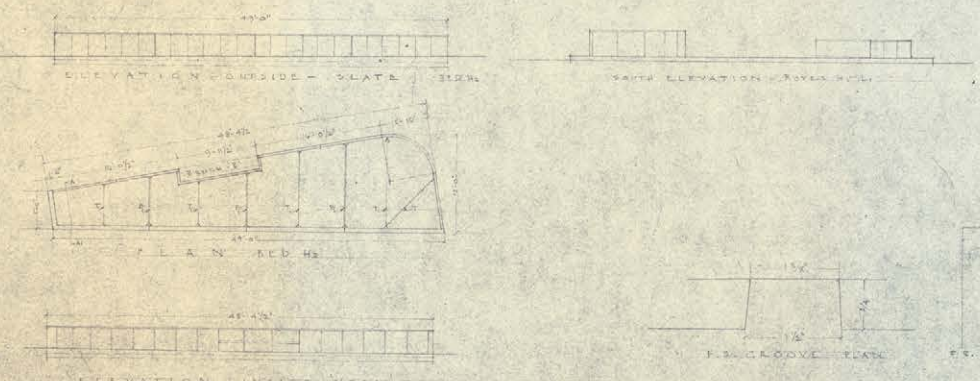
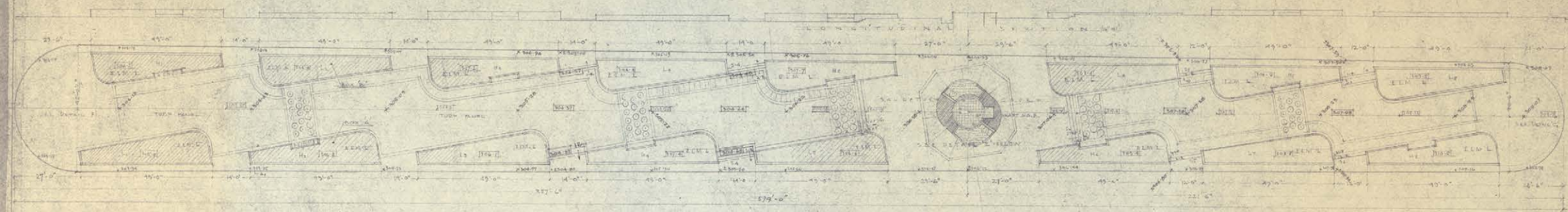
MUNICIPALITY OF METROPOLITAN TORONTO
 UNIVERSITY AVENUE
 ISLANDS A-K OVERLAP
 Dimpson, Crabb & Stenson
 Landscape Architects
 Date: 20 of Nov. 1962
 Doc. No. 3



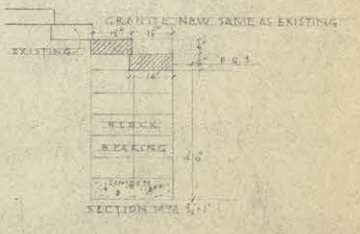
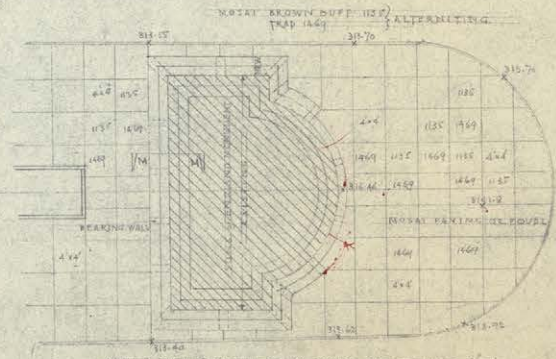
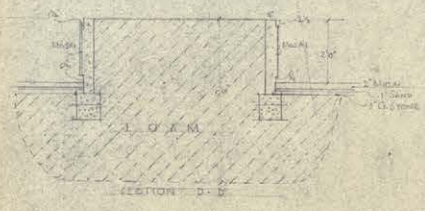
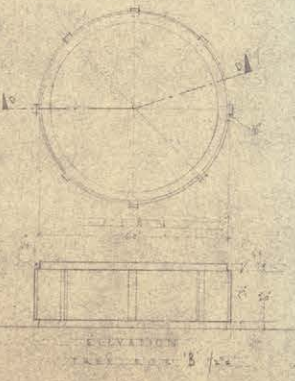
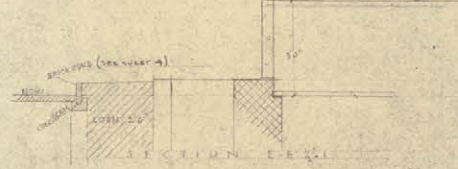
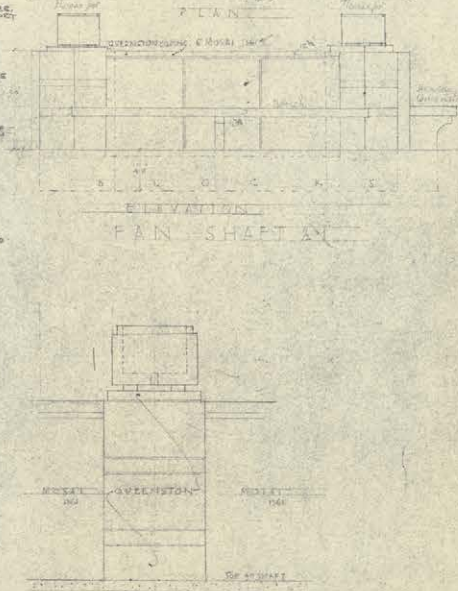
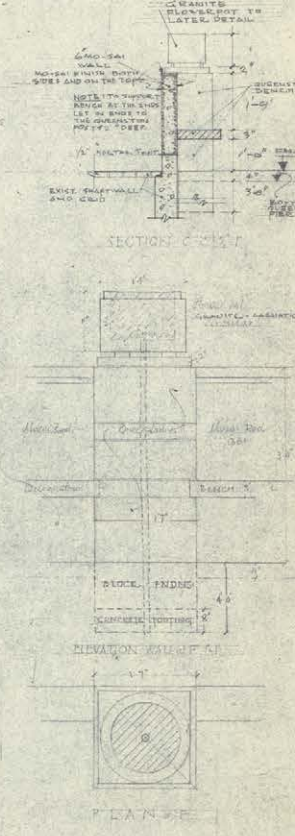
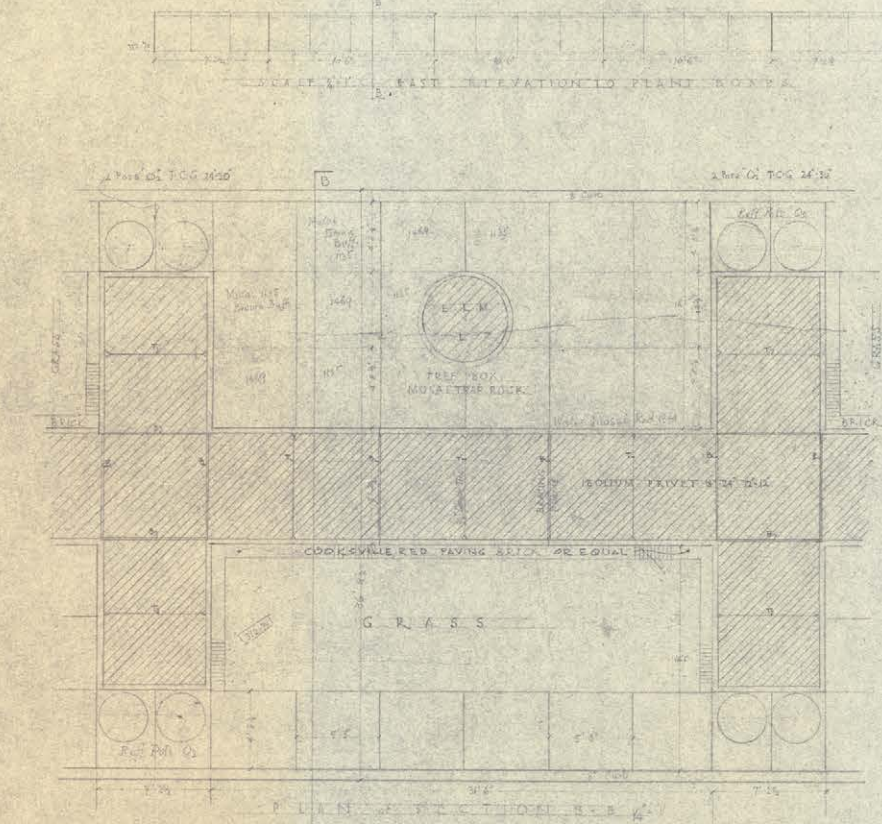
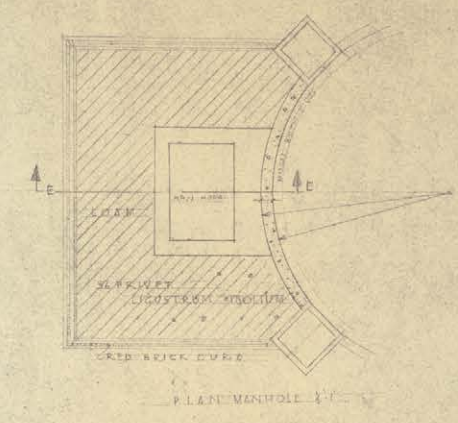
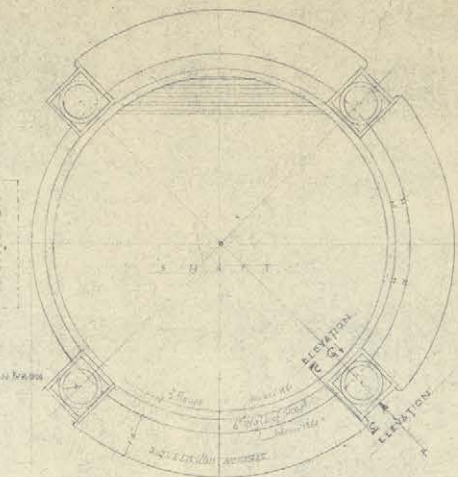
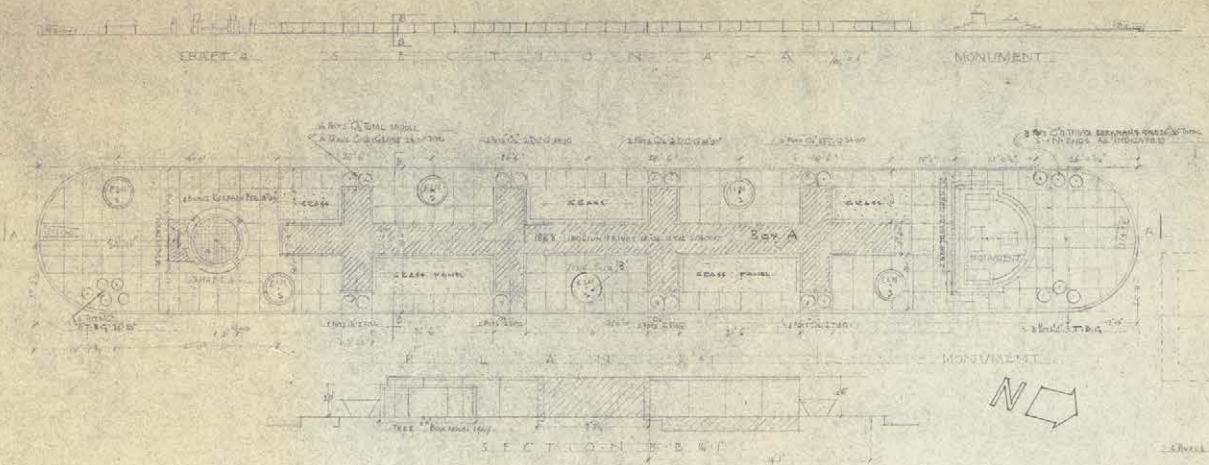
TREE BOXES 8' x 8' GRANITE PAVED
 BLUE MARBLE GRANITE COPIES SET BACK 2'
 WALLS 10' HIGHER THAN FLOOR LINE GRANITE
 PAVEMENT 3' THICK MOSAIC GRANITE
 FOUNTAIN BOXES 8' x 8' GRANITE
 FLOOR 10' HIGHER THAN FLOOR LINE
 4' BENCHES 1' x 1' x 1' x 1' x 1' x 1'
 12' x 12' x 12' x 12' x 12' x 12'
 12' x 12' x 12' x 12' x 12' x 12'
 12' x 12' x 12' x 12' x 12' x 12'
 12' x 12' x 12' x 12' x 12' x 12'
 12' x 12' x 12' x 12' x 12' x 12'

MUNICIPALITY OF METROPOLITAN TORONTO
 UNIVERSITY AVENUE
 ISLAND 'C'
 1910





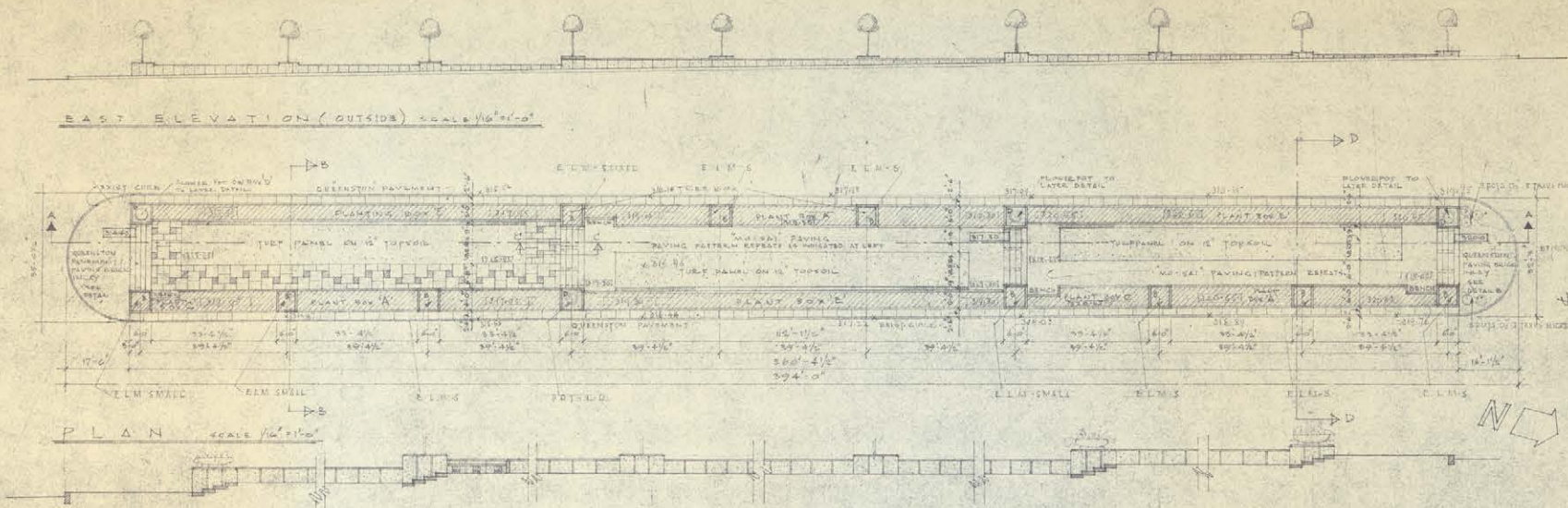
MUNICIPALITY OF METROPOLITAN TORONTO
 UNIVERSITY AVENUE
 ISLAND E
 DATE: 1907
 DRAWN BY: 7



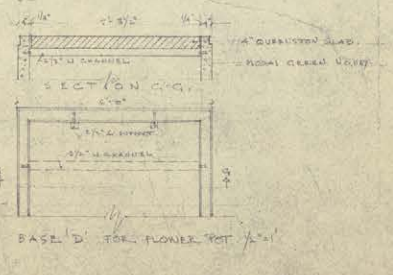
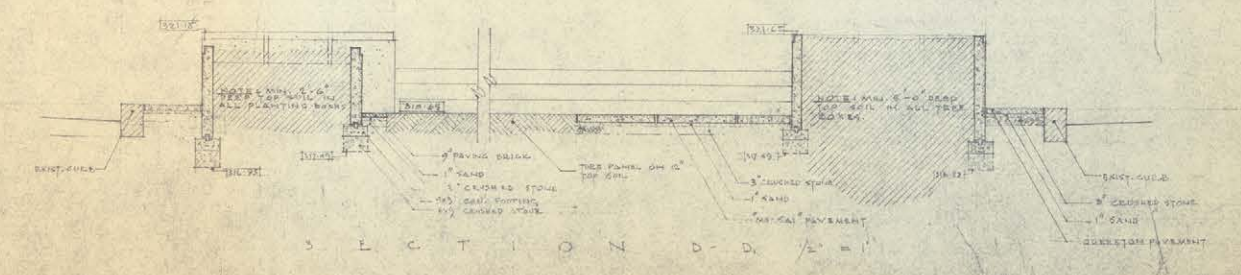
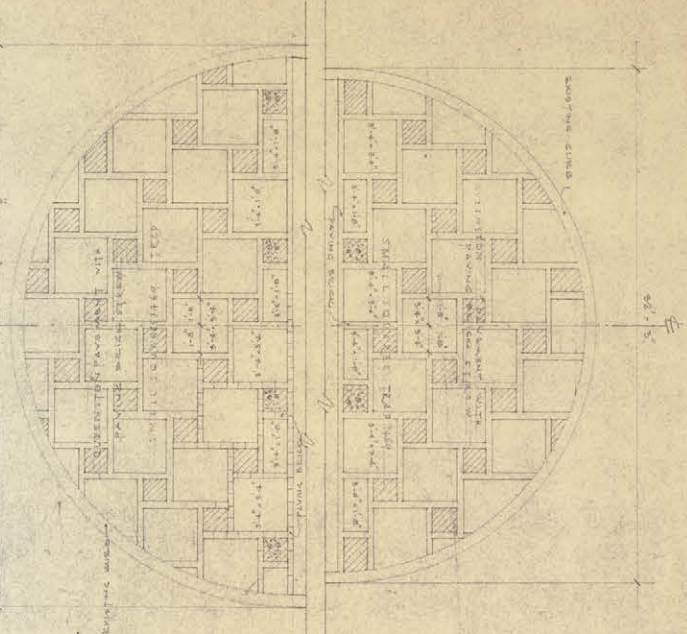
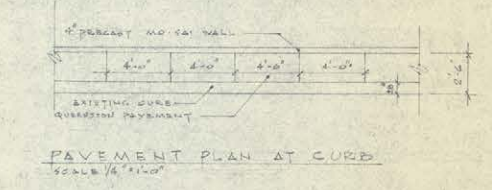
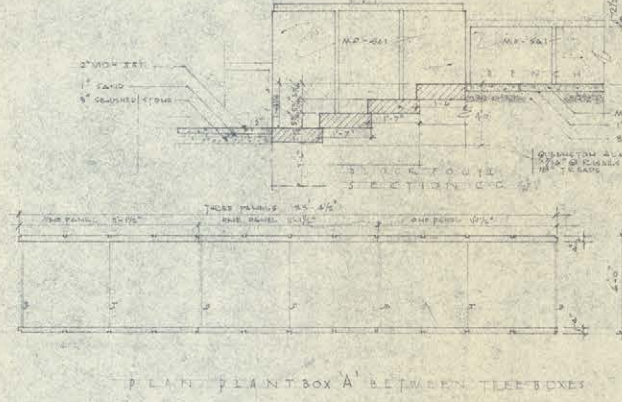
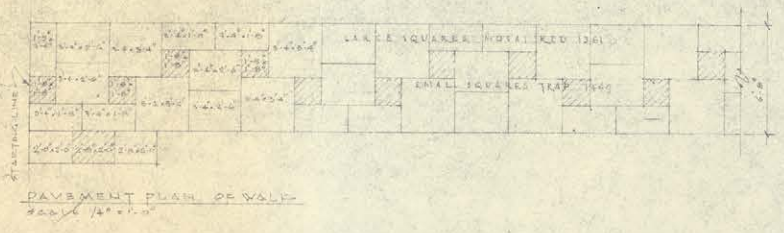
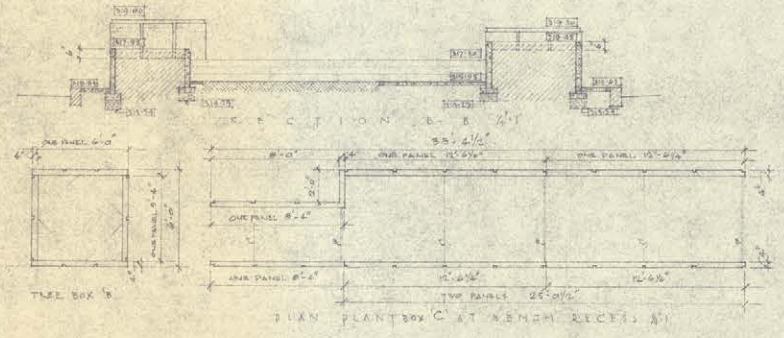
TREE BOXES MOSAIC TRAP 1469
 PLANTING BOXES MOSAIC RED 1461
 PAVING MOSAIC BROWN BUFF & TRAP 1461
 PAVING BRICK COUSSEVILLE 146

REF. POTS 11'0" 16'0"
 6 ELM. S. - ULMUS CARPINIFOLIA IMBRICATA SHADE 12-24 1465
 12-24 1465
 12-24 1465
 12-24 1465
 12-24 1465

MUNICIPALITY OF METROPOLITAN TORONTO	
UNIVERSITY AVENUE	DATE 1900
ILLINOIS	DATE 1900
ILLINOIS	DATE 1900



A-A SECTION AND EAST ELEVATION (INSIDE) SCALE 1/8" = 1'-0"

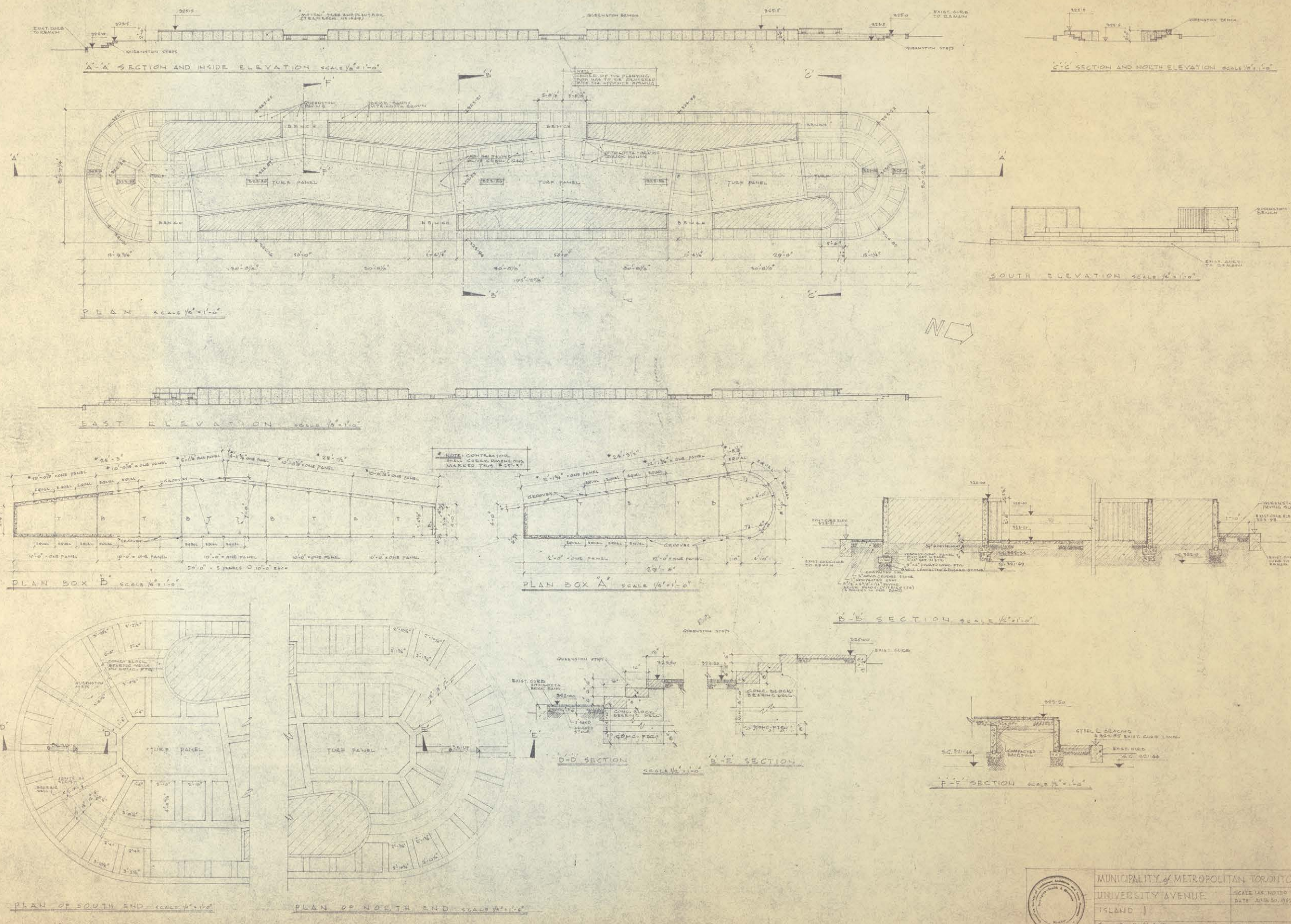


TREE BOXES MOD. GREEN 407
PLANTING BOXES ORIVE GR. 1046
PAVING LANS SQUARES TRAP ROCK MESA 1041
SMALL SQUARES, ETC. MOD. 1041
END WORKS (CONCRETE) AND CENTER WORK
BRICK STRAW

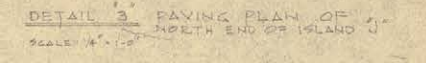
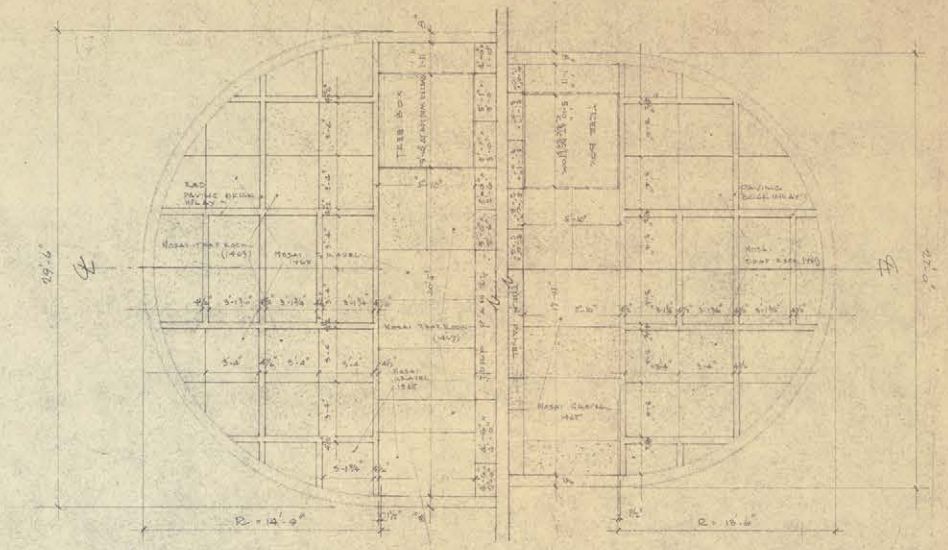
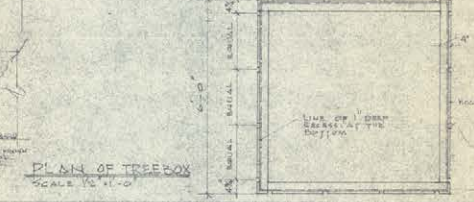
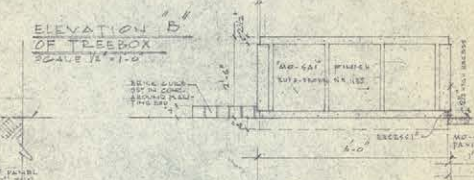
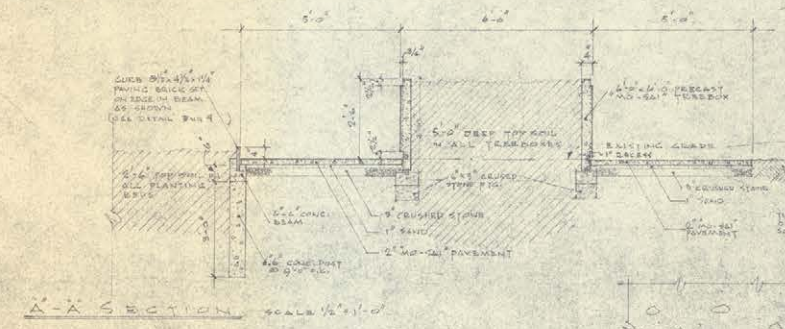
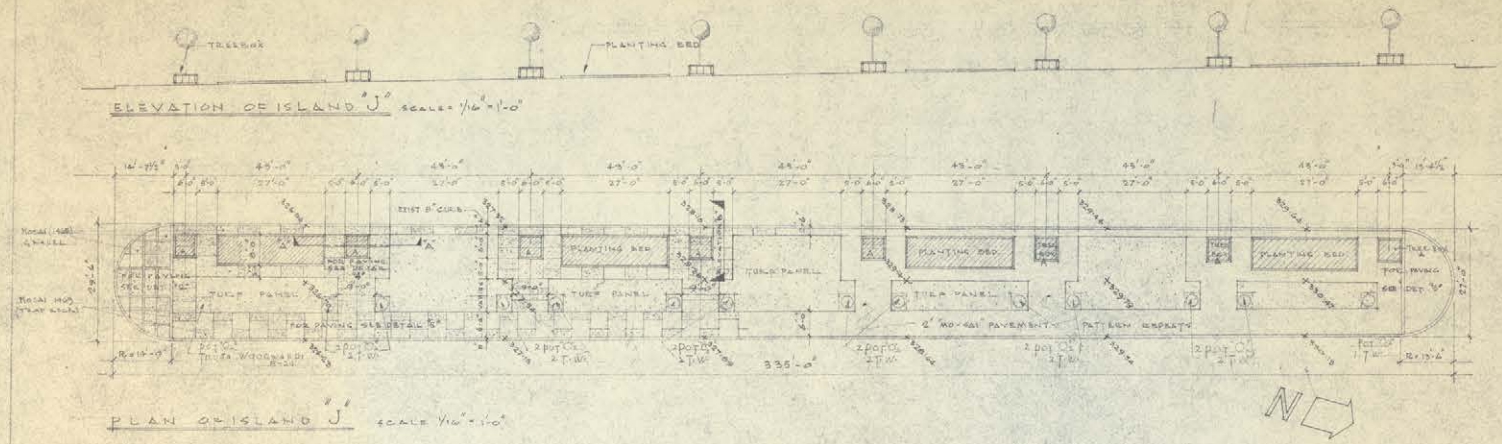
1. 12" LATERAL DETAIL (SEE SHEET NO. 1)
12" DOLLARS
12" TANKS HICKEY 10 50
A BENCH TYPE 10

ELM-5 - QUERUS ILEX HYBRIDA UNANIMIFLORA WATSON

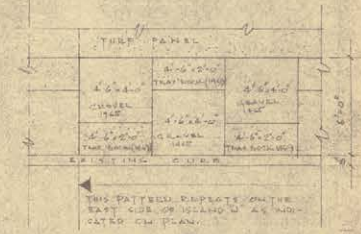
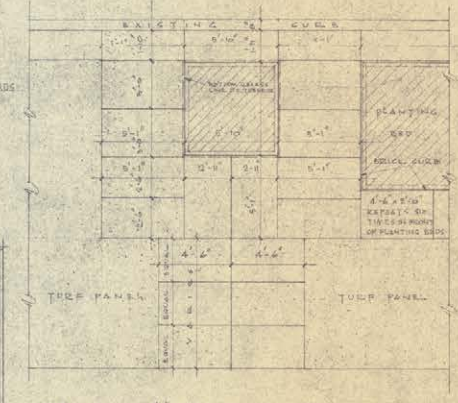
A SHOW EXISTING LEVEL
[] PROPOSED LEVELS



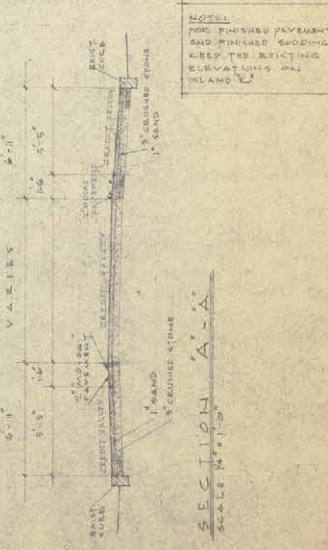
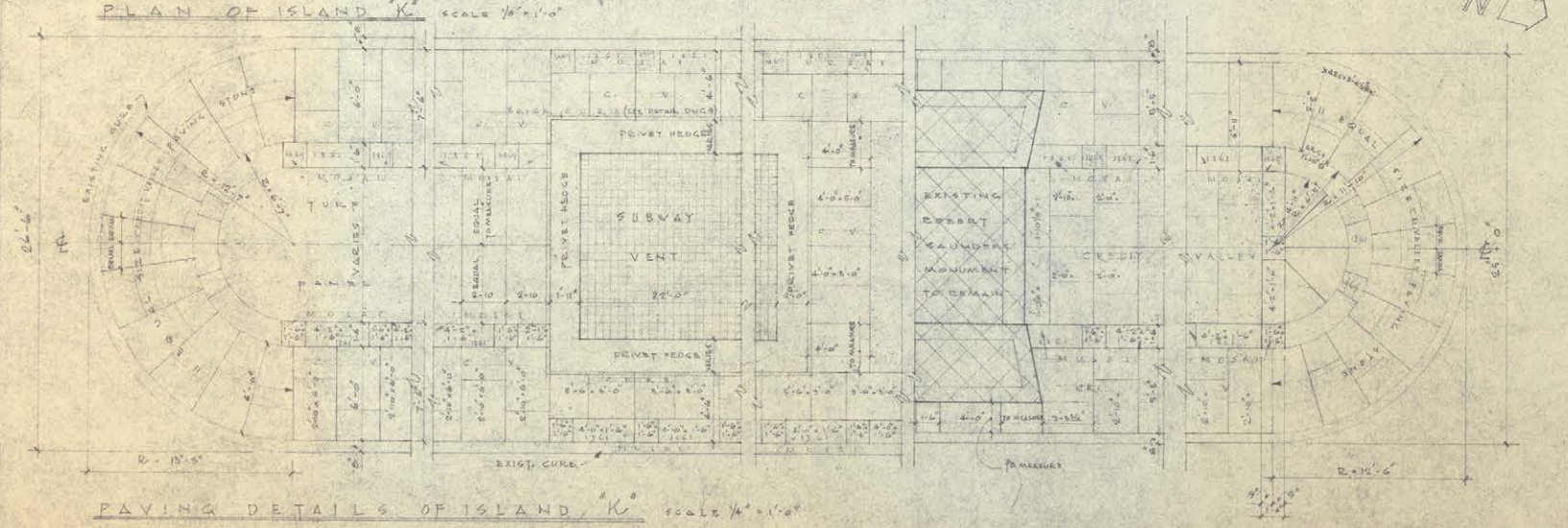
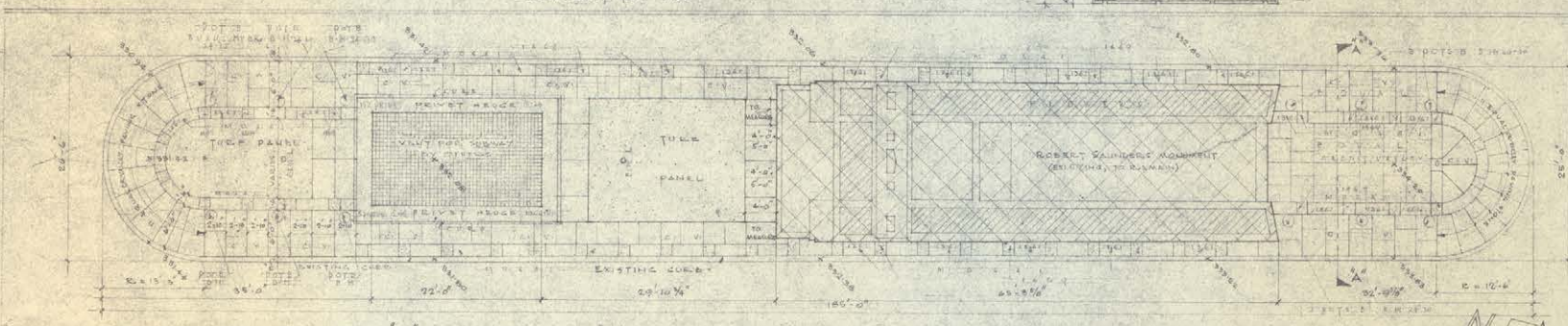
MUNICIPALITY OF METROPOLITAN TORONTO		
UNIVERSITY AVENUE		SCALE (AS NOTED)
ISLAND 1		DATE: APR 30, 1922
Prepared by: Smith & Johnston		DEVELOPED BY: J. J.
Engineer: Smith & Johnston		



NOTE: FOR FINISHED PAVEMENT AND FINISHED SODDING KEEP THE EXISTING ELEVATIONS ON ISLAND J.

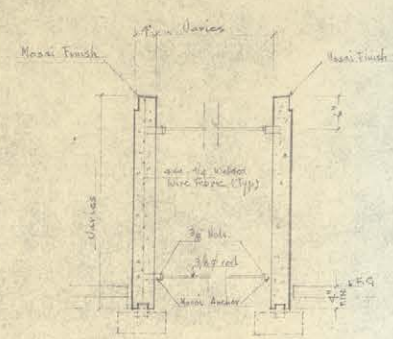


TREE BOXES: MONUMENT STONE CURBS: RED BRICK PAVING: TRIPLEX GRAVEL: ALTERNATING ENDS: RED BRICK & MOSAIC OR PATS OF 4" THICK WOODWARD 24" x 36"

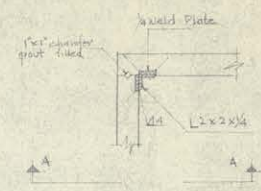


LEGEND:
 - = EXISTING ELEVATION
 - = PROPOSED (NEW) ELEVATION
 - = PLANTING
 - = 2" x 4" x 8" MONUMENT STONE CURBS
 - = 12" x 12" x 4" GRAVEL
 - = PAVEMENT: CREST VALLEY AND MOSAIC
 - = CURBS: RED BRICK

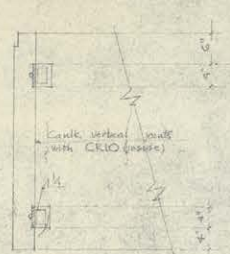
REVISIONS:



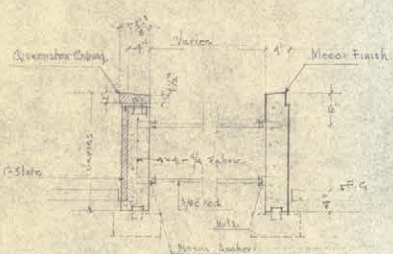
DETAIL (1) TYPICAL SECTION - 1/2"=1'



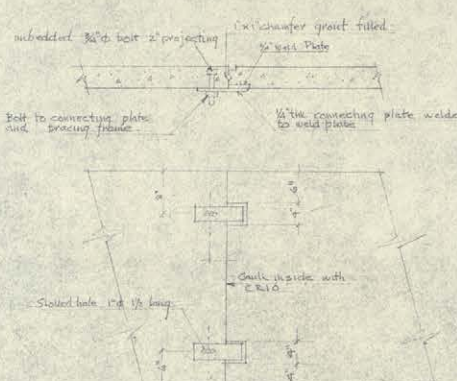
8 TYPICAL CORNER CONNECTION



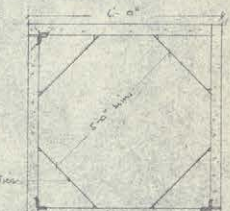
SECTION A-A



2 TYPICAL SECTION SLATE & MOSAIC FACING - 1/2"=1'



9 PANEL CONNECTION 1/2"=1'

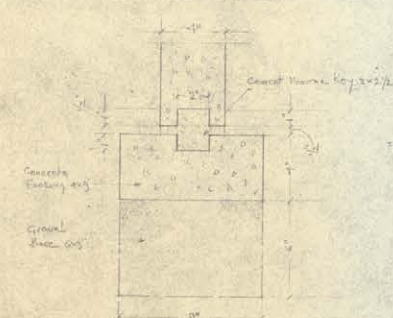


10 TYPICAL BRACING - TREE BOX (ISLAND) 1/2"=1' (See Detail 8 & 9)



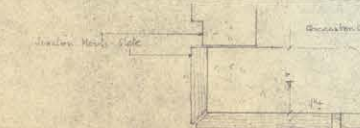
3 TYPICAL BRACING FRAME 1/2"=1'

- NOTES**
1. Structural steel angles & bracing frames to be given coats of asphaltic paint.
 2. All panel and bracing frame connections to be painted with an approved bituminous paint after erection.
 3. Bracing frame hooks to be placed on inside face below 6" floor level.
 4. If thick wall to be keyed to 4" x 8" concrete footing.



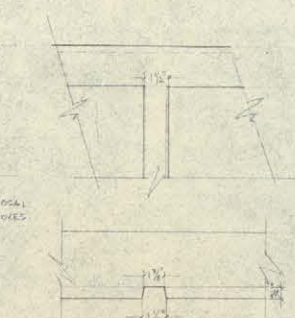
6 TYPICAL TOP OF MOSAIC WALL PLANT & TREE BOXES 1/2"=1'

4 SECTION - FOOTING PRECAST WALLS SCALE 3/4"=1'

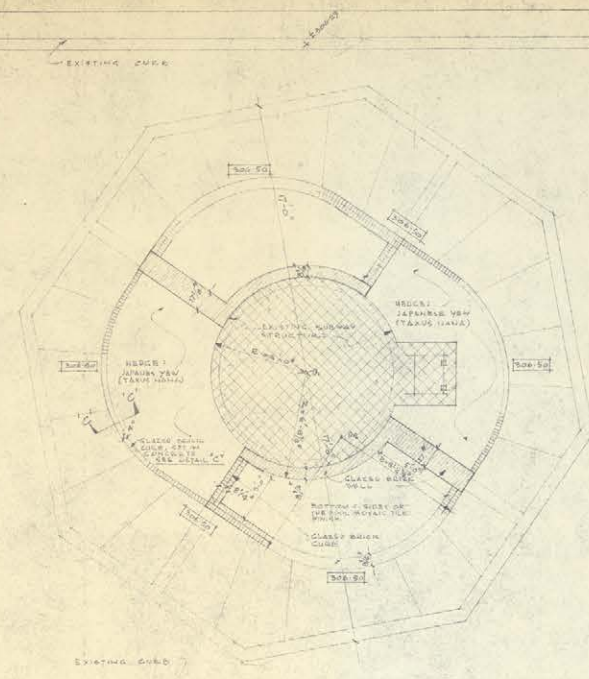


5 TYPICAL DETAIL SLATE POINTING 3/4"=1'

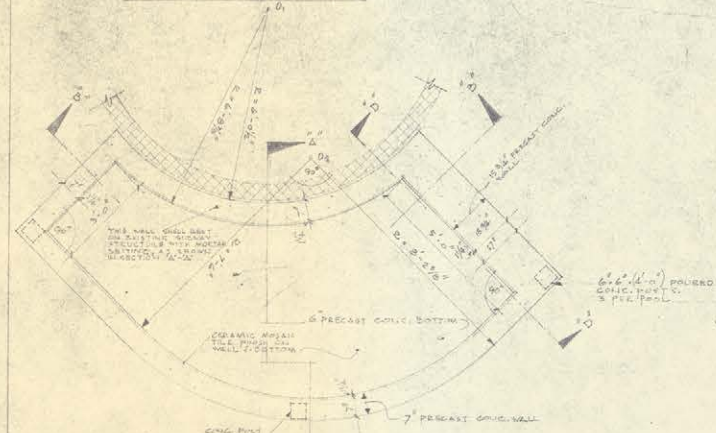
7 TYPICAL GROOVE MOSAIC WALL



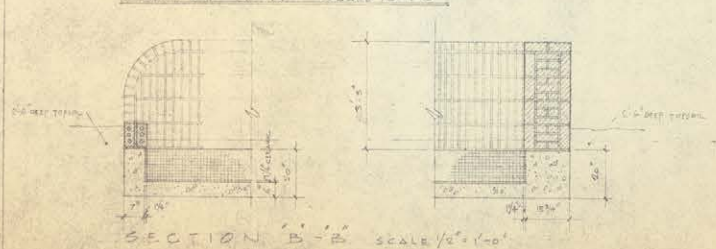
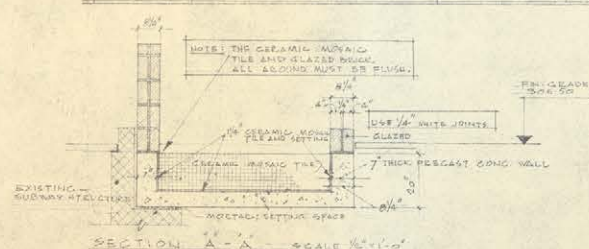
DETAILS FOR SCULPTURE COURT, ISLAND E



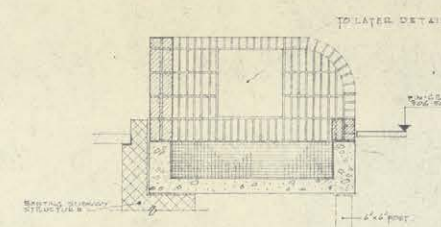
PLAN SCALE 1/4" = 1'-0"



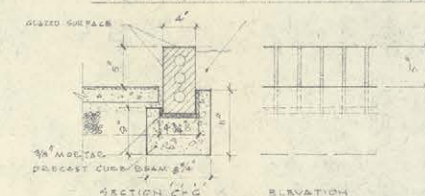
PLAN OF PRECAST CONCRETE POOL SCALE 1/2" = 1'-0"



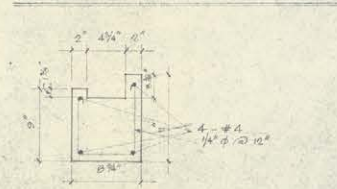
SECTION B-B SCALE 1/2" = 1'-0"



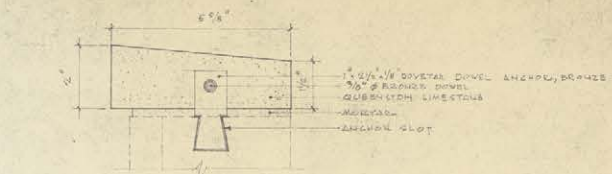
SECTION ELEVATION D-D SCALE 1/2" = 1'-0"



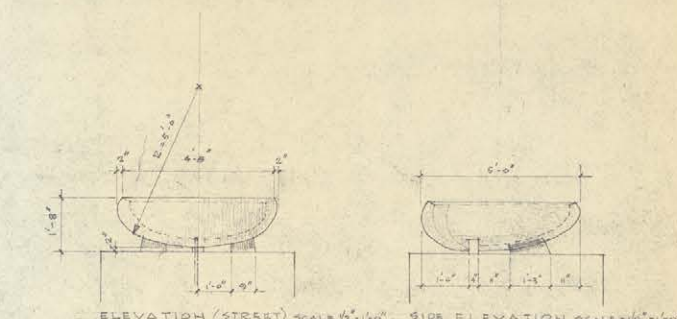
DETAIL C CURB SCALE 1/2" = 1'-0"



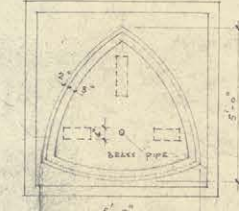
REINFORCING DETAIL FOR CURB SCALE 1/2" = 1'-0"



TYPICAL QUEENSTON COPING HALF BULL SIZE



ELEVATION (STREET) SCALE 1/2" = 1'-0" SIDE ELEVATION SCALE 1/2" = 1'-0"



PLAN SCALE 1/2" = 1'-0"
INDIANA LIMESTONE FLOWERPOT FOR ISLAND "H"
REQUIRED 4

DATE	REVISIONS
8-24-62	PLAN OF INDIANA LIMESTONE FLOWERPOT FOR ISLAND "H" ADDED



MUNICIPALITY OF METROPOLITAN TORONTO	SCALE 1/2" = 1'-0"
UNIVERSITY AVENUE	JAN 24, 1963
FOUNTAIN COURT DETAIL/ISLAND E	DEVELOP 15
CUT STONE DETAILS	DEVELOP 15
Quarrying, Green & Thompson	DEVELOP 15
Landscaping Architects	DEVELOP 15

APPENDIX D: ARCHIVAL MAPS, PLANS AND AERIAL IMAGES OF THE STUDY AREA

SOURCES AS NOTED

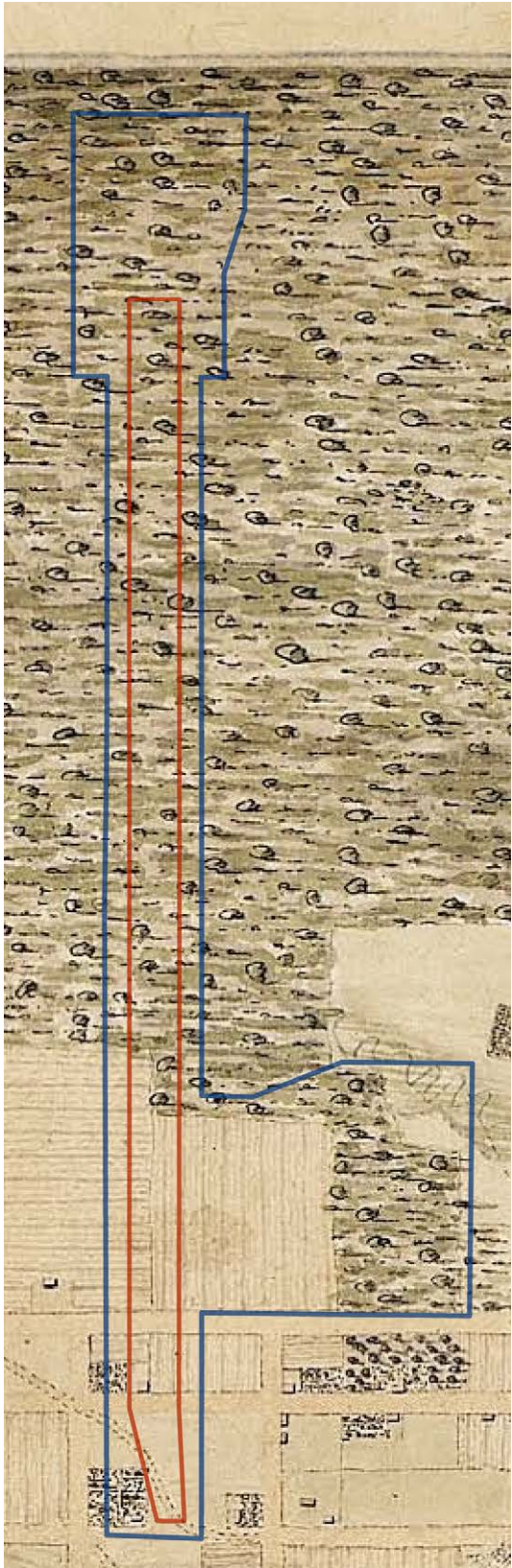
Abbreviations:

COTA: City of Toronto Archives

LAC: Library & Archives Canada

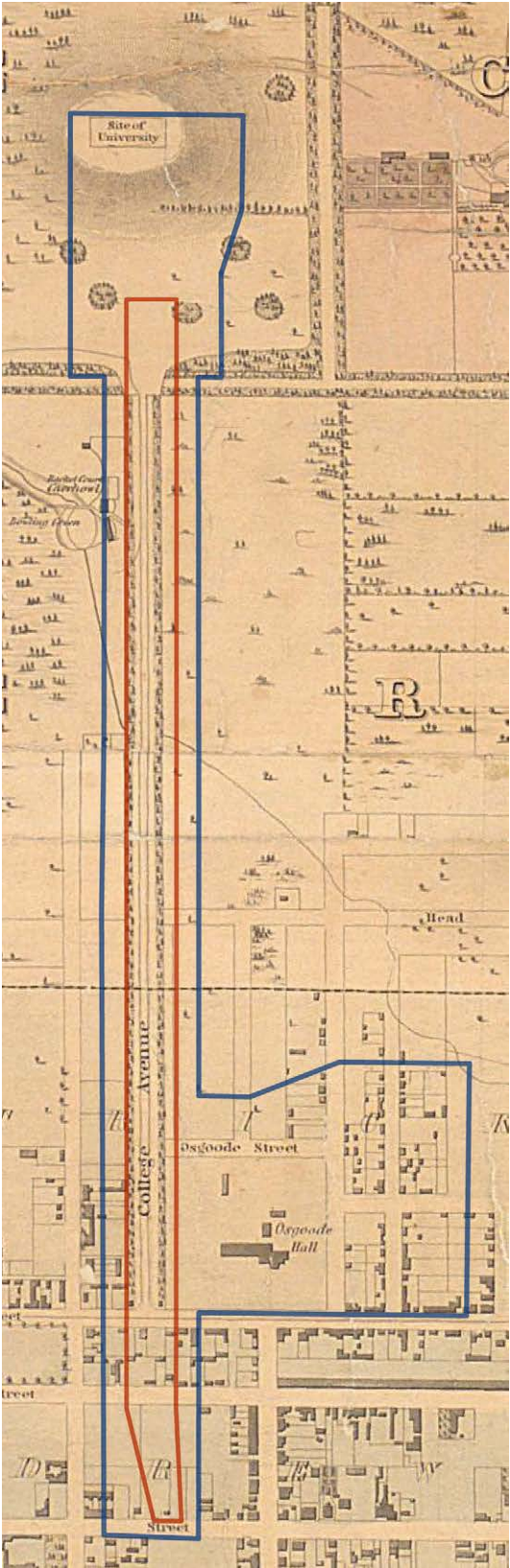
TPL: Toronto Public Library

1818



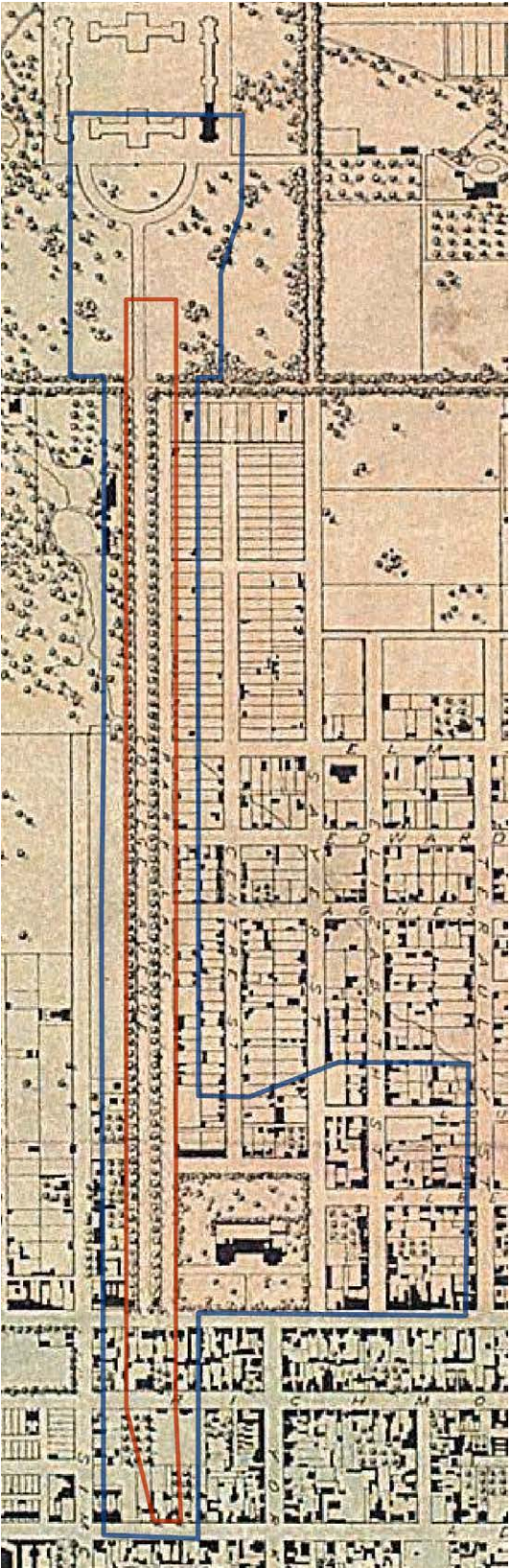
(LAC, NMC 17026)

1842



(TPL, T1842/4Mlrg c1)

1851



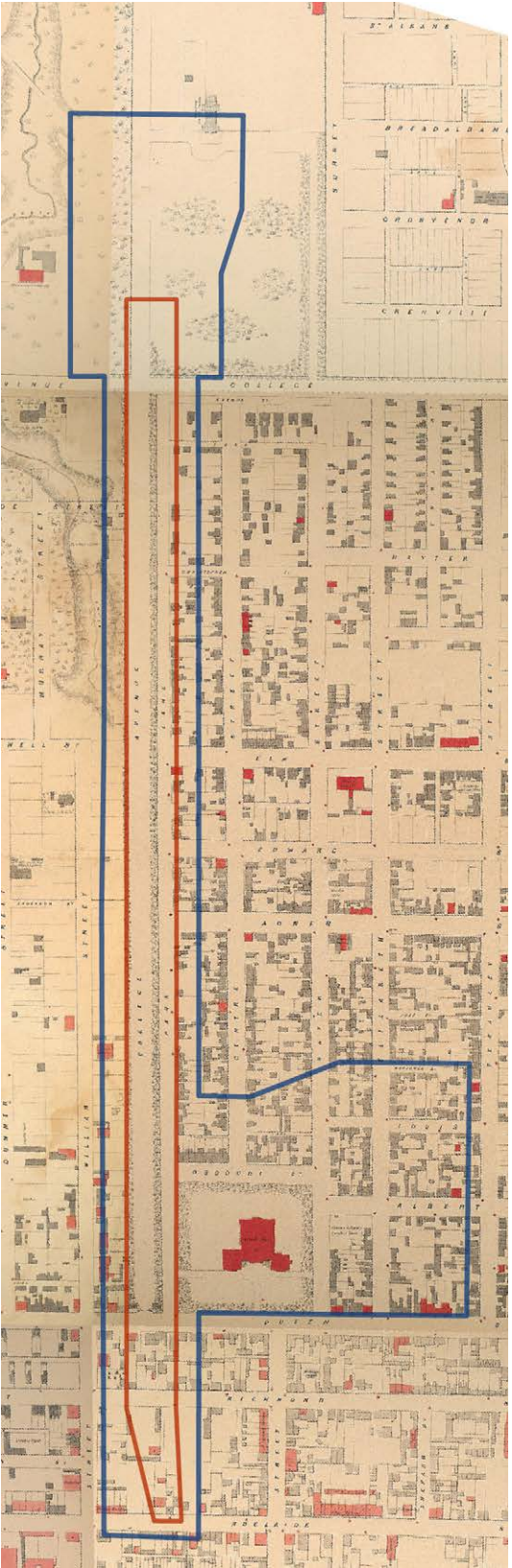
(TPL, T1851/4Mlrg)

1857



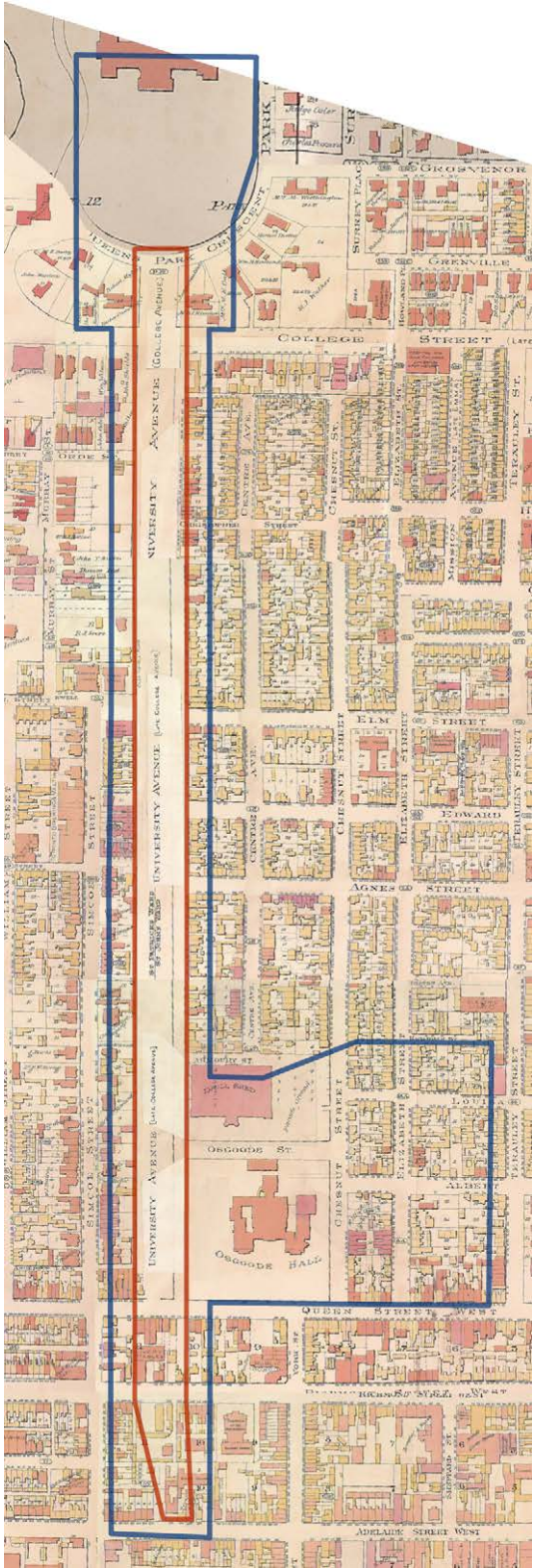
(COTA, Fonds 2, Series 88, Item 13)

1858



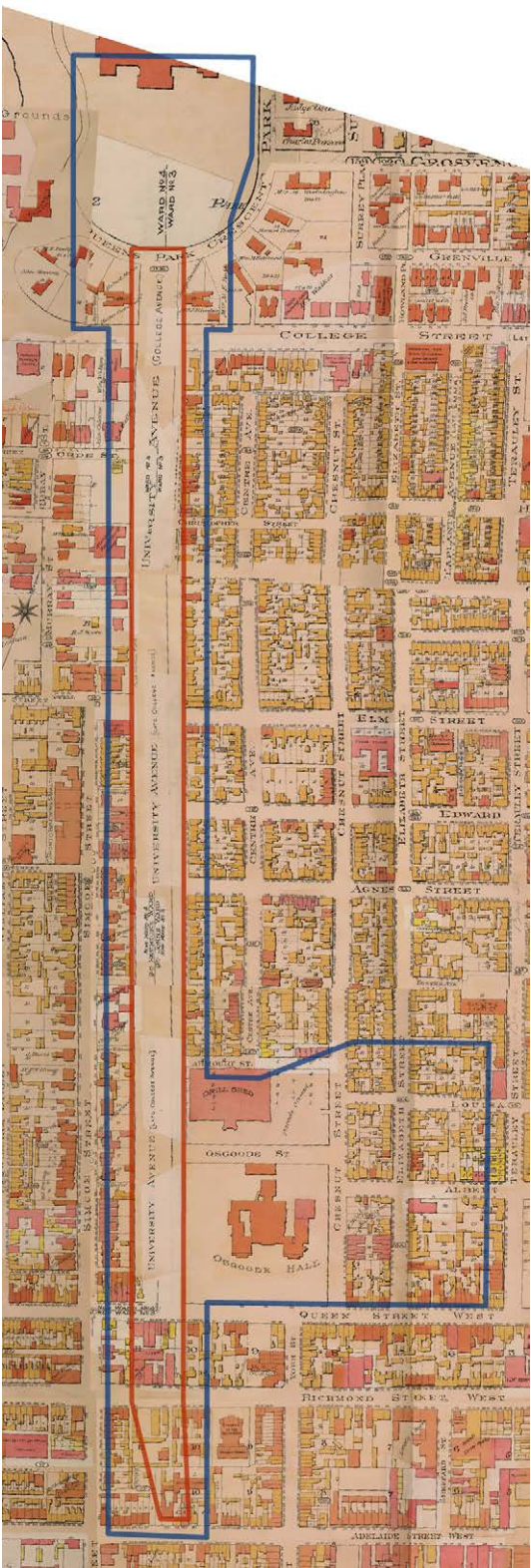
(<https://map.toronto.ca/torontomaps/>)

1889



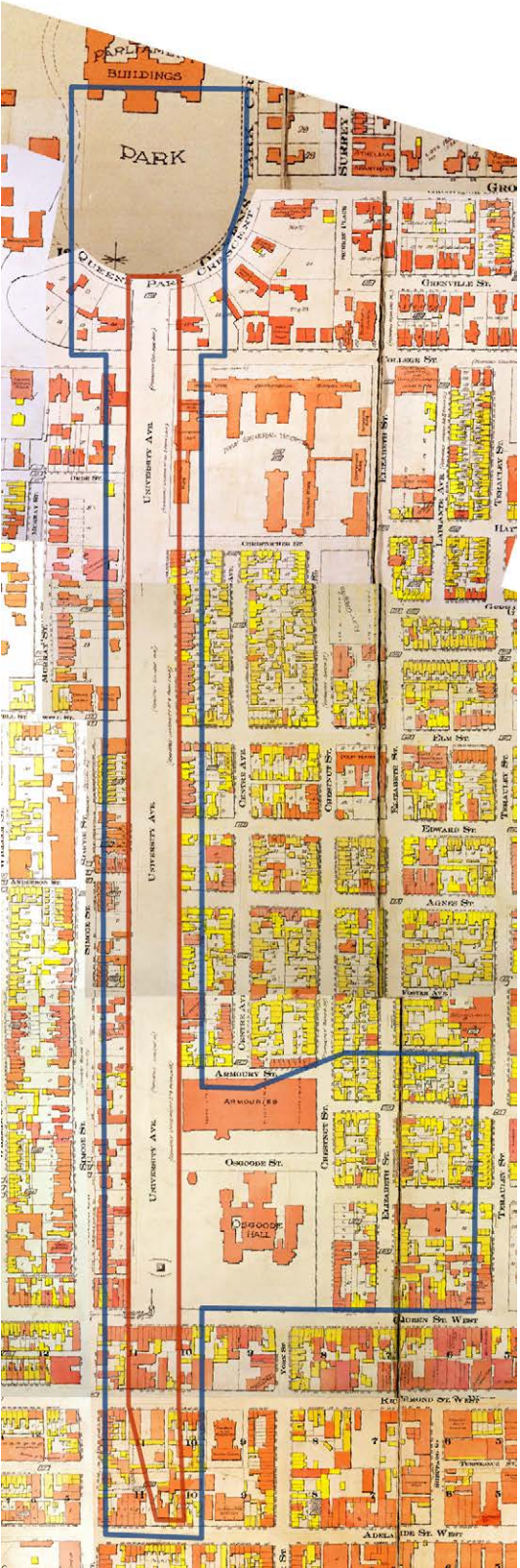
(<https://map.toronto.ca/torontomaps/>)

1903



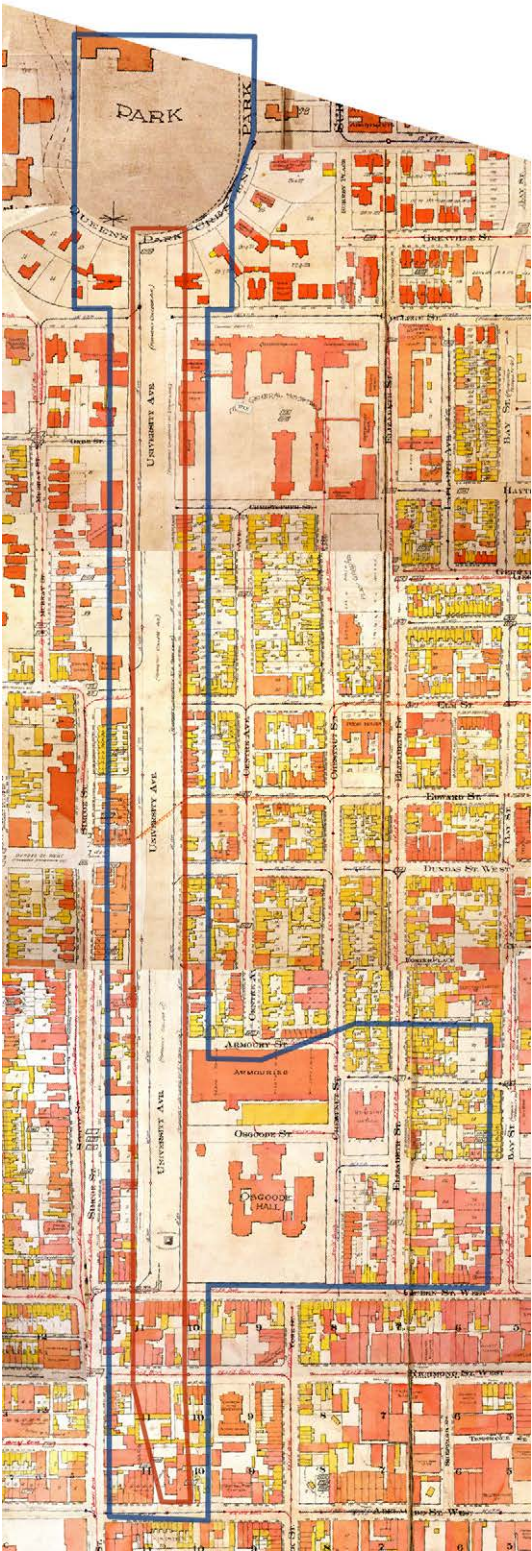
(<https://map.toronto.ca/torontomaps/>)

1913



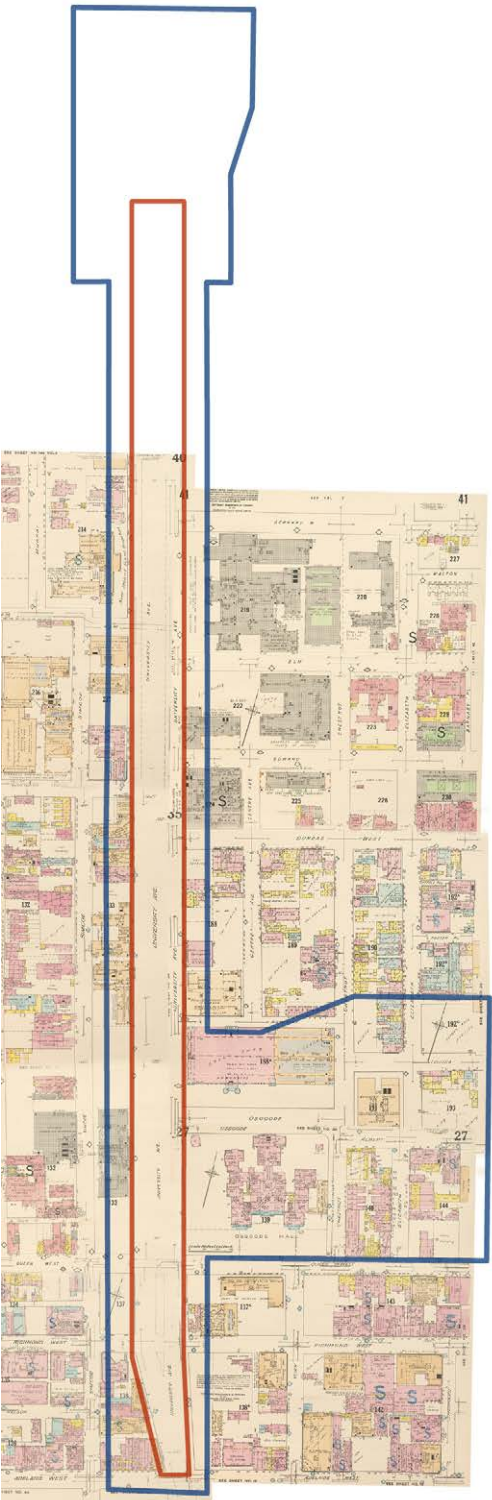
(<https://map.toronto.ca/torontomaps/>)

1924



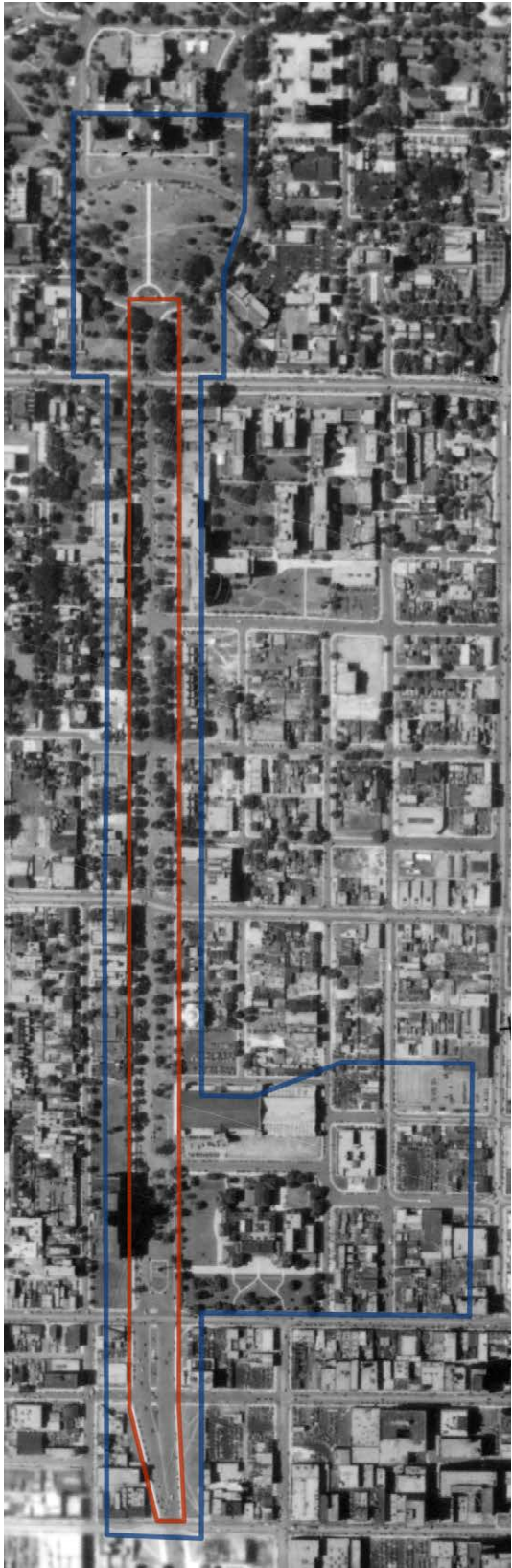
(<https://map.toronto.ca/torontomaps/>)

1954



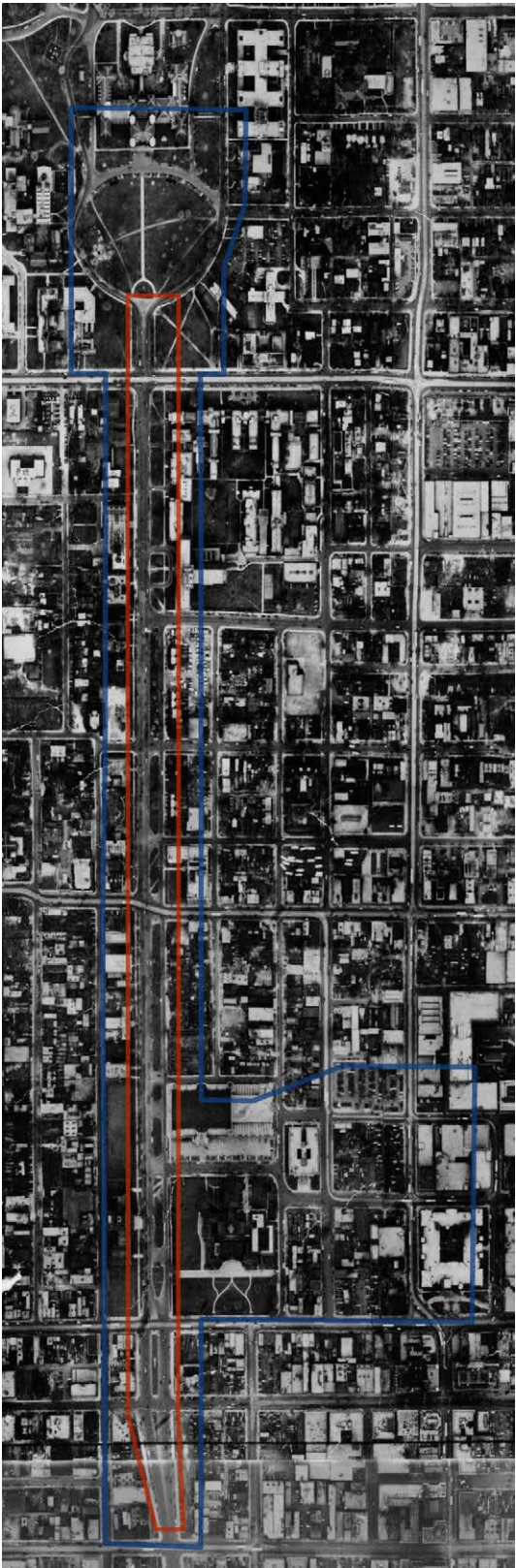
University of Toronto Fisher Rare Book Library

1939



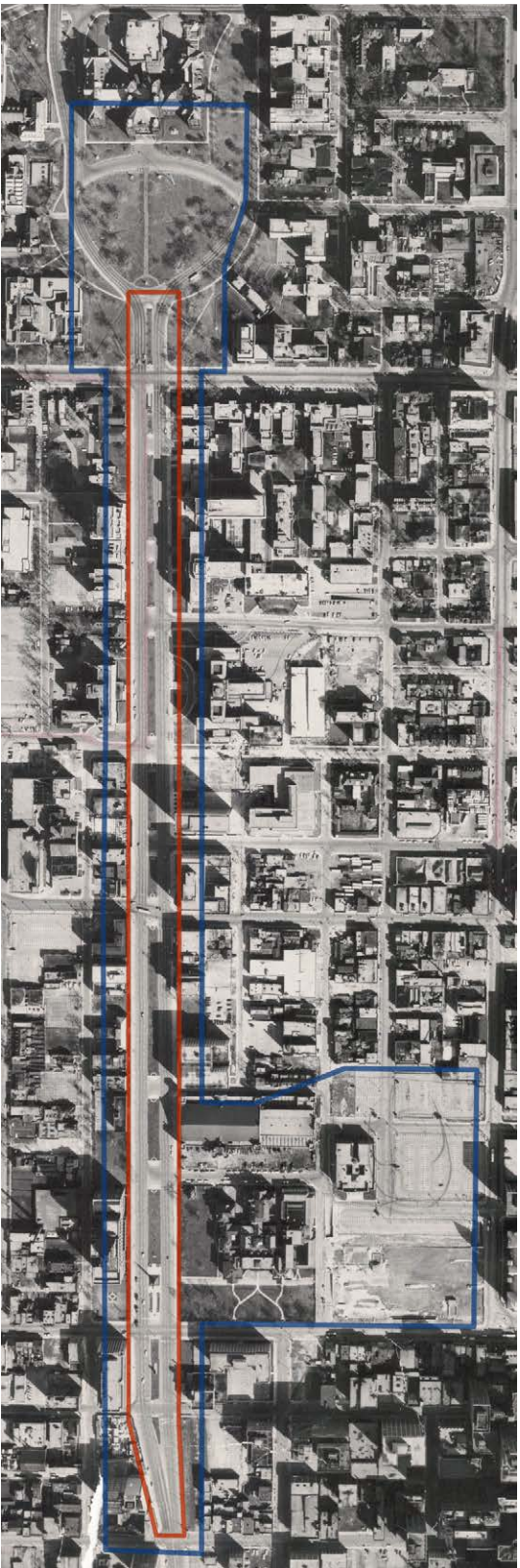
(<https://map.toronto.ca/torontomaps/>)

1947



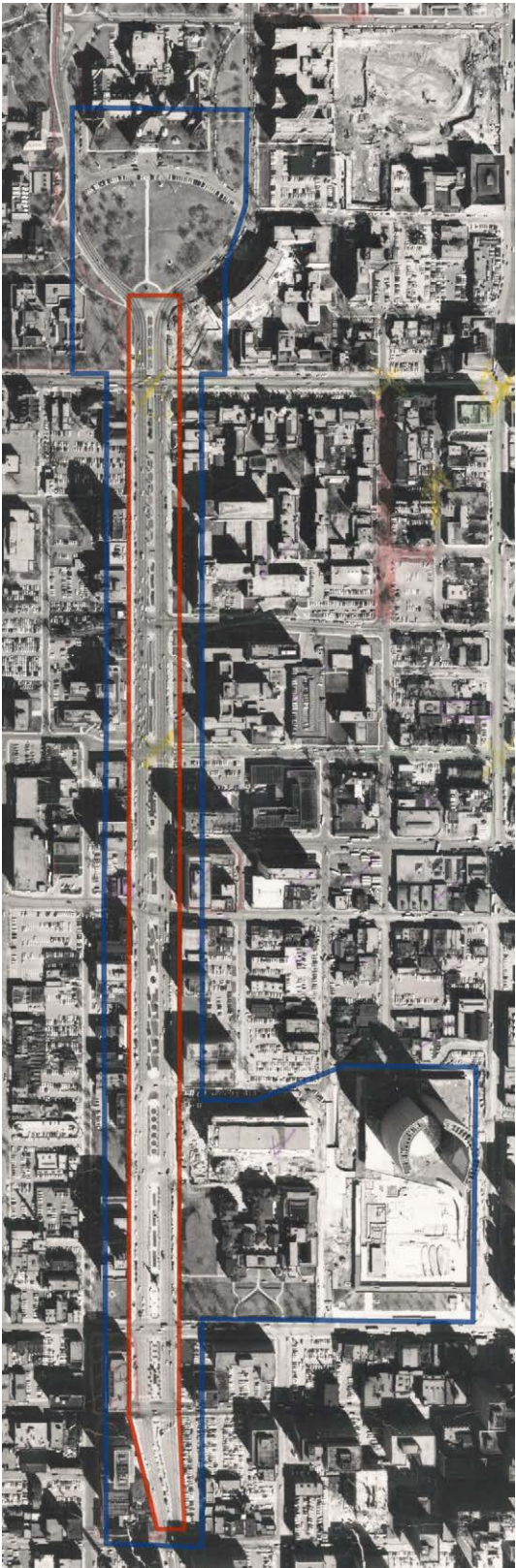
COTA, Series 2, File 1947, Item 18g, 22c

1959



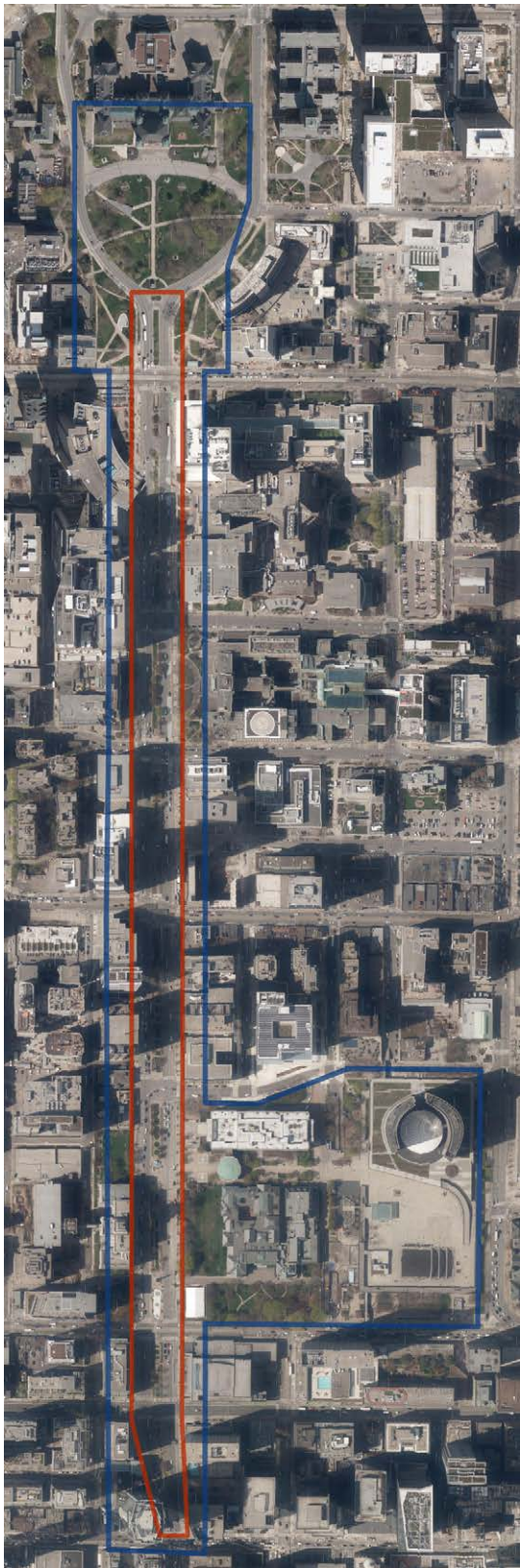
COTA, Series 2, File 1959, Item 25

1965



COTA, Series 2, File 1965, Item 63

2022

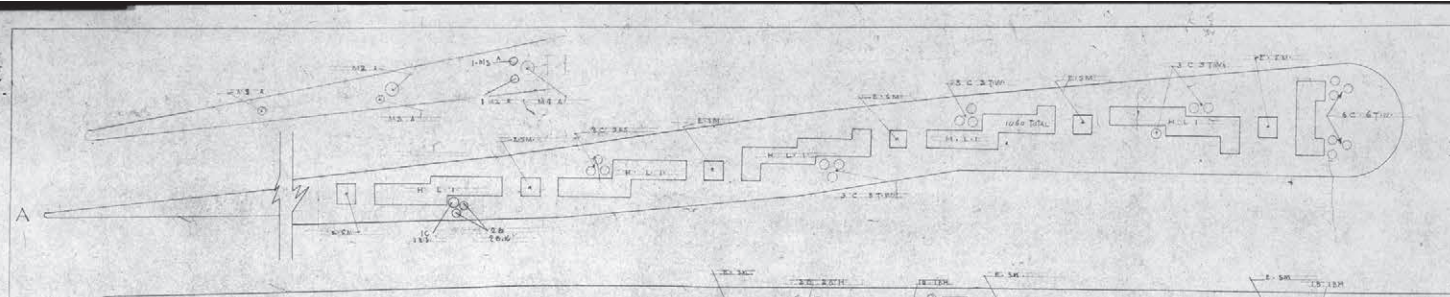


City of Toronto

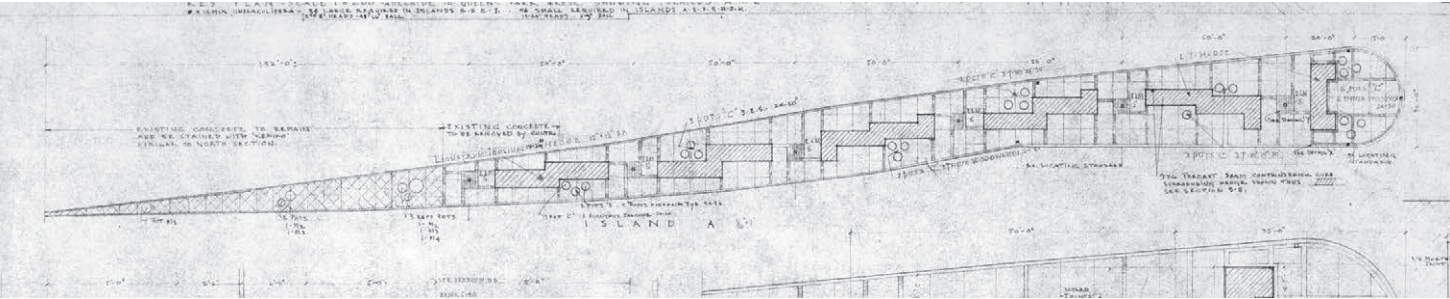
APPENDIX E: ISLAND DESCRIPTION SHEETS

Island A

1962 Planting Plan



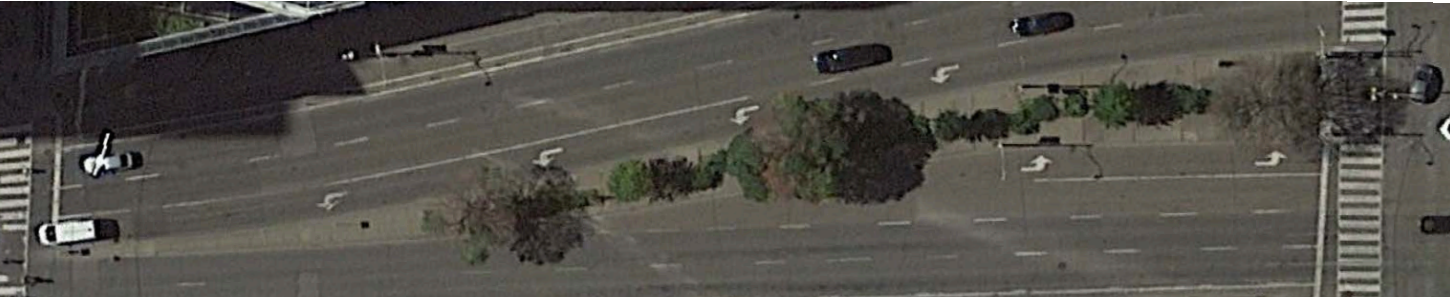
1962 Drawing



1967 Aerial



2022 Aerial



DIMENSIONS

Island A has an irregular plan, tapering from a 30 foot [9.14m] wide semicircle at its north end, roughly 410 feet [124.97m] to its southern tip.

DESCRIPTION

Island A resolves its irregular shape using a rectilinear layout defined by a square grid, with five irregular planting beds meandering through the interior of the island. The surface treatment is primarily poured-in-place concrete with pre-cast unit paving as accents and planting bed edging. Several of the planting beds contain contemporary trellis structures with vines. There are trees planted at the end of several planters. There is one contemporary bench at the north end of the island, next to the road crossing.

FUNCTION

Island A functions primarily as a visual element. It is inaccessible from the south and not clearly approachable from the north. The island's narrow, tapered geometry places hard surfaces along the edges—directly adjacent to vehicle lanes—while planting is concentrated in the centre. The layout aligns more with traffic infrastructure than with pedestrian-oriented space.

CONDITIONS & ALTERATIONS

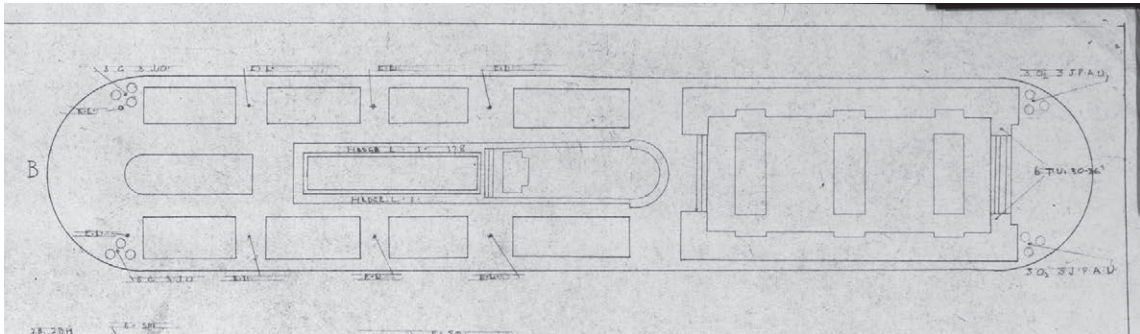
- The following conditions were noted based on site and document review:
- » Planting is overgrown and contains invasive species.
 - » Weeds are growing from the cracks between paving.
 - » Trees are planted in limited soil volume and show signs of distress. Many trees have been replaced as older trees have died due to the poor growing conditions. Trees in this condition typically have a short lifespan and need frequent replacement.
 - » Some of the original paving (including at north end) has been replaced by poured-in-place concrete with no attempt to match surface finishes.
 - » Panel tiling around elms is no longer present.
 - » Detectable warning surfaces have been installed at the pedestrian crossing at the north end of the island.
 - » Most panels of feature paving from the original plan have been removed over time.



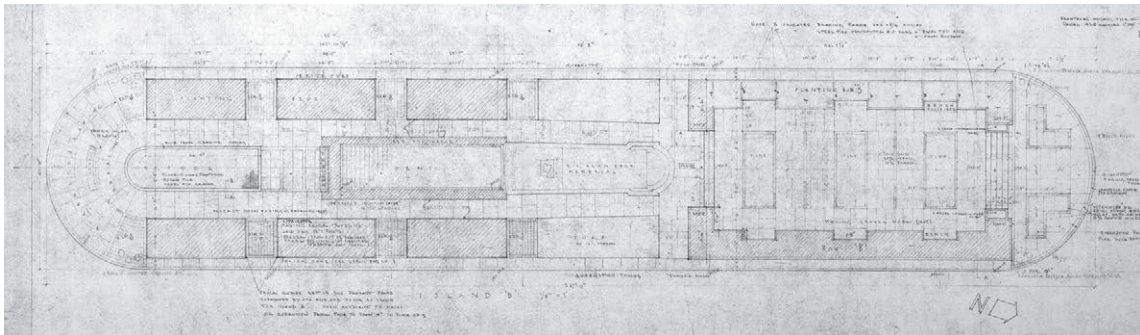
(CBCollective 2024)

Island B

1962 Planting Plan



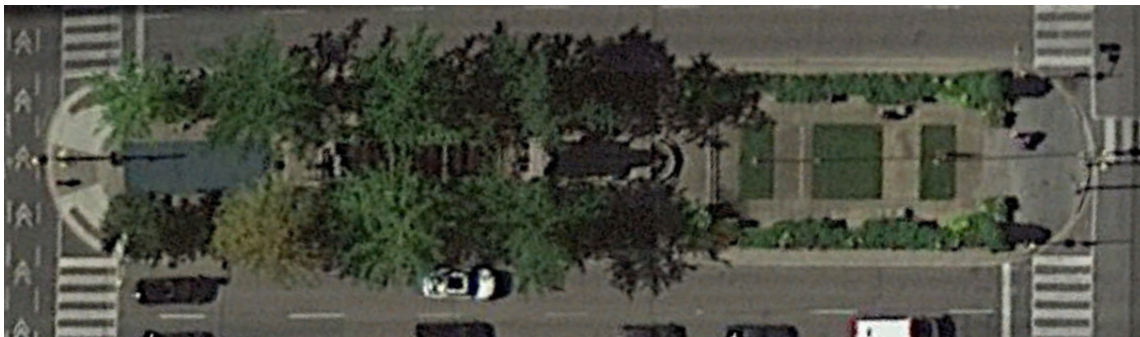
1962 Drawing



1967 Aerial



2022 Aerial



DIMENSIONS

Island B runs 237 feet [72.24m] long, with a consistent width of 45 feet [13.72m] between its two rounded ends.

DESCRIPTION

The island is arranged about the centrally located Sir Adam Beck statue. A sunken court lies directly north, and to the south is a vent and covered pool. Two irregular raised planting boxes (900m height) frame the sunken court - one on the west side and one on the east side. Six rectangular curbed planting beds frame the space south of the statue - three on the west side and three on the right side. The surface treatment on the interior is primarily exposed aggregate paving slabs in a warm colour, poured-in-place concrete, and lawn panels.

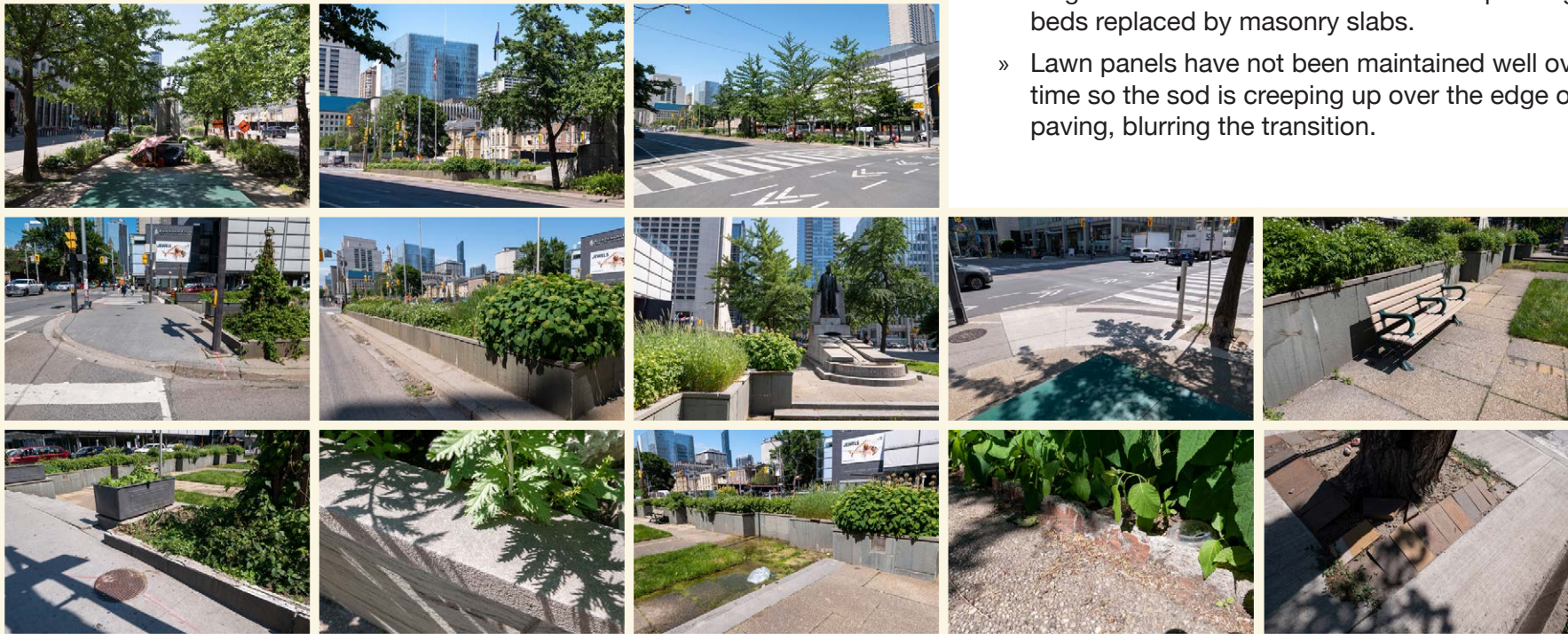
FUNCTION

Island B frames the Sir Adam Beck statue and includes two defined areas at its ends. The southern end contains a paved surface enclosed by planting and the statue. The northern end features a slightly sunken paved court, partially enclosed by raised planters. However, both areas are isolated by traffic and lack accessible routes from the adjacent sidewalks.

CONDITIONS & ALTERATIONS

The following conditions were noted based on site and document review:

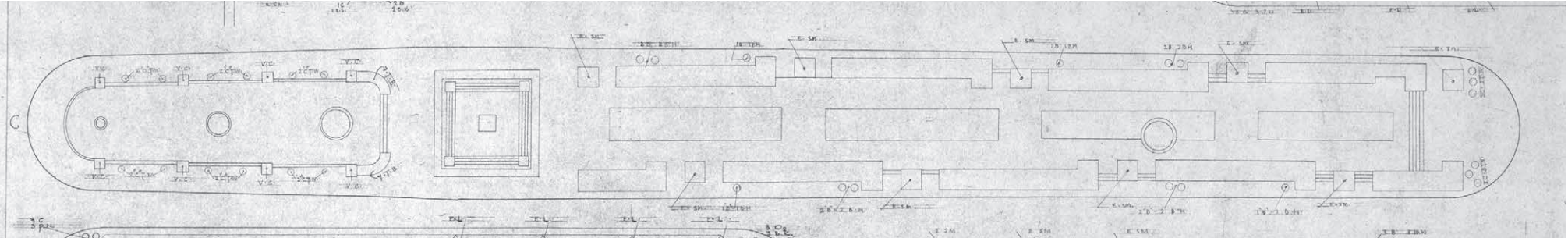
- » Planting is overgrown and contains invasive species.
- » Weeds are growing from the cracks between paving.
- » The pool is non-functional.
- » The water feature integrated into the statue is non-functional.
- » Original paving at each end of the island has been replaced with detectable warning surfaces and new poured-in-place concrete.
- » Original natural stone raised planter walls are generally in good condition considering their age.
- » Some original natural stone coping on raised planters is damaged and heaving.
- » Patching to original natural stone raised planters is irregular and unsightly.
- » Many panels of original poured-in-place exposed aggregate concrete paving are damaged with cracks or sections missing.
- » Panel tiling around elms is no longer present.
- » The middle lawn panel in the sunken courtyard has been enlarged and paving reduced.
- » Original brick curbs around southern 6 planting beds replaced by masonry slabs.
- » Lawn panels have not been maintained well over time so the sod is creeping up over the edge of paving, blurring the transition.



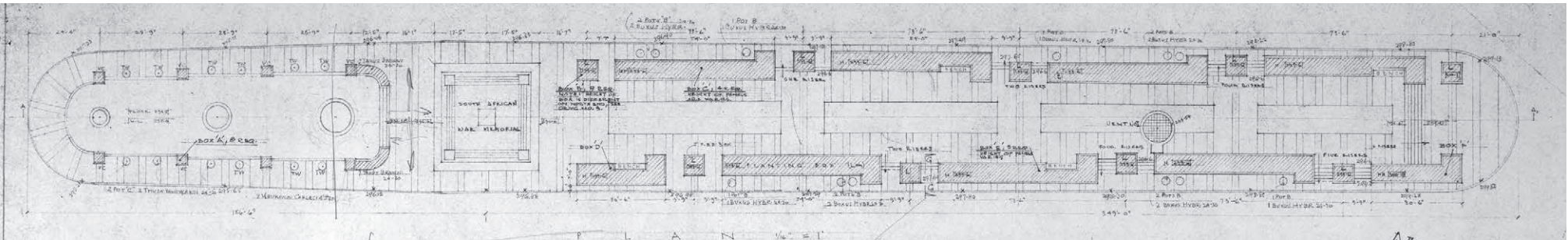
(CBCollective 2024)

Island C

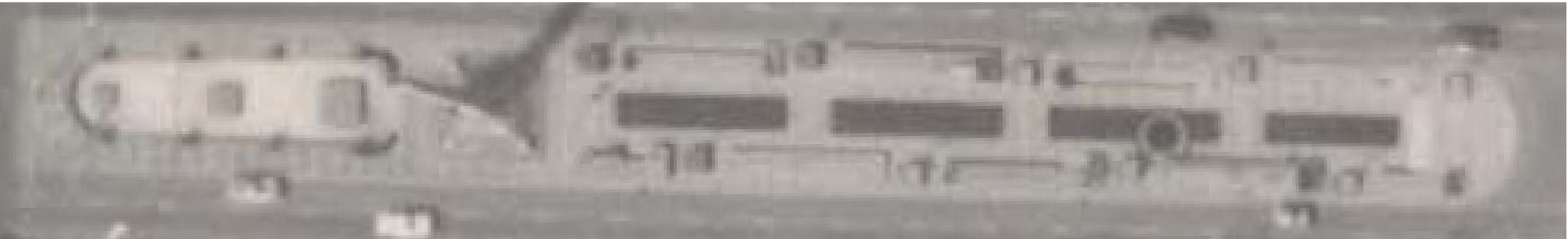
1962 Planting Plan



1962 Drawing



1967 Aerial



2022 Aerial



DIMENSIONS

505.5 feet [154.08m] x ~50 feet [15.24m].

DESCRIPTION

All architectural and landscape elements on Island C, except for one tree, have been removed to facilitate the construction of the Ontario Line.

FUNCTION

Island C has been fully deconstructed and is currently non-functional. It is fenced off and inaccessible to the public.

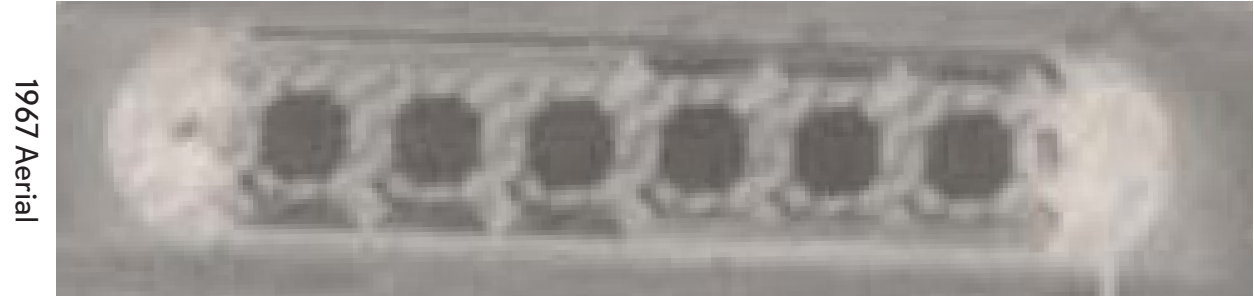
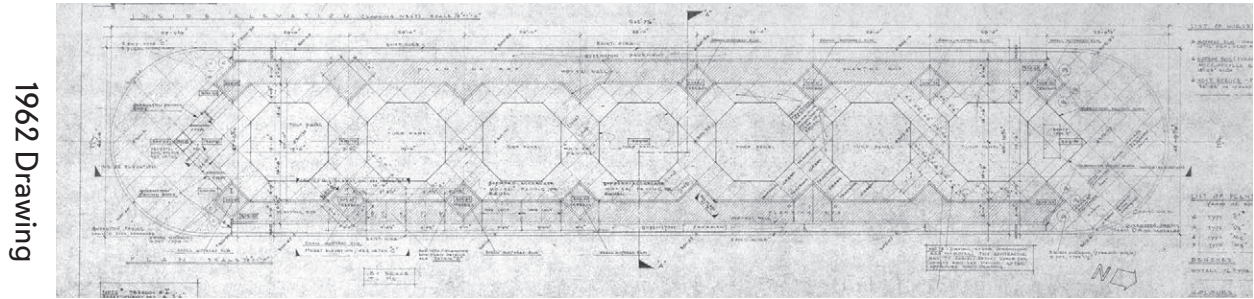
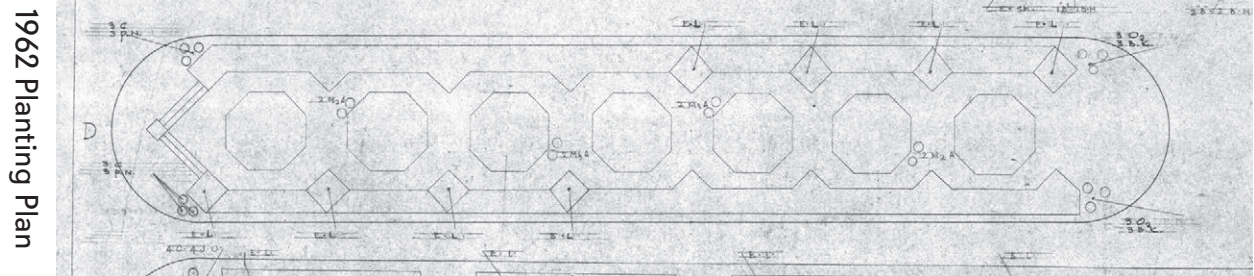
CONDITIONS & ALTERATIONS

n/a - Island C is currently an inaccessible construction staging area



(CBCollective 2024)

Island D



DIMENSIONS

~233 feet [71.02m] x 46.5 feet [14.17m].

DESCRIPTION

Island D is a regular composition of raised planters (450mm and 650mm heights) on the periphery of the island sheltering an interior space composed of octagonal sod panels framed by exposed aggregate unit paving in alternating colours (white and grey). The raised planters are made from natural stone slabs on the exterior (road) side and pre-cast concrete panels with exposed aggregate on the inside. Trees occupy some of the triangular planter widenings and a mix of annuals, perennials, and shrubs fill the planting beds. A set of stairs is located at the south end of the island. Inside the planters, there are several contemporary benches.

FUNCTION

Island D creates a relatively pleasant environment for passive use, offering slight separation from traffic. However, stairs at both ends restrict entry, limiting accessibility and use. The island lacks notable elements such as statues or water features that might otherwise attract attention or engage passersby, diminishing its appeal beyond sitting for brief periods of time.

CONDITIONS & ALTERATIONS

The following conditions were noted based on site and document review:

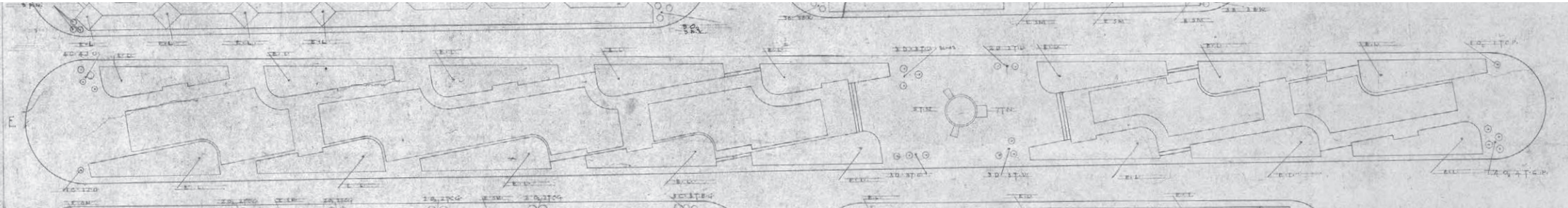
- » Overall plan reduced by one octagon to permit widening of Armoury Street.
- » Planters no longer present.
- » Some of north end concrete paving was replaced (2002), but matches original design.
- » Planting is overgrown and contains invasive species.
- » Weeds are growing from the cracks between paving.
- » The three southernmost trees on the west side of the island are missing.
- » Original natural stone raised planter walls are generally in good condition considering their age. Some slabs have been replaced.
- » Some original natural stone coping on raised planters is damaged and heaving.
- » Patching to original natural stone raised planters is irregular and unsightly.



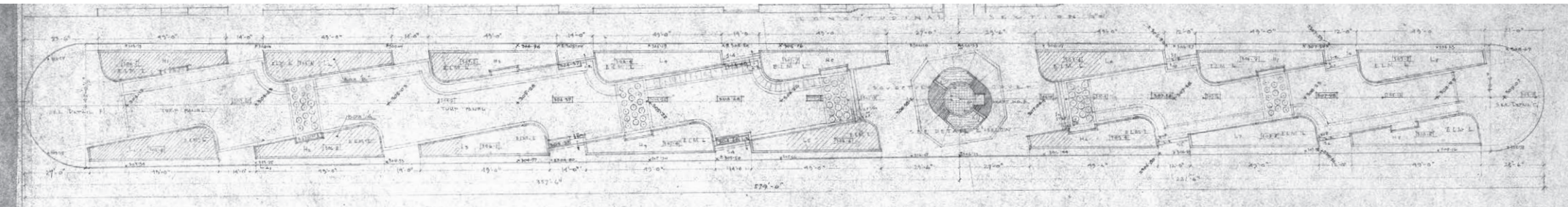
(CBCollective 2024)

Island E

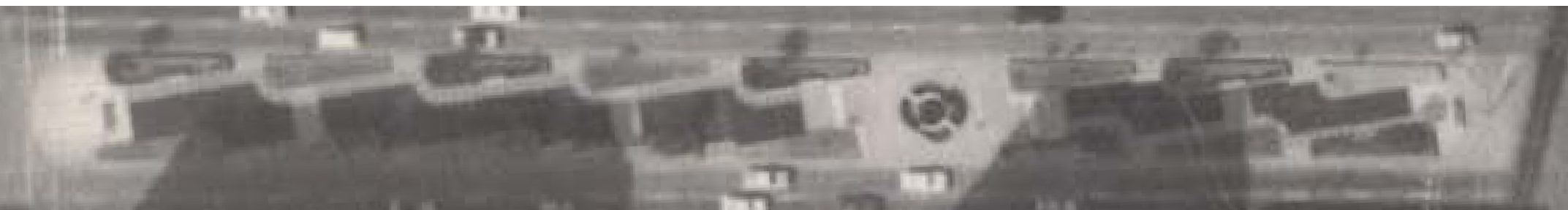
1962 Planting Plan



1962 Drawing



1967 Aerial



2022 Aerial



DIMENSIONS

579 feet [176.48m] x 45 feet [13.78m] (south end) to 40.5 feet [12.34m] (north end).

DESCRIPTION

Island E is composed of twelve irregular raised planters located on the periphery - six on the west side and six on the east side. The raised planters do not form a continuous barrier to the street – instead there are a series of openings between each individual planter. The raised planters alternate between 600mm and 900mm in height. The raised planters are made from natural stone slabs on the exterior (road) side and pre-cast concrete panels with exposed aggregate on the inside. On the interior, the surface treatment is composed of sod with natural flagstone and exposed aggregate unit paving in alternating colours (white and red). A TTC ventilation duct is located in the northern part of the island with a set of stairs to its north and south. The Canadian Airmen’s Memorial (c. 1984) is located at the north end of the island facing north with a double row of trees providing its southern setting. The overall effect of the composition is the establishment of a rhythm of solid and void; contrasting colour; and varied textures.

FUNCTION

Island E allows entry along most of its length, unlike many other islands that are accessible only at their ends. Original stone benches remain in place, but there is no direct crossing from the adjacent sidewalks. A non-functional water feature and a commemorative monument are present but do not appear to draw regular use.

CONDITIONS & ALTERATIONS

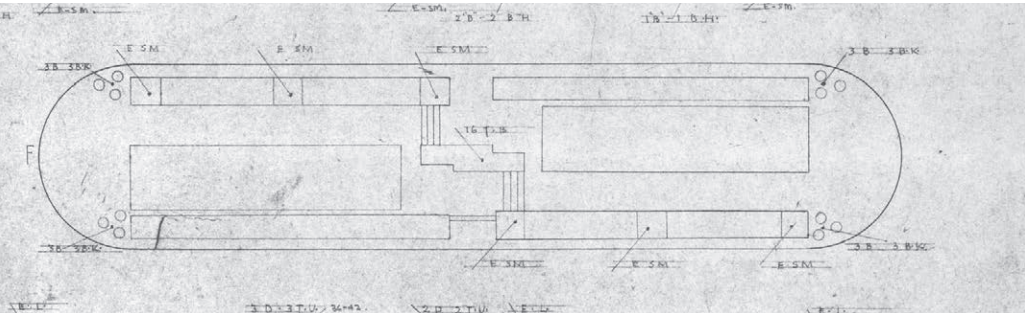
The following conditions were noted based on site and document review:

- » Northern four planters replaced by Canadian Airmen’s Memorial in 1984 with redesigned landscape not sympathetic to original design.
- » Planting is overgrown and contains invasive species.
- » Weeds are growing from the cracks between paving.
- » Trees appear to be in good condition. Some trees have been replaced over time as there is a random mix of species and tree ages.
- » Original natural stone raised planter walls are generally in good condition considering their age. Some slabs have been replaced.

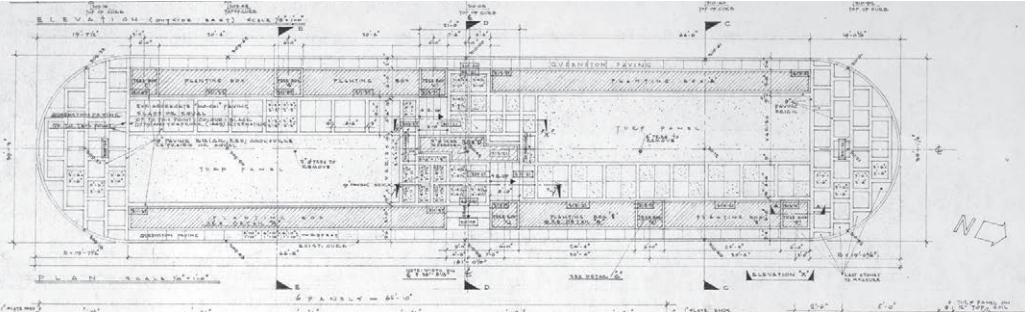
- » Some original natural stone coping on raised planters is damaged and heaving.
- » Patching to original natural stone raised planters is irregular and unsightly.
- » Sod panels have not been maintained well over time so the sod is creeping up over the edge of paving, blurring the transition.
- » Original paving at each end of the island has been replaced with detectable warning strips and new poured-in-place concrete.
- » Water Feature is non-functional and may have been modified from original design with travertine copings.
- » There is differential settlement between paving types, creating trip hazards across the island.

Island F

1962 Planting Plan



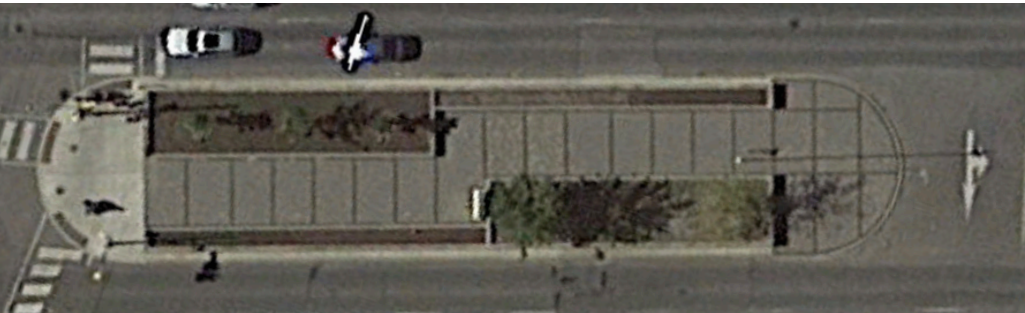
1962 Drawing



1967 Aerial



2022 Aerial



DIMENSIONS

181 feet [55.17m] x 39 feet [11.96m] (south end) to 38 feet[11.61m] (north end).

DESCRIPTION

Island F is composed of two rectangular raised planters (600mm and 900mm heights) located on the periphery of the island - one on the west and one on the east side of the island. Island F has been largely reconstructed and reconfigured, with few original features remaining. The reconstruction occurred in between c.2018. The exterior (road) \ raised planter walls are the original slate walls, but interior planter walls have been replaced with unfinished cast-in-place concrete walls of poor-quality workmanship. The new planters are in a different configuration from the original. Two larger raised planters have been created to increase soil volume for the three trees that have been planted in each planter. On the interior, all original paving and lawn panels have been replaced with a generic, modern pre-cast concrete paver in grey with black banding.

FUNCTION

Island F was redesigned with a simplified layout consisting of tree planters, standard paving, and minimal detail. The island does not include significant design elements or user-oriented features. Its appearance suggests cost-driven implementation rather than integration with the broader boulevard concept.

CONDITIONS & ALTERATIONS

The following conditions were noted based on site and document review:

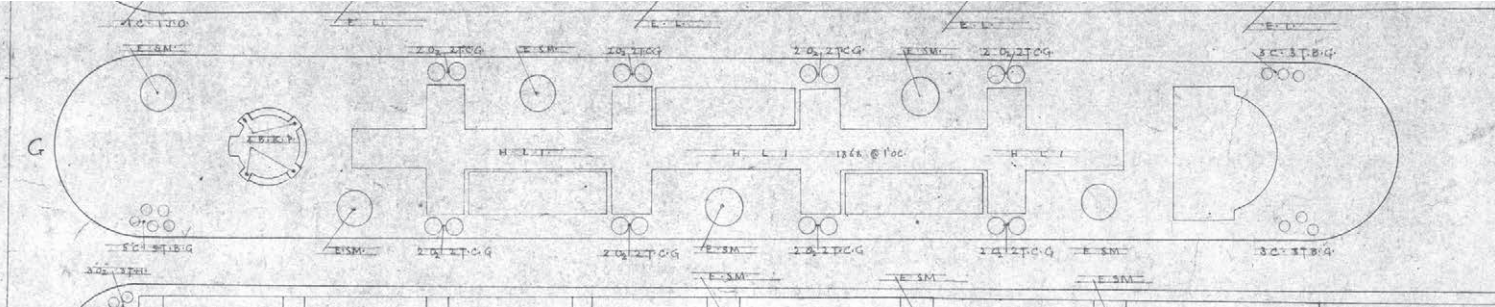
- » The original raised planter and paving design has been substantially reconfigured with the 2018 reconstruction.
- » All original paving and lawn panels have been removed.
- » The exterior (road) raised planter walls are the original slate walls, but interior planter walls have been replaced with unfinished cast-in-place concrete walls of poor-quality workmanship with uneven finishing.
- » Newly planted trees are healthy.
- » Planting beds below trees are full of weeds upon the time of inspection, so look unkempt.
- » New paving is in good condition – level and weed free.



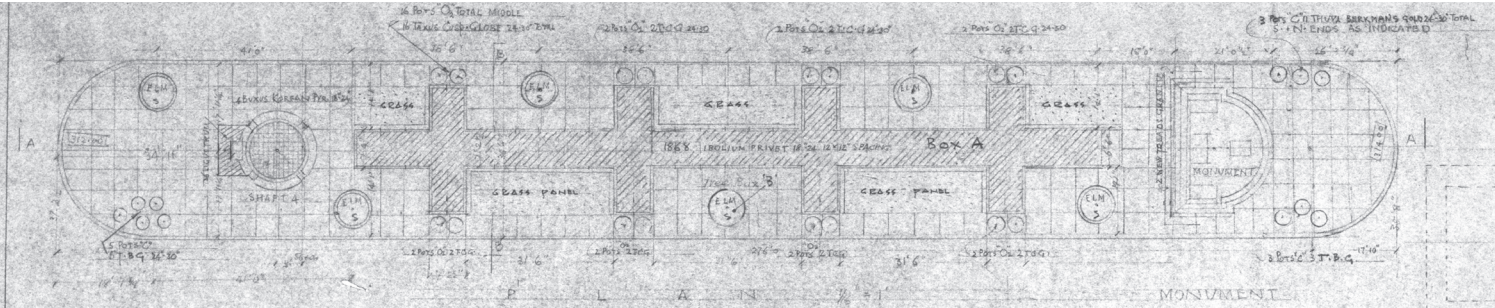
(CBCollective 2024)

Island G

1962 Planting Plan



1962 Drawing



1967 Aerial



2022 Aerial



DIMENSIONS

276 feet [84.12m] x ~36.5 feet [11.13m]

DESCRIPTION

Island G is centered around a 600mm height multi-cruciform shaped raised planter. This unique shape creates three bays on both sides of the island. Each bay houses a circular planter, each of which contains a tree. Raised planters are constructed from original pre-cast concrete panels with exposed aggregate finish in a reddish colour. At the south end of the island, a TTC vent is hidden inside a circular stone seating feature, also made from the same reddish precast concrete panels. The seating feature has inset benches and sandstone plinths. Original polished granite flowerpots adorn each plinth. The Sons of England War Monument (1921) occupies the north end of the island. It was originally located south of College Street and moved to its current location in 1949 when the central boulevard was realigned. Encircling the central planter and additional features, the original bold checkerboard paving pattern is preserved. This pattern is formed using dark grey and taupe precast concrete pavers. The overall effect of the composition of planters and bold paving pattern is one of a strong rhythm.

FUNCTION

Island G is characterized by a large central planter that pushes circulation space to the island's edges, placing pedestrians close to adjacent traffic lanes. While seating niches exist, they are unbuffered and exposed. The design emphasizes visual rhythm and material contrast over usability.

CONDITIONS & ALTERATIONS

The following conditions were noted based on site and document review:

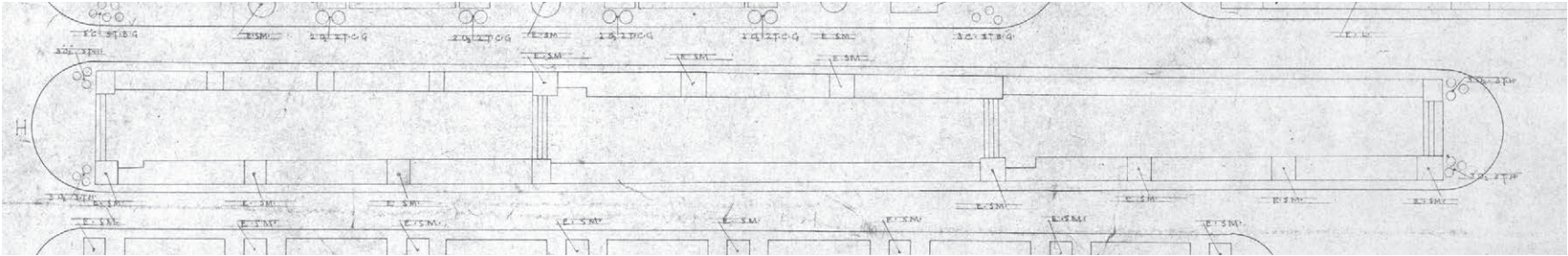
- » Island G is remarkably intact – most original features remain.
- » Planting is overgrown and contains invasive species.
- » Weeds are growing from the cracks between paving.
- » Trees appear to be in good condition. Some trees have been replaced over time as there is a random mix of species and tree ages.
- » Original pre-cast concrete raised planter walls are generally in good condition considering their age.
- » Original paving is in poor to good condition. Colour have faded with age and there is some damage due to age and use over time.
- » Original paving at north end of the island has been replaced with detectable warning surfaces and new poured-in-place concrete.



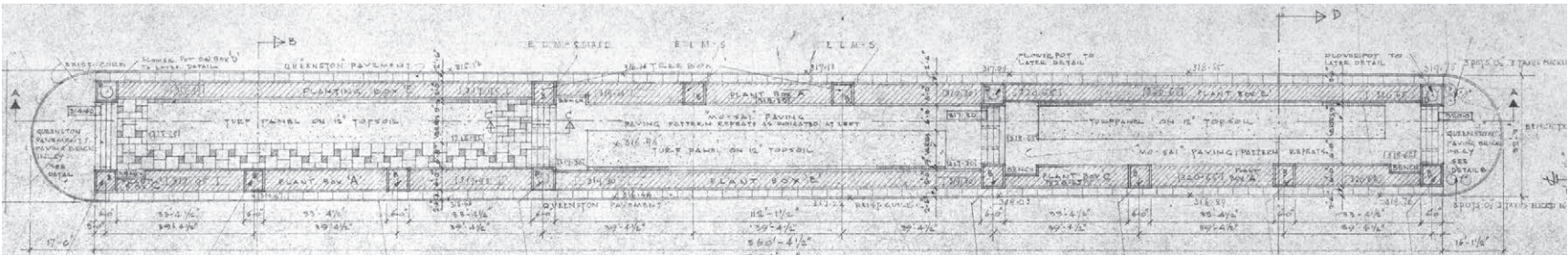
(CBCollective 2024)

Island H

1962 Planting Plan



1962 Drawing



1967 Aerial



2022 Aerial



DIMENSIONS

~400 feet [121.92m] x 35 feet [10.68m]

DESCRIPTION

Island H is composed of two rectangular raised planters (600mm height) on the west and east sides of the island. There is a rhythm of regularly spaced taller planters (900mm height) inline with the long planters. Taller planters contain trees except for two plinths, which have rounded triangular limestone planting urns. The raised planters are made from natural stone slabs on the exterior (road) side and pre-cast concrete panels with exposed aggregate on the inside.

On the interior, surface treatment is composed of rectangular sod panels and exposed aggregate unit pavers in alternating colours (red and gray). The island contains four sets of stairs - one set at the north end, one set at the south end, and two sets in the centre. Limestone ramps have been added beside the stairs at the south end as well as beside the two in the middle of the island. There are several contemporary city standard benches placed randomly inside the island. A contemporary lighting installation has been installed in the interior of the island c.2021.

FUNCTION

Although Island H was originally designed to accommodate pedestrians, it currently feels uncomfortable due to its poor state of repair, accumulation of litter, and exposure to street noise. These maintenance issues significantly detract from its intended function.

CONDITIONS & ALTERATIONS

The following conditions were noted based on site and document review:

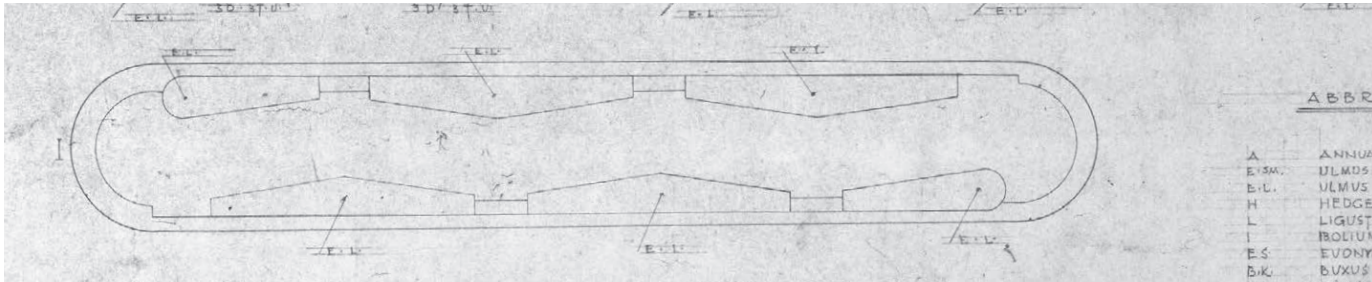
- » Planting is overgrown and contains invasive species.
- » Weeds are growing from the cracks between paving.
- » Many wall panels are cracking or crumbling.
- » Original paving at each end of the island has been replaced with detectable warning strips and new poured-in-place concrete.
- » Original natural stone raised planter walls and pre-cast concrete raised planter walls are generally in good condition considering their age.
- » Some original natural stone coping on raised planters is damaged and heaving.
- » Patching to original natural stone raised planters is irregular and unsightly.
- » Ramps and hand railings have been added to each set of steps.
- » Lawn panels have not been maintained well over time so the sod is creeping up over the edge of paving, blurring the transition.



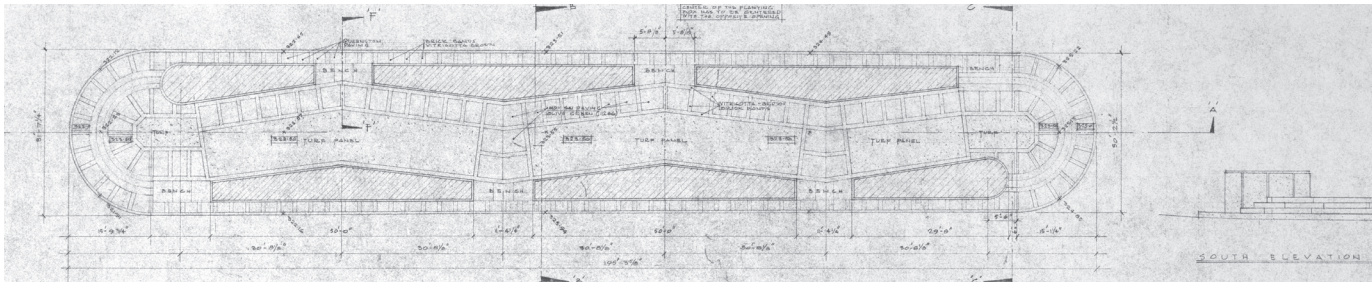
(CBCollective 2024)

Island I

1962 Planting Plan



1962 Drawing



1967 Aerial



2022 Aerial



DIMENSIONS

~203 feet [61.87m] x 31.5 feet [9.63m] (south end), 30 feet [9.21m] (north end)

DESCRIPTION

Island I is composed of six irregular raised planting boxes (varying in height between 450-900mm) located on the periphery of the island - three on the west side and three on the east side. The raised planters are made from natural stone slabs on the exterior (road) side and pre-cast concrete panels with exposed aggregate on the inside. In the gaps between each raised planter, some original stone benches remain. One original bench is missing, and it has been replaced by a contemporary city standard bench. On the interior, surface treatment is composed of irregularly sodded areas and original exposed aggregate unit paving and pre-cast concrete unit pavers as accents. The island contains two sets of stairs at either end. Semi-circular stairs are located at the north and south ends of the island. Two sets of stairs are located on the west side of the island and two sets on the east side. These are located between the raised planting beds.

FUNCTION

Island I provides a relatively comfortable place to sit, benefiting from its raised elevation above street level and several benches. Mature overstory trees and lawn panels create a pleasant microclimate. However, stairs at both ends render the island inaccessible, limiting its potential user base.

CONDITIONS & ALTERATIONS

The following conditions were noted based on site and document review:

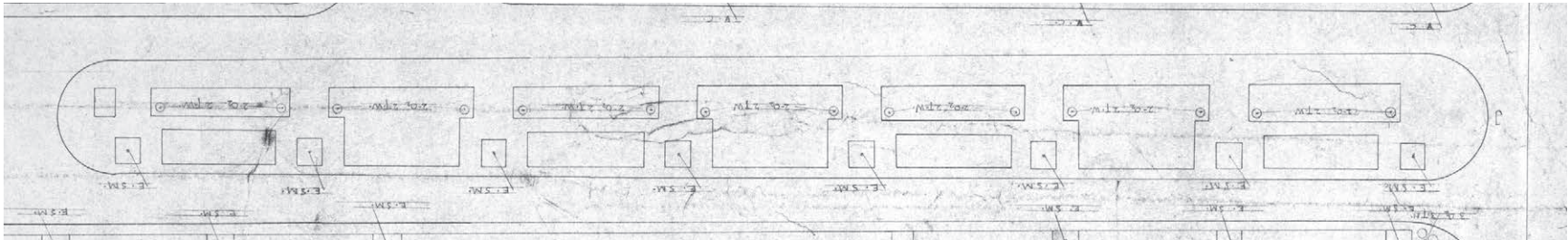
- » Island I is remarkably intact – most original features remain.
- » Many wall panels are cracking or crumbling.
- » Original paving at each end of the island has been replaced with detectable warning surfaces and new poured-in-place concrete.
- » Original natural stone raised planter walls and pre-cast concrete raised planter walls are generally in good condition considering their age. However some sections of wall at each end of the island are deteriorating.
- » Original stairs at the south end of the island are in poor condition. Many stairs are damaged, chipped, and settling. The stairs at the north end of the island are in the same poor condition.
- » Some original natural stone coping on raised planters is damaged and heaving.
- » Patching to original natural stone raised planters is irregular and unsightly.
- » There is differential settlement between paving types, creating trip hazards across the island.
- » Planting is overgrown and contains invasive species.
- » Weeds are growing from the cracks between paving.



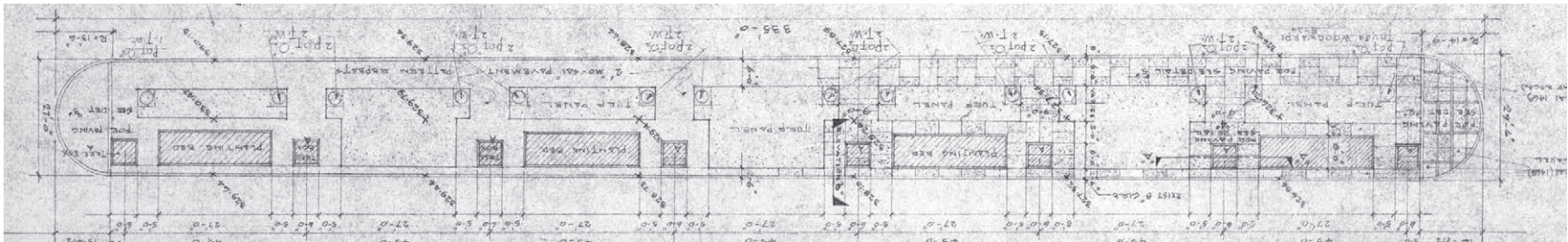
(CBCollective 2024)

Island J

1962 Planting Plan



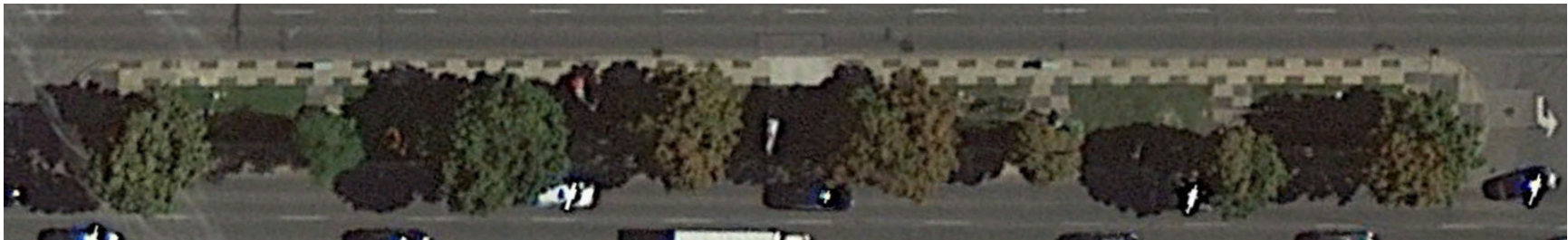
1962 Drawing



1967 Aerial



2022 Aerial



DIMENSIONS

335 feet [102.11m] x 29.5 feet [8.99m] (south end), 27 feet [8.23m] (north end)

DESCRIPTION

Island J consists of four rectangular curbed planting beds situated on the interior of the island. The island is punctuated by eight square raised planters(900mm height0, located on its western side, with each planter containing a tree. The surface treatment of the island features rectangular lawn panels and original exposed aggregate unit pavers in alternating white and gray colors. A plaque and flagpoles from the “Toronto Summit” have been added, along with contemporary city standard benches. A contemporary lighting installation has been installed in the interior of the island c.2021.

FUNCTION

Island J feels narrow and unsafe, as pedestrian pathways directly border active vehicle lanes, compromising both comfort and perceived safety.

CONDITIONS & ALTERATIONS

The following conditions were noted based on site and document review:

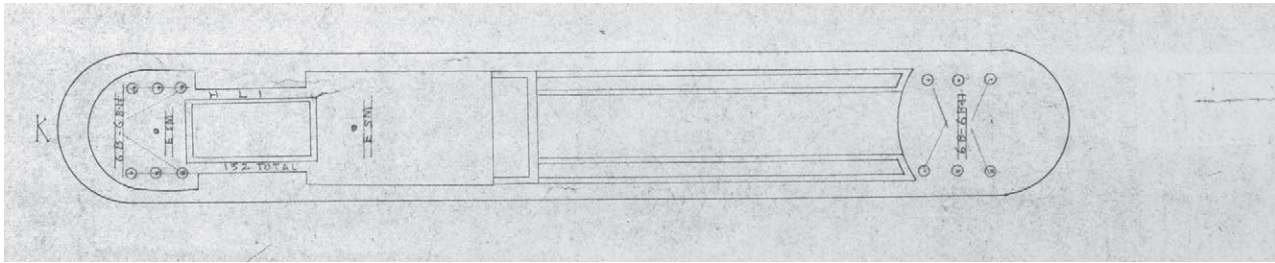
- » Planting is overgrown and contains invasive species.
- » Weeds are growing from the cracks between paving.
- » Planting beds' original brick curbs replaced with masonry slabs.
- » The coping of the original raised planter walls is damaged or dislodged in many areas.
- » Lawn panels have not been maintained well over time so the sod is creeping up over the edge of paving, blurring the transition.
- » Planting beds moved east from original design (as seen on 1965 aerial), to be directly adjacent with turf (sometime between 1978 and 2005).
- » Two clusters of three contemporary planters adorn the island.



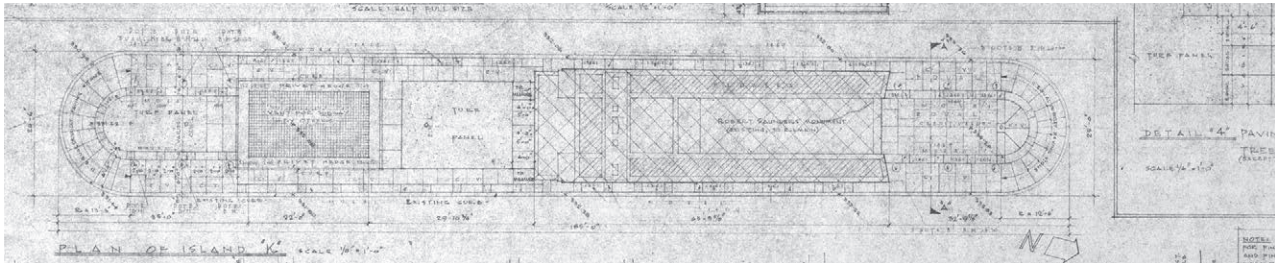
(CBCollective 2024)

Island K

1962 Planting Plan



1962 Drawing



1967 Aerial



2022 Aerial



DIMENSIONS

~190 feet [57.91m] x 26 feet 6 inches [8.08m] (south end), 25 feet [7.62m] (north end)

DESCRIPTION

Island K is composed mainly of a flush surface of lawn and paving. Paving is a mix of original precast concrete pavers in red and white, with an exposed aggregate finish. There are also areas of flagstone paving, which have regular and irregular units. Two trees are planted in the south half of the island, both planted in lawn. At the north end of the island, there is a raised planter (900mm height) in the centre of the island that provides the setting for the Robert Saunders monument (1955-7), which faces north. The island contains one set of stairs at the north end of the raised planter, leading up to the memorial. The raised planter walls are composed of original natural stone slabs with coping. At the south end of the island, there is a TTC vent surrounded by a narrow planting bed.

FUNCTION

Island K is primarily decorative rather than usable. Its original design intent has become obscured over time due to neglect and insufficient maintenance.

CONDITIONS & ALTERATIONS

The following conditions were noted based on site and document review:

- » Original natural stone raised planter walls are generally in good condition considering their age. Some slabs have been replaced.
- » Two contemporary planting pots have been added to the raised planter area.
- » Planting is overgrown and contains invasive species.
- » Weeds are growing from the cracks between paving.
- » Lawn panels have not been maintained well over time so the sod is creeping up over the edge of paving, blurring the transition.
- » Original paving at south end of the island has been replaced with new poured-in-place concrete.



(CBCollective 2024)

Island L

1962 Planting Plan

1964 Drawing

1967 Aerial

2022 Aerial

DIMENSIONS

~233 feet [71.02m] x 23 feet 9 inches [7.24m]

DESCRIPTION

Island L consists of one raised planter of contemporary pre-cast concrete blocks (600mm height) on the interior of the island. The majority of the island is lawn. On the east side of the island, there is a band of exposed aggregate unit pavers in red and black along the perimeter. On the west side of the island, there is a band of poured-in-place concrete paving along the perimeter.

FUNCTION

Island L currently has no meaningful function for pedestrian use.

CONDITIONS & ALTERATIONS

The following conditions were noted based on site and document review:

» Almost all original features have been removed or

replaced. The only original feature is one strip of paving at the curb on the east side of the island.



(CBCollective 2024)

APPENDIX F: EVOLUTION OF UNIVERSITY AVENUE

PLANS 1830, 1892, 1947 & 1963

DTAH, The Art of the Avenue, June 1989

PLAN 1914

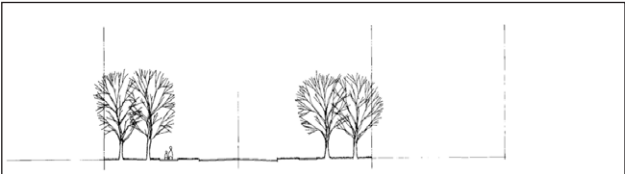
City of Toronto Archives: Series 724, Item 207.

SECTIONS 1830, 1892, 1947 & 1963

DTAH, The Art of the Avenue, June 1989

SECTION 1914

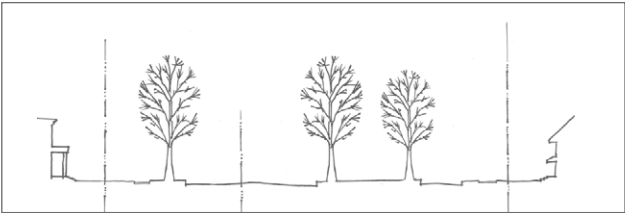
Common Bond Collective



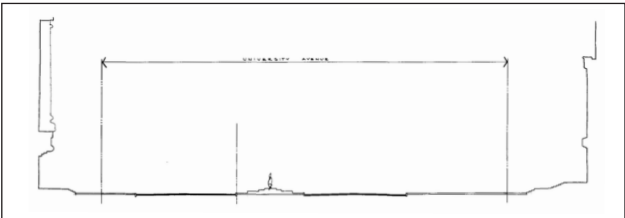
1830



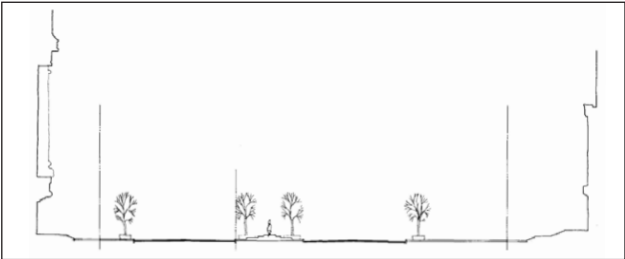
1892



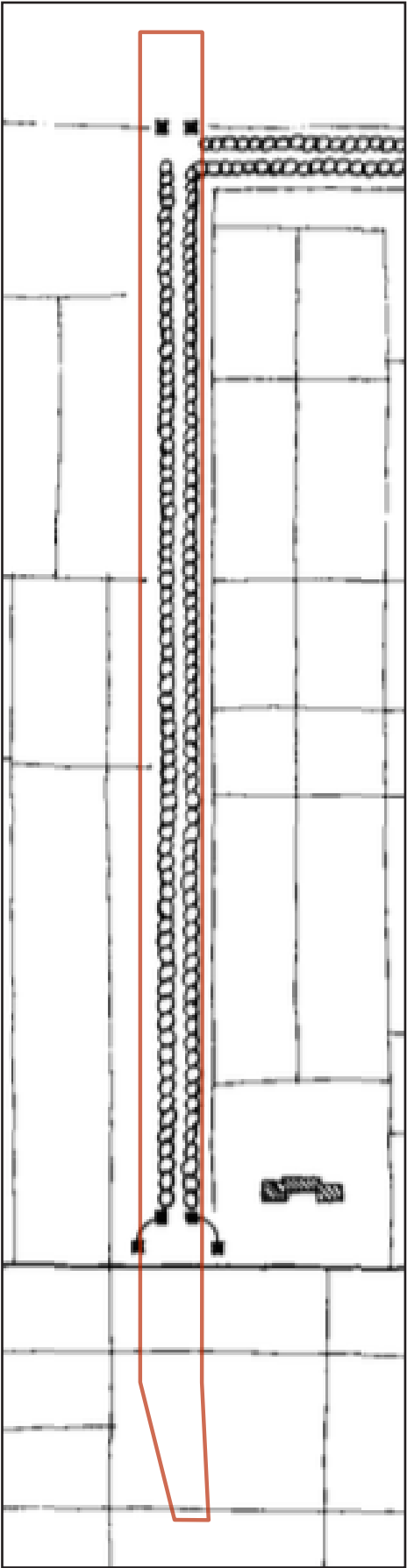
1914



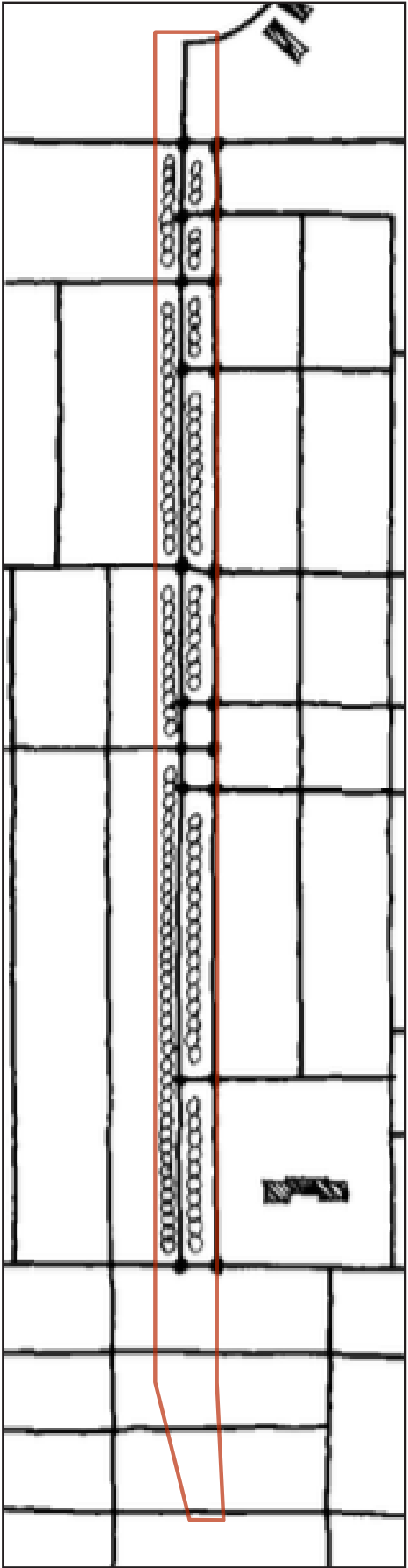
1947



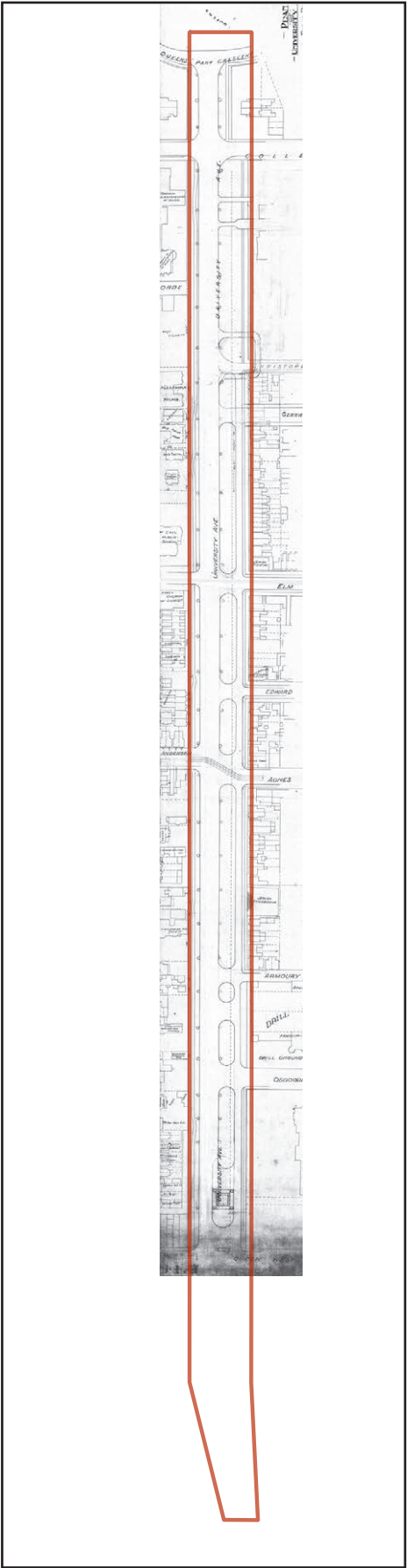
1963



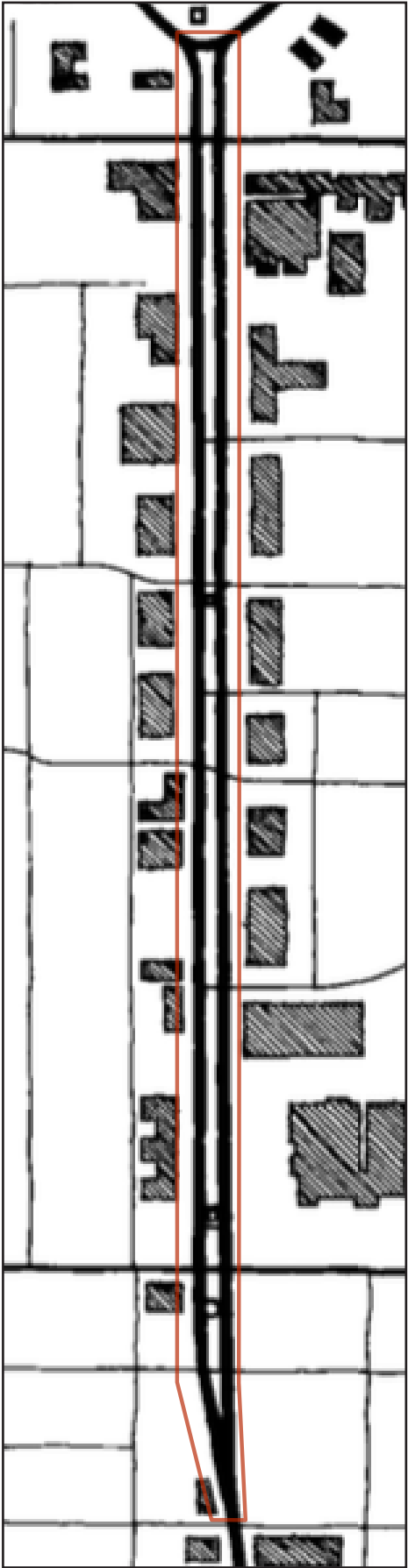
1830



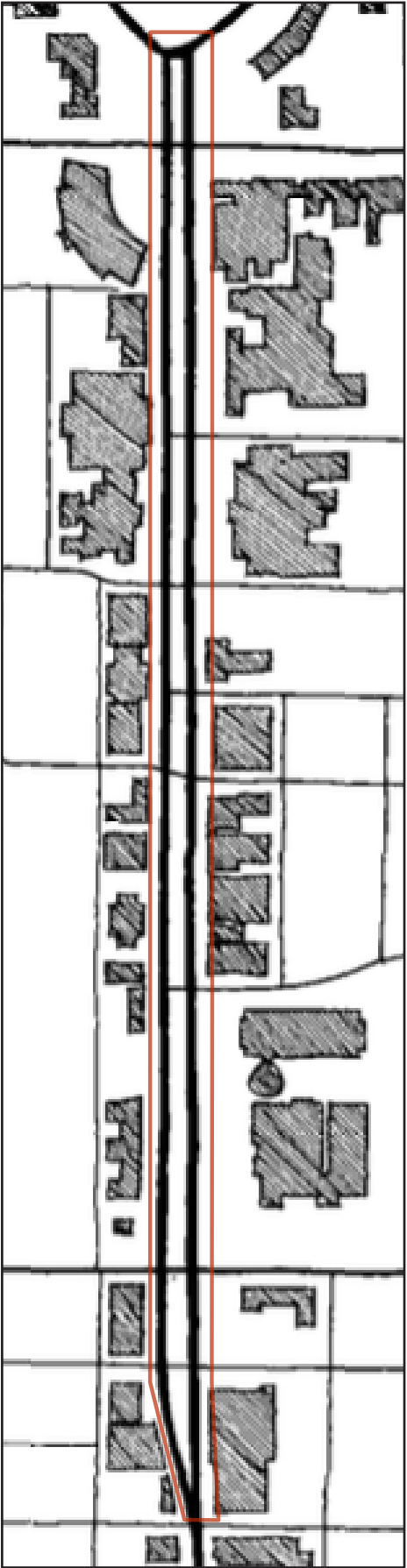
1892



1914



1947



1963

APPENDIX G: CHRONOLOGY

CHRONOLOGY

This chronology was created from the primary and secondary research conducted by Common Bond Collective for the University Avenue Cultural Heritage Study. Names of individuals and organizations are spelled based on the source document. Where source documents reference plans, sketches and drawings, the chronology indicates if they were located during the course of research.

Date	Notes	Source
1797-1798	The land north of modern day Queen Street was divided into lots. 100 acres ran north/south to modern day Bloor Street on the boundary line between lot #11 and #12 which became University Avenue.	Art of the Avenue, p. 129.
1797	July 18: Lot #11 is patented to Reverend Thomas Raddish.	Art of the Avenue, p. 129
1798	December 15: Raddish deeded the land to John Elmsley, Chief Justice. [abstract book says December 18]	Art of the Avenue, p. 129; Land Registry Office, Book 113, p. 217
1798	May 1: Lot #12 deeded to William D. Powell.	Art of the Avenue, p. 129.
1818	College Avenue appears on map between lots #11 and #12 as does the Law Society land on the north/east corner of College Avenue and Lot Street [see Plan of the Town of York #53 W.C 9 June 1818, original Ontario Department of Land & Forests]. Note: This plan appears to be a facsimile and likely includes later information ie College Avenue is not shown on the 1818 Phillpots map.	Art of the Avenue, p. 130.
1822	December 27: Widow of John Elmsley (Mary Elmsley) sold 50 acres (of 100) to Alex Wood.	Art of the Avenue, p. 129.
1825	April 20: Alex Wood sold the southern 50 acres to Honourable J.R. Robinson.	Art of the Avenue, p. 129.
1827	College Avenue appears on map(s) between lot #11 and #12 [see Plan of the Town of York, J.G. Chewett 1827, original Dominion Archives].	Art of the Avenue, p. 130.
1827	University of King’s College chartered as an arts college for Upper Canada.	Dendy, Lost Toronto, p.180.
1828	June 17: J.R. Robinson sold 6 acres (7 chains on Lot Street) to the Law Society of Upper Canada (site of Osgoode Hall).	Art of the Avenue, p. 129.
1829	King’s College council purchases lands for campus (including present Queen’s Park) as well as two approach avenues connecting to existing streets: College Avenue south to Queen Street, and Yonge Street Avenue east to Yonge Street.	Dendy, Lost Toronto, p.180.
1829	College Avenue is laid out as a private street by the Trustees of King’s College. Bishop Strachan and a group of Trustees, all members of the Church of England, had obtained a Royal Charter for the University in 1827. But, due to the public outcry against the charter, it was abolished in 1837 and a new charter was issued which required a belief in ‘the trinity’ (Christian church).	Art of the Avenue, p. 130.
1829	May 19: Mary Elmsley sold the north 50 acres of lot #11 and one chain through lots #9 and #10 for a road from Yonge Street to King’s College.	Art of the Avenue, p. 129.
1829	December 2: Robinson sold a 66 foot strip (1 chain wide by 51 chains on the front southern half of lot #11) to King’s College. King’s College’s beginnings were tumultuous at first, with development stalling under lieutenant-governor Sir John Colborne’s tenure (1828-1835). The acquisition and clearing of the south and east approaches to the campus were some of the only projects undertaken before an architect was engaged in 1837, and the first building completed in 1845.	Art of the Avenue, p. 129; Dendy, Lost Toronto, p. 180-181.

Date	Notes	Source
1829	August 15: William Powell sold the north half of lot #12 (north 51 chains of lot #12 and 1 chain of the south 51 chains) to King’s College.	Art of the Avenue, p. 129.
1830	1830-1833: College Avenue and Yonge Street Avenue are landscaped according to the plans of André Parmentier, a Belgian horticulturalist who had moved to Brooklyn in 1824. The trees and shrubs for College Avenue were supplied by New York nurseries. Parmentier died in 1830. John Wedd (as original Superintendent of Grounds at King’s College, see UofT Monthly, Vol. II No. 1 p.26) was in charge of supervising construction. Parmentier created a grand and secluded processional approach to King’s College based on European precedent. The design was based on a 120’ thoroughfare, configured with a central carriageway flanked by boulevards and walkways on both sides. Double rows of flowering pink chestnuts provided shade on each side. Avenue intentionally isolated from surroundings from the start, reflecting its private purpose and tranquil intentions. Gates and lodges were installed at both Queen and Yonge street entrances, denying access to commercial traffic. No cross streets were permitted for the length of College Avenue. As a result College Avenue effectively obstructed the growing city’s grid, leading to movement to permit intersections by 1850s at the latest.	Dendy, Lost Toronto, p. 182.
1830	1830-1833: Both avenues were fenced for their entire length, with pink horse-chestnuts planted as well.	Bain, p. 195.
1830	Scadding gives description of work being done to clear College Avenue and plant horse-chestnut saplings, which were at the time rare in York.	Scadding, Toronto of Old, p. 231.
1830s	College Avenue was designed by John Wedd to be 120 feet wide with a boulevard and walkways on both sides of a central carriageway. The original plan had a double row of pink flowering chestnut trees on either side and was designed as a processional approach to the University. All east/west streets stopped at College Avenue with fences blocking their access to the Avenue. It would appear that a variety of trees were planted and there was a mixture of trees with the predominant chestnuts.	Art of the Avenue, p. 130.
1830s	College Avenue became a popular promenade for walking through the 1830s, ‘40s and ‘50s, as Toronto grew and existing accessible open space was disappearing. Also supported footraces, concerts and parades. Waterfront promenade in particular was, being gradually eliminated by industry. Prior to the opening of Queen’s Park in 1860s, College Avenue was the park. Campus itself was not easy to access and lightly used. (See 1858 entry quote below)	Bain, The Queen's Park and its Avenues, p. 194-6.
1833	October: College Avenue lodge and gates completed at Queen Street entrance, to Cottage Gothic style design by John G. Howard. Lodge and gates were set on a semi-circular plan off Queen Street. Second lodge built before 1851.	Dendy, Lost Toronto, p. 182.
1834	City of Toronto is incorporated, extending northern limits to Bloor Street.	

Date	Notes	Source
1837	May: Architect Thomas Young engaged to design and supervise King’s College campus buildings. Young designed three stone buildings connected by cloisters in a Greek Revival style.	Dendy, Lost Toronto, p. 180.
1838	College Avenue ran north from Lot Street (now Queen Street) to the University and then east to Yonge Street (east/west portion sometimes referred to as Yonge Street Avenue).	Art of the Avenue, p. 130.
1842	College Street is a broad street with trees on either side [Topographical City and Liberties, James Cane].	Art of the Avenue, p. 130.
1842	April 23: Construction of the new King’s College building commences. The ceremonies begin with a grand procession traveling north along College Avenue to the campus site: “A procession such as had never before been seen in these parts slowly defiled up the Avenue to the spot where the cornerstone of the proposed university was to be laid.’	Scadding, Toronto of Old, pp. 233-4.
1843	King’s College is erected at the head of ‘College Avenue’. The south wing is built and opened.	Art of the Avenue, p. 130.
1845	The southwest block of King’s College is completed, the only part of Young’s scheme to be built.	Dendy, Lost Toronto, p. 181.
1849	King’s College is secularized, creating the University of Toronto.	Dendy, Lost Toronto, p. 181.
1850	Plan 16 in Park Lot 11 (superceded as Plan D14 in 1861) is first of residential subdivisions on east side of University Avenue, believed to be origin of Park Lane, followed by Plan 24 in same year, and Plan 147 in 1856.	LRO 80, Book 604A, 604B PLAN D14, pp.2-3
1851	Park Lane appears running parallel, but separate, on the east side of College Avenue with a high board fence separating Park Lane from College Avenue [Toronto Topographical Plan].	Art of the Avenue, p. 5.
1852	Reference to Park Lane is made in The Globe.	“City Council.” The Globe, June 15, 1852, p. 286.
1855	George Brown, the editor of The Globe was to complain that “College Avenue, being surrounded by fences, imposes a positive barrier to the locomotion of the residents in that vicinity, and they find themselves called on every occasion when journeying in a westerly or easterly direction, to make a considerable detour to arrive at their destination. This is an inconvenience which, in the interest of land owners and residents, should at once be remedied. It is no argument to contend that the beauty of the Avenue would thus be destroyed, and its privacy as a park intruded on...”	Art of the Avenue, p. 131.
1856	University Park was originally the northern section of park lots #9,#10,#11, and #12. In total, 168 acres was taken over by the Government, two-thirds of which (about 140 acres) west of Queen Street avenue [sic] was set aside for the use of the University. The remainder was reserved for the Parliament Buildings and Government House. Dendy notes bill passed in 1853 for the expropriation of part of University Park (including buildings) for Parliament Buildings. The university briefly returned to facilities before they were again appropriated in 1856 as the University Lunatic Asylum. Construction of University College began that same year.	Art of the Avenue, p. 131; Dendy, Lost Toronto, p. 181.

Date	Notes	Source
1858	College Avenue is described as “our chief promenade, and its shady walks in sunny days and moonlight nights, are the resort of large numbers from the city” [Guide to Toronto, 1858].	Art of the Avenue, p. 131.
1858	August 16: Provincial act to create park passed, giving Toronto British North America’s first municipally operated park. Named University Park until 1860. Park included much of previous King’s College campus east of Taddle Creek, plus the two avenues (~44ac = 49 ac less 5 ac for lunatic asylum). [See 1859 Map to illustrate extent] 51 villa building lots registered east of the park boundary.	Bain, p. 197.
1859	City of Toronto takes a 999 year lease on both avenues, as well as parkland north of legislature. The avenue is opened up to general traffic, and quickly becomes important north-south artery. Transition from private (but accessible) landscape to public.	Dendy, Lost Toronto, p. 183.
1859	The City of Toronto takes a 999 year lease on 49 acres of park and two avenues, for the purpose of a public park, which was to be called ‘[The] Queen’s Park’. Building lots for ‘Villas’ are laid out around the park and several had been built on by 1868.	Art of the Avenue, p. 5, 131.
1859	January 1: Lease agreement between City and University established rules and regulations: asylum to remain within park area; University retained right to sell lease lots around the park; all maintenance of roads, grounds, lodges and construction of roads, fences, and a gas lighting system was city responsibility.	Bain, p. 197.
1859	City considered allowing crossroad intersection to College Avenue between Queen and Queen’s Park connecting Agnes and Anderson streets (near present Dundas Street West), rousing strong debate. Council ultimately created two east-west entry gates, rather than disrupt the integrity of the promenade.	Bain, p. 198.
1859	“The Park also became the city's principal location for memorials. Two canons from the Battle of Sebastopol, presented to the city in 1859,”... followed by Canadian Volunteers Monument, George Brown, Northwest Rebellion.	Bain, p. 198.
1860	September 12: Opening ceremonies naming ‘[The] Queen’s Park’ and opening to the public. [see 1882 account of College Avenue and Queens Park in Picturesque Canada Vol.1 , pp. 417, 432 & 436].	Dendy, Lost Toronto, p. 183.
1860	Parkland in Toronto was hitherto private or informal, however in the 1840s and 1850s land for informal public use was disappearing. The Queen’s Park became the first ‘public’ park, (being a government-owned park for public use) in Canada., and this included College Avenue. [refers to English landscape park, rather than square, common or grove.] September opening of new Park by Prince of Wales brought improvements: New gates built at Queen Street entrance for new park, with old gates relocated to Yonge Street entrance; fences rebuilt, Queen Street lodge improved. IN the park trees were planted, and new roadway built.	Bain, p. 193-4. Bain, p. 196.

Date	Notes	Source
1860-1871	Queen Victoria monument created by Marshall Wood. Located in the southern area of Queen's Park. [Wood meant to sell, not donate the statue to the city. Price rose from \$3,000 to \$7,500 by 1874. Council asked Wood to remove it and replaced it with cast iron fountain (\$559.50), surrounded by fence and landscaping criticized as meagre.].	Art of the Avenue, p. 135.
1861	The name 'University Street' is listed in the City Directory; formerly known as 'Park Lane', it ran parallel to College Avenue on the eastern edge.	Art of the Avenue, p. 131.
1866	The name 'Queen's Park' appears on map(s), it was formerly known as 'University Park'.	Art of the Avenue, p. 131.
1868	Toronto in the Camera describes the street as a <i>“beautiful Avenue, which is three-quarters of a mile in length and 120 feet in width, and through the centre of which there is a carriage drive flanked by a grass border of considerable width, leads from Queen Street to the Queen's Park; and immediately opposite to its northern terminus is the site of the proposed statue of Her Most Gracious Majesty. The Avenue is one of, if not the finest in the Dominion, or perhaps on this Continent,...The Park forms one of the great lungs of the City, form [from] a place of considerable beauty and attraction.”</i>	Art of the Avenue, p. 131.
1868	Original iron and wood Queen Street gate replaced by cast iron fence. New design related to Osgoode Hall fence, may have been by Cumberland and Storm.	Dendy, Lost Toronto, pp. 182-3.
1870	City of Toronto undertakes replanting of the avenues, repairs to College Avenue gates and Queen Street cottages.	Dendy, Lost Toronto, p. 183.
1870	Canadian Volunteers, or Ridgeway Monument created by Robert Reid. Located in the southwest area of Queen's Park. An ad hoc public committee of Toronto Volunteers Relief Committee invited proposals and raised funds by general public subscription (including various municipal councils), and selected the artist for the creation of the monument.	Art of the Avenue, p. 136.
1874	Queen Victoria statue was removed and the purchase was proposed by the City Committee of Walks and Gardens, subsequently by an ad hoc public committee.	Art of the Avenue, p. 135.
1879	DESCRIPTION OF COLLEGE AVENUE: "This drive and promenade, leading from Queen street to the University grounds, is about one mile in length, 120 feet in width, and contains about 10 acres of land, It is handsomely planted with trees on either side, through the centre of which there is a carriage drive, flanked by a grass border of considerable width. The avenue is one of the finest in the Dominion, or perhaps on this continent, and was laid out about the year 1830, by the University of King's College, now the University of Toronto, and planted by the late Mr. John Wedd, In 1859 this with the Yonge street avenue, which is much narrower, and crosses it at right angles at its northerly termination, together with fifty acres of the University Park, were granted to the Corporation of the City of Toronto, on a lease for a period of 999 years, for the purpose of a Public Park, to be kept in order by the city, Building lots for villas of a certain description have also been laid out for leasing around this Park, and many elegant residences have already been erected."	Phelan Bros.' St. Lawrence Traveller, 1879. pp.16-17.

Date	Notes	Source
1881	Work underway on College Avenue by City: -Curbing and grading west side of Park Lane for boulevard -Old picket fence is torn down -4’ wide sidewalk to be laid on former fence line “This will be greatly appreciated by the citizens, as it will afford a pleasant walk under the shade of the trees.”	The Globe, August 11 1881, p. 8.
1882	Fence running along College Avenue removed, creating wider avenue.	Bain, p. 204.
1882	Description of College Avenue: "... Adjoining the grounds of Osgoode Hall, and facing the College Avenue, is University Street, or, as it was formerly called. Park Lane. The latter designation was no doubt given it in imitation of the Belgravian thoroughfare which forms the eastern end of Hyde Park, in the British metropolis; but the fitness of the appellation, in Toronto, is due to the fact that the street skirts one of the finest natural avenues on the Continent, and not to any architectural beauty. The street, however, ought to be one of the favourite portions of the town for residence. Turning into the avenue on a summer day one gets a glimpse of sylvan beauty such as rarely meets the eye. A mile of chestnuts and maples flanks a carriage-drive and pathway which, in the vista, open out upon the Queen's Park. For the tourist the city has no sight so charming, unless it be a view of the bay on a still afternoon when the setting sun paves it with flame."	Picturesque Canada Vol.1 , p. 432.
1882-1884	George Brown statue created by Charles Bell Birch. Located in the southern area of Queen's Park. Provided by the City while an ad hoc public committee selected the artist and raised the funds (£1,000) by general public subscription.	Art of the Avenue, p. 137.
1883	Queen Street gates and fencing removed, replaced by a post and chain. City commences streetcar service on College Street.	Bain, p. 196. Bain, p. 207.
1884	Taddle Creek Channeled underground.	Bain, p. 204.
1886	Work is begun on the new Legislative Buildings. And the old King's College building is demolished.	Art of the Avenue, p. 131; Dendy, Lost Toronto, p. 181.
1888	January 31: The lease between the City and the University is canceled by the courts after a lengthy dispute between them over violations of the terms of the lease, including upkeep, rowdy activities, and the use of College Street as streetcar route. Lease reinstated later that year, with new clauses and commitment from city. Traffic restrictions removed: “The said avenues are to be dedicated by the Crown to the public, and all restrictions as to traffic thereon removed, except in so far as the city may choose to restrain and regulate the same as hereinafter provided. [xxxx] the Crown reserves any right in may have to require from adjacent owners payment for the right to front upon these avenues, and the price to be paid for such right shall, in case of difference between the parties, be settled by arbitration as provided for in the Municipal Act. The city is to have power to restrain and [xxxxxxx] traffic in said Queen's Park and avenues.” ('Municipal Affairs,' Globe, 28 Nov. 1888, p.5.)	Bain, p. 207.

Date	Notes	Source
1889	With the renegotiation of the lease between the University of Toronto and the City. The City was to pay \$6,000.00 per annum, in effect endowing two chairs and the salary of two professors. College Avenue is opened up as a public street, allowing east/west cross-streets. The fences and gates were removed but the trees remain. Park Lane remains as a side street to the wider College Avenue and Queen's Park Crescent appears on map(s) as a through street. Avenue Road is then widened north of Bloor Street to Upper Canada College.	Art of the Avenue, p. 132.
1889	City's lease terms were renegotiated and the avenue was opened as a public thoroughfare.	Art of the Avenue, p. 7.
c.1890	Dendy notes, 'The avenue itself was widened, with a boulevard dividing northbound and southbound roadways.'	Dendy, Lost Toronto, p. 183.
1891-1894	Sir John A. Macdonald statue created by Hamilton MacCarthy. Located in the southern area of Queen's Park. Provided by the city and an ad hoc public committee raised funds by general public subscription. A site sub-committee and a selection sub-committee invited design submissions, selected the artist, and gave out 1st (\$11,000), 2nd (\$200), and 3rd (\$100) prizes to the winners.	Art of the Avenue, p. 138.
1892	The Legislative Assembly Building was opened [built], on an axis at the top of College Avenue. Park function is somewhat diminished, with the Queen's Park reduced to front and rear yards of provincial parliament buildings and northern half converted from recreational to treed/planted landscape. Preaching also banned.	Art of the Avenue, p. 7, 132; Dendy, Lost Toronto, p. 183; Bain, p. 208.
1893	Toronto Armories construction completed (- begun 1891)	Dendy, Lost Toronto, p. 184.
1894	The name 'Queen Street Avenue' is listed in the City Directory; it was previously known as 'College Avenue' and as 'Queen's Avenue'.	Art of the Avenue, p. 132.
1895	Northwest Rebellion of 1885 monument created by Walter Allward. Located in the southern area of Queen's Park. Provided by the City and an ad hoc public committee of women raised the funds (\$4000) by public subscription and with federal, provincial, and city grants. It is not known how the artist was selected.	Art of the Avenue, p. 139.
1895	Bridle path built on College Avenue.	Bain, p. 208.
1895	Sub-committee of Council on street naming sends for approval a list of street name changes in Toronto, among them College Avenue to University Avenue	The Globe, 10 Oct 1895, p. 5.
1896	Toronto Board of Control established. An executive committee of City Council. "When it was initially created in 1896 by mandate of the provincial government, it consisted of three Controllers appointed from and by the aldermen, and presided over by the Mayor of Toronto. Beginning in 1904, the Board of Control was directly elected by the city's electorate and consisted of four Controllers, presided over by the Mayor. Each voter could vote for up to four candidates, and the four with the most votes were elected. By tradition the controller who received the most votes would get the powerful budget chief position."	https://en.wikipedia.org/wiki/Toronto_Board_of_Control
1896	Bain ties the name University Avenue to this year.	Bain, footnote 2.

Date	Notes	Source
1897	Bicycle path built on University Avenue	Bain, p. 208.
1897	The name 'University Avenue' is listed in the City Directory, combining 'Queen Street Avenue' and 'University Street' (formerly Park Lane).	Art of the Avenue, p. 132.
1902	Queen Victoria statue created by Mario Raggi. Located in the southern area of Queen's Park. Provided by the Province. A letter from Saint George's Society from 1886 was found reminding the Province of the failed commitment to erect a Victoria monument in Queen's Park. The Province then donated funds following Victoria's death (\$10,000) and the statue was installed in 1902 with the unveiling in 1903.	Art of the Avenue, p. 141.
1903	City officials considered closing University Avenue to traffic, creating a true park, but no action was taken. The opposite happened, with University Avenue evolving into a busy thoroughfare in the first half of the 20th century. Effect was accentuated by more cross streets, 1931 extension south, and 1949 redesign.	Bain, p. 208-9.
1903	John Graves Simcoe statue created by Walter Allward. Located in the southern area of Queen's Park. Provided by the Province and an ad hoc committee raised funds (\$6000) by public subscription, provincial, and city grants. The committee also organized an international competition and selected the artist. The choice was then approved by the Guild of Civic Art.	Art of the Avenue, p. 140.
1903	Sir Oliver Mowat statue created by Walter Allward. Located in the southern area of Queen's Park. Provided by the Province. Province voted funds following Mowat's death (\$10,000), and commission began in 1903.	Art of the Avenue, p. 142.
1906	City and Toronto General Hospital agree on new site at University Avenue and College Street.	The Globe, April 7, 1906, p. 26.
1907	John Sandfield Macdonald statue by Walter Allward. Located in the southern area of Queen's Park. Provided by the Province with the funds voted on in 1907 (\$10,000) the commission was won by Allward in 1907.	Art of the Avenue, p. 143.
1908	University Avenue is labeled as "The Avenue", by the Toronto Guild of Civic Art, in their plan of improvements. The full-size city plan proposed two major avenues focussing on Queen and University and Queen and Church Streets. Although, in the same year the street would still be referred to as 'College Avenue' on some maps.	Art of the Avenue, p. 132.
1910	South African War Memorial/Boer War Memorial created by Walter Allward. Located north of Queen Street facing south, provided by the City and Metropolitan Toronto. An ad hoc public committee was formed 1903, they raised funds by public subscription, provincial and city grants (\$38,000). The Plans and Design Committee organized an international competition and selected the artist by 1904. A City Architect was chosen to supervise the installation and was selected by 1905. The site location of the Armories facing east was chosen in 1908 but the approval of the Guild of Civic Art was denied. The location site to the south on the east boulevard was then chosen by City Council in 1909 and cornerstone laid the same year. The memorial was unveiled in 1910 with the top figure installed the following year.	Art of the Avenue, p. 7, 144.
1911	In the Civic Improvement Committee Report (Federal Avenue proposal) University Avenue still ends at Queen Street.	Art of the Avenue, p. 132.

Date	Notes	Source
1914	Formalization of off-central median by City of Toronto Parks Department.	1914 Plan of University Avenue, Parks Department (COTA: Fonds 724, Item 207)
1917	Dundas Street West crosses University Avenue.	Bain, p. 209.
1918	DESCRIPTION OF UNIVERSITY AVENUE: "Turning back for a short distance, the above avenue is reached. At the entrance of the avenue will be seen the magnificent monument to the Canadian heroes who fell in the South African War of 1899 to 1901. The drive and promenade, leading from Queen Street to the University grounds, is nearly one mile in length, 120 feet in width, and contains about ten acres of land. It is handsomely planted with trees on either side, through the centre of which runs a carriage drive, flanked by a grass border of considerable width. The avenue is one of the finest in the Dominion, or perhaps on the continent, and was laid out about the year 1829 or 1830, by the University of King's College, now the University of Toronto, and planted by the late John Wedd. In 1859, this, with College Street avenue, which is much narrower, and crosses it at right angles at its northerly termination, together with fifty acres of the University Park, were granted to the Corporation of the City of Toronto, on a lease for a period of 999 years, for the purpose of a public park, to be kept in order by the city. Building lots for villas of a certain description have also been laid out for leasing around this park, and many elegant residences have been erected."	The Queen's Hotel Traveller's Guide, 1918.
1918	First suggestion of extending University Avenue south to Front Street. W.J. Gage proposed it as a fitting memorial to those who died in the First World War, "Hero Avenue" [Toronto Scrapbook, volume 4, p. 255].	Art of the Avenue, p. 132.
1918	Memorial to nurse Edith Cavell created by Florence Wyle. Located on the southeast corner of College Avenue. The funds were raised through public subscription by the Toronto General Hospital (TGH) Board of Trustees in 1918. The artist was then commissioned in 1919.	Art of the Avenue, p. 19, 145.
1919-1927	Sir James Pliny Whitney statue created by Hamilton MacCarthy. Located in the southern area of Queen's Park. Provided by the Province. Province voted funds (\$15,000) and the original commission of Walter Allward was transferred to MacCarthy in 1923.	Art of the Avenue, p. 148.
1921	Sons of England War Memorial created by Charles Adamson originally located south of College Street facing north. Now located south of Elm Street facing north. Provided by the City and Metropolitan Toronto Council. Funds raised and work commissioned by the Sons of England Benefit Society, accepted by City Council on direct application in 1921 but was subject to approval of Commissioner of Parks.	Art of the Avenue, p. 19, 146.
1923	48th Highlanders Memorial created by Mathers and Haldernbly. Located in the northern area of Queen's Park. Provided by the City. The funds were raised and the design commissioned by the regiment (48th Highlanders). It was accepted by the Council on recommendation of the Commissioner of Parks in 1922. It was then subject to approval of the design and placement by the Parks Department and unveiled in 1923.	Art of the Avenue, p. 147.

Date	Notes	Source
1928	The province passes the 'University Avenue Extension Act'. The act allows the City of Toronto to expropriate certain lands between Queen Street and Fleet Street for the purpose of extending University Avenue south and pay for the work. It also allows the City to pass by-laws to regulate setbacks, heights, materials, 'character', and use of buildings along University Avenue 'as so extended'. It proposes a circular plaza at Richmond Street, named 'Vimy Circle'. The southern extension was to be called 'Queen's Park Avenue'. It was to be 100 feet wide from Richmond to Front Street and York Street [S.G. Curry, 'University Avenue Extension, Toronto' in Construction (Toronto) volume 21 (December 1928), p. 420,428 and the Report of the Advisory City Planning Commission,, p. 23-25, 29-31, plus S.G. Curry 'Suggested Improvements for Toronto's Downtown Section' in Construction (Toronto) volume 22 (Feb. 1929), p. 62-8;	Art of the Avenue, p. 132.
1929	The Advisory Planning Commission recommends that University Avenue be extended southward to connect with Front Street, at a slight easterly angle.	Art of the Avenue, p. 132.
1930	Robert Raikes statue created by Sir Thomas Brock (after the original of 1880). Located in the south west area of Queen's Park Crescent. Provided by the City and donated by the International Council of Religious Education on occasion of quadrennial convention. The design was accepted by Council in 1930 on recommendation of the Committee on Parks and Exhibitions (donor rejected site in Allan gardens and requested the Queen's Park Crescent Site).	Art of the Avenue, p. 149.
1930	Bylaw #12790 passed on 22 September 1930, resulting in University Avenue being extended south to Front Street.	Art of the Avenue, p. 9, 132.
1931	October: University Avenue is officially opened south from Queen Street to Front Street. November 2: Bylaw #13409 passed [University Avenue By-Law], regulating height, materials, design and use of buildings on University Avenue between Front Street and College Street. Creates a strong, consistent building line and height limit to the Avenue.	Art of the Avenue, p. 133. See by-law amendment/ repeal history at CoT by-law registry

Date	Notes	Source
1931	<p>November: Objectives of By-law No. 13409 were “that the Avenue should:</p> <ul style="list-style-type: none">• Serve as a major vehicular traffic route linking the Parliament Buildings on Queen’s Park and Union Station;• Provide access to the downtown civic and business areas;• Be a street of distinction lined with solid and impressive buildings;• Have a sense of unity derived from the form and finishes of the buildings flanking it.” <p>Diagram of the area of application of the by-law included.</p> <p>It contained provisions regarding building developments:</p> <p>1) restrictions on building use;</p> <p>2) review of plans and design by an advisory committee appointed by council</p> <p>3) regulations regarding the volume of buildings</p> <p>4) regulations regarding maximum building heights, setbacks and angular planes</p> <p>5) permitted materials for building facades ie colour and texture</p> <p>6) elimination of commercial advertising and restrictions on signage</p> <p>7) standards for fencing vacant lots</p> <p>8) penalties for non-conformance</p>	1971.02 A review of the University Avenue by-law, City of Toronto Planning Board, pp. 4-5.
1931	Canada Life Building is completed (begun 1929), reflecting a contemporary vision for avenue - ‘imperial and elegant, rather than pastoral and quiet’	Dendy, Lost Toronto, p. 183.
1932	Sir Adam Beck statue created by Emanuel Hahn. Located south of Queen Street facing north. Provided by the City and Metropolitan Toronto. An ad hoc City Council committee was created in 1929 to select the site and arrange a national competition for the design. \$15,000 in funds was voted on by the city in 1929. Toronto Hydro-Electric Commission added \$10,000 and a selection by a three-person board of assessors was then appointed by OAA, OSA, and SSC in 1929. The raised funds included 2nd prize (\$250), 3rd prize (\$100), and four students’ competition awards. Various sites were debated by the Council on recommendations of the Commissioner of Parks, City Architect, and Commissioner of Works. The current site was approved in March 1932.	Art of the Avenue, p. 20, 150.
1940	William Lyon Mackenzie statue created by Walter Allward. Located in the western area of Queen’s Park. Provided by the Province. Funds were raised by public subscription and work was commissioned by an ad hoc public committee. The original reflecting pool was filled in shortly after installation.	Art of the Avenue,, p. 151.
1947	Park Lane merged into University Avenue. Causing the realignment of the median between the two streets and shift off axis for the Legislative Assembly.	Art of the Avenue, p. 11; No Mean City epilogue.

Date	Notes	Source
1947	May: Bylaw #16929 passed, authorizing the widening of the existing pavement on University Avenue, north of Queen Street up to, and including, Queen’s Park Crescent [west and east]. This increased the size from 60-82 feet to 90-102 feet.	Art of the Avenue, p. 133.
1947	May - First consideration given to improving University Avenue by the Toronto City Planning Board when it reviewed a plan for a ‘modernistic’ treatment. Unsatisfied with the proposed design, the Board requested a more traditional design from H.B. Dunington-Grubb.	University Avenue Decorative Treatment, COTA, Fonds 2032, Series 721, p. 1.
1947	August: City approves the moving of the South African War Memorial in connection with the widening of University Avenue. By the end of 1947 the trees on the side boulevards of University Avenue had been removed. The Avenue was redesigned, with Park Lane being widened to take the north-bound lanes, and the wide boulevard between it and the Avenue was reduced to the present width.	Art of the Avenue, p. 133.
1947	October: South African War Memorial relocated to the centre median.	Art of the Avenue, p. 144.
1948	February - H.B. Dunington-Grubb prepares a general program of development at the cost of \$193,000. While the City Architect and Commissioner of Works were satisfied, the Board considered them too elaborate and in April instructed that plans be prepared for a more simple and symmetrical layout. While Dunington-Grubb prepared a revised sketch, the Board felt it lacked detail sufficient to enable a decision. Asks the Toronto Board of Trade to provide cost estimates.	University Avenue Decorative Treatment, COTA, Fonds 2032, Series 721, p. 1.
1948	April - Council approves Planning Board’s general proposals for ‘decorative treatment’ of University Avenue, following completion of the ‘new dual highway’.	Art of the Avenue, p. 133.
1948	April 14 - Dunington-Grubb writes to Tracy leMay, Commissioner of City Planning with a further explanation of the proposal for University Avenue, stating “Our work is not to be regarded as a set design for the Avenue as a whole but rather as ten alternative designs as we have used a different style for each block. Among them will be found symmetry and asymmetry, regularity and irregularity, formality and informality. We were expecting that a choice would probably be made from amongst these various designs perhaps accepting some and discarding others. The Board might even feel that it would prefer one uniform scheme from end to end like the Prado at Havana, Cuba, but my own opinion is that variation would provide more interest.”	H. B. Dunington Grubb to Tracy D. leMay, April 14, 1948. COTA, Fonds 2032, Series 721, Files 31/32.
1948	April 23 - Letter from the City Planning Board to H.B. Dunington-Grubb, expressing the opinion that the design was too complicated and that it would prefer a simpler and symmetrical pattern. Requests a rough sketch.	Letter from [H.E. McCallum, Mayor] to H.B. Dunington-Grubb, April 23, 1948. COTA, Fonds 2032, Series 721, Files 31/32.
1948	May 10 - H.B. Dunington-Grubb replies with a revised sketch that was presented to leMay the week prior. Dunington-Grubb studied the ‘problem’ as a symmetrical scheme and eliminated the central promenade feature, proposed a continuous planting of dwarf growing flowering trees throughout the length. “Each block is shown as a different alternative design.”	Letter from H.B.D. Grubb to Toronto City Planning Board, May 10, 1948. COTA, Fonds 2032, Series 721, Files 31/32.

Date	Notes	Source
1948	June 30 - Letter from Toronto Hydro-Electric System to Mayor McCallum with a proposed design for light standards for University Avenue. It was proposed to have the city’s coat of arms on the aluminum base and have no overhead wires. It was to be manufactured by the System. It complied with the Illuminating Engineering Society’s recommendations to have the “light 25 feet above the pavement and 8 feet out from the curb.”	Letter from E.M. Ashworth to H.E. McCallum, June 30, 1948. COTA, Fonds 2032, Series 721, Files 31/32.
1948	August 25 - Letter from DND noting that no space has been left for Troops and guns to come to the west [?]. “As you know the people line up on both sides when there is a parade and with the street as it is now this will be impossible and certainly spoil the whole view of the parade.”	Letter from Capt. Guy Mackenzie to Controller J. Innis, August 25, 1948. COTA, Fonds 2032, Series 721, Files 31/32.
1948	September 21 - Discussing amended design by Dunington-Grubb and Stensson submitted July 15 and 16, 1948 which provided a stone-paved walkway for its length between Adelaide and College streets. The Board of Trade recommended accepting the plan for University Avenue’s ‘decorative treatment.’	Letter from Board of Trade [President and General Manager] to Toronto City Planning Board, September 21, 1948. COTA, Fonds 2032, Series 721, Files 31/32.

Date	Notes	Source
1948	<p>November 16 - Report of DGS to Toronto City Planning Board.</p> <p>“University Avenue is almost the only wide street in Toronto. Connecting the Union Station with the Parliament Buildings, the University, and the north, its location makes it inevitably, not only a great traffic artery, but also a monumental street providing sites for many of the City’s most important buildings Adequately developed it could become the most famous street in the Dominion of Canada.” The report then asks a fundamental question as to whether the public should be kept out. DGS favoured a layout for use but with access restricted to street intersections.</p> <p>The Central Boulevard strip - “The axis line between the two roadways has been centered on the Parliament Buildings, resulting in a boulevard strip of a width diminishing from fifth feet at Queen Street to twenty-two feet north of College Street.” DGS suggests a paved walk flanked on either side by low hedges, dwarf trees, flower beds and benches but not grass. Recommend a uniform width of 14’.</p> <p>Materials - Are the walks to be concrete or stone? Given their width they should be as attractive as possible with the introduction of bands of brick to provide colour and interest.</p> <p>Curbs - Recommend curbs to protect planting areas. Recommend sawn Queenston instead of concrete.</p> <p>Benches - Anticipate a great demand and suggest double benches at the southern end.</p> <p>South African War Memorial - This is the most important section of the project. Treatment here should be intense and consider water, walls and steps.</p> <p>Outer Boulevards - Grass cannot be maintained in satisfactory condition between the sidewalks and the curbs and recommend that “this eight foot strip be treated as concrete paving with sixty-four square feet fo Credit Valley flagging laid dry for each tree.”</p> <p>Planting - To be large, free-growing trees at 100’ spacing in the outer boulevards such as European Linden or Norway Maple. For the central boulevard, dwarf trees such as lilac, flowering cherry, camperdown elms would be suitable. Japanese Yew would have a chance in downtown.</p> <p>Included are sketches for monuments at the northeast and northwest corners of College and Queen’s Park.</p>	Decorative Treatment of University Avenue, Board of Control Report 33, November 18, 1948. COTA, Fonds 2032, Series 721, Files 31/32. See also: 1948.11.16 DGS to Toronto City Planning Board- TPL711.409713 U54_ocr for the original letter
1948	November 17 - The Committee on Works recommended to City Council that approval be given to the proposal submitted to the Toronto City Planning Board for the decorative treatment for University Avenue with flagstone paving at an estimated cost of \$235,000 subject to “decorative treatment being Canadian in nature, particularly the trees...”	Decorative Treatment of University Avenue, Board of Control Report, November 18, 1949. COTA, Fonds 2032, Series 721, Files 31/32.
1949	February 2 - Toronto Planning Board rejects the idea of indentation in the central medians to allow left turning at Dundas and College because the “distortion of the central boulevard would constitute an important interference with the decorative project now under consideration by the City Council which should be avoided if at all possible.”	Letter from Toronto Planning Board to F.D. Tolchard, February 2, 1949. COTA, Fonds 2032, Series 721, Files 31/32.

Date	Notes	Source
1949	February 21 - The Conference is strongly of the view that “Utility must take precedence over symmetry of design if the flow of traffic on University Avenue during rush hours is to be cleared efficiently through the intersection of Dundas and College Streets.	Letter from Toronto Traffic Conference to Tracy D. leMay, February 21, 1949. COTA, Fonds 2032, Series 721, Files 31/32.
1949	March - “In our opinion [Dunington-Grubb] the intersection of Queen Street with its two important monuments, one to the north and the other to the south, is the focal point of the entire University Avenue project. ... An architectural treatment, such as we suggest, with low walls will give a far better setting to these two important monuments than any flat scheme based on hedges.”	Extracts from Correspondence re Decorative Treatment of University Avenue. COTA, Fonds 2032, Series 721, Files 31/32.
1949	May - Toronto City Planning Board advised that the province which had been approached for financial assistance advised that it was opposed to inviting pedestrians to use the central boulevard by constructing a walk. The Board of Trade, on the other hand, felt strongly that the provision of a walk was necessary.	University Avenue Decorative Treatment, COTA, Fonds 2032, Series 721, File ??, p. 1-2.
1949	June 21 - Letter from H.B. Dunington-Grubb to City Planning Board with five alternatives to exclude the public from the central boulevard; any of the schemes can be adopted to the varying widths; Scheme F1 is for a post and chain fence; Scheme F2 is for a hedge with strands of wire inside mounted on low wall; Scheme F4 is for low iron railings on wall; not possible to fence in the monuments. Scheme F1 shows a typical block surrounded by post and chain with formal round headed trees in tree boxes down both sides at fifty foot spacing. The open post and chain treatment makes it possible for the public to see the flower beds inside.	Letter from Dunington Grubb & Stensson to Toronto City Planning Board, June 21, 1949.
1949	June 23 - H.B. Dunington-Grubb provides approximate estimates for Scheme F1; anticipates Queenston flagging; hardscaping ie benches, drinking fountains, water supply, lighting were excluded from the estimates.	Dunington-Grubb & Stensson. University Avenue, Approximate Estimate June 23rd 1949, Scheme F1. COTA, Fonds 2032, Series 721, File 31/32.
1949	June 23 - Letter from the Toronto City Planning Board to the Mayor re the boulevard design which seeks to embody the “thought that the central boulevard on this thoroughfare should not be developed as a pedestrian pathway or promenade because, firstly, of the hazard incurred in crossing the east and west roadways and secondly, because of the doubt whether it would be sufficiently used.” H.B. Dunington-Grubb presented six sketch plans showing various treatments and the Board selected drawing No. 1 as carrying out the most effective way of creating a decorative and dignified thoroughfare. DG suggested round headed trees (lindens) in tree boxes spaced 50; apart with a chain and post barrier between them to prevent pedestrian access. Central treatment is turf with flower beds inside the barrier and between the trees. The ends of the blocks are paved to assist pedestrians crossing the street. Outer boulevards were suggested to be planted with pyramid oaks which fit the formal character of the central treatment but without destroying the view of existing and future buildings which flank the street.	Letter from Tracy D. leMay to H.E. McCallum, June 23, 1949. COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178.

Date	Notes	Source
1949	June 24 - Construction of a landscaped pedestrian boulevard between Queen and College was approved by the Board of Control. It was intended to beautify the approach to Queen’s Park (provide a suitable entrance) and will provide a six foot wide concrete walk lined with trees and lower beds. Planning Commissioner Tracy Lemay was instructed by the Board to discuss the plans with provincial officials.	Toronto Telegram, June 24, 1949, COTA, Fonds 2032, Series 721, Files 31/32.
1949	June 29 - re construction of University as a ‘dual highway’ the city selected a suitable decorative treatment for both the “central boulevard and the outer boulevards adjoining the sidewalks on the east and west sides of the street.”; plans are considered to be finalized; city wants to emphasize the importance of University Avenue as a main approach to the ‘Seat of Government of the Province’ which is a matter of great concern to the province and city; city is appealing for financial assistance and is not without precedent, citing the development of Ottawa as the Federal Capital and the federal government’s involvement; letter provides a short description of the treatment (see June 23 entry); the centre of the boulevard is proposed to be sodded with the exception of a walkway in the middle; special treatment for the South African War Memorial and the Sir Adam Beck Memorial with a view to setting them off to the greatest possible advantage; respecting the outer boulevards - it is proposed that the grass strip between the existing sidewalk and curb be paved in a manner similar to the central boulevard and planted with pyramid-shaped trees; benches to be placed between trees; re illumination - ornamental standards carrying modern luminaire at a mounting height of 25’ on the outer boulevards and designed pillar lights be placed between trees in the central boulevard which will illuminate both the boulevard and adjoining roadways; re hazard for pedestrians previously expressed by the province, the city proposes that this can be minimized by limiting access and by signal control at Queen, Dundas and College streets.	COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178.
1949	July 12 - Plans were reviewed by the Chief Conservation Engineer of the Department of Planning and Development and reported on to the Minister.	Letter from A.H. Richardson to Col. The Hon. W. Griesinger, July 12, 1949. COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178.
1949	August - the design was forwarded to the Provincial Department of Planning and Development, the Conservation Branch of which prepared a modified plan. This included special treatment of the two memorials and the planting of trees on the outer boulevard, eliminated all paving except a narrow safety strip along the outer edges of the central boulevard and at the ends of the blocks were necessary for pedestrian crosswalk. The Board preferred the design as previously forwarded including a central concrete walkway.	University Avenue Decorative Treatment, COTA, Fonds 2032, Series 721, File ??, p. 2.
1949	Sons of England monument is moved to the newly aligned central boulevard.	Art of the Avenue, p. 133, 146.

Date	Notes	Source
1949-1950	Sigmund Samuel Collection Facade created by Maters & Haldenby and Jacobine Jones. Located the south western area of Queen's Park Crescent. Provided by The University of Toronto (UofT) and the Royal Ontario Museum (ROM), subject of theme and historical detail chosen by committee. The creation of the facade was integral to commission for the combined Sigmund Samuel Collection and Ontario Archives Building.	Art of the Avenue, p. 152.
1950	William Lyon Mackenzie monument was entirely removed when the road was relocated.	Art of the Avenue, p. 151.
1950	February: Council adopts Commissioner of City Planning’s suggestions for treatment of outer boulevards and improved street lighting. The grassed areas were vetoed in favour of concrete and flagstone. Lighting to be ‘standard luminaries on suitable poles on the outer boulevards and pillar lighting on each side of central boulevard.”	Art of the Avenue, p. 133.
1950	February 23 - Toronto Works Committee in favour of “the eight-foot strip between the sidewalk and the curb on the west side of University Avenue being concreted with the exception of the places where the trees will be planted. These should be treated with stone flagging over an area of about eight feet square surrounding each tree.” The treatment on the east side should be similar and would necessitate a new concrete walk in place of existing. This was thought advantageous to the hospitals as it removes the sidewalk as far as possible from the buildings and provides better opportunities for landscaping in front of the buildings. Illumination was to take the form of modern luminaire on the outer curbs with pedestal lighting in the central boulevard; the later being for decorative purposes rather than illumination.	Tracy D. leMay, Memorandum for His Worship the Mayor Re: University Avenue Improvements, February 23, 1950. COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178.
1950	March: City Council adopts the proposal to proceed with minimal decorative treatment of central boulevards on University Avenue; under pressures from the Provincial Government to keep it simple and cheap.	Art of the Avenue, p. 133.
1950	March 20 - re entrance driveway for new Sick Children’s Hospital; circular driveway resulting in a semi-circular boulevard area between the hospital and sidewalk; Toronto Planning Board considered the effect on the decorative treatment of University Avenue and determined it would be a desirable feature.	COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178.
1950	April 27 - H.B. Dunington-Grubb writes to the Mayor that the Parks Commissioner is to proceed with a modified version of DGS Scheme F5 which was their last suggestion to the City Planning Board. Dunington-Grubb wants the city to keep them in mind as the project develops should they require assistance.	Letter from H.B. Dunington-Grubb to His Worship the Mayor, April 27, 1950. COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178.
1950	June: City Council hears objections to the above plan from a group of interested parties, whose landscape expert, Mr. P.J. McKenna proposes a much more lavish treatment of said boulevard.	Art of the Avenue, p. 133.

Date	Notes	Source
1950	June 13 - The Canadian Society of Landscape Architects and Planners writes to the Mayor expressing concern that the plan proposed by the Province (Dept. of Planning) is the one for which tenders are being called even though the Planning Board and Board of Trade had approved the DGS plan. “It is inconceivable to the Society that the City of Toronto would proceed with a scheme for such an important thoroughfare, without the approval of the Planning Board or without consulting capable Landscape Architects concerning its suitability. One of the expressed aims of the Society is ‘To stimulate public thought and opinion for outstanding public works”. We would be remiss if we did not protest the development of University Avenue as now proposed.”	Letter from Edward I. Wood to His Worship the Mayor, June 13, 1950. COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178.
1950	June 21 - P.J. McKenna, Consulting Landscape Architect, NYC submits recommendations to the Mayor and Board of Control. Presents two plans; second plan proposes to convert the central boulevard to a traffic artery. Re the outer boulevards, the report notes: “Since there is a greater space between the building line and curb on the east side than on the west side, some modification of this will be necessary in gaining the factor of balance.” The plan proposes Red Maples for the Centre Island and Sugar or Rock Maples for the Boulevards as one of the objectives for this proposed treatment was “To give University Avenue a distinctly Canadian atmosphere, and typical of Canada’s climate.”	P. J. McKenna, Recommendations Relative to the Landscape Planning of University Avenue, City of Toronto, Canada as Presented to the Mayor of the City of Toronto Board of Control, June 21, 1950. COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178.
1950	June 26 - Concern among citizen groups about reconsidering the decorative treatment was forwarded by Mr. H.W.A. Foster who represented the Garden Club of Ontario, the Canadian Society of Landscape Architects and Town Planners, The Men of Trees. Landscape expert Mr. P. J. McKeena of New York City revised the plan (Dept. of Planning) and visited the site, opining that the plan would not give the thoroughfare the distinctive appearance it merits. As a result, city officials are to withhold further action; immediately proceed with sodding the central boulevard, Commissioner of City Planning to consult with H.W.A. Foster and interested groups to work out a suitable and satisfactory plan.	Board of Control Report No. 22, Council Approval, June 26, 1950. COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178.

Date	Notes	Source
1950	<p>July 11 - A special meeting was held by the Commissioner of City Planning to discuss the decorative treatment; the minutes include a list of attendees; leMay provided a synopsis of events to date:</p> <p>-1947 Board of Control request the Toronto City Planning Board prepare a scheme for decorative treatment</p> <p>-Board decided treatment should be rural (trees and grass) or formal (flower beds, shrubs, flagstone) and since there were few opportunities for formal gardens in the city, the Board instructed DGS to prepare sketches</p> <p>-1948 Dunington-Grubb & Stensson presents sketches to the Board; treatment considered too complicated and lacking the dignity which University Avenue demanded</p> <p>-Dunington-Grubb revised the sketches and these were approved by the Planning Board, City Council and the Committee on Parks</p> <p>-when submitted to the provincial government, however, it objected to the promenade in the central median as they thought it was a traffic hazard</p> <p>-city engages with Advisory Planning Committee of the Board of Trade</p> <p>-Dunington-Grubb & Stensson submits another plan and when presented to the provincial government it was thought to be too expensive and the sketch redrawn by the Dept. Planning and Development to eliminate the tree boxes bringing to estimate down to \$58,000 of which the province would contribute \$30,000</p> <p>-this plan was accepted by City Council</p>	<p>Minutes of a Special Meeting Held on Tuesday, July 11th 1950. COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178.</p>
1950	<p>July 20 - Tracy D. leMay engages E.G. Faludi to study the matter of the decorative treatment and to forward a report setting our recommendations.</p>	<p>Letter from Tracy D. leMay to E.G. Faludi, July 20 1950. COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178.</p>
1950	<p>September 1 - Faludi of Town Planning Consultants writes to leMay that the study is complete and recommending materials for a presentation and that after approval that the materials be exhibited for the general public.</p>	<p>Letter from E.G. Faludi to Tracy D. leMay, September 11, 1950. COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178.</p>
1950	<p>October 3: Faludi Consultants' study proposes Linden trees to be planted on the outer boulevards, and sloped flagstone paving and planting beds to be added to the central boulevard. The Parks Board then proposes a much simpler treatment; this latter was accepted by Council. The report includes a summary of previous proposals for design, memorials and plantings. The centre boulevards were designed for decorative purposes and not use, as crossing traffic is hazardous.</p>	<p>Art of the Avenue, p. 134. COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178. See also: https://drive.google.com/file/d/19tgysE4j8MH_U-8OmlKyaSuTYAHyqcoa/view?usp=sharing For an additional version of the report</p>

Date	Notes	Source
1950	<p>October 4 - City Planning Board recommends that the Works Committee approve the Faludi plan. Letter contains a summary of the proposed treatment.</p>	<p>Letter from Tracy D. leMay to Alderman Cowling, October 4, 1950. COTA, Fonds 2032, Series 721, File 31, Box 143177 and File 32, Box 143178.</p>
1953	<p>“Bill 80 for the creation of Metropolitan Toronto was introduced into the Legislature in 1953 and upon enactment</p> <p>became The Municipality of Metropolitan Toronto Act now Chapter 73 of the Statutes of Ontario, 1953. This Act provides for a federal system of municipal government. The area municipalities retain their autonomy in respect of local matters and have representation on the Metropolitan Council which is responsible for the provision of the Metropolitan services.”</p>	<p>Metropolitan Toronto, Annual Report, 1953, p. 2.</p>
1954	<p>Plans and discussion concerning the proposed University Avenue subway line begin.</p>	<p>Art of the Avenue, p. 134.</p>
1954	<p>January - The Toronto Transportation Commission (TTC) assumed responsibility for providing public passenger transportation services in the metropolitan area.</p>	<p>THA, TTC Heritage Screening Report, November 2014, p. 40.</p>
1956	<p>Metropolitan Toronto council authorized the TTC to engage consulting engineers to prepare functional plans for the Bloor Street and University Avenue lines.</p>	<p>THA, TTC Heritage Screening Report, November 2014, p. 41.</p>
1955-1957	<p>Robert H. Saunders Memorial created by K.H. Candy and Emanuel Hahn. Located south of College Street facing north. Following Saunders' death, an ad hoc public committee (including mayor and Metropolitan Toronto chair) raised funds by direct solicitation, from various municipal hydro utilities. The Ontario Hydro employees, and the commissioned artist were to work with the Ontario Hydro Commission architect's office for the installation and creation of this piece.</p>	<p>Art of the Avenue, p. 154.</p>
1957	<p>Bank of Canada Building north and south sides designed by Cleeve Horne. Located at the south western area of Richmond and Adelaide Block. This design was integral to the commission for building for architects Marani, Morris and Alan.</p>	<p>Art of the Avenue, p. 153.</p>
1957	<p>December - City Toronto Planning Board report as a response to requests of the Technical Sub-Committee of the Toronto Traffic Conference through the City Traffic Engineer about the possibility of extending University Avenue southerly from Front Street to connect with York Street at the tunnel, or at its extension to Fleet Street; question of how to connect to the Lakeshore Expressway.</p>	<p>Toronto City Planning Board, University Avenue Extension at Front Street, December 1957.</p>
1958	<p>McClelland and Stewart Building, north and south sides, designed by Elizabeth Wyn Wood. Located at the southeastern area of the Edward and Dundas block. This design was integral to the commission for the McLean Hunter Building architects Marani, Morris and Alan.</p>	<p>Art of the Avenue, p. 155.</p>

Date	Notes	Source
1958	The Ontario Municipal Board (OMB) authorized the TTC to construct the Bloor-Danforth and University Avenue subways in three stages over a period of ten years. Stage I to be undertaken immediately was the extension of the Yonge Street subway up University Avenue from Union Station to Bloor Street and construction of the west leg of a wye connecting to the Bloor-Danforth subway. Stage II to commence not before January 1, 1962 and to include the Bloor-Danforth subway between St. George Street and Greenwood Avenue and the east leg of the wye from the University subway. Stage III to commence not before January 1, 1967 and include sections of the Bloor-Danforth subway from St. George Street to Keele Street and from Greenwood to Woodbine.	Toronto Transit Commission, Annual Report 1958, p. 10. COTA, Fonds 16, Series 1591.
1959	November 16 - Premier Leslie Frost broke ground for the Stage I - University Line. Construction was let in three contracts: Contract U-1: Cut and cover section from Front Street to Queen Street awarded to Johnson-Perini-Kiewit in the amount of \$6,454,000. Contract U-2: Tunnel section from Queen Street to the north end of Queen's Park Crescent - awarded to Robert McAlpine in the amount of \$7,659,000. Contract U-3: Cut and cover section from Queen's Park to Huron including the west leg of the wye connection awarded to McNamara-Raymond in the amount of \$6,874,000.	Toronto Transit Commission, Annual Report 1959, p. 13. COTA, Fonds 16, Series 1591.
1960	Bloor-Danforth-University Subway line construction announcement in Metro Toronto Report. Says the Metropolitan Toronto and the Toronto Transit Commission sharing the cost of the \$200,000,000 project. The line will be 10 miles with 25 stations and 280 subway cars. The first stage of construction is the University Line from Union Station to Saint George at Bloor, and it will require 48 subway cars and the construction will take 3 ½ years.	1960 Metropolitan Toronto Report, p. 24-25; p. 25 has an illustration of the stages.
1960	Toronto General Hospital, Urquhart Wing Facade designed by Jacobine Jones. Located at the southern area of College Street. Provided by the Toronto General Hospital (TGH). This design was integral to commission for the Urquhart Wing for architects Mathers and Haldenby.	Art of the Avenue, p. 156.
1960	Sir Adam Beck statue removed for subway construction, concrete base destroyed and recreated in marble (Elizabeth Wyn Wood consultant).	Art of the Avenue, p. 150.
1960	South African War Memorial moved south in-line with "the terrace in front of Osgoode Hall."	Art of the Avenue, p. 144.

Date	Notes	Source
1961	February - Dunington-Grubb & Stensson engaged by the Metro Toronto Planning Board to prepare a plan for suitable landscape treatment that “would be in harmony with any plans of the City of Toronto and the Province of Ontario for the Queen’s Park area.” During preparation of the plan, meetings with the Parks Commissioner were arranged and several principles emerged: “The continuity and importance of University Avenue is emphasized by rows of similar trees running along its length. Trees are proposed on both sidewalks and on the centre boulevard. The trees proposed for the sidewalks are relatively high compared to those proposed for the centre boulevard.” “The Avenue as a whole should be carefully integrated with open spaces and development on either side. Of particular importance are Queen’s Park at the northern end and the Civic Square with its two entries north and south of Osgoode Hall. In addition, there is a possibility of widening on the south side of Dundas Street westwards from University Avenue to the Art Gallery in Grange Park. At the southern end of the Avenue a terminal feature should, if possible, be achieved at Front Street. The overall composition would thus be the major Avenue extending from Queen’s Park to the terminal feature on Front Street, with the major extensions east and est into the Civic Square and Grange Park In addition, there is landscaping in front of many existing buildings on the Avenue and this should, as far as possible be integrated into the design.” “Careful consideration should be given to the landscaping of sidewalks as well as to the centre boulevard.” “The centre boulevard should provide for pedestrian usage.” “An integrated scheme of lighting shall be considered by the architect in consultation with lighting experts.” The Planning Boards makes three recommendations: Greater visual integration between University Avenue and Nathan Phillips Square Consider the centre boulevard as something to be considered from afar for its visual appreciation rather than for use by pedestrians. More detail about plantings on outer sidewalks and adjoining buildings	Report from M.V. Jones, Commissioner of Planning Metropolitan Planning Board, October 13, 1961.
1961	June - Dunington-Grubb & Stensson approached by the MTPB to prepare preliminary sketches with a contract being entered into in September; Dunington-Grubb & Stensson was employed to design the landscape, assist and advise on coordination of plans with the city and provide general supervision of work when the contract was awarded.	Revised draft letter from Dunington-Grubb & Stensson to Executive Committee, Municipality of Metropolitan Toronto, October 6, 1964. COTA, Fonds 416, File 30, Box 576277.

Date	Notes	Source
1961	The landscape plan drawing is dated September 1961 and a subsequent report states that the landscaping plan was presented to the Metropolitan Parks and Recreation Committee at its meeting on September 21, 1961.	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 1. COTA Fonds 416, File 33, Box 576277.
1961	Existing Conditions: University Avenue is one mile in length and about 180 feet wide; central boulevard varies in width from 50’ at Queen Street to 25’ at College Street; average width of 16’ from curb to property line on each side and a rise of 75 feet from Front Street to College Street; central boulevard is broken up into 12 islands of various lengths and has been treated in the past as a plan grassed area from curb to curb with a row of crab trees down the centre and some flower beds. There are four monuments: 1) Sir Adam Beck south of Queen Street; 2) South African monument north of Queen Street; 3) Sons of England memorial at Elm Street; 4) Robert Saunders monument at College Street.	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 1. COTA Fonds 416, File 33, Box 576277.
1961	Consideration to be given to the air vents servicing the subway. They are mostly circular grids 10 feet in diameter occurring at various points in some of the islands. Most are treated inconspicuously and are flush to the ground; the exception being Island E and the large, rectangular vents in Islands B and K which are proposed to be screened by hedges.	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 1. COTA Fonds 416, File 33, Box 576277.

Date	Notes	Source
1961	<p>The plan considers the issue of winter damage and splash from cars; it is proposed to create a low wall 1’ to 3’ high down each side set back two feet from the curb; the wall could form the outer boundary of raised planting beds; a row of formal trees such as Mophead Elms space about 40’ apart the length of the centre boulevard will provide continuity especially if planted in individual boxes; for the outer boulevards, larger trees such as Lindes growing to 40 feet and spaced 100’ (due to underground services) should be planted.</p> <p>Access to all islands should be provided between raised beds; people can sit and enjoy the outdoors while protected by the flower boxes and plantings.</p> <p>Levels: the use of walls will make it possible to break up some of the islands into levels necessitating small flights of steps which provide a decorative effect.</p> <p>Pavements: Design and colour is one of the most important features, especially colour. “Precast slabs of mosai (exposed aggregate) in various colours and textures mixed with brick in design will brighten the effect; consideration should be given to replacing the outer sidewalks [boulevards] with paving in line with that in the centre boulevard.</p> <p>Walls: DGS provided direction that walls shall be in natural materials with a backing of precast reinforced concrete faced with limestone, granite, slate or brick.</p> <p>Furniture: benches and pots are the furniture of gardens; benches are introduced ‘for use and needed detail’; pots are planted with flowers or evergreen are for ‘detail alone.’</p> <p>Grass: interior of the islands will be treated in pavement and grass panels in design.</p> <p>Lighting: underwater lighting for the pool and fountains is proposed; monuments to be lighted from existing standards in outer boulevards; centre island to be lighted by recessed fixtures in the interior walls; special lighting outlets for Christmas; consideration should be given to replacing the existing standards on the outer boulevard with those of a more modern design.</p> <p>Plan revised to September 1961 included on p. 9 (p. 10 of the pdf).</p>	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, pp. 1-2. COTA Fonds 416, File 33, Box 576277.
1961	Island A - Design shows beds of wide hedges surrounded by brick curb; all the rest is paving except for Mophead Elms at 40’ spacing.	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 3. COTA Fonds 416, File 33, Box 576277.

Date	Notes	Source
1961	Island B - Including Adam Beck Memorial. Design calls for a sunken court north of the monument surrounded by beds raised on walls and six benches; on the south is a vent surrounded by hedge, 8 Mophead Elms and a small pool; the remainder is paving except grass in three small panels.	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 3. COTA Fonds 416, File 33, Box 576277.
1961	Island C - This is the most important island and is dominated by the South African War Memorial; model has been made showing a pool to the south with three fountains and a sunken central area to the north flanked by raised beds; Mophead Elms are planted in separate boxes of brick; interior is treated as pavement with grass panels; the monument has been moved further south so that it comes now on axis with the terrace in front of Osgoode Hall.	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 3. COTA Fonds 416, File 33, Box 576277.
1961	Island D - Design of tree boxes, pots, grass and paving with steps and raised beds. Same as Islands E, F, G, H, I and J.	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 3. COTA Fonds 416, File 33, Box 576277.
1961	Island E - Design of tree boxes, pots, grass and paving with steps and raised beds. Same as Islands D, F, G, H, I and J. Additionally Island E there is a vent with seats which can be used as a central feature for a sculpture court. This court could be lit by high standards. Perspective sketch on p. 7 (p. 8 of pdf).	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 3. COTA Fonds 416, File 33, Box 576277.
1961	Island F - Design of tree boxes, pots, grass and paving with steps and raised beds. Same as Islands D, E, G, H, I and J. Perspective sketch on p. 8 (p. 9 of pdf).	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 3. COTA Fonds 416, File 33, Box 576277.
1961	Island G - Design of tree boxes, pots, grass and paving with steps and raised beds. Same as Islands D, E, F, H, I and J.	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 3. COTA Fonds 416, File 33, Box 576277.

Date	Notes	Source
1961	Island H - Design of tree boxes, pots, grass and paving with steps and raised beds. Same as Islands D, E, F, G, I and J. Perspective sketch on p. 6 (p. 7 of pdf).	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 3. COTA Fonds 416, File 33, Box 576277.
1961	Island I - Design of tree boxes, pots, grass and paving with steps and raised beds. Same as Islands D, E, F, G, H, and J.	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 3. COTA Fonds 416, File 33, Box 576277.
1961	Island D - Design of tree boxes, pots, grass and paving with steps and raised beds. Same as Islands E, F, G, H, and I.	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 3. COTA Fonds 416, File 33, Box 576277.
1961	Island K - Island has been developed with walls in Queenston and Credit Valley paving; DGS suggest hedges on the south to enclose the air vent.	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 3. COTA Fonds 416, File 33, Box 576277.
1961	Island L - Due to the slower speed of traffic around this island north of College Street walls have been omitted and the design limited to grass panels and Mophead Elms in large boxes separated by paving.	Dunington-Grubb and Stensson, University Avenue, Municipality of Metropolitan Toronto: A Plan for its Landscape Treatment, c1961, p. 3. COTA Fonds 416, File 33, Box 576277.
1961	<i>Metro Toronto Report explains their Plan of Residential Development for the city. This plan includes the expansion of the under construction University Subway line.</i>	1961 Metropolitan Toronto Report, p. 5.
1961	<i>Metro Toronto Report gives an update on the construction of the subway tunnels along University Avenue. States “...progress on the tunnel section has been satisfactory and the 16-foot pilot (right of way tunnels) are completed, except for the concrete floors, from the south end of Queen's Park Crescent to Osgoode Station...it is fully expected the job will remain on schedule for the opening in the Spring of 1963.” Cut-away diagram showing portions which are cut and cover and which are tunnel.</i>	1961 Metropolitan Toronto Report, p. 24.

Date	Notes	Source
1961	October 30 - Letter from A. Douglas Ford, Commissioner to W.W. Gardhouse, City Clerk notes that “University Avenue has been designated as an arterial highway in the Metropolitan roads system and the new connection to York Street...”	Letter from A. Douglas Ford, Commissioner to W.W. Gardhouse, October 30, 1961.
1961	December 12: The Dunington-Grubb & Stensson landscaping proposal for University Avenue is accepted by Metropolitan Toronto.	Art of the Avenue, p. 134; See also Memo, Metropolitan Parks Department, May 28, 1962.
1961	200 University Avenue is built. Its design reflects John C. Parkin’s successful challenge to the provisions of the 1931 by-law, as seen in curtain wall, anodized aluminum materials,	200 University Avenue - Notice of Intention to Designate, p. 12.
1962	<i>Metro Toronto Report says new Chairman Gardiner will pursue a “... total transportation policy.. Expand the rapid transit system, which now consists of five miles of subway along Yonge Street...including the first two-mile-section of the University connection expected to be in operation early 1963.”</i>	1962 Metropolitan Toronto Report, p. 10.
1962	<i>Metro Toronto Report gives an update on construction of subway line down University Avenue, it says, “Construction of the Bloor-Danforth-University Subway was started in September 1959...The 2-mile University section is nearing completion, and tiled walls, polished terrazzo floors, soundproof ceilings and escalators are installed at all five stations on the line.”</i>	1962 Metropolitan Toronto Report, p. 25.
1962	October - City awards contract for University Avenue landscaping to Conniston Construction; value is \$474,900; contract signed November 27, 1962 and called for completion within one year.	Letter from Dunington-Grubb & Stensson to the Municipality of Metropolitan Toronto, October 6, 1964, p. 1. COTA Fonds 416, File 30, Box 576277.
1963	February 28: University Avenue subway is officially opened although it was substantially complete by the end of 1962. Starting at the Saint George Station on the Bloor line, it followed University Avenue connecting with the Yonge Street line at Union Station. A system of underground parking lots were built from Front Street to King Street, with access ramps at the Front Street/University Avenue intersection. Note: until the subway opened there was no public transit on University Avenue.	Art of the Avenue, p. 134.
1963	“A 2 mile extension, from Union Station, underneath University Avenue to Bloor Street and Saint George” successfully added to the Yonge Street subway.	Metropolitan Toronto 1953-1963, 10 Years of Progress, p. 25
1964	12 landscaped islands in the central median completed.	COTA, Aerial Photograph 1964, Sheet 48.
1964	The bronze statue of Sir Adam Beck reinstalled after subway opening.	Art of the Avenue, p. 150.
1965	Toronto's new City Hall and Nathan Phillips Square completed.	City of Toronto Heritage Register
1966	McMurtry Gardens of Justice completed at the Superior Court of Justice (361 University Avenue).	COTA, Aerial Photograph 1966, Sheet 44.

Date	Notes	Source
1968	Following meetings with the Chief Planner on design controls for University Avenue, the Ontario Association of Architects provided feedback on the new median design. The OAA recommended informal plantings of massive trees down the centre of the islands, “ignoring the ‘landscaping’ which would we hope eventually disappear.” OAA questioned the aesthetic treatment of the design, and found it underused by pedestrians, and out of scale / lacking a meaningful relationship with the larger Avenue.	City of Toronto Planning Board. A Review of the University Avenue By-law, pp. 45-51.
1969	Edward VII Equestrian Statue created by Sir Thomas Brock. Located in the northern area of Queen’s Park. Provided by The Toronto Historical Board (THB). This statue was originally created in 1911 and installed in Delhi India in 1919. It was brought to Toronto at personal expense (\$10,000) by H.R. Jackman. Jackman offered, through the mayor, to donate it to the City if the City would pay the installation costs.This was rejected by Council, and \$18,000 was raised by public subscription through the mayor’s office. Public debate was created over the appropriateness of subject and location. Ultimately it was accepted by Council on recommendation of the Committee on Parks and Recreation and the site chosen by the same.	Art of the Avenue, p. 157.
1969	Board of Control abolished.	https://en.wikipedia.org/wiki/Toronto_Board_of_Control
1970	Flight II created by Sorel Etrog. Located south of Wellington Street West (Marathon Realty). Work was created between 1965-1966 but was purchased for the property by Marathon Realty.	Art of the Avenue, p. 158.
1970s	Mayor Art Eggleton proposes introducing commercial uses, retail and restaurants on University Avenue, creating public life on the street. First proposals for an ‘Avenue of Light’ (floodlit buildings etc).	Art of the Avenue, p. 134.
1971	February - COT Planning Board review of University Avenue bylaw. Review required because of the number of surface parking lots; numerous exceptions had been granted; the University Avenue By-law Advisory Committee questioned the validity of their work; doubts about the quality of the Avenue. Diagram 1 depicts existing land use; Diagram 2 depicts existing surface parking lots.	1971.02 A review of the University Avenue by-law, City of Toronto Planning Board, p. 5.
1971	February - COT Planning Board recommendations for: trees and plantings; lights; median strip; floor surfaces; open spaces (outer boulevard); pedestrian movements; functional relationships; furnishings.	1971.02 A review of the University Avenue by-law, City of Toronto Planning Board, pp. 45-49.
1972	October: Metropolitan Toronto appoints the Metropolitan Parks Commissioner as a member of the University Avenue By-Law Advisory Committee, and adopts Planning Commissioner’s proposals regarding pedestrians, central boulevard improvements, land use changes, sign control and landscaping.	Art of the Avenue, p. 134.
1983	Mary Pickford statue created by Eino. Located at the northeast corner of Elm Street on the location of the Hospital for Sick Children. It was commissioned by the Mary Pickford Foundation and donated to the hospital by the same to accompany. The included plaque was installed in 1972 to mark Pickford’s birthplace. Site found to be on street right-of-way, and the hospital sought and received Metropolitan Toronto’s approval before installation.	Art of the Avenue, p. 159.

Date	Notes	Source
1984	The Endless Bench created by Lea Vivot. Located on the southeast area of the Gerrard and Elm, on the location of The Hospital for Sick Children. The piece was donated to the hospital by the artist, assisted by funds she raised by public subscription (\$15,000), from Kleinburg City Council (\$10,000), Metropolitan Toronto(\$5,000), and Toronto City Council (\$10,000). The unveiling was at the Hospital's official sesquicentennial event.	Art of the Avenue, p. 161.
1984	Canadian Airmen’s Memorial created by Oscar Nemon. Located south of Dundas Street facing North. Donated by H.R. Jackman who died in 1979. It was realized by the Jackman Foundation and after his death an ad hoc public committee including Metropolitan Toronto Commissioner of Parks and Property raised funds for installation.	Art of the Avenue, p. 163
1984	Pas de Trois created by Russell Jacques. Located on the northwest corner of Wellington Street. Provided by Hammerson Canada Incorporated. The piece was commissioned for the site through an initiative of the president of Hammerson Canada Incorporated. The artist was purportedly selected through limited competition and the total cost was \$75,000.	Art of the Avenue, p. 160.
1984	Triad created by Ted Bieler. Located on the southwest corner of Front Street (Marathon Realty). Limited competition occurred for this design and the selection was done by Marathon Realty and Parkins Partnership,the architects of the building. The piece was unveiled at Marathon Realty’s sesquicentennial event.	Art of the Avenue, p. 162.
1984	Sunlife designed by Sorel Etrog. Located on the northeast corner of King Street (Sun Life Building). Commissioned for the site by Sun Life Assurance Company of Canada.	Art of the Avenue, p. 164.